

**Planning Justification Report
Official Plan Amendment & Zoning By-law Amendment
3355 The Collegeway
City of Mississauga**

1.0 INTRODUCTION

Glen Schnarr & Associates Inc. has been retained 3355 The Collegeway G.P. Inc. to assist in obtaining the necessary planning approvals to allow for the redevelopment of the subject property located at the northeast corner of the intersection of The Collegeway and Ridgeway Drive in the City of Mississauga. The development proposal includes 336 stacked townhouses within eleven (11) blocks with one vehicular access to Colonial Drive, as well as a retail and office commercial building with a gross floor area of 1123.34 square metres with access from Ridgeway Drive.

The purpose of this report is to outline the nature of the proposed development and to evaluate the proposal in the context of the policies of the Provincial Policy Statement, the Growth Plan for the Greater Golden Horseshoe, the Region of Peel Official Plan, and the City of Mississauga Official Plan and Zoning By-law 0225-2007.

2.0 SITE DESCRIPTION AND SURROUNDING LAND USES

The subject property is located at the northeast corner of the intersection of The Collegeway and Ridgeway Drive in the City of Mississauga, legally described as PCL PLAN-1, SEC 43M695, BLK 1, PL 43M695. The subject property has a frontage of approximately 218 metres on The Collegeway, and of 91 metres on Ridgeway Drive. The property has a total site area of 26,606 square metres (2.66 hectares). The subject property currently contains a one-storey multi-unit retail commercial plaza that was constructed in 1990.

Surrounding land uses include townhouse and apartment dwellings to the south, two-storey commercial and office uses to the west, apartment dwellings to the north, and low-density residential to the east.

2.1 Transit Service

The subject property is serviced by MiWay bus route 36 seven days per week and route 47 on weekdays. Both routes provide a connection to and from the South Common Centre commercial plaza where connections can be made to routes 1C, 13, 26, 29, 31, 36, 47, 48, 101, 110, as well as Oakville Transit route 24. These routes provide connections between the subject property and nearby GO Transit stations (Clarkson, Erindale and Square One) for regional transit service as well as the nearby University of Toronto Mississauga Campus. The nearest bus stops from the subject property are located at the intersection of The Collegeway and Ridgeway Drive.

3.0 DEVELOPMENT PROPOSAL

3355 The Collegeway G.P. Inc. is proposing to develop the subject property for 336 stacked townhouse units (horizontal multiple dwellings) within 11 blocks with one vehicular access to Colonial Drive. Each horizontal multiple dwelling block is proposed contain either 24, 32 or 40 units. A total of 324 dwelling units will include 2-bedrooms and 12 dwelling units will include 3-bedrooms. The development (residential and commercial combined) will represent a total floor space index (FSI) of 1.4, with the residential-only portion of the development having a proposed FSI of 1.62.

The commercial component of the development, located at the northeast corner of the intersection of The Collegeway and Ridgeway Drive, will include 1123.34 square metres of commercial gross floor area with to be divided between retail uses at-grade and office uses in the second storey.

Resident parking for the proposed horizontal multiple dwellings is provided at 1.1 spaces per 2-bedroom unit and 1.7 spaces per 3-bedroom for a total of 344 at-grade spaces and 35 surface spaces. A total of 45 at-grade spaces are provided in the commercial portion of the development to be shared between visitors of the residential development and the commercial building. It is intended for an additional 18 spaces in the parking area provided for the commercial development to be reserved for purchase by owners of the horizontal multiple dwellings.

The development proposal includes a total proposed amenity area of 3,144 square metres throughout the site. This includes a contiguous outdoor amenity area of 1,120 square metres in the centre of the residential portion of the development. The subject property is within very short walking distance of Tom Chater Memorial Park which provides recreational open space including baseball and soccer playing fields. The subject property is also within walking distance of a small local park at the intersection of Colonial Drive and Huxley Drive which has a playground.

4.0 LAND USE POLICIES

4.1 Provincial Policy Statement 2014

The Provincial Policy Statement (PPS 2014) provides policy direction on matters of provincial interest related to land use planning and development. Section 3 of the *Planning Act* requires that decisions affecting planning matters “shall be consistent with” policy statements issued under the Act. Section 1 of the PPS outlines policies associated with future development and land use patterns. Policies in Sections 1.1.1 and 1.1.3 state:

“1.1.1 Healthy, liveable and safe communities are sustained by:

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including*

- places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
 - e) promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;*
- 1.1.3.1 Settlement areas shall be the focus of growth and their vitality and regeneration shall be promoted;*
- 1.1.3.2 Land use patterns within settlement areas shall be based on:*
- a) densities and a mix of land uses which:*
 - 1. efficiently use land and resources;*
 - 2. are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
 - 3. minimize negative impacts to air quality and climate change, and promote energy efficiency;*
 - 4. support active transportation;*
 - 5. are transit-supportive, where transit is planned, exists or may be developed; and*
 - 6. are freight-supportive; and*
- 1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.*
- 1.1.3.4 Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.*
- 1.1.3.6 New development taking place in designated growth areas should occur adjacent to the existing built-up area and shall have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.”*

The proposed development conforms to Sections 1.1.1 and 1.1.3 as it is located within a Settlement Area and contributes to the range of land uses within the Mississauga Official Plan. The proposed development provides heights and densities that are compatible with land uses in the immediate neighbourhood which already contains medium and high density residential developments. The proposal represents an efficient development and land use pattern in which existing services can accommodate the proposed development.

Section 1.4 of the PPS 2014 provides policies relating to housing:

- “1.4.1 To provide for an appropriate range and mix of housing types and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:*
- a) maintain at all times the ability to accommodate residential growth for a minimum of 10 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development;*
- 1.4.3 Planning authorities shall provide for an appropriate range and mix of housing types and densities to meet projected requirements of current and future residents of the regional market area by:*
- d) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use;*
- e) establishing development standards for residential intensification, redevelopment and new residential development which minimize the cost of housing and facilitate compact form, while maintaining appropriate levels of public health and safety.”*

The proposed development of stacked townhouse dwellings represent an appropriate form of low-rise, medium density infill within an existing neighbourhood that provides more affordable market housing options for the community while minimizing the impact on existing services and infrastructure. The proposed development therefore conforms to Section 1.4 of the PPS 2014.

The following policies in Section 1.6 relate to land use and transportation and infrastructure facilities:

- “1.6.7.2 Efficient use shall be made of existing and planned infrastructure, including through the use of transportation demand management strategies, where feasible;*
- 1.6.7.4 A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.”*

The proposal conforms to Section 1.6 as the proposed development will be serviced using existing infrastructure and roads. The subject property is also located in close proximity to multiple existing local bus public transit routes. The proposed development will be integrated into the existing pedestrian sidewalk system to provide direct pedestrian connection from abutting roads.

4.2 Growth Plan for the Greater Golden Horseshoe, 2006 (Office Consolidation, June 2013)

The Growth Plan for the Greater Golden Horseshoe, 2006 (Office Consolidation, June 2013) (Growth Plan) is intended to be a framework for implementing strong prosperous communities through managing growth in the region to 2031. The Growth Plan policies relevant to the proposed development include Policies 2.2.2 ‘Managing Growth’ and 2.2.3 ‘General Intensification’ which state:

“2.2.2 Managing Growth

- 1. Population and employment growth will be accommodated by –
 - a. directing a significant portion of new growth to the built-up areas of the community through intensification*
 - b. focusing intensification in intensification areas*
 - c. reducing dependence on the automobile through the development of mixed-use, transit-supportive, pedestrian-friendly urban environments*
 - d. providing convenient access to intra- and inter-city transit*
 - e. planning and investing for a balance of jobs and housing in communities across the GGH to reduce the need for long distance commuting and to increase the modal share for transit, walking and cycling*
 - f. encouraging cities and towns to develop as complete communities with a diverse mix of land uses, a range and mix of employment and housing types, high quality public open space and easy access to local stores and services*
 - g. directing development to settlement areas, except where necessary for development related to the management or use of resources, resource-based recreational activities, and rural land uses that cannot be located in settlement areas*
 - h. directing major growth to settlement areas that offer municipal water and wastewater systems and limiting growth in settlement areas that are serviced by other forms of water and wastewater services**

2.2.3 General Intensification

- 1. By the year 2015 and for each year thereafter, a minimum of 40 per cent of all residential development occurring annually within each upper- and single-tier municipality will be within the built-up area.*
- 7. All intensification areas will be planned and designed to –
 - a. cumulatively attract a significant portion of population and employment growth*
 - b. provide a diverse and compatible mix of land uses, including residential and employment uses, to support vibrant neighbourhoods*
 - c. provide high quality public open spaces with site design and urban design standards that create attractive and vibrant places*
 - d. support transit, walking and cycling for everyday activities*
 - e. generally achieve higher densities than the surrounding areas**

f. achieve an appropriate transition of built form to adjacent areas.”

The proposed development promotes the policies of the Growth Plan as it represents residential infill development in a settlement area with existing municipal water and wastewater systems. The proposed development is located in close proximity to local public transit service which allows for the opportunity for higher-order transit connections. The Mississauga Official Plan identifies The Collegeway and Ridgeway Drive as ‘Major Collector’ roads and Colonial Drive as a ‘Minor Collector’ road. The proposed development has a compact built-form which will help to reduce dependence on the automobile in being located along these identified road networks.

4.3 Region of Peel Official Plan (Office Consolidation October, 2014)

The Region of Peel Official Plan provides guidance to the area municipalities in the preparation and implementation of their local Official Plans. The subject property is located within the “Urban System” designation in the Region of Peel Official Plan. General Objectives and General Policies of the “Urban System” relevant to the proposed development include:

- “5.3.1.2 To achieve sustainable development within the Urban System.
- 5.3.1.3 *To establish healthy urban communities that contain living, working and recreational opportunities, which respect the natural environment, resources and the characteristics of existing communities;*
- 5.3.1.4 *To achieve intensified and compact form and a mix of land uses in appropriate areas that efficiently use land, services, infrastructure and public finances while taking into account the characteristics of existing communities and services;*
- 5.3.1.5 *To achieve an urban structure, form and densities which are pedestrian-friendly and transit-supportive;*
- 5.3.2.2 *Direct urban development and redevelopment to the Urban System within the 2021 Regional Urban Boundary, as shown on Schedule D, consistent with the policies in this Plan and the area municipal official plans; and*
- 5.3.2.3 *Plan for the provision and financing of Regional services so as to efficiently use existing services and infrastructure, and generally accommodate a pattern of compact forms or urban development and redevelopment.*
- 5.3.2.6 *Direct the area municipalities, while taking into account the characteristics of existing communities, to include policies in their official plans that:*
 - a) support the Urban System objectives and policies in this Plan;*
 - b) support pedestrian-friendly and transit-supportive urban development;*
 - c) provide transit-supportive opportunities for redevelopment, intensification and mixed land use;”*

The proposed development conforms to the Region of Peel Official Plan as it provides for living opportunities and contributes to the mix of land uses in the Urban System which make efficient use of land, thereby having respect for the natural environment and resources. The proposed development is pedestrian friendly as it is to be integrated into the existing and proposed public sidewalk system along adjacent Major and Minor Collector Roads and is transit-supportive as it is in close proximity to an existing bus routes. No amendment to the Region of Peel Official Plan is required to allow for the proposed development.

4.4 City of Mississauga Official Plan (Office Consolidation March 11, 2016)

The City of Mississauga Official Plan was adopted by City Council on September 29, 2010 and partially approved by the Region of Peel on September 22, 2011. The entire Official Plan was subsequently appealed to the Ontario Municipal Board (OMB) but is now in partial effect as of March 11, 2016 except for specific policies that are still under appeal. The Mississauga Official Plan sets out goals, objectives and policies to guide development of the City.

4.4.1 Land Use Designations

The subject property is designated “Mixed Use” on Schedule 10 – Land Use Designations and is identified with the “Neighbourhood” designation on Schedule 1 – Urban System. Chapter 16 – *Neighbourhoods* identifies the subject property within the Erin Mills Neighbourhood Character Area. Within the Erin Mills Neighbourhood Character Area section in the Official Plan, there are no general or site specific policies applicable to the subject property.

Section 11.2.6 of the Mississauga Official Plan establishes policies for the Mixed Use designation. The Mixed Use designation permits the following uses:

- a. commercial parking facility;
- b. conference centre;
- c. entertainment, recreation and sports facility;
- d. financial institution;
- e. funeral establishment;
- f. motor vehicle rental;
- g. motor vehicle sales;
- h. overnight accommodation;
- i. personal service establishment;
- j. post-secondary educational facility;
- k. residential;
- l. restaurant;
- m. retail store; and
- n. secondary office

The Mixed Use designation does not permit the following uses:

- a. self-storage facility; and
- b. detached and semi-detached dwellings

Other applicable policies listed for the Mixed Use designation include:

- “11.2.6.2 *Lands designated Mixed Use will be encouraged to contain a mixture of permitted uses.*
- 11.2.6.3 *Mixed Use development will be encouraged through infilling to consolidate the potential of these areas and to restrict their linear extension into stable, non-commercial areas.*
- 11.2.6.4 *Residential uses will be combined on the same lot or same building with another permitted use.*
- 11.2.6.5 *Residential uses will be discouraged on the ground floor.*
- 11.2.6.6 *Notwithstanding 11.2.6.4 and 11.2.6.5, development applications proposing residential uses that are not combined in the same building with another permitted use may be required to submit a development master plan to the City’s satisfaction.*

Although both commercial and residential uses are proposed, the horizontal multiple dwellings and a separate commercial building (which would be proposed to be severed into separate properties at a later date) would not be permitted uses within the current ‘Mixed Use’ designation in the Official Plan. An Official Plan Amendment is therefore required to facilitate the proposed development.

4.4.2 Housing Policies

The Mississauga Official Plan includes policies for the City’s current and future housing stock in Chapter 7:

- “7.2.1 *Mississauga will ensure that housing is provided in a manner that maximizes the use of community infrastructure and engineering services, while meeting the housing needs and preferences of Mississauga residents.*
- 7.2.2 *Mississauga will provide opportunities for:*
- a. *the development of a range of housing choices in terms of type, tenure and price;*
 - b. *the production of a variety of affordable dwelling types for both the ownership and rental markets;”*

The proposed development represents housing types that contribute towards more affordable housing alternatives in the low-rise housing market. This aligns with the City's housing policy framework of providing a variety of dwelling types for different life cycles and income levels.

4.4.3 Complete Community Principles

One of the key tenants of recognized good planning is the principle of creating and encouraging 'complete communities'. Complete communities are addressed in Chapter 7 of the Mississauga Official Plan and are based on the notion that communities should be self-sustaining in providing opportunities for living, working, playing, and learning. Overall complete communities should provide a high quality of life and provide for the day-to-day needs of residents.

Relevant Official Plan policies on creating complete communities include:

- Complete communities require compact development that reduces travel time and encourages alternate modes of transportation (Section 7.1.3.a);
- Ensure housing can accommodate a range of socioeconomic backgrounds and preferences (Section 7.1.6);
- Provide opportunities for 'aging-in-place' that allows residents to stay in the community as they progress through their lifecycle by introducing alternate forms of housing (Section 7.2);
- Ensure housing maximizes the use of community infrastructure and engineering services (Section 7.2.1);
- Provide opportunities for a range of housing choices in terms of type, tenure and price (Section 7.2.2);

The proposed development meets these listed criteria for creating complete communities. In proposing an alternative form of housing type (horizontal multiple dwellings) as well as a two-storey retail and office building, the proposal provides for aging-in-place opportunities as well as other housing types in the neighbourhood other than detached lots or apartment buildings. It will also allow current and future residents to have access to convenience commercial uses within easy walking distance. The compact and efficient form on the proposed development enhances the existing character of neighbourhood by replaced an older commercial plaza which is dominated by surface parking.

4.4.4 Neighbourhoods and Non-Intensification Areas (Corridors)

Schedule 1 (Urban System) of the Official Plan identifies the subject property as 'Neighbourhood'. The Official Plan's policies on Neighbourhoods (Section 5.3.5 in Chapter 5) outline that although Neighbourhoods are intended to remain physically stable and protected, new development is permitted and does not have to mimic previous development patterns. Instead, it is noted that new development should be sensitive to the existing and planned character of Neighbourhoods.

Relevant policies include:

- “5.3.5.1 Neighbourhoods will not be the focus for intensification and should be regarded as stable residential areas where the existing character is to be preserved.*
- 5.3.5.2 Residential intensification within Neighbourhoods will generally occur through infilling and the development of existing commercial sites as mixed use areas.*
- 5.3.5.3 Where higher density uses are proposed, they should be located on sites identified by a local area review, along Corridors or in conjunction with existing apartment sites or commercial centres.*
- 5.3.5.5 Intensification within Neighbourhoods may be considered where the proposed development is compatible in built form and scale to surrounding development, enhances the existing or planned development and is consistent with the policies of this Plan.*
- 5.3.5.6 Development will be sensitive to the existing and planned context and will include appropriate transitions in use, built form, density and scale.”*

The proposed development conforms to the above-noted policies as the subject property is adjacent to existing medium and high density residential uses, and represents an infill redevelopment of an existing underutilized commercial plaza. The proposed height of the horizontal multiple dwellings is a maximum of 4-storeys and the commercial building is a maximum of 2-storeys which is compatible with existing surrounding uses. The subject property contains a dated commercial plaza that has mostly outlived its planned function and contains a large underutilized surface parking lot. As specifically noted in policy 5.3.5.2, redevelopment within Neighbourhoods is explicitly encouraged on existing commercial sites to provide both residential and commercial uses which this proposal achieves.

Section 9.2.2 of the Official Plan provides policies on ‘Non-Intensification Areas’ including Neighbourhoods and Corridors. This section restates the policy of Chapter 5 that new development in Neighbourhoods need not mirror existing development but must respect the built form context and be compatible with the surrounding area. Relevant policies include:

- “9.2.2.3 While new development need not mirror existing development, new development in Neighbourhoods will:*
 - a. respect existing lotting patterns;*
 - b. respect the continuity of front, rear and side yard setbacks;*
 - c. respect the scale and character of the surrounding area;*

- d. minimize overshadowing and overlook on adjacent neighbours;*
- e. incorporate stormwater best management practices;*
- f. preserve mature high quality trees and ensure replacement of the tree canopy; and*
- g. be designed to respect the existing scale, massing, character and grades of the surrounding area.*

The proposed development conforms to the policies for Non-Intensification Areas in proposing an appropriate and efficient use of land that respects the existing character and scale of the existing neighbourhood. The built form and scale of the proposed horizontal multiple dwellings conforms to the policies outlined for Non-Intensification Areas in remaining compatible with adjacent uses and in encouraging more active forms of transportation and reducing reliance on travel by private cars.

In encouraging principles of good urban design and built form as outlined in Section 9.1 of the Official Plan, the proposed development places building frontages close to existing lot lines to create the impact of a contiguous built form along street edges. The existing retail plaza reflects outdated design by placing a large surface parking lot between the building and the street on The Collegeway.

4.4.5 Official Plan Amendment

Section 19.5 of the Official Plan sets out criteria for Site Specific Official Plan Amendments, these include that:

- “a. that the proposed redesignation would not adversely impact or destabilize the following:*
 - the achievement of the overall intent, goals, objectives, and policies of this Plan; and*
 - the development or functioning of the remaining lands that have the same designation, or neighbouring lands; and*
- b. that a municipal comprehensive review of land use designations or a five year review is not required;*
- c. that the lands are suitable for the proposed use, and a planning rationale with reference to the policies of this Plan, other applicable policies, and sound planning principles is provided, setting out the merits of the proposed amendment in comparison with the existing designation;*
- d. land use compatibility with the existing and future uses of surrounding lands; and*

- e. *the adequacy of engineering services, community infrastructure and multi-modal transportation systems to support the proposed application.”*

As demonstrated in the above discussion and review of applicable Official Plan chapters, the application for an Official Plan Amendment complies with the policies of Section 19.5 of the Official Plan. In our opinion it is appropriate to amend the Official Plan to re-designate the subject property from “Mixed Use” to “Residential Medium Density” and to “Convenience Commercial”. The proposed development otherwise conforms to the applicable policies of the Official Plan. A Draft Official Plan Amendment to the Mississauga Official Plan is included at the end of this report as Appendix 1.

4.5 City of Mississauga Zoning By-Law 0225-2007

The subject property is zoned C2 (Neighbourhood Commercial) in Zoning By-law 0225-2007 which permits a range of retail, service, office, and recreational commercial uses.

To allow for the development of the proposed horizontal multiple dwellings and commercial building, it is proposed that the subject property be rezoned to RM9 (Horizontal Multiple Dwellings with more than 6 dwelling units) for the residential portion of the proposal and to C1 (Convenience Commercial) for the commercial portion. The proposed rezoning from C2 to C1 for the new commercial proposal is being proposed to better reflect the nature of the development in retaining commercial uses on the site to still respond to local shopping needs. A draft of the Zoning By-law Amendment is appended to this Report as Appendix 2.

5.0 SUPPORTING STUDIES AND REPORTS

The Development Application Review Committee (DARC) meeting on January 20, 2016 set out the required supporting studies to support the proposed applications. The findings of key reports are summarized in this section.

5.1 Transportation Impact Assessment

A Transportation Impact Assessment, prepared by LEA Consulting Ltd., dated May, 2016, has been prepared to determine the impact of the proposed development on the existing surrounding road network and assess the proposed parking demand. The analysis concluded acceptable levels of service (LOS) for existing and future background transportation conditions. The report included a proxy survey of a similarly developed stacked townhouse complex at 1701 Lampman Avenue in the City of Burlington to evaluate residential and visitor parking demand. A survey was also undertaken at the subject property to determine existing retail parking demand. It was found that the peak parking demand for retail and visitors was 45 spaces. As the commercial portion of the proposal includes 63 parking spaces, it is recommended that 18 spaces of the commercial portion be offered for purchase by residents of the stacked townhouses to mitigate the lower rate of parking being proposed for the dwelling units than what is required in the Zoning By-law.

5.2 Noise Feasibility Study

The supporting Environmental Noise Assessment prepared by YCA Engineering dated June 2016 has been prepared to present the assessment of the proposed development and recommend any noise abatement features necessary to achieve sound levels acceptable to the City of Mississauga and the Ministry of the Environment. The report concludes that sound levels acceptable to the Ministry and the City of Mississauga are expected to be achieved using the abatement measures in the report. The report recommends that mandatory air conditioning be required for all Stacked Townhouses and the Proposed Commercial Building and that upgraded windows and exterior wall constructions are required for Blocks 1 and 6 to 11.

5.3 Commercial Needs Analysis

The Commercial Needs Study, prepared by UrbanMetrics Inc., dated April 29, 2016 has been prepared to examine the appropriateness from a market perspective of redeveloping the existing shopping centre on the subject site. The study concludes that the site is considered to be under-utilized and unviable in its current form, particularly given that it is unable to obtain market rents achieved by other centres located in the area that are located on a major arterial road. The centre is not well situated as a neighbourhood centre as it is located on the periphery of a residential area and is not on a major arterial road, which is necessary for a well-functioning neighbourhood centre. The proposed development would maintain a selection of retail and service establishments that would be more appropriate for the subject site. The convenience commercial development on the site will continue to serve the needs of the local area while other commercial needs can be adequately met in existing retail centres in the area.

6.0 CONCLUSION

It is our opinion that the proposed Official Plan Amendment and Zoning By-law Amendment are justified and represent good planning for the following reasons:

1. The proposal conforms to and promotes the policies of the Provincial Policy Statement, the Growth Plan, and the Region of Peel Official Plan;
2. The proposed horizontal multiple dwellings represent a compact form of development and have a level of density which can be supported by the existing surrounding community infrastructure;
3. The proposed development is pedestrian friendly and transit-supportive as the development will be incorporated into the existing public sidewalk system and is supporting by local bus routes.
4. The existing commercial plaza represents an underutilized site that is dominated by surface parking area between the building and the site. The Official Plan's policies on Non-Intensification Areas support redevelopment and adaptive re-use of such commercial sites for a mix of uses. A Commercial Needs Study by UrbanMetrics Inc. confirms that the existing plaza, constructed in 1990, is no longer viable in its current form.

5. The proposed development represents housing types that contribute towards more affordable housing alternatives and aligns with the City's Official Plan housing policy framework and complete community principles of providing dwelling types for different life cycles and income levels;
6. The proposed development is within close proximity to and compatible with several existing medium and high density residential developments and is located along Major and Minor Collector roads.
7. The proposal to include a portion of the subject property to be re-designated and rezoned to Convenience Commercial allows for existing and future residents to have access to commercial uses within walking distance and encourages the principles of complete communities.

Respectfully submitted,

GLEN SCHNARR & ASSOCIATES INC.



Jim Levac, MCIP, RPP
Associate

Figures:

1. Aerial Context
2. Mississauga Official Plan Schedule 10
3. Erin Mills Neighbourhood Character Area
4. Mississauga Zoning By-law 0225-2007
5. Site Plan SP-1

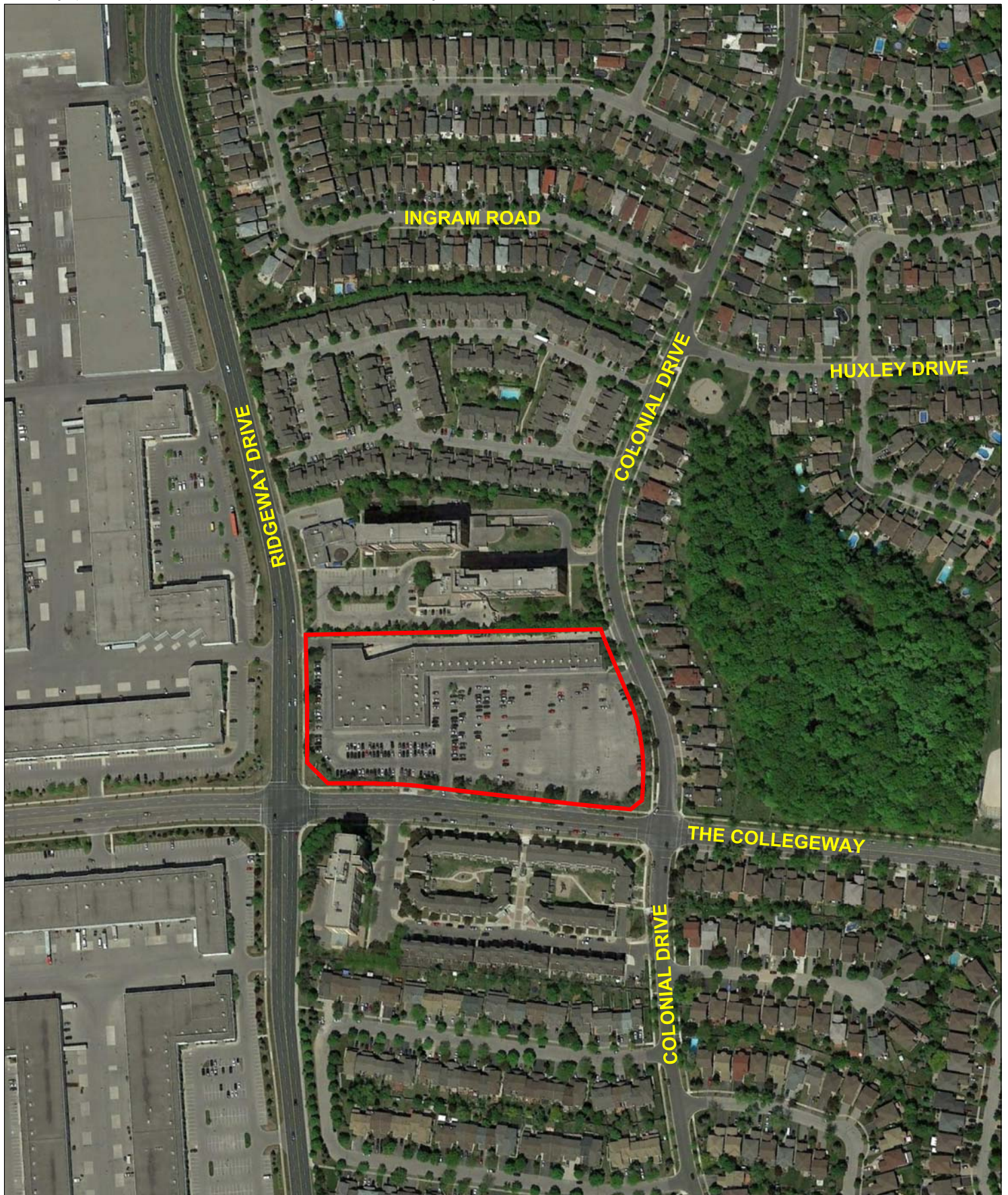


Figure 1 - Location Plan

— Subject Property

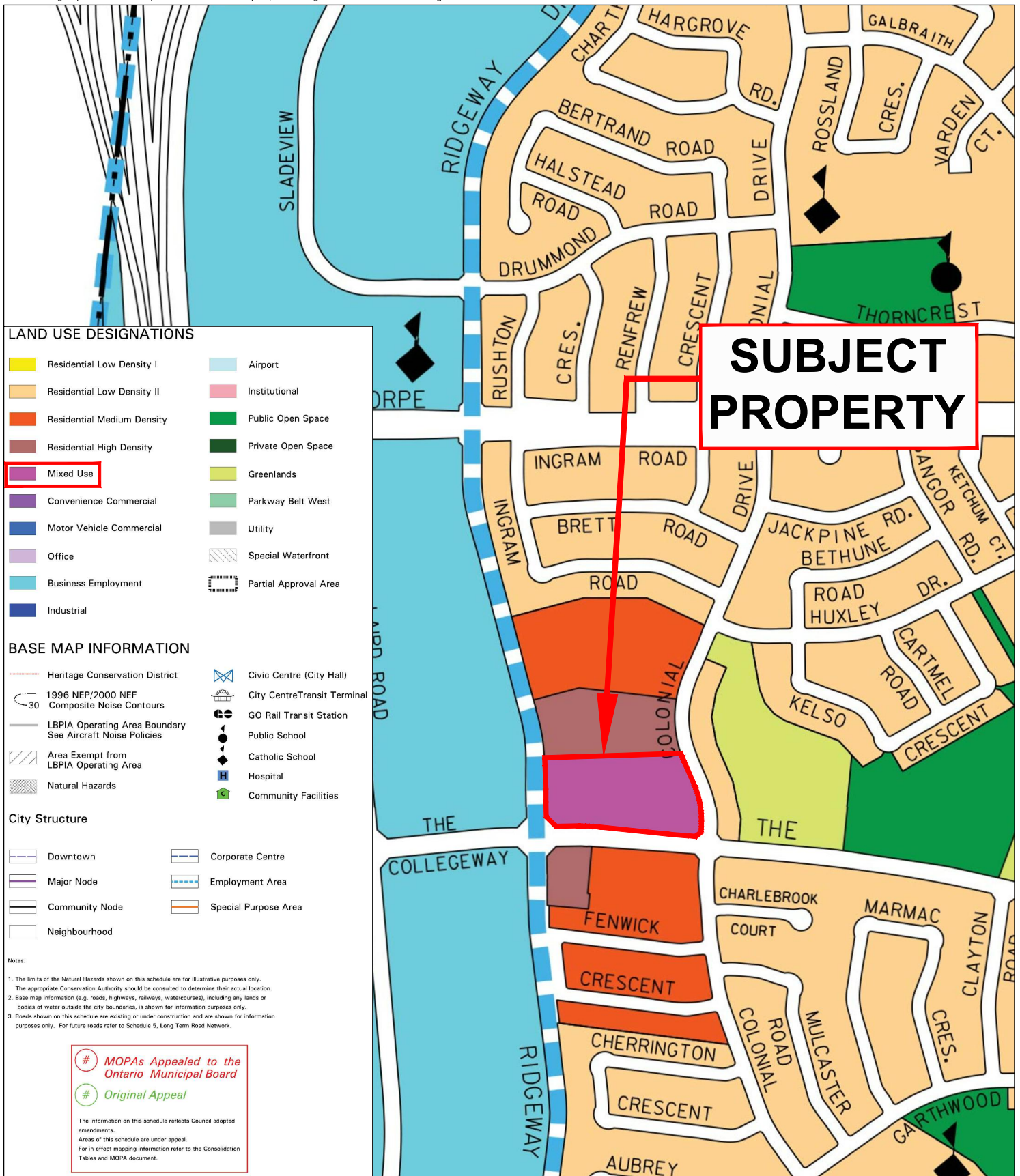


Figure 2 -

Mississauga Official Plan Land Use Designations (Schedule 10)

— Subject Property

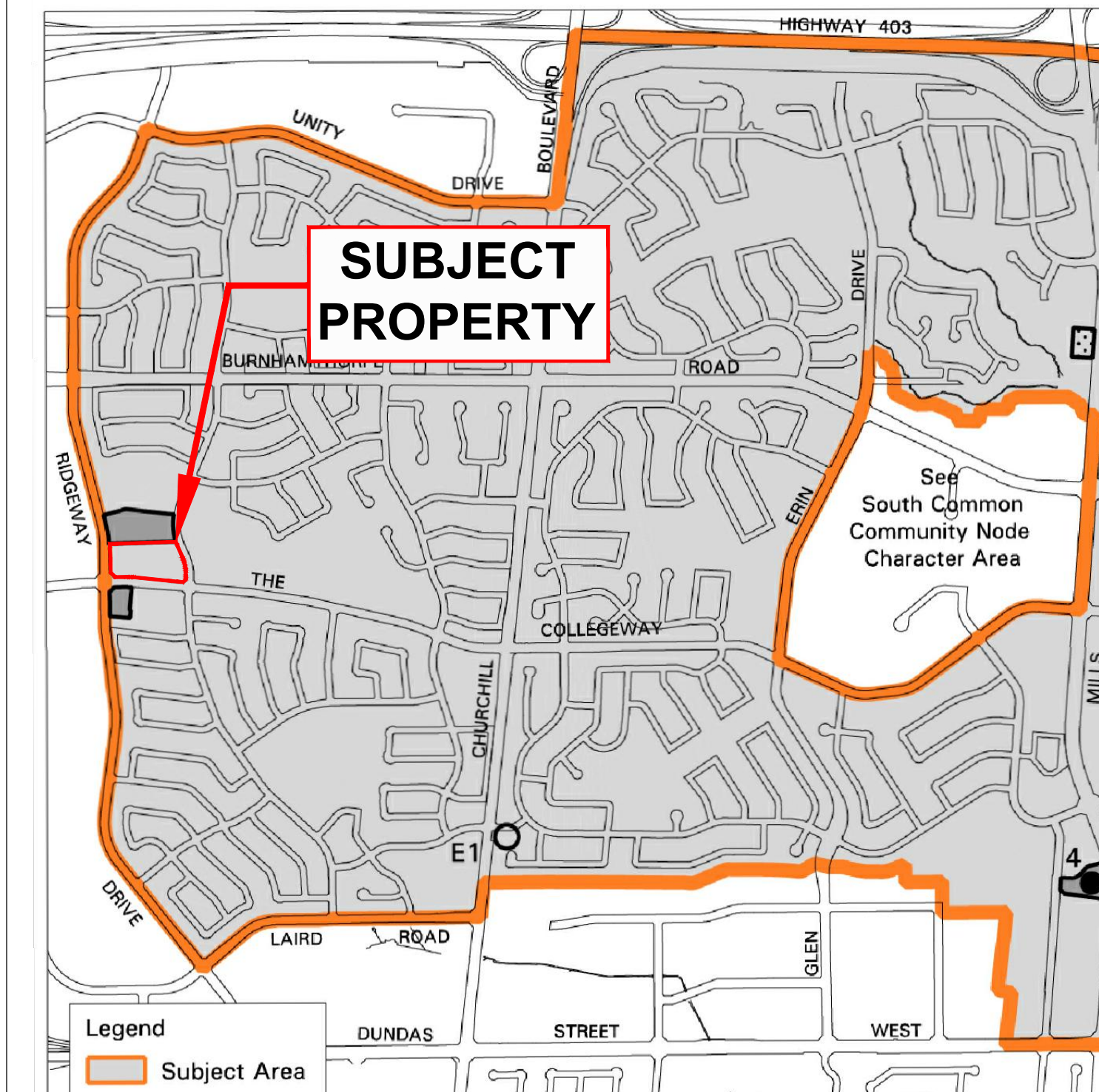


June 2, 2016



GLEN SCHNARR & ASSOCIATES INC.
URBAN & REGIONAL PLANNERS, LAND DEVELOPMENT CONSULTANTS
SUITE 700 10 KINGSBRIDGE GARDEN CIRCLE,
MISSISSAUGA, ONTARIO, L5R 3K6
TEL (905) 568-8888 FAX (905) 568-8894 www.gsal.ca

16.10 Erin Mills



**Figure 3 -
Erin Mills Neighbourhood
Character Area (Map 16-10)**

— Subject Property



June 2, 2016



GLEN SCHNARR & ASSOCIATES INC.
URBAN & REGIONAL PLANNERS, LAND DEVELOPMENT CONSULTANTS
SUITE 700 10 KINGSBRIDGE GARDEN CIRCLE
MISSISSAUGA, ONTARIO, L5R 3K6
TEL (905) 568-8888 FAX (905) 568-8894 www.gsa.ca

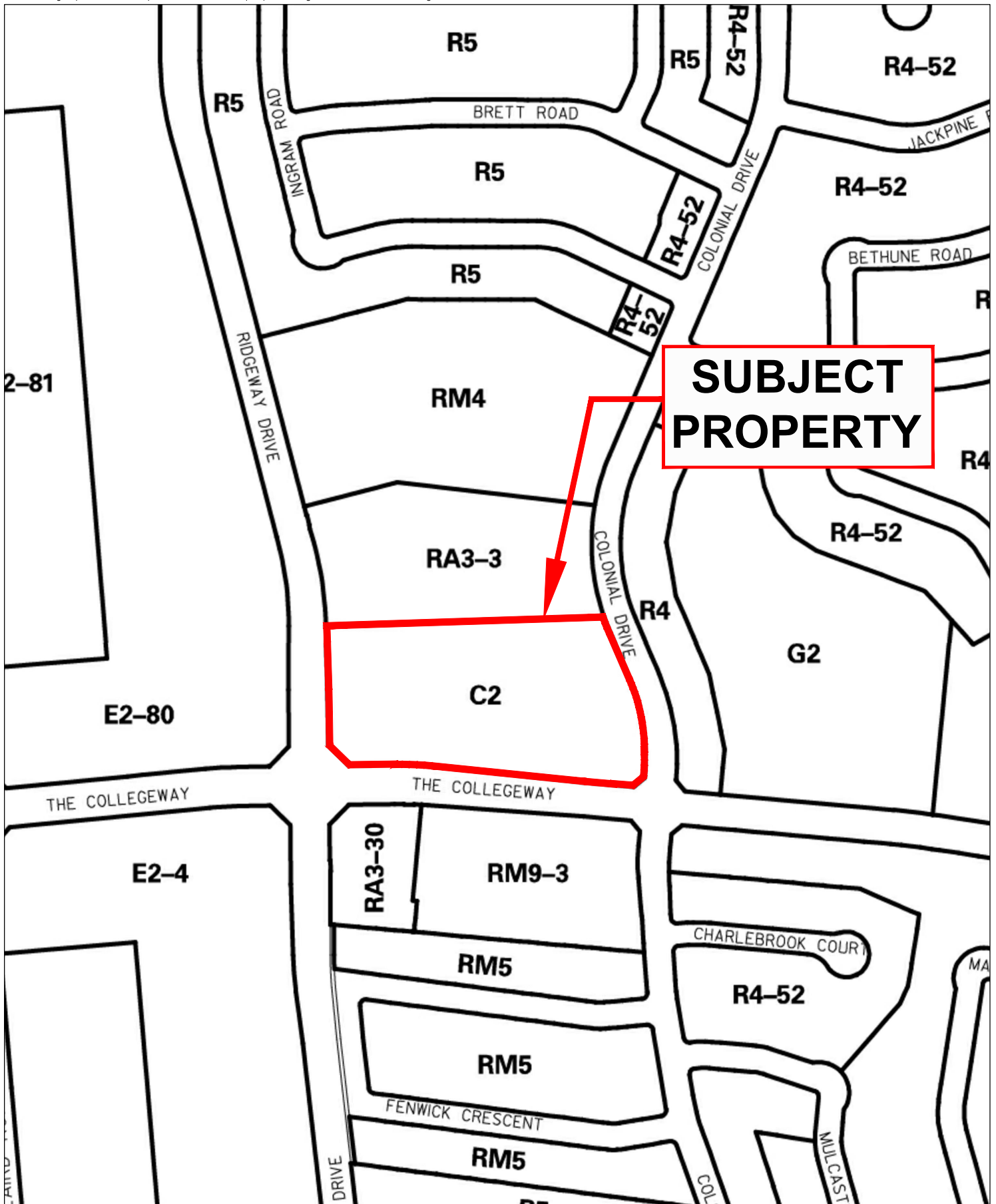


Figure 4 - Mississauga Zoning By-law

— Subject Property



June 2, 2016



GLEN SCHNARR & ASSOCIATES INC.
URBAN & REGIONAL PLANNERS, LAND DEVELOPMENT CONSULTANTS
SUITE 700 10 KINGSBRIDGE GARDEN CIRCLE,
MISSISSAUGA, ONTARIO, L5R 3K6
TEL (905) 568-8888 FAX (905) 568-8894 www.gsal.ca

- All drawings are the property of the Architect and must be returned upon request.
 - Contractor shall check all dimensions and report any discrepancies to the Architect before proceeding with the work.
 - The contractor and/or engineer shall verify all footing elevations and soil bearing capacity prior to excavation and the commencement of work.
- DO NOT SCALE DRAWINGS**

no.	date	revision
1	JAN 10, 2016	ISSUED FOR RECONING APPLICATION



A R C H I T E C T S

1770 Brown's Line, Toronto, Ontario M8W 3W2

3355 THE COLLEGEWAY L.P.
3355 THE COLLEGEWAY
MISSISSAUGA, ONTARIO

project

3355 THE COLLEGEWAY STACKED TOWNHOUSES

SITE PLAN

scale 1:400
date JANUARY 2016

drawn	CW
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chk'd	AM
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project number

Appendix 1

Draft Official Plan Amendment

Amendment No. XX

to

Mississauga Official Plan

for the

City of Mississauga Planning Area

The following text and map designated Schedule “A” attached hereto constitutes Amendment No. XX.

Also attached hereto but not constituting part of the Amendment and Appendices I, and II.

Appendix I is a description of the Public Meeting held in connection with this Amendment.

Appendix II is a copy of the Planning and Building Department report dated XX, XX, 201X, pertaining to this Amendment.

PURPOSE

The purpose of submitting this Official Plan Amendment is to add a Special Site policy to the Erin Mills Neighbourhood Character Area policies, to permit 336 stacked townhouse units (horizontal multiple dwellings) within eleven (11) blocks with access from Colonial Drive as well as a two-storey convenience commercial building with access from Ridgeway Drive.

LOCATION

The lands affected by this Amendment are located on the northeast intersection of The Collegeway and Ridgeway Drive in the City of Mississauga. The subject lands are located in the Erin Mills Neighbourhood Character Area in the Mississauga Official Plan.

BASIS

The subject lands are designated “Mixed Use” and are located within the Erin Mills Neighbourhood Character Area of the Mississauga Official Plan.

The proposal is to re-designate the subject property to “Residential Medium Density” and “Convenience Commercial”.

The proposed Official Plan Amendment is acceptable from a planning standpoint and should be approved for the following reasons:

1. The proposed development is consistent with and promotes the growth management and intensification policies of the Provincial Policy Statement, the Growth Plan for the Greater Golden Horseshoe, and the Region of Peel Official Plan.
2. The policies and objectives of the Mississauga Official Plan are supported by the proposal as it contributes a range of housing types, sizes and tenures; it is compatible from a density, size, scale, and massing perspective; it efficiently and effectively utilizes existing community infrastructure and facilities.

Details regarding this Amendment to the Mississauga Official Plan are contained in the Planning and Building Report dated xxxxxx xx, 201X, attached to this Amendment as Appendix II.

DETAILS OF THE AMENDMENT AND POLICIES RELATIVE THERETO

The Mississauga Official Plan is hereby amended by the following:

1. Schedule 10: Land Use Designation, of the Mississauga Official Plan, is hereby amended by re-designating the subject property to “Residential Medium Density” and “Convenience Commercial”, as shown on Schedule ‘A’ of this Amendment.

IMPLEMENTATION

Upon the approval of this Amendment by the Council of the Corporation of the City of Mississauga, the Mississauga Official Plan will be amended in accordance with this Amendment, and thereafter forms part of the Mississauga Official Plan. The lands will be rezoned concurrently to implement this Amendment.

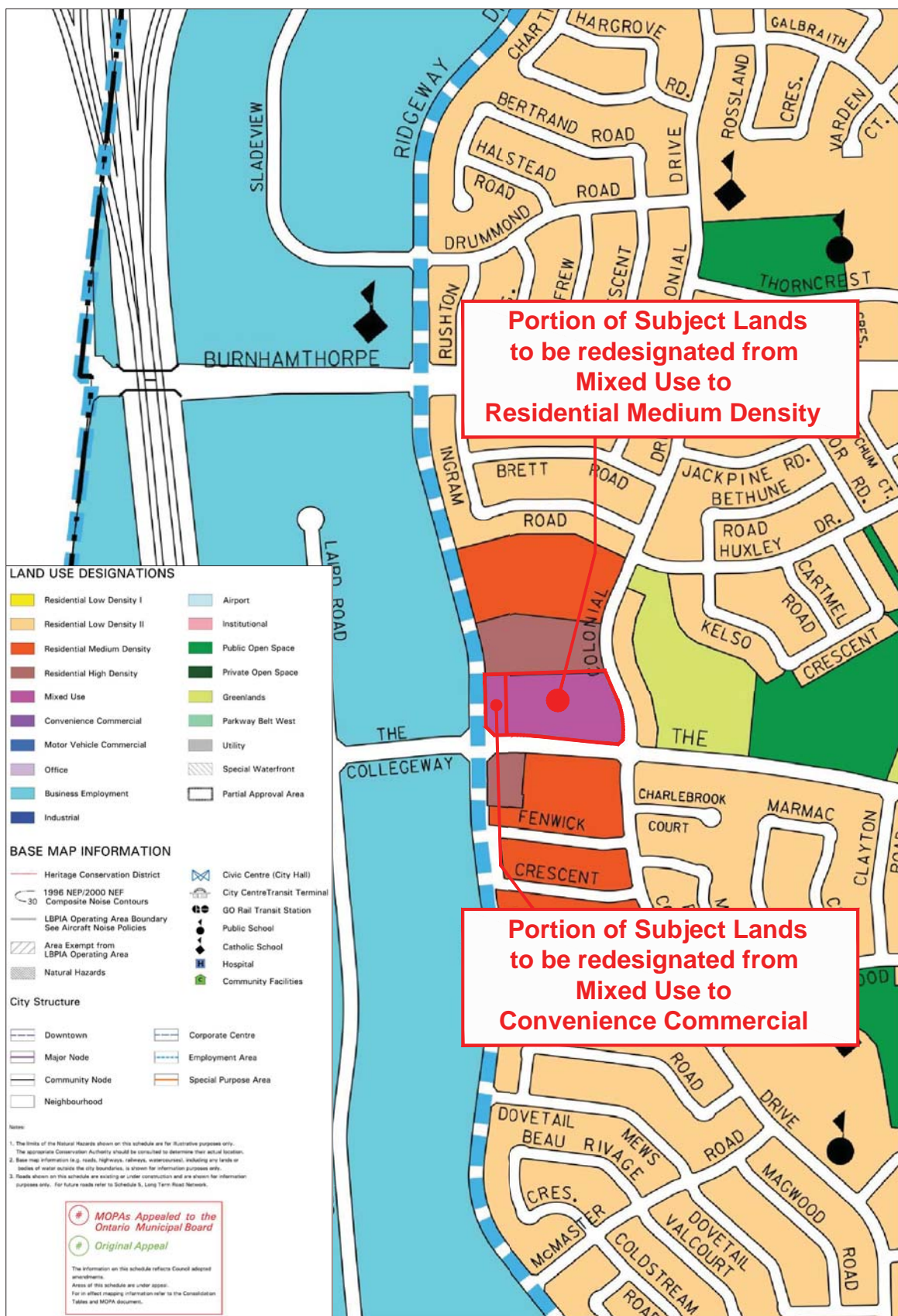
Provisions will be made through the Zoning By-law Amendment for the lands subject to this Official Plan Amendment, for development to occur subject to approved site development, architectural and landscaping plans, to ensure that site access, buildings, parking and landscaping are satisfactorily located and designed.

This Amendment has been prepared based on the Office Consolidation of the Mississauga Official Plan dated March 11, 2016.

INTERPRETATION

The provisions of the Mississauga Official Plan, as amended from time to time regarding the interpretation of that Plan, shall apply in regard to this Amendment.

Upon approval of this Amendment, the Mississauga Official Plan, will be amended in accordance with the intent of this Amendment.



Schedule A



March 21, 2016



GLEN SCHNARR & ASSOCIATES INC.
URBAN & REGIONAL PLANNERS, LAND DEVELOPMENT CONSULTANTS
SUITE 700 10 KINGSBRIDGE GARDEN CIRCLE
MISSISSAUGA, ONTARIO, L5R 3K6
TEL (905) 568-8888 FAX (905) 568-8894 www.gsa.ca

Appendix 2

Draft Zoning By-law Amendment

A By-law to amend By-law 0225-2007, as amended.

WHEREAS pursuant to section 34 of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, the Council may amend a Zoning By-law;

NOW THEREFORE the Council of The Corporation of the City of Mississauga ENACTS as follows:

1. By-law 0225-2007, as amended, being a City of Mississauga Zoning By-law, is amended by adding the following Exception Table:

4.14.2.XX	Exception: RM9-XX	Map # 59	By-law:
In a RM9-XX zone the permitted uses and applicable regulations shall be as specified for a RM9 Zone except that the following uses /regulations shall apply:			
Regulations			
4.14.2.XX.1	The regulations of Subsection 2.1.14 and Line 10.6 contained in Table 4.14.1 of this By-law shall not apply		
4.14.2.XX.2	Maximum floor space index	1.62	
4.14.2.XX.3	Maximum dwelling height flat roof, exclusive of mechanical penthouse	16.5 m	
4.14.2.XX.4	Minimum Front Yard	2.5 m	
4.14.2.XX.5	Minimum Exterior Side Yard	4.5 m	
4.14.2.XX.6	Minimum Interior Side Yard	3.5 m	
4.14.2.XX.7	Maximum encroachment into required front and rear yards of a porch or deck , inclusive of stairs	3.5 m	
4.14.2.XX.8	Maximum encroachment into required exterior side yard of a porch or deck , inclusive of stairs	3.0 m	
4.14.2.XX.9	Maximum encroachment into required yards of a balcony , box-out bay window or awning	0.3 m	
4.14.2.XX.10	Minimum setback from a horizontal multiple dwelling to:		
	(a) an internal road	2.4 m	
	(b) a sidewalk	0.9 m	
	(c) a parking space	2.5 m	
4.14.2.XX.11	Minimum setback from a porch or deck , inclusive of stairs, to:		
	(a) an internal road	2.4 m	
	(b) a sidewalk	0.9 m	

4.14.2.XX	Exception: RM9-XX	Map # 59	By-law:
In a RM9-XX zone the permitted uses and applicable regulations shall be as specified for a RM9 Zone except that the following uses /regulations shall apply:			
4.14.2.XX.12	Minimum setback from a side wall of a horizontal multiple dwelling to:		
	(a) an internal walkway	0.0 m	
	(b) an internal road	2.4 m	
	(c) a parking space	2.5 m	
4.14.2.XX.13	Minimum setback from an awning, window, chimney, pilaster, corbel or other architectural feature to an internal road or sidewalk	0.6 m	
4.14.2.XX.14	Minimum setback required between a parking space and a street	2 m	
4.14.2.XX.15	Minimum setback required between a parking space and any other lot line	2 m	
4.14.2.XX.16	Minimum number of on-site resident parking spaces per two-bedroom dwelling unit	1.1	
4.14.2.XX.17	Minimum number of on-site resident parking spaces per three-bedroom dwelling unit	1.7	
4.14.2.XX.18	Additional resident parking spaces may be provided on abutting lands zoned C1-XX		
4.14.2.XX.19	Minimum number of on-site visitor parking spaces	0.0	
4.14.2.XX.20	Minimum number of off-site visitor parking spaces provided on abutting lands zoned C1-XX	45	
4.14.2.XX.21	Required visitor parking spaces provided on abutting lands zoned C1-XX shall be shared with required commercial parking spaces through a shared parking arrangement		
4.14.2.XX.22	Minimum setback of a parking structure	2.0 m	
4.14.2.XX.23	Minimum width of a sidewalk	1.5 m	
4.14.2.XX.24	Minimum amenity area to be provided in one contiguous area	1120 m ²	

2. By-law 0225-2007, as amended, is further amended by adding the following Exception Table:

6.2.3.XX	Exception: C1-XX	Map # 59	By-law:
In a C1-XX zone the permitted uses and applicable regulations shall be as specified for a C1 Zone except that the following uses /regulations shall apply:			
Additional Permitted Use			
(a) Shared visitor parking spaces and additional resident parking spaces for abutting lands zoned RM9-XX			
Regulations			
6.2.2.XX.1 The regulations of Line 3.0 contained in Table 2.1.2.1.1 and Subsection 2.1.14 of this By-law shall not apply			
6.2.2.XX.2	Minimum rear yard to an accessory structure		1.0 m
6.2.2.XX.3	Minimum interior side yard to an accessory structure		0.0 m
6.2.2.XX.4	Minimum depth of a landscaped buffer measured from a lot line that is a street line		1.0 m
6.2.2.XX.5	Minimum depth of a landscaped buffer measured from the rear lot line		2.0 m
6.2.2.XX.6	Minimum depth of a landscaped buffer measured from the interior side lot line		0.0 m
6.2.2.XX.7	Minimum number of required commercial parking spaces		45
6.2.2.XX.8	Required commercial parking spaces shall be shared with required visitor parking spaces for abutting lands zoned RM9-XX through a shared parking arrangement		
6.2.2.XX.9	For the purpose of this by-law height shall not include roof top mechanical rooms or elevator overrun		

3. Map Number 59 of Schedule "B" to By-law 0225-2007, as amended, being the City of Mississauga Zoning By-law, is amended by changing thereon from "C2" to "C1-XX" and "RM9-XX" PROVIDED HOWEVER THAT the "C1-XX" and "RM9-XX" zoning shall only apply to the lands which are shown on the attached Schedule "A" outlined in the heaviest broken line with the "C1-XX" and "RM9-XX" zoning indicated thereon.

ENACTED and PASSED this _____ day of _____ 2016.

Mayor

Clerk

APPENDIX “A” TO BY-LAW NO. _____

Explanation of the Purpose and Effect of the By-law

This By-law amends the zoning of the property outlined on the attached Schedule “A” from “C2” to “C1-XX” and “RM9-XX”.

“C2” permits commercial uses

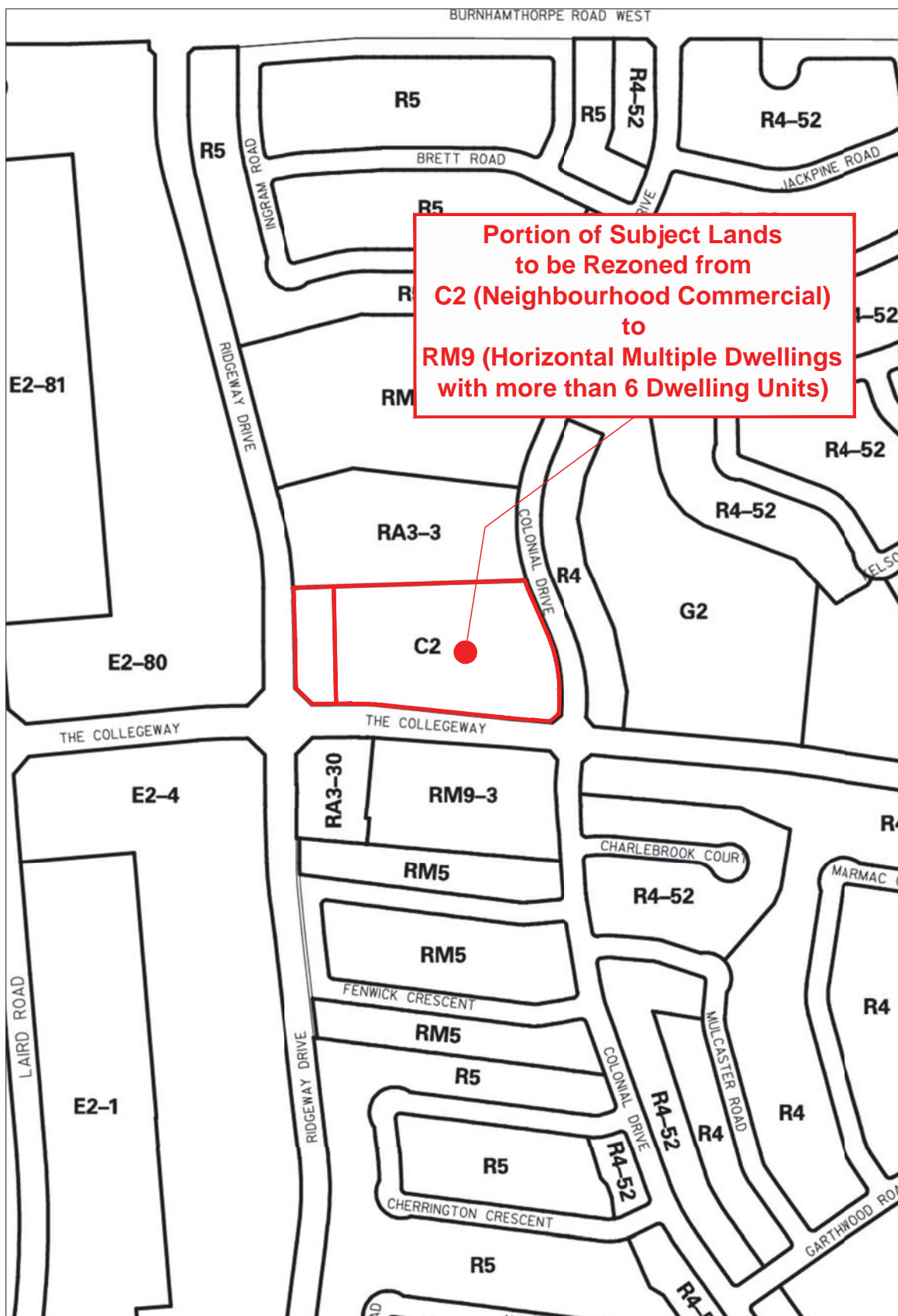
“C1-XX” continues to permit commercial uses, but with a small sized building

“RM9-XX” permits Horizontal Multiple Dwellings to a maximum Floor Space Index of 1.4

Location of Lands Affected

North side of The Collegeway, between Ridgeway Drive and Colonial Drive, as shown on the attached Map designated as Schedule “A”.

Further information regarding this By-law may be obtained from XXXXXXX of the City Planning and Building Staff at (905) 615-3200 x XXXX.



Schedule A



March 21, 2016



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SUITE 700 10 KINGSBIDGE GARDEN CIRCLE
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