
Heritage Advisory Committee

Date

2018/01/09

Time

9:30 AM

Location

Civic Centre, Committee Room A - Second Floor,
300 Civic Centre Drive, Mississauga, Ontario, L5B 3C1 Ontario

Members

Councillor George Carlson, Ward 11 (Chair)
Rick Mateljan, Citizen Member (Vice-Chair)
Councillor Carolyn Parrish, Ward 5
Michael Battaglia, Citizen Member
Elizabeth Bjarnason, Citizen Member
Robert Cutmore, Citizen Member
Lindsay Graves, Citizen Member
James Holmes, Citizen Member
Cameron McCuaig, Citizen Member
Melissa Stolarz, Citizen Member
Matthew N. Wilkinson, Citizen Member

Contact

Mumtaz Alikhan, Legislative Coordinator, Legislative Services
905-615-3200 ext. 5425
mumtaz.alikhan@mississauga.ca

NOTE: To support corporate waste reduction efforts the large
appendices in this agenda can be viewed at:
<http://www.mississauga.ca/portal/cityhall/heritageadvisory.ca>

Find it Online

<http://www.mississauga.ca/portal/cityhall/heritageadvisory>

-
1. CALL TO ORDER
 2. APPROVAL OF AGENDA
 3. DECLARATION OF CONFLICT OF INTEREST
 4. MINUTES OF PREVIOUS MEETING
 - 4.1. Heritage Advisory Committee Minutes - November 14, 2017
 5. DEPUTATIONS
 - 5.1. Waterfront Parks Strategy Refresh - Jane Darragh, Planner, Parks Planning
 6. PUBLIC QUESTION PERIOD - 15 Minute Limit (5 Minutes per Speaker)
Pursuant to Section 42 of the Council Procedure By-law 0139-2013, as amended the Heritage Advisory Committee may grant permission to a member of the public to ask a question of the Committee with the following provisions:
 1. The question must pertain to a specific item on the current agenda and the speaker will state which item the question is related.
 2. A person asking a question shall limit any background explanation to two (2) statements, followed by the question.
 3. The total speaking time shall be five (5) minutes maximum per speaker.
 7. MATTERS TO BE CONSIDERED
 - 7.1. Request to Alter a Heritage Designated Property: 41 Bay Street (Ward 1) Request
 - 7.2. to Alter a Heritage Designated Property: 1155 Willow Lane (Ward 11) Request to
 - 7.3. Alter a Heritage Designated Property: 1066 Old Derry Road (Ward 11) Request to
 - 7.4. Alter a Heritage Designated Property: 1059 Old Derry Road (Ward 11) Request to
 - 7.5. Alter a Heritage Designated Property: 1036 Old Derry Road (Ward 11) Request to
 - 7.6. Demolish a Heritage Listed Property: 36-46 Main Street (Ward 11) Request to
 - 7.7. Demolish a Heritage Listed Property: 7177 Lancaster Avenue (Ward 5) Request to
 - 7.8. Demolish a Heritage Listed Property: 1775 Thorny Brae Place (Ward 8)
 - 7.9. 29 Stavebank Road (Vimy Park)

7.10. Credit River Lakeshore Railway Bridge

7.11. Alterations to a Heritage Listed Property: 1576 Dundas Street West

7.12. Robert Cotton House, 1234 Old River Road

8. SUBCOMMITTEE UPDATES

8.1. Meadowvale Village Heritage Conservation District Advisory Sub-Committee Report dated November 28, 2017

8.2. Heritage Designation Sub-Committee Update

8.3. Public Awareness Sub-Committee Update

9. INFORMATION ITEMS

10. OTHER BUSINESS

11. DATE OF NEXT MEETING - February 6, 2017

12. ADJOURNMENT

City of Mississauga Minutes



Heritage Advisory Committee

Date

2017/11/14

Time

9:30 AM

Location

Civic Centre, Council Chamber,
300 City Centre Drive, Mississauga, Ontario, L5B 3C1 Ontario

Members Present

Councillor George Carlson, Ward 11 **(Chair)**
Rick Mateljan, Citizen Member **(Vice-Chair)**
Councillor Carolyn Parrish, Ward 5
Michael Battaglia, Citizen Member
Beth Bjarnason, Citizen Member
Robert Cutmore, Citizen Member
Lindsay Graves, Citizen Member
James Holmes, Citizen Member
Cameron McCuaig, Citizen Member
Melissa Stolarz, Citizen Member
Matthew N. Wilkinson, Citizen Member

Members Absent

Staff Present

Mark Warrack, Manager, Culture and Heritage Planning
Paula Wubbenhorst, Senior Heritage Coordinator, Culture Division
Mumtaz Alikhan, Legislative Coordinator

Find it online

<http://www.mississauga.ca/portal/cityhall/heritageadvisory>

1. CALL TO ORDER – 9:33 AM

2. APPROVAL OF AGENDA
APPROVED (R. Mateljan)

3. DECLARATION OF CONFLICT OF INTEREST –
 Lindsay Graves declared conflicts with Items 7.4 and 7.5, and Rick Mateljan declared a conflict with Item 7.1 due to the involvement of their respective companies with the files concerned.

4. MINUTES OF PREVIOUS MEETING

4.1. The Minutes of the Heritage Advisory Committee Meeting held on October 17, 2017 Meeting were approved as presented.

APPROVED (M. Wilkinson)

5. DEPUTATIONS

5.1. Draft Culture Master Plan (Plan) - Mojan Jianfar, Assistant Planner, Culture Planning

Mojan Jianfar, Assistant Planner, Culture Planning, reviewed the Draft Culture Master Plan. She spoke to five strategic priorities:

- Build an arts friendly city;
- Enhance and improve cultural spaces and places;
- Expand and grow leadership in the cultural sector;
- Support an authentic cultural identity that is welcoming, inspiring, and
- Share our story beyond our borders

Ms. Jianfar invited Committee Members to a public meeting being held on November 30, 2017 to get feedback. She spoke to the next steps in the process and advised that the final Plan is expected to go for Council adoption in June 2018.

The Committee spoke to the space, funding, private sponsorship communication, and amending by-laws to allow for public art such as creative hoarding on buildings in Heritage Conservation Districts and Cultural Landscapes.

Ms. Jianfar noted that initially, the plan is to open up City owned properties. Mark Warrack, Manager, Culture and Heritage Planning, spoke to the budgeting of funds to enable City owned properties to be opened.

The Chair thanked Ms. Jianfar for her informative presentation.

RECOMMENDATION

HAC-0074-2017

That the Power Point Presentation with respect to the Draft Culture Master Plan by Mojan Jianfar, Assistant Planner, Culture Planning, to the Heritage Advisory Committee on November 14, 2017, be received.

RECEIVED (R. Mateljan)5.2. Story of M Project Update - Meghan Johnston, Marketing Coordinator, Culture Division

Meghan Johnston, Marketing Coordinator, Culture Division, provided an overview with respect to the Story of *M* Project. She spoke to the three phases of the Project with Theme collection, outreach and engagement in Phase 1, a call for stories, artifacts and establishment of themes in Phase 2, and ongoing call focussed on artifacts and cultural planning.

The Committee spoke to the enhancing of the indigenous identity.

The Chair thanked Ms. Johnston for her informative presentation.

RECOMMENDATION

HAC-0075-2017

That the Power Point Presentation with respect to the Story of *M* Project by Meghan Johnston, Marketing Coordinator, Culture Division, to the Heritage Advisory Committee on November 14, 2017, be received.

RECEIVED (C. McCuaig)

6. PUBLIC QUESTION PERIOD – Nil.

7. MATTERS TO BE CONSIDERED

Rick Mateljan left the meeting due to a conflict of interest before the Committee considered Item 7.1.

7.1. Request to Alter a Heritage Designated Property: 34 John Street South (Ward 1)
Corporate Report dated October 26, 2017, from the Commissioner of Community Services.**RECOMMENDATION**

HAC-0076-2017

That the proposed alteration of 34 John Street South, as outlined in the Corporate Report from the Commissioner of Community Services, dated October 26, 2017, be approved.

APPROVED (Councillor C. Parrish)

Mr. Mateljan returned to the meeting.

7.2. Request to Alter a Heritage Designated Property: 50 Bay Street (Ward 1)

Corporate Report dated October 26, 2017 from the Commissioner of Community Services.

RECOMMENDATION

HAC-0077-2017

That the proposed alteration of 50 Bay Street, as outlined in the Corporate Report from the Commissioner of Community Services, dated October 26, 2017, be approved.

APPROVED (M. Wilkinson)

7.3. Request to Demolish a Heritage Listed Property: 3151 Churchill Avenue (Ward 5)

Corporate Report dated October 12, 2017 from the Commissioner of Community Services.

RECOMMENDATION

HAC-0078-2017

That the property at 3151 Churchill Avenue, which is listed on the City's Heritage Register, is not worthy of heritage designation, and consequently, that the owner's request to demolish proceed through the applicable process as outlined in the Corporate Report from the Commissioner of Community Services dated October 12, 2017.

APPROVED (Councillor C. Parrish)

At this point Lindsay graves left the meeting due to a conflict of interest before the Committee considered Item 7.4.

7.4. Alterations to a Listed Property: 6545 Creditview Road

Memorandum dated October 23, 2017 from Paul Damaso, Director, Culture Division.

RECOMMENDATION

HAC-0079-2017

That the Memorandum dated October 23, 2017 from Paul Damaso, Director, Culture Division, with respect to alterations to the property located at 6545 Creditview Road, be received for information.

RECEIVED (M. Wilkinson)

Ms. Graves returned to the meeting.

- 7.5. New Construction on Listed Property: 2300 Speakman Drive
Memorandum dated October 6, 2017 from Paul Damaso, Director, Culture Division.

RECOMMENDATION

HAC-0080-2017

That the Memorandum dated October 6, 2017 from Paul Damaso, Director, Culture Division, with respect to new construction at the property located at 2300 Speakman Drive, be received for information.

RECEIVED (R. Cutmore)

Ms. Graves left the meeting due to a conflict of interest before the Committee considered Item 7.6.

- 7.6. New Construction on Listed Property: 2660 Speakman Drive
Memorandum dated October 6, 2017 from Paul Damaso, Director, Culture Division.

RECOMMENDATION

HAC-0081-2017

That the Memorandum dated October 6, 2017 from Paul Damaso, Director, Culture Division, with respect to new construction at the property located at 2660 Speakman Drive, be received for information.

RECEIVED (R. Mateljan)

Ms. Graves returned to the meeting.

- 7.7. 2018 Heritage Advisory Committee Meeting Schedule
Memorandum dated October 30, 2017, from Mumtaz Alikhan, Legislative Coordinator.

RECOMMENDATION

HAC-0082-2017

That the Heritage Advisory Committee Meeting Schedule for 2018 as outlined in the Memorandum dated October 30, 2017 from Mumtaz Alikhan, Legislative Coordinator, be received for information.

RECEIVED (J. Holmes)

8. SUBCOMMITTEE UPDATES
- 8.1. Heritage Designation Sub-Committee – Nil.
- 8.2. Public Awareness Sub-Committee – Nil.
9. INFORMATION ITEMS – Nil.

10. OTHER BUSINESS

- (a) Councillor Parrish thanked Melissa Stolarz, Citizen Member, for her recent deputation to General Committee with respect to the Erindale Village Hall.
- (b) Councillor Parrish invited members of the Committee to a meeting today at 1:00 pm at the site of the Britannia United Church in order to consider the feasibility of moving it. She noted that the cemetery will be acquired by the City and will remain where it is presently located.

11. DATE OF NEXT MEETING - January 9, 2017

12. ADJOURNMENT – 10:45 am

City of Mississauga
Corporate Report



Date: December 7, 2017

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
 Community Services

Originator's files:

Meeting date:
 2018/01/09

Subject

Request to Alter a Heritage Designated Property: 41 Bay Street (Ward 1)

Recommendation

That the proposed alteration to 41 Bay Street, as per the Corporate Report from the Commissioner of Community Services, dated December 7, 2017 be approved.

Background

The City designated the Old Port Credit Village Heritage Conservation District (HCD) under Part V of the Ontario Heritage Act in 2004. The subject property is included in the district and identified as a "historic" property in the plan. As such, the property is subject to the heritage permitting requirements outlined in the plan for this classification.

The City issued a heritage permit for an addition and replacement garage in 2015. At that time, heritage permits expired after one year. (The original application is available as item 5 here: http://www7.mississauga.ca/documents/agendas/committees/heritage/2015/HAC_Agenda_2015_06_23.pdf.) The permit is expired but the plans have also been modified. The proposed addition is slightly larger and taller. Since the 2015 application, the owners have completed restoration work on the original house. As such, the current application is primarily focused on the proposed addition and garage. However, the application includes an extended driveway and an intervention of three pairs of French doors on the existing dwelling. The proposal is attached as Appendix 1. A Heritage Impact Assessment and two addendums, addressing the pool house and garage, and updates to the proposed addition are attached as Appendices 1, 2 and 3 respectively.

Comments

As per the original report on the matter, for the most part, the proposal conforms to the guidelines. Where the addition meets the original house is slightly inset. The bulk of the addition is slightly higher than the original house but, is set back. The siding material matches the original house but there is allowance for this consistency in the current plan. The “hyphen” that connects the addition to the existing house is the element that differentiates new from old. Three pairs of French doors are proposed on the ground floor of the existing dwelling, two on the front façade and one on the west. The proposed garage is set well back from the house and accessed by a proposed single-lane driveway. This is in accordance with the HCD plan.

Financial Impact

There is no financial impact.

Conclusion

The owner of the subject property proposes a rear addition and garage at the subject address. The proposed addition is large and slightly taller than the existing dwelling. However, it is set back with an inset “hyphen” feature to differentiate it. As such, the proposal should be approved.

Attachments

Appendix 1: Detailed drawings showing the restoration and proposal

Appendix 2: Heritage Impact Assessment

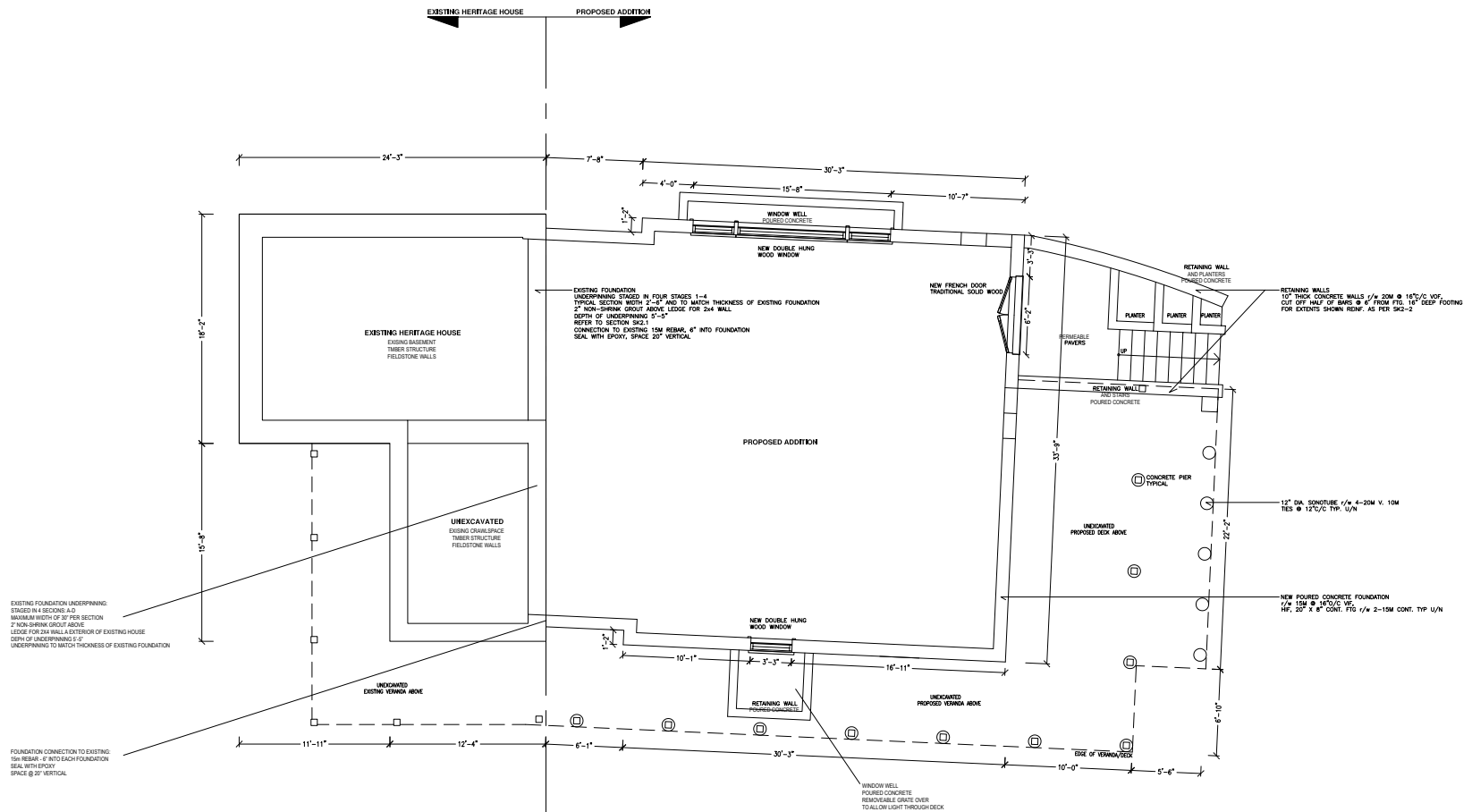
Appendix 3: Addendum regarding pool house and proposed garage

Appendix 4: Addendum regarding changes to proposed addition



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: P. Wubbenhorst, Senior Heritage Coordinator



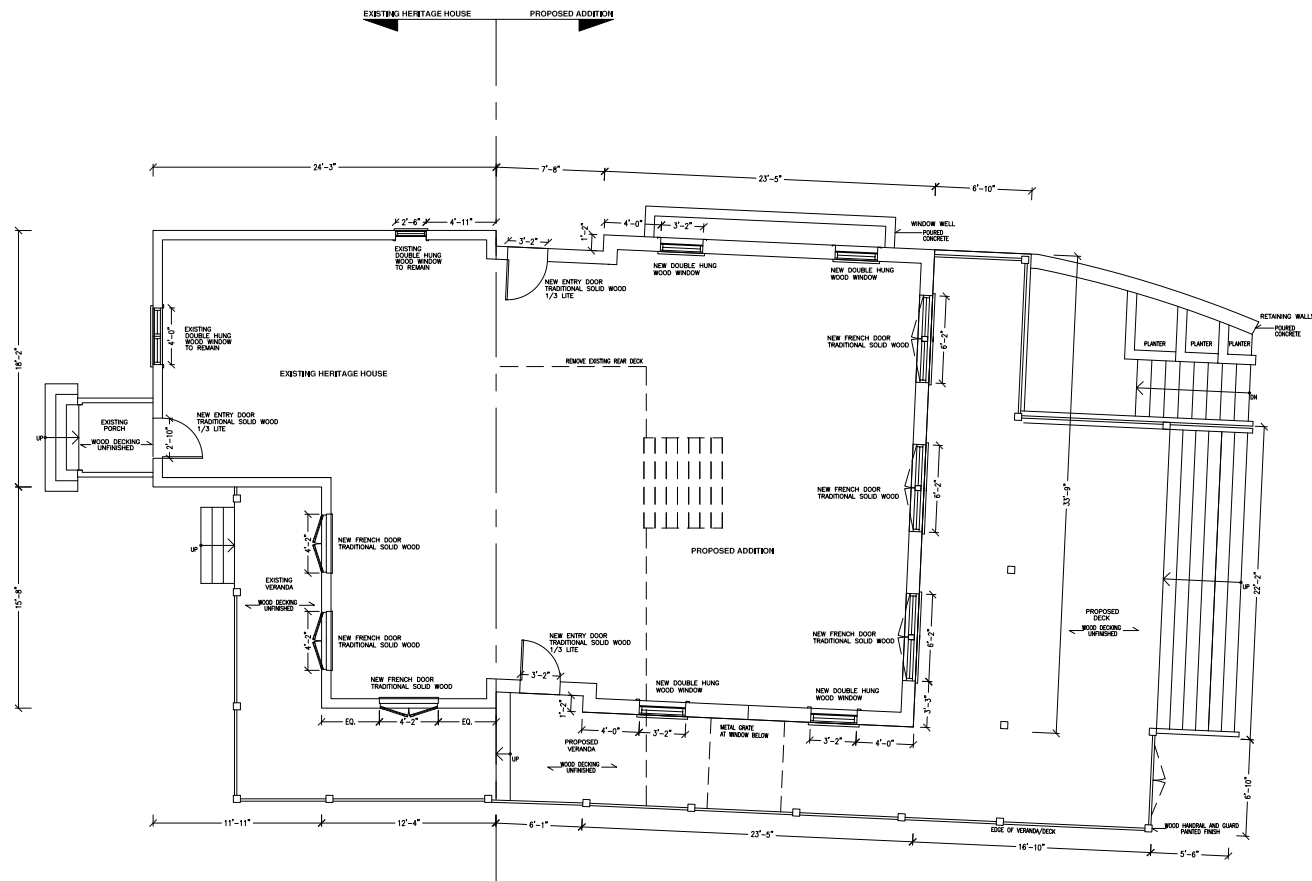
41 Bay St.
Heritage Addition

A-2.01

Proposed Basement Floor Plan

November 29, 2017
Scale: 1/4" = 1'-0"





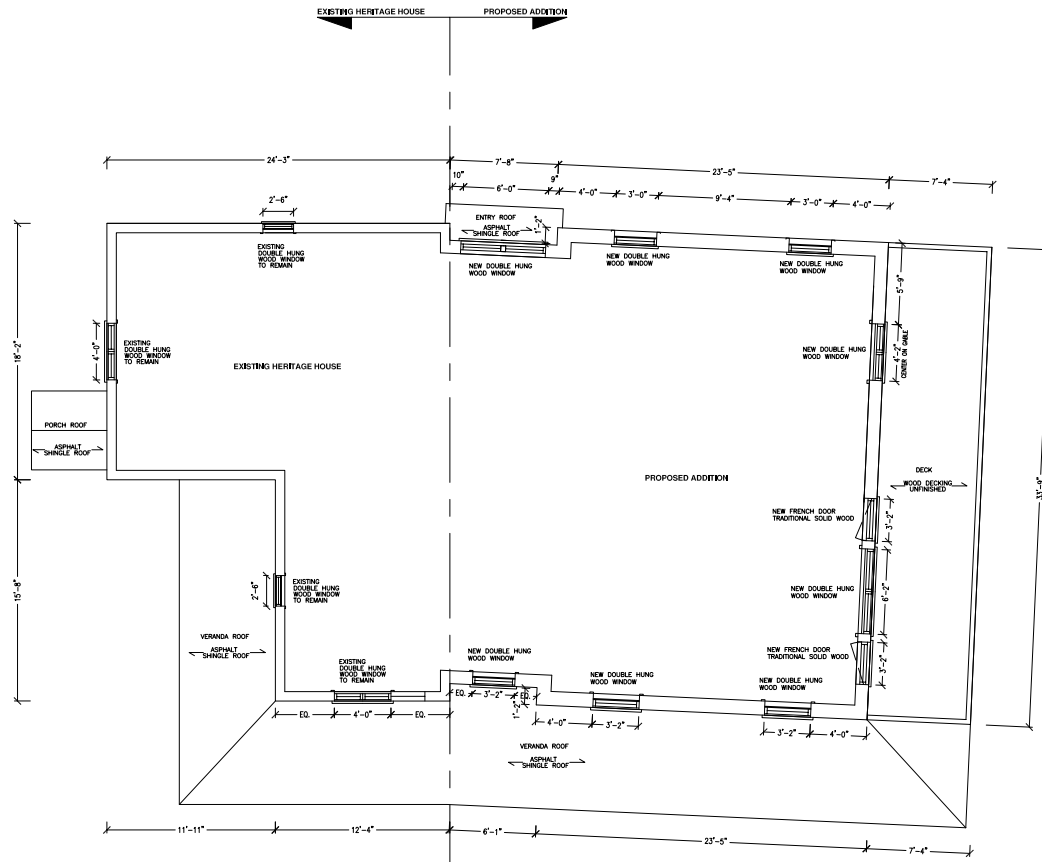
41 Bay St.
Heritage Addition

A-2.02

Proposed 1st Floor Plan

November 29, 2017
Scale: 1/4" = 1'-0"





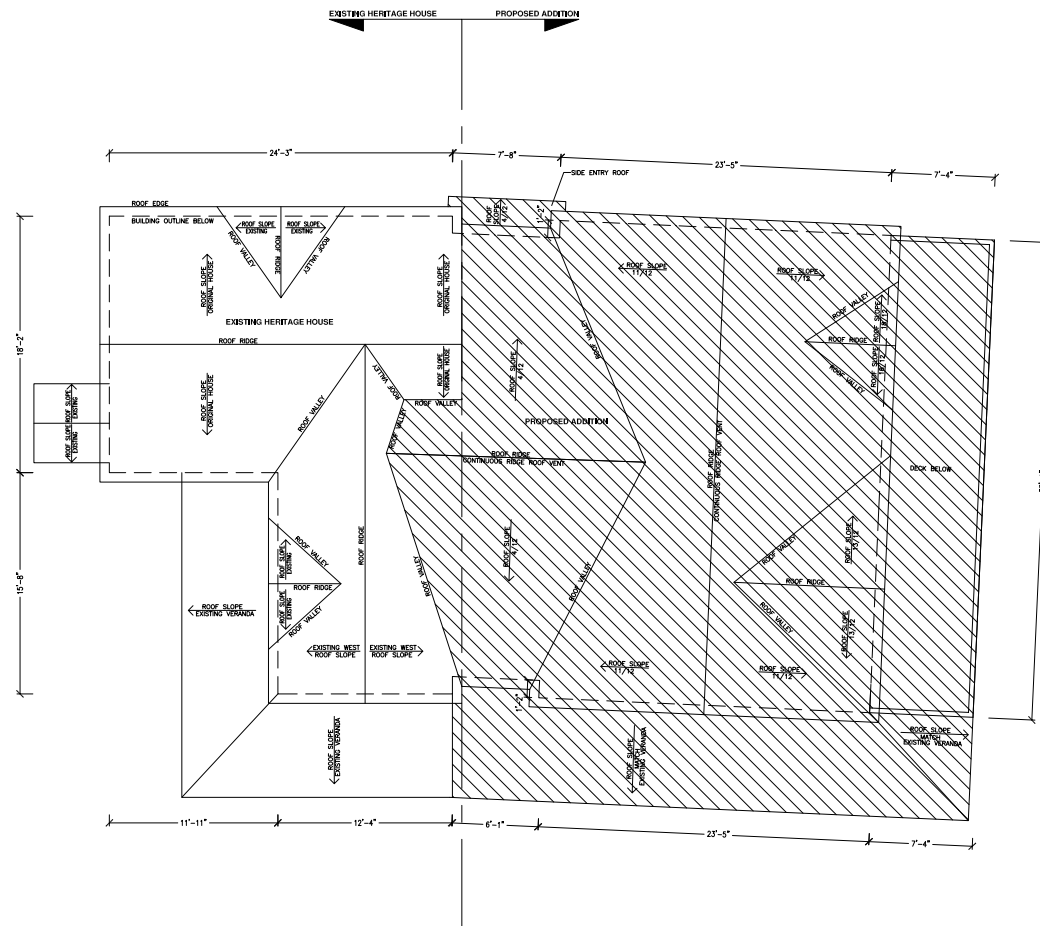
41 Bay St.
Heritage Addition

A-2.03

Proposed 2nd Floor Plan

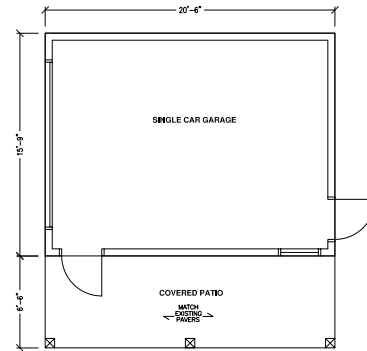
November 29, 2017
Scale: 1/4" = 1'-0"



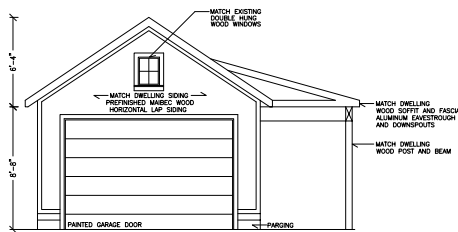


41 Bay St.
Heritage Addition

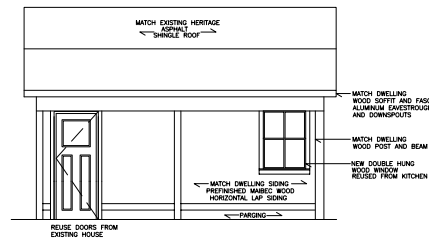
A-2.04 Proposed Roof Plan
November 29, 2017
Scale: 1/4" = 1'-0"



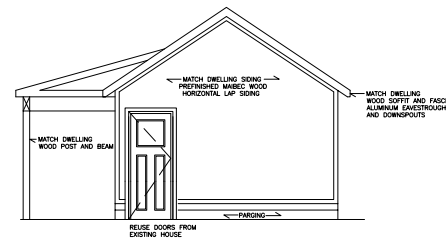
Plan



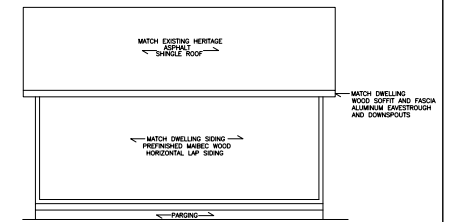
North Elevation



West Elevation



South Elevation

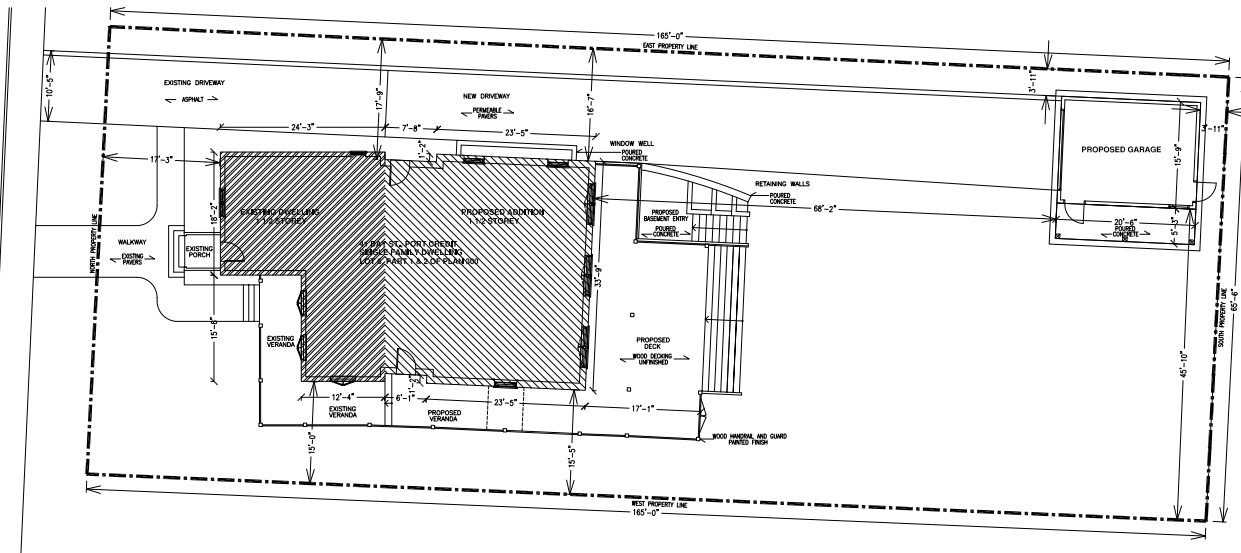


East Elevation

41 Bay St.
Heritage Addition

A-2.05

Proposed Garage
November 29, 2017
Scale: 1/4" = 1'-0"



41 Bay St.
Heritage Addition

A-2.06 Site Plan

November 29, 2017
Scale: Noted



EXISTING NORTH ELEVATION - CURRENT

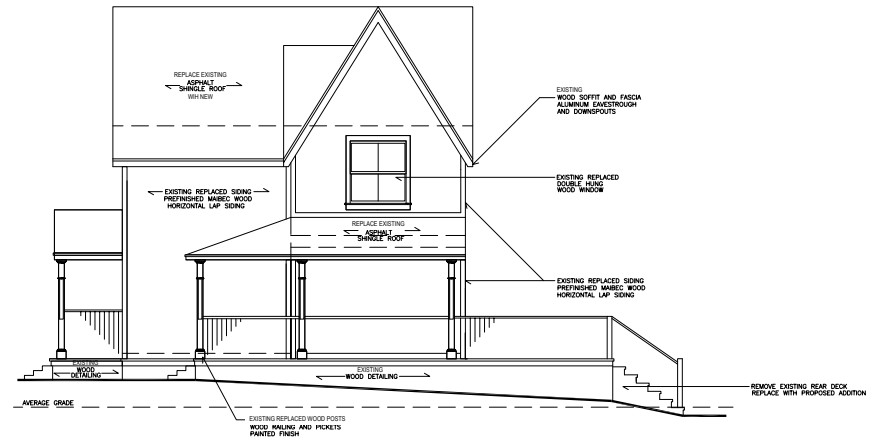


41 Bay St.
Heritage Addition

A-4.01 North Elevation - Restoration
November 29, 2017
Scale: 1/4" = 1'-0"



EXISTING WEST ELEVATION - CURRENT

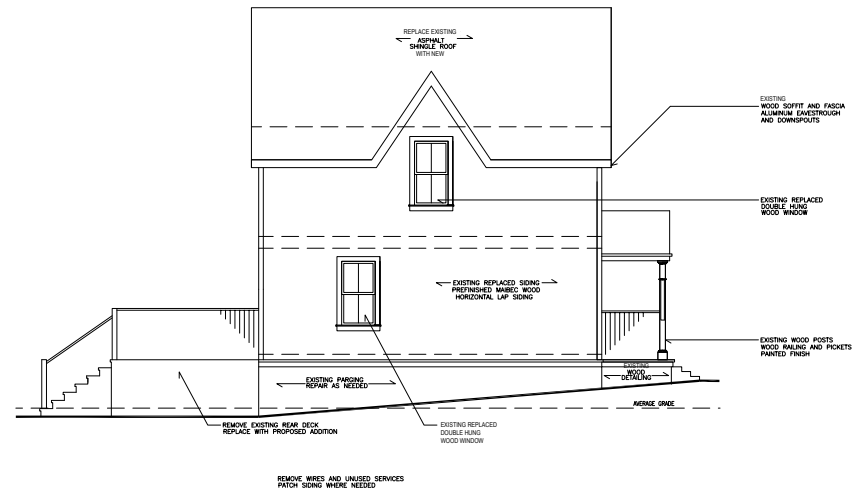


41 Bay St.
Heritage Addition

A-4.02 West Elevation - Restoration
November 29, 2017
Scale: 1/4" = 1'-0"



EXISTING EAST ELEVATION - CURRENT



41 Bay St.
Heritage Addition

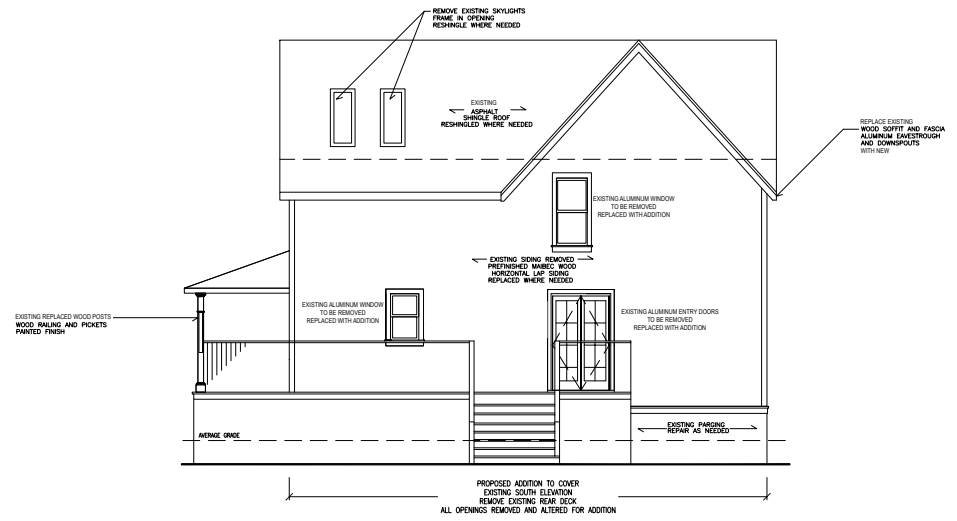
A-4.03

East Elevation - Restoration

November 29, 2017
Scale: 1/4" = 1'-0"

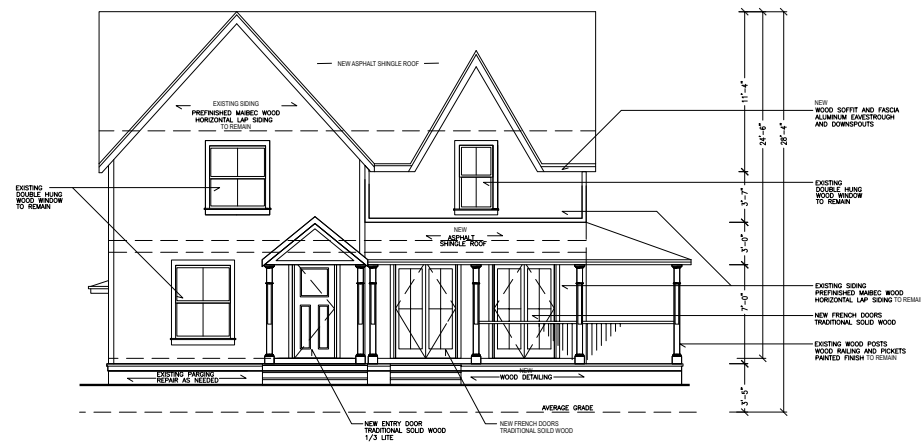


EXISTING SOUTH ELEVATION - CURRENT



41 Bay St.
Heritage Addition

A-4.04 South Elevation - Restoration
November 29, 2017
Scale: 1/4" = 1'-0"

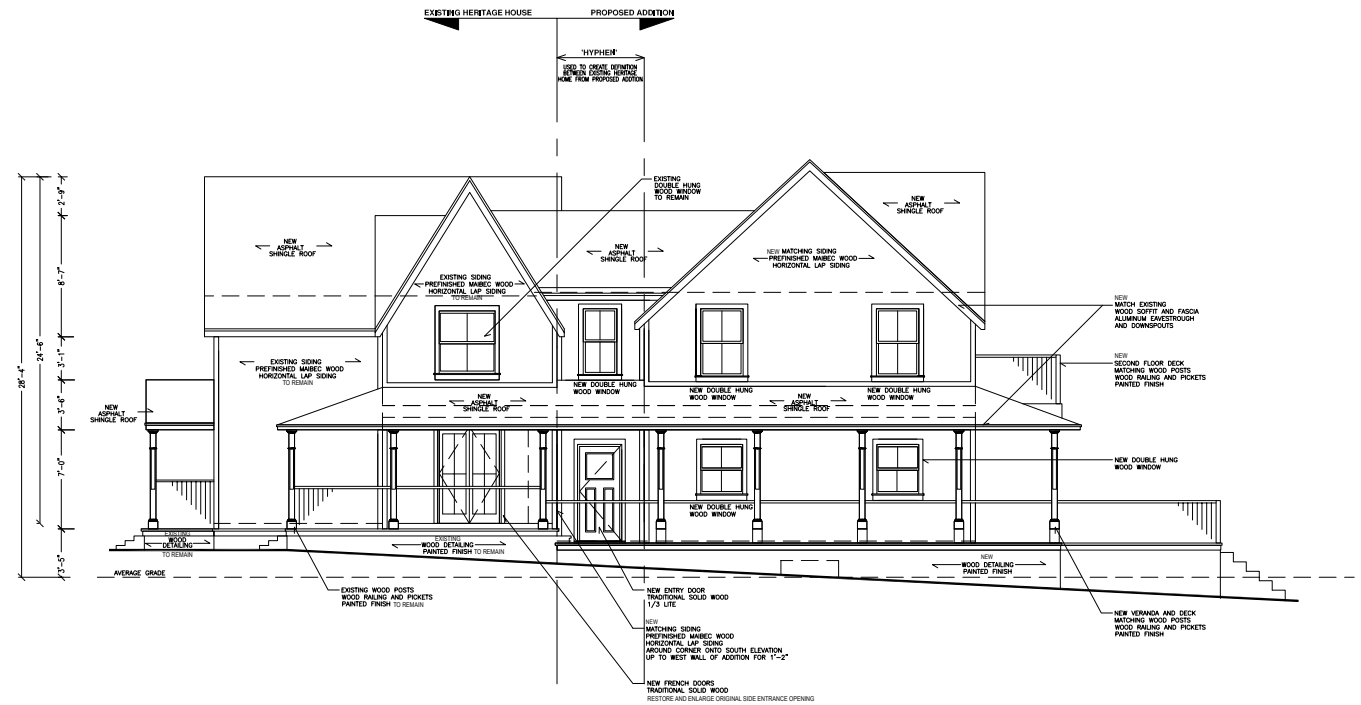


41 Bay St.
Heritage Addition

A-5.01 North Elevation - Proposed
November 29, 2017
Scale: 1/4" = 1'-0"

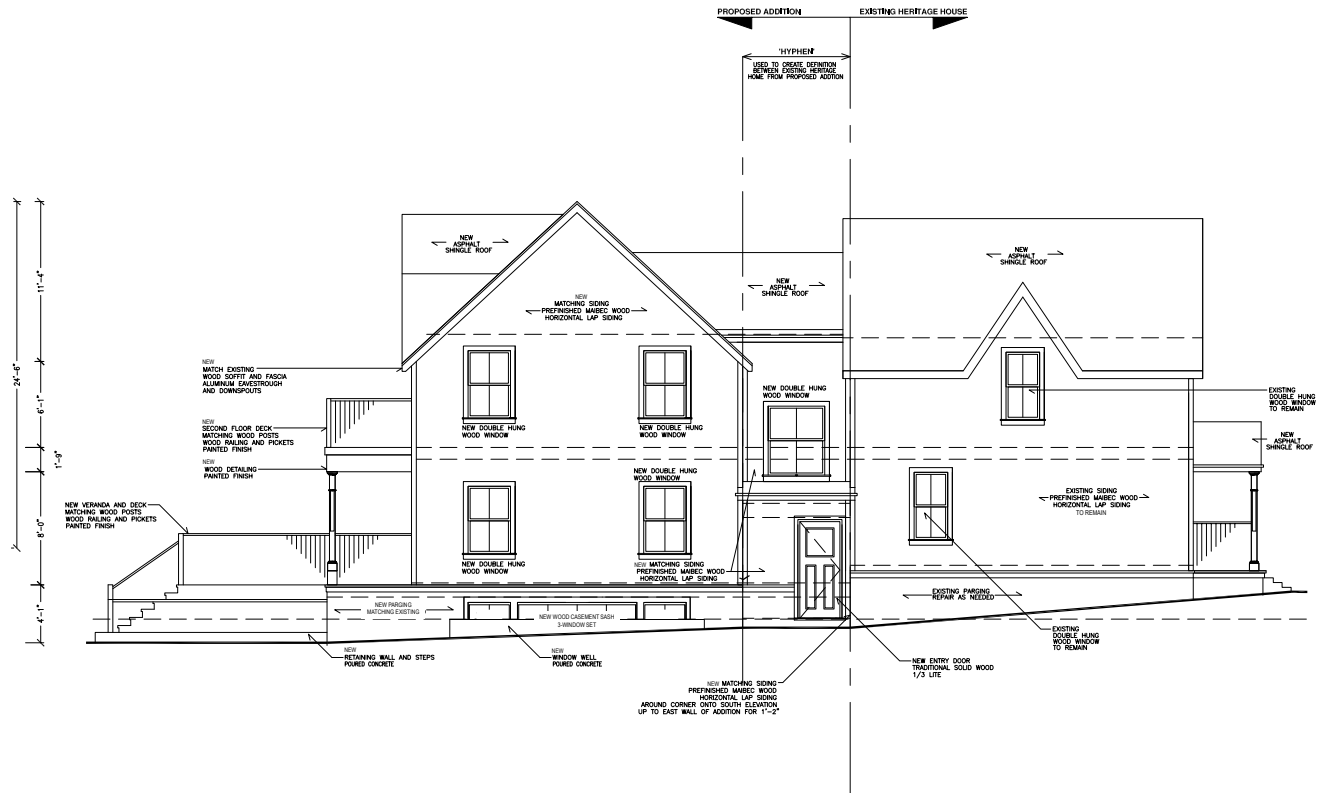


WEST ELEVATION - HERITAGE



41 Bay St.
Heritage Addition

A-5.02 West Elevation - Proposed
November 29, 2017
Scale: 1/4" = 1'-0"



41 Bay St.

Heritage Addition

A-5.03

East Elevation - Proposed

November 29, 2017
Scale: 1/4" = 1'-0"



South Elevation - Proposed

November 29, 2017
Scale: 1/4" = 1'-0"

HERITAGE IMPACT STATEMENT



A PHYSICAL, HISTORICAL AND CONTEXTUAL ASSESSMENT OF
41 BAY STREET
MISSISSAUGA, ONTARIO

This document is a resubmission of the
March 4, 2015 Heritage Impact Assessment
for 41 Bay Street, Port Credit,
now with updated 2016 Plans.

The following pages have been replaced from the original file:

Page 28 – North & West Elevations
Page 29 – East & South Elevations
Page 30 – Ground Floor Plan
Page 31 – Second Floor Plan
Page 32 – Basement Floor Plan
Page 33 – Garage Elevations & Floor Plan
Page 34 – Site Plan

1 .0 IDENTIFICATION

- 1.1 Name(s)
- 1.2 Recognition
- 1.3 Location
- 1.4 Coordinates
- 1.5 Boundaries
- 1.6 Contact Information
- 1.7 Property

2 .0 DESIGN AND PHYSICAL VALUE

- 2.1 Architectural Style

3 .0 HISTORICAL VALUE

- 3.1 History of the Building
- 3.2 History of the Property
- 3.3 List of Grantees
- 3.4 History of the Families

4 .0 CONTEXTUAL VALUE

- 4.1 A Part of the Historic Community

5 .0 ASSESSMENT

- 5.1 Elements that Contribute to Design and/or Physical Value
- 5.2 Elements that Contribute to Historical Value
- 5.3 Elements that Contribute to Contextual Value
- 5.4 Dates of Significance

6 .0 REGULATION 9 / 0 6

- 6.1 Analysis of Compliance with Section 29 of the Ontario Heritage Act
- 6.2 Summary of Suitability for Designation
- 6.3 Conclusion

7 .0 PROPOSED ALTERATIONS

- 7.1 Plans and Elevations
- 7.2 Old Port Credit Village Architectural Statement Table

8 .0 ADDENDUM

1.0 IDENTIFICATION

Name(s)

1.11 Official Place Name

none

1.12 Other Name(s)

McGregor-Lord residence, McGregor-Thompson residence

Recognition

1.21 Status

designated under Part V of the Ontario Heritage Act
enacted under bylaw 272-2004

1.22 Inventory Number

189

Location

1.31 Address

41 Bay Street

1.32 Postal Code

L5H 1C4

1.33 Lower Tier

City of Mississauga

1.34 Upper Tier

Regional Municipality of Peel

Coordinates

1.41 Latitude

43° 32' 51.7" North

1.42 Longitude

79° 35' 08.9" West

Boundaries

1.51 Lot

part of Lot 7 of the Broken Range, Credit Indian Reserve
Plan 300 West, Lot 6 of the block south of Bay Street

1.52 Property Area

1,043.58 m²

1.53 Depth

50.29 m

1.54 Water Frontage

not applicable

Contact Information

1.61



Looking north



Looking west



Looking east



Looking south

1.71 Property Description

The building is located on the south side of Bay Street, east of Peter Street South and west of John Street South, in the former Town of Port Credit, Ontario. The building faces northward toward Bay Street. The house is at approximately the same elevation as the street and the neighbouring buildings. The foremost part of the building is set seven metres in from the sidewalk.

1.72 Inventory of Structures on Property

There are presently three structures on the property:

- the main one-and-a-half storey residence,
- a one-storey brick outbuilding to the west side of the property, south of the house,
- a small wood shed at the southwest corner of the lot.

The outbuilding and shed are not represented as “sold fill” on the 2010 City of Mississauga’s e-maps database. The 2009 e-map shown on page 6 (a sharper image than the 2010 aerial image) still identifies a pool that was removed.



41 Bay Street from 2009 aerial mosaic, excluding removed pool.



Outbuildings at the back of the property.



A view of the south side of Bay Street.

2.0 DESIGN AND PHYSICAL VALUE

2.1 Architectural Style

Like all settlers who built homes out of necessity, Alexander McGregor didn't really have an architectural style in mind when he built his home. He simply put up a home in a style that was easy to build for a man without professional homebuilding skills.

Architectural academicians later categorized styles based on superficial features. Because of a lack of standardization in architectural styling classification, 41 Bay Street can be identified as being anything from Ontario vernacular to mid-Victorian to Gothic Revival., with the last of the three being the most common and least fitting coinage. The Victorian era was a time of prosperity when Canadians set themselves to the task of building a better future. Homebuilders did not look to the European past for inspiration. Progress and wealth in a new nation was the inspiration for Victorian-era homes. McGregor intended his home to have a dignified character consistent with other middle-class Victorian-era homes in Port Credit,

The house, as it appears today, was shaped by the home's second owner, Joseph Thompson. The L-shape plan of the present home, with the porch located in the recess of the "L", is typical of late Victorian (or "high Victorian") architecture. Thompson rebuilt the home larger than McGregor's original reflecting his own, and Canada's, increased wealth.

According to descendant, Etta M. Pearson, who owned 41 Bay Street from 1946 to 1978, the home was enlarged in 1900.

The porch, or verandah, is typical of this high Victorian era when people relaxed in the evening and looked forward to meeting their friends and neighbours as they strolled by along the nearby sidewalk. There were no strangers in this western neighbourhood of Port Credit. People knew their neighbours by their first name. The verandah is a prominent element of the street façade of 41 Bay Street.

Purist will bemoan the use of the term "Victorian" to describe any architectural style but in function the Victorian era in Canada was one of casual social interaction. The architecture of 41 Bay Street clearly defines this era of congeniality and is defined by it.

The following architectural description of the house is from the City of Mississauga's property database:

"This is a one and one half storey frame structure is constructed of wood and aluminum siding. The building is in a L-shape, with a covered porch in the "L" which wraps around to the side. There is a steep pitched gable roof with a peak dormer and asphalt shingles. Sky lights have been added to the rear roofline, but are not visible from the street. There is a four bay facade on the north face of the structure. A small portico covers the entrance in the projecting "L", in addition there is also an entrance under the porch. The pillars are squared with fan brackets under the eaves. The balustrade is also squared, all in proportion with the other porch detail. The windows are double hung one over one. There is a stone foundation, stuccoed. There is a garage on the property. The "L"-shaped, frame house occupies one of the original one-quarter acre lots in the village survey."



41 Bay Street: north elevation.



41 Bay Street: southward streetscape, with neighbouring buildings.



41 Bay Street: east elevation.



41 Bay Street: east elevation.



41 Bay Street: west elevation.



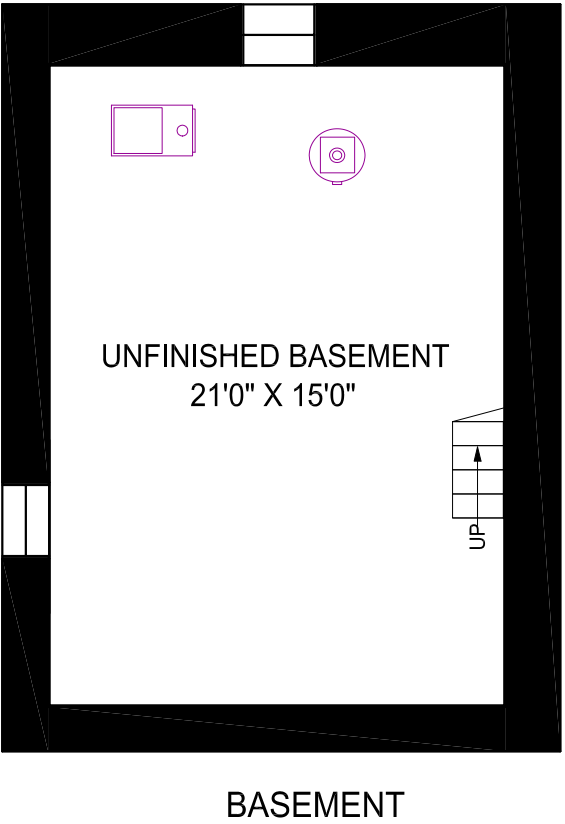
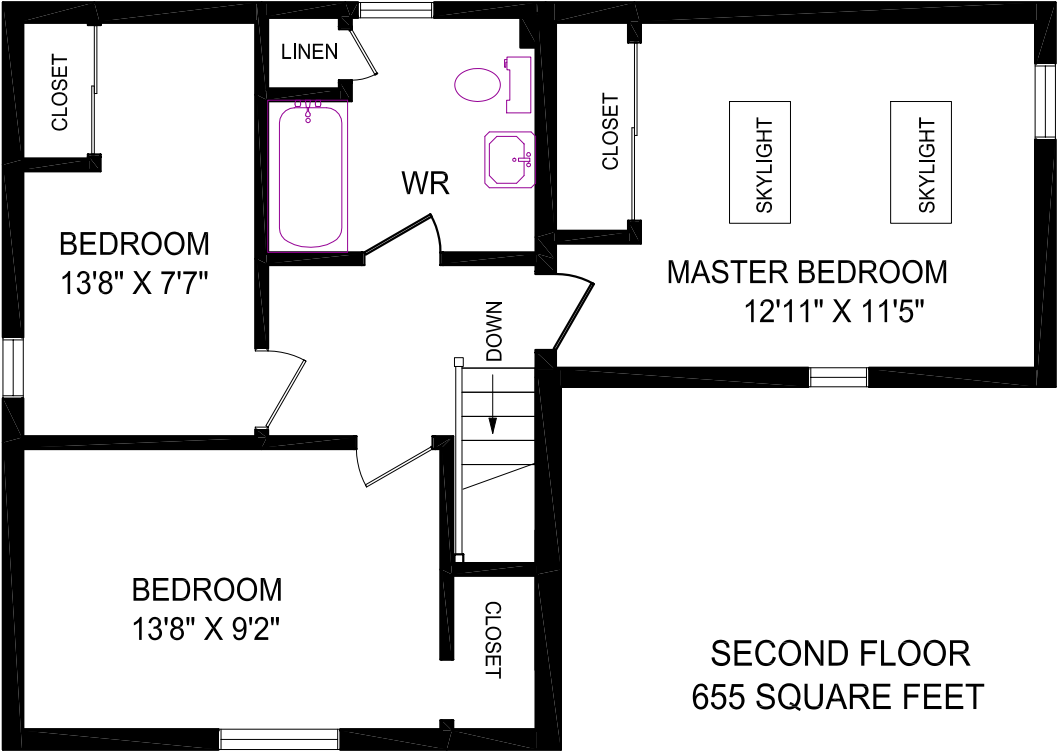
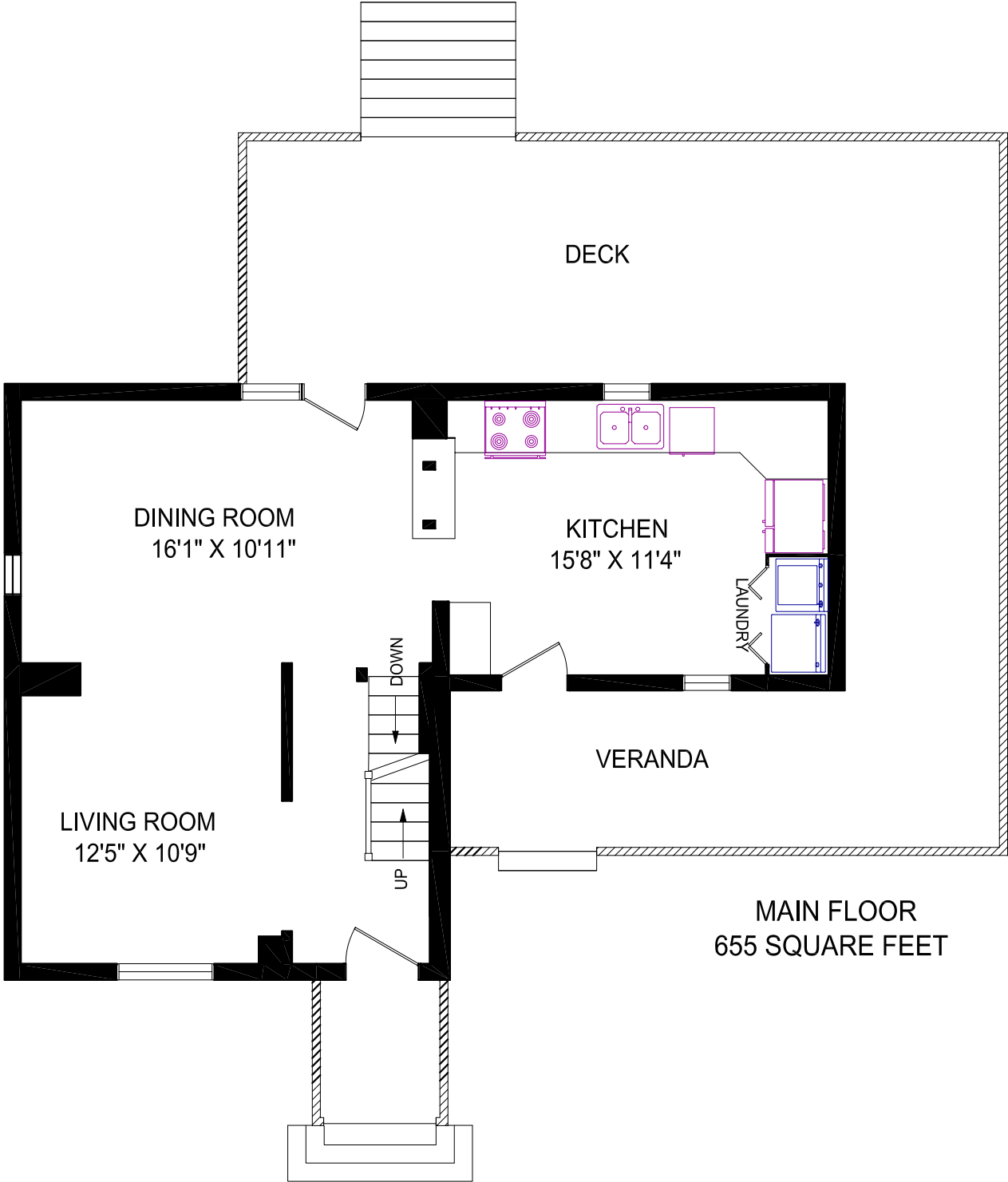
41 Bay Street: south streetscape looking east, with neighbouring buildings.



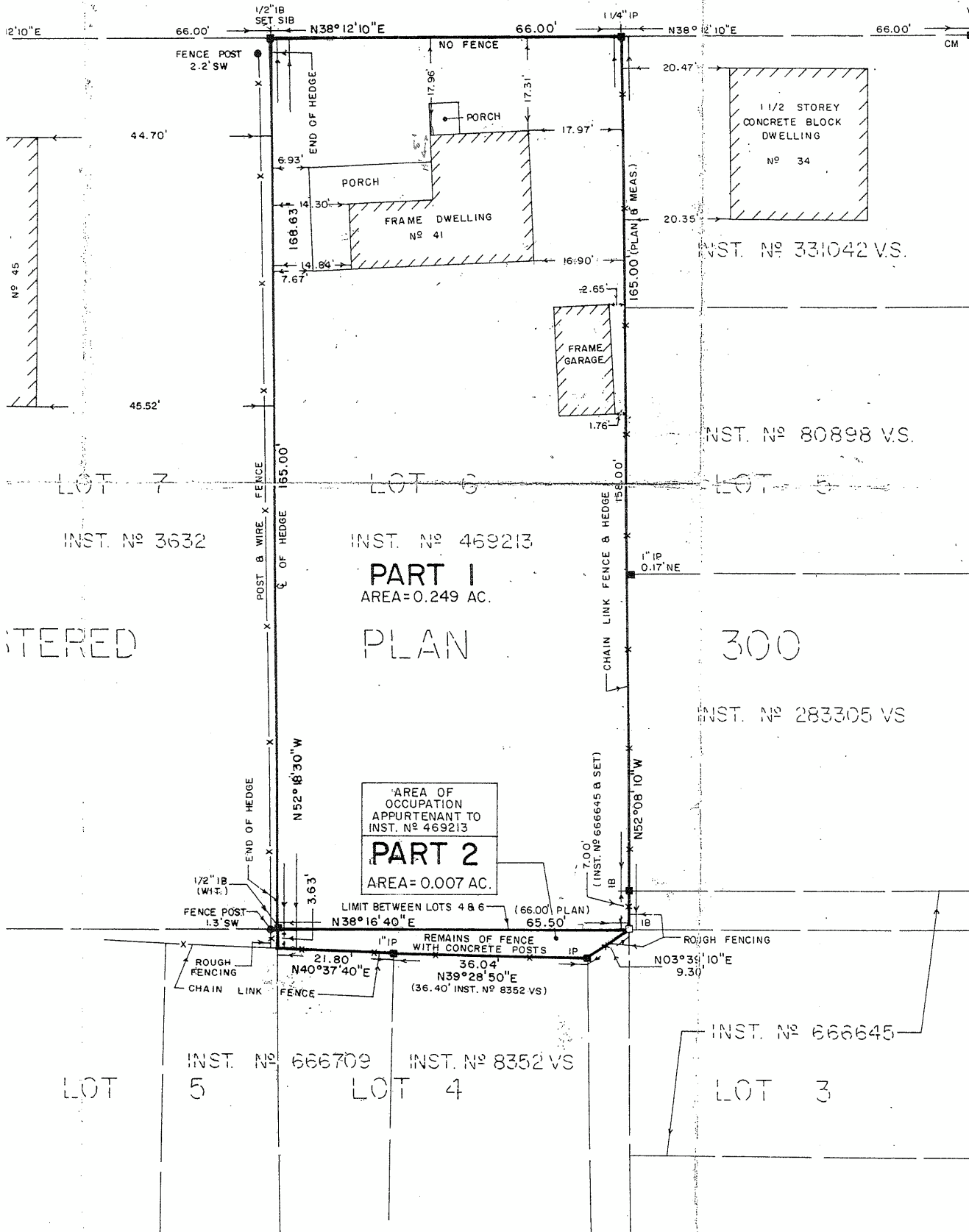
41 Bay Street: south elevation.



41 Bay Street: back yard.



BAY STREET



STREET
JOHN

3.0 HISTORICAL VALUE

3.1 History of the Building

Date of Construction

Like most of the historic buildings in this neighbourhood, it is not known when the house at 41 Bay Street was built.

Uncertainty is typical of homes built in the 19th century in Canada. For farm families who had to till their fields by hand, butcher their animals, preserve their fruits and vegetables for winter months, make their own farm implements and care for their children without the assistance of a doctor, the early settlers of Port Credit simply accepted homebuilding as another of life's necessities. The families who built the early homes in Port Credit, west of the Credit River, didn't bother to record the date of construction of their home. Building permits weren't required and there was no building inspector to assure adherence to building code regulation which hadn't yet been passed.

Homebuilding, as a profession, arrived when homes became more complex – notably with the arrival of electricity and plumbing. Some of the homes on Bay Street, further east, built after World War I, were constructed by professional contractors. Given the simplicity of the plan and the austere outside detailing, it is likely that 41 Bay Street was originally built by the first owner of the property, Alexander M. McGregor.

Aside from that, homes from this period typically wouldn't have a specific year of construction since "home-made" homes were typically built over many years as financial resources permitted, and when the family had the time – usually during the winter months when there was a lull in local Port Credit trades such as farming, fishing and stonehooking.

At best, it is possible to narrow the date of the construction of the present home at 41 Bay Street to sometime in the 1850s. This is based partly on the architectural styling (which reveals architectural elements common to homes built at that time) and also on the recollections of Etta Pearson – a descendent of Alexander McGregor.

Possible Early School House

One of Pearson's most notable memories of 41 Bay Street is the story she tells in regard to her grandmother, Elizabeth McGregor, who may have taught the children of the local families on the west side of Port Credit. Elizabeth was Alexander's wife.

The absence of a nearby school prior to 1893 lends credence to Pearson's claim. Toronto Township was conveniently subdivided into more-or-less square "school sections" based on the 1805 survey lines. Tucked into the extreme southeast corner of the township, Lakeview and Port Credit shared the same school section – S.S. #7. The practical farmers located the first school about midway within the section, at what is now Lakeshore Road East and Shaw Drive. In this location the school was equally convenient (or, more to the point, equally inconvenient) to students in Lakeview, well east of the school, and to Port Credit families, two kilometres to the west.

To accommodate the families on the west side of the Credit River, McGregor may have opened her house to local children.

Port Credit did not get its own school until 1893, when the disused S.S.

#19 designation for the region north of Streetsville was re-assigned specifically to Port Credit (separate from Lakeview, which became the independent holder of the S.S. #7 designation). Riverside Public School was built that year. However, by that time, the McGregors were no longer living in the home. The students probably met at 41 Bay Street for the last time sometime before December 1882, when 41 Bay Street was granted to Joseph Thompson.

3.2 History of the Property

Aside from possibly serving as the first “school” in Port Credit, the history of 41 Bay Street is not a monumental one. Rather, this home is a microcosm of the story of the historic neighbourhood for which it is part.

The homes, the streets and the natural environment of this neighbourhood (including the property at 41 Bay Street) are designated today under the terms of the Ontario Heritage Act as the “Old Port Credit Heritage Conservation District”, and deservedly so. Port Credit’s history started here.

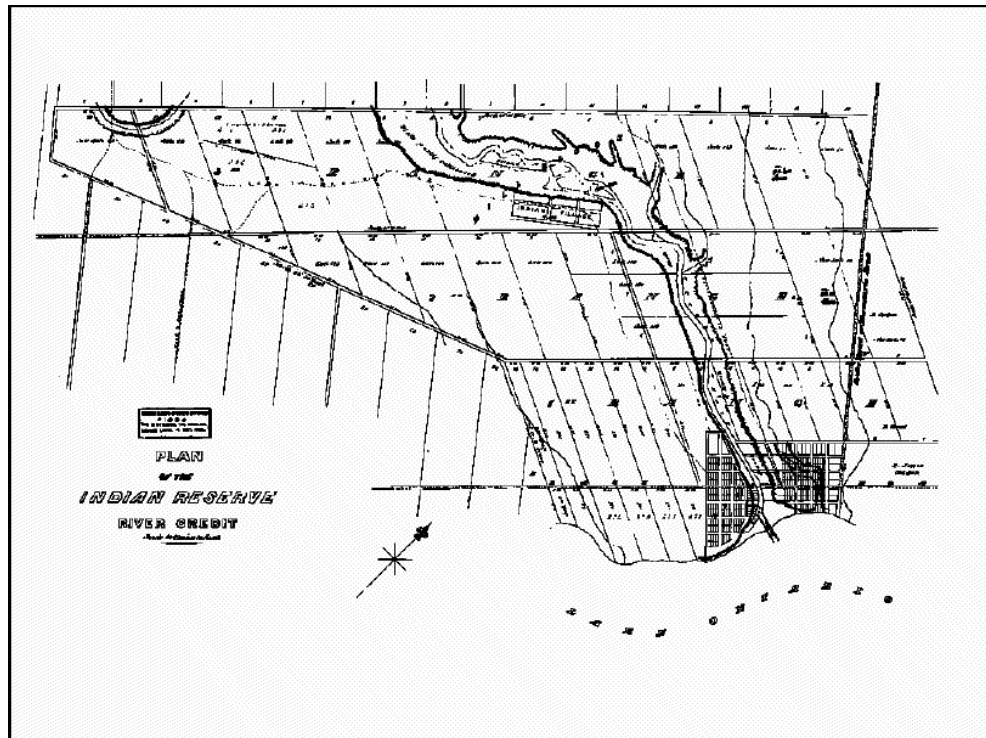
The Port Credit HCD is the oldest part of Mississauga’s youngest village. Mississauga was called Toronto Township when settlement of the area began in the first decade of the 19th century. The British crown purchased the land from Etobicoke Creek to Burlington Bay, southward from today’s Eglinton Avenue, from the Mississauga nation in 1805, but the Mississaugas wisely maintained exclusive rights to the land for one mile (1.6 km) on both sides of the Credit River, to preserve their sacred waterway and their main resource for proteins, derived from the Atlantic salmon that once teemed in the river.

The Mississauga sold most of this two-mile strip to the British crown in 1820 but by this time other villages, like Cooksville, Dixie and Sheridan (all on the Dundas Road) and the Merigold Settlement (on the lake shore, near the future site of Clarkson) were already thriving. Port Credit remained wilderness until 1834, when British investors anxious to profit from the potential trade to be realized at the mouth of the Credit, formed a partnership with the councilors of the Credit Mississauga Reserve (who had purchased land at the mouth of the river). Together these British and aboriginal businessmen built the first harbour in Toronto Township.

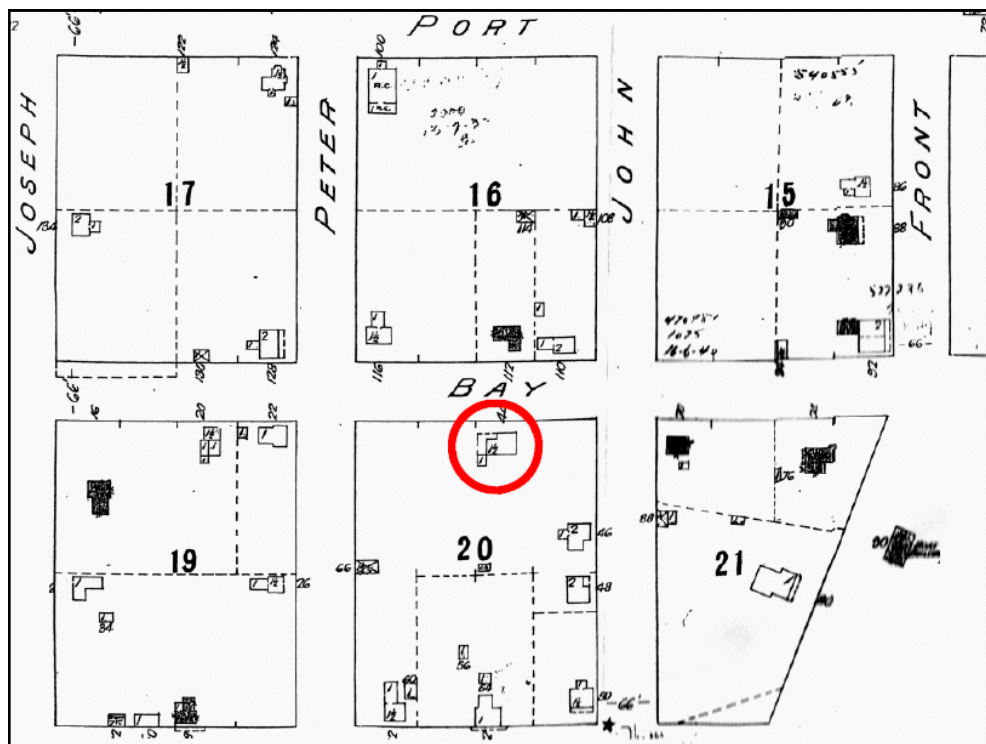
The 1830s was an opportune time to start a harbour at the mouth of the Credit River. Peel County was filling up quickly with loyal British settlers from the United States, plus a steady arrival of immigrants from across Europe looking for a better life in Canada. As these farmers cleared their land, they sent rafts of valuable timber down the Credit River. Once their land was clear these same settlers sent grain downstream to be sold in nearby Toronto. The grain had to be stored over the winter and the logical place to do this was at the mouth of the river.

Within a decade, Port Credit evolved from wilderness to one of the busiest ports in Canada West. Many of the historic homes that still stand in Port Credit’s HCD today were built during these early decades of growth. These buildings served as stores, inns and workers’ residences.

The house at 41 Bay Street is typical of the modest, but dignified wood frame homes built in this early Victorian era, when Port Credit was at its peak as a harbour town.



The Credit Indian Reserve.



41 Bay Street as seen on a 1910 fire insurance map.

3.3 List of Grantees

Land registry information for 41 Bay Street is incomplete. There is no entry in Book 1 of Peel County's land register. The first entry appears in Book 2, with the granting of the property on December 9, 1882 from Alexander McGregor to his son-in-law Joseph Thompson for \$25. However, Ontario Archives records indicate that McGregor did indeed own the land prior to 1882. Their records state that McGregor took possession of Lot 6 on August 16, 1850. Peel County data states that Lot 6 was originally owned by John Thompson who is recorded as having owned all the lots south of Bay Street.

McGregor's descendents lived in this home for more than 100 years. Joseph Thompson's sons Wentford and Graham transferred 41 Bay Street to their sister Etta, when she married William Pearson. Etta bequeathed the home to her daughter Pearl and her husband Richard A. Grant in 1978. The house left the family when the Grants sold 41 Bay Street to Roderick Billson in July 1984.

3.4 History of the Families

There is no record to indicate specifically why Alexander McGregor moved to Port Credit, but like most of the neighbourhood's early residents, he likely moved here either to work in some aspect of the harbour trade (as part of a crew of a vessel, or as a worker in a warehouse or grain storage elevator) or in an ancillary trade (as a carpenter or mason, for example) which would be in demand for construction and maintenance of harbour facilities.

If Elizabeth was a teacher, she would have been a volunteer teacher. Until 1850 (and the passing of the second Common Schools Act) most teachers worked for little or no pay. McGregor taught for the betterment of the community.

The school probably closed on or before 1882 when Joseph Thompson and his wife Victoria (née, Grafton) moved into 41 Bay Street.

Thompson was a common enough name at the time but there is a good chance that the Thompson, in this case, was related to the Thompsons who settled at what is now 48 Lake Street. The Thompson home still stands at the southwest corner of the same block as 41 Bay Street. John Thompson owned the block, including the property at 41 Bay Street. Alexander McGregor may have purchased his lot from the senior Thompson.

John Thompson moved to Port Credit West in 1840 as a farmer. He invested in the early harbour at Port Credit, on the west side of the river, and by 1852 he was the harbour master, responsible for the safe entry and departure of all freight and passengers ships at the harbour.

The younger Joseph Thompson (probably John's son) was a tailor in 1882 when ownership of 41 Bay Street was transferred to him upon marrying a daughter of Alexander McGregor.



Mississauga library image filed as "41 Bay St."; house is possibly 40 Lake St.

ONTARIO ARCHIVES LAND RECORD INDEX									
ALPHABETICAL LISTING BY NAME OF LOCATEE									
NAME OF LOCATEE	TOWNSHIP / TOWN / CITY	LOT	CONC.	DATE ID.	ISSUE DATE	TRANS. TYPE	TYPE OF LEASE / SALE	ARCHIVAL REFERENCE	
								RG SERIES	VOL PG
MACGLAUGHLIN HUGH RESIDENCE	MAPOC	N1/2 6	6	2	18490228	A	DEED NO.	01 C15	003 090
MACGLAUGHLIN WILLIAM RESIDENCE	CORNWALL (T)	955 FIFTH ST		2	18440827	A	DEED NO.	01 C15	001 397
MACGOWAN JOHN RESIDENCE	VESPRE	E 1/2 11	4	8	18600112	FG	FF DEED NO.	01 C13	054 222
MACGOWAN JOHN RESIDENCE	ORILLIA YORK	W 1/2 4	1NB	8	18320723	FG	M DEED NO.	01 C13	124 114
MACGOWEN HUGH RESIDENCE	SHERBROOKE S SHERBROOKE S	NE 1/2 19	7	8	18360218	FG	ME DEED NO.	01 C13	096 140
MACGOWN CATHERINE RESIDENCE	ST VINCENT KINGSTON	12	6	8	18330725	FG	DUE DEED NO.	01 C13	084 005
MACGRATH DENNIS RESIDENCE	PITTSBURGH	N1/2 2	6	5	18360131	S	DEED NO.	CC B3	024 142
MACGRATH MICHAEL RESIDENCE	HUNTLEY	S1/2 24	11	2	18570523	A	DEED NO.	01 C15	003 135
MACGREGOR ALEXANDER RESIDENCE	PORT CREDIT	6E BAY ST		5	18500816	S	DEED NO.	01 C117	003 016
MACGREGOR ALEXANDER RESIDENCE	ROXBOROUGH	S1/2 1	8	2	18470416	A	DEED NO.	01 C15	001 508
MACGREGOR ANN RESIDENCE	KENYON KENYON	11&12	11	5	18500718	S	DEED NO.	01 C117	003 017
MACGREGOR ARCHIE A RESIDENCE	PARDEE TORONTO	NW1/4 11	8	1	19040203	FG	DEED NO.	01 CV118	006 229
MACGREGOR DUNCAN RESIDENCE	CHARLOTTENBURGH			8	18370216	FG	SUE DEED NO.	01 C13	085 009
MACGREGOR JOHN RESIDENCE	GREENOCK	3	1NDR			FG	DEED NO.	01 C13	149 079
MACGREGOR JOHN RESIDENCE	WINCHESTER	10	12	2	18531026	A	DEED NO.	01 C15	002 146

Ontario Archives record of land transfer to Alexander McGregor.

65191

Mr John Thompson Senr.
Port Credit
Ont

January 20 1886

Sir,

In reply to the
inquiries contained in your
letter of the 8th instant
respecting Lots 6, 7 & 8
west of Bay St and west
of the River Credit I
have to inform you that
these Lots were Presented
to the Hon A McLean
in 1854.

Request for information on Lot 6, by John Thompson, 1886 - 1.

No. 65191	ACTION.	ACTION CONTINUED.
Department of Indian Affairs. 1886	32/34-2	RED 65191 489578
John Thompson Esq. Port Credit 11/Jan		
For advice in re Lots 6, 7 & 8 west of Bay St & west of the River Port Credit.		
Jan 22 let of Thompson to Dept of Indian Affairs 6 Sept 1886 Joseph Thompson 1/3 Sept. let of Thompson to Dept of Ind for Thompson		

Request for information on Lot 6, by John Thompson, 1886 - 2.

DATE PLAN REGISTERED..... 3 Oct. 1927		Abstract Index		Page 1		
OWNERS..... Crown Survey		Répertoire par lot		300		
LOTS SUBDIVIDED..... Town Plot 4		Plan/Concession		West of Credit River		
DAY MON YR		Lot 6		BAY S. Side		
Instrument Number No. de l'instrument	Instrument Type Type d'acte	Parties from Parties de	Parties to Parties à	Consideration Contenance	Land/Remarks Branche/Observations	
3785	Q.C.	9 Dec 1882	Alex M. McGregor	Joseph Thompson	\$25.00 All	
300	Plan	3 Oct 1927	Municipal Survey made under Instructions of Minister of Lands and under Provisions of the Surveys Act			
1674	Tax Deed	10 May 1945	Willaco of Port Credit	Harold U. Hare	\$417.21 All	
1702	Grant	7 June 1945	Harold U. Hare et ux	Montford L. Thompson	\$600.00 All	
2191	Grant	31 Oct 1946	Graham V. Thompson	Etta M. Pearson	\$1000.00 All. Consents (O.L.) for Montford L. Thompson. Est-attached	
460350	Cert	3 Mar 1978	Treasurer's Consent	Etta M. Pearson Est.	All re: 2191	
469213	Grant	31 Mar 1978	Alfred J. Pearson & George M. Pearson et ux of Etta M. Pearson et ux	Richard A. Grant & Pearl Grant as joint tenants	\$1.00 & C All	
6 DATE 9/10/86 607311	Grant	16 07 84	GRANT, Richard A. GRANT, Pearl	BILLSON, Roderick	All	
Deposit			See Deposit No. 690591		RE: No. 469213	
774826	Mort.	31-08-27	BILLSON, Roderick	Canotian Imperial Bank of Commerce	170,000.00 All DISCHARGED BY \$ 40 1579 580 ASST. DEP. LAND REG. L.R. 94 1115 DISCHARGED BY \$ 40 1159 11 ASST. DEP. LAND REG. L.R. 95 03 04	
80-1065594	Charge	24-05-10	BILLSON, Roderick	CIBC Mortgage Corporation	100,000.00 All	
M0 1104068	Transfer	95 11 30	BILLSON, Roderick	MORRIS, Stephen Crawford	222,000.00 All	
RD 1104059	Charge	95 11 30	MORRIS, Stephen Crawford	The Toronto-Dominion Bank	216,172.60 All	
Entered in G.R. 95.12.21 ADLER.						
Roll 104069 Charge 95.11.30 MORRIS, Stephen Crawford The Toronto-Dominion Bank 216,172.60 ALL						
Re-ordered 95.12.21 ADLER PC.						

List of Grantees.

4.0 CONTEXTUAL VALUE

4.1 A Part of the Historic Community

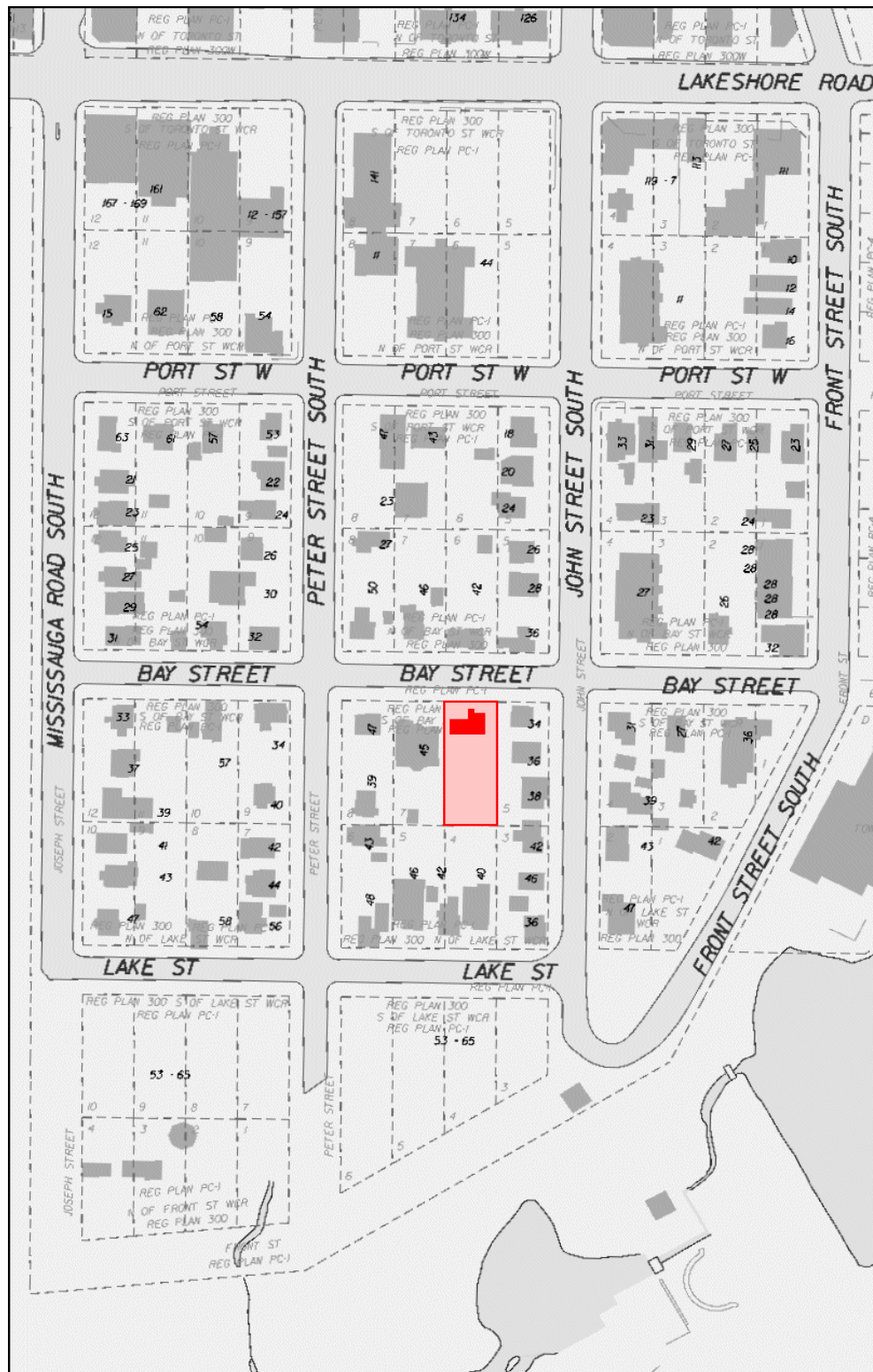
As mentioned, 41 Bay Street is at its finest in context of the architectural and historic setting of the heritage conservation district in which it stands.

The home is of the same massing and height as the historic houses on the same street and on the same block, defined by Bay, John, Lake and Peter streets.

Like most of the homes in the HCD that have been classified as “buildings of historical significance”, 41 Bay Street is of wood frame construction. Compared to stone and brick (which was favoured by affluent homeowners), lumber was inexpensive and balloon frame construction with “two by fours” was simple and practical for do-it-yourself homebuilders.

Like its Victorian contemporaries, the scale of 41 Bay Street is modest compared to the modern homes in the HCD or the historic homes altered prior to the implementation of the HCD. In today’s “monster homes”, the family has a den and playroom and walk-in closets. There’s space for an office or two and every child has his or her own bedroom. The McGregors and the Thompsons, by comparison, lived at a time when home life was humble and honest. Their children shared not just a bedroom but also the same bed – girls in one room, boys in the other. That’s how they stayed warm on cold winter nights. The playroom was the workroom. The second floor was built as a “half storey” (with sloped ceilings) ostensibly to avoid paying additional land taxes to the Queen (for having a second floor), but more often the smaller, half-floor upstairs was installed to conserve heat. If only children used the second floor, and only for sleeping, there was little need for it to be full-height. As with its neighbouring buildings, 41 Bay Street was initially a one-and-a-half storey home until altered in the early 20th century.

Unlike the sunrooms and solariums of today’s homes, sheltered to the rear of the house, the verandah at the front of 41 Bay Street, at street level and just a few metres from the sidewalk, was the place where residents relaxed through casual socialization with their friends in the neighbourhood.



The location of 41 Bay Street in the Port Credit Heritage Conservation District.

5.0 ASSESSMENT

5.1 Elements that Contribute to Design and/or Physical Value

- The home reveals many of the common architectural characteristics and construction materials of high Victorian-era homes, which are rarely used in modern buildings in Mississauga.

5.2 Elements that Contribute to Historical Value

- 41 Bay Street may be the site of Port Credit's first school.
- Otherwise it was still the home of one of Port Credit's earliest families.

5.3 Elements that Contribute to the Contextual Value

- The massing, height, plan, architectural style and construction materials of 41 Bay Street are consistent with other "historically significant" homes in the Heritage Conservation District.

5.4 Dates of Significance

1805	The British crown purchases the "Mississauga Tract" from the Mississauga nation, excluding the Credit River allotment which includes the property for 41 Bay Street.
1820	The Mississauga nations sells the land on which 41 Bay Street now stands to the British crown.
1834	The first homes are built on the west side of the Credit River in the village still known formally just as "Credit".
1850s	Alexander McGregor builds 41 Bay Street.
1884	Joseph Thompson inherits the home.
1900	According to an account by Etta M. Pearson, Thompson enlarges the home, adding the Victorian architectural elements.
1946	Etta M. Pearson inherits 41 Bay Street..
1978	Richard A. and Pearl Grant inherit 41 Bay Street.
1984	41 Bay Street is sold to Roderick Billson.
1987	41 Bay Street is enlarged. Repairs are made to the foundation.
1988	An in-ground pool is installed to the south end of the property.
1995	Property is sold to Steven Crawford Morris.
1999	Property is sold to Elizabeth Maureen Guy.
2000	Property is sold to Jack Lord.
2003	Two skylights are added to roof line. These are not visible from street level.
2004	The house is designation under Part V on the Ontario Heritage Act, along with other buildings in the newly-established Old Port Credit Heritage Conservation District.
2009	Garage and pool are removed.
2010	Repairs are made to the verandah and foundation. The owners receive a grant of \$2,061 from Mississauga's Heritage Advisory Committee towards repairs.
2011	Property is sold to

6.0 REGULATION 9 / 06

A municipal council may designate heritage resources by by-law pursuant to Section 29 of the Ontario Heritage Act based on criteria set forth in Ontario Regulation 9 / 06; Criteria for Determining Cultural Heritage Value or Interest.

Subsection 1

The property has design value or physical value because it;

- i: is a rare, unique, representative or early example of a style, type, expression, material or construction method,
- ii: displays a high degree of craftsmanship or artistic merit, or
- iii: demonstrates a high degree of technical or scientific achievement.

Subsection 2

The property has historical value or associative value because it;

- i: has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,
- ii: yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or
- iii: demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.

Subsection 3

The property has contextual value because it is;

- i: important in defining, maintaining or supporting the character of area,
- ii: physically, functionally, visually or historically linked to its surrounding,
- iii: a landmark.

6.1 Analysis of Compliance with Section 29

As summarized in item 5.0, page 23, the subject property exhibits the following merits for designation under the Ontario Heritage Act.

Subsection 1

The property has design value or physical value because it;

- i: is an early example of a style.

Subsection 2

The property has historical value because it;

- ii: yields information that contributes to an understanding of a community.

Subsection 3

The property has contextual value because it is;

- i: important in defining and maintaining the character of the area,
- ii: physically, functionally, visually and historically linked to its surrounding.

6.2 Summary of Suitability for Designation

Under the terms of the Ontario Heritage Act a property needs to comply with any one of the items of Regulation 9/06, in any one of the three subsections of Clause 29 of the Ontario Heritage Act to be considered suitable for designation.

The subject property complies with one item of Subsection 1 of Regulation 9/06 in that it is an early example of a style.

The subject property complies with one item of Subsection 2 of Regulation 9/06 in that it yields information that contributes to an understanding of a community.

The subject property complies with two items of Subsection 3 of Regulation 9/06 in that it is important in defining and maintaining the character of the area, and because it is physically, functionally, visually and historically linked to its surrounding,

6.3 Conclusion

The subject property is already designated under the terms of Part V of the Ontario Heritage Act. This report therefore concludes that any changes to the building and property at 41 Bay Street in Mississauga, Ontario respect the four items of Regulation 9/06 for which the building and property complies.

Alterations to the building must not alter architectural features that define the house as an early example of a middle-class Victorian-era residence or to alter the position of the home on the property and the massing of the home in any manner that would detract from the home's existing physical, functional, visual and historical link to the Old Port Credit Heritage Conservation District.

The following items provide information on the proposed alterations to the building and property at 41 Bay Street, Mississauga.

7.0 PROPOSED ALTERATIONS

7.1 Plans and Elevations

- site plan
- north (front) and west elevations
- east and south (back) elevations
- first floor plan
- second floor plan

7.2 Old Port Credit Village Architectural Statement Table

41 Bay Street is part of the Old Port Credit Heritage Conservation District, established in June 2004.

Architect George Robb developed a set of objectives to guide property owners in the heritage conservation district.

Section 7.2 of this report reviews these objectives and clarifies how the proposed additions to the current heritage building at 41 Bay Street comply with those objectives.



North Elevation

West Elevation

41 Bay St.
Heritage Addition

Z05

North & West Elevations
June 30, 2016
Scale: 1/4" = 1'-0"



East Elevation

South Elevation

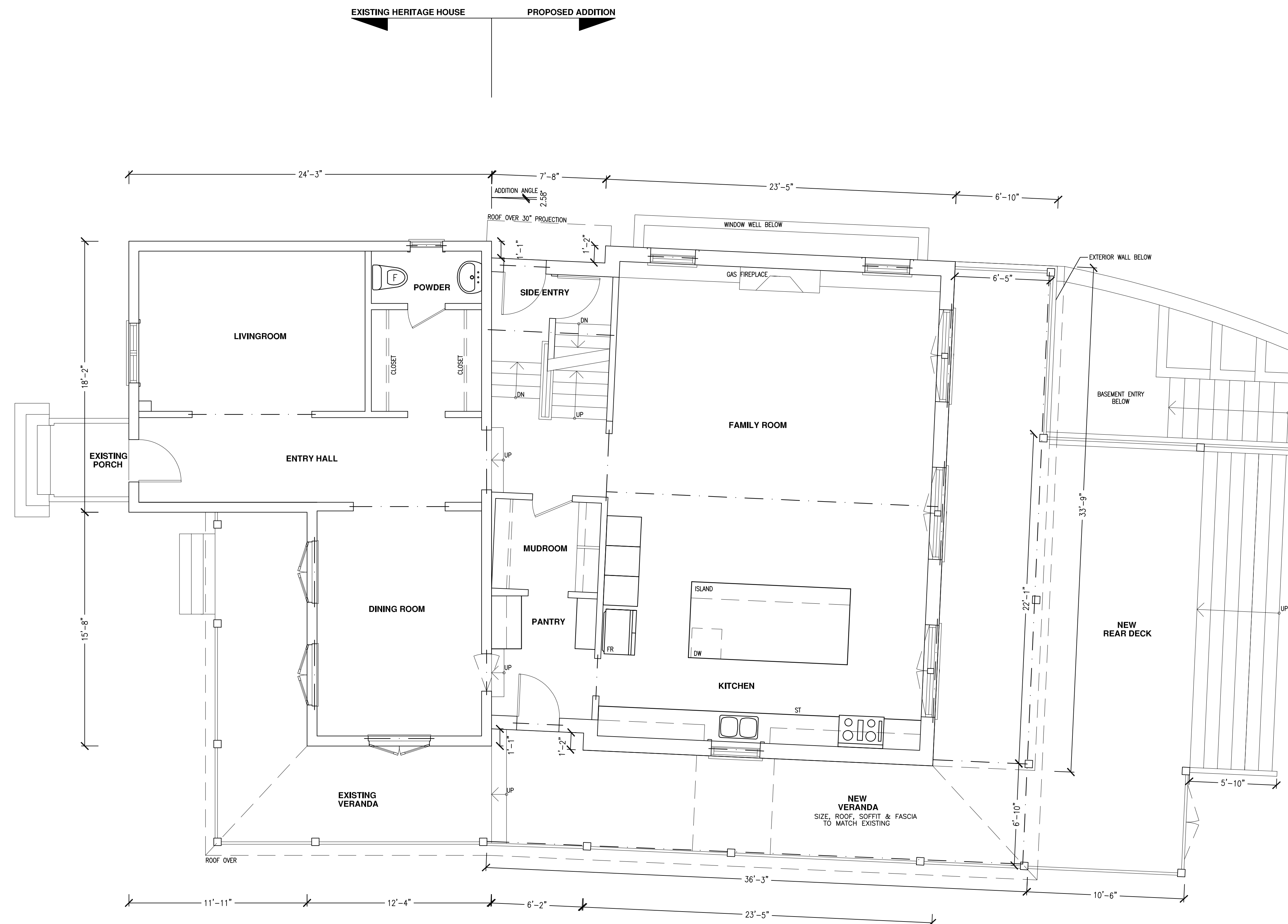
41 Bay St.

Heritage Addition

Z06

East & South Elevations

June 30, 2016
Scale: 1/4" = 1'-0"

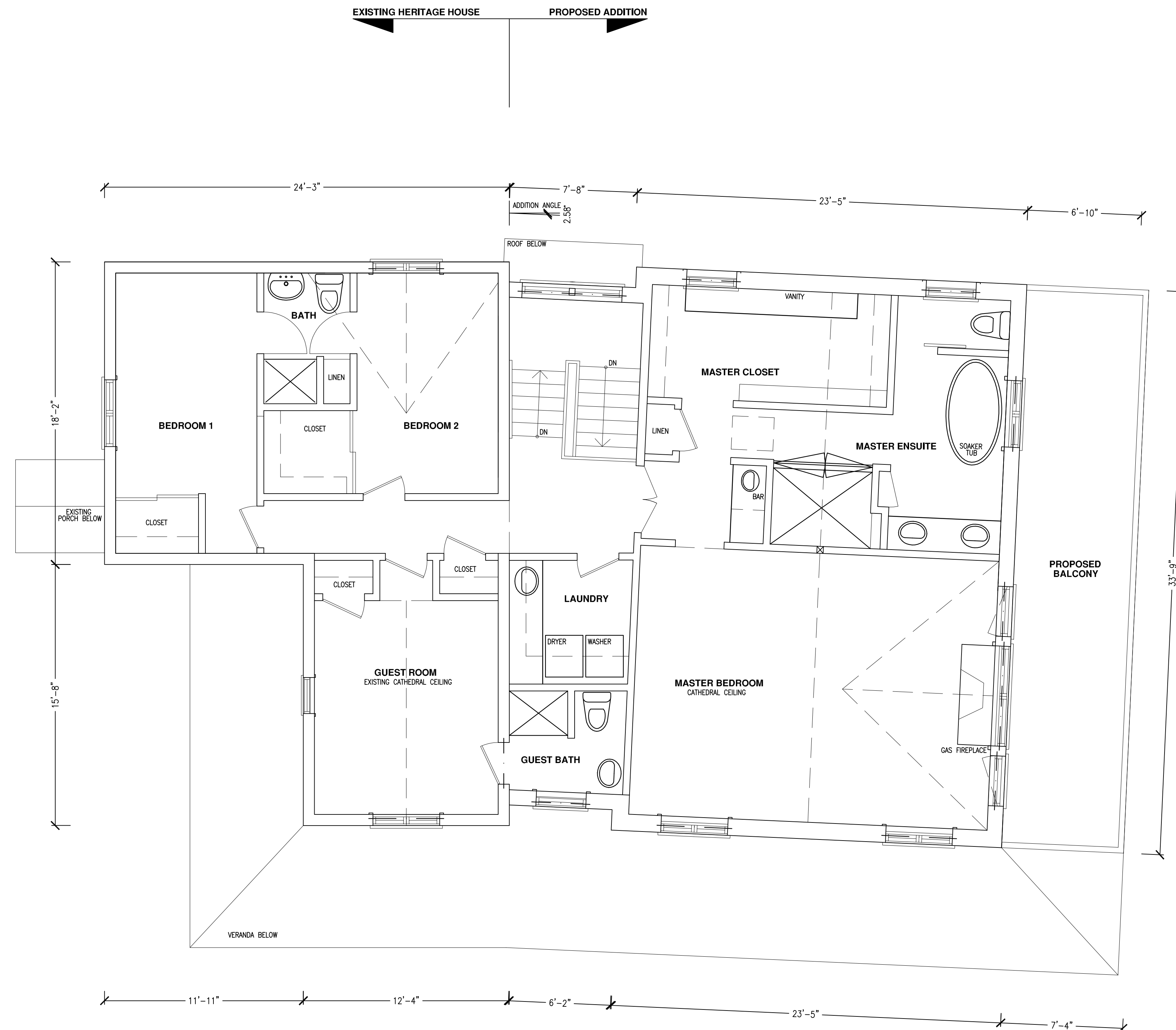


41 Bay St.
 Heritage Addition

Z03

1st Floor Plan

April 24, 2016
 Scale: 1/4" = 1'-0"

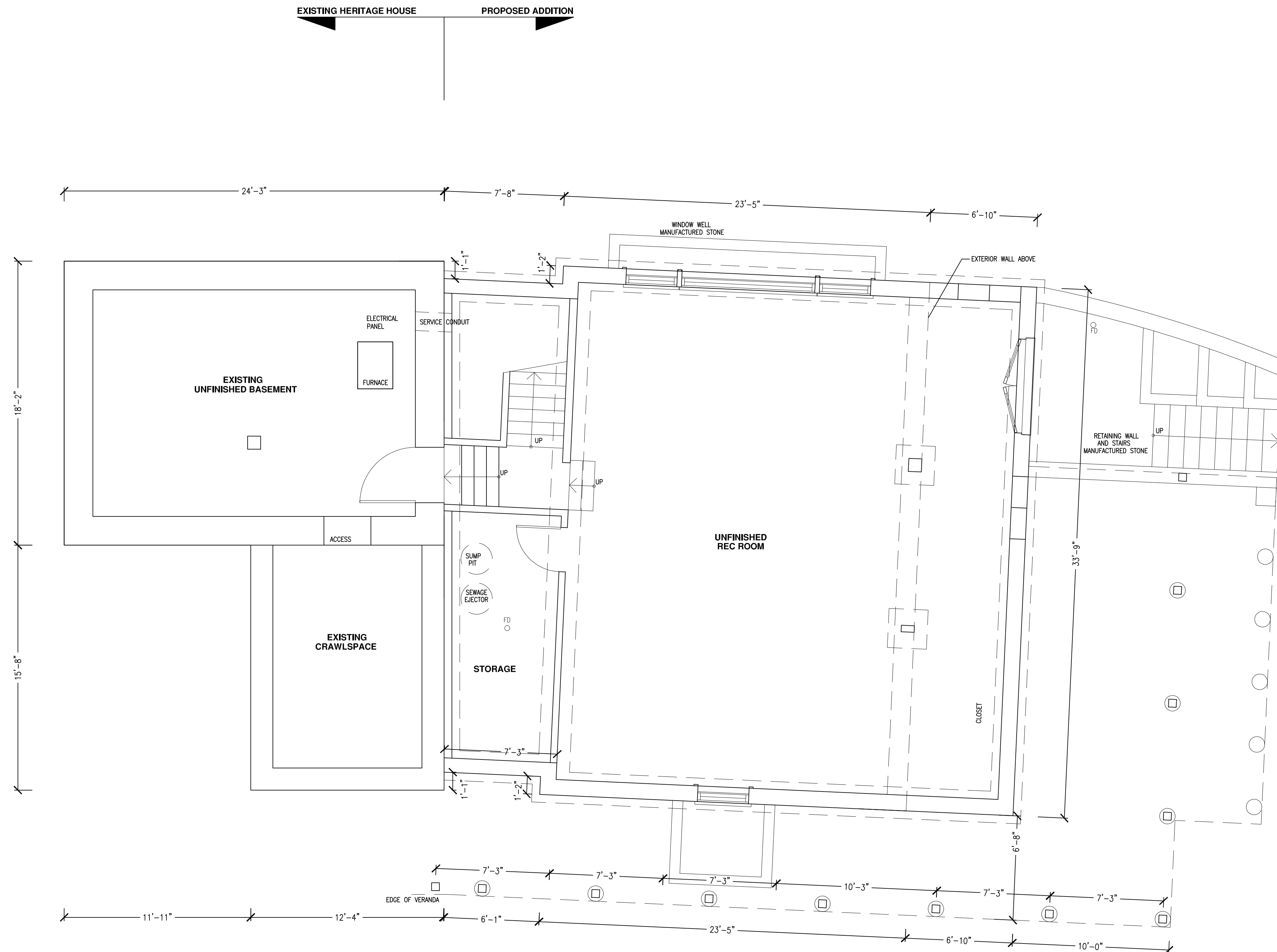


41 Bay St.
Heritage Addition

Z04

2nd Floor Plan

June 30, 2016
Scale: 1/4" = 1'-0"

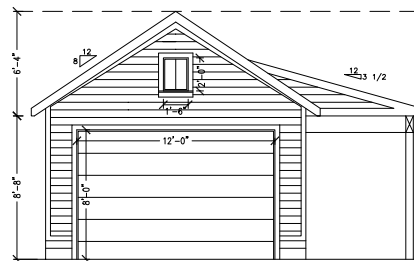
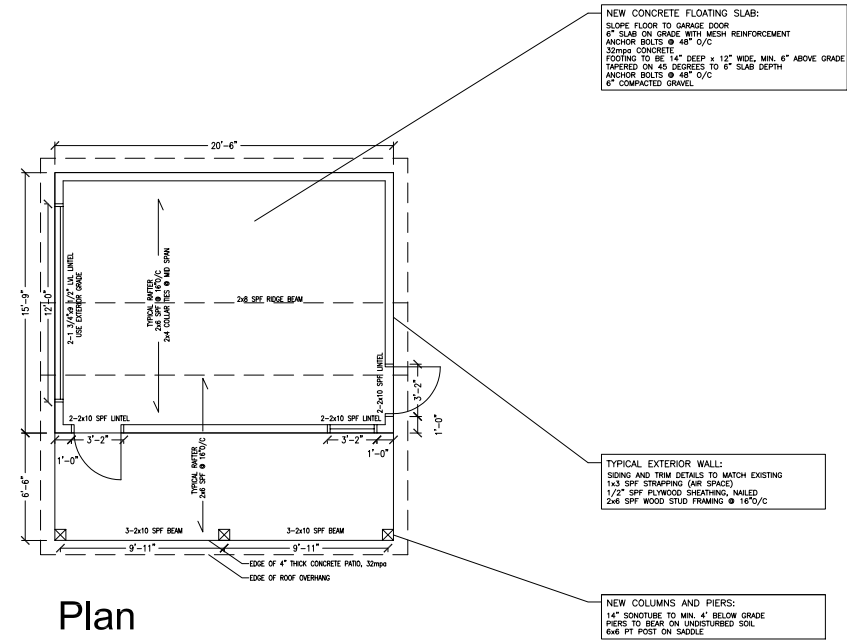


41 Bay St.
Heritage Addition

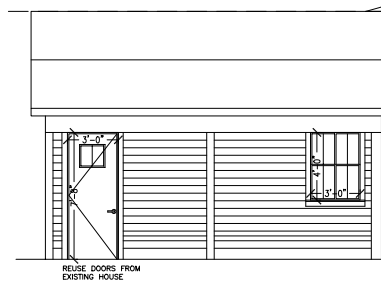
Z02

Basement Floor Plan

June 30, 2016
Scale: 1/4" = 1'-0"



North Elevation



West Elevation

ROOF CONSTRUCTION:
CONTINUOUS ROSE VENT
ASPHALT SHINGLES
ICE AND WATER SHEILD MEMBRANE
5/8" PLYWOOD SHEATHING, GLUED AND NAILED
ROOF FRAMING MEMBERS
SOFFIT AND FASCIA DETAILS TO MATCH EXISTING

METRIC
DISTANCES SHOWN ON THIS PLAN ARE
IN METRES AND CAN BE CONVERTED
TO FEET BY DIVIDING BY 0.3048

7:1 - 50

DEVELOPMENT & DESIGN CONSTRUCTION HOARDING

1. HOARDING DETAILS TO BE DETERMINED FOLLOWING INITIAL SITE INSPECTION.
2. HOARDING TO BE APPROVED BY DEVELOPMENT AND DESIGN.
3. HOARDING NOT TO BE SUPPLIED, INSTALLED, AND MAINTAINED BY THE APPLICANT THROUGHOUT ALL PHASES OF CONSTRUCTION UNLESS APPROVED BY DEVELOPMENT AND DESIGN.
4. TO NOT ALLOW WATER TO COLLECT AND POSE DRAINAGE OR OTHER HAZARDS.
5. 1" X 4" POSTS FOR SOLID HOARDING WILL ONLY BE ALLOWED WITH THE APPROVAL FROM DEVELOPMENT AND DESIGN.
6. 5.08m (16'6") HOARDING SHALL NOT BE ACCEPTED FOR SOLID HOARDING.

SCALE: N.T.S.
DATE: JUNE, 2014

MISSISSAUGA Development and Design

CONFORMITY TO SITE PLAN
I HEREBY CERTIFY THAT THIS DRAWING CONFORMS IN ALL RESPECTS TO THE SITE DEVELOPMENT PLANS AS APPROVED BY THE CITY OF MISSISSAUGA UNDER THE FILE NO. SP1 16-073 W1.

DATE: JULY 25, 2016 SIGNED: *[Signature]*

EXISTING TREE PRESERVATION
THE OWNER IS RESPONSIBLE FOR ENSURING THAT THE TREE PROTECTION HOARDING IS MAINTAINED THROUGHOUT ALL PHASES OF DEMOLITION AND CONSTRUCTION IN THE LOCATION AND CONDITIONS APPROVED BY THE PLANNING AND BUILDING DEPARTMENT. NO MATERIALS (BUILDING MATERIALS, SOIL, ETC.) MAY BE STOCKPILED WITHIN THE AREA OF HOARDING. FAILURE TO MAINTAIN THE HOARDING AS SOLEMNLY APPROVED, OR THE STORAGE OF MATERIALS WITHIN THE HOARDING WILL CAUSE FOR THE LETTER OF CREDIT TO BE HELD FOR TWO (2) YEARS FOLLOWING COMPLETION OF ALL SITE WORKS.

DATE: MAY 3, 2016 OWNERS SIGNATURE: *[Signature]*

SHOULD THE INSTALLATION OF BELOW GROUND SERVICES REQUIRE HOARDING TO BE REMOVED, PLANNING AND BUILDING STAFF ARE TO BE CONTACTED PRIOR TO THE COMMENCEMENT OF SUCH WORK. SHOULD AN ALTERNATE SERVICE ROUTE NOT BE POSSIBLE STAFF WILL INSPECT AND DOCUMENT THE CONDITION OF THE VEGETATION AND SERVING INSTALLATION IN ORDER TO MINIMIZE DAMAGE TO THE VEGETATION.

HOARDINGS MUST BE INSPECTED PRIOR TO REMOVAL OF ANY TREE PROTECTION HOARDING FROM THE SITE.

INSTALLATION OF HOARDING
THE APPLICANT WILL BE REQUIRED TO CONTACT ALL UTILITY COMPANIES TO OBTAIN ALL REQUIRED LOCATES PRIOR TO THE INSTALLATION OF HOARDING WITHIN THE MUNICIPAL RIGHT OF WAY.

PRIOR TO ANY CONSTRUCTION TAKING PLACE, HOARDING ADJACENT TO EXISTING PROPERTIES TO PROTECT FROM CONSTRUCTION ACTIVITY, AND ALL REQUIRED HOARDING IN ACCORDANCE WITH THE ONTARIO OCCUPATIONAL HEALTH AND SAFETY ACT AND REGULATIONS FOR CONSTRUCTION PROJECTS MUST BE ERECTED AND MAINTAINED THROUGHOUT ALL PHASES OF CONSTRUCTION.

BUILDING PERMIT DRAWINGS
THE CITY OF MISSISSAUGA REQUIRES THAT ALL WORKING DRAWINGS SUBMITTED TO THE BUILDING DIVISION, PLANNING AND BUILDING DEPARTMENT AS PART OF AN APPLICATION FOR THE ISSUANCE OF A BUILDING PERMIT SHALL BE CERTIFIED BY THE ARCHITECT OR ENGINEER AS BEING IN CONFORMITY WITH THE SITE DEVELOPMENT PLANS AS APPROVED BY THE CITY OF MISSISSAUGA.

Average grade calculations

	LOCATION	EXIST. GRADE	EXIST. GRADE
1	CENTRE LINE OF ROAD	79.55	79.64
2	FRONT CORNER	79.81	79.79
3	MIN. FRONT SETBACK	79.79	79.89
4	15 m BACK	79.30	79.70
SUM OF GRADE ELEVATIONS=		637.47	
AVERAGE GRADE [metres] =		79.68	

EXTERIOR LIGHTING

ALL EXTERIOR LIGHTING WILL BE DIRECTED ONTO THE SITE AND WILL NOT INFRINGE UPON THE ADJACENT PROPERTIES.

RETAINING WALLS

THE STRUCTURAL DESIGN OF ANY RETAINING WALL OVER 0.6m IN HEIGHT OR ANY RETAINING WALL LOCATED ON A PROPERTY LINE IS TO BE SHOWN ON THE SITE GRADING PLAN FOR THIS PROJECT AND IS TO BE APPROVED BY THE CONSULTING ENGINEER FOR THE PROJECT.

EXISTING GRADES

ALL EXISTING GRADES AROUND THE PERIMETER AND THE EXISTING GRADING PATTERN OF THE SITE SHALL BE MAINTAINED.

GRADING

GRADES WILL BE MET WITHIN 3% MAXIMUM SLOPE AT THE PROPERTY LINES AND WITHIN THE SITE.

DRIVEWAY

THE PORTIONS OF THE DRIVEWAY WITHIN THE MUNICIPAL BOULEVARD WILL BE PAVED BY THE APPLICANT.

EXISTING WELLS/ SEPTIC SYSTEMS

IF A WELL IS DISCOVERED, IT WILL BE DECOMMISSIONED IN ACCORDANCE WITH THE ONTARIO WATER RESOURCES ACT REGULATION 903 (FORMERLY 61284) AND ANY OTHER APPLICABLE REGULATIONS AND GUIDELINES.

ANY SEPTIC SYSTEM ON SITE WILL BE DECOMMISSIONED AND REMOVED ACCORDING TO ALL APPLICABLE GUIDELINES AND REGULATIONS.

SUMP PUMP

FOUNDATION WEEPERS AND FLOOR DRAINS WILL BE SUMPED AND PUMPED TO THE SURFACE AND WILL DISCHARGE ONTO A CONCRETE SPLASH PAD IN THE FRONT OF THE HOUSE.

RAIN WATER

ALL DOWNSPOUTS SHALL DISCHARGE ONTO SPLASH PADS AT GROUND LEVEL AT THE LOCATIONS INDICATED ON THIS PLAN.

PROPOSED SWALES TO HAVE MAXIMUM 1:3 SIDE SLOPES.

WATERMANS AND WATER SERVICE

ALL MATERIALS AND CONSTRUCTION METHODS MUST CORRESPOND TO THE CURRENT PEEL PUBLIC WORKS STANDARDS AND SPECIFICATIONS

WATERMANS AND/OR WATER SERVICE MATERIALS 100 mm (4") AND LARGER MUST BE PVC. SIZE 50 mm (2") AND SMALLER MUST BE COPPER 'K'.

WATERMANS AND/OR WATER SERVICES ARE TO HAVE A MINIMUM COVER OF 1.7 m (5'6") WITH A MINIMUM HORIZONTAL SPACING OF 1.2 m (4') FROM THEMSELVES AND ALL OTHER UTILITIES

PROVISIONS FOR FLUSHING WATER LINE PRIOR TO TESTING, ETC., MUST BE PROVIDED WITH AT LEAST A 50 mm (2") OUTLET ON 100 mm (4") AND LARGER LINES. COPPER LINES ARE TO HAVE FLUSHING POINTS AT THE END. THE SAME SIZE AS THE LINE. THE MUST ALSO BE HOSED OR PIPED TO ALLOW THE WATER TO DRAIN INTO A PARKING LOT OR DOWN A DRAIN. ON FIRE LINES, FLUSHING OUTLET TO BE 100 mm (4") DIAMETER MINIMUM ON A HYDRANT

ALL CURB STOPS TO BE 3.0 m (10') OFF THE FACE OF THE BUILDING UNLESS OTHERWISE NOTED

HYDRANT AND VALVE SET TO REGION STANDARD 1-4-1 DIMENSION A AND B, 0.7 m (2') AND 0.9 m (3') AND TO HAVE PUMPER NOZZLE

WATERMANS TO BE INSTALLED TO GRADES AS SHOWN ON APPROVED SITE PLAN. COPY OF GRADE SHEET MUST BE SUPPLIED TO INSPECTOR PRIOR TO COMMENCEMENT OF WORK, WHERE REQUESTED BY INSPECTOR

WATERMANS MUST HAVE A MINIMUM CLEARANCE OF 0.3 m (12") OVER / 0.5 m (20") UNDER SEWERS AND ALL OTHER UTILITIES WHEN CROSSING

ALL PROPOSED WATER PIPING MUST BE ISOLATED FROM EXISTING LINES IN ORDER TO ALLOW INDEPENDENT PRESSURE TESTING AND CHLORINATING FROM EXISTING SYSTEMS

ALL LIVE TAPPING AND OPERATION OF REGION WATER VALVES SHALL BE ARRANGED THROUGH THE REGIONAL INSPECTOR ASSIGNED OR BY CONTACTING THE OPERATIONS AND MAINTENANCE DIVISION

LOCATION OF ALL EXISTING UTILITIES IN THE FIELD TO BE ESTABLISHED BY THE CONTRACTOR

THE CONTRACTOR(S) SHALL BE SOLELY RESPONSIBLE FOR LOCATES, EXPOSING, SUPPORTING AND PROTECTING OF ALL UNDERGROUND AND OVERHEAD UTILITIES AND STRUCTURES EXISTING AT THE TIME OF CONSTRUCTION IN THE AREA OF THEIR WORK, WHETHER SHOWN ON THE PLANS OR NOT AND FOR ALL REPAIRS AND CONSEQUENCES RESULTING FROM DAMAGE TO SAME

THE CONTRACTOR(S) SHALL BE SOLELY RESPONSIBLE TO GIVE 72 HOURS WRITTEN NOTICE TO THE UTILITIES PRIOR TO CROSSING SUCH UTILITIES. FOR THE PURPOSE OF INSPECTION BY THE CONCERNED UTILITY, THIS INSPECTION WILL BE FOR THE DURATION OF THE CONSTRUCTION, WITH THE CONTRACTOR RESPONSIBLE FOR ALL COSTS ARISING FROM SUCH INSPECTION

NOTES

ANY LANDSCAPING WITHIN THE MUNICIPAL BOULEVARD WILL BE SUBJECT TO PRIOR APPROVAL BY THE PUBLIC UTILITIES CO-ORDINATING COMMITTEE.

THE APPLICANT WILL BE RESPONSIBLE FOR THE COST OF ANY UTILITIES RELOCATIONS NECESSITATED BY THE SITE PLAN.

ON SITE WASTE COLLECTION WILL BE REQUIRED THROUGH A PRIVATE WASTE HAULER.

AT THE ENTRANCES TO THE SITE, THE MUNICIPAL CURB AND SIDEWALK WILL BE CONTINUOUS THROUGH THE DRIVEWAY AND CURB DEPRESSION WILL BE PROVIDED FOR EACH ENTRANCE.

ALL PROPOSED CURBING (IF ANY) AT THE ENTRANCES TO THE SITE IS TO STOP AT THE PROPERTY LINE OR AT THE MUNICIPAL SIDEWALK.

ANY EXCESS EXCAVATED MATERIAL IS TO BE REMOVED FROM THE SITE.

ALL DAMAGED AREAS ARE TO BE REINSTATE WITH TOPSOIL AND SOD PRIOR TO THE RELEASE OF SECURITIES.

ALL EXISTING TREES OVER 0.15 m TRUNK DIAMETER WILL NOT BE REMOVED UNLESS OTHERWISE SHOWN.

GRADING AT THE BASE OF EXISTING TREES MUST BE PRESERVED ADJACENT THE CONSTRUCTION ZONE.

ALL SURFACE DRAINAGE WILL BE SELF CONTAINED, COLLECTED AND DISCHARGED AT A LOCATION TO BE APPROVED PRIOR TO THE ISSUANCE OF A BUILDING PERMIT.

PROPOSED GRADES AND DRAINAGE PATTERNS SHALL NOT HAVE NEGATIVE IMPACT ON THE ADJACENT PROPERTIES.

- DIRECT DOWN SPOUTS TO THE FRONT OR REAR.

- DRAINAGE SWALE INVERTS SHALL BE 0.20 m BELOW THE ADJACENT GRADE ELEVATION.

WORKS IN THE MUNICIPAL RIGHT-OF-WAY BEING PERFORMED BY THE CITY'S CONTRACTOR WILL REQUIRE 4 TO 6 WEEKS NOTICE PRIOR TO COMMENCEMENT OF CONSTRUCTION AFTER ALL DRAWINGS HAVE BEEN APPROVED AND SECURITIES HAVE BEEN RECEIVED.

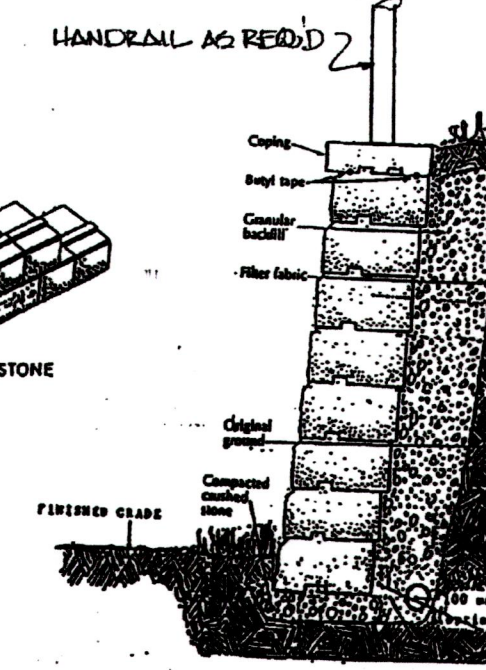
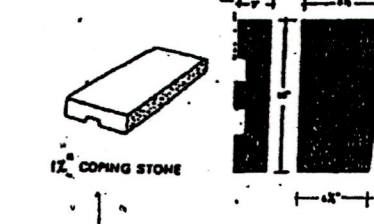
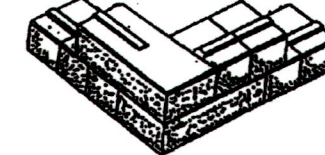
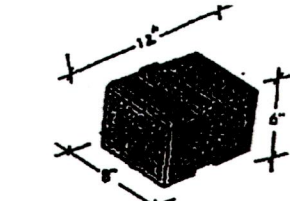
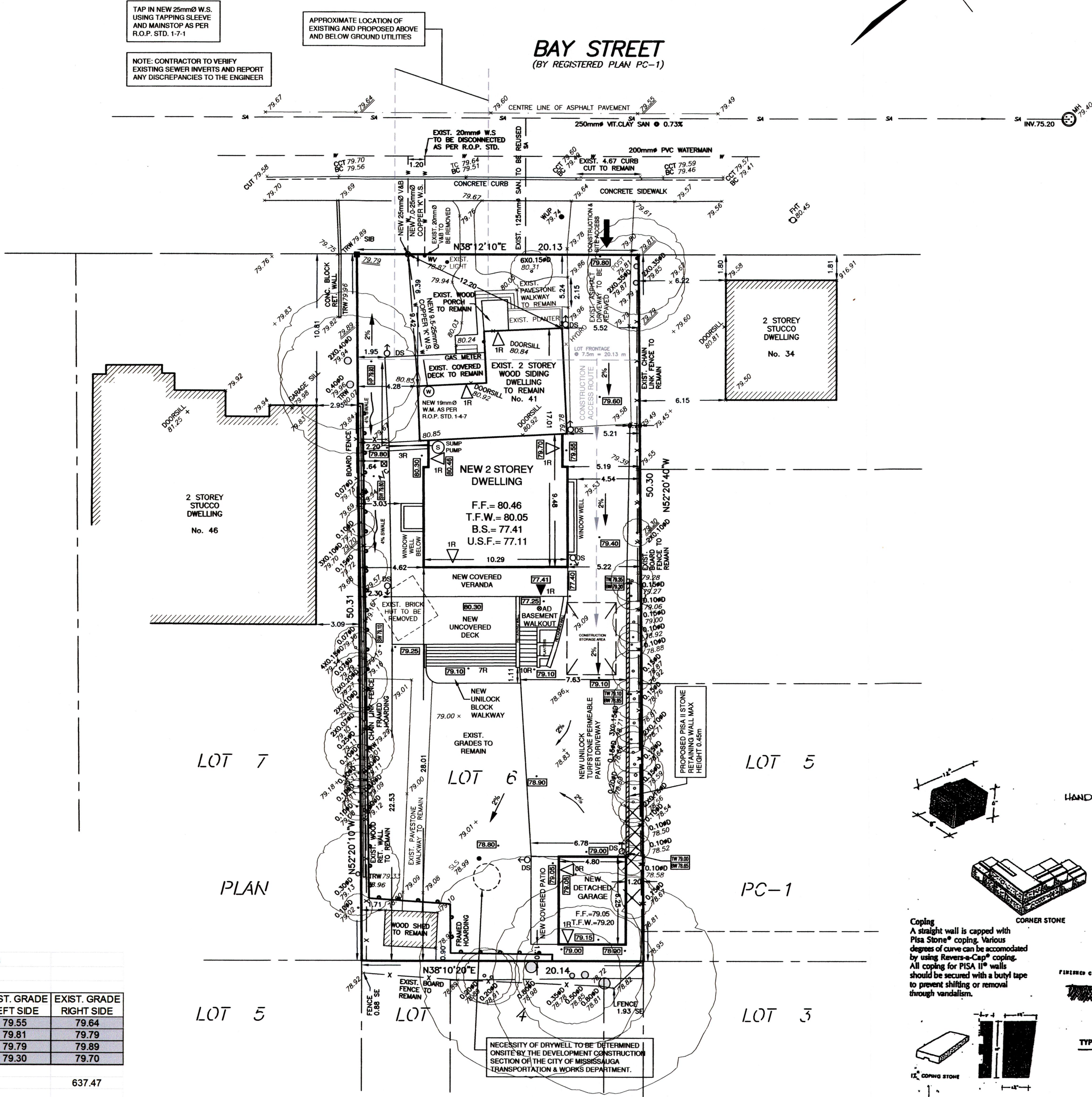
SEWERS

EXISTING SANITARY SEWER TO BE USED.

EXISTING STORM SEWER TO BE USED PROVIDED

A) APPROVAL FROM CITY OF MISSISSAUGA.

DEPTH OF SANITARY SEWER CONNECTION AT THE STREET LINE TO BE CONFIRMED IN FIELD PRIOR TO CONSTRUCTION TO ENSURE GRAVITY FLOW.



NOTES

1. WALL TO BE BUILT ONLY AGAINST UNDISTURBED, LAYER OR COMPACTED FILL.
2. MINIMUM SLOPE OF WALL TO BE A VERTICAL TO 1 HORIZONTAL.
3. MAXIMUM HEIGHT OF WALL TO BE 1.2 m ABOVE FINISHED GRADE.
4. REINFORCING WALL TO BE SHOWN BELOW GRADE TO BE 150 mm.
5. WALL TO BE FURNISHED WITH 100 mm COMPACTED COVERED STONE BASE.
6. REINFORCING REQUIRED SAFE SOIL ALLOWABLE GRADING PATTERN TO BE 144 kPa (3000 lb/ft²).
7. WALL TO BE BACKFILLED WITH FREE DRAINING GRANULAR MATERIAL (TYPICAL MIXTURE OF FINE SOIL PARTICLES).
8. REINFORCING CONNECTION TO BE 90° ST. PROTECT EXPOSED.
9. BACKFILLING TO BE PLACED IN LAYERS NOT EXCEEDING 150 mm (6") IN THICKNESS.
10. PROTECT DRAINAGE TO EXPOSURE AND INSTALLATION OF FILTER FABRIC AND REINFORCING TIE IS SUBJECT TO INSPECTION & TEST.
11. GRADING SHALL BE TO BE ACCORD TO WALL WITH LEVEL TAP.
12. SOIL CONDITIONS MUST BE VERIFIED BY A GEOTECHNICAL ENGINEER PRIOR TO CONSTRUCTION.

KEY PLAN
NOT TO SCALE

LEGAL DESCRIPTION:
LOT 6
SOUTH SIDE OF BAY STREET
WEST OF CREDIT RIVER
PLAN PC-1 (ALSO KNOWN ON PLAN 300)
CITY OF MISSISSAUGA
REGIONAL MUNICIPALITY OF PEEL

SURVEY INFORMATION
SURVEY INFORMATION IS TAKEN FROM PLAN PREPARED BY TARASICK McMILLAN KUBICKI LIMITED, ONTARIO LAND SURVEYORS, APRIL 14, 2016.

ELEVATION NOTE
ELEVATIONS ARE REFERRED TO CITY OF MISSISSAUGA DATUM AND WERE DERIVED FROM CITY OF MISSISSAUGA BENCHMARK No. 731, HAVING A PUBLISHED ELEVATION OF 81.500 metres.

- LEGEND**
- | | | |
|-----|---------|--|
| MH | DENOTES | MANHOLE |
| FH | DENOTES | FIRE HYDRANT |
| WUP | DENOTES | WOOD UTILITY POLE |
| WV | DENOTES | WATER VALVE |
| Δ | DENOTES | ENTRY POINT AT LOWER LEVEL |
| Δ | DENOTES | ENTRY POINT AT GROUND FLOOR |
| Δ | DENOTES | EXISTING SPOT ELEVATION |
| Δ | DENOTES | PROPOSED SPOT ELEVATION |
| Δ | DENOTES | PROPOSED RAINWATER DOWNSPOUT |
| Δ | DENOTES | PROPOSED SUMP PUMP LOCATION AND OUTLET |
| Δ | DENOTES | AREA DRAIN |
| Δ | DENOTES | CONIFEROUS TREE WITH TRUNK DIAMETER |
| Δ | DENOTES | DECIDUOUS TREE WITH TRUNK DIAMETER |
| Δ | DENOTES | TREE TO BE REMOVED |
| Δ | DENOTES | FRAMED HOARDING |
| Δ | DENOTES | SOLID WOOD HOARDING |
| Δ | DENOTES | DIRECTION AND GRADIENT OF DRAINAGE |
| Δ | DENOTES | DIRECTION OF SWALE |

DO NOT SCALE DRAWINGS

- 1) CONTRACTOR MUST CHECK AND VERIFY ALL DIMENSIONS, SPECIFICATIONS AND DRAWINGS ON SITE AND REPORT ANY DISCREPANCIES TO THE ENGINEER/ARCHITECT PRIOR TO PROCEEDING WITH ANY OF THE WORK.
- 2) THESE PLANS ARE TO REMAIN THE PROPERTY OF THE DESIGNER AND MUST BE RETURNED UPON REQUEST. THESE PLANS MUST NOT BE USED IN ANY OTHER LOCATION WITHOUT THE WRITTEN APPROVAL OF THE DESIGNER.
- 3) ALL WORKS TO BE DONE IN ACCORDANCE WITH THE ONTARIO BUILDING CODE.

1	JULY 6/16	REVISED AS PER CITY COMMENTS
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REVISIONS

CERTIFICATION FOR PROPOSED GRADING
I HAVE REVIEWED THE PLANS FOR THE CONSTRUCTION OF A BUILDING LOCATED AT 41 BAY STREET AND HAVE PREPARED THIS PLAN TO INDICATE THE COMPATIBILITY OF THE PROPOSAL TO EXISTING ADJACENT PROPERTIES AND MUNICIPAL SERVICES. IT IS MY BELIEF THAT ADHERENCE TO THE PROPOSED GRADES AS SHOWN WILL PRODUCE ADEQUATE DRAINAGE AND COMPATIBILITY OF MUNICIPAL SERVICES WITHOUT ANY DETRIMENTAL EFFECT TO THE EXISTING ADJACENT PATTERNS OR ADJACENT PROPERTIES.

DATE: JULY 25, 2016
MISSISSAUGA, ONTARIO
BORYS KUBICKI, O.L.S.

APPLICANT/ OWNER

FILE: SP1 16-073 W1

PROJECT:
PROPOSED 2 STOREY ADDITION AND DETACHED GARAGE
41 BAY STREET, MISSISSAUGA

SHEET TITLE:
SITE GRADING PLAN

SCALE: 1:200

TARASICK McMILLAN KUBICKI LIMITED
ONTARIO LAND SURVEYORS
4181 SLADEVIEW CRESCENT, UNIT 42, MISSISSAUGA, ONTARIO L5L 5R2
TEL: (905) 569-8849 FAX: (905) 569-3160
E-MAIL: office@tmsurveyors.com

DRAWN BY: A.R. **FILE No. 7246-SP-2016-07-06**

2.2.2 Buildings of Historic Interest

Objectives HCDP

HCDP Direction	Compliance
a. To strive in retaining buildings of historic interest in situ.	The original building remains unaltered and prominent on the site
b. To encourage the appropriate care of buildings of historic interest.	41 Bay St. has been and will be sensitively cared for
c. To make alterations and additions to any building of historic interest in keeping with the architectural character of the building.	The character of the addition draws from the details and character of the original
d. To base any intended restorations of buildings of historic interest on documentary and/or as-found evidence.	No significant alterations to the existing house are planned

HSDP Policies

Compliance

2.2.2.1 The plan's policies will also apply to properties formerly designated under Part IV of the Ontario Heritage Act, which are now included in the Old Port Credit Village Heritage Conservation District. The Part IV properties will be treated as buildings of historic interest.	The property has historic interest and will be protected
2.2.2.2 The City will make every effort to have buildings of historic interest remain in situ, and avoid their demolition or removal.	The existing heritage asset will remain in place
2.2.2.3 In cases where a building of historic interest is proposed for demolition, the applicant for a demolition permit may be requested to prepare a heritage impact statement prior to the issuing of the permit. In addition, the City may encourage archaeological assessment on the site while the building remains standing	Not applicable as no demolition is planned
2.2.2.4 Before a demolition permit is issued for a building of historic interest, the applicant will have first obtained a building permit for a new building that	Not applicable as no demolition is planned

meets the plan's design guidelines for new construction (Section 6.0) and zoning by-law standards.	
2.2.2.5 The removal (relocation) of a building of historic interest to another site will be a last resort. If removal is unavoidable, the building of historic interest will be moved to a site as close as possible to its original site or to the most appropriate site.	Not applicable
2.2.2.6 The City will enter into heritage conservation easements with willing property owners where desirable.	An easement is not required
2.2.2.7 The City will care for City-owned buildings of historic interest in the district as currently recognized standards for building conservation recommend. Private property owners will be urged to use the same degree of care.	Not applicable
2.2.2.8 When repairing, altering, adding to or restoring buildings of historic interest, property owners will have regard for:	The addition will have the same siding and trim finishes as the original house.
a. the building's historic materials and distinctive features;	
b. the building's history as documented in the district Building Inventory, fire insurance and other plans, historic photographs and other historical sources and as revealed on the building itself;	The image of the building from the public streets will remain significantly unaltered
c. the building's structural support and its physical condition; and,	Structures are unaltered
d. the plan's guiding principles for the conservation of buildings of historic interest (Section 4.0).	The guiding principles are followed
2.2.2.9 An addition to a building of historic interest will be lower in height and smaller in size than the building of historic interest wherever possible;	The addition comprises 2 components- a "hyphen" that is of less height and plan width, and a rear addition that is similar in size to the original house with a reduced 2nd floor height to effect a similar wall scale
and in designing additions, property owners will have regard for the plan's guiding principles (Section 4.0) and any impact the addition may have on adjacent properties in terms of scale, massing, height and setback.	The subject lot is large enough to accept the addition without any zoning amendment or impact on adjacent properties

2.2.2.10 The aim of any change to a building of historic interest will be to safeguard the character-defining elements of the building and not to falsify its appearance by making it look older or newer than it is.	The original building remains unaltered and authentic
2.2.2.11 Any institutional building of historic interest that cannot continue in its intended use and must be reused for other appropriate purposes will be adapted for the new use with the greatest degree of respect for the building's character.	Not applicable

4.15 Principle: Distinguish new work from old, but complement it.

Design a needed addition to your building that contributes to the appearance of the building in a way that is true to our own time.

<ul style="list-style-type: none"> Setting an addition as far back from the building of historic interest as possible on the property is a well-established method for giving prominence to the historic building. New garages are best designed as separate buildings sited behind, or towards the back of, the house. 	The addition sits fully behind the existing historic house and has limited visibility from the street. A new garage is located to the rear of the property as a separate detached structure
<ul style="list-style-type: none"> Limiting the size and scale of the addition in relation to the building of historic interest also gives prominence to the historic building. The addition should be lower in height and smaller in size than the historic building. The worst place to add onto a historic building is on its roof. 	No roof additions are planned and all new work occurs beyond the rear wall of the existing house
<ul style="list-style-type: none"> Ensuring that the size of the addition maintains ample open space around the house (front, side and rear yards) helps preserve the village's private open space character and protects neighbours' privacy. 	The total building footprint occupies only 14% of the subject lot with a landscaped area of 65% providing ample landscape space around the house. This allows existing trees and landscape to flourish
<ul style="list-style-type: none"> When ramps, elevators and other barrier-free access devices need to be added, design them with respect for 	Not applicable

the historic building's setting, scale, distinctive features and historic materials.	
<ul style="list-style-type: none">• The design features you find on your building of historic interest can inspire the design features you put on the addition. What kind of wall covering exists or used to exist on your building? A wood-sided building suggests a wood-sided addition.	The addition is clad in the same siding as the historic house. Also window design, porch columns and bracket details match the original details.

8.0 ADDENDUM

8.1 Resources:

Bradley, Ida Lynd
 The Early Families of Port Credit (unnumbered pages)
 Google Earth
 Google Streets
 Gowans, Alan
 An Architectural History of Canadian Life; pages 86-122
 Heritage Mississauga
 A Heritage Tour of Old Port Credit HCD; item 10
 Hicks, Kathleen
 Port Credit: Past to Present; pages 153-157
 mississauga.ca - Services Online - e-maps
 mississauga.ca - Services Online - Property Information
 Mississauga Heritage Advisory Committee
 minutes: September 22, 2009; pages 1-2
 minutes: May 25, 2010; page 2
 Region of Peel Land Registry Office
 Riverside Centennial Committee
 Riverside Public School – Celebrating 100 Years; pages 8-13
 Service Ontario at www.e-laws.gov.on.ca
 Ontario Heritage Act, RSO 1990, Chapter O.18
 Walker and Miles;
 Historical Atlas of Peel County, 1877; pages 24-25 and 52-53
 Weeks, Verna Mae
 A Glimpse of Other Days, pages 13-24, 64-72 and 122-132

Thank you to members of the Mississauga South Historical Society

8.2 Author:

Since 2007 Richard Collins has prepared Heritage Impact Statements for sites in Burlington, Gravenhurst, Mississauga, Oakville and Welland Ontario, including three pro bono publico works for community and ratepayers groups.

Clarkson 1808-2008 Committee; heritage coordinator
 Heritage Mississauga; volunteer, recipient of the 2007 Lifetime Membership Award and the 2008 Member's Choice Award
 Mississauga HAC; member of the Heritage Designation Subcommittee
 Mississauga South Historical Society; president
 Museums of Mississauga, historical interpreter
 Muskoka Steamship Society, restoration fundraiser for R.M.S. Segwun
 Page+Steele Architects, Toronto; past archivist
 Peel District School Board Heritage Fair, member and adjudicator
 Port Credit 175th Anniversary Committee; project leader and secretary
 Port Credit Village Project; secretary and co-chair of the Heritage Circle
 The Booster; author of over 200 articles on Mississauga's history

May 3, 2015 Addendum to the Heritage Impact Assessment for 41 Bay Street, Port Credit with updated June 7, 2016 Garage Plans

Outbuildings:

Current photos of the existing pool house are provided below. This brick building originally functioned as a pool house and storage. It was built in 1988 when the pool was installed or shortly after. The building is red brick with a black roof and does not share any of the same materials as the house. It was situated poolside but now that the pool has been removed the pool house is noticeably oddly placed on the property and in relation to the house.

The building is not in good shape, has been repeatedly and problematically inhabited by raccoons and is in need of a new roof, doors and windows. This building holds no significant heritage value. It is not only in disrepair, but is currently situated where the new extension would be located, requiring it to be removed.



Proposed Single Car Garage:

The following is a drawing of the proposed single car garage to be situated at the South West corner of the property, taking into account the required setbacks from the neighbouring properties as per Principle 4.15 of the Old Port Credit Heritage Conservation District Plan:

- “Setting an addition as far back from the building of historic interest as possible on the property is a well-established method for giving prominence to the historic building. New garages are best designed as separate buildings sited behind, or towards the back of, the house.”

The garage is clad in the same siding as the historic house. Also window design, porch columns and bracket details will match the original details.

It is comparatively small in scale to the house as per Objective 2.2.5a and is the preferred one-storey detached design as per Policy 2.2.5.3.

LICENSED PROFESSIONAL ENGINEER

2016-06-22

S.E. RAYMENT

100183890

PROVINCE OF ONTARIO

Blackwell

19 Duncan Street, Suite 405
Toronto, ON M5H 3H1
T 416-975-5300

31 King Street N, 2nd Floor
Waterloo, ON N2J 2W6
T 519-616-0895
blackwell.ca

This Seal applies only to the
design of structural elements.

NEW CONCRETE FLOATING SLAB:
SLOPE FLOOR TO GARAGE DOOR
6" SLAB ON GRADE WITH MESH REINFORCEMENT
ANCHOR BOLTS @ 48" O/C
32mpa CONCRETE
FOOTING TO BE 14" DEEP x 12" WIDE, MIN. 6" ABOVE GRADE
TAPERED ON 45 DEGREES TO 6" SLAB DEPTH
ANCHOR BOLTS @ 48" O/C
6" COMPACTED GRAVEL

TYPICAL EXTERIOR WALL:
SIDING AND TRIM DETAILS TO MATCH EXISTING
1x3 SPF STRAPPING (AIR SPACE)
1/2" SPF PLYWOOD SHEATHING, NAILED
2x6 SPF WOOD STUD FRAMING @ 16" O/C

NEW COLUMNS AND PIERS:
14" SONOTUBE TO MIN. 4" BELOW GRADE
PIERS TO BEAR ON UNDISTURBED SOIL
6x6 PT POST ON SADDLE

ROOF CONSTRUCTION:
CONTINUOUS RIDGE VENT
ASPHALT SHINGLES
ICE AND WATER SHIELD MEMBRANE
5/8" PLYWOOD SHEATHING, GLUED AND NAILED
ROOF FRAMING MEMBERS
SOFFIT AND FASCIA DETAILS TO MATCH EXISTING

Plan

North Elevation

West Elevation

41 Bay St.
Heritage Addition

A10 | GARAGE
June 7, 2016
Scale: 1/4" = 1'-0"

Exterior Wall Covering:

In accordance with Principle 4.15 of the Old Port Credit Heritage Conservation District Plan the addition is clad in white wood siding to complement the original house.

“4.15 Principle: Distinguish new work from old, but complement it.

- The design features you find on your building of historic interest can inspire the design features you put on the addition. What kind of wall covering exists or used to exist on your building? A wood-sided building suggests a wood-sided addition.”

The original building is distinguished from the new addition by use of a "hyphen" that is of less height and plan width, connecting the rear addition that is similar in size to the original house with a reduced second floor height to effect a similar wall scale. The image of the building from the public streets will remain significantly unaltered.

Interior Architectural Features:

A complete renovation of the interior of the house was done in the 1990's. The floors were replaced linoleum in the kitchen, ceramic tiles in the foyer and bathroom and prefinished maple wood boards throughout. Plaster walls and ceilings were removed and the house was insulated and re-dry walled. The west-facing side door into the kitchen was dry walled over on the inside to allow for more cupboards and counter space. No original architectural features remain inside.

3. Summary Statement

In summary there are no negative impacts on the cultural heritage resource as a result of the proposed addition to the McGregor Thompson House at 41 Bay Street. There is no destruction of any significant heritage attribute. The original building remains unaltered, authentic and prominent on the site. The proposed addition is sympathetic to the heritage attributes and carefully scaled to be diminutive in respect to the original house. The character of the addition draws from the details and character of the original building and will have the same siding and trim finishes as the original house. The addition sits fully behind the existing historic house and has limited visibility from the street. The addition is complementary to the surrounding neighbourhood in design and scale.

No roof additions are planned and all new work occurs beyond the rear wall of the existing house. The subject lot is large enough to accept the addition without any zoning amendment or impact on adjacent properties. The total building footprint occupies less than 15% of the subject lot with a landscaped area of 65% providing ample landscape space around the house and allowing existing trees and landscape to flourish. No shadow or incompatible massing is created by the addition that alters the appearance of or changes the viability of the heritage attribute. There is no land disturbance such as a grade change that alters soils and drainage patterns that adversely affect this cultural heritage resource.

The property is designated under the terms of Part V of the Ontario Heritage Act. The proposed addition and garage respect the four items of Regulation 9/06 for which the building and property complies. The proposed addition and garage do not alter architectural features that define the house as an early example of a middle-class Victorian-era residence nor do they alter the position of the home on the property and the massing of the home in any manner that would detract from the home's existing physical, functional, visual and historical link to the Old Port Credit Heritage Conservation District.

Given the above analysis, the proposed design is deemed to be compatible and desirable for the subject heritage asset while respecting and maintaining its heritage designation and the integrity of its attributes. The proposed design respects the objectives, policies and principles of the Old Port Credit Village Heritage Conservation District Plan. 41 Bay Street has been and will be sensitively cared for. The Heritage Advisory Committee can recommend the proposed design with confidence.

This document is an addendum to the March 4, 2015 Heritage Impact Statement for 41 Bay Street, Port Credit, now with updated current plans.

Since the approval of the HIS in June 2015, the following changes have been made to the plans in the process of pursuing the subsequent building permit:

- 1) Upon meeting with Planning and Building Feb 3, 2016, we revised our plans to comply with the allowable GFA for a detached garage and reduced the size of the covered porch, considered a gazebo.
- 2) The pitch of the roof has been altered for engineering and the rear of the house has been extended by 3ft, evenly distributed across the back. This change is undetectable to passersby.
- 3) In engineering the roof, it became apparent that the floor and roof heights as drawn would not accommodate the joists and duct work required to support and supply the house, and so the overall height of the addition was revised to comply with the maximum allowable height. The respectful heritage design cleverly ensures the rear extension is completely tucked behind the existing house, and that the view of the house from the street is virtually unchanged.
- 4) The complete reconstruction of the 2 front verandahs, replacement of aluminum siding with real wood siding and replacement of all aluminum windows with double hung wood windows on the West, North and East facing exterior walls of the existing house is now complete. Mississauga Heritage permits were obtained for all work done and has been inspected and approved as part of the Heritage Grant program. Therefore, the HIA now applies only to the rear extension and does not include any alterations to the North, West or East-facing sides of the existing house.
- 5) The original dormer on the East elevation of the existing house size will remain the same as existing in size and location. All other changes are minor, such as the addition or removal of a new window, and do not affect the dimensions of the house itself; they are listed in detail below.

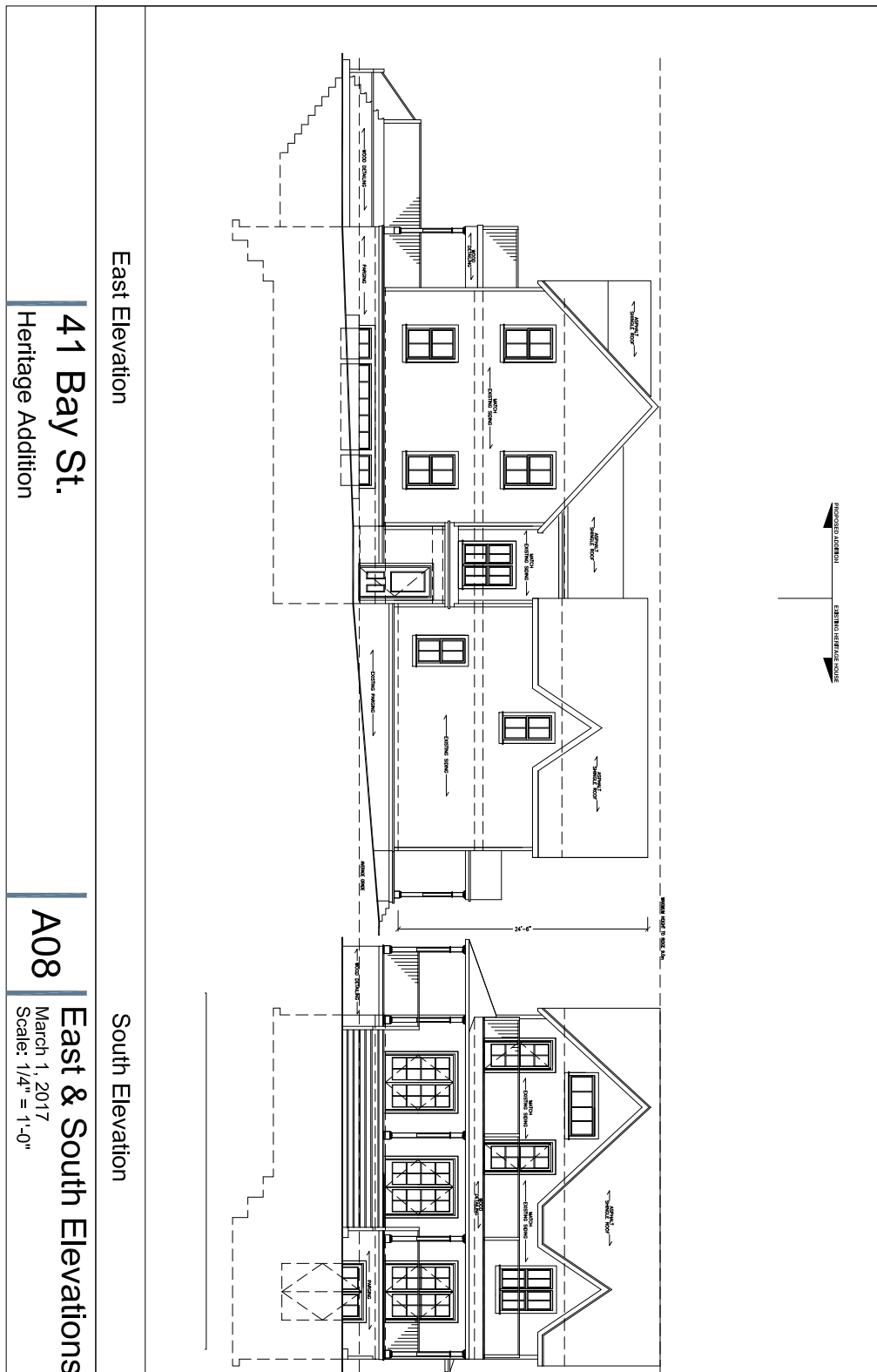
The following items have now been altered from the original drawings:

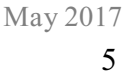
1. size of proposed garage has increased, complies with GFA; size of gazebo on garage has decreased
1. design of driveway, width and depth have been reduced
2. depth of rear deck has increased, still allows for more than required greenspace
3. depth of rear addition has increased slightly, by approx. 3ft.
4. added window in the kitchen on west elevation
5. basement window on original west elevation is actually not visible on side of porch
6. master bedroom windows are now smaller on the west elevation
7. overall height of the addition is to the max. allowable height to allow for engineering
8. keeping original dormer size and location on existing house east elevation
9. added a roof overhang at the east side entry
10. added basement windows on east elevation
11. lowered the window at the stair landing, in the 'hyphen' on the east elevation
12. removed basement window on south elevation
13. extended deck stairs across south elevation
14. mirrored the dormers and altered doors and windows on the second floor to reflect revised interior layout on the south elevation

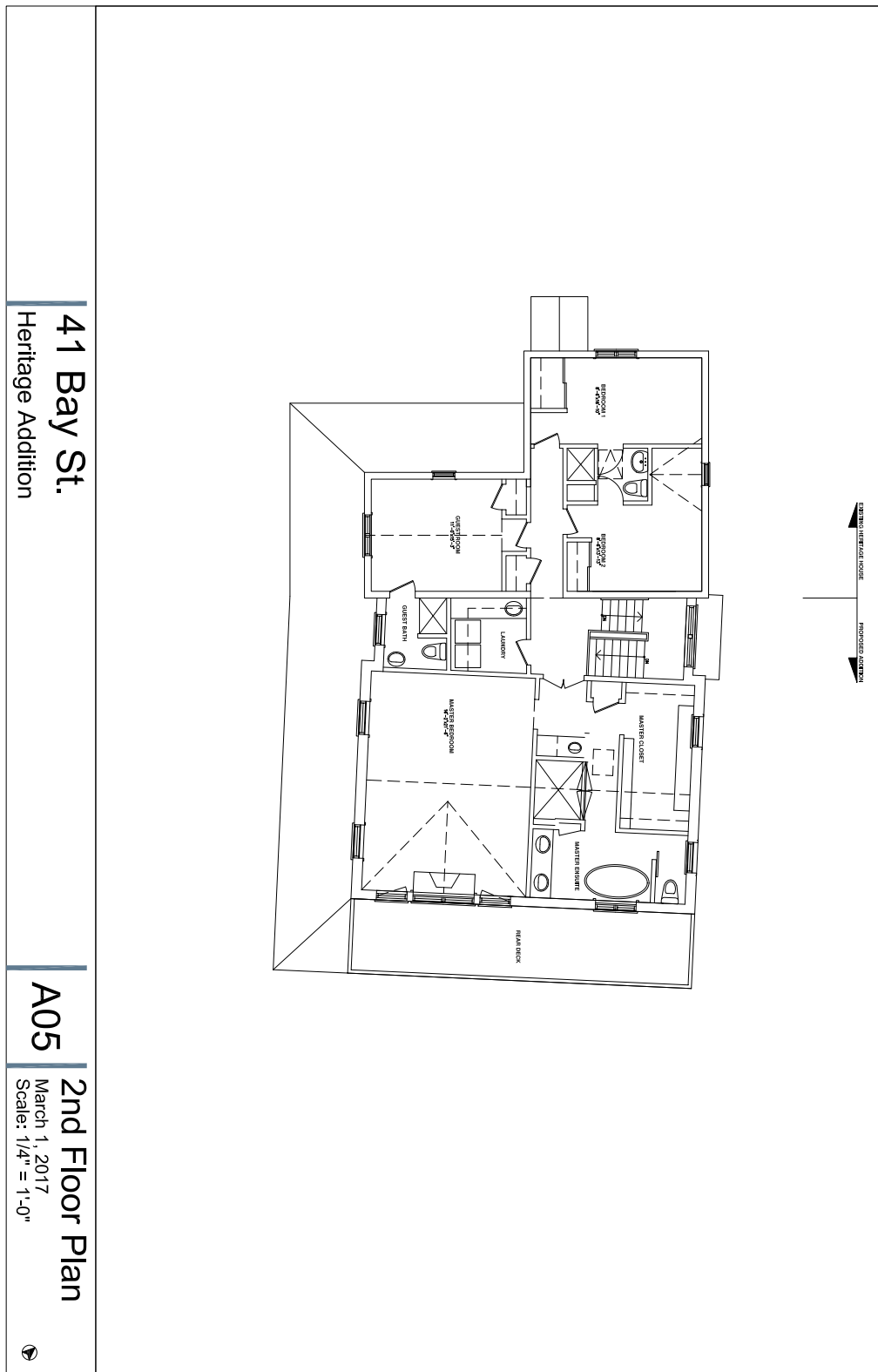
Therefore, the following pages have been updated from the original file to reflect these amendments to the plans:

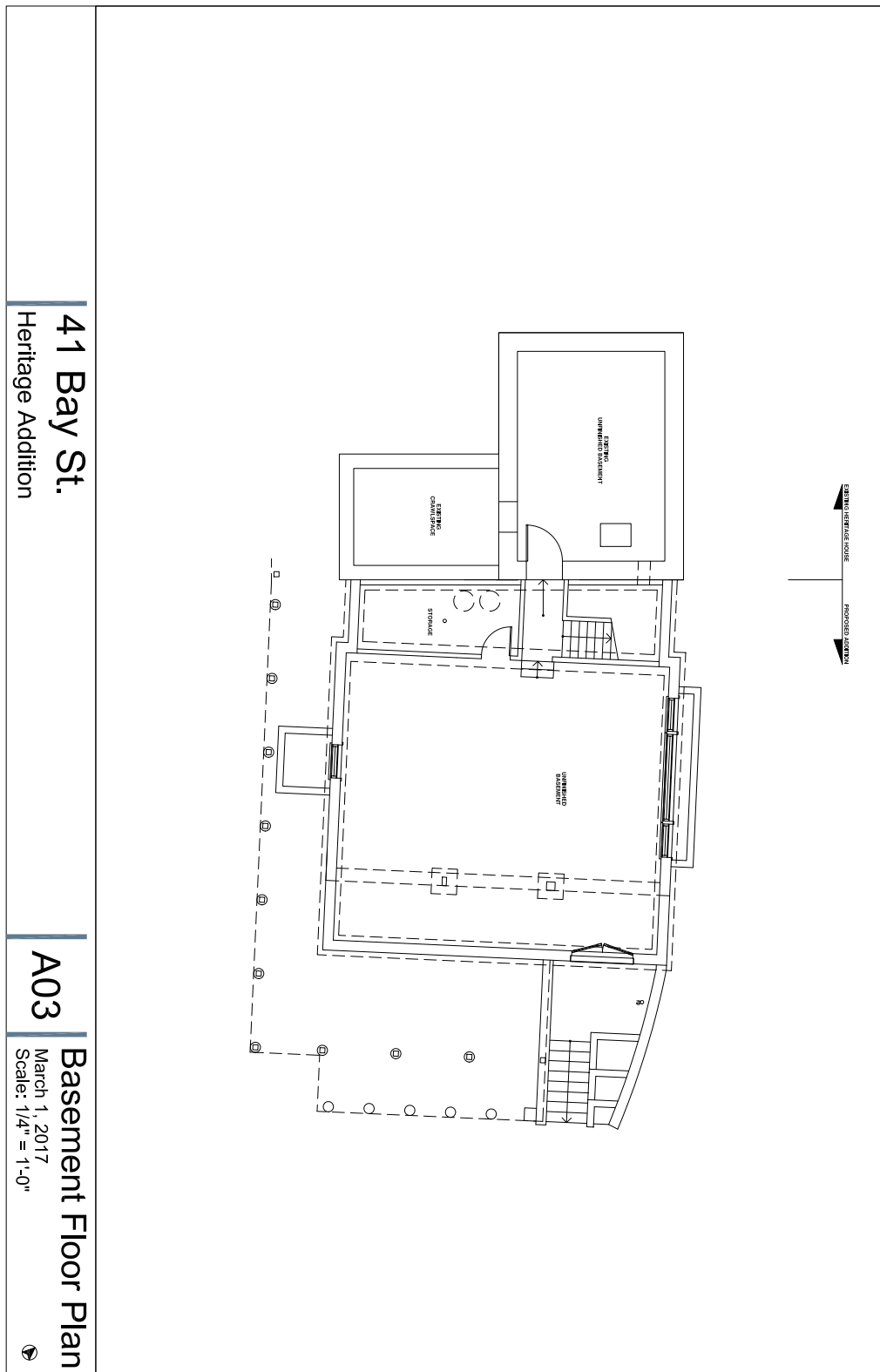
Page 28 – North & West Elevations
 Page 29 – East & South Elevations
 Page 30 – Ground Floor Plan
 Page 31 – Second Floor Plan
 Page 32 – Basement Floor Plan
 Page 33 – Garage Elevations & Floor Plan
 Page 34 – Site Plan

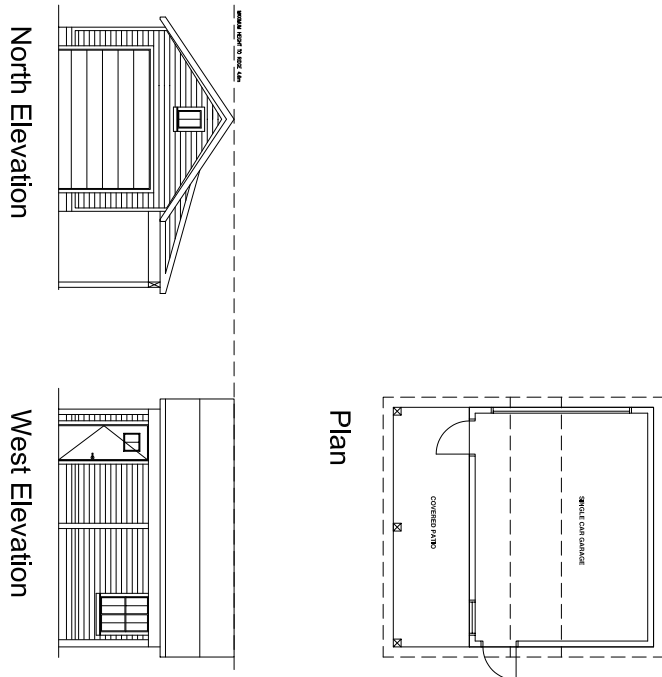










<div data-bbox="305 573 401 800"> <p>41 Bay St. Heritage Addition</p> </div> <div data-bbox="305 1297 401 1589"> <p>A10 Garage March 1, 2017 Scale: 1/4" = 1'-0"</p> </div>	<div data-bbox="790 1071 823 1140"> <p>Plan</p> </div> 
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Addendum to Heritage Impact Statement for 41 Bay Street, Port Credit



The existing pool house was built in 1988 when the pool was installed, or shortly after. Below are elevation photos of the current building, in disrepair:

North Elevation:



South Elevation:



East Elevation:



West Elevation:



City of Mississauga
Corporate Report



Date: December 7, 2017

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
 Community Services

Originator's files:

Meeting date:
 2018/01/09

Subject

Request to Alter a Heritage Designated Property: 1155 Willow Lane (Ward 11)

Recommendation

That the request to alter the property at 1155 Willow Lane as per the Corporate Report from the Commissioner of Community Services dated December 7, 2017, be approved.

Background

The subject property is designated under Part V of the *Ontario Heritage Act* as it forms part of the Meadowvale Village Heritage Conservation District (HCD). Changes to the property are subject to the Meadowvale Village HCD Plan, 2014, and substantive changes identified in said plan require a heritage permit. The proposal builds upon a 2015 heritage permit for the subject property as outlined in item 1 here:

http://www7.mississauga.ca/documents/agendas/committees/heritage/2015/HAC_Agenda_2015_09_15.pdf. The proposal has been revised, as per the drawings attached as Appendix 1. The revised scope of work includes the following:

- Rebuild front porch (columns and roof)
- Re-stucco dwelling to match existing stucco
- Replace existing windows with new mostly "one over one" wood windows
- Underpin/rebuild/reinforce existing stone foundation
- New floor joists and subfloor in living room area
- Framing and electrical/plumbing/mechanical repairs
- Some changes to window openings
- New garage and shed

Comments

The proposal does not negatively impact the property's heritage attributes. It is simple, employs traditional materials, and, with the foundation work in particular, will aid in the longevity of the dwelling. The new outbuildings are proposed where previous ones existed to the west of the house. I.e. they will not impact the open green space and view of the property from Willow Lane, one of the property's heritage attributes. The Meadowvale Heritage Conservation District Advisory Subcommittee concurs with the staff recommendation for approval.

Financial Impact

There is no financial impact.

Conclusion

The owner of the subject property has applied to modify the property as per the attached drawings. The proposal is sympathetic to the character of the property and should be approved.

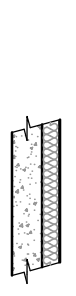
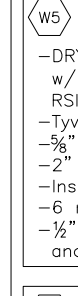

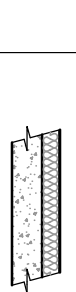
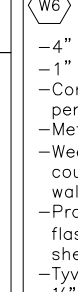

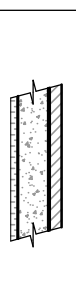
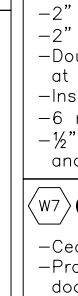
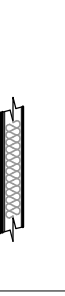
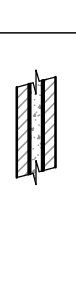
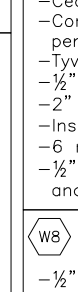

Attachments

Appendix 1: Drawings



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: P. Wubbenhorst, Senior Heritage Coordinator

<p>(W) (Basement Foundation Walls)</p> <ul style="list-style-type: none">-Footing as indicated-Drainage course-Bituminous dampproofing-$\frac{1}{2}$" paring (block only)-Concrete foundation wall and reinforcing as indicated-Wood sill & anchor bolts as indicated-15 lb. building paper-2" x 6" @ 16" o.c. framing-Continuous insulation per approved package-6 mil. poly V.B./air barrier-$\frac{1}{2}$" drywall filled, taped, sanded and ready for point.		<p>(W5) (Stucco Walls)</p> <ul style="list-style-type: none">-DR/RTU Outslution Plus (NC) w/ Cont. Rigid insulation (min. RSI 1.76c)-Tyvek housewrap-$\frac{1}{2}$" exterior gypsum sheathing-2" x 6" @ 16" o.c. framing-Insulation per approved package-6 mil. poly V.B./air barrier-$\frac{1}{2}$" drywall filled, taped, sanded and ready for point.		<p>(W9) (Attic Wall)</p> <ul style="list-style-type: none">-Tyvek housewrap-$\frac{1}{2}$" Plywood sheathing-2" x 6" @ 16" o.c. framing-Insulation per approved package-6 mil. poly V.B./air barrier-$\frac{1}{2}$" drywall filled, taped, sanded and ready for point.	
<p>(W2) (Crawspace Foundation Walls)</p> <ul style="list-style-type: none">-Drainage course-Bituminous dampproofing-$\frac{1}{2}$" paring (block only)-Concrete foundation wall as indicated-15 lb. building paper-2" x 6" @ 16" o.c. framing-Insulation per approved package-6 mil. poly V.B./air barrier-Tyvek housewrap-$\frac{1}{2}$" plywood sheathing-2" x 6" @ 16" o.c. framing-2" x 6" blocking at mid-height-Double plates at top, sill plate at bottom-Insulation per approved package-6 mil. poly V.B./air barrier-$\frac{1}{2}$" drywall filled, taped, sanded and ready for point.		<p>(W6) (Brick/Stone Exterior Walls)</p> <ul style="list-style-type: none">-4" brick/stone facing-Continuous Rigid insulation as per package-Metal ties as per O.B.C.-Weep holes @ 24" o.c. at corners & top of foundation wall and above all openings.-Provide through wall base flashing up min. 12" behind sheathing paper.-Tyvek housewrap-$\frac{1}{2}$" plywood sheathing-2" x 6" @ 16" o.c. framing-2" x 6" blocking at mid-height-Double plates at top, sill plate at bottom-Insulation per approved package-6 mil. poly V.B./air barrier-$\frac{1}{2}$" drywall filled, taped, sanded and ready for point.		<p>(W3) (Masonry Retaining Walls)</p> <ul style="list-style-type: none">-2" high density polystyrene-Bituminous dampproofing-$\frac{1}{2}$" paring (if block)-Concrete foundation wall as indicated-Reinforcement as indicated-1" airspace, masonry ties, weep holes-4" brick or stone facing as indicated-Copper flashing at base of masonry veneer-Continuous Rigid insulation as per package-Tyvek housewrap-$\frac{1}{2}$" plywood sheathing-2" x 6" @ 16" o.c. framing-2" x 6" blocking at mid-height-Double plates at top, sill plate at bottom-Insulation per approved package-6 mil. poly V.B./air barrier-$\frac{1}{2}$" drywall filled, taped, sanded and ready for point.	
<p>(W4) (Masonry Garden Wall)</p> <ul style="list-style-type: none">-4" brick or stone facing as indicated-1" airspace, masonry ties, weep holes-Concrete foundation wall as indicated-Reinforcement as indicated-1" airspace, masonry ties, weep holes-1/2" paring if block-4" brick or stone facing as indicated-Copper flashing at base of masonry veneer (both sides)-Stone or pre-cast cap		<p>(W7) (Wood Siding Exterior Walls)</p> <ul style="list-style-type: none">-Cedar shingles or Siding-Provide flashings at all windows, doors and projections-Cedar breather or 1/4" Strapping-Continuous Rigid insulation as per package-Tyvek housewrap-1/2" plywood sheathing-2" x 6" @ 16" o.c. framing-Insulation per approved package-6 mil. poly V.B./air barrier-1/2" drywall filled, taped, sanded and ready for point.		<p>(W10) (Interior Walls)</p> <ul style="list-style-type: none">-1/2" drywall filled, taped, sanded and ready for point.-2" x 6" @ 16" o.c. framing unless otherwise noted-1/2" drywall filled, taped, sanded and ready for point.	
<p>(W11) (Soundproof Walls)</p> <ul style="list-style-type: none">-1/2" drywall filled, taped, sanded and ready for point.-2" x 6" @ 16" o.c. framing unless otherwise noted-1/2" drywall filled, taped, sanded and ready for point.		<p>(W12) (Sauna Walls)</p> <ul style="list-style-type: none">-1" x 4" T&G V-groove cedar installed vertically-1" x 3" @ 16" o.c. horizontal strapping-Double layer polyethylene type vapour barrier, tape all joints-1/2" drywall filled, taped, sanded and ready for point.		<p>(W13) (Soundproof Ceilings)</p> <ul style="list-style-type: none">-1/2" drywall filled, taped, sanded and ready for point.-2" x 6" @ 16" o.c. framing unless otherwise noted-1/2" drywall filled, taped, sanded and ready for point.	

NOTES:

NOTE:
Refer to HVAC drawings and specs for other requirements related to floor heating system

<p>(F1) (Basement Floor)</p> <ul style="list-style-type: none"> -4" concrete slab -6 mil. poly V.B. -2" rigid insulation -8" granular fill "A" -Undisturbed or compacted soil Note: Seal surface w/concrete sealer. Provide floor drains as per plans. Slope to drains as required. 	<p>(F4) (Tile Finish)</p> <ul style="list-style-type: none"> -Ceramic or Stone tile on Schluter-DITRA mat installed as per manufacturers recommendations -F4 Floor Construction 	<p>(F7) (Exterior Decking)</p> <ul style="list-style-type: none"> -2" x 4" cedar boards with 1" spacing -Deck joists size as indicated -Void space -2" layer of granular fill on grade 	<p>(F8) (Entrance Porches/Terrace/Screened Porch)</p> <ul style="list-style-type: none"> -Flagstone & Mortarbed -6" 32 mpa (or as indicated) concrete slab 5-8% air entrainment -6" x 6" x 6/8 wire mesh -6" granular fill "A" -Undisturbed soil or compacted material 	<p>(F9) (Porch Conc. Floor with room below)</p> <ul style="list-style-type: none"> -Flagstone & Mortarbed -Waterproof Membrane -Slope to scupper -R.C. Slab as Noted -Insulation per approved package -2" x 6 framing -6 mil Poly V.B./air barrier -1/2" Drywall
<p>(F2) (Crawl Space Floor)</p> <ul style="list-style-type: none"> -2" concrete slab -6 mil. poly V.B. -5" granular fill "A" -Undisturbed or compacted soil 	<p>(F5) (Floor Above Sauna)</p> <ul style="list-style-type: none"> -$\frac{1}{2}$" T&G spruce plywood subfloor nailed and glued -Floor joists size as indicated -R31 batt insulation -Double layer polyethylene type vapour barrier, tape all joints -1" x 3" @ 16" o.c. strapping -2" x 4" T&G V-groove cedar 	<p>(F6) (Insulated Floor/Ceiling)</p> <ul style="list-style-type: none"> -$\frac{1}{2}$" T&G spruce plywood subfloor nailed and glued -6 mil. poly V.B./Air barrier -floor joists size as indicated -Insulation per approved package -$\frac{1}{2}$" drywall filled, taped, sanded and ready for point if interior. Soffit per elevations if exterior. 	<p>(F3) (Garage Floor)</p> <ul style="list-style-type: none"> -4" concrete slab 5-8% air entrainment -Slope slab to drain min. 2% -Saw cuts as indicated -6" x 6" x 6/8 wire mesh -5" granular fill "A" on V.B. -Undisturbed soil or compacted material Note: Seal surface w/ concrete sealer. 	<p>(F4) (First & Second Floor)</p> <ul style="list-style-type: none"> -Floor Finish -$\frac{1}{2}$" T&G spruce plywood subfloor nailed and glued -Floor joists size as indicated -$\frac{1}{2}$" drywall ceiling filled, taped, sanded and ready for point.

NOTE:
Unobstructed vent area shall be not less than 1/200 of the insulated ceiling area. When the roof slope is less than 1 in 6 the unobstructed vent area shall be not less than 1/150 of the insulated ceiling area.

NOTE:
At cathedral ceiling situations, provide 2" cross ventilation above insulation or fill joist cavity solid with 2 pound spray foam insulation.

<p>(R1) (Main Roof)</p> <ul style="list-style-type: none"> -Asphalt shingles unless noted otherwise -Ice & watershed -Eaves protection in accord with O.B.C. -$\frac{1}{2}$" spruce plywood sheathing -Rattlers or trusses/ceiling joists size as indicated -Ventilated attic space as per O.B.C. -Insulation per approved package -6 mil. poly V.B./Air barrier -$\frac{1}{2}$" drywall filled, taped, sanded and ready for point. 	<p>(R4) (Flat Roof)</p> <ul style="list-style-type: none"> -Single ply mineral impregnated membrane plywood sheathing -Roof/Ceiling joists size as indicated -Ventilated attic space as per O.B.C. or closed cell spray foam applied to underside of sheathing -Insulation per approved package -6 mil. poly V.B./Air barrier -$\frac{1}{2}$" drywall filled, taped, sanded and ready for point. 	<p>(R7) (Insulated Green Roof)</p> <ul style="list-style-type: none"> -Soil (Depth as Noted) -$\frac{1}{2}$" Protection Board -Membrane Roofing -$\frac{1}{2}$" Plywood Sheathing -1" X Sleepers @ Sloped Roof (Sloped Sleepers @ Flat Roof) -Roof/Ceiling Joists as Noted -Insulation per approved package -$\frac{1}{2}$" drywall filled, taped, sanded and ready for point. 	<p>(R8) (Green Roof over Porch)</p> <ul style="list-style-type: none"> -Soil (Depth as Noted) -$\frac{1}{2}$" Protection Board -Membrane Roofing -$\frac{1}{2}$" Plywood Sheathing -1" X Sleepers @ Sloped Roof (Sloped Sleepers @ Flat Roof) -Roof/Ceiling Joists as Noted -$\frac{1}{2}$" Plywood Sub-Ceiling -1x4 V Groove Cedar Boards
<p>(R2) (Porch Roof)</p> <ul style="list-style-type: none"> -Asphalt shingles -Ice & watershed -Eaves protection in accordance with O.B.C. -$\frac{1}{2}$" plywood sheathing -Rattlers as indicated -Ventilated attic space as per O.B.C. -Ceiling joists size as indicated -1" x 4" V-groove cedar boards 	<p>(R5) (Balcony Roof over Heated Space)</p> <ul style="list-style-type: none"> -1x6 IPE Decking -Reverse Wood strapping min 1 1/2" -Membrane Roofing -$\frac{1}{2}$" Plywood Sheathing sloped to Drain or Scuppers -Sloped Wood Strapping -Roof/Ceiling Joists size as indicated -Ventilated space as per O.B.C. or closed cell spray foam applied to underside of sheathing -Insulation per approved package -6 mil Poly V.B./Air barrier -$\frac{1}{2}$" drywall filled, taped, sanded and ready for point. 	<p>(R6) (Balcony Roof over Porches)</p> <ul style="list-style-type: none"> -1x6 IPE Decking -Reverse wood strapping min 1 1/2" -Membrane Roofing -$\frac{1}{2}$" Plywood Sheathing sloped to Drain or Scuppers -Sloped Wood Strapping -Roof/Ceiling Joists size as indicated -1x4 V Groove Cedar Boards 	<p>(R3) (Metal Roof)</p> <ul style="list-style-type: none"> -Standing Seam metal roofing -Layer of dry felt -Ice & watershed -Eaves protection in accordance with O.B.C. -$\frac{1}{2}$" spruce plywood sheathing -Rattlers or trusses/ceiling joists size as indicated -Ventilated attic space as per O.B.C. -Insulation per approved package -6 mil. poly V.B./Air barrier -$\frac{1}{2}$" drywall filled, taped, sanded and ready for point.

ARCHITECTURAL NOTES

GENERAL

- THESE DRAWINGS ARE THE PROPERTY OF THE DESIGNER AND ENGINEER AND MAY NOT BE COPIED WITHOUT THEIR APPROVAL AND ARE SUBJECT TO RETURN UPON REQUEST.
- ALL DIMENSIONS SHALL NOT BE SEALED.
- ALL CONSTRUCTION TO COMPLY WITH WITH ONTARIO BUILDING CODE LATEST EDITION INCLUDING:
 - SUBSECTION 9.3.3.4 NAILING OF FRAMING
 - SENTENCES 9.10.9.18 (415) OPERATION OF STORAGE GARAGES
 - C) SUBSECTION 9.10.19 SMOKE ALARMS
 - D) SENTENCE 9.17.4.2 (2) MATERIAL (BUILT-UP COLUMNS)
 - E) SUBSECTION 9.19.1 VENTING
 - F) SENTENCE 9.19.2.1 (2) ATTIC ACCESS
 - G) ARTICLE 9.20.13.12 DRIPS BENEATH WINDOW SILLS
 - H) TABLE 9.23.3.5 FASTENERS FOR EXTERIOR AND SUBFLOORING
 - I) SENTENCES 9.23.3.1 (7) OR (8) BUILT-UP WOOD BEAMS
 - J) SUBSECTION 9.23.14 SUBFLOORING
 - K) SUBSECTION 9.23.15 ROOF SHEATHING
 - L) CLAUSE 9.26.2.1 (1)(U) MATERIAL STANDARDS, ROOFING MATERIALS SHALL CONFORM TO, (I) CSA A123.1/A123.5, ASPHALT SHINGLES MADE FROM ORGANIC FELD AND SURFACED WITH MINERAL GRANULES/ASPHALT SHINGLES MADE FROM GLASS FELT AND SURFACED WITH MINERAL GRANULES
 - M) ARTICLE 9.26.2.2 NAILS AND 9.26.2.3 STAPLES
 - N) ARTICLE 9.26.4.3 VALLEY FLASHING
 - O) ARTICLE 9.26.4.5 INTERSECTION OF SHINGLED ROOFS AND WALLS OTHER THAN MASONRY
 - P) SUBSECTION 9.26.7 ASPHALT SHINGLES ON SLOPES OF 1 IN 3 OR GREATER
 - Q) SUBSECTION 9.29.5 GYPSUM BOARD FINISHED (TAPED JOINTS)
 - R) SUBSECTION 9.33.4 CARBON MONOXIDE DETECTOR
- ALL DIMENSIONS AND EXISTING CONDITIONS SHALL BE VERIFIED BY THE GENERAL CONTRACTOR AT THE SITE PRIOR TO CONSTRUCTION. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE TO NOTIFY THE DESIGNER OF ANY DISCREPANCIES BETWEEN THE SITE CONDITIONS AND THE ASSUMED DESIGN CONDITIONS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. IN ADDITION, THE GENERAL CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE CONSTRUCTION METHOD OR ERECTION OF HIDDEN SERVICES BEFORE PROCEDURES OF THE STRUCTURAL MEMBERS INCLUDING THE ERECTION OF STEEL BEAMS. THE GENERAL CONTRACTOR SHALL VERIFY LOCATIONS OF ANY UNDERGROUND OR HIDDEN SERVICES BEFORE COMMENCING DEMOLITION OR CONSTRUCTION. EXCAVATION WORK IS TO USE METHODS WHICH PREVENT MOVEMENT OR DAMAGE TO ADJACENT STRUCTURES, PROPERTIES, ROADS AND SIDEWALKS. THE GENERAL CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND CARE DURING THE DEMOLITION PROCESS OF EXISTING STRUCTURE AND SUPPORTING WALLS AND BE SOLELY RESPONSIBLE TO SUPPORT THE EXISTING STRUCTURE DURING THE CONSTRUCTION. THE GENERAL CONTRACTOR SHALL CALL THE STRUCTURAL ENGINEER FOR AN INSPECTION PRIOR TO CUTTING EXISTING MEMBERS AND REMOVING WALLS. CUTS FOR NEW OPENINGS IN LOAD BEARING MASONRY TO BE DONE BY SAW CUTTING.

- ALL DIMENSIONS ON PLAN WILL BE TO BEAM CENTERS AND WALL EDGES U.N.O.
- PROVIDE WIND BRACING AS PER O.B.C.
- ALL GUARDS TO COMPLY WITH O.B.C. 9.8.8 AND SG-7.
- VAPOUR BARRIERS ARE TO BE OVERLAPPED MIN. 4" AND BE SEALED WITH ACOUSTIC SEALANT.
- ALL EXTERIOR DOORS TO BE INSULATED C/W WEATHER STRIPPING AND MINIMUM THRESHOLD U.N.O.
- ALL BEAMS TO BE LATERALLY SUPPORTED U.N.O.
- ALL BOLT HOLES SHALL BE $\frac{1}{2}$ " OVERSIZED AND BOLT THREADS SHALL NOT BE ON THE WOOD MEMBERS.
- MAKE ALL POSTS CONTINUOUS THROUGH FLOOR AND ROOF FRAMING BY INSTALLING VERTICAL BLOCKING FOR THE FULL AREA OF THE POST.
- SHOP DETAILS OF ALL FABRICATION ITEMS SHALL BE SUBMITTED FOR APPROVAL PRIOR TO FABRICATION.

MASONRY

- MORTAR SHALL CONFORM TO O.B.C. 9.20.3.
- C.M.U.'S SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OVER NET AREA IN ACCORDANCE WITH TABLE 9.20.2.7, AS PER PART 9 OF THE O.B.C.
- BRICK AND BLOCK SHALL BE TIED AND BONDED BY HEAVY DUTY "BLACKLOCK" @ 16" O.C. VERTICALLY
- MINIMUM BEARING OF STRUCTURAL MEMBERS ON MASONRY SHALL BE AS FOLLOWS:
 - CONCRETE AND STEEL BEAMS = 8"
 - CONCRETE SLABS = 4"
 - C/WALL = 4"
 - WOOD BEAMS AND JOISTS = 3 1/2"

STRUCTURAL STEEL

- ALL STRUCTURAL STEEL SHALL BE CSA G40.21 GRADE 350W. FABRICATION, CONNECTION DESIGN AND WELDING SHALL CONFORM TO ANS 5.16.1-M88 AND WSM-1-M89. PROVIDE SOLID BEARING TO BEAMS OR FOUNDATION AT POINT LOADS OR BEARING WALLS.
- STRUCTURAL STEEL FABRICATOR TO PROVIDE SHOP DETAIL DRAWINGS AND PROVIDE BY THE FABRICATOR METHOD OF ERECTION OF STEEL BEAMS. THE GENERAL CONTRACTOR SHALL VERIFY LOCATIONS OF ANY UNDERGROUND OR HIDDEN SERVICES BEFORE COMMENCING DEMOLITION OR CONSTRUCTION. EXCAVATION WORK IS TO USE METHODS WHICH PREVENT MOVEMENT OR DAMAGE TO ADJACENT STRUCTURES, PROPERTIES, ROADS AND SIDEWALKS. THE GENERAL CONTRACTOR SHALL EXERCISE EXTREME CAUTION AND CARE DURING THE DEMOLITION PROCESS OF EXISTING STRUCTURE AND SUPPORTING WALLS AND BE SOLELY RESPONSIBLE TO SUPPORT THE EXISTING STRUCTURE DURING THE CONSTRUCTION. THE GENERAL CONTRACTOR SHALL CALL THE STRUCTURAL ENGINEER FOR AN INSPECTION PRIOR TO CUTTING EXISTING MEMBERS AND REMOVING WALLS. CUTS FOR NEW OPENINGS IN LOAD BEARING MASONRY TO BE DONE BY SAW CUTTING.

FOOTINGS AND FOUNDATIONS

<p>(1) (Stepped Footings)</p> <p>Stepped Footing in Compliance with O.B.C. 9.15.1.9</p>	<p>(8) (Soffit)</p> <p>Pre-finished or paint grade plywood with continuous vent behind fascia. Prime and paint, colour as per designer. Provide insect screen at vent.</p>	<p>(16) (Masonry wood burning fireplace)</p> <p>Refer to typical masonry fireplace detail</p>
<p>(2) (Crawspace Hatch)</p> <p>Serving more than 1 dwelling unit 19x12 1/2" Insulated crawspace access hatch as per O.B.C. 9.18.2.1</p>	<p>(9) (Pre-finished metal or copper flashing)</p> <p>Provide stepped through wall flashing at sloping interface(s) and capping as required. Colour as per designer.</p>	<p>(17) (Chimneys & Flues)</p> <p>Chimneys and flues shall be min. 3"-0" above any point of the roof within a 10'-0" radius of the chimney or flue.</p>
<p>(3) (Attic Hatch)</p> <p>Serving a single dwelling unit 19x12 1/2" Insulated attic hatch as per O.B.C. 9.19.2.1</p>	<p>(10) (Eavestrough)</p> <p>Pre-finished copper/metal eavestrough on finished fascia board.</p>	<p>(18) (Zero clearance gas fireplace)</p> <p>Gas outlet and type "B" vent to roof or direct vent to outside wall as per manufacturers specifications. Diameter of flue and vent as per O.B.C. Section 9.21.2.5</p>
<p>(4) (Guard Rails)</p> <p>3'-6" High unless lower height allowed by O.B.C. (4" max. space between pickets. Guards to resist loads as per O.B.C. Sec. 4.1.10.1)</p>	<p>(11) (Rainwater Leader)</p> <p>Pre-finished copper/metal rainwater leader (RWL) = to match eavestrough.</p>	<p>(19) (Sightlight)</p> <p>Curb mounted double glazed sightlight. Provide inside curb dimensions as indicated on drawings. Co-ordinate with manufacturer. Install as per manufacturer's instructions.</p>
<p>(5) (Stairs)</p> <p>Refer to Table 9.8.4.1 in the O.B.C. for Rise and Run requirements. Min. Stair Width = 3'-0" Min. Clear Height = 6'-9" Max. Height of Stairs = 12'-2" Max. No.ing = 1" Railing at stair = 3'-0" above tread</p>	<p>(12) (Washroom Vent)</p> <p>Washrooms to be mechanically vented to exterior. Provide required air changes per hour per O.B.C.</p>	<p>(20) (Exterior Stairs)</p> <p>Number of risers to grade is to be coordinated with grading plan and site conditions. In case of discrepancy, grading plan will govern. Report any discrepancies to the architectural designer.</p>
<p>(6) (Decorative Wood Trim)</p> <p>Wood trim as per elevations and detail drawing. Prime and paint with 3 coats Benjamin Moore exterior alkyl. All work to becco-ordinated with on site dimensions and proportioned accordingly.</p>	<p>(13) (Dryer/Cooktop)</p> <p>Dryer/Cooktop to be vented directly outside through wall.</p>	<p>(21) (CO Detector)</p> <p>Multi-station alarm style carbon monoxide detector permanently mounted on ceiling and connected to the building electrical supply without a disconnect wall switch and having a circuit not interconnected to any wall outlet. ULC or equal tested & approved, with indicator for operating condition.</p>
<p>(7) (Cut limestone or precast trim)</p> <p>Dimensions as per detail drawings. Provide shop drawings for designer's review. Provide copper/metal flashing at all horizontal planes.</p>	<p>(14) (Combustion Detector)</p> <p>Single station alarm style product of combustion detector permanently mounted on ceiling connected to any wall outlet. ULC or equal tested & approved, with indicator for operating condition.</p>	<p>(22) (Thermoplastic LED Exit Sign)</p> <p>Thermoplastic LED Exit Sign C/W LONG LIFE LED LAMPS, END, CEILING OR WALL MOUNT AND UNIVERSAL SNAP-IN CHEVRON ARROWS "PRESOLITE" NV3RNEW</p>

TYPICAL FIRE SEPARATION RATING NOTES:

EXIT AND EMERGENCY LIGHTING NOTES:

- THE FOLLOWING NOTES SHALL APPLY TO THE CONSTRUCTION GOVERNED BY ALL DRAWINGS ATTACHED HEREIN. ALL REFERENCES ARE TO THE APPLICABLE REQUIREMENTS OF THE ONTARIO BUILDING CODE (OBC).
- INSTALLATION OF ALL PRODUCTS SHALL BE IN STRICT ACCORDANCE WITH MANUFACTURER'S WRITTEN INSTRUCTIONS AND SPECIFICATIONS. WHERE IN CONFLICT WITH GOVERNING CODES AND/OR REGULATIONS, THE CODES/REGULATIONS SHALL APPLY.
- EXIT LIGHTS SHALL BE PROVIDED AS PER OBC ARTICLE 3.4.5.11.
- MINIMUM LIGHTING SHALL BE PROVIDED IN CONFORMANCE WITH OBC 3.2.7.1.
- EMERGENCY LIGHTING SHALL BE PROVIDED IN EXITS AND PRINCIPLE ROUTES PROVIDING ACCESS TO AN EXIT IN OPEN FLOOR AREAS AS PER OBC 3.2.7.3.
- APPLY FOR ALL PERMITS, SERVICES AND PAY ALL FEES TO THE AUTHORITIES HAVING JURISDICTION.
- CONTRACTORS SHALL BE RESPONSIBLE FOR EXAMINING THE SITE TO DETERMINE THE EXTENT OF EXISTING CONDITIONS PRIOR TO PROCEEDING WITH ANY QUOTATIONS.
- ELECTRICAL WIRING AND CABLES EXPOSED WITHIN THE CEILING SPACE MUST CONFORM TO THE PLENUM REQUIREMENTS OF OBC 3.6.4.3.(1).
- BATTERY OPERATED EMERGENCY LIGHTING UNITS MUST HAVE SUFFICIENT BATTERY CAPACITY TO OPERATE THE EMERGENCY LIGHTS FOR AT LEAST 30 MINUTES AND CONFORM TO CSA 22.2 NO. 141-02.
- COMMERCIAL EMERGENCY LIGHT C/W 2x4.4W INCANDESCENT LAMP HEADS, MAINTENANCE FREE BATTERY AND THERMALLY COMPENSATED BATTERY CHARGER. LAMP HEADS MAY BE TOP OR SIDE MOUNTED. "PRESOLITE" NV3CON-12V FIRE ALARM PULL STATION
- REMOTE EMERGENCY HEAD "PRESOLITE" NVH1212

Thermoplastic LED Exit Sign C/W LONG LIFE LED LAMPS, END, CEILING OR WALL MOUNT AND UNIVERSAL SNAP-IN CHEVRON ARROWS "PRESOLITE" NV3RNEW

REMOTE EMERGENCY HEAD "PRESOLITE" NVH1212

Thermoplastic LED Exit Sign C/W LONG LIFE LED LAMPS, END, CEILING OR WALL MOUNT AND UNIVERSAL SNAP-IN CHEVRON ARROWS "PRESOLITE" NV3RNEW

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<p>(W11) (Interior 3/4 Hr. FRR Walls)</p> <p>(Construction conforming to O.B.C. SB-3 Table 1 Assembly W1e -12.7mm Type X drywall -2" x 4" @ 16" o.c. framing unless otherwise noted -12.7mm Type X drywall</p>	<p>(W12) (Interior 1 Hr. FRR Walls)</p> <p>(Construction conforming to O.B.C. SB-3 Table 1 Assembly W1d) -15.9mm Type X drywall -2" x 4" @ 16" o.c. framing unless otherwise noted -15.9mm Type X drywall</p>
<p>(F11) (1 Hr. FRR Floor)</p> <p>(Construction conforming to ULC M500/UL Design L501) -Floor Finish -Existing sheathing to remain -Existing joists to remain -5/8" Type X drywall ceiling</p>	<p>(F12) (1 Hr. FRR Ceiling)</p> <p>(Construction conforming to ULC M500/UL Design L501) -Floor Finish -Existing sheathing to remain -Existing joists to remain -5/8" Type X drywall ceiling</p>

- SA SMOKE ALARM**
- CA CARBON MONOXIDE ALARM**
- NOTE: INSTALL SMOKE ALARMS AS PER O.B.C. 9.10.1.9 & 9.36.2.4.(1)
- NOTE: INSTALL CO ALARMS AS PER O.B.C. 9.33.4

Thermoplastic LED Exit Sign C/W LONG LIFE LED LAMPS, END, CEILING OR WALL MOUNT AND UNIVERSAL SNAP-IN CHEVRON ARROWS "PRESOLITE" NV3RNEW

REMOTE EMERGENCY HEAD "PRESOLITE" NVH1212

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REMOTE EMERGENCY HEAD "PRESOLITE" NVH1212

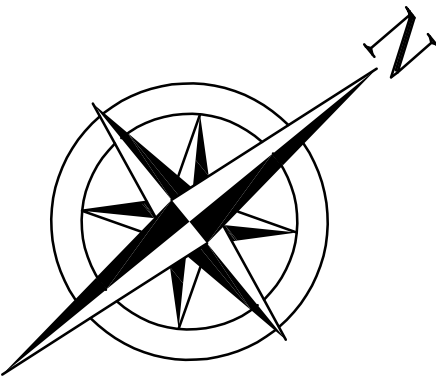
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REMOTE EMERGENCY HEAD "PRESOLITE" NVH1212

THIS SITE PLAN BASED UPON:

TOPOGRAPHIC SURVEY OF
LOTS 83 AND 84
REGISTERED PLAN TORONTO-5
CITY OF MISSISSAUGA
REGIONAL MUNICIPALITY OF PEEL

SITE STATISTICS				
DESCRIPTION	NEW BY-LAW	EXISTING	PROPOSED	TOTAL
ZONED	PBI-5			
LOT AREA	-	1499.38 m ²	-	1499.38 m ²
LOT FRONTAGE	-	56.33 m	-	56.33 m
LOT DEPTH	-	35.71 m	-	35.71 m
COVERAGE				
- Dwelling	-	63.45 m ²	(-17.33) m ²	46.12 m ²
- Covered Porches	-	-	32.30 m ²	32.30 m ²
- Detached Garage	-	53.54 m ²	-	53.54 m ²
- Detached Shed	-	27.77 m ²	-	27.77 m ²
TOTAL:	-	144.76 m ²	14.97 m ²	159.73 m ²
LOT COVERAGE %	-	9.65%	1.0%	10.65%
GROSS FLOOR AREA:				
- Main Floor	-	63.45 m ²	(-17.33) m ²	46.12 m ²
- Second Floor	-	32.32 m ²	-	32.32 m ²
- Third Floor	-	29.40 m ²	-	29.40 m ²
TOTAL:	-	125.17 m ²	(-17.33) m ²	107.84 m ²
G.F.A. %	-	8.59%	(-1.16%)	7.43%
BUILDING HEIGHT:				
- House	-	+/- 9.41 m	-	+/- 9.41 m
HOUSE SETBACKS:				
- Front	18.0 m	2.90 m	-	2.90 m
- Side	7.50 m	18.49 m	-	18.49 m
- Flankage	7.50 m	11.07 m	-	11.07 m
- Rear	15.00 m	19.89 m	-	19.89 m



SCALE 1 : 250

PART 1
PLAN 43R-17252
PIN 14085-2910

LOT 82
PIN 14085-0251
REG'D PLAN TORONTO 5

PART 4
PLAN 43R-17252
PIN 14085-2910

PART 18
PLAN 43R-17252
PIN 14085-2910

LOT 85
PIN 14085-0251

LEGEND

F.F. Finished Floor
UP. Utility Pole
DEC. Deciduous
CON. Coniferous
Ø Diameter
OWH Overhead Wires
BOB Bottom of Bank
Elevations

REFERENCE BEARING

Bearings are Astronomic and are Referred to the Southeasterly Limit of Part 14 being N 39° 15' 40" E as shown on Plan 43R-17252.

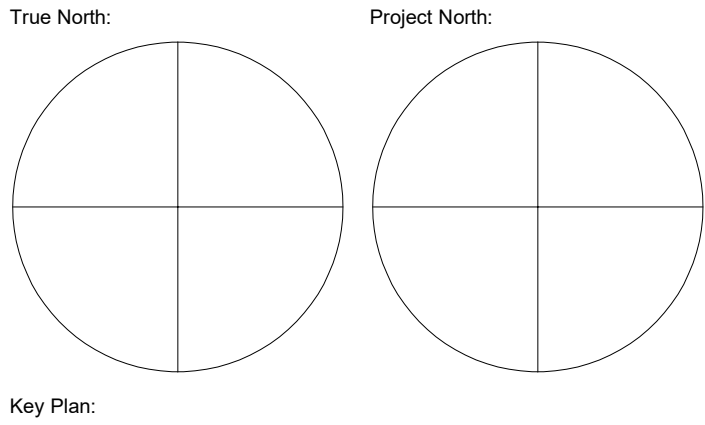
METRIC

Distance shown on this plan are in metric and can be converted to feet by dividing by 0.3048.

BENCHMARK

Elevations are Referred to the City of Mississauga Benchmark No. 241 having an Elevation of 174.317 m.

Topographic Survey completed on the 16th day of March, 2010.



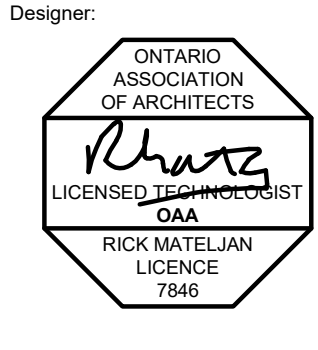
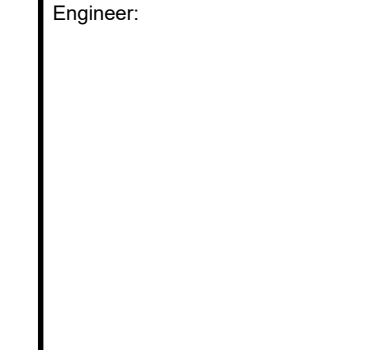
Key Plan:

3	Nov 03/17	Per Heritage Comments	K.A.
2	Oct 30/17	For Heritage Permit	K.A.
1	Oct 06/17	First Issue	K.A.

No.	Date	Issued/Revision	By:
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Designer:  Engineer: 

Project:

1155 Willow Lane
Mississauga, ON

Sheet Title:
Site Plan

Design By:	Drawn By:	Approved By:
R.M.	K.A.	R.M.
Scale:	Date:	Project No:
N.T.S.	Aug/18	17.43

Drawing No:

A001.1

road widening

DERRY ROAD WEST

[illegible]

1 BASEMENT FLOOR PLAN
A2.0 Scale: 1/4"=1'-0"

Key Plan

No.	Date	Issued/Revision	By:
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Ontario Association of Architects
Rick Mateljan
Licensed Technologist
OAA
Rick Mateljan
Licence
7846

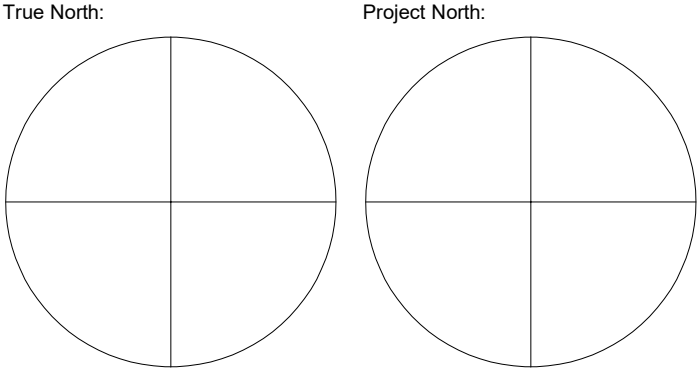
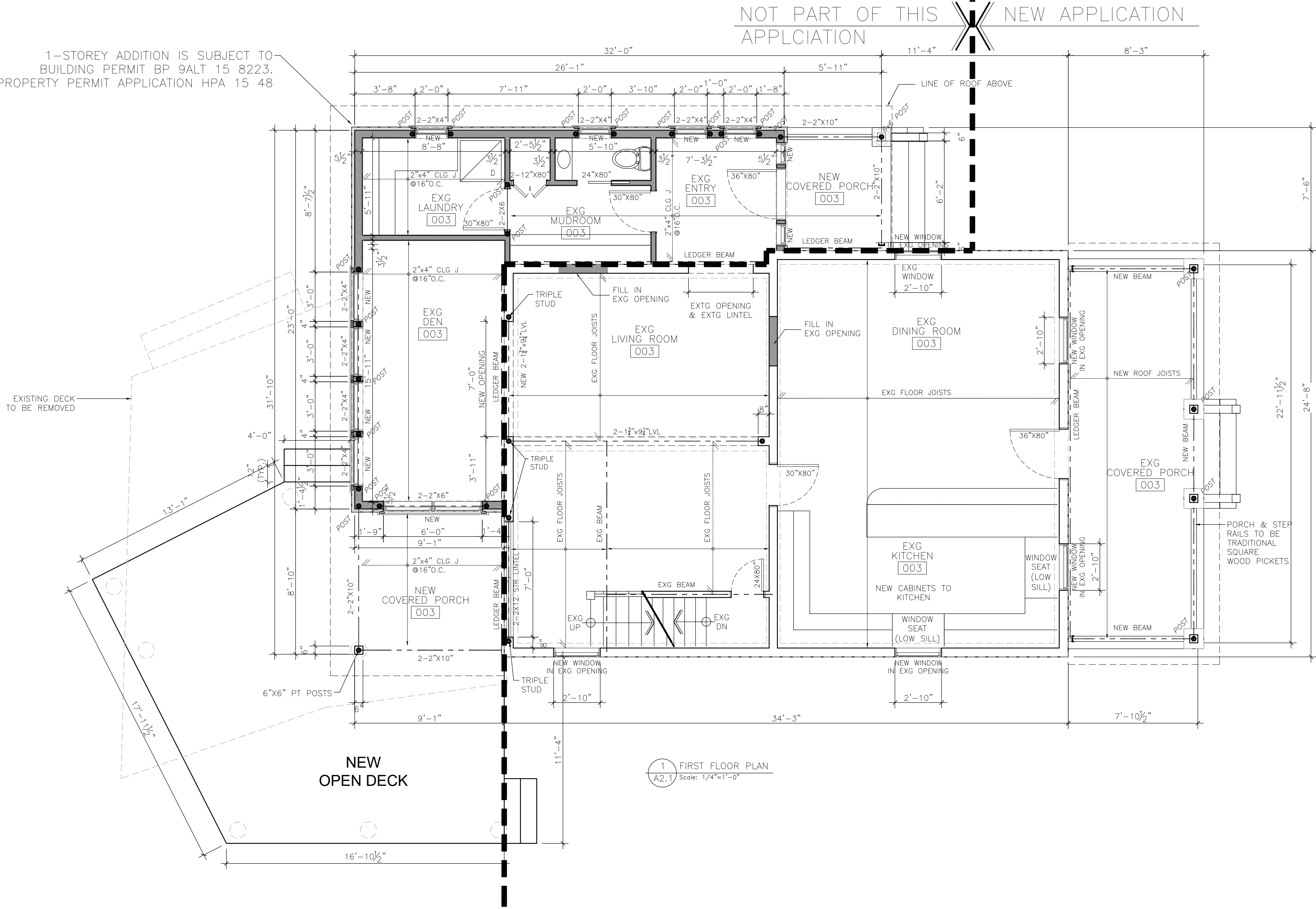
1155 Willow Lane
Mississauga, ON

Scale: 1/4" = 1' - 0"	Date: Aug/2017	Project No: 17.43
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Drawing No:

A002.0

1-STORY ADDITION IS SUBJECT TO
BUILDING PERMIT BP 9ALT 15 8223.
HERITAGE PROPERTY PERMIT APPLICATION HPA 15 48



Key Plan:

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Designer:  RICK MATELIAN LICENCE 7846	Engineer:
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Project:

1155 Willow Lane
Mississauga, ON

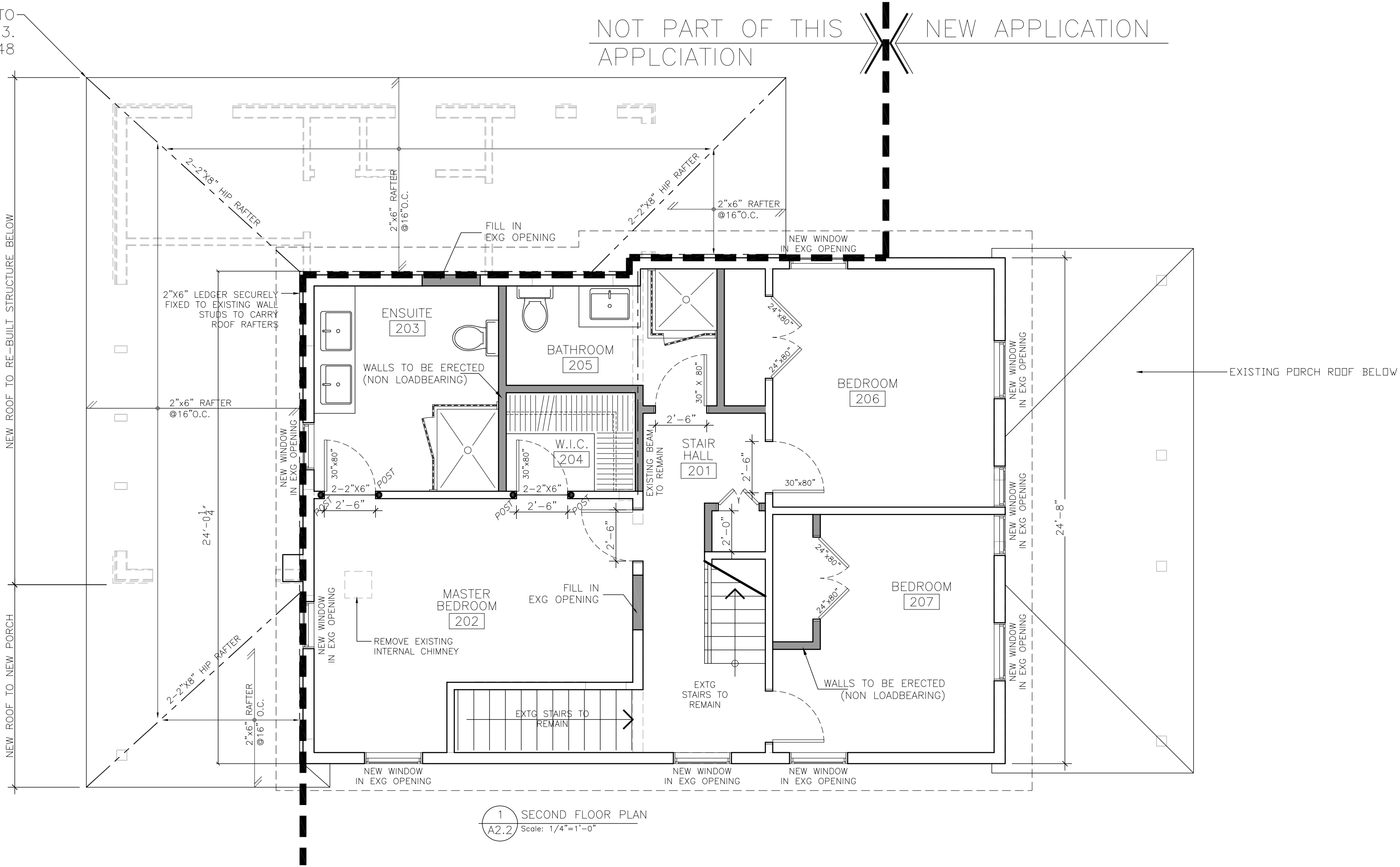
Sheet Title:
First Floor Plan

Design By: R.M.	Drawn By: R.M.	Approved By: R.M.
Scale: 1/4" = 1' - 0"	Date: Aug/2017	Project No: 17.43

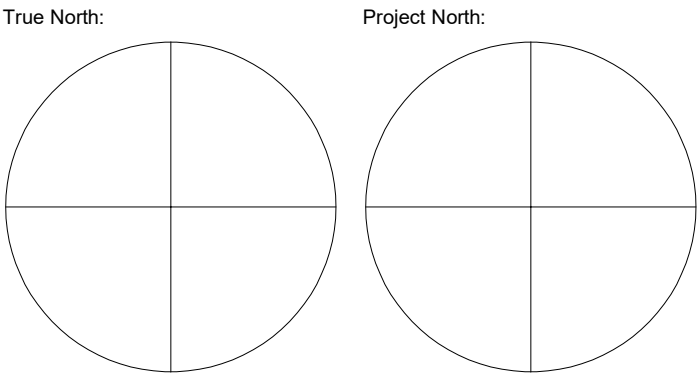
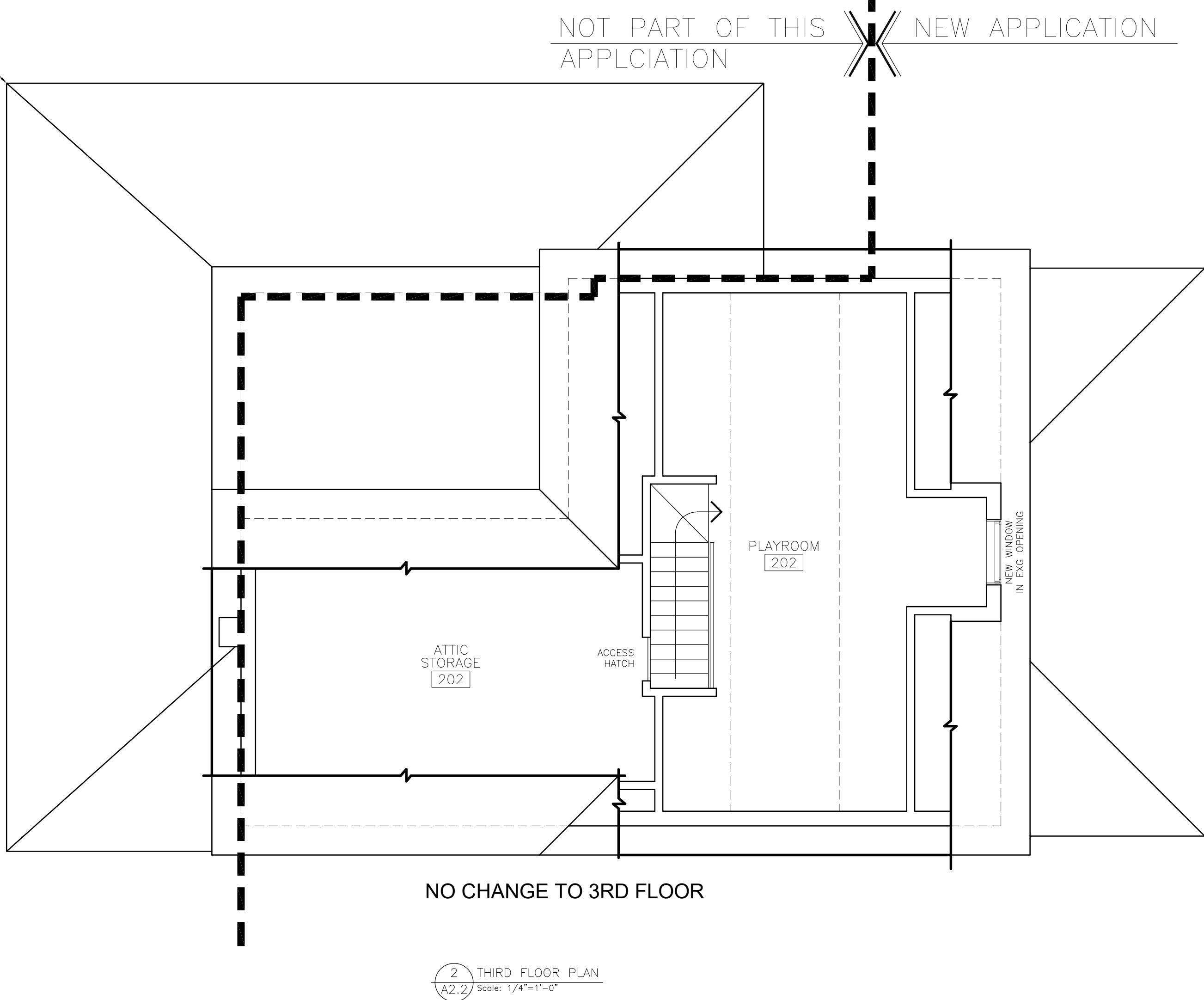
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BUILDING PERMIT BP 9ALT 15 8223.
HERITAGE PROPERTY PERMIT APPLICATION HPA 15 48



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Designer: RICK MATELIAN LICENCE 7846	Engineer:
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Project:

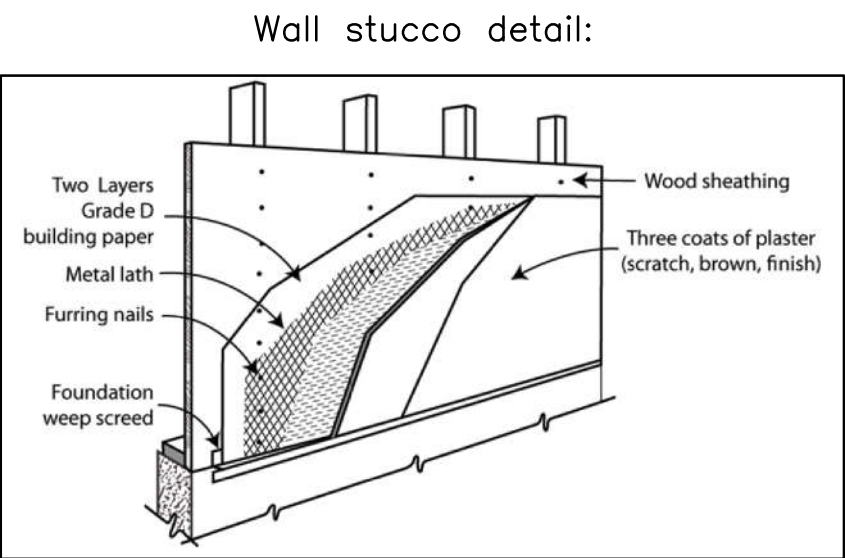
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Mississauga, ON

Sheet Title:
**Second Floor Plan
Third Floor Plan**

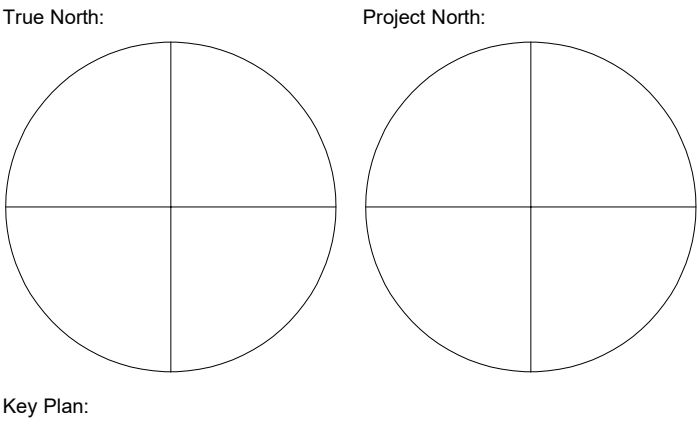
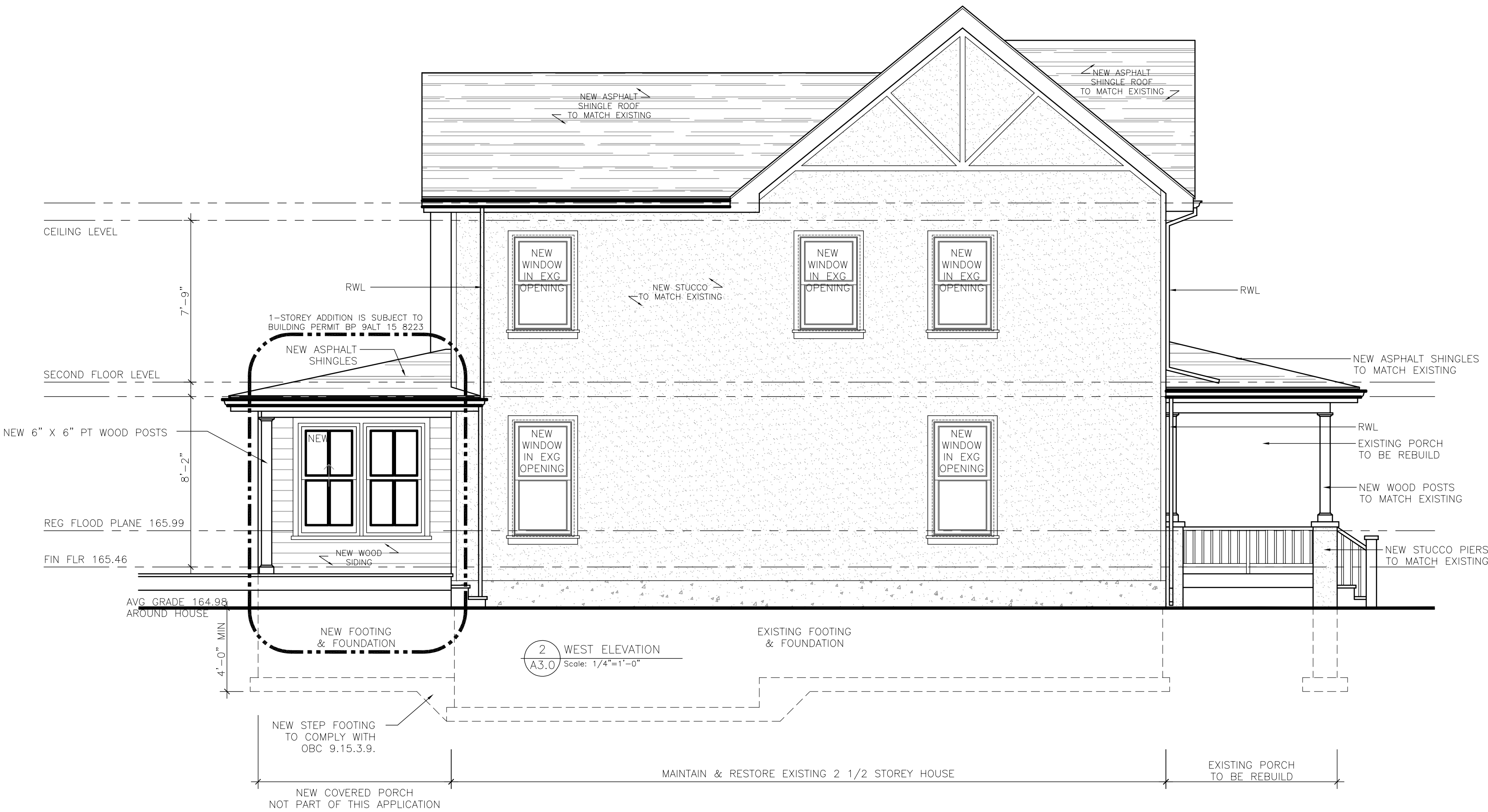
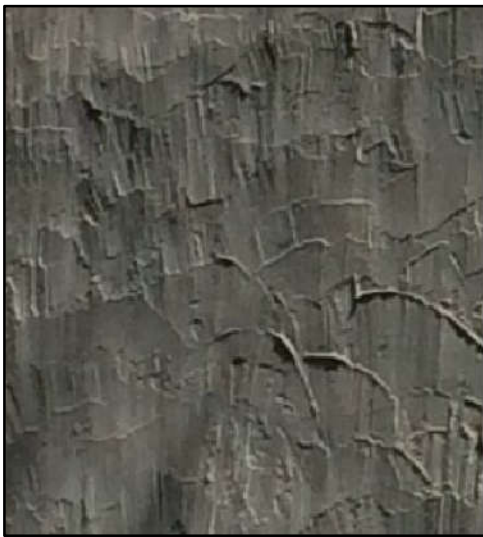
Design By: R.M.	Drawn By: R.M.	Approved By: R.M.
Scale: 1/4" = 1' - 0"	Date: Aug/2017	Project No: 17.43

Drawing No:

A002.2



Stucco to match existing:



Key Plan:

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No.	Date	Issued/Revision	By:
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Designer:  RICK MATELIAN LICENCE 7846	Engineer:
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Project:

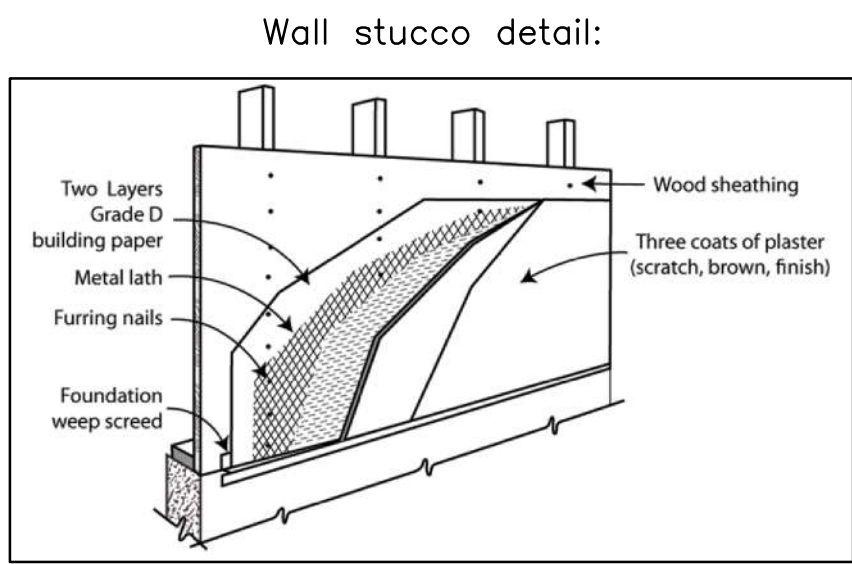
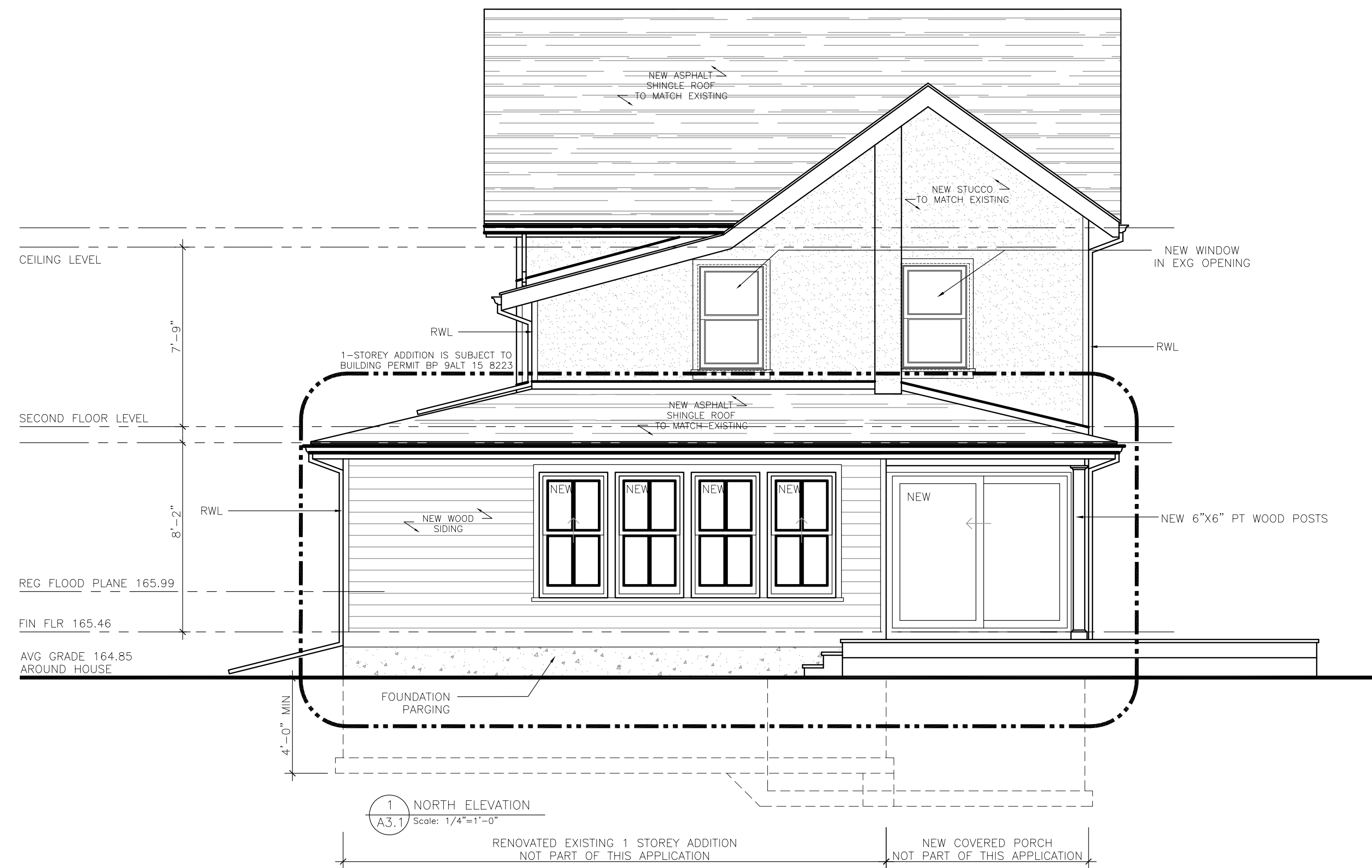
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Mississauga, ON

Sheet Title:
**South Elevation
West Elevation**

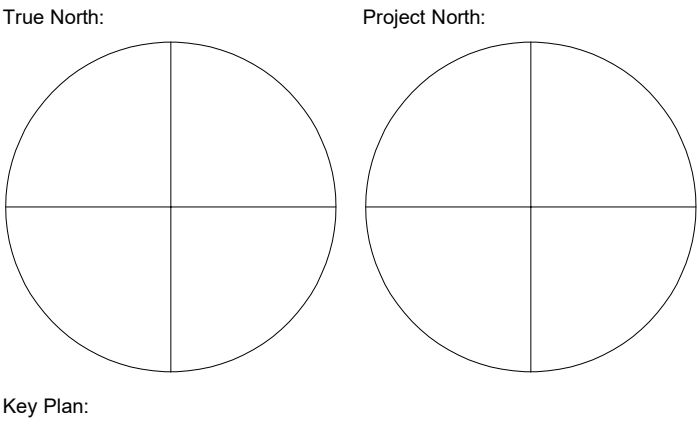
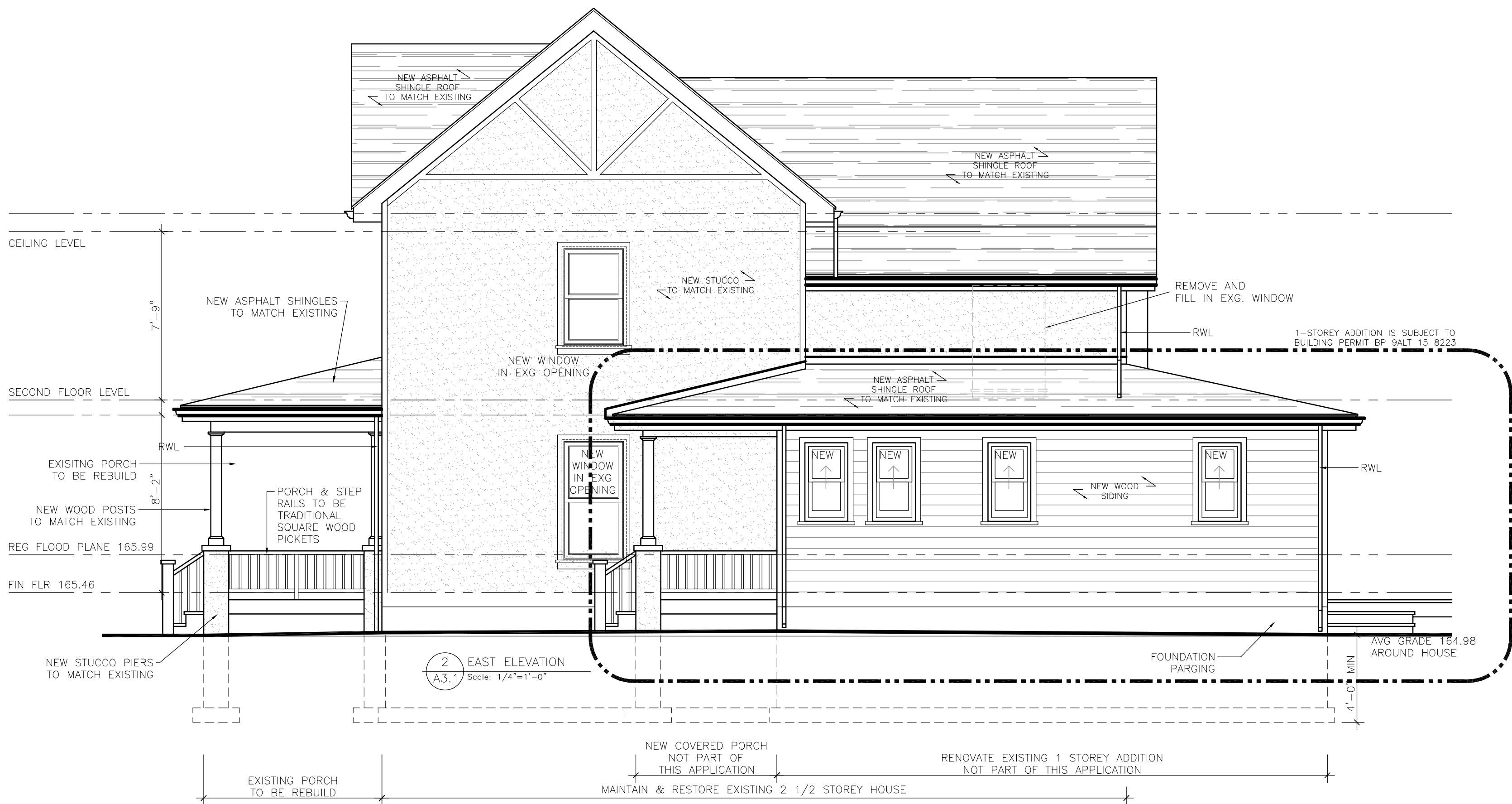
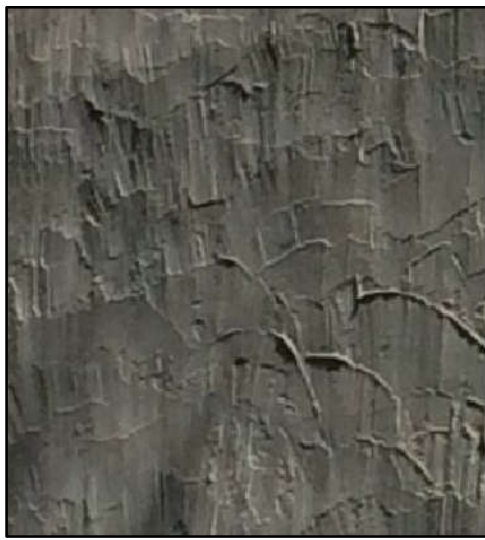
Design By: R.M.	Drawn By: R.M.	Approved By: R.M.
Scale: 1/4" = 1' - 0"	Date: Aug/2017	Project No: 17.43

Drawing No:

A003.0



Stucco to match existing:



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Designer:



Ontario
Association
of Architects

Chavez

LICENSED ARCHITECT
OAA
RICK MATELIAN
LICENCE
7846

Engineer:

Project:

1155 Willow Lane
Mississauga, ON

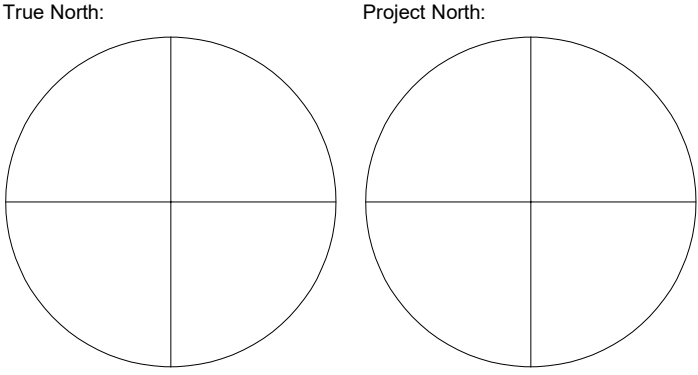
Sheet Title:

North Elevation
East Elevation

Design By: R.M.	Drawn By: R.M.	Approved By: R.M.
Scale: 1/4" = 1' - 0"	Date: Aug/2017	Project No: 17.43

Drawing No:

A003.1



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Designer:

ONTARIO

ASSOCIATION

OF ARCHITECTS



LICENSED TECHNICIAN

OAA

RICK MATELIAN

LICENCE

7846

Engineer:

Project:

1155 Willow Lane
Mississauga, ON

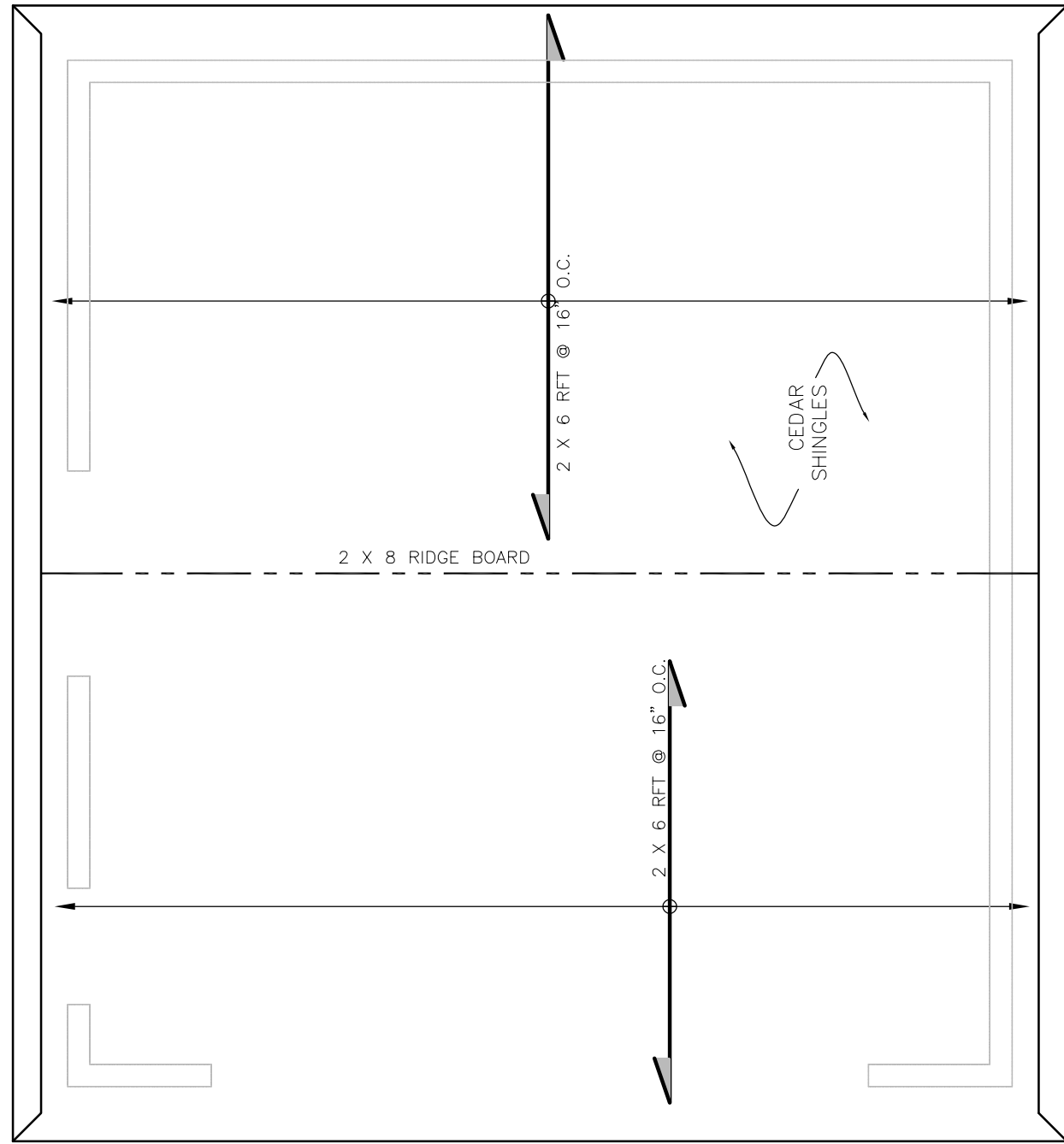
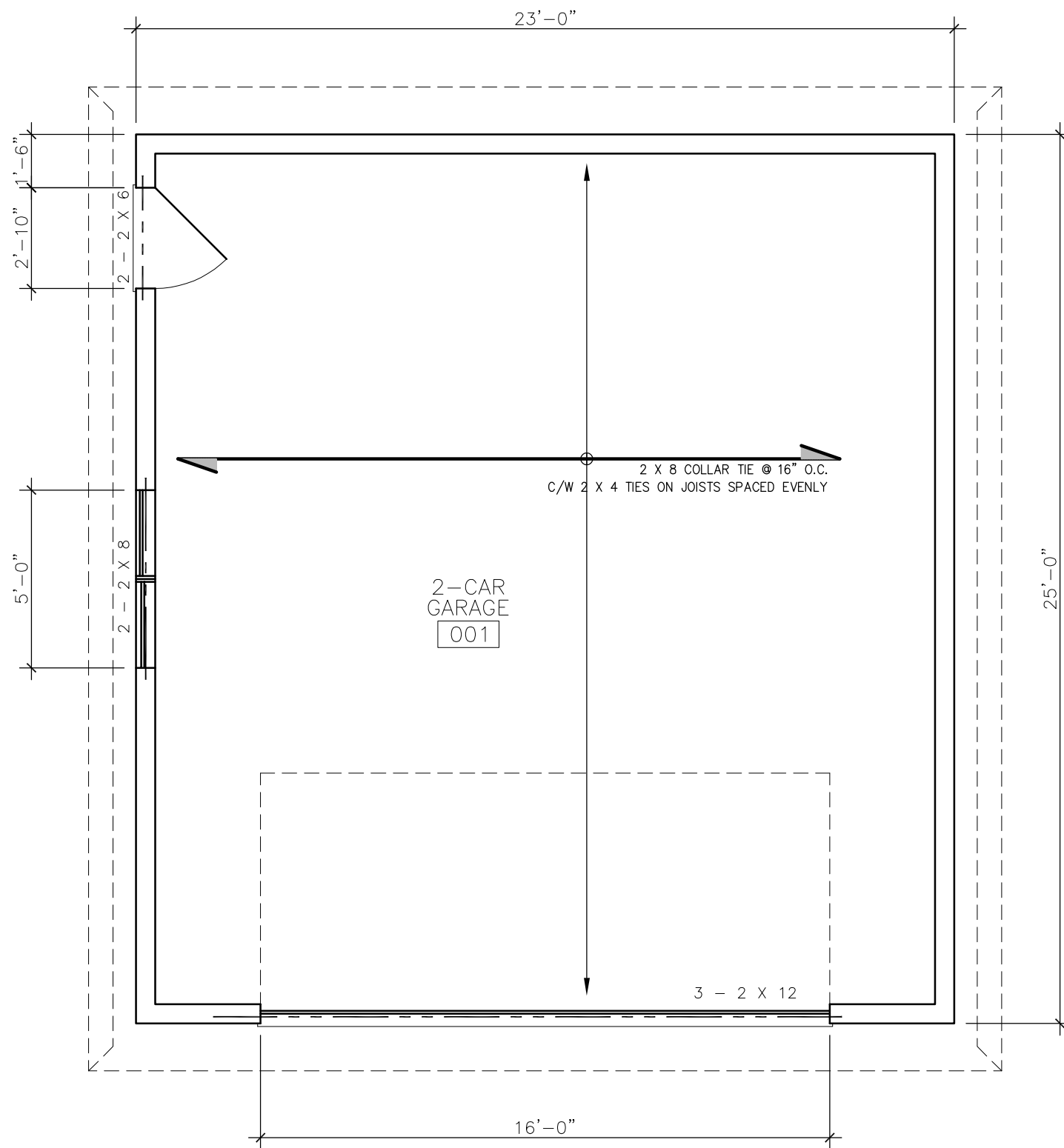
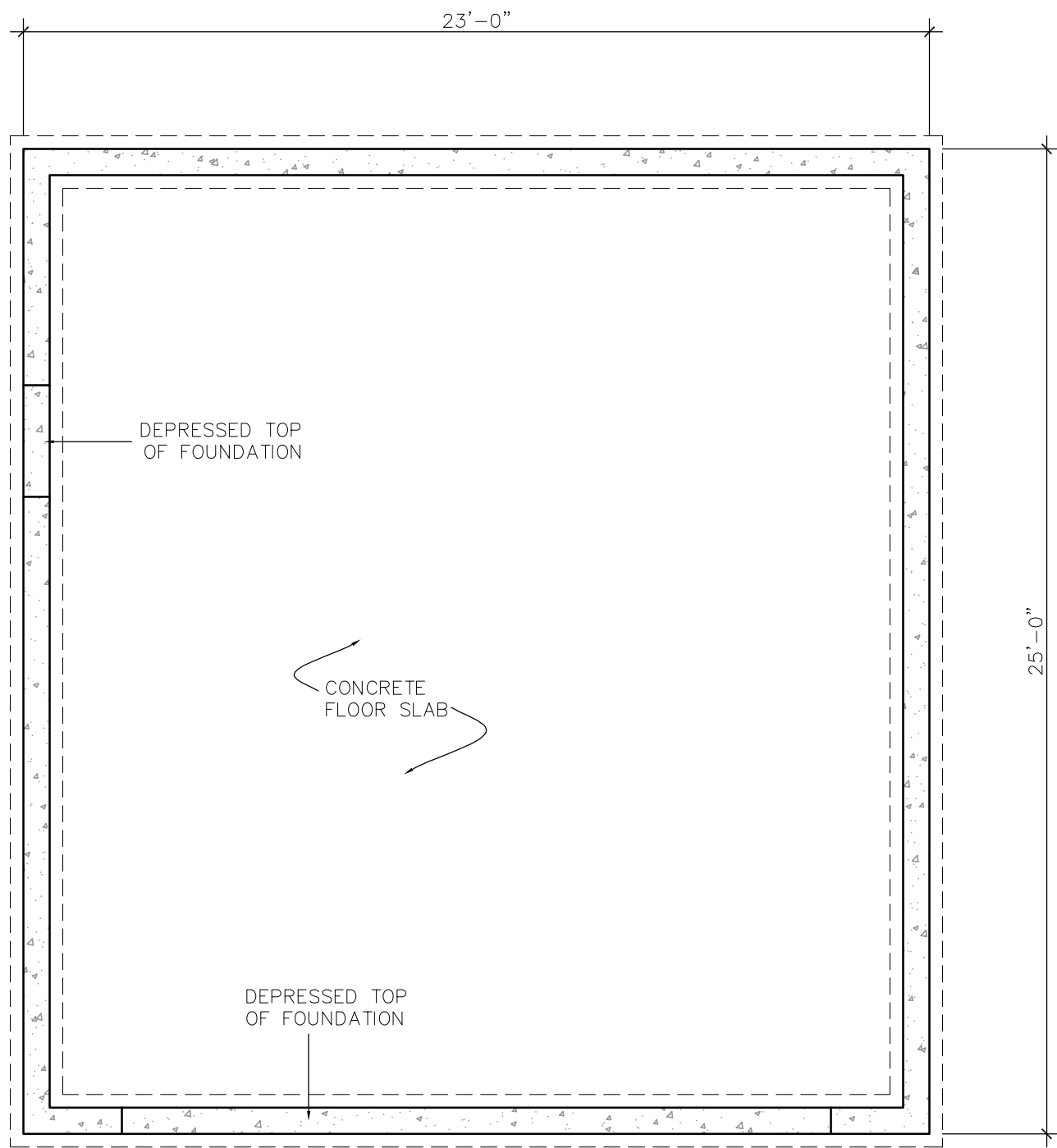
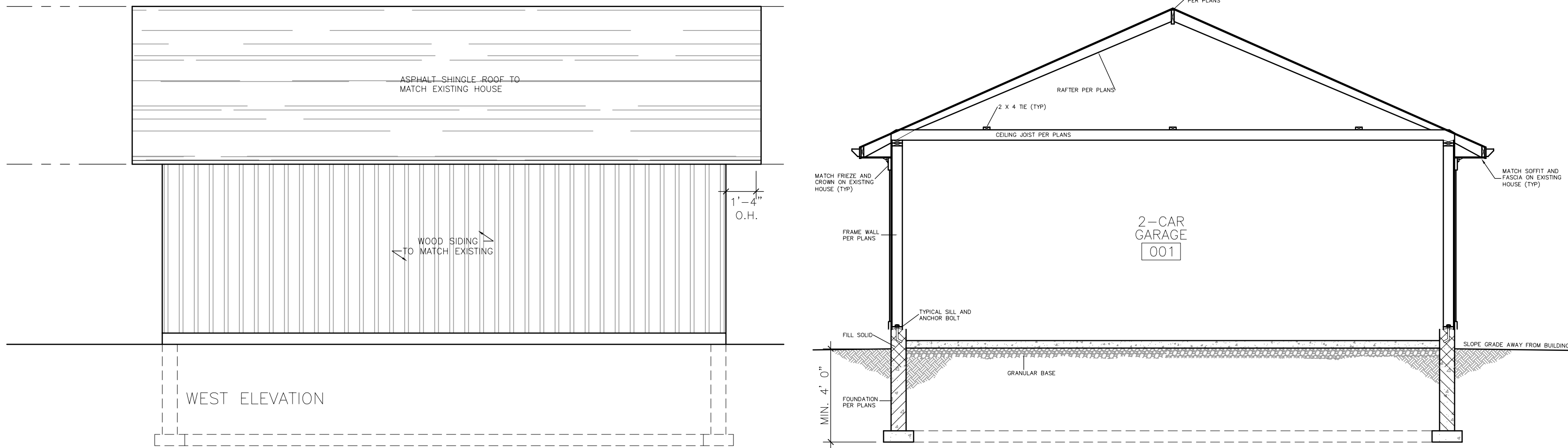
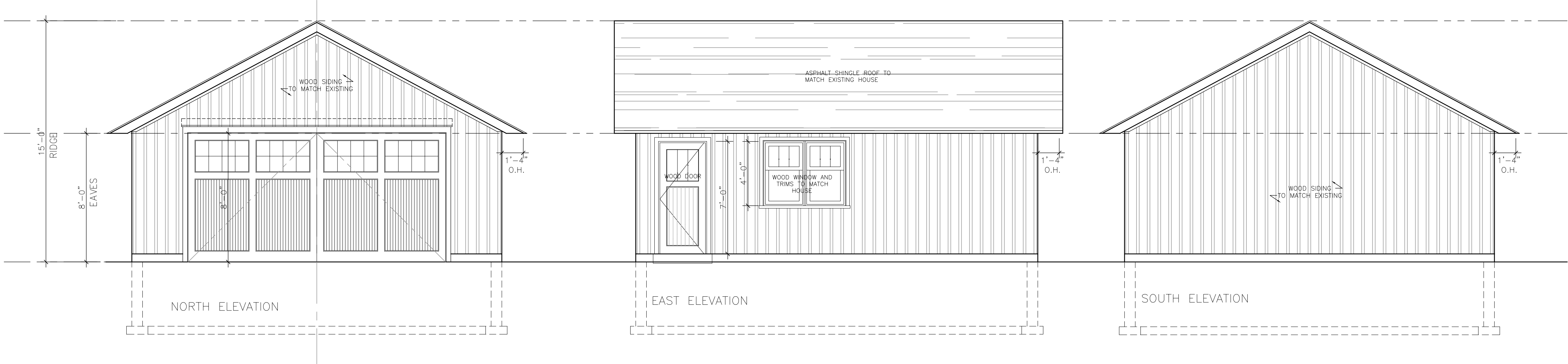
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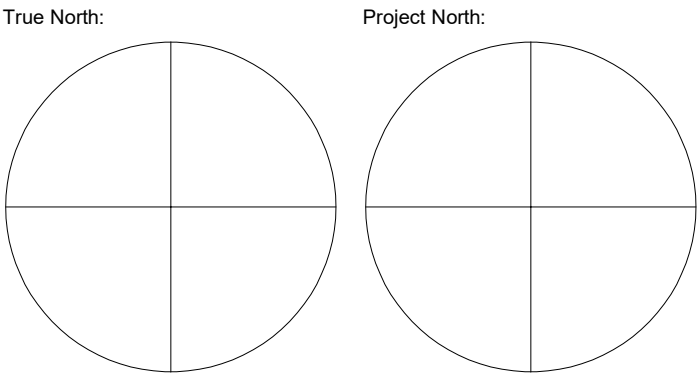
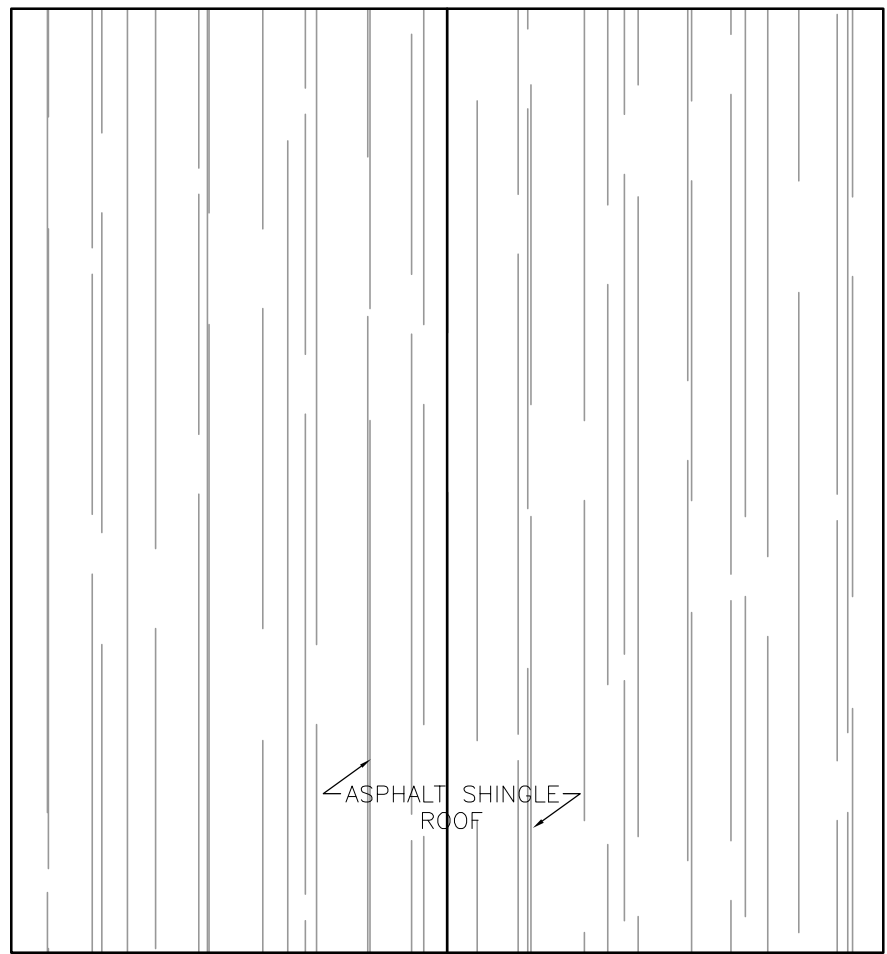
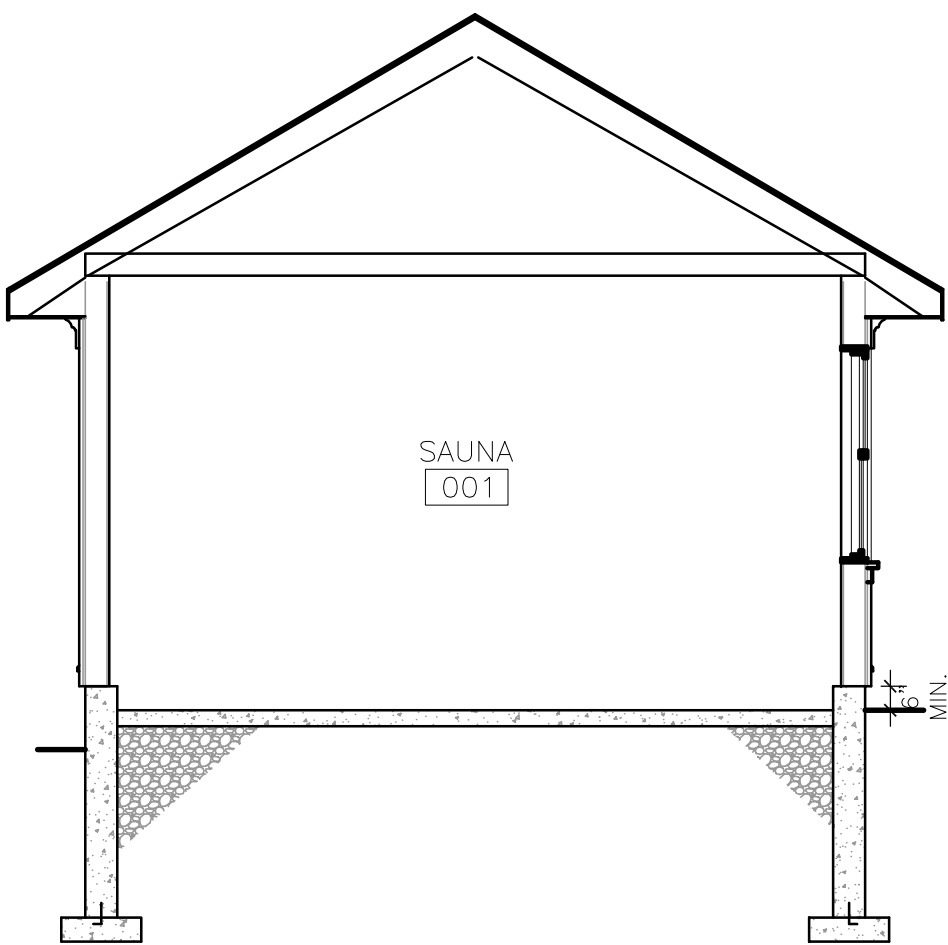
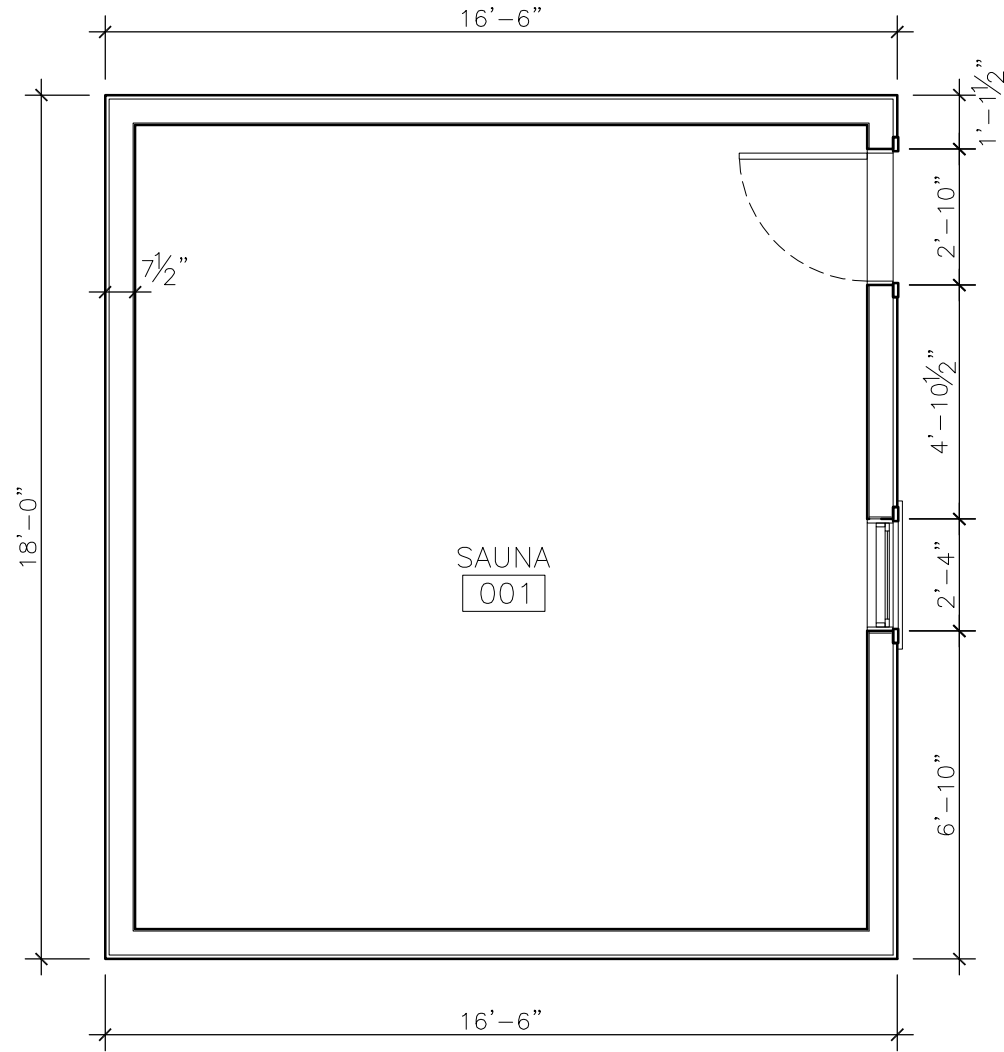
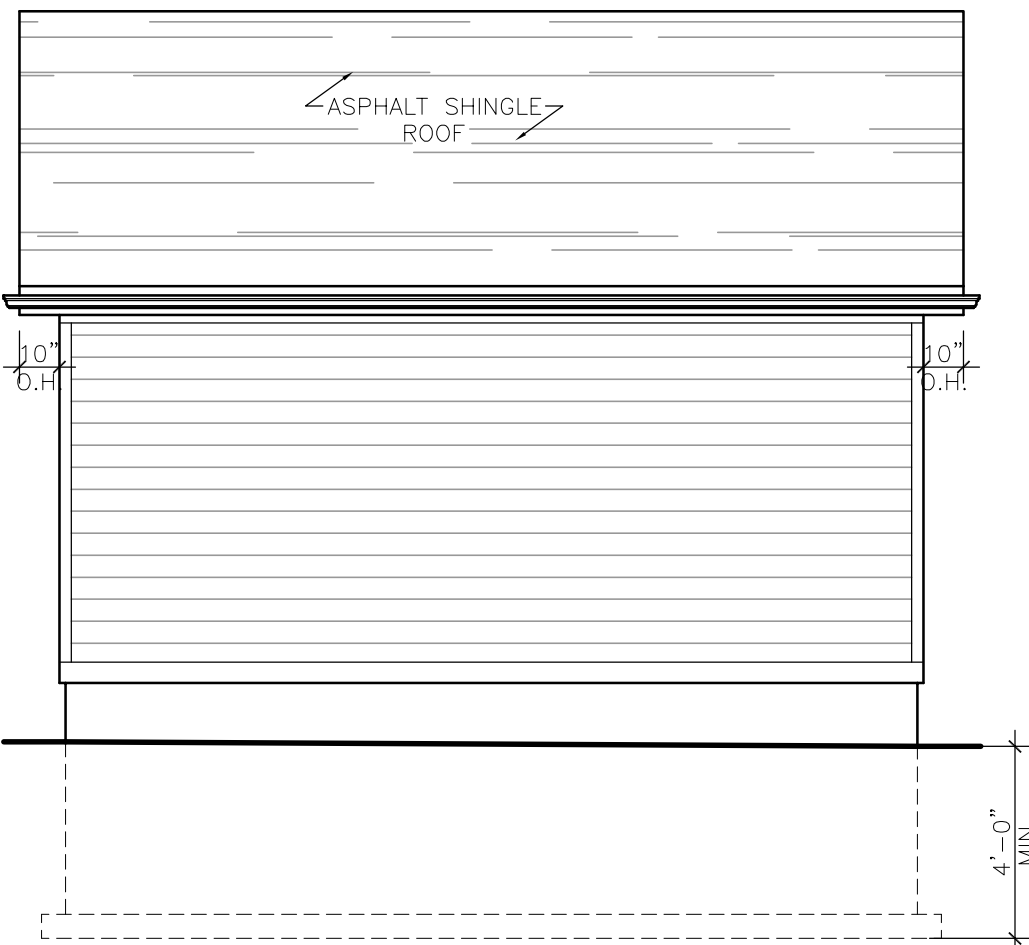
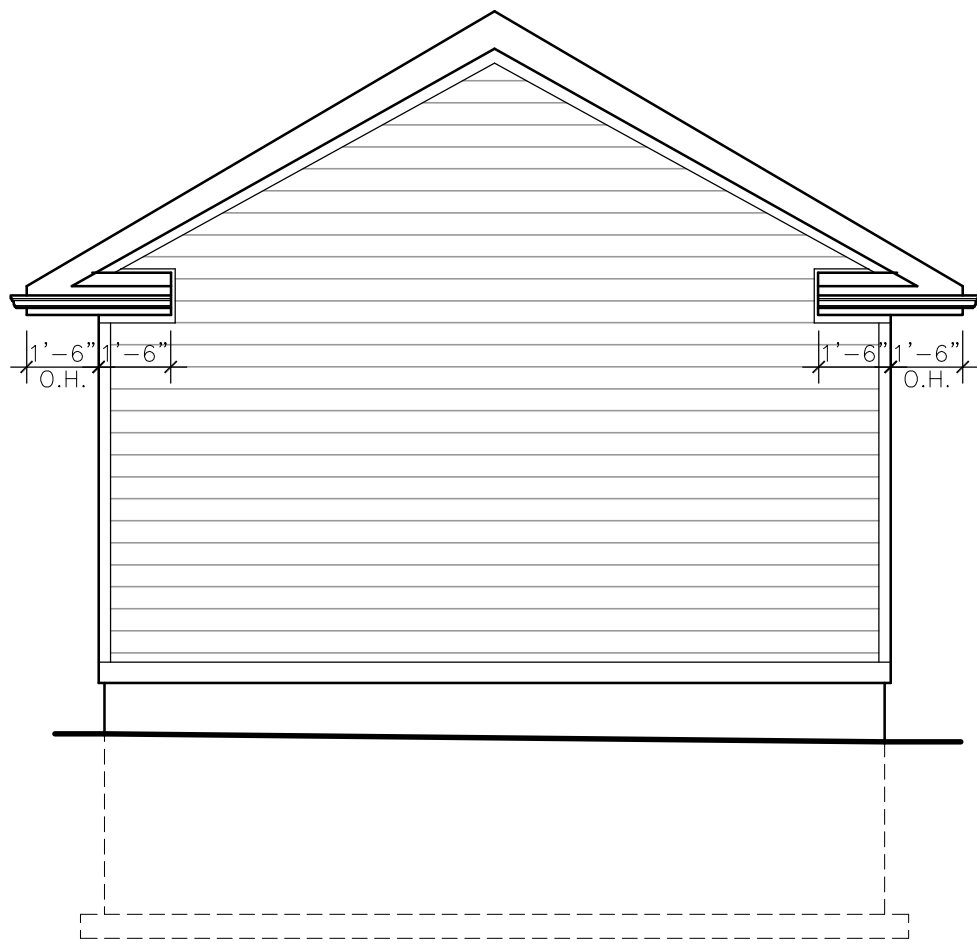
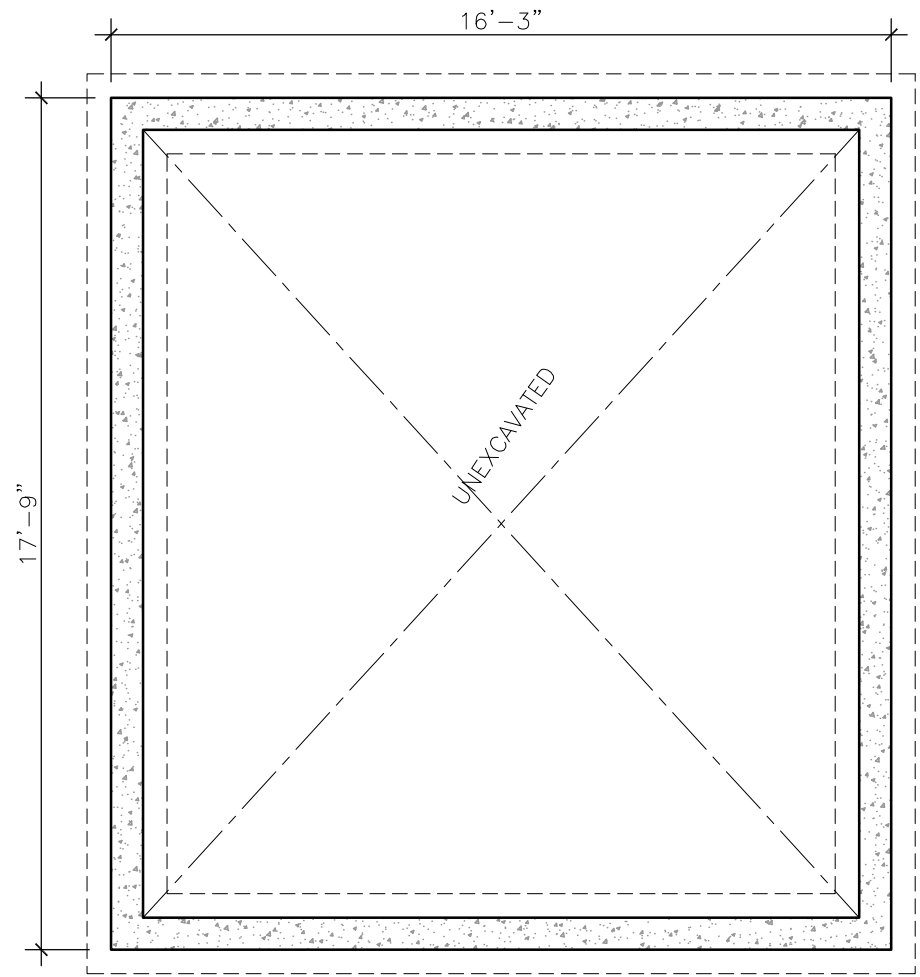
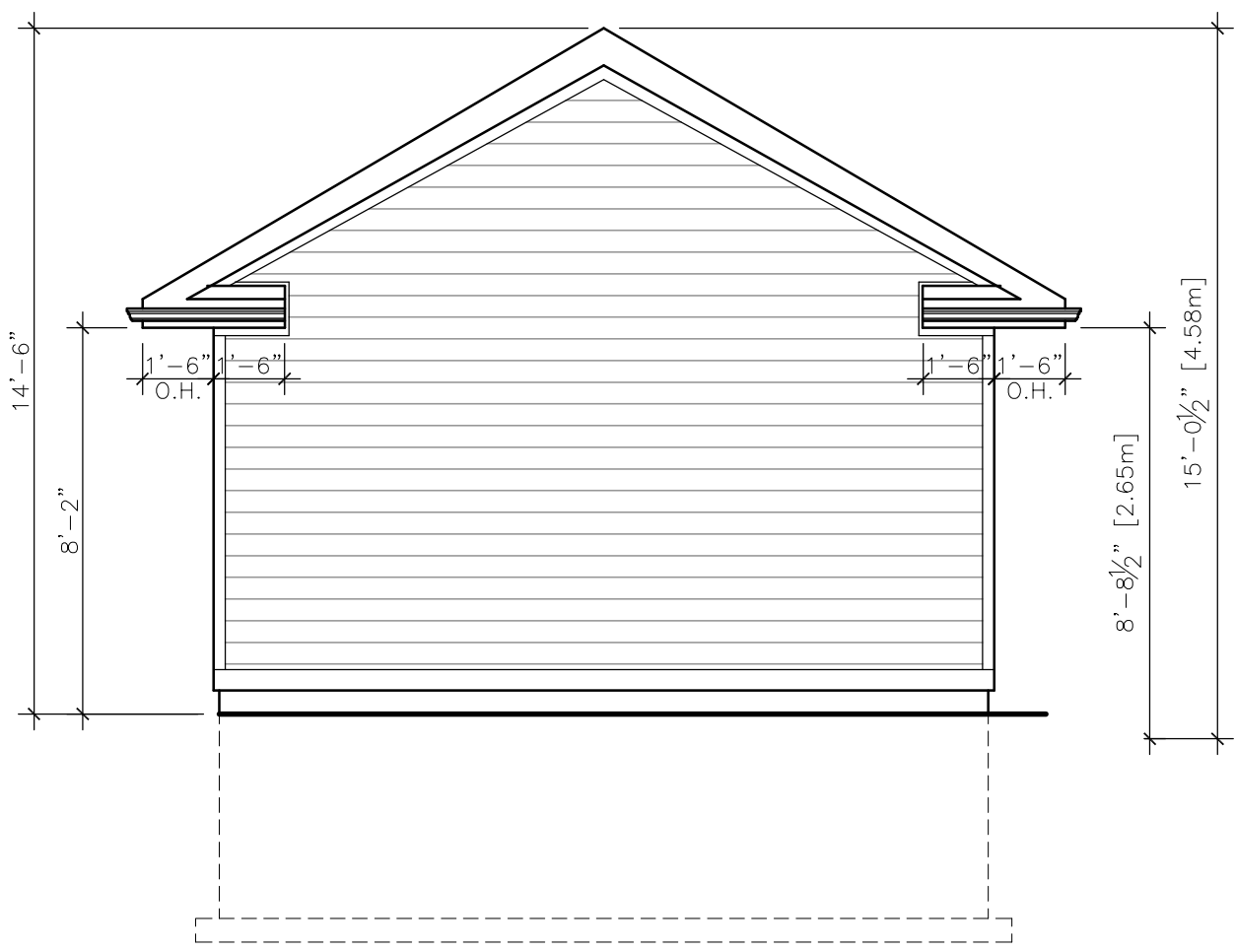
Detached Garage
Plans and Elevations

Design By:	Drawn By:	Approved By:
R.M.	R.M.	R.M.
Scale:	Date:	Project No:
1/4" = 1' - 0"	Aug/2017	17.43

Drawing No:

A004.0





Key Plan:


3	Nov 03/17	Per Heritage Comments	K.A.
2	Oct 30/17	For Heritage Permit	K.A.
1	Oct 06/17	First Issue	K.A.
No.	Date	Issued/Revision	By:



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Designer:	Engineer:
	

Project:

1155 Willow Lane
Mississauga, ON

Sheet Title:

**Shed
Plans and Elevations**

Design By: R.M.	Drawn By: R.M.	Approved By: R.M.
Scale: 1/4" = 1' - 0"	Date: Aug/2017	Project No: 17.43

Drawing No:

A005.0

City of Mississauga

Corporate Report



Date: December 7, 2017

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
Community Services

Originator's files:

Meeting date:
2018/01/09

Subject

Request to Alter a Heritage Designated Property: 1066 Old Derry Road (Ward 11)

Recommendation

1. That the existing sliding glass doors at the rear of the property at 1066 Old Derry Road be permitted to remain.
2. That the request to revise the garage door design at the property at 1066 Old Derry Road be refused.

Background

The subject property is designated under Part V of the *Ontario Heritage Act* as it forms part of the Meadowvale Village Heritage Conservation District (HCD). Changes to the property are subject to the Meadowvale Village HCD Plan, 2014 and substantive changes identified in said plan require a heritage permit.

The City issued a conditional approval for an infill dwelling in 2015. See item 3 here: http://www7.mississauga.ca/documents/agendas/committees/heritage/2015/HAC_Agenda_2015_07_21.pdf. The applicant fulfilled the condition – to reduce the front yard setback – in 2016. The house was built but some design elements do not comply with the approved plans.

This summer the applicant submitted a heritage permit application to rectify the situation. (See item 7.2 here: https://www7.mississauga.ca/documents/committees/heritage/2017/2017_09_05_HAC_Agenda.pdf.) An approval was granted for the modern exterior finishes and the enclosed rear extension on the condition that the extension be modified to match the original approved rear elevation, with a pair of traditional doors and a traditional sash window in place of the sliding glass doors. The applicant now requests permission to modify this elevation with a pair of French doors in place of the window, and to allow the modern garage door, which does not match the approved drawings, to remain. (See Appendices 1 and 2 respectively.)

The Meadowvale Village Heritage Conservation District Advisory Subcommittee met on November 28, 2017. The subcommittee made the following recommendations:

- That the existing sliding glass doors at the rear be permitted (See item 7.2 here: https://www7.mississauga.ca/documents/committees/heritage/2017/2017_09_05_HAC_Agenda.pdf)
- That the proposed modern garage door (installed) be refused and the owner replace it with the original approved design
- That a gravel driveway be permitted in place of the approved paved stone driveway
- That steps be taken – including landscaping – to prevent parking on the front lawn

Comments

Staff had recommended to the Meadowvale Village Heritage Conservation District Advisory Subcommittee that the French doors be refused and the modern garage door be approved as per direction in the HCD plan. The original commentary is available as item 1 here: https://www7.mississauga.ca/documents/committees/heritage/2017/2017_11_28_MVHCD_Agenda.pdf. The community was fully supportive of the applicant's request for the paired sliding glass doors. The sliding glass doors are a minor design element with little impact to the overall property.

Financial Impact

There is no financial impact.

Conclusion

The owner of the subject property requests permission to allow a modern garage door and sliding glass rear doors, which are installed but do not match the approved drawings. Staff support the Subcommittee's recommendation to allow the sliding glass rear doors but refuse the modern garage door and to install the garage door to its original proposed design.

Attachments

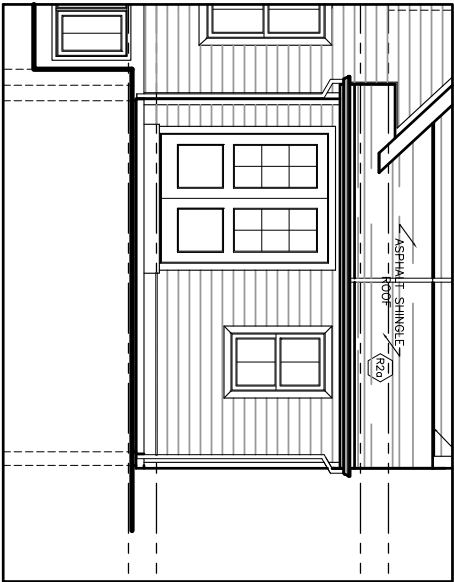
Appendix 1: Revised drawing of rear of dwelling

Appendix 2: Revised drawing of garage



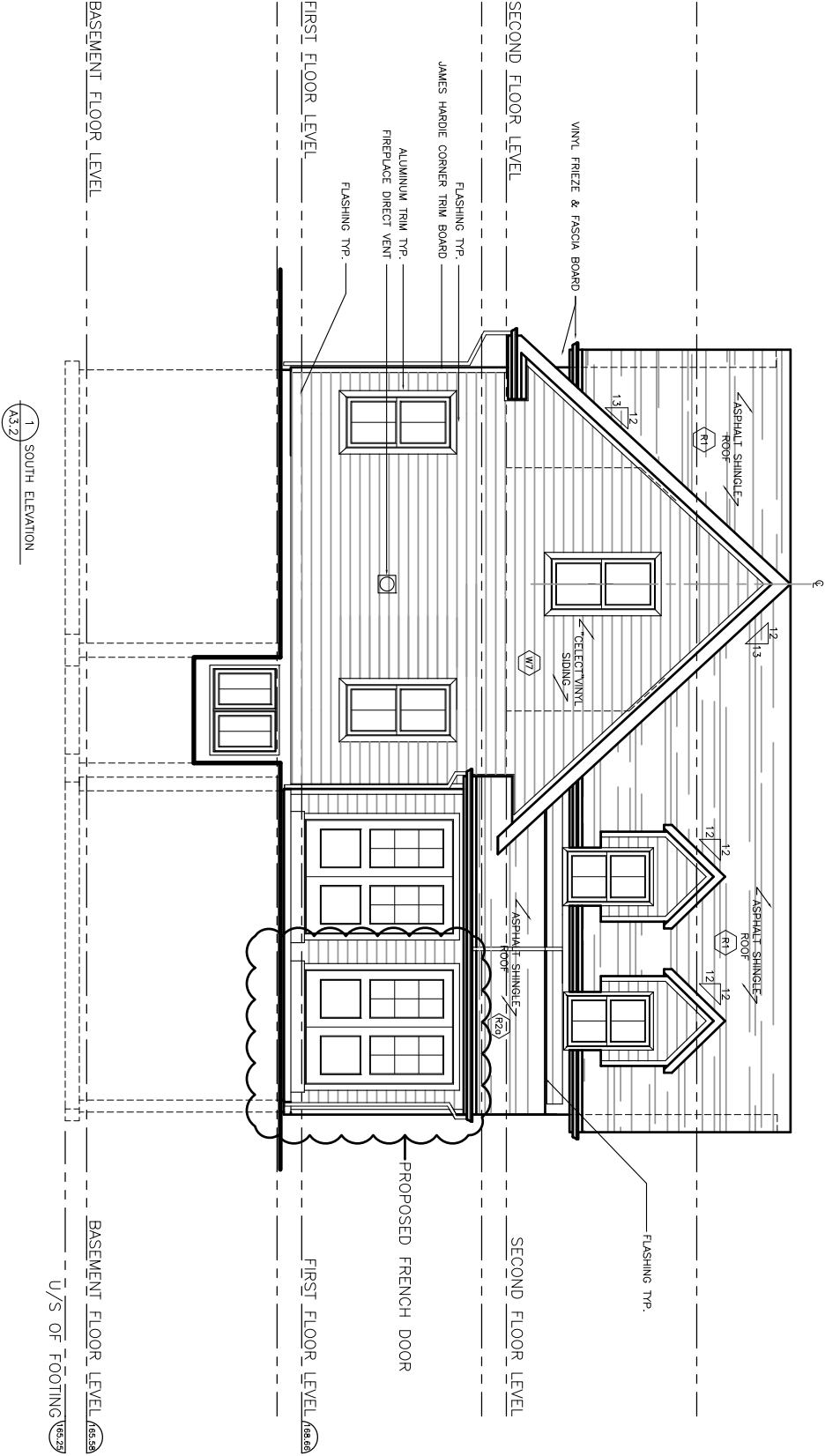
Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: P. Wubbenhorst, Senior Heritage Coordinator

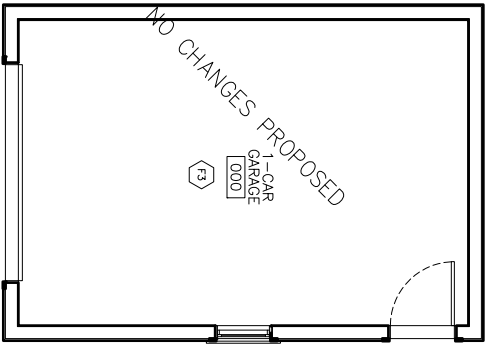
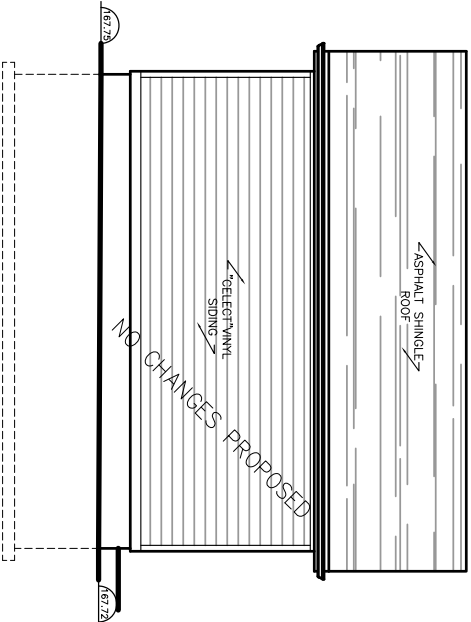
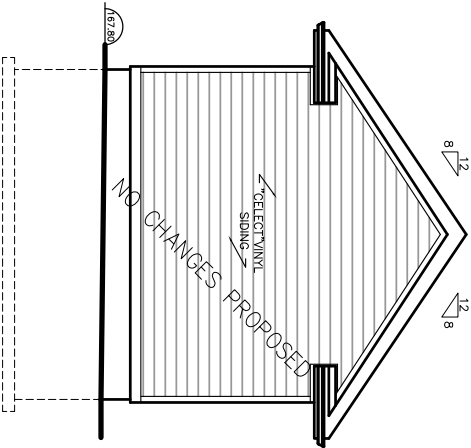
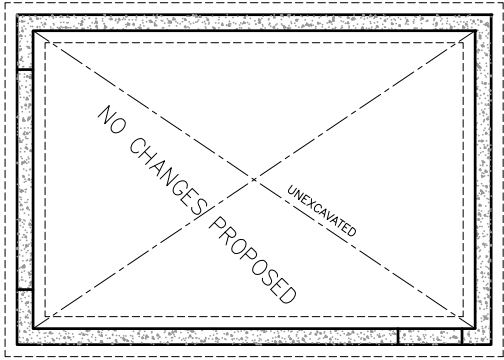
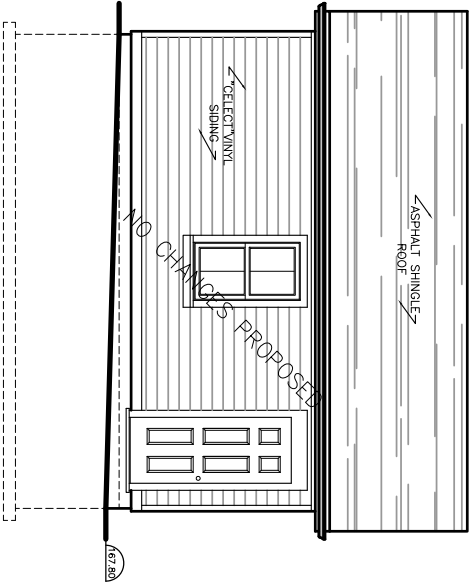
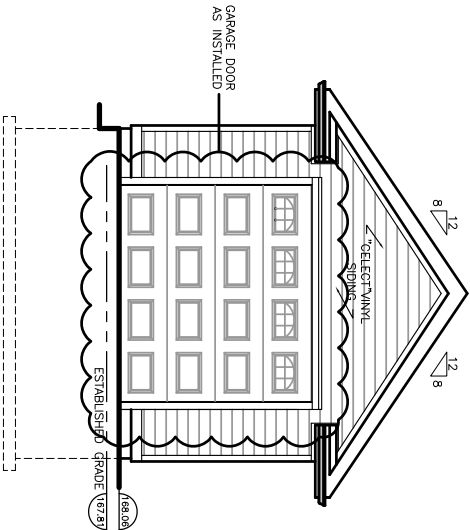
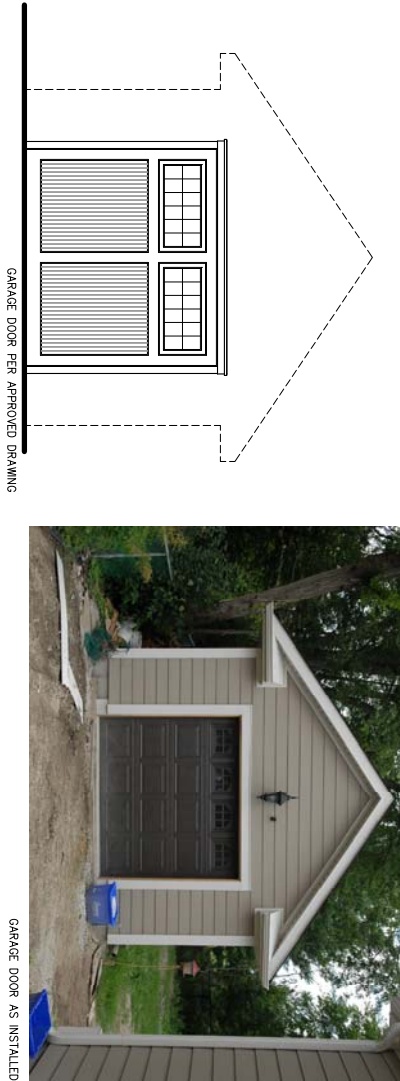


AS APPROVED

7.3 - 3



1 SOUTH ELEVATION



City of Mississauga
Corporate Report



Date: December 7, 2017

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
 Community Services

Originator's files:

Meeting date:
 2018/01/09

Subject

Request to Alter a Heritage Designated Property: 1059 Old Derry Road (Ward 11)

Recommendation

That the request to install a concrete deck at the rear of the property at 1059 Old Derry Road as per the Corporate Report from the Commissioner of Community Services dated December 7, 2017, be approved.

Background

The subject property is designated under Part V of the *Ontario Heritage Act* as it forms part of the Meadowvale Village Heritage Conservation District (HCD). Changes to the property are subject to the Meadowvale Village HCD Plan, 2014 and substantive changes identified in said plan require a heritage permit. Non-substantive changes that do not comply with the design guidelines also require a heritage permit. The subject proposal is for a new rear concrete deck, which has been installed. Appendix 1 outlines the proposal.

Comments

The subject property has been undergoing a major redevelopment, including the installation of a cold room in the basement. The applicant was advised to install a concrete deck to prevent rain/snow leakage below. The proposed deck is simple and, additionally, has limited visibility from the public realm. As such, it should be approved. The Meadowvale Village Heritage Conservation District Advisory Subcommittee concurs with the staff recommendation.

Financial Impact

There is no financial impact.

Conclusion

The owner of the subject property has applied to install a concrete deck at the rear of the property. As the proposal is simple and has limited visibility from the public realm, it should be approved.

Attachments

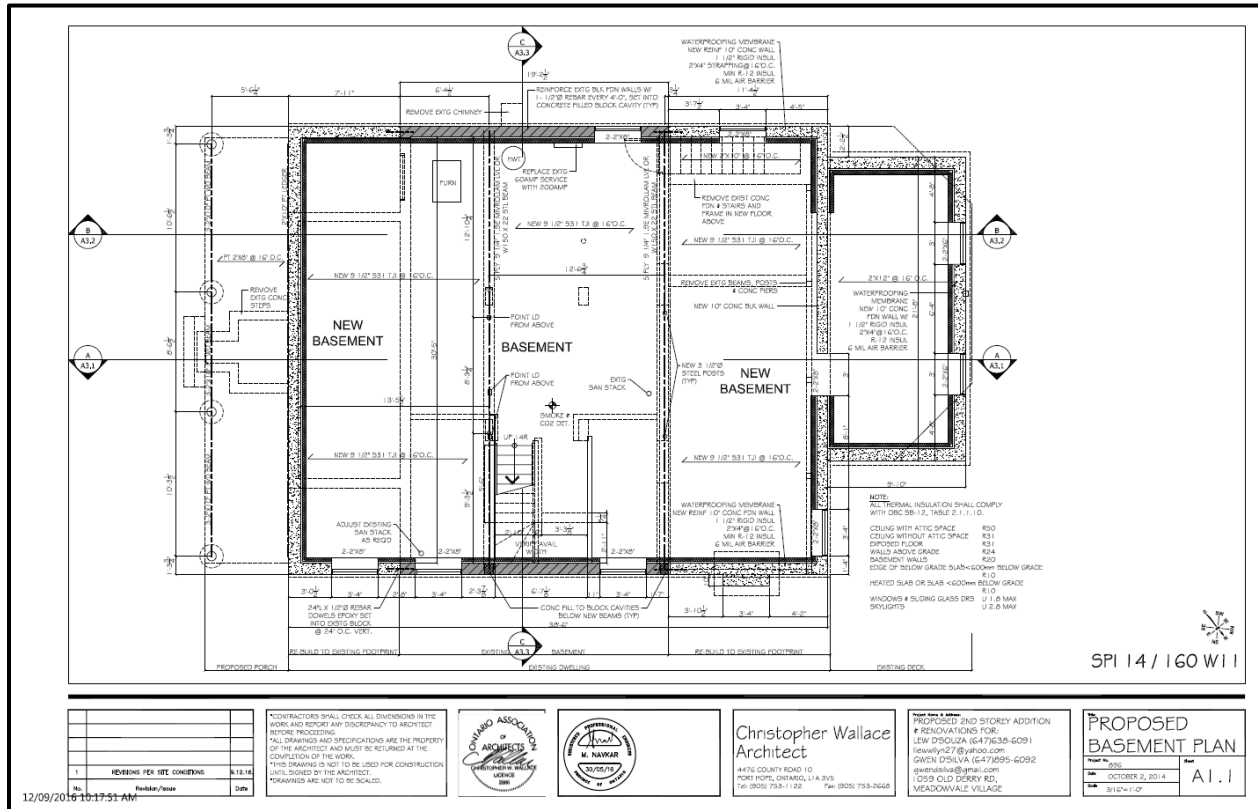
Appendix 1: Supporting documentation



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: P. Wubbenhorst, Senior Heritage Coordinator

As part of the original designs and permit submitted, it was intended to have a wooden deck in the back of the house. In Aug – 2016, we submitted a revision to this plan. The revised plan proposed a cold room under the backyard deck and a porch at the front of the house without a crawl space. We removed the crawl space from our proposal as we were concerned about the integrity of a very mature tree at the front of the house. (see figure 1)



During construction, we were advised that there was a good chance of rain or snow leaking into the cold room below the backyard deck since the deck was without a roof (ex: Pergola, Awning). The contractor recommended a concrete deck. We submitted this change as part of BPA-9896 to the city of Mississauga, Planning & building on Dec 13- 2016. As illustrated in the picture 2 and 3, the proposal was to have a concrete deck that would be clad with pressure treated decking wood.



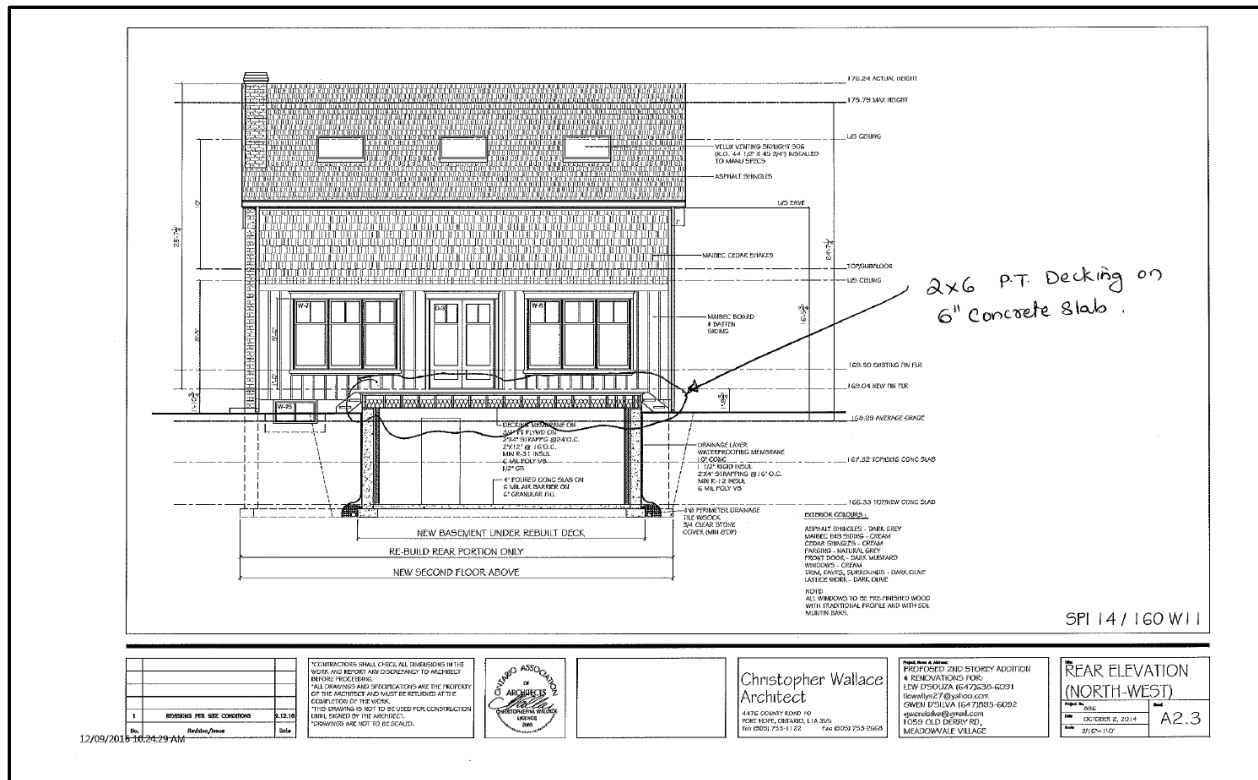


Figure 3: Rear view of backyard deck showing concrete deck clad with PT wood

My proposal is to leave the concrete deck exposed and not to put any pressure treated wood cladding over the concrete deck. Doing so would be a waste of financial resources and does not add value to the property. Being in the rear of the house and coupled with the fact that the deck is narrower than the width of the house, this deck is not visible from Old Derry road or from the adjacent roads. As the backyard is fenced in, it is impossible for this deck whether it is concrete or wood clad to be visible to neighbors or passerby's. (See Figure 4,5)



Figure 4: View of Concrete deck from backyard – note width of deck is less than width of house



Figure 5: Side view of Concrete deck from backyard - note width of deck is less than width of house

City of Mississauga

Corporate Report



Date: December 7, 2017

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
Community Services

Originator's files:

Meeting date:
2018/01/09

Subject

Request to Alter a Heritage Designated Property: 1036 Old Derry Road (Ward 11)

Recommendation

That the request to install an asphalt driveway at the property at 1036 Old Derry Road as per the Corporate Report from the Commissioner of Community Services dated December 7, 2017, be approved.

Background

The subject property is designated under Part V of the *Ontario Heritage Act* as it forms part of the Meadowvale Village Heritage Conservation District (HCD). Changes to the property are subject to the Meadowvale Village HCD Plan, 2014 and substantive changes identified in said plan require a heritage permit. Non-substantive changes that do not comply with the design guidelines also require a heritage permit. The subject proposal is for a new asphalt driveway. Appendix 1 outlines the extent of the driveway, which, in its entirety, is proposed to be non-permeable asphalt.

Comments

The Meadowvale Village HCD plan stipulates that "permeable paving methods are permitted." As such, non-permeable paving requires a heritage permit. As per the proponent's heritage permit application: "The driveway at this residence is steeply inclined and there are safety implications for vehicles and pedestrians." The driveway is currently gravel and the owner has found it "unsafe to back the vehicle down." Due to the safety concerns, staff recommend that the asphalt driveway be approved. The Meadowvale Heritage Conservation District Advisory Subcommittee concurs with the staff recommendation.

Financial Impact

There is no financial impact.

Conclusion

The owner of the subject property has applied to install a non-permeable asphalt driveway. Due to the safety concerns, the driveway should be approved.

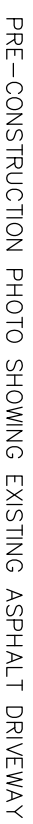
Attachments

Appendix 1: Site plan drawing

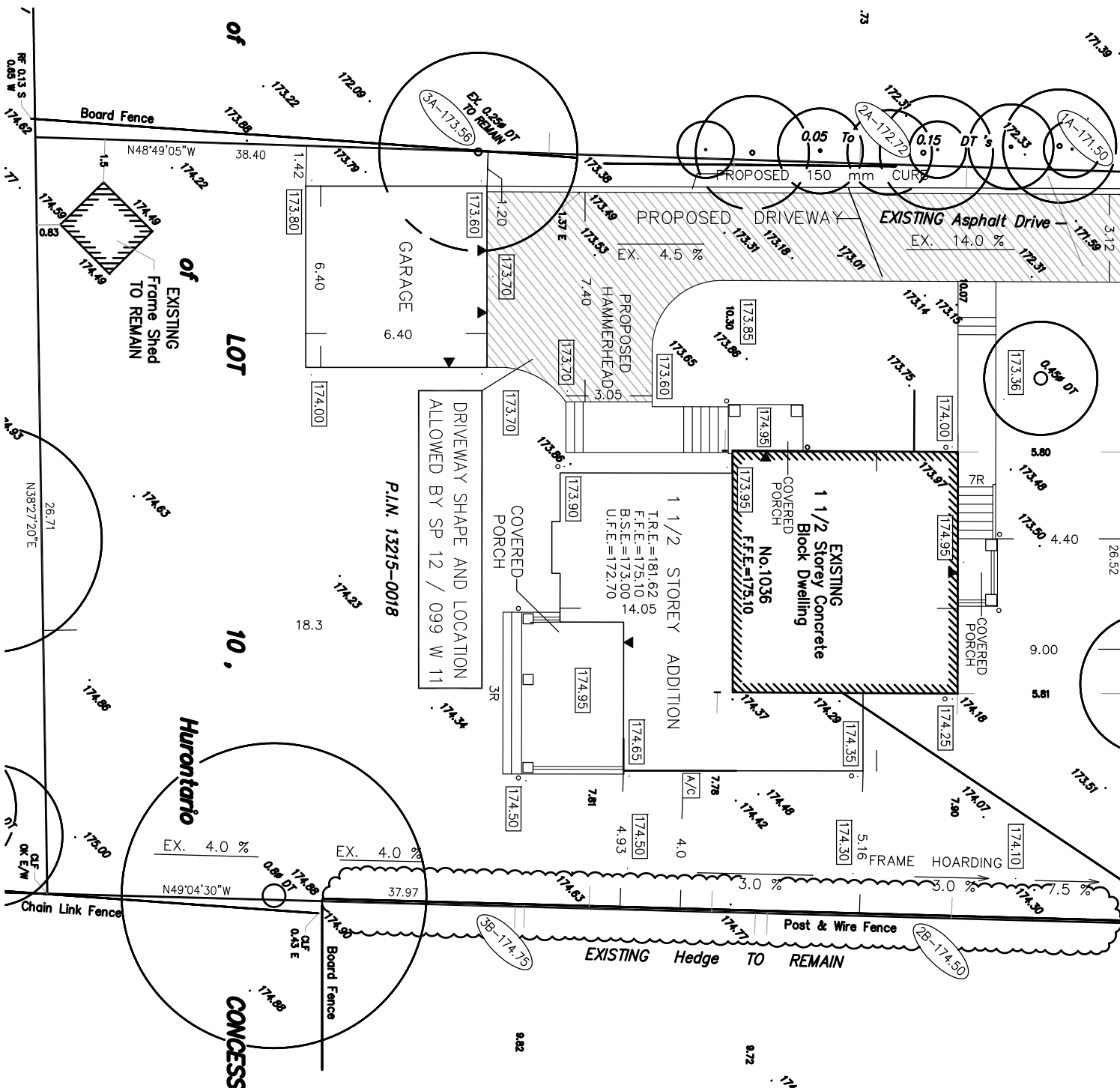


Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: P. Wubbenhorst, Senior Heritage Coordinator



(Road Allowance Between LOTS 10 and 11, Concession 3, West of Yonge Street)

[illegible]

City of Mississauga
Corporate Report



Date: December 7, 2017

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
 Community Services

Originator's files:

Meeting date:
 2018/01/09

Subject

Request to Demolish a Heritage Listed Property: 36-46 Main Street (Ward 11)

Recommendation

That the property at 36-46 Main Street, which is listed on the City's Heritage Register, is not worthy of heritage designation, and consequently, that the owner's request to demolish proceed through the applicable process.

Background

Section 27.3 of the Ontario Heritage Act states that structures or buildings on property listed on the City's Heritage Register cannot be removed or demolished without at least 60 days' notice to Council. This legislation allows time for Council to review the property's cultural heritage value to determine if the property merits designation.

The owner of the subject property has submitted a heritage permit application to demolish and replace the existing detached dwelling. The subject property is listed on the City's Heritage Register as it forms part of the Streetsville Village Core and Credit River Corridor cultural landscapes. The City's Heritage Register includes the following description: "Streetsville is recognized as a significant cultural landscape because it retains a portfolio of heritage buildings of a consistent scale and portrays a period landscape of a small village" as well as "including extant churches, cemeteries, public buildings and open spaces." The Credit River Corridor cultural landscape is significant due to it being a "scenic rare natural landmark in the city." The Credit River is an ecologically, archaeologically and historically significant feature in the City, noted for its importance in the development of Mississauga.

Comments

The owner of the subject property has requested permission to demolish the existing structure. The applicant has provided a Heritage Impact Assessment, Arborist Report and Tree Preservation Plan attached as Appendices 1, 2 and 3 respectively. The consultant has concluded that the structure at 36-46 Main Street is not worthy of designation. Staff concurs with this finding.

Financial Impact

There is no financial impact.

Conclusion

The owner of 36-46 Main Street has requested permission to demolish a structure on a property that is listed on the City's Heritage Register. The applicant has submitted a documentation report which provides information which does not support the building's merit for designation under the Ontario Heritage Act. Staff concurs with this finding.

Attachments

Appendix 1: Heritage Impact Assessment

Appendix 2: Arborist Report

Appendix 3: Tree Preservation Plan



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: P. Wubbenhorst, Senior Heritage Coordinator

**Heritage Impact Assessment
36, 38, 40, 44 & 46 Main Street
Mississauga (Streetsville)**



prepared by

CHC Limited

87 Liverpool Street, Guelph, ON N1H 2L2
(519) 824-3210 email oscott87@rogers.com

October 17, 2017
addendum - November 4, 2017

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all photographs by Owen R. Scott of CHC Limited, October 5, 2017 and November 3, 2017 unless otherwise noted.

1.0 BACKGROUND - HERITAGE IMPACT ASSESSMENT (HIA)

The property at 36, 38, 40, 42 & 46 Main Street in Mississauga (Streetsville) is listed on the City's Heritage Register. It is not designated under Part IV of the *Ontario Heritage Act*. It is located in the "Streetsville Village Core Cultural Landscape" and adjacent the "Credit River Corridor Cultural Landscape".¹

This Heritage Impact Assessment (HIA) follows the City of Mississauga *Cultural Landscape Heritage Impact Assessment (HIA) Terms of Reference*² and was prepared in response to a request from the owner.

Figure 1 illustrates the east central location of the property in the historic Streetsville Village Core and the adjacent Credit River Corridor cultural landscapes.



Figure 1

Site Context - <http://www.mississauga.ca/portal/services/maps>

2.0 THE HERITAGE IMPACT ASSESSMENT

2.1 Site history

The settlement story of Streetsville begins in 1818 when the Crown acquired all lands north of modern Eglinton Avenue, throughout Halton and Peel counties, from the Native Mississaugas. The Government commenced formal survey of these lands in 1819. Timothy Street financed the survey and Richard Bristol oversaw the work. Following this, settlers began to apply for land grants in and around what would

¹ *Cultural Landscape Inventory, City of Mississauga, The Landplan Collaborative Ltd., January 2005*

² *City of Mississauga Cultural Landscape Heritage Impact Assessment (HIA) Terms of Reference, 2017*

become the Streetsville area.

By 1835, Streetsville had attracted many merchants and tradesmen. The community was becoming the political and economic centre of the surrounding township, with the Credit River acting as the backbone of the village. Grist mills, sawmills and tanneries were established milling enterprises along the river. Just south of Streetsville was William Comfort's mill site, which was purchased by the Barber Brothers in 1843. At its height the Barber mill was home to one of the largest woollen manufacturing centres in Canada.

By 1850, with a population of 1000, Streetsville had emerged as the most prosperous and populous village in Peel County. Early directories list several mills, a tannery, foundry, cooperage, pottery, brickyard, blacksmiths, shoemakers, carriage shops, tinsmith, brewery, telegraph office, physicians, tailors, gunsmith, watchmaker, broom and pail factory, millinery, carpenter, furniture manufacturer, stave factory, bobbin factory, four churches, an Orange Lodge, and two schools.

The intersection of Queen Street and Main Street quickly became the commercial hub of the community, anchored in large part by the enterprises of the Barnhart's Montreal House and John Embleton's store. In 1858, Streetsville had a population of around 1,500, and incorporated as a village, with John Street, Timothy's son, serving as the first Reeve. Streetsville was considered by many as the "Queen of the County", and was the most populated and prosperous area in Peel County. The coming of the railways in the 1850s, which initially bypassed Streetsville, brought a halt to the village's prosperity. By Confederation the population had dwindled to 750 inhabitants.

Although Streetsville's prosperity peaked before 1867, the village continued to thrive after the arrival of the Credit Valley Railway in 1879. It was too late, however, for the village to supplant Brampton as the business and political centre of Peel. Much of the existing built form of Streetsville dates from the post-Confederation period, and reflects the story of this prosperous and industrial rural village.

Many of the mills, which were once the lifeblood of the village, began to close in the early 20th century. Timothy's mill burned in 1929. The Temperance Act spelled the end for most of Streetsville's inns and hotels. The Royal Hotel, the last operating hotel in Streetsville, closed in the 1940s. The village gradually changed from an industrial mill-town into a small business and services centre.

By 1951, the population of Streetsville was registered as 1,139 people. The village officially became a town on January 1st, 1962.... However, the town could not expand, as it was surrounded by the new Town of Mississauga (formerly Toronto Township), and bordered on one side by the Credit River. In 1974, the Town of Streetsville amalgamated with the Towns of Mississauga and Port Credit to form the City of Mississauga.³

Of the five properties fronting on Main Street, 36 Main Street is the only one with an extant building as of October 2017. The property (PIN 13199-0516) is illustrated on the 1856 "A New Plan of Streetsville from actual survey & careful reference to original plans & documents published for subscribers by Bristow, Fitzgerald & Spencer".⁴ (Figures 2 & 3). The Plan shows a building at the corner of Main and Wyndham Streets, which is not the current house. In 1881 the property was willed by Michael McDonnell, tailor, to Edward McDonnell "of the Village of Streetsville in the County of Peel and Province of Ontario, Gardener (an unmarried man)". The title search failed to reveal earlier ownerships of the property; however, the 1856 Plan shows a McDonnell as the owner.

³ Heritage Mississauga web page <http://www.heritagemississauga.com/history.htm>,

⁴ Bristow, Fitzgerald & Spencer, *A New Plan of Streetsville, 1856* [map]. Library and Archives Canada.



Figure 2

"A New Plan of Streetsville", 1856 - *Library and Archives Canada* - subject property in red



Figure 3 detail from “A New Plan of Streetsville, 1856” - subject property in red

On Figure 3, 36 Main Street (part of Lot 1 on the *Plan*) is shown with three buildings on the property. An outbuilding is to the rear along Wyndham Street where one of four houses is now located. Another building is located on Main Street. Water Street is shown along the west bank of the Credit River. While the author of the report has not discovered its history, this portion of Water Street appears to have remained an unopened road allowance throughout. Lot 2 on the *Plan* is shown as being owned by J. Sterling. John Sterling’s name appears on a number of lots in the Village of Streetsville.

Tremaine’s map of 1859⁵ (Figures 4 & 5) shows a similar situation to that of “A New Plan of Streetsville” with buildings in the same locations on Lot 1 (36 Main Street).

⁵ Tremaine’s Map of the County of Peel Canada West, compiled and drawn by Geo. R. Tremaine from actual survey, Toronto, published by G. R. & G. M. Tremaine. 1859

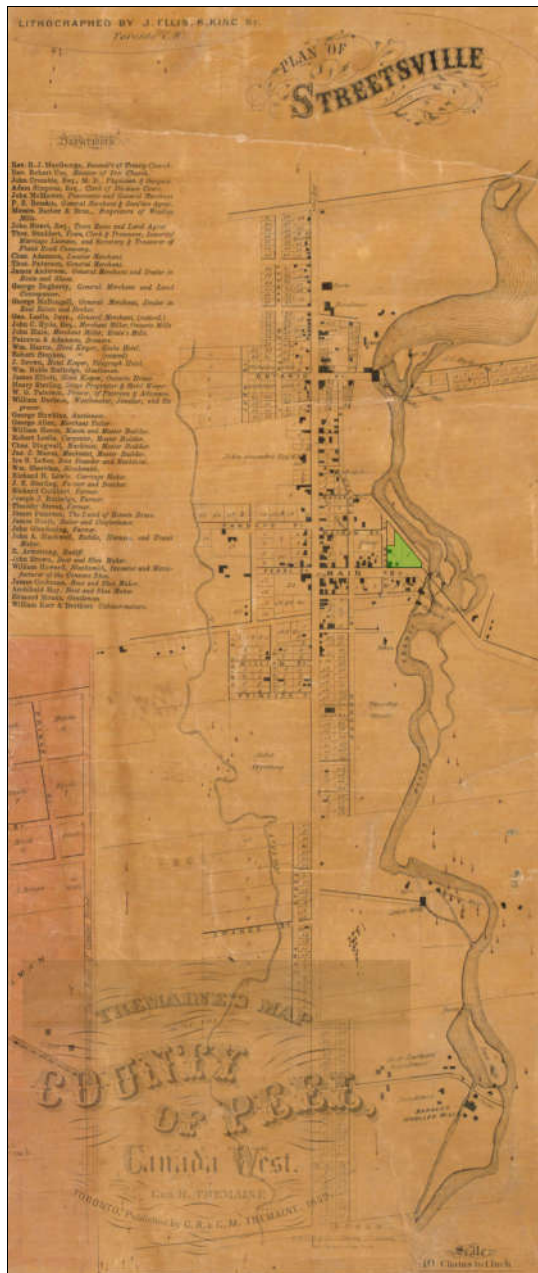


Figure 4 Tremain's Map 1859 - Streetsville



Figure 5

detail from Tremain's Map 1859



Figure 6 2007 airphoto - original house at 36 Main, black square

A series of airphotos of the neighbourhood, from 1944 to 2017 (Figures 7 through 15) shows the evolution of the area. In 1944, there appear to be three buildings on Main Street on the subject properties (Figure 6). By 1954 there appear to be 5 buildings (Figure 8). The building at 36 Main Street appears to be much smaller in the early photos when compared to the current building. Figure 5 is a 2007 airphoto showing the house at 36 Main Street and the adjacent, now demolished houses. What is surmised to be the original house at 36 Main is outlined by the black square. The +/- 600 ft.² outline is similar in size to its neighbours. Whether the house was originally a single storey or 1½ storeys is not known. The current house, with its additions, can be described as “Dutch Colonial Revival” referring to the Dutch colonists who settled in the lower parts of New York and New Jersey. The style was popular in Ontario from about 1900 to the 1930s. The most characteristic feature is the gambrel roof.



Figure 7 - subject property & environs 1944

<http://www.mississauga.ca/portal/services/maps>



Figure 8 - subject property & environs 1954

<http://www.mississauga.ca/portal/services/maps>

In both the 1944 and 1954 airphotos, the east side of the Credit River is farmland and the Village hugs the west bank (Figures 7 & 8). The subject property is in red



Figure 9 - subject property & environs 1966

<http://www.mississauga.ca/portal/services/maps>

By 1966 Wyndham Street had been developed and a building north and west of the subject property had been erected (Figure 9). Nearly 20 years later, there was little obvious change or growth in the immediate environs or on the east side of the river except for the construction of an apartment building on the west side of Wyndham Street (Figure 10).



Figure 10 - subject property & environs 1985

<http://www.mississauga.ca/portal/services/maps>



Figure 11 - subject property & environs 1989

<http://www.mississauga.ca/portal/services/maps>

The change in the landscape on the east side of the river is dramatic (Figure 11). Little has changed in the immediate environs of the subject property. The 5 houses fronting on Main Street are extant in this 2007 photograph. Four houses are now found on Wyndham Street (Figure 12).

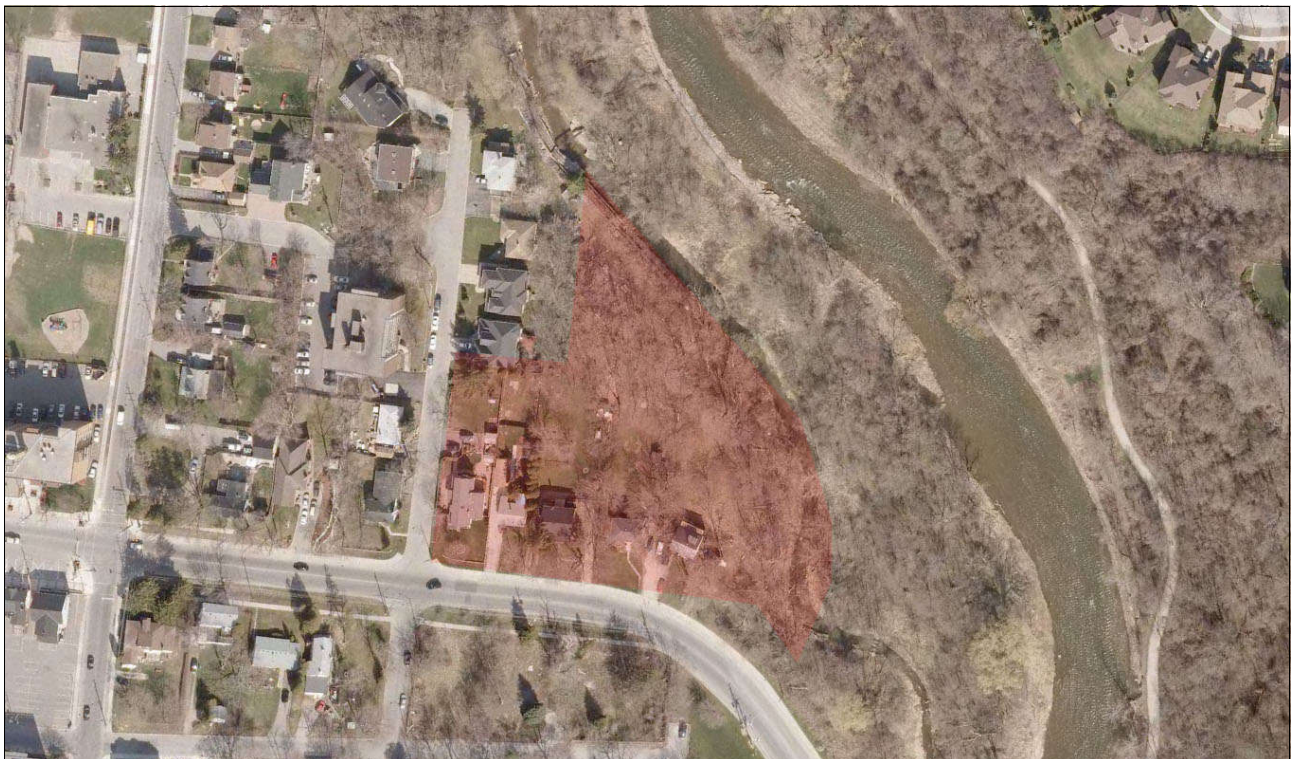


Figure 12 - subject property & environs 2007

<http://www.mississauga.ca/portal/services/maps>



Figure 13 - subject property & environs 2008

<http://www.mississauga.ca/portal/services/maps>

One year later (2008), there were 2 houses remaining on the Main Street frontage (Figure 13). By 2009, only one house remained, 36 Main Street (Figure 14).

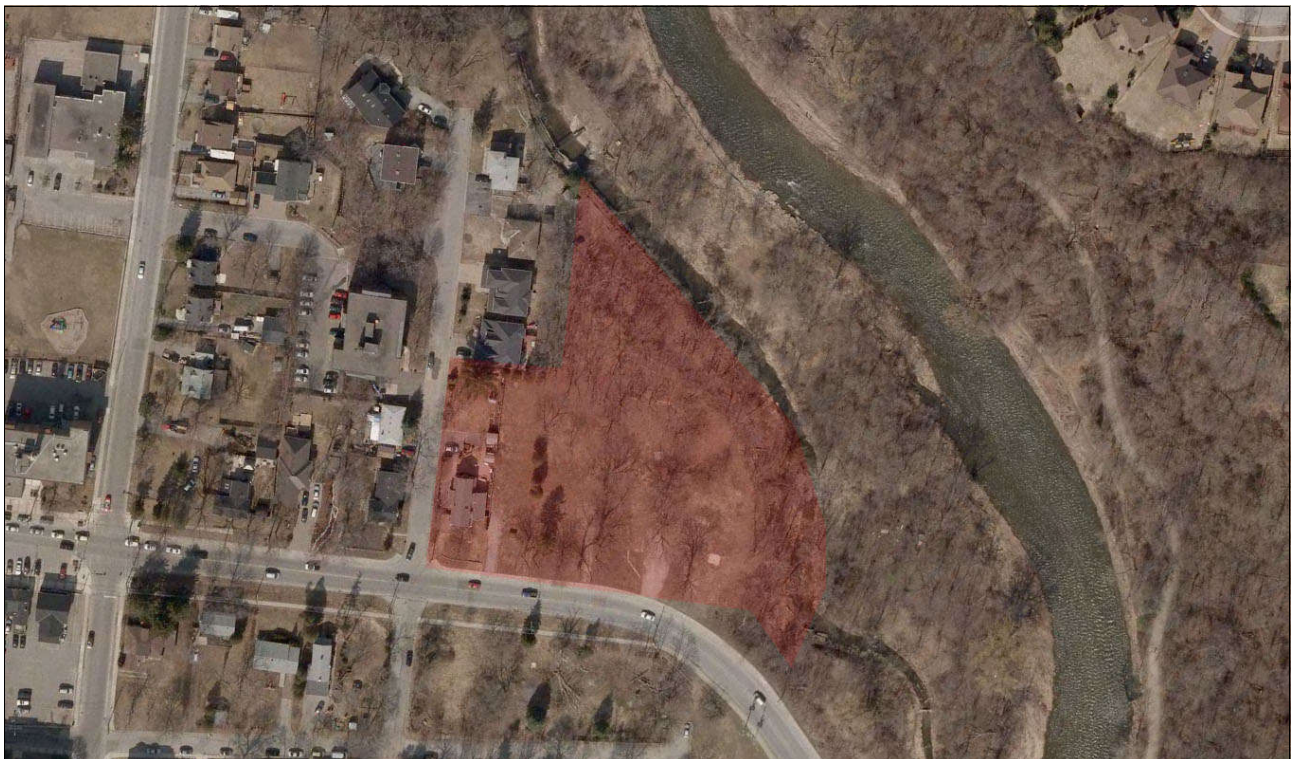


Figure 14 - subject property & environs 2009

<http://www.mississauga.ca/portal/services/maps>



Figure 15 - subject property & environs 2017

<http://www.mississauga.ca/portal/services/maps>

The 2017 airphoto (Figure 15) shows the townhouse redevelopment of the south side of Main Street opposite the subject property, the first major change in the immediate environs in ten years (development of Wyndham Street north).

After Michael McConnell's bequest to Edward McConnell, the younger McConnell sold the property to Henry Rundle, a farmer, in 1887 for \$400. The property stayed in the Rundle family (see Appendix 2) through three generations when it was sold to James Hammond and Timothy and Nancy Burns in 1984. In the interim years, members of the Rundle family sold portions of the original Lot 1 (in 1947, 1952, 1965 & 1969). The Burns sold their half interest in 36 Main Street to James Hammond in 1986. The property was transferred to James and Linda Hammond in 1991. In 2008 the property was purchased by Gova Enterprises Ltd. and was transferred to the current owner in 2017 (see Appendix 1 for further ownership details).

2.2 Listing and written description of existing structures, significance and heritage attributes

The City of Mississauga's 'property Heritage Detail' provides a brief description of the properties - see Appendix 3. The City has no heritage file on the building at 36 Main Street⁶ other than the description in Appendix 3. The City file shows that demolition permits were issued for 38, 40 and 44 Main Street in 2007 and for 46 Main Street in 2008⁷ (Figure 18).

⁶ pers. com. email correspondence Paula Wubbenhorst, October 3, 2017 - "There's no historical information on file."

⁷ City of Mississauga webpage 'property information' <https://www.mississauga.ca/portal/services/property?>



Figure 16

36 Main Street - unknown date <http://www.mississauga.ca/portal/services/property?>

Figure 17

36 Main Street - unknown date <http://www.mississauga.ca/portal/services/property?>

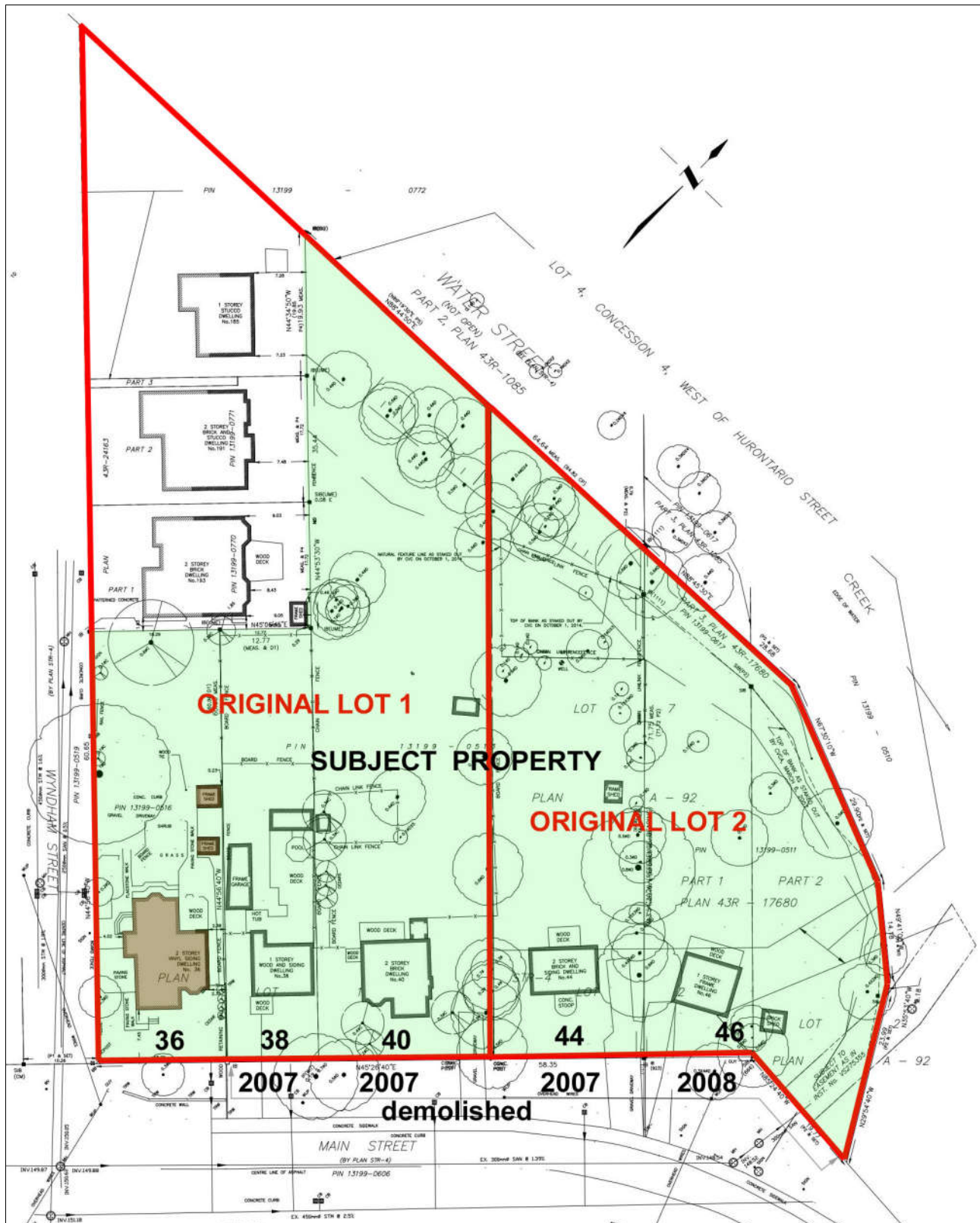


Figure 18

from Tarasick, McMillan Limited, OLS, August 23, 2005

This August 23, 2005 survey (Figure 18) shows the original Lots 1 and 2 from the 1856 “New Plan of Streetsville”, the lots at 36, 38, 40, 44 and 46 Main Street, and houses on each of the lots that comprise the subject property, all demolished save 36 Main Street. Unopened Water Street road allowance is to the east.

The date of construction of the house is unknown. As the property sold in 1932 for \$200, it is unlikely that the house was built before that date. A search of the City files for Building Permits shows only one permit being granted, and that for an addition in 1998. The first substantial mortgage recorded for the property (\$2,000) was in 1950 when the property was in the ownership of Ruby and Charles Rundle who had purchased the property in 1928. The house has been modified over time with structural features that might provide clues of its age covered. The foundation appears to be concrete. In places, the foundation has been veneered with concrete brick. Siding is vinyl; soffits and fascia are aluminum. The interior has been gutted and recently vandalized. Stud walls for the most part are of contemporary dimensional lumber (2 x 4s) indicating mid 20th century to date construction.⁸ The exception is the small front portion of the house where the studs are rough sawn pre WW II 2" x 4" (Figure 19). While an investigation of the full-height basement might have offered more information, it is inaccessible, as was the upper floor during the author's site visits.

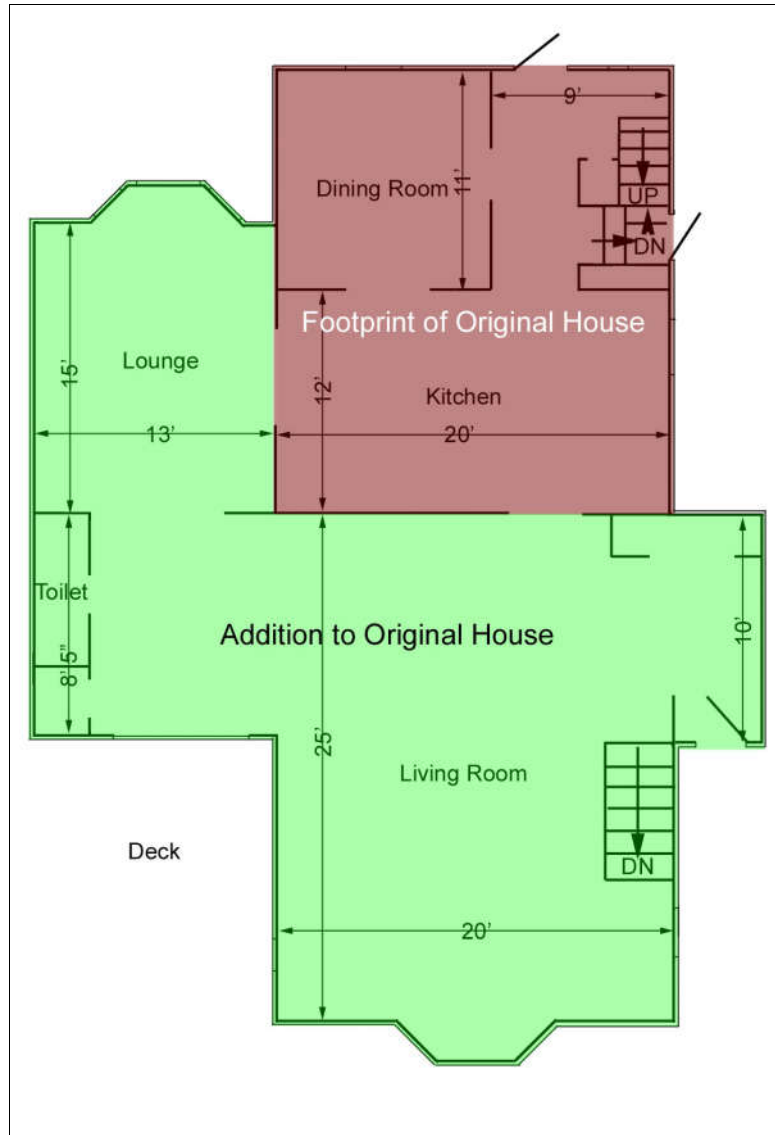


Figure 19 footprints of original house & addition(s)

The house is a 1½ storey, wood frame, gambrel roof, side hall plan, approximately 6.5m x 15m (21' x 49') with a 4m x 8m (13' x 25') addition on the east side (Figure 19). The living room addition (Figure 19) may be the same vintage. It is assumed that this is the addition for which a Building Permit was obtained in 1998. The original cladding is not known. Horizontal vinyl siding has been applied to the entire house. Aluminum soffits and eavestroughs are also extant. Insulated windows are throughout; some have faux window muntins.



Figure 20

garden sheds

Two wood garden sheds are found in the rear yard (Figure 20).

⁸ *History of Yard Lumber Size Standards*, Forest Products Laboratory, Forest Service, U. S. Department of Agriculture, September 1964



Figure 21

front facade - 36 Main Street

The front facade (Figure 21) shows a symmetrical top floor and asymmetrical ground floor with the entrance to the left side. The 1998 addition is set back on the right from the front facade. Faux window muntins are apparent.



Figure 22

rear facade - 36 Main Street

The rear facade (Figure 22) sports a bay window on the ground floor. Concrete brick has been applied to the foundation. Sills are concrete as well. The 1998 addition has sliding patio glass doors.



Figure 23

west elevation on Wyndham Street



Figure 24

east elevation showing addition & 2nd storey dormers in gambrel roof



Figure 25 bump-out on west side

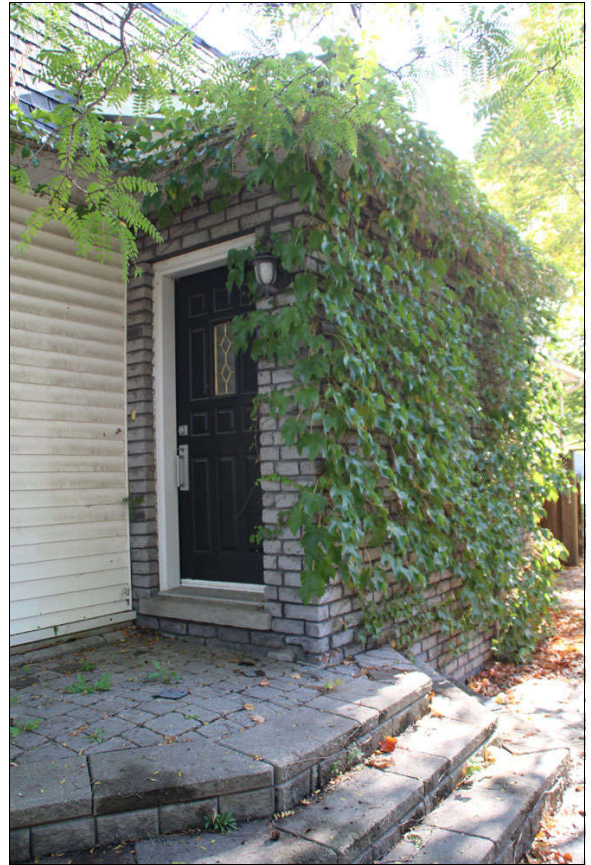


Figure 26 bump-out, entrance



Figure 27 bay window at rear - concrete brick veneer

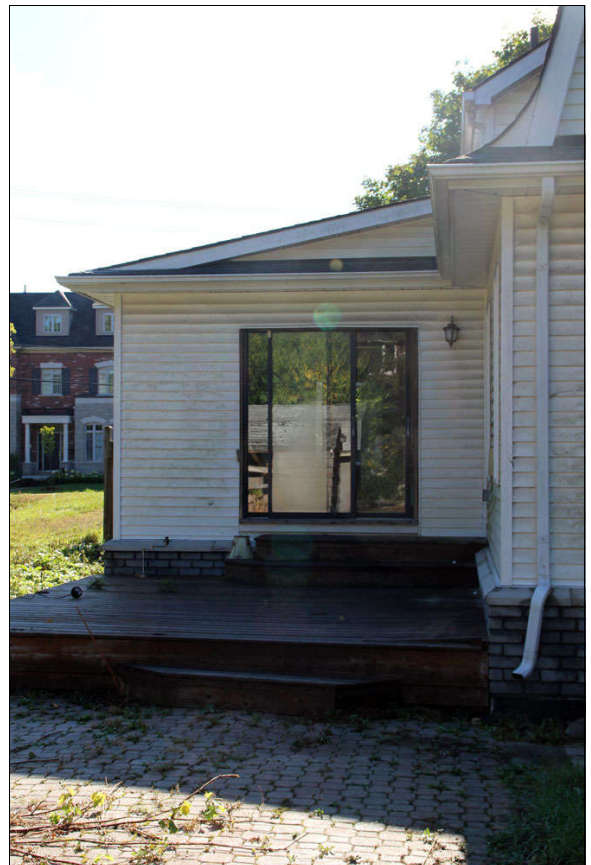


Figure 28 addition from the rear



Figure 29

concrete foundation at southeast corner



Figure 30

vinyl siding, aluminum soffits, troughs & downspouts



Figure 31 typical window - faux muntins

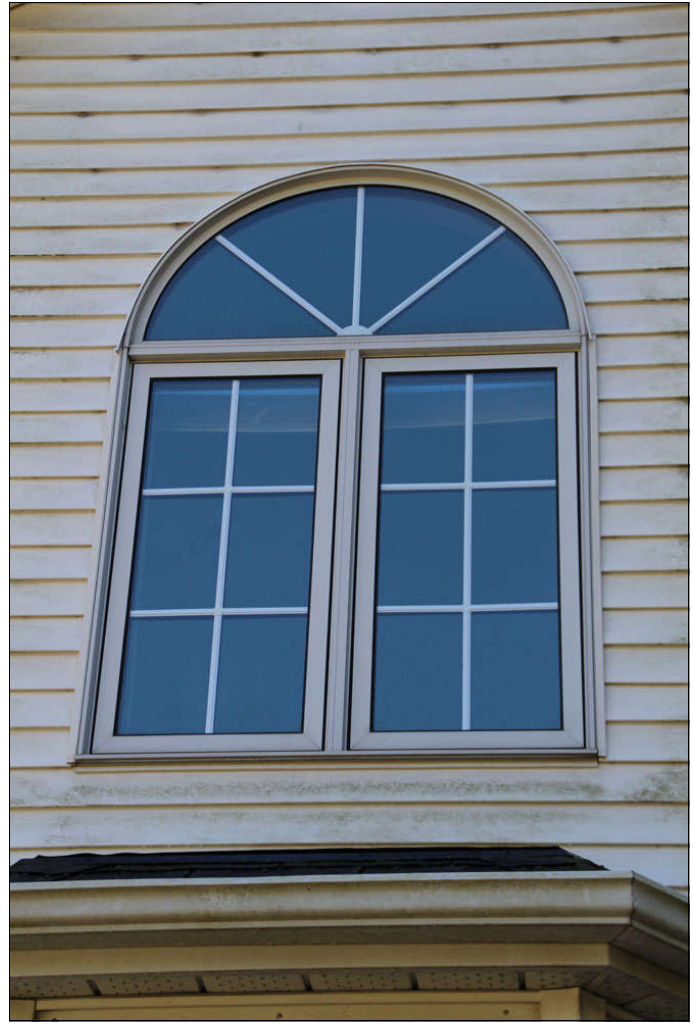


Figure 32 upper storey casement and half-round window



Figure 33 living room at rear with bay window

All windows appear to be relatively new, perhaps installed with the 1998 addition. All have faux muntins. The interior has been vandalized and appears to be of recent vintage. There is evidence that for roughly two-thirds of the house, all trim, flooring, wall materials, studs, *etc.* are *circa* late 20th century (Figures 33 - 34).



Figure 34

kitchen (original portion of house) - looking west



Figure 35

easterly addition - looking south



Figure 36

rear and easterly addition - looking east



Figure 37

stair in addition to basement



Figure 38

stair in original part
to side door & basement



Figure 39 stair to upper floor



Figure 40 rough sawn, pre WW II 2" x 4" studs



Figure 41 one of 2 original windows

Section 2 of the *Planning Act* indicates that City Council shall have regard to matters of Provincial interest such as the conservation of features of significant architectural, cultural, historical, archaeological or scientific interest. In addition, Section 3 of the *Planning Act* requires that decisions of Council shall be consistent with the *Provincial Policy Statement* (PPS-2014). Policy 2.6.1 of the PPS requires that significant built heritage resources and significant cultural heritage landscapes shall be conserved.⁹

The PPS defines “built heritage resource” as a building, structure, monument, installation or any manufactured remnant that **contributes to a property’s cultural heritage value or interest as identified by a community**, including an Aboriginal community. Built heritage resources are generally located on property that has been designated under Parts IV or V of the *Ontario Heritage Act*, or included on

⁹ *Provincial Policy Statement (PPS, 2014) Cultural Heritage and Archaeology Policies 2.6, InfoSheet #5, Heritage Impact Assessments and Conservation Plans, Winter 2006*

local, provincial and/or federal registers. The term “significant” means resources **valued for the important contribution they make to our understanding of the history of a place, an event, or a people**. “Conserved” means the **identification, protection, use and/or management of cultural heritage and archaeological resources in such a way that their heritage values, attributes and integrity are retained under the *Ontario Heritage Act***. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact assessment.

Ontario Regulation 9/06 ‘Criteria for Determining Cultural Heritage Value or Interest’¹⁰ states for a property to be considered of cultural heritage value or interest, it must meet one or more of the following criteria. The following table lists the criteria and answers the question “is the criterion met?”

1. have design value or physical value because it,	meets?
• is a rare, unique, representative or early example of a style, type, expression, material or construction method,	no
• displays a high degree of craftsmanship or artistic merit, or	no
• demonstrates a high degree of technical or scientific achievement.	no
2. have historical value or associative value because it,	
• has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,	no
• yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or	no
• demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.	no
3. have contextual value because it,	
• is important in defining, maintaining or supporting the character of an area,	no
• is physically, functionally, visually or historically linked to its surroundings, or	partially
• is a landmark.	no

The house is likely of mid-20th construction, or at least reconstruction, with late 20th century renovations and addition. It does not meet any of the criteria for significance under *Regulation 9/06*. It is not part of an early phase in Mississauga’s physical development. There is no known historical interest. It has little aesthetic/visual quality and is not a designated structure. No adjacent structures are designated; the closest designated structure is the Heron-Dandie House at 21 Main Street (Figure 35) which is in the next block.

¹⁰ *Ontario Heritage Act, Ontario Regulation 9/06* ‘Criteria for Determining Cultural Heritage Value or Interest’ January 25, 2006



Figure 35

Heron-Dandie House - 21 Main Street

2.3 Addressing the Cultural Landscape criteria¹¹

Heritage Impact Statements for properties within a Cultural Heritage Landscape must demonstrate how the proposed development will conserve the criteria that render it a cultural heritage landscape and/or feature. Each cultural heritage landscape and feature includes a checklist of criteria.

The checked criteria for the Streetsville Village Core Cultural Heritage Landscape are:

¹¹ *Cultural Landscape Inventory*, City of Mississauga, The Landplan Collaborative Ltd. January 2005
http://www5.mississauga.ca/pdfs/Cultural_Landscape_Inventory_Jan05.pdf

HISTORICAL ASSOCIATION

- ☒ Illustrates Style, Trend or Pattern
- ☒ Illustrates Important Phase in Mississauga's Social or Physical Development

BUILT ENVIRONMENT

- ☒ Aesthetic/Visual Quality
- ☒ Designated Structures

OTHER

- ☒ Historical or Archaeological Interest

To conserve the "historical associations", "aesthetic/visual qualities" and "historical interest" criteria, the proposed alteration must be consistent with the retention of the appearance of Streetsville to ensure that the character of this part of Mississauga remains intact. Streetsville retains a portfolio of heritage buildings of a consistent scale and portrays a period landscape of a small village. It is important that this appearance and character be retained.¹²

The checked criteria for the Credit River Corridor Cultural Heritage Landscape are:

LANDSCAPE ENVIRONMENT

- ☒ Scenic and Visual Quality
- ☒ Natural Environment
- ☒ Landscape Design, Type and Technological Interest

HISTORICAL ASSOCIATION

- ☒ Direct Association with Important Person or Event
- ☒ Illustrates Important Phase in Mississauga's Social or Physical Development

OTHER

- ☒ Historical or Archaeological Interest
- ☒ Outstanding Features/Interest
- ☒ Significant Ecological Interest

The river provides the residents of Mississauga with a variety of recreational and educational opportunities. The Credit River Valley is the most significant natural feature remaining in the City of Mississauga.¹³

Figure 36 is an aerial view of the context within which the subject property is located. The area is comprised of a mix of single family, apartment, town house, commercial, conservation & open space zoning.

Figures 37 - 45 are of the neighbouring properties and landscape.

¹² *Ibid*

¹³ *Ibid*

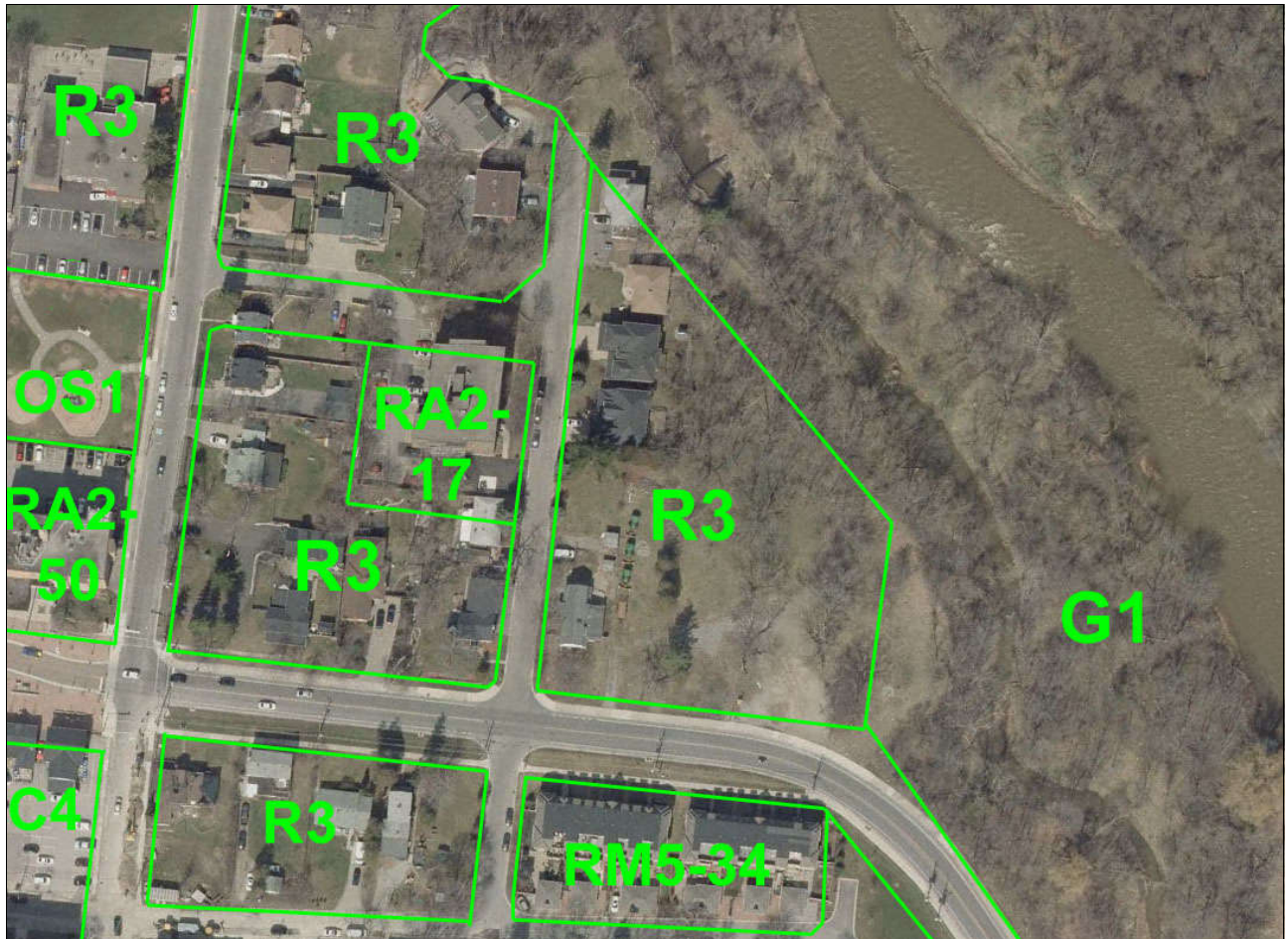


Figure 36 neighbourhood cultural landscape context & zoning - <http://www.mississauga.ca/portal/services/maps>



Figure 37 project site from west of Wyndham Street on Main Street



Figure 38

project site from east, looking west



Figure 39

project site looking east on Main Street



Figure 40

homes north on Wyndham Street



Figure 41

194 Wyndham Street - across from 36 Main Street



Figure 42

townhomes on Main Street opposite project site



Figure 43

27 Main Street



Figure 44

looking east on Main Street from project site



Figure 45

looking west on Main Street from project site

The subject property Main Street streetscape is illustrated in Figure 46.



Figure 46

32 to 46 Main Street

2.4 The proposed development

The development proposal for this property is illustrated in Figure 47.



Figure 47

proposed streetscape - 31 to 46 Main Street

A proposed Site Plan is illustrated in Figure 48.



Figure 48

Proposed Site Plan - *flanagan beresford & patteson architects*, 05 August 2017

Nineteen, 3-storey, freehold townhomes fronting on Wyndham Street, Main Street, and an internal street are proposed to replace the existing dwelling. Site statistics are found in Figure 49. The proposed development complements the relatively recent townhome development across Main Street in scale, materials and massing.

Figure 50 illustrates typical elevations of the townhome blocks.

SITE STATISTICS – COMMON ELEMENT TOWNHOUSES			
SITE AREA	–	8144.35 m ² / 2.012 Ac/ 0.814 Ha	
NATURAL FEATURE & 10m BUFFER AREA	–	2812.98 m ² / 0.695 Ac/ 0.281 Ha	
FREEHOLD	–	1229.32 m ²	
NET SITE AREA	–	4102.05 m ² / 1.014 Ac/ 0.4102 Ha	
BUILDING COVERAGE (including Porch)		1521.23m ² 37.09%	
LANDSCAPED OPEN SPACE		1370.67 m ² 33.41%	
ROADS, DRIVEWAYS, PARKING (including Sidewalks)		1210.15 m ² 29.50%	
PARKING		REQUIRED	PROVIDED
REQUIRED PARKING @ 2 SPACES/UNIT 19 units X 2/unit = 38		38	
RESIDENT PARKING PROVIDED:			
1 per unit on driveway for units 1–25			19
1 per unit in attached garage for units 1–25			19
TOTAL RESIDENT PARKING PROVIDED			38
VISITOR PARKING			
VISITOR PARKING REQUIRED 19 units X 0.25/unit = 4.75		5	
VISITOR PARKING PROVIDED (including one Type A accessible parking)			5
TOTAL PARKING REQUIRED		43	
TOTAL PARKING PROVIDED			43
DENSITY		$\frac{19 \text{ UNITS}}{0.4102 \text{ Ha}} = 46.32 \text{ UpHa}$	
FLOOR SPACE INDEX (FSI)		$\frac{\text{Gross floor area}}{\text{Gross site area}} = \frac{3857.08 \text{ m}^2}{4102.05 \text{ m}^2} = 0.940$	
HEIGHT		3 STOREY	
ROAD WIDTH (including curb)		7.4m	
CL ROAD RADIUS		12.0m	

SITE STATISTICS – FREEHOLD TOWNHOUSES			
TOTAL SITE AREA	–	1229.32 sq.m/ 0.1229 Ha /0.3038 Ac	
BUILDING COVERAGE		505.63 sq.m. 41.13%	
LANDSCAPED OPEN SPACE		628.14 sq.m. 51.10%	
D/WAYS		95.55 sq.m. 7.77%	
PARKING			
1 in attached garage, 1 on dway for 7 units		14	
TOTAL		14	
DENSITY		$\frac{14 \text{ UNITS}}{0.1229 \text{ HA}} = 113.91 \text{ UpHA}$	
FLOOR SPACE INDEX (FSI)		$\frac{\text{Gross floor area}}{\text{Gross site area}} = \frac{1245.21}{1229.32} = 1.01$	

Figure 49 proposed development site statistics
flanagan beresford & patteson architects - 05 August 2017



Figure 50

Wyndham and Main Street typical blocks - *flanagan beresford & patteson architects* June 2017

With respect to the Streetsville Village Core and Credit River Corridor Landscapes, the potential impacts and an assessment of the proposed site alteration follows.

<i>Potential Impact</i>	<i>Assessment</i>
<ul style="list-style-type: none"> • Destruction of any, or part of any, significant heritage attributes or features 	No significant heritage attributes or features.
<ul style="list-style-type: none"> • Removal of natural heritage features, including trees 	No significant natural heritage features - many trees are invasive weed species, Manitoba Maple, Norway Maple, and Black Locust.
<ul style="list-style-type: none"> • Alteration that is not sympathetic, or is incompatible, with the historic fabric and appearance 	Property has been vacant since 2008 except for one building. Neighbourhood is in transition from older single family to new single family and townhomes.
<ul style="list-style-type: none"> • Shadows created that alter the appearance of a heritage attribute or change the viability of an associated natural feature, or plantings, such as a garden 	Not applicable.
<ul style="list-style-type: none"> • Isolation of a heritage attribute from its surrounding environment, context or a significant relationship 	Not applicable.
<ul style="list-style-type: none"> • Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features 	No direct or indirect obstruction of significant views or vistas - no negative impact.
<ul style="list-style-type: none"> • A change in land use where the change in use negates the property's cultural heritage value 	No land use change.
<ul style="list-style-type: none"> • Land disturbances such as change in grade that alter soils, and drainage patterns that adversely affect cultural heritage resources 	Not applicable.

The impact of the proposed development/site alteration is a change in the view on Main Street, from a vacant lot and one vacant 1½ storey single family storey home to nineteen three storey townhomes. The architectural style of the new proposed built form reflects the values of the Streetsville Village Core Cultural Heritage Landscape and its characterizations that make up that cultural landscape in a similar vein as another recent new built-form example across Main Street. (Figures 42, 51 & 52).



Figure 51 townhomes across Main Street



Figure 52 townhomes across Main Street

With respect to the *Design Guidelines, Historic Streetsville*¹⁴ the subject property is in the “Residential Character” area of Streetsville (Figure 53). The general heritage guidelines apply to this area as well as the “new construction” item for the “Residential Character Areas”.

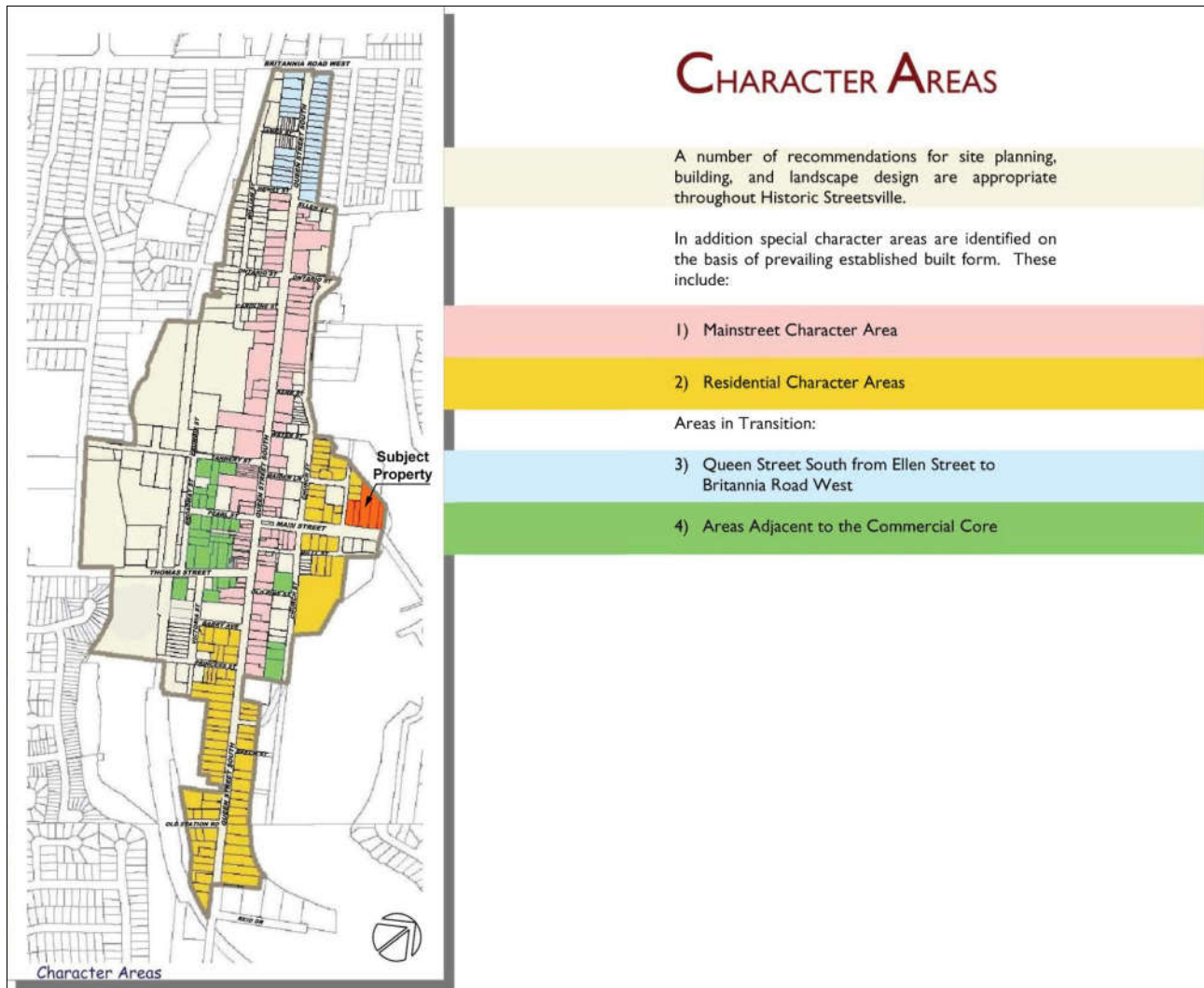


Figure 53

Character Areas from: *Design Guidelines, Historic Streetsville*, July 2011

Applicable general guidelines are:

- *Buildings and additions should be designed to reflect the nearby scale, character, and massing of construction with particular attention to detailing, trim, materials, colours, proportions, and the orderly arrangement of windows, dormers, and roof forms.*

The proposed buildings (Figures 47 & 53) reflect the scale of the newer buildings across Main Street, but not the 19th century immediately adjacent single family homes or the mix of new single family homes and 7 storey apartment building.

- *Designs rich in architectural detail, and which respect the rhythm and pattern of surrounding buildings through the alignment of windows, doors, cornices, and fascias, are encouraged.*

¹⁴ *Design Guidelines, Historic Streetsville*, Planning and Building Department, Development and Design, City of Mississauga, July 2011

The design of the proposed residences is rich in architectural detail with both symmetrical and asymmetrical facades aligning one with the other.

- *Predominantly vertical proportions are preferred in most cases.*
Vertical proportions are used throughout.
- *Window styles should be consistent with the architectural period of the building.*
Complies
- *Broad expanses of glass should be partitioned to create smaller rectangular units and vertical proportions.*
Complies
- *Building setbacks are determined through the requirements of Mississauga Zoning By-law #0225-2007 and on the basis of neighbourhood context. The precedents established by surrounding development should be used to establish appropriate setbacks so that development reinforces the existing scale and character of the community.*
Building setbacks are consistent with the zoning by-law and the neighbourhood.

Applicable “Residential Character Area” new construction guidelines are:

- *The scale, character, and nature of building in the surrounding neighbourhood, including building height, setbacks, roof forms, the number of bays, the predominance of porches, the placement of garages and openings, and building materials should be considered precedents for new building design, and the design of additions and secondary structures.*
The area of the subject property, and specifically Main and Wyndham Streets, is a *potpourri* of 19th and 20th century houses, town homes and a high rise apartment building. There is no consistency of scale or character; nonetheless, the proposed townhomes relate, especially to the development across Main Street.
- *New construction should not be made to appear "traditional" through the application of inappropriate architectural elements and details, but should instead reflect the materials, scale, rhythm, and proportions of nearby dwellings.*
Complies
- *Front doors should always face the street for purposes of aesthetics, safety, surveillance, and crime prevention.*
Complies

2.5 Assessment of alternative development options and mitigation measures

The Heritage Impact Assessment is to assess alternative development options and mitigation measures in order to avoid or limit the negative impact on the cultural heritage resources. Methods of minimizing or avoiding negative impact on cultural heritage resources, noted by the Ministry of Culture, include but are not limited to the following:

- *Alternative development approaches*
Alternative development approaches have not, to our knowledge, been proposed. It would appear that, to comply with the R3 zoning regulations, alternatives would consist of detached dwellings.

- *Isolating development and site alteration from the significant built and natural heritage features and vistas*
There are no significant built heritage features on site or adjacent. Natural heritage features and vistas are preserved and available from the proposed development and to the public.
- *Design guidelines that harmonize mass, setback, setting and materials*
Massing, setback and materials comply with the urban design guidelines and the neighbourhood.
- *Limiting height and density*
Height and density comply with the neighbourhood.
- *Allowing only compatible infill and additions* - infill is compatible (see above).
- *Reversible alterations* - not applicable.

2.6 Conservation - principles and mitigation

The City's terms of reference require the following with respect to this summary: "*A summary of conservation principles and how they will be used must be included. The conservation principles may be found in publications such as: Parks Canada – Standards and Guidelines for the Conservation of Historic Places in Canada; Eight Guiding Principles in the Conservation of Historic Properties, Ontario Ministry of Culture. (Both publications are available online.)*"¹⁵

The historic place is not considered significant. The proposal is to demolish the property, rendering the standards, guidelines and guiding principles not applicable.

Pertinent cultural heritage policies of the City of Mississauga's *Official Plan* (October 14, 2015), 7 - Complete Communities section include:

7.4.1.10 *Applications for development involving cultural heritage resources will be required to include a Heritage Impact Assessment prepared to the satisfaction of the City and other appropriate authorities having jurisdiction.*

The purpose of this HIA is to satisfy this policy.

7.4.2.2 *Prior to the demolition or alteration of a cultural heritage resource, documentation will be required of the property to the satisfaction of the City, and any appropriate advisory committee. This documentation may be in the form of a Heritage Impact Assessment.*

The purpose of this HIA is to satisfy this policy.

2.7 Proposed demolition / alterations explained

No loss of a significant cultural heritage resource will result from the demolition. The impact on the streetscape of the proposal is expected to be minimal, being compatible with the existing streetscape and eclectic immediate environs.

2.8 Alternatives for salvage mitigation

There appears to be no useful original fabric on either the interior or the exterior of this building. Material

¹⁵ City of Mississauga Heritage Impact Assessment Terms of Reference, 2017

salvage should be conducted with demolition.

2.9 Qualifications of the author completing the Heritage Impact Assessment

See Appendix 6.

3.0 SUMMARY STATEMENT and CONSERVATION RECOMMENDATIONS

- The cultural heritage resource is not significant.
- No negative impact from the proposed development is expected.

4.0 MANDATORY RECOMMENDATION

The terms of reference require the consultant to write a recommendation as to whether the subject property is worthy of heritage designation in accordance with the heritage designation criteria per Regulation 9/06, *Ontario Heritage Act*. The following questions must be answered in the final recommendation of the report:

“1. Does the property meet the criteria for heritage designation under the Ontario Regulation 9/06, Ontario Heritage Act?”

It is the opinion of the consultant that the property at 36 Main Street does not meet the criteria for Part IV heritage designation.

“2. If the subject property does not meet the criteria for heritage designation then it must be clearly stated as to why it does not.”

The potential built heritage resource and potentially significant heritage resource on this property is the c. mid-century, much modified house. The house is listed on the City’s *Heritage Register* because it is in the historic Streetsville Village Core and adjacent to the Credit River Corridor cultural landscapes. The properties that comprise the application once contained a number of homes, now demolished. The extant house is now vacant. The house has been “updated” over time with horizontal vinyl siding, aluminum soffits and fascia, an addition that is 2/3 the size of the house, replacement/new windows and doors, gutted and modernized interior, *etc.* The house **does not have design value or physical value**. It is not a representative or early example of a style, type, expression, material or construction method; it does not display a high degree of style and craftsmanship or artistic merit, nor does it demonstrate a high degree of technical or scientific achievement. The property **does not have historical value or associative value** as it does not have direct associations with a theme, event, belief, person, activity, organization or institution that is significant to the community. It does not yield nor have the potential to yield, information that would contribute to an understanding of the community or culture, nor does it demonstrate or reflect the work or ideas of an architect, artist, builder, designer or theorist who is significant to the community. Its potential **contextual value** is much diminished by the many changes to the immediate neighbourhood including a high rise apartment building, the addition of modern suburban bungalows on the side street and townhomes across Main Street.

“3. Regardless of the failure to meet criteria for heritage designation, does the property warrant conservation as per the definition in the Provincial Policy Statement.”

The property is not considered significant; it is not worthy of conservation in the consultant’s opinion.

This Heritage Impact Assessment is respectfully submitted by:

CHC Limited

A handwritten signature in black ink, appearing to read "Owen R. Scott", with a stylized flourish at the end.

per: Owen R. Scott, OALA, FCSLA, CAHP

REFERENCES

Bristow, Fitzgerald & Spencer, *A New Plan of Streetsville, 1856* [map]. Library and Archives Canada.

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City of Mississauga Heritage Impact Assessment Terms of Reference, February 2017

City of Mississauga Cultural Landscape Heritage Impact Assessment Terms of Reference 2017

City of Mississauga heritage files - <http://www.mississauga.ca/portal/services/property>

City of Mississauga maps - <http://www.mississauga.ca/portal/services/maps>

City of Mississauga Heritage Mississauga website - <http://www.heritagemississauga.com/history.htm>

History of Yard Lumber Size Standards, Forest Products Laboratory, Forest Service, U. S. Department of Agriculture, September 1964

Landplan Collaborative Ltd. (The) *Cultural Landscape Inventory, City of Mississauga*, January, 2005

Library and Archives Canada, 1881 Census, <http://www.bac-lac.gc.ca/eng/census/1881/Pages/about-census.aspx>

Ontario Regulation 9/06 'Criteria for Determining Cultural Heritage Value or Interest' January 25, 2006

Ontario Ministry of Tourism, Culture and Sport website

http://www.mtc.gov.on.ca/en/publications/InfoSheet_8%20Guiding_Principles.pdf

Mississauga Library System Historic Images Gallery

<http://www.mississauga.ca/portal/residents/streetsvillegallery>

Parks Canada website www.parkscanada.gc.ca

Province of Ontario *Ontario Heritage Act, R.S.O. 1990, c. O.18*

Province of Ontario *Provincial Policy Statement (PPS, 2014)* Cultural Heritage and Archaeology Policies 2.6

Province of Ontario *InfoSheet #5, Heritage Impact Assessments and Conservation Plans*, Winter 2006

Tremaine, George R. *Tremaine's Map of the County of Peel, Canada West*. Toronto, lithographed by John Ellis for G. R. and G. M. Tremaine. 1859.

Appendix 1

Chain of Title - PINS 13122-0004 & 13122-0005 - 36 Main Street, Mississauga

no.	instrument	instrument date	registered date	Lot	from	to	sale price
	Plan STR4	15 July 1856					
356	Will	15 October 1881	19 October 1881	1	Michael McDonnell, tailor	Edward McDonnell, gardener	
522	Bargain & Sale	15 October 1887	19 October 1887	1	Edward McDonnell	Henry Rundle, farmer	\$400
534	Mortgage	15 October 1887	22 October 1887	1	Rebecca Oliver	Henry Rundle	
535	D. M.	11 October 1890	20 October 1890	1	Henry Rundle	Rebecca Oliver	
661	Will	25 March 1889	24 Sept 1891	1	Henry Rundle	Elizabeth Rundle <i>et. al.</i>	
1403	Grant	26 August 1920	27 August 1920	1	Elizabeth & James Rundle	Phillip Rundle	\$200
1694	Grant	17 October 1928	18 October 1928	1	Phillip Rundle	Charles P. & Ruby Rundle	\$1
1808	Mortgage	3 Sept 1932	30 August 1932	1	Phillip Rundle <i>et. ux.</i>	Mabel Graydon	\$900
2226	D.M.	8 Sept 1945	10 Sep 1945	1	Mabel Graydon	Phillip Rundle	
2267	Notice	15 March 1946	26 March 1946	1	Old Age Pension Commission	Phillip Rundle	
2376	Grant	6 June 1947	19 June 1947	part Lot 1*	Phillip Rundle <i>et. ux.</i>	Mildred L. & John L. McLintock	\$300
2828	Discharge Notice	4 October 1951	24 October 1951	1	Old Age Pension Commission		
2950	Grant	26 April 1952	7 July 1952	part Lot 1*	Phillip Rundle <i>et. ux.</i>	Maria & Corrado Zurina	\$2,200
673407	Grant		24 February 1984	1	Charles P. Rundle	James Hammond, Timothy & Nancy Burns	
749816	Grant		28 April 1986	1	Timothy & Nancy Burns	James Hammond	
749817	Mortgage		24 April 1986	1	James Hammond	Scotia Mortgage Corp	\$64,000
925556	Mortgage		24 January 1990	1	James Hammond	Bank of Nova Scotia	\$75,000
970163	Grant		14 March 1991	1	James Hammond	James & Linda Hammond	
970164	Mortgage		15 May 1991	1	James & Linda Hammond	Bank of Nova Scotia	\$120,000

Appendix 1

Chain of Title - PINS 13122-0004 & 13122-0005 - 36 Main Street, Mississauga

no.	instrument	instrument date	registered date	Lot	from	to	sale price
	Permit App.		25 November 1998	1	Building Permit for addition		
PR1485699	Transfer		15 July 2008	1	James & Linda Hammond	Gova Enterprises Ltd.	
PR3147782	Transfer		16 June 2017	1 & 2	Gova Enterprises Ltd.	Current owner	\$4,900,000

* sale of parts of Lot 1

Appendix 2

Rundle family notes

*The small community that began to develop around the mill site became dubbed "Barberton". Barberton never gained village status on its own, but it grew steadily. The Barbers built 43 buildings for mill workers and their families. Some of the workers came from Scotland as skilled weavers (John Rutledge was one). Most of the workers lived in close proximity to the mill. Entire families often relied on the mill for their income and livelihood. In many cases, members of an entire family were employed in the mill. Such was the case with the Henry Rundle family. Henry was employed for a time as a servant in the Barber household, while son James was employed as a millwright, Thomas as a weaver, and daughters Delia and Anna as spinners.*¹⁶

Henry Rundle (1834 - 1890) probably came to Canada from England in the 1860s. He is not listed in the 1861 Census, but is in the 1871 and 1881 Censuses¹⁷. The 1881 Census lists his occupation as labourer. It appears that he purchased a modest acreage on Main Street in 1887, part of which is the subject property, and became a farmer after his stint as a servant in the Barber household.

His wife Elizabeth O'Neil died Feb 21, 1923, aged 90 years and is buried in the St Joseph & St Dunstan Roman Catholic Cemetery in Streetsville¹⁸.

For more information on the Rundle family, see *The Henry Rundle Family Tree: A Genealogy of Henry Rundle 1834-1890 of Streetsville, Ontario, Canada* by William Joseph Rundle, [Livermore, California, 1990, 22 pages]

(<http://pama.peelregion.ca/en/index.asp>)

from 1871 Census (Ontario)

Surname: RUNDLE
Given Name: HENRY
Occupation: SERVENT
Gender: Male
Age: 45
Place of Birth: ENGLAND
Religion: Church of England, Anglican
Ethnic Origin: ENGLISH
District Name: PEEL
District Number: 39
Sub-District Name: Toronto
Sub-District Number: A
Division Number: 2
Page Number: 44
Microfilm: C-9957
Reference: RG31 - Statistics Canada
Item Number: 170484

NAMES	SEX	AGE	Born while last census month	Country or Province of Birth	RELIGION	ORIGIN	Profession, Occupation or Trade	Married or Widowed	Going to School
Rundle Henry	M	46	-	England	Church	English	Laborer	✓	-
" Elizabeth	F	46	-	Ireland	Catholic	Irish	-	✓	-
" "	F	22	-	O	"	English	Drum-maker	✓	-
" Annie	F	21	-	O	"	English	Weaver	✓	-
" Wm Henry	M	19	-	O	"	"	-	-	-
" Mary Jane	F	18	-	O	"	"	-	-	-
" Delia	F	17	-	"	"	"	-	-	-
" James	M	11	-	"	"	"	-	-	-
" Philip	M	9	-	"	"	"	-	-	-

from 1881 Census(Ontario)

¹⁶ Heritage Mississauga webpage: <https://www.heritagemississauga.com/page/Barberton>

¹⁷ Library and Archives Canada, *Census of 1871 (Ontario)*, Item: HENRY RUNDLE & Census 1881 Province of Ontario

¹⁸ Canada Gen Webs Cemetery Project <http://geneofun.on.ca/names/photo/874326>

Appendix 3

Property Heritage Detail ¹⁹

Property Information

Roll number 21-05-120-005-11100-0000, PLAN STR 4 PT LOT 1 WYNDHAM MAIN

Property Heritage Detail

Address: 36 MAIN ST Area: STREETSVILLE
 Type: Reason: Cultural Landscape
 Style:



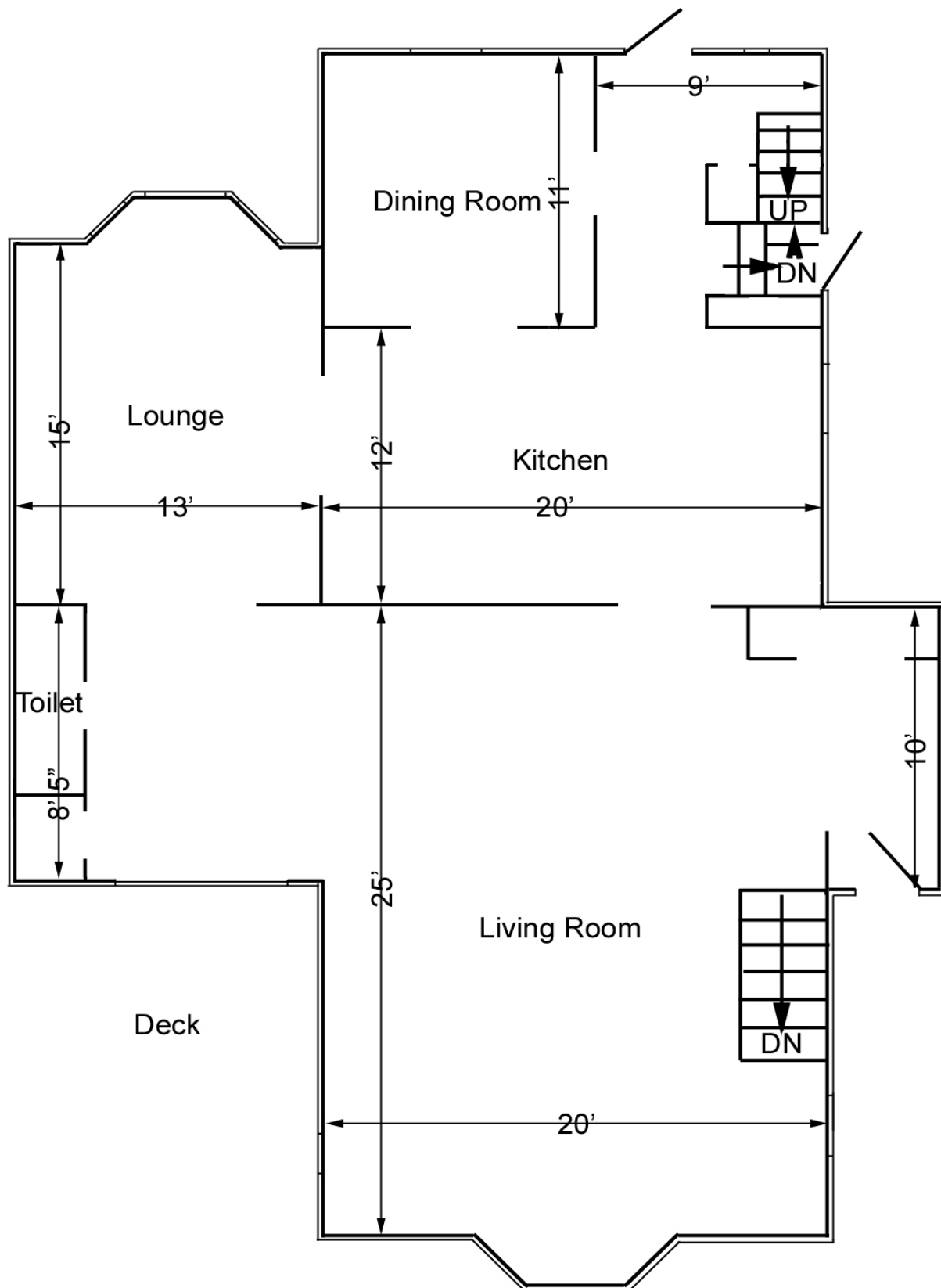
History

STREETSVILLE CORE CULTURAL LANDSCAPE: The main core of the community retains the distinct scale and character of a rural farming town. New developments continue to respect the scale of shop fronts along the main portion of Queen Street South, and the residential character of large lots with mature trees is typified in the south end transitional approach to the Village. The north end of the Village is also characterized with a residential and commercial mix found in many small towns throughout Ontario. Care should be taken to ensure that the appearance of Streetsville, including extant churches, cemeteries, public buildings and open spaces, is retained in the face of future development pressures to ensure that the character of this part of Mississauga remains intact. There are over ninety heritage properties listed, many which are designated. Streetsville is recognized as a significant cultural landscape because it retains a portfolio of heritage buildings of a consistent scale and portrays a period landscape of a small village.

¹⁹ City of Mississauga website: <http://www.mississauga.ca/portal/services/property?>

Appendix 4

Measured drawing - ground floor 36 Main Street





Cultural Landscape Inventory

Streetsville Village Core

L-HS-3

Heritage or Other Designation Numerous designated properties

Location Located on Mississauga Road west of the Credit River and south of Britannia Road

Landscape Type Historic Settlement (Village)

LANDSCAPE ENVIRONMENT

- ☐ Scenic and Visual Quality
- ☐ Natural Environment
- ☐ Horticultural Interest
- ☐ Landscape Design, Type and Technological Interest

HISTORICAL ASSOCIATION

- ☒ Illustrates Style, Trend or Pattern
- ☐ Direct Association with Important Person or Event
- ☒ Illustrates Important Phase in Mississauga's Social or Physical Development
- ☐ Illustrates Work of Important Designer

BUILT ENVIRONMENT

- ☒ Aesthetic/Visual Quality
- ☐ Consistent Early Environs (pre-World War II)
- ☐ Consistent Scale of Built Features
- ☐ Unique Architectural Features/Buildings
- ☒ Designated Structures

OTHER

- ☒ Historical or Archaeological Interest
- ☐ Outstanding Features/Interest
- ☐ Significant Ecological Interest
- ☐ Landmark Value





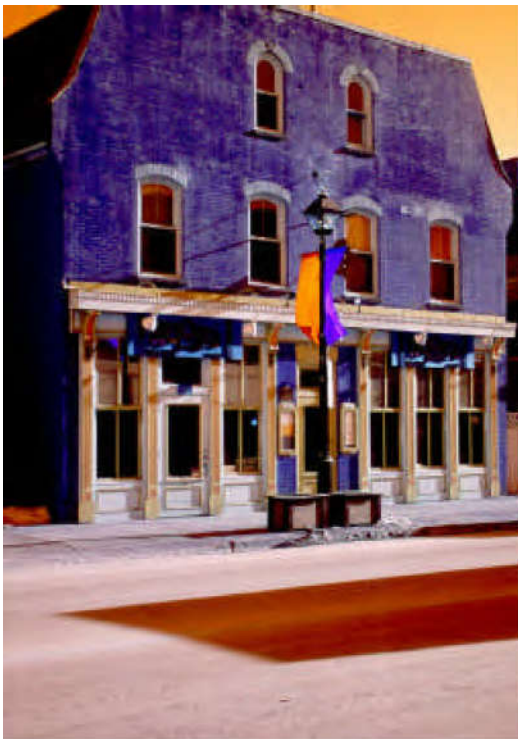
Cultural Landscape Inventory

Streetsville Village Core

L-HS-3

SITE DESCRIPTION

Despite the encirclement of Streetsville by encroaching urbanization over the past twenty years, the main core of the community retains the distinct scale and character of a rural farming town. New developments continue to respect the scale of shop fronts along the main portion of the street and local features have crept into the many forecourt walls fronting buildings to the north end of the core area. Because of its integration with the surrounding development, the core area remains a local service centre to its surrounding community - albeit to a much larger population base. Care should be taken to ensure that the appearance of Streetsville, including extant churches, cemeteries and public buildings, is retained in the face of future development pressures to ensure that the character of this part of Mississauga remains intact. There are over ninety heritage properties listed, many of which are designated. Streetsville is recognized as a significant cultural landscape because it retains a portfolio of heritage buildings of a consistent scale and portrays a period landscape of a small village.



http://www5.mississauga.ca/pdfs/Cultural_Landscape_Inventory_Jan05.pdf



Cultural Landscape Inventory

Credit River Corridor

L-NA-2

Location The River runs north south and transects the City from the Brampton border to the Lake Ontario shoreline.

Heritage or Other Designation None

Landscape Type Natural Area

LANDSCAPE ENVIRONMENT

- ☒ Scenic and Visual Quality
- ☒ Natural Environment
- ☐ Horticultural Interest
- ☒ Landscape Design, Type and Technological Interest

HISTORICAL ASSOCIATION

- ☐ Illustrates Style, Trend or Pattern
- ☒ Direct Association with Important Person or Event
- ☒ Illustrates Important Phase in Mississauga's Social or Physical Development
- ☐ Illustrates Work of Important Designer

BUILT ENVIRONMENT

- ☐ Aesthetic/Visual Quality
- ☐ Consistent Early Environs (pre-World War II)
- ☐ Consistent Scale of Built Features
- ☐ Unique Architectural Features/Buildings
- ☐ Designated Structures

OTHER

- ☒ Historical or Archaeological Interest
- ☒ Outstanding Features/Interest
- ☒ Significant Ecological Interest
- ☐ Landmark Value





Cultural Landscape Inventory

Credit River Corridor

L-NA-2

SITE DESCRIPTION

The Credit River is 58 miles long in total and has a drainage area of 328 square miles. From south of Georgetown to Erindale, the river cuts through the boulder till of the Peel Plain and in some areas exposes the underlying Paleozoic bedrock of shales and sandstones. The River flows through a wide alluvial terrace at Meadowvale where its banks are gentle and tree covered. As it approaches the old Shoreline of glacial Lake Iroquois at Erindale it cuts deeper and deeper into the Peel Plain creating steep valley walls in excess of 75 feet deep. In several locations, such as on the former Bird property north of Burnhamthorpe, intermediate benches were formed as the water levels of the glacial lakes receded. These benches and alluvial terraces provide wonderful natural and recreational settings for trails and other recreational activities. South of the Iroquois shoreline the River cuts through the sands and boulder till of the Iroquois Plain. The last mile of the river is drowned and marshy. The wave action of Lake Ontario continues in its efforts to build a bar across the mouth of the river which is periodically removed by dredging. Despite its size, the River has had significant impact on the settlement of the area. At one time, Erindale had a mill and for a short while a small hydroelectric generating station. At Streetsville, four flour mills operated some of which remain today as modern mills. Two sawmills and a carding mill were built in Meadowvale. The banks of the river continue to be developed for attractive residential neighborhoods, parks and special uses such as the University of Toronto Erindale campus. The river provides the residents of Mississauga with a variety of recreational and educational opportunities. The Credit River Valley is the most significant natural feature remaining in the City of Mississauga. (excerpts from The Physiography of Southern Ontario)



Appendix 6

Qualifications of the Author

R E S U M E

OWEN R. SCOTT, *OALA, FCSLA, CAHP*

Education:

Master of Landscape Architecture (MLA) University of Michigan, 1967

Bachelor of Science in Agriculture (Landscape Horticulture), (BSA) University of Guelph, 1965

Professional Experience:

1965 - present President, CHC Limited, Guelph, ON
 1977 - present President, The Landplan Collaborative Ltd., Guelph, ON
 1977 - 1985 Director, The Pacific Landplan Collaborative Ltd., Vancouver and Nanaimo, BC
 1975 - 1981 Editor and Publisher, *Landscape Architecture Canada*, Ariss, ON
 1969 - 1981 Associate Professor, School of Landscape Architecture, University of Guelph
 1975 - 1979 Director and Founding Principal, Ecological Services for Planning Limited, Guelph, ON
 1964 - 1969 Landscape Architect, Project Planning Associates Limited, Toronto, ON

Historical Research, Heritage Planning and Conservation Experience and Expertise

Current Professional and Professional Heritage Associations Affiliations:

Member: Alliance for Historic Landscape Preservation (AHLP) - 1978 -
 Member: Canadian Association of Heritage Professionals (CAHP) - 1987 -
 Member: Ontario Association of Landscape Architects (OALA) - 1968 - (Emeritus 2016)
 Member: Canadian Society of Landscape Architects (FCSLA) - 1969 - (Fellow 1977, Life Member 2016)

Community and Professional Society Service (Heritage):

Director: Canadian Association of Heritage Professionals (CAHP), 2002 - 2003
 Member: Advisory Board, Architectural Conservancy of Ontario, 1980 - 2002
 Member: City of Guelph Local Architectural Conservation Advisory Committee (LACAC), 1987 - 2000 (Chair 1988 - 1990)
 Member: Advisory Council, Centre for Canadian Historical Horticultural Studies, 1985 - 1988

Professional Honours and Awards (Heritage):

Merit Award	2016	Canadian Association of Heritage Professionals Awards, City of Kitchener Cultural Heritage Landscapes
National Award	2016	Canadian Society of Landscape Architects (CSLA), City of Kitchener Cultural Heritage Landscapes
Mike Wagner Award	2013	Heritage Award - Breithaupt Block, Kitchener, ON
People's Choice Award	2012	Brampton Urban Design Awards, Peel Art Gallery, Museum and Archives, Brampton, ON
Award of Excellence	2012	Brampton Urban Design Awards, Peel Art Gallery, Museum and Archives, Brampton, ON
National Award	2009	Heritage Canada Foundation National Achievement, Alton Mill, Alton, ON
Award of Merit	2009	Canadian Association of Heritage Professionals Awards, Alton Mill, Alton, ON
Award	2007	Excellence in Urban Design Awards, Heritage, Old Quebec Street, City of Guelph, ON
Award	2001	Ontario Heritage Foundation Certificate of Achievement
Award	1998	Province of Ontario, Volunteer Award (10 year award)
Award	1994	Province of Ontario, Volunteer Award (5 year award)
Regional Merit	1990	CSLA Awards, Britannia School Farm Master Plan
National Honour	1990	CSLA Awards, Confederation Boulevard, Ottawa
Citation	1989	City of Mississauga Urban Design Awards, Britannia School Farm Master Plan
Honour Award	1987	<i>Canadian Architect</i> , Langdon Hall Landscape Restoration, Cambridge, ON
Citation	1986	<i>Progressive Architecture</i> , The Ceremonial Routes (Confederation Boulevard), Ottawa,
National Citation	1985	CSLA Awards, Tipperary Creek Heritage Conservation Area Master Plan, Saskatoon, SK
National Merit	1984	CSLA Awards, St. James Park Victorian Garden, Toronto, ON
Award	1982	Ontario Ministry of Municipal Affairs Ontario Renews Awards, Millside, Guelph, ON

Appendix 6

Qualifications of the Author

Selected Heritage Publications:

- Scott, Owen R., The Southern Ontario “Grid”, *ACORN* Vol XXVI-3, Summer 2001. *The Journal of the Architectural Conservancy of Ontario*.
- Scott, Owen R. *19th Century Gardens for the 20th and 21st Centuries*. Proceedings of “Conserving Ontario’s Landscapes” conference of the ACO, (April 1997). Architectural Conservancy of Ontario Inc., Toronto, 1998.
- Scott, Owen R. *Landscapes of Memories, A Guide for Conserving Historic Cemeteries*. (19 of 30 chapters) compiled and edited by Tamara Anson-Cartright, Ontario Ministry of Citizenship, Culture and Recreation, 1997.
- Scott, Owen R. Cemeteries: A Historical Perspective, *Newsletter, The Memorial Society of Guelph*, September 1993.
- Scott, Owen R. The Sound of the Double-bladed Axe, *Guelph and its Spring Festival*. edited by Gloria Dent and Leonard Conolly, The Edward Johnson Music Foundation, Guelph, 1992. 2 pp.
- Scott, Owen R. Woolwich Street Corridor, Guelph, *ACORN* Vol XVI-2, Fall 1991. Newsletter of the Architectural Conservancy of Ontario Inc. (ACO)
- Scott, Owen R. guest editor, *ACORN*, Vol. XIV-2, Summer 1989. Cultural Landscape Issue, Newsletter of the ACO.
- Scott, Owen R. Heritage Conservation Education, Heritage Landscape Conservation, *Momentum 1989*, Icomos Canada, Ottawa, p.31.
- Scott, Owen R. Cultivars, pavers and the historic landscape, *Historic Sites Supplies Handbook*. Ontario Museum Association, Toronto, 1989. 9 pp.
- Scott, Owen R. Landscape preservation - What is it? *Newsletter*, American Society of Landscape Architects - Ontario Chapter, vol. 4 no.3, 1987.
- Scott, Owen R. Tipperary Creek Conservation Area, Wanuskewin Heritage Park. *Landscape Architectural Review*, May 1986. pp. 5-9.
- Scott, Owen R. Victorian Landscape Gardening. Ontario Bicentennial History Conference, McMaster University, 1984.
- Scott, Owen R. Canada West Landscapes. *Fifth Annual Proceedings Niagara Peninsula History Conference (1983)*. 1983. 22 pp.
- Scott, Owen R. Utilizing History to Establish Cultural and Physical Identity in the Rural Landscape. *Landscape Planning*, Elsevier Scientific Press, Amsterdam, 1979. Vol. 6, No. 2, pp. 179-203.
- Scott, Owen R. Changing Rural Landscape in Southern Ontario. *Third Annual Proceedings Agricultural History of Ontario Seminar (1978)*. June 1979. 20 pp.
- Scott, Owen R., P. Grimwood, M. Watson. George Laing - Landscape Gardener, Hamilton, Canada West 1808-1871. *Bulletin, The Association for Preservation Technology*, Vol. IX, No. 3, 1977, 13 pp. (also published in *Landscape Architecture Canada*, Vol. 4, No. 1, 1978).
- Scott, Owen R. The Evaluation of the Upper Canadian Landscape. Department of Landscape Architecture, University of Manitoba. 1978. (Colour videotape).

Following is a **representative listing of some of the heritage consultations undertaken by Owen R. Scott** in his capacity as a principal of The Landplan Collaborative Ltd., and principal of CHC Limited.

Heritage Master Plans and Landscape Plans

- Alton Mill Landscape, Caledon, ON
- Black Creek Pioneer Village Master Plan, Toronto, ON
- Britannia School Farm Master Plan, Peel Board of Education/Mississauga, ON
- Confederation Boulevard (Sussex Drive) Urban Design, Site Plans, NCC/Ottawa, ON
- Doon Heritage Crossroads Master Plan and Site Plans, Region of Waterloo/Kitchener, ON
- Downtown Guelph Private Realm Improvements Manual, City of Guelph, ON
- Downtown Guelph Public Realm Plan, City of Guelph, ON
- Dundurn Castle Landscape Restoration Feasibility Study, City of Hamilton, ON
- Elam Martin Heritage Farmstead Master Plan, City of Waterloo, ON
- Exhibition Park Master Plan, City of Guelph, ON
- George Brown House Landscape Restoration, Toronto, ON
- *Grand River Corridor Conservation Plan*, GRCA/Regional Municipality of Waterloo, ON
- Greenwood Cemetery Master Plan, Owen Sound, ON
- Hamilton Unified Family Courthouse Landscape Restoration Plan, Hamilton, ON
- John Galt Park, City of Guelph, ON
- Judy LaMarsh Memorial Park Master Plan, NCC/Ottawa, ON
- Langdon Hall Gardens Restoration and Site Plans, Cambridge, ON

Appendix 6

Qualifications of the Author

- London Psychiatric Hospital Cultural Heritage Stewardship Plan, London, ON
- McKay / Varley House Landscape Restoration Plan, Markham (Unionville), ON
- Museum of Natural Science/Magnet School 59/ Landscape Restoration and Site Plans, City of Buffalo, NY
- Muskoka Pioneer Village Master Plan, MNR/Huntsville, ON
- Peel Heritage Centre Adaptive Re-use, Landscape Design, Brampton, ON
- Phyllis Rawlinson Park Master Plan (winning design competition), Town of Richmond Hill, ON
- Prime Ministerial Precinct and Rideau Hall Master Plan, NCC/Ottawa, ON
- Queen/Picton Streets Streetscape Plans, Town of Niagara-on-the-Lake, ON
- Regional Heritage Centre Feasibility Study and Site Selection, Region of Waterloo, ON
- Rockway Gardens Master Plan, Kitchener Horticultural Society/City of Kitchener, ON
- St. George's Square, City of Guelph, ON
- St. James Cemetery Master Plan, Toronto, ON
- St. James Park Victorian Garden, City of Toronto, ON
- Tipperary Creek (Wanuskewin) Heritage Conservation Area Master Plan, Meewasin Valley Authority, Saskatoon, SK
- Whitehern Landscape Restoration Plan, Hamilton, ON
- Woodside National Historic Park Landscape Restoration, Parks Canada/Kitchener, ON

Cultural Heritage Evaluation Reports (CHER), Cultural Heritage Inventories and Cultural Heritage Landscape Evaluations

- Adams Bridge (Structure S20) Cultural Heritage Evaluation Report, Southgate Twp., ON
- Belfountain Area Heritage Inventory for Environmental Assessment, Peel Region, ON
- Bridge #9-WG Cultural Heritage Evaluation Report, Township of Centre Wellington, ON
- Bridge #20 Cultural Heritage Evaluation Report, Blandford-Blenheim Township, ON
- Bridge #25 Cultural Heritage Evaluation Report, Blandford-Blenheim Township, ON
- Chappell Estate / Riverside / Mississauga Public Garden Heritage Inventory, Mississauga, ON
- 8895 County Road 124 Cultural Heritage Opinion Report, Erin (Ospringe), ON
- Cruickston Park Farm & Cruickston Hall - Cultural Heritage Resources Study, Cambridge, ON
- Doon Valley Golf Course - Cultural Heritage and Archaeological Resources Inventory, Kitchener/Cambridge, ON
- Government of Ontario Light Rail Transit (GO-ALRT) Route Selection, Cultural and Natural Resources Inventory for Environmental Assessment, Hamilton/Burlington, ON
- Hancock Woodlands Cultural Heritage Assessment, City of Mississauga, ON
- Hespeler West Secondary Plan - Heritage Resources Assessment, City of Cambridge, ON
- Highway 400 to 404 Link Cultural Heritage Inventory for Environmental Assessment, Bradford, ON
- Highway 401 to 407 Links Cultural Heritage Inventory for Environmental Assessment, Pickering/Ajax/Whitby/Bowmanville, ON
- Holland Mills Road Bridge Cultural Heritage Evaluation Report, Wilmot Township, ON
- Homer Watson House Cultural Heritage Evaluation Report, Kitchener, ON
- Irvine Street (Watt) Bridge Cultural Heritage Evaluation Report, Township of Centre Wellington, ON
- Lakewood Golf Course Cultural Landscape Assessment, Tecumseh, ON
- Landfill Site Selection, Cultural Heritage Inventory for Environmental Assessment, Region of Halton, ON
- Niska Road Cultural Heritage Landscape Addendum, City of Guelph, ON
- 154 Ontario Street, Historical - Associative Evaluation, Guelph, ON
- 35 Sheldon Avenue North, Cultural Heritage Evaluation Report, Kitchener, ON
- Silvercreek (LaFarge Lands) Cultural Landscape Assessment, Guelph, ON
- South Kitchener Transportation Study, Heritage Resources Assessment, Region of Waterloo, ON
- 53 Surrey Street East and 41, 43, 45 Wyndham Street South Cultural Heritage Evaluation Guelph, ON
- Swift Current CPR Station Gardens condition report and feasibility study for rehabilitation/reuse, Swift Current, SK
- University of Guelph, McNaughton Farm House, Cultural Heritage Resource Assessment, Puslinch Township, ON
- University of Guelph, Trent Institute Cultural Heritage Resource Assessment, Guelph, ON
- University of Guelph, 1 and 10 Trent Lane Cultural Heritage Resource Assessments, Guelph, ON
- Uno Park Road Bridge, Cultural Heritage Evaluation Report, Harley Township, ON
- 2007 Victoria Road South Heritage Evaluation, Guelph, ON
- Waterloo Valleylands Study, Heritage and Recreational Resources mapping and policies, Region of Waterloo

Appendix 6

Qualifications of the Author

Cultural Heritage Resource Impact Assessments (CHRIA/CHIA/HIS/HIA) and Cultural Landscape Heritage Impact Statements

- Adams Bridge (Structure S20) Heritage Impact Assessment, Southgate Township, ON
- 33 Arkell Road Heritage Impact Assessment, Guelph, ON
- 86 Arthur Street, Heritage Impact Assessment, Guelph, ON
- William Barber House, 5155 Mississauga Road , Heritage Impact Assessment, Mississauga, ON
- Barra Castle Heritage Impact Assessment, Kitchener, ON
- Biltmore Hat Factory Heritage Impact Assessment, Guelph, ON
- 140 Blue Heron Ridge Heritage Impact Assessment, Cambridge, ON
- 25 Breithaupt Street Heritage Impact Assessment, Kitchener, ON
- 51 Breithaupt Street Heritage Impact Assessment, Kitchener, ON
- Bridge #20 Heritage Impact Assessment, Blandford-Blenheim Township, ON
- Bridge #25 Heritage Impact Assessment, Blandford-Blenheim Township, ON
- 215 Broadway Street Heritage Impact Statement, Mississauga, ON
- Cambridge Retirement Complex on the former Tiger Brand Lands, Heritage Impact Assessment, Cambridge, ON
- Cambridge Retirement Complex on the former Tiger Brand Lands, Heritage Impact Assessment Addendum, Cambridge, ON
- 27-31 Cambridge Street, Heritage Impact Assessment, Cambridge, ON
- 3075 Cawthra Road Heritage Impact Statement, Mississauga, ON
- 58 Church Street Heritage Impact Assessment, Churchville Heritage Conservation District, Brampton, ON
- City Centre Heritage Impact Assessment, Kitchener, ON
- 175 Cityview Drive Heritage Impact Assessment, Guelph, ON
- 12724 Coleraine Drive Cultural Heritage Impact Statement, Caledon (Bolton), ON
- 12880 Coleraine Drive Cultural Heritage Impact Statement, Caledon (Bolton), ON
- Cordingly House Heritage Impact Statement, Mississauga, ON
- 264 Crawley Road Heritage Impact Assessment (farmstead, house & barn), Guelph, ON
- 31-43 David Street (25 Joseph Street) Heritage Impact Assessment, Kitchener, ON
- 35 David Street (Phase II) Heritage Impact Assessment, Kitchener, ON
- 75 Dublin Street Heritage Impact Assessment, Guelph, ON
- 24, 26, 28 and 32 Dundas Street East Heritage Impact Statement, Mississauga, (Cooksville), ON
- 1261 Dundas Street South Heritage Impact Assessment, Cambridge, ON
- 172 - 178 Elizabeth Street Heritage Impact Assessment, Guelph, ON
- 19 Esandar Drive, Heritage Impact Assessment, Toronto, ON
- 14 Forbes Avenue Heritage Impact Assessment, Guelph, ON
- 369 Frederick Street Heritage Impact Assessment, Kitchener, ON
- 42 Front Street South Heritage Impact Assessment, Mississauga, ON
- Grey Silo Golf Course/Elam Martin Farmstead Heritage Impact Assessment, City of Waterloo, ON
- GRCA Lands, 748 Zeller Drive Heritage Impact Assessment Addendum, Kitchener, ON
- Hancock Woodlands Heritage Impact Statement, City of Mississauga, ON
- 132 Hart's Lane, Hart Farm Heritage Impact Assessment, Guelph, ON
- Holland Mills Road Bridge Heritage Impact Assessment, Wilmot Township, ON
- 9675, 9687, 9697 Keele Street Heritage Impact Assessment, City of Vaughan (Maple) ON
- 13165 Keele Street Cultural Heritage Resource Impact Assessment, King Township (King City), ON
- 151 King Street North Heritage Impact Assessment, Waterloo, ON
- Kip Co. Lands Developments Ltd. Cultural Heritage Resource Impact Assessment - Woodbridge Heritage Conservation District, City of Vaughan (Woodbridge) ON
- 20415 Leslie Street Heritage Impact Assessment, East Gwillimbury, ON
- 117 Liverpool Street Heritage Impact Assessment, Guelph, ON
- 30 - 40 Margaret Avenue Heritage Impact Assessment, Kitchener, ON
- 19 - 37 Mill Street Scoped Heritage Impact Assessment, Kitchener, ON
- 2610, 2620 and 2630 Mississauga Road, Cultural Landscape Heritage Impact Statement, Mississauga, ON
- 4067 Mississauga Road, Cultural Landscape Heritage Impact Statement, Mississauga, ON
- 1142 Mona Road, Heritage Impact Assessment, Mississauga, ON
- 1245 Mona Road, Heritage Impact Statement, Mississauga, ON
- 15 Mont Street, Heritage Impact Assessment, Guelph, ON

Appendix 6

Qualifications of the Author

- Proposed Region of Waterloo Multimodal Hub at 16 Victoria Street North, 50 & 60 Victoria Street North, and 520 & 510 King Street West, Heritage Study and Heritage Impact Assessment, Kitchener, ON
- 6671 Ninth Line Heritage Impact Statement, Cordingley House Restoration & Renovation, Mississauga, ON
- 266-280 Northumberland Street Heritage Impact Assessment, North Dumfries (Ayr), ON
- 324 Old Huron Road Heritage Impact Assessment, Kitchener, ON
- 40 Queen Street South Heritage Impact Statement, Mississauga, (Streetsville), ON
- Rockway Holdings Limited Lands north of Fairway Road Extension Heritage Impact Assessment, Kitchener, ON
- 259 St. Andrew Street East Cultural Heritage Assessment, Fergus, ON
- 35 Sheldon Avenue, Heritage Impact Assessment, Kitchener, ON
- 2300 Speakman Drive Heritage Impact Assessment, Mississauga, ON
- 10431 The Gore Road Heritage Impact Assessment, Brampton, ON
- Thorny-Brae Heritage Impact Statement, Mississauga, ON
- 7 Town Crier Lane, Heritage Impact Assessment, Markham, ON
- University of Guelph, 3 - 7 Gordon Street Houses, Heritage Impact Assessment, Guelph, ON
- University of Guelph, Harrison House, Heritage Impact Assessment, Guelph, ON
- Uno Park Road Bridge, Heritage Impact Assessment, Harley Township, ON
- Victoria Park Proposed Washroom Cultural Heritage Impact Assessment, Kitchener, ON
- 927 Victoria Road South (barn) Heritage Impact Assessment, Guelph, ON
- 272-274 Victoria Street Heritage Impact Assessment, Mississauga, ON
- 26 - 32 Water Street North Heritage Impact Assessment, Cambridge (Galt), ON
- Winzen Developments Heritage Impact Assessment, Cambridge, ON
- 35 Wright Street Cultural Heritage Resource Impact Assessment, Richmond Hill, ON
- 1123 York Road Heritage Impact Assessment, Guelph, ON
- 14288 Yonge Street, Heritage Impact Assessment, Aurora, ON

Heritage Conservation Plans

- William Barber House, 5155 Mississauga Road , Heritage Conservation Plan, Mississauga, ON
- 51 Breithaupt Street Heritage Conservation Plan, Kitchener, ON
- Hamilton Psychiatric Hospital Conservation Plan, for Infrastructure Ontario, Hamilton, ON
- Harrop Barn Heritage Conservation Plan, Milton, ON
- 324 Old Huron Road Conservation Plan, Kitchener, ON
- 264 Woolwich Street Heritage Conservation Plan, Guelph, ON
- 14288 Yonge Street Heritage Conservation Plan, Aurora, ON
- 1123 York Road Heritage Conservation Plan, Guelph, ON

Heritage Conservation District Studies and Plans

- Downtown Whitby Heritage Conservation District Study and Plan, Town of Whitby, ON
- MacGregor/Albert Heritage Conservation District Study and Plan, City of Waterloo, ON
- Queen Street East Heritage Conservation District Study, Toronto, ON
- University of Toronto & Queen's Park Heritage Conservation District Study, City of Toronto, ON

Cultural Heritage Landscape Inventories/Studies

- Cultural Heritage Landscape Study, City of Kitchener, ON
- Cultural Heritage Landscape Inventory, City of Mississauga, ON

Peer Reviews

- Acton Quarry Cultural Heritage Landscape & Built Heritage Study & Assessment Peer Review, Acton, ON
- Belvedere Terrace - Peer Review, Assessment of Proposals for Heritage Property, Parry Sound, ON
- Heritage Square Heritage Impact Assessment Peer Review for Township of Centre Wellington (Fergus), ON
- Little Folks Heritage Impact Assessment Peer Review for Township of Centre Wellington (Elora), ON

Expert Witness Experience

- Oelbaum Ontario Municipal Board Hearing, Eramosa Township, ON, 1988
- Roselawn Centre Conservation Review Board Hearing, Port Colborne, ON, 1993

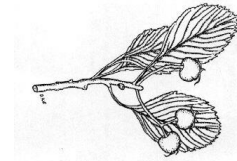
Appendix 6

Qualifications of the Author

- Halton Landfill, Joint Environmental Assessment Act and Environmental Protection Act Board Hearing, 1994
- OPA 129 Ontario Municipal Board Hearing, Richmond Hill, ON, 1996
- Diamond Property Ontario Municipal Board Hearing, Aurora, ON, 1998
- Harbour View Investments Ontario Municipal Board Hearing, Town of Caledon, ON, 1998
- Aurora South Landowners Ontario Municipal Board Hearing, Aurora, ON, 2000
- Ballycroy Golf Course Ontario Municipal Board Hearing, Palgrave, ON, 2002
- Doon Valley Golf Course Ontario Municipal Board Hearing, Cambridge, ON, 2002
- Maple Grove Community Ontario Municipal Board Hearing, North York, ON, 2002
- Maryvale Crescent Ontario Municipal Board Hearing, Richmond Hill, ON, 2003
- LaFarge Lands Ontario Municipal Board Mediation, Guelph, ON, 2007
- 255 Geddes Street, Elora, ON, heritage opinion evidence - Ontario Superior Court of Justice, 2010
- Downey Trail Ontario Municipal Board Hearing, Guelph, ON, 2010
- Wilson Farmhouse Conservation Review Board Hearing, Guelph, ON, 2014
- 85 Victoria Street, Churchville Heritage Conservation District, Ontario Municipal Board Hearing, Brampton, ON, 2016

Arborist Report for Wyndham Street & Main Street junction

Mississauga, Ontario



DAWhiteTreeCare.com

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D. Andrew White M. Sc. ISA Certified Arborist ON-0734. 78 Marcella St. Toronto, ON, M1G 1L2.

1. Introduction

The following is an arborist report for the property at Wyndham Street and Main Street junction Drive, in Mississauga Ontario. The purpose of this report was to ascertain the potential impacts of the proposed construction of a new development on the trees on the site and on adjacent properties.

2. Methods

An on-site inspection was made on October 13, 2017. The sizes of individual trees were measured as diameter at breast height (DBH), breast height being 137 cm from ground level. The locations of these trees are indicated on the modified site plan (Fig. 1). From the data collected plant Condition Rating (CR), Location Rating (LR), Species Rating (SR), and minimum Tree Protection Zones (TPZ), were estimated.

It is necessary to protect all trees designated for preservation during both demolition and construction. This tree protection can be accomplished by protecting the said trees with *tree protection barriers*.

Tree barriers for road allowance areas would be composed of a 1.2 metres (4 ft.) high orange plastic web snow fencing secured on 2"x4" wood frames. Usually, tree protection barriers, not on road allowance, are to be 1.2 metres (4 ft.) high, and composed of plywood

3. Discussion

There are plans to develop the site at Wyndham Street and Main Street junction several non-exempt trees would need to be injured or removed, in order to allow for the proposed development (Table 1, Fig. 1).

Roadside Trees:

One (1) roadside tree over 15cm DBH would be removed (Table 1, Fig. 1 Trees #4). The tree is in conflict with the proposed development.

One (1) exempt roadside tree (DBH less than 15cm) would also need to be removed. (Table 1, Fig. 1 Trees #3).

Three (3) roadside trees are to be preserved. (Table 1, Fig. 1 Trees #5a, 5b, 10).

Table #1. Road Allowance Tree Protection & Replacement Chart

Tree number (No.), species, Comments with: diameter at breast height (DBH); and Condition Rating (CR).

No.	Tree Species	DBH	Comments
#4	Manitoba Maple	78+	Remove: In conflict with the entrance to Block 3 townhouse 2 replacement trees required CR: 55%
#3	Black Locust	<15	Remove: Exempt Tree CR: 70%
#5a, 5b & 10	Green Ash Norway Maple	<15	Protect: Proposed grade changes beyond TPZ of trees. Mitigation: The tree would be protected by a web-fence barrier TPZ: 1.8 m (min)

Private Trees:

Ten (10) privately owned trees over 50cm DBH would be removed. These trees are in conflict with the proposed development. (Table 2, Fig. 1 Trees #2, 9, 18, 20, 22, 23, 24, 29, 31, 32).

Twenty-five (25) private owned trees over 15cm DBH would be removed. These trees are in conflict with the proposed development. . (Table 2, Fig. 1 Trees #6, 7, 8, 12, 13, 14, 15, 16, 17, 21, 25, 26, 27, 28, 30, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53).

One (1) exempt privately owned tree (DBH less than 15cm) would also need to be removed. This tree is in conflict with the proposed development (Table 2, Fig. 1 Tree # 19).

Thirty (30) privately owned trees are to be preserved. Four (4) of which are at risk of injury due to construction of the proposed development.

Table #2. Private Tree Protection & Replacement Chart

Tree number (No.), species, Comments with: diameter at breast height (DBH); and Condition Rating (CR).

No.	Tree Species	DBH	Comments
#2, 9, 18, 20, 22, 23, 24, 29, 31, 32)	Black Locust White pine White spruce Manitoba maple Black walnut Sugar maple	50+	Remove: In conflict with the proposed development 20 replacement trees required
#6, 7, 8, 12, 13, 14, 15, 16, 17, 21, 25, 26, 27, 28, 30, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53	Sugar Maple Blue Spruce White Pine White Mulberry White Cedar White Spruce Black Locust Norway Maple Manitoba Maple Pear Tree	15-50	Remove: In conflict with the proposed development 25 replacement trees required
#19	Black Locust	<15	Remove: Exempt Tree
#33, 34, 35, 36, 38, 40, 41, 42, 43, 54 -70	Black Walnut Manitoba Maple Black Locust Norway Maple	15+	Protect: Proposed grade changes beyond TPZ of trees - no replacement trees required. Mitigation: No protection fencing required – trees are far away enough from construction areas
#1, 11, 37 ,39	White Elm Juniper Manitoba Maple	24+	Risk of Injury: proposed grade changes within TPZ of trees. Mitigation: These trees would be protected by a web-fence barrier TPZ: 1.8 m (min)

Neighbours' Trees:

All trees on a neighbouring property could be retained without risk of injury. The tree would be over its TPZ from the worksite (Table 3, Fig. 1, Tree #71-82).

Table #3. Neighbouring Tree Protection & Replacement Chart

Tree number (No.), species, Comments with: diameter at breast height (DBH); and Condition Rating (CR).

No.	Tree Species	DBH	Comments
#71-82	Black Walnut Sugar Maple White Elm Green Ash Black Locust Manitoba Maple Basswood	20+	Proposed grade changes beyond TPZ of trees – no replacement trees required. Mitigation: No protection fencing required – trees are far away enough from construction areas TPZ: 1.8 m (min)

3.2 Replacement Trees:

MSLA Landscape Architects has developed a landscape plan for the Wyndham & Main St property (L2-01 – MSLA Landscape Plan).

The City of Mississauga tree replacement requirements are as follows:

- If you remove a healthy tree with a diameter that is 49 cm or less, one replacement tree is required.
- If you remove a healthy tree with a diameter of 50 cm or greater, two replacement trees are required.
- Replacement trees must be at least 1.8 m tall if coniferous or at least 6 cm in diameter if deciduous

Eleven (11) trees with a DBH over 50cm are being removed. Therefore twenty-two (22) replacement trees would be required

Twenty-five (25) trees with a DBH less than 49cm are being removed. Therefore twenty-five (25) replacement trees would be required.

A total of forty-seven (47) replacement trees would be required.

Tree locations would be such as not to interfere with underground or overhead utility lines. The trees are to be planted after the construction and landscaping work on the site have been completed. The trees would best be transplanted during the spring or autumn. Mid-summer transplanting should be avoided. These trees are to be maintained in good condition. Supplemental watering may be required during the drier periods of the year, especially during the first two or three years after their transplantation

4. Conclusions

In order to allow for the development at Wyndham St. & Main St. Thirty-six (36) trees over 15cm DBH would have to be removed.

Thirty-five (35) privately owned tree over 15 cm DBH would be removed.
Forty-five (45) replacement trees required.

One (1) road allowance tree over 15cm DBH would be removed.
Two (2) replacement trees required.

No (0) trees on neighbouring properties are at risk of injury or removal

All of the trees to be retained are to be protected by barriers during the demolition and construction work on the site.

MSLA Landscape Architects is developing a landscape plan & tree replacement plan for the Wyndham St. & Main St. property.

D. Andrew White M. Sc.



October 16, 2017

Existing Tree Schedule

No.	Tree Species	DBH (cm)	Location	CR (%)	TC	Comments
#1	white elm	36-39	SE Main St.	55	private	To be Preserved
#2	black locust	86	SE tag# 13	60	private	To be Removed
#3	black locust	8-12	SW coppice	70	City	To be Removed
#4	Manitoba maple	78	SW Main St.	55	City	To be Removed
#5	green ash	8-10	SW corner	70	City	To be Preserved
#5b	green ash	8-10	SW corner	70	City	To be Preserved
#6	sugar maple	34	SW Wyndham St.	65	private	To be Removed
#7	honey locust	35	SW Wyndham St.	65	private	To be Removed
#8	blue spruce	24	SW Wyndham St.	70	private	To be Removed
#9	white pine	71	W near house	65	private	To be Removed
#10	Norway maple	10	SW Main St.	70	City	To be Preserved
#11	juniper	24	W near house	65	private	To be Preserved
#12	white mulberry	29-34	SW field tag# 5	65	private	To be Removed
#13	white cedar	28-34	SW field	60	private	To be Removed
#14	white cedar	36	SW field	65	private	To be Removed
#15	white cedar	29	SW field tag# 7	65	private	To be Removed
#16	white cedar	43-44	SW field tag# 8	60	private	To be Removed
#17	white spruce	28	SW side tag# 10	65	private	To be Removed
#18	white spruce	56	SW side tag# 11	65	private	To be Removed
#19	black locust	9-12	S side	70	private	To be Removed
#20	black locust	62	S side	65	private	To be Removed
#21	Norway maple	43	S side	65	private	To be Removed
#22	Manitoba maple	69	S side	70	private	To be Removed
#23	black walnut	52	E field tag# 18	65	private	To be Removed
#24	black locust	30-58	S side tag# 68	70	private	To be Removed
#25	Manitoba maple	46-48	SE side tag# 22	65	private	To be Removed
#26	Manitoba maple	39	SE side	65	private	To be Removed
#27	Manitoba maple	22	SE side	65	private	To be Removed
#28	Manitoba maple	20	SE side	65	private	To be Removed
#29	black locust	55	SE side tag #14	65	private	To be Removed
#30	black locust	42	SE side tag #13	65	private	To be Removed
#31	Manitoba maple	38-96	SE side	55	private	To be Removed
#32	sugar maple	52	SE side	60	private	To be Removed
#33	black walnut	15-18	SE corner	70	private	To be Preserved
#34	Manitoba maple	16-29	SE margin	65	private	To be Preserved
#35	Manitoba maple	29-44	SE margin	60	private	To be Preserved
#36	Manitoba maple	32-38	SE margin	60	private	To be Preserved
#37	Manitoba maple	39-54	SE margin	55	private	To be Preserved
#38	black walnut	45	E margin	65	private	To be Preserved
#39	Manitoba maple	59	E margin	60	private	To be Preserved
#40	Manitoba maple	19-22	E margin	55	private	To be Preserved
#41	black locust	39	N side tag# 47	65	private	To be Preserved

No.	Tree Species	DBH (cm)	Location	CR (%)	TC	Comments
#42	black locust	14	N side tag# 42	70	private	To be Preserved
#43	black locust	15	N side tag# 45	70	private	To be Preserved
#44	black locust	24	N side tag# 44	65	private	To be Removed
#45	black walnut	18	N side tag# 41	70	private	To be Removed
#46	black locust	29	N side tag# 48	70	private	To be Removed
#47	black locust	44	NW side tag# 72	65	private	To be Removed
#48	black locust	39	NW side tag# 71	65	private	To be Removed
#49	black locust	39	NW side	65	private	To be Removed
#50	black locust	38	NW side	65	private	To be Removed
#51	black locust	34	NW side	65	private	To be Removed
#52	black locust	32-34	NW side tag# 75	65	private	To be Removed
#53	pear tree	18	NW corner	60	private	To be Removed
#54	black walnut	19	N side tag #49	70	private	To be Preserved
#55	black locust	28	N side tag #41	70	private	To be Preserved
#56	black locust	27	N side tag #50	70	private	To be Preserved
#57	black walnut	39	N side tag# 51	65	private	To be Preserved
#58	black locust	29	N side tag# 62	65	private	To be Preserved
#59	black locust	74	N side tag# 63	60	private	To be Preserved
#60	black locust	30	N side tag# 56	65	private	To be Preserved
#61	black locust	36-42	N side	60	private	To be Preserved
#62	black locust	88	N side tag# 65 ?	40	private	To be Preserved
#63	black locust	46	N side tag# 64	65	private	To be Preserved
#64	black locust	62-83	N side	55	private	To be Preserved
#65	black locust	48-64	NW side	60	private	To be Preserved
#66	black locust	72	NW side	55	private	To be Preserved
#67	black locust	52	NW side	60	private	To be Preserved
#68	Norway maple	16	NW side	70	private	To be Preserved
#69	black walnut	28	NW side tag# 85	65	private	To be Preserved
#70	black locust	36	NW side tag# 84	65	private	To be Preserved
#71	black walnut	28	E offsite	65	neighbour	To be Preserved
#72	sugar maple	64	E offsite	60	neighbour	To be Preserved
#73	black walnut	38	E offsite	65	neighbour	To be Preserved
#74	white elm	28	E offsite	60	neighbour	To be Preserved
#75	sugar maple	62	NE offsite	60	neighbour	To be Preserved
#76	sugar maple	82	NE offsite	55	neighbour	To be Preserved
#77	sugar maple	20	NE offsite	70	neighbour	To be Preserved
#78	green ash	30	NE offsite, dead	0	neighbour	To Remain
#79	black locust	58-62	N offsite	60	neighbour	To be Preserved
#80	Manitoba maple	42	N offsite	65	neighbour	To be Preserved
#81	basswood	56-62	NW offsite	55	neighbour	To be Preserved
#82	black locust	38-44	NE offsite	65	neighbour	To be Preserved

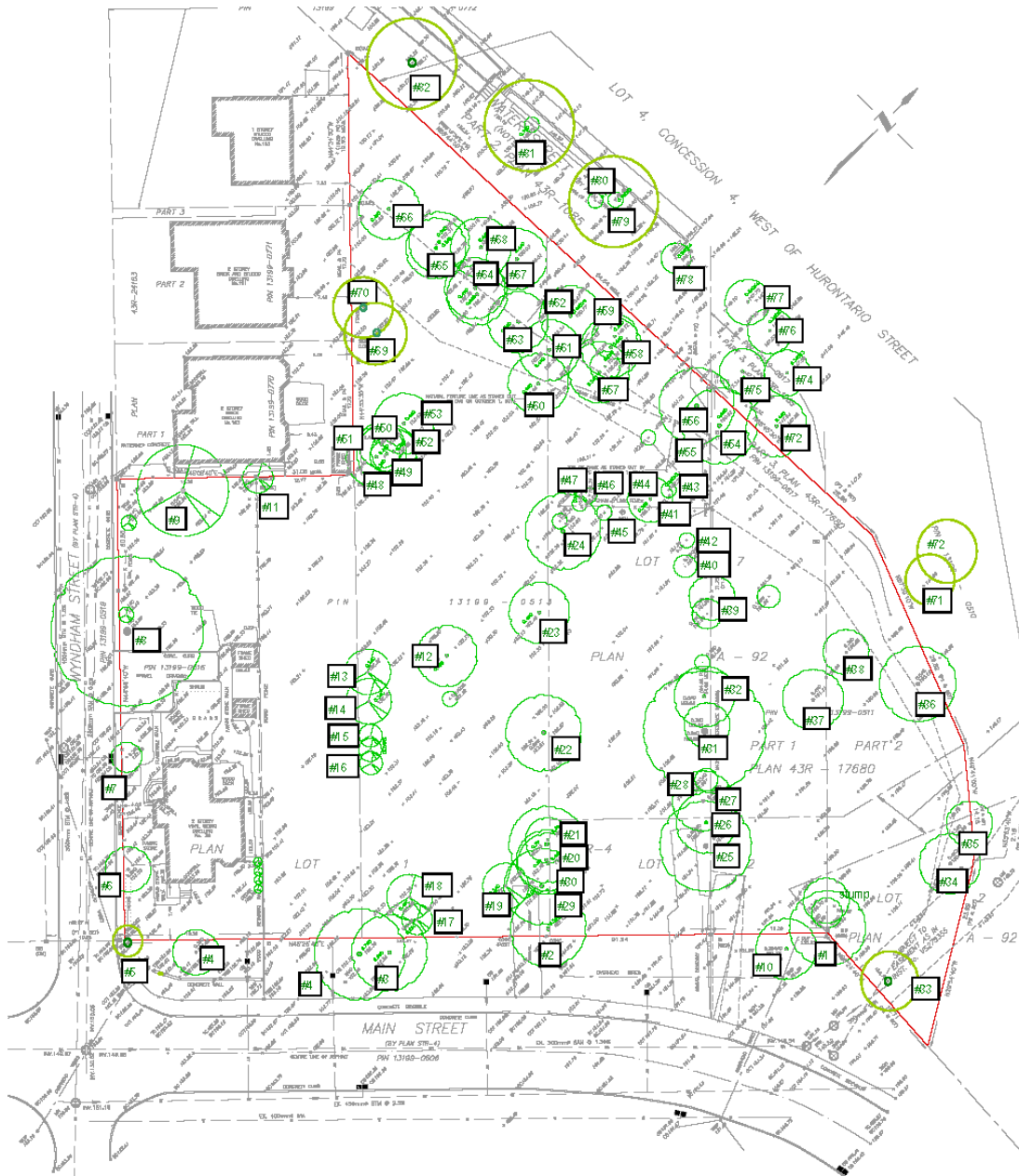


Figure #1: Tree locations on the Wyndham Street and Main Street junction development site

5. References

- 1- Council of Tree Landscape Appraisers. 2000. Guide for Plant Appraisal. 9th Edition. International Society of Arboriculture.
- 2- International Society of Arboriculture of Ontario. 1998. Ontario Supplement to Guide for Plant Appraisal 8th Edition. Ontario Chapter, International Society of Arboriculture.
- 3- City of Mississauga. 2001. THE TREE PERMIT BY-LAW NUMBER 474-05. Corporation of the City of Mississauga, ON.
- 4- City of Mississauga. 2017. Application to Permit the Injury or Destruction of Trees on Private Property. City of Mississauga Community Services Department.



Existing Tree Schedule

Number, Tree Species, DBH, Location, Condition Rating, Tree Category, Comments

No.	Tree Species	DBH (cm)	Location	CR (%)	TC	Comments
#1	white elm	36-39	SE Main St.	55	City	To be Preserved
#2	black locust	86	SE tag# 13	60	City-border	To be Removed
#3	black locust	8-12	SW coppice	70	City	To be Removed
#4	Manitoba maple	78	SW Main St.	55	City	To be Removed
#5	green ash	8-10	SW corner	70	City-border	To be Preserved
#5b	green ash	8-10	SW corner	70	City-border	To be Preserved
#6	sugar maple	34	SW Wyndham St.	65	City	To be Removed
#7	honey locust	35	SW Wyndham St.	65	City	To be Removed
#8	blue spruce	24	SW Wyndham St.	70	City-border	To be Removed
#9	white pine	71	W near house	65	private	To be Removed
#10	Norway maple	10	SW Main St.	70	City	To be Preserved
#11	juniper	24	W near house	65	private	To be Preserved
#12	white mulberry	29-34	SW field tag# 5	65	private	To be Removed
#13	white cedar	28-34	SW field	60	private	To be Removed
#14	white cedar	36	SW field	65	private	To be Removed
#15	white cedar	29	SW field tag# 7	65	private	To be Removed
#16	white cedar	43-44	SW field tag# 8	60	private	To be Removed
#17	white spruce	28	SW side tag# 10	65	private	To be Removed
#18	white spruce	56	SW side tag# 11	65	private	To be Removed
#19	black locust	9-12	S side	70	private	To be Removed
#20	black locust	62	S side	65	private	To be Removed
#21	Norway maple	43	S side	65	private	To be Removed
#22	Manitoba maple	69	S side	70	private	To be Removed
#23	black walnut	52	E field tag# 18	65	private	To be Removed
#24	black locust	30-58	S side tag# 68	70	private	To be Removed
#25	Manitoba maple	46-48	SE side tag# 22	65	private	To be Removed
#26	Manitoba maple	39	SE side	65	private	To be Removed
#27	Manitoba maple	22	SE side	65	private	To be Removed
#28	Manitoba maple	20	SE side	65	private	To be Removed
#29	black locust	55	SE side tag #14	65	private	To be Removed
#30	black locust	42	SE side tag #13	65	private	To be Removed
#31	Manitoba maple	38-96	SE side	55	private	To be Removed
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#35	Manitoba maple	29-44	SE margin	60	private	To be Preserved
#36	Manitoba maple	32-38	SE margin	60	private	To be Preserved
#37	Manitoba maple	39-54	SE margin	55	private	To be Preserved
#38	black walnut	45	E margin	65	private	To be Preserved
#39	Manitoba maple	59	E margin	60	private	To be Preserved
#40	Manitoba maple	19-22	E margin	55	private	To be Preserved
#41	black locust	39	N side tag# 47	65	private	To be Preserved
#42	black locust	14	N side tag# 42	70	private	To be Preserved
#43	black locust	15	N side tag# 45	70	private	To be Preserved
#44	black locust	24	N side tag# 44	65	private	To be Removed
#45	black walnut	18	N side tag# 41	70	private	To be Removed
#46	black locust	29	N side tag# 48	70	private	To be Removed
#47	black locust	44	NW side tag# 72	65	private	To be Removed
#48	black locust	39	NW side tag# 71	65	private	To be Removed
#49	black locust	39	NW side	65	private	To be Removed
#50	black locust	38	NW side	65	private	To be Removed
#51	black locust	34	NW side	65	private	To be Removed
#52	black locust	32-34	NW side tag# 75	65	private	To be Removed
#53	pear tree	18	NW corner	60	private	To be Removed
#54	black walnut	19	N side tag #49	70	private	To be Preserved
#55	black locust	28	N side tag #41	70	private	To be Preserved
#56	black locust	27	N side tag #50	70	private	To be Preserved
#57	black walnut	39	N side tag# 51	65	private	To be Preserved
#58	black locust	29	N side tag# 62	65	private	To be Preserved
#59	black locust	74	N side tag# 63	60	private	To be Preserved
#60	black locust	30	N side tag# 56	65	private	To be Preserved
#61	black locust	36-42	N side	60	private	To be Preserved
#62	black locust	88	N side tag# 65 ?	40	private	To be Preserved
#63	black locust	46	N side tag# 64	65	private	To be Preserved
#64	black locust	62-83	N side	55	private	To be Preserved
#65	black locust	48-64	NW side	60	private	To be Preserved
#66	black locust	72	NW side	55	private	To be Preserved
#67	black locust	52	NW side	60	private	To be Preserved
#68	Norway maple	16	NW side	70	private	To be Preserved
#69	black walnut	28	NW side tag# 85	65	private	To be Preserved
#70	black locust	36	NW side tag# 84	65	private	To be Preserved
#71	black walnut	28	E offsite	65	neighbour	To be Preserved
#72	sugar maple	64	E offsite	60	neighbour	To be Preserved
#73	black walnut	38	E offsite	65	neighbour	To be Preserved
#74	white elm	28	E offsite	60	neighbour	To be Preserved
#75	sugar maple	62	NE offsite	60	neighbour	To be Preserved
#76	sugar maple	82	NE offsite	55	neighbour	To be Preserved
#77	sugar maple	20	NE offsite	70	neighbour	To be Preserved
#78	green ash	30	NE offsite, dead	0	neighbour	To Remain
#79	black locust	58-62	N offsite	60	neighbour	To be Preserved
#80	Manitoba maple	42	N offsite	65	neighbour	To be Preserved
#81	basswood	56-62	NW offsite	55	neighbour	To be Preserved
#82	black locust	38-44	NE offsite	65	neighbour	To be Preserved



MARTON SMITH LANDSCAPE ARCHITECTS

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Client:

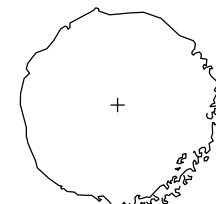

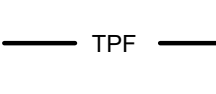
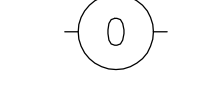

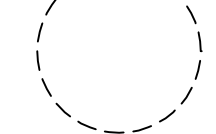
CITY PARK
(MAIN ST.) INC.

Municipality:



Notes:

Legend

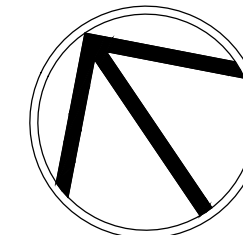
- Existing Deciduous Tree Canopy
- Property Line
- Tree Protection Fencing
- Existing Tree Reference Key
- Existing Tree to be Removed
- Tree Protection Zone

01 Issued for Review 11/03/17

No. Revision Date

North:

Stamp:



Project:

Proposed Residential Development
Wyndham St. & Main St.
Mississauga, Ontario

Scale: 1:300 Date: Oct. 2017

Drawn By: S.P. Checked By: L.M.

Drawing Title:

Tree Preservation Plan

Project No. 17178 Sheet No. L1-01

All information contained in these plans shall be checked by the contractor. All discrepancies shall be reported to the Landscape Architect (L.A.) before construction. All drawings have copyright and shall remain the property of the Landscape Architect. Copying or any reproduction in part or whole shall not be permitted without written consent of L.A. Drawings shall not be used for construction unless sealed & SIGNED. NOTE: FOR ANY AND ALL GRADING INFORMATION REFER TO SITE GRADING ENGINEERING DRAWINGS. NO SLOPES ARE TO EXCEED 3:1. PROPERTY LINES AND SETBACKS MUST BE CONFIRMED BY CONTRACTOR PRIOR TO CONSTRUCTION. ANY AND ALL REMAINING WALLS MUST BE APPROVED BY STRUCTURAL ENGINEER PRIOR TO CONSTRUCTION. DO NOT SCALE DRAWINGS. * PRELIMINARY NOT FOR CONSTRUCTION * THESE DRAWINGS ARE NOT TO BE USED FOR CONSTRUCTION UNTIL COUNTERSIGNED BY L.A. ARCHITECT. * SIGNED DATE

City of Mississauga
Corporate Report



Date: December 7, 2017

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
 Community Services

Originator's files:

Meeting date:
 2018/01/09

Subject

Request to Demolish a Heritage Listed Property: 7177 Lancaster Avenue (Ward 5)

Recommendation

That the property at 7177 Lancaster Avenue, which is listed on the City's Heritage Register, is not worthy of heritage designation, and consequently, that the owner's request to demolish proceed through the applicable process.

Background

Section 27.3 of the Ontario Heritage Act states that structures or buildings on property listed on the City's Heritage Register cannot be removed or demolished without at least 60 days' notice to Council. This legislation allows time for Council to review the property's cultural heritage value to determine if the property merits designation.

The owner of the subject property has submitted a heritage permit application to demolish and replace the existing detached dwelling. The subject property is listed on the City's Heritage Register as it forms part of the Malton Wartime Housing cultural landscape. This cultural landscape is noted for being a planned subdivision of the WWII and post-war era government efforts to provide mass produced housing to workers in industry related to the war effort and to veterans respectively within the city of Mississauga.

Comments

The owner of the subject property has requested permission to demolish the existing structure. The applicant has provided a Heritage Impact Assessment. It is attached as Appendix 1. The consultant has concluded that the structure at 7177 Lancaster Avenue is not worthy of designation. Staff concurs with this finding.

Financial Impact

There is no financial impact.

Conclusion

The owner of 7177 Lancaster Avenue has requested permission to demolish a structure on a property that is listed on the City's Heritage Register. The applicant has submitted a documentation report which provides information which does not support the building's merit for designation under the Ontario Heritage Act. Staff concurs with this finding.

Attachments

Appendix 1: Heritage Impact Assessment



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: P. Wubbenhorst, Senior Heritage Coordinator

Heritage Impact Assessment

7177 Lancaster Avenue

Mississauga, ON



November 28, 2017

Prepared for

by Robinson Heritage Consulting

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1.0 Executive Summary

Balkar Singh Garcha (property owner), through agent Tirth Singh, has retained Robinson Heritage Consulting (RHC) to conduct a Heritage Impact Assessment of the existing dwelling at 7177 Lancaster Avenue (Figure 1) as a built heritage resource within the Victory Wartime Housing cultural landscape in the Malton neighbourhood of the City of Mississauga to assess the impact of demolition in order to build a new detached dwelling on the property.

The scope of the Heritage Impact Assessment is limited geographically to what is referred to legally as Lot 194 within Registered Plan 436. The subject property is known municipally as 7177 Lancaster Avenue and is the third lot south of Etude Drive on the east side of Lancaster Avenue (Figure 2).

The property is in the northeast corner of what is recognized by the City of Mississauga Official Plan as the Victory Wartime Housing Cultural Landscape (Victory CL), bounded by Victory Crescent on the north, Airport Road on the west, Merritt Avenue on the south and Lancaster Avenue on the east. The neighbourhood arose, as a planned community, out of the need for affordable housing for thousands of employees in the adjacent airplane manufacturing plants and related industries at the beginning of World War II. The cultural landscape has a distinct character of modest 1 to 1½-storey



Figure 1 – 7177 Lancaster Avenue (Photo: RHC 2017).

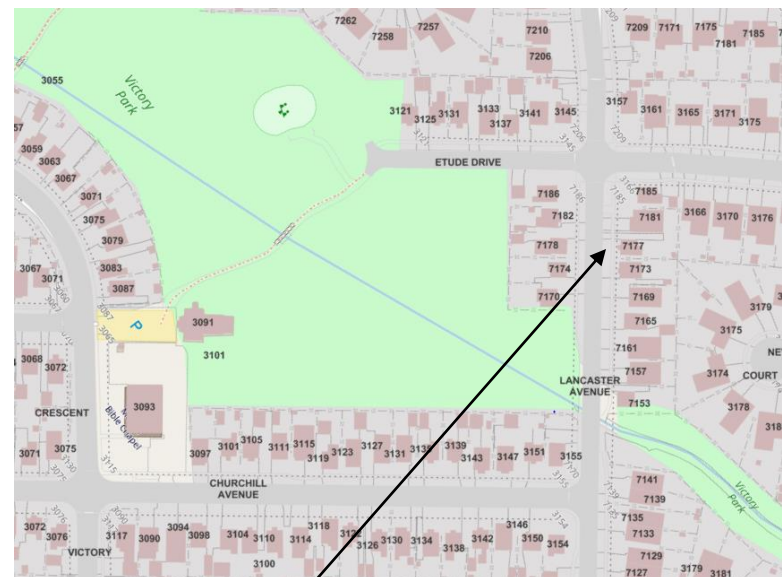


Figure 2 - Property location (Image: City of Mississauga Maps Online).



Figure 3 – Merritt Avenue, within the Victory Wartime Housing Cultural Landscape, Mississauga. (Photo: Google 2017).

residential structures, mature boulevard trees and consistent setbacks (Figure 3).

All properties identified by the Cultural Landscapes Inventory (2005), which includes the Victory Wartime Housing cultural landscape, have been listed as non-designated on the City of Mississauga's Heritage Register. The subject dwelling has limited architectural design value on its own but does contribute to the overall character of the Victory CL having retained its original 1-storey, L-plan massing and side gable roof form. The large, mature silver maple tree in the boulevard in front of 7177 Lancaster Avenue (Figure 4) is a proud survivor that is likely from the original or early subdivision plantings.

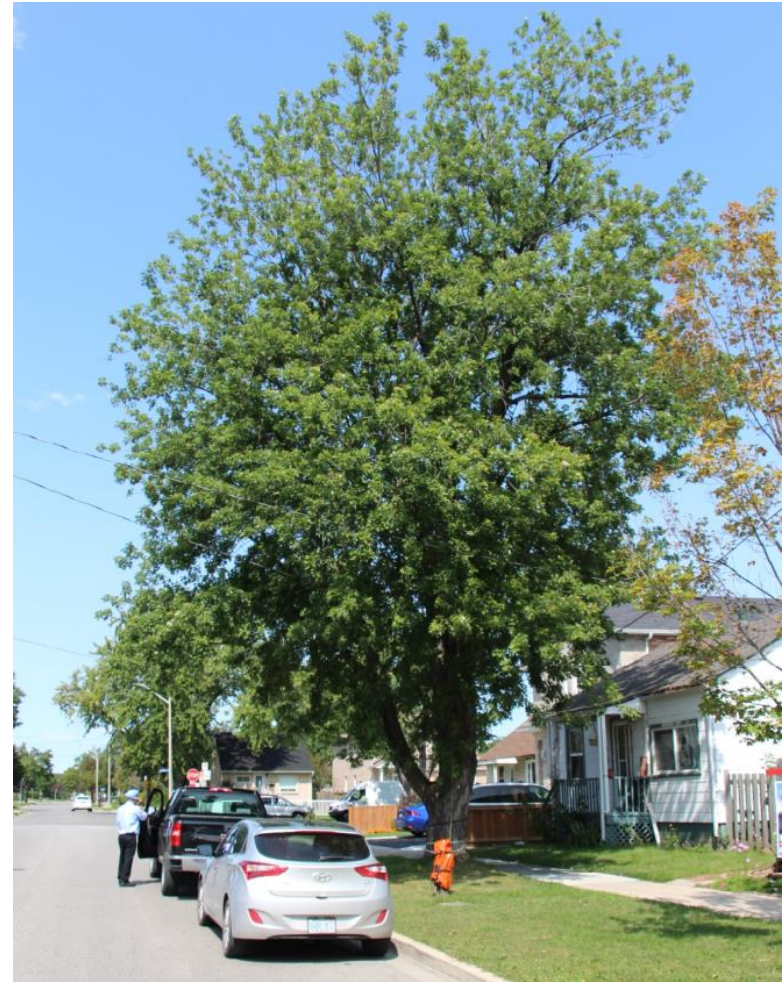


Figure 4 – Silver maple in front of 7177 Lancaster Avenue (Photo: RHC 2017).

2.0 Study Rationale and Methodology

This study was undertaken according to the City of Mississauga's Cultural Landscape Heritage Impact Assessment (HIA) Terms of Reference¹ as well as guidelines set out in the Ministry of Tourism, Culture, and Sport's booklet "Heritage Resources in the Land Use Planning Process" from the *Ontario Heritage Toolkit*.

A site visit was conducted by RHC and photographs were taken of the subject property on August 25, 2017. The day was warm and sunny.

A Heritage Impact Assessment is a study to determine the impacts to known and potential heritage resources within a defined area proposed for future development. The study would include an inventory of all heritage resources within the planning application area. The study results in a report which identifies all known heritage resources, an evaluation of the significance of the resources, and makes recommendations toward mitigation measures that would minimize negative impacts to those resources.²

LANDSCAPE ENVIRONMENT

- ☐ Scenic and Visual Quality
- ☐ Natural Environment
- ☐ Horticultural Interest
- ☐ Landscape Design, Type and Technological Interest

HISTORICAL ASSOCIATION

- ☒ Illustrates Style, Trend or Pattern
- ☒ Direct Association with Important Person or Event
- ☒ Illustrates Important Phase in Mississauga's Social or Physical Development
- ☐ Illustrates Work of Important Designer

¹

https://www7.mississauga.ca/documents/culture/heritage/CulturalLandscapeHIA_TermsOfRef2017.pdf

² Ibid., Page 1.

Cultural Heritage Landscape Inventory Heritage Impact

Assessments must demonstrate how the proposed development will conserve the criteria that render it a cultural heritage landscape and/or feature. Each cultural heritage landscape and feature includes a checklist of criteria. The Heritage Impact Assessment need only address the checked criteria for the pertinent cultural heritage landscapes or features.

The criteria from the City of Mississauga's Cultural Landscape Inventory analysis of the Victory Wartime Housing Cultural Landscape are shown on this and the preceding page (as well as in Figure 9) will be discussed in the HIA.

- Illustrates a style, trend or pattern
- Direct association with an important person or event
- Illustrates an important phase in Mississauga's social or physical development
- Consistent scale of built features
- Historical interest

BUILT ENVIRONMENT

- ☐ Aesthetic/Visual Quality
- ☐ Consistent Early Environs (pre-World War II)
- ☒ Consistent Scale of Built Features
- ☐ Unique Architectural Features/Buildings
- ☐ Designated Structures

OTHER

- ☒ Historical or Archaeological Interest
- ☐ Outstanding Features/Interest
- ☐ Significant Ecological Interest
- ☐ Landmark Value

3.0 Legislation and Policy Framework

3.1 Planning Act

Part 1, Section 2 of the (Ontario) Planning Act identifies matters of provincial interest, which includes the conservation of significant features of architectural, cultural, historical, archaeological or scientific interest. Section 3 of the Planning Act allows the province to issue policy statements on matters of provincial interest. In respect of the exercise of any authority that affects a planning matter, Section 3 of the Planning Act requires that decisions affecting planning matters “shall be consistent with” policy statements issued under the Planning Act.

3.2 Provincial Policy Statement 2014

The Provincial Policy Statement (issued under the authority of Section 3 of the Planning Act) was introduced in 2005 and updated April 30, 2014. PPS (2014), Section 2: Wise Use and Management of Resources, states that

Ontario's long-term prosperity, environmental health, and social well-being depend on conserving biodiversity, protecting the health of the Great Lakes, and protecting natural heritage, water, agricultural, mineral and cultural heritage and archaeological resources for their economic, environmental and social benefits.

Section 2.6: Cultural Heritage and Archaeology

2.6.1 Significant built heritage resources and significant cultural heritage landscapes shall be conserved.

The 2014 Provincial Policy Statement provides definitions of key terms in the heritage planning process.

Built heritage resource: means a building, structure, monument, installation or any manufactured remnant that contributes to a property's cultural heritage value or interest as identified by a community, including an Aboriginal community. Built heritage resources are generally located on property that has been designated under Parts IV or V of the Ontario Heritage Act, or included on local, provincial and/or federal registers.

Conserved: means the identification, protection, management and use of built heritage resources, cultural heritage landscapes and archaeological resources in a manner that ensures their cultural heritage value or interest is retained under the Ontario Heritage Act. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact assessment. Mitigative measures and/or alternative development approaches can be included in these plans and assessments.

Heritage attributes: means the principal features or elements that contribute to a protected heritage property's cultural heritage value or interest, and may include the property's built or manufactured elements, as well as natural landforms, vegetation, water features, and its visual setting (including significant views or vistas to or from a protected heritage property).

3.3 Ontario Heritage Act

Typically, the significance of a built heritage resource is identified by evaluation criteria that define the characteristics that have cultural heritage value or interest to local, provincial, or federal jurisdictions. Criteria to define cultural heritage significance are prescribed in Regulation 9/06 made pursuant to section 29(1) (a) of the Ontario Heritage Act.

3.3.1 Ontario Regulation 9/06

A property may be determined to have cultural heritage value or interest and may be designated under section 29 of the Ontario Heritage Act if it meets one or more of the following criteria:

1. The property has **design value or physical value** because it,
 - i. is a rare, unique, representative or early example of a style, type, expression, material or construction method,
 - ii. displays a high degree of craftsmanship or artistic merit, or
 - iii. demonstrates a high degree of technical or scientific achievement.

2. The property has **historical value or associative value** because it,
 - i. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,
 - ii. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or
 - iii. demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.

3. The property has **contextual value** because it,
 - i. is important in defining, maintaining or supporting the character of an area,
 - ii. is physically, functionally, visually or historically linked to its surroundings, or
 - iii. is a landmark.

3.3.2 Heritage Impact Assessments and Conservation Plans for Cultural Heritage Resources

The assessment of potential impact by development on cultural heritage resources is guided by Ministry of Culture, Tourism and Sport (MCTS) InfoSheet #5 – *Heritage Impact Assessments and Conservation Plans* contained within *Ontario Heritage Tool Kit* booklet “*Cultural Heritage Resources in the Land Use Planning Process: Cultural Heritage and Archaeology Policies of the Ontario Provincial Policy Statement, 2005.*”³

To conserve a cultural heritage resource, a municipality or approval authority may require a heritage impact assessment and/or a conservation plan to guide the approval, modification, or denial of a proposed development or site alteration that affects a cultural heritage resource.

A heritage impact assessment generally contains, but is not limited to the following information:

- Historical Research, Site Analysis and Evaluation;
- Identification of the Significance and Heritage Attributes of the Cultural Heritage Resource;
- Description of the Proposed Development or Site Alteration;
- Measurement of Development or Site Alteration Impact;
- Consideration of Alternatives, Mitigation and Conservation Methods;
- Implementation and Monitoring;
- Summary Statement and Conservation Recommendations.

A Conservation Plan generally contains, (but is not limited to) the following information:

- Identification of the conservation principles appropriate for the type of cultural heritage resource being conserved;

³ The Provincial Policy Statement was subsequently updated in 2014.

- Analysis of the cultural heritage resource, including documentation of the resource, descriptions of cultural heritage value or interest, assessment of resource conditions and deficiencies, discussion of historical, current and proposed use;
- Recommendations for conservation measures and interventions, short or long-term maintenance programs, implementation, and the qualifications for anyone responsible for the conservation work;
- Schedule for conservation work, inspection, maintenance, costing, and phases of rehabilitation or restoration work;
- Monitoring of the cultural heritage resource and the development of a long-term reporting structure.

MCTS InfoSheet #5 describes “Principles in the Conservation of Historic Properties”:

- | | |
|--------------------------------------|---|
| • Respect for Documentary Evidence | Do not base restoration on conjecture. |
| • Respect for Original Location | Do not move buildings unless there is no other means to save them. |
| • Respect for Historic Material | Repair/conservate rather than replace building materials and finishes, except where absolutely necessary. |
| • Respect for Original Fabric | Repair with like materials. |
| • Respect for the Building’s History | Do not restore to one period at the expense of another period. |
| • Reversibility | Alterations should allow a resource to return to its original conditions. |
| • Legibility | New work to be distinguishable from old. |
| • Maintenance | With continuous care, future restoration will not be necessary. |

Negative impacts on a cultural heritage resource identified in MCTS InfoSheet #5 include, but are not limited to:

- Destruction of any, or part of any, significant heritage attributes or features;
- Alteration that is not sympathetic, or is incompatible, with the historic fabric and appearance;

- Shadows created that alter the appearance of a heritage attribute or change the viability of a natural feature or plantings, such as a garden;
- Isolation of a heritage attribute from its surrounding environment, context or a significant relationship;
- Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features;
- A change in land use such as rezoning a battlefield from open space to residential use, allowing new development or site alteration to fill in the formerly open spaces;
- Land disturbances such as a change in grade that alters soils, and drainage patterns that adversely affect an archaeological resource.

MCTS InfoSheet #5 recommends methods of minimizing or avoiding a negative impact on a cultural heritage resource. These include, but are not limited to:

- Alternative development approaches;
- Isolating development and site alteration from significant built and natural features and vistas;
- Design guidelines that harmonize mass, setback, setting, and materials;
- Limiting height and density;
- Allowing only compatible infill and additions;
- Reversible alterations;
- Buffer zones, site plan control, and other planning mechanisms.

3.4 City of Mississauga Official Plan

The following Official Plan Policies within Section 7.1 “Complete Communities” are applicable to the proposed demolition and development at 7177 Lancaster Avenue.

7.1.8 - Mississauga will recognize the significance of and act responsibly in the identification, protection, and enhancement of structures, sites, cultural heritage landscapes, environments, artifacts, traditions, and streetscapes of historical, architectural or archaeological significance.

7.1.10 - When making planning decisions, Mississauga will identify, maintain and enhance the distinct identities of local communities by having regard for the built environment, natural or heritage features, and culture of the area.

7.4.1.3 - Mississauga will require development to maintain locations and settings for cultural heritage resources that are compatible with and enhance the character of the cultural heritage resource.

7.4.1.7 - Mississauga will maintain a Heritage Register of property, including structures and cultural landscapes that should be preserved as cultural heritage resources. The cultural heritage resources in the Heritage Register will be assessed based on their design or physical value, historical or associative value, contextual value and archaeological significance including the aggregation of both natural and cultural heritage resources.

7.4.1.12 - The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a Heritage Impact Assessment, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction.

7.4.1.14 - Cultural heritage resources will be integrated with development proposals.

7.4.2.2 - Prior to the demolition or alteration of a cultural heritage resource, documentation will be required of the property to the satisfaction of the City, and any appropriate advisory committee. This documentation may be in the form of a Heritage Impact Assessment.

Mississauga's Official Plan describes the Victory Wartime Housing cultural landscape (Victory CL) as an important element of the Malton Neighbourhood (Figure 5). The Victory CL is bounded by Victory Crescent on the north, Airport Road on the west, Merritt Avenue on the south and Lancaster Avenue on the east. The neighbourhood arose, as a planned community, out of the need for affordable housing for the thousands of employees in the adjacent airplane manufacturing plants and related industries at the beginning of World War II. There is a distinct character of modest 1 to 1½-storey residential structures, mature boulevard trees and consistent setbacks.

The OP Land Use Designation for the subject property is Residential Low Density 1 (Figure 7). The property is part of an R4-64 zone on the east side of Lancaster Avenue (Figure 8) with a maximum height at the highest ridge (with a sloped roof) of 9.0 m. The R4-64 Zoning allows for 5% more maximum lot coverage than that of the R4-1 Zoning on the west side of Lancaster Avenue and in the balance of the Victory CL. The maximum gross floor area for infill in the R4-64 Zone area is 150m² (plus 0.2 times the lot area) which is 50m² greater than that of the R4-1 Zone.

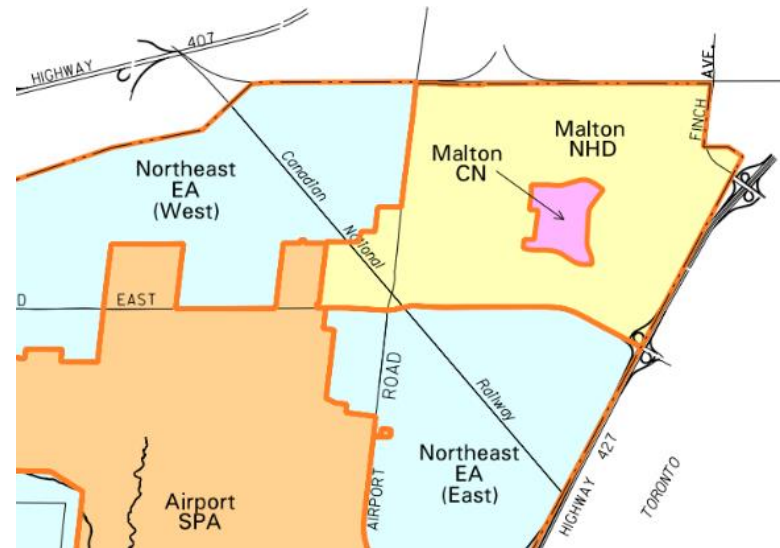


Figure 5 - Malton Neighbourhood (NBH). (Image: Mississauga Official Plan Schedule 9: Character Areas)

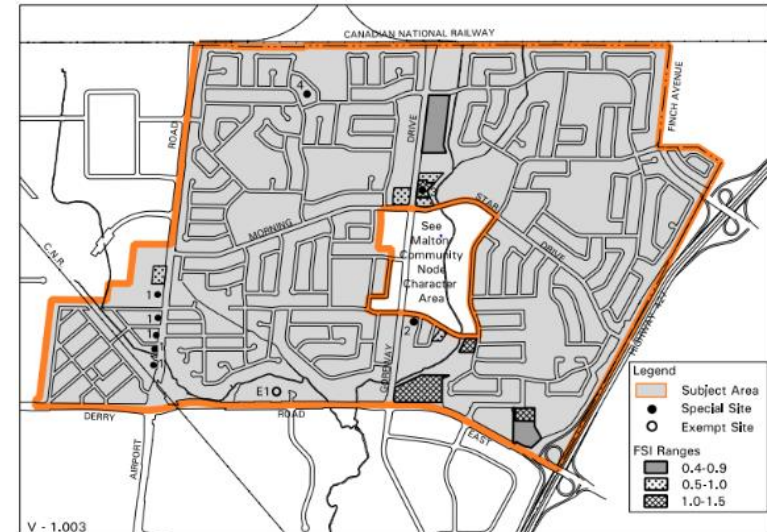


Figure 6 - Map 16-15: Malton Neighbourhood Character Area. (Image: City of Mississauga Official Plan Part 3, page 16-73)

4.0 Cultural Heritage Landscape

The Provincial Policy Statement has defined the term “cultural heritage landscape” as a geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an Aboriginal community. The area may involve features such as structures, spaces, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association. Examples may include, but are not limited to, heritage conservation districts designated under the Ontario Heritage Act; villages, parks, gardens, battlefields, main streets and neighbourhoods, cemeteries, trail ways, viewsheds, natural areas and industrial complexes of heritage significance; and areas recognized by federal or international designation authorities (e.g. a National Historic Site or District designation, or a UNESCO World Heritage Site).

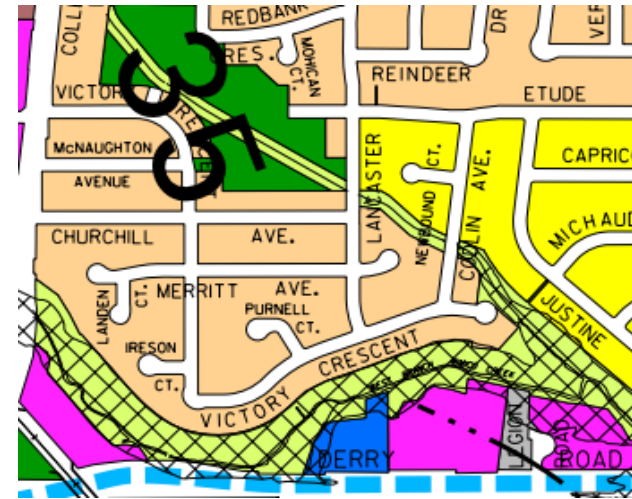


Figure 7 - Land Use Designations (Image: City of Mississauga Official Plan, Schedule 10)

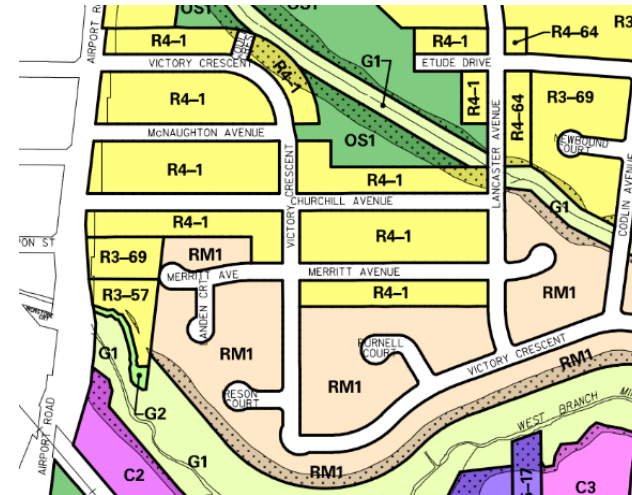


Figure 8 - Detail from Zoning Map 48W (Source: City of Mississauga Interactive Zoning By-Law)

4.1 City of Mississauga's Cultural Landscape Inventory

The Victory Wartime Housing Cultural Landscape in the Malton Neighbourhood is described in the City of Mississauga's Cultural Landscape Inventory, Section L-RES-5 as:

This planned subdivision is located opposite the north-east corner of Pearson International Airport. The neighbourhood is close to where the original Malton Terminal was located and remains close to the present airplane manufacturing and service industry. Although some of the original houses have been altered with newer porches, dormers, raised basements and garages, many retain characteristics typical of the period with 1 to 1 roof pitches, central front doors, picture windowed living rooms to one side, kitchen and eating areas on the opposite side and bedrooms and bathrooms to the rear. According to local sources, one in four of the houses was moved from Bramalea Road when the airport was expanded in 1950. The relocated houses and lots sold for \$2,500.00 each. The street names in the area, including Churchill Avenue and Victory Crescent, act as reminders that this area was developed during the post-war period [and also the war period as later described]. Its significance lies in the fact that it retains a number of post-war houses which represent some of the first mass produced housing in the GTA.

CITY OF MISSISSAUGA Cultural Landscape Inventory	
War Time Housing (Malton)	L-RES-5
Location	Located north of Pearson International Airport bounded by Derry Road on the south and Airport Road on the west
Heritage or Other Designation	None
Landscape Type	Residential (Neighbourhood)
LANDSCAPE ENVIRONMENT <input type="checkbox"/> Scenic and Visual Quality <input type="checkbox"/> Natural Environment <input type="checkbox"/> Horticultural Interest <input type="checkbox"/> Landscape Design, Type and Technological Interest	BUILT ENVIRONMENT <input type="checkbox"/> Aesthetic/Visual Quality <input type="checkbox"/> Consistent Early Environments (pre-World War II) <input checked="" type="checkbox"/> Consistent Scale of Built Features <input type="checkbox"/> Unique Architectural Features/Buildings <input type="checkbox"/> Designated Structures
HISTORICAL ASSOCIATION <input checked="" type="checkbox"/> Illustrates Style, Trend or Pattern <input checked="" type="checkbox"/> Direct Association with Important Person or Event <input checked="" type="checkbox"/> Illustrates Important Phase in Mississauga's Social or Physical Development <input type="checkbox"/> Illustrates Work of Important Designer	OTHER <input checked="" type="checkbox"/> Historical or Archaeological Interest <input type="checkbox"/> Outstanding Features/Interest <input type="checkbox"/> Significant Ecological Interest <input type="checkbox"/> Landmark Value
	
SITE DESCRIPTION <p>This planned subdivision is located opposite the northeast corner of Pearson International Airport. The neighbourhood is close to where the original Malton Terminal was located and remains close to the present airplane manufacturing and service industry. Although some of the original houses have been altered with newer porches, dormers, raised basements and garages, many retain characteristics typical of the period with 1 to 1 roof pitches, central front doors, picture windowed living rooms to one side, kitchen and eating areas on the opposite side and bedrooms and bathrooms to the rear. According to local sources, one in four of the houses were moved from Bramalea Road when the airport was expanded in 1950. The relocated houses and lots sold for \$2,500.00 each. The street names in the area, including Churchill Avenue and Victory Crescent, act as reminders that this area was developed during the post-war period. Its significance lies in the fact that it retains a number of post-war houses which represent some of the first mass produced housing in the GTA.</p>	
	

Figure 9 – (Image: City of Mississauga Cultural Landscape Inventory, January 2005.)

4.2 Wartime and Victory Housing

Wartime and Victory communities of homes were constructed in response to a critical need during the mid-20th Century and that makes them unique to other types of communities that we study. It is the first time in Canada that large scale mass produced subdivisions were constructed. The subdivisions were usually highly planned, typical of wartime, and constructed of many prefabricated parts that required less skilled labour. As a result, a few modest styles of homes would be repeated throughout the community giving it a rather homogenous appearance constructed in modest 1 and 1 ½ storey styles with simple lines and materials.

Although thought to be temporary in nature it is interesting that principals from the City Beautiful movement were applied and the houses were located on spacious lots with large set backs, along streets that moved away from straight grid formations instead to streets that were laid out with curves, cul-de-sacs and crescents. Boulevards, trees and wooden sidewalks and gravel roads were also characteristic of this movement and often applied to the communities. Lands were usually set aside for a park, school, community centre and church (Figure 10). The government established Wartime Housing Limited (WHL) in 1940 to address the housing crisis for employees in communities across the country employed in the war effort and would see 32,000 rental units built by



Figure 10 – 3093 Churchill Ave, Malton Bible Chapel (Photo: RHC 2017).

1947. It was believed that the housing was temporary and would be removed after the war. Instead the Veteran's Housing Program grew in response to the concern that returning veterans were finding themselves homeless and the new program offered the wartime housing to veterans affordably. With amenities such as community centres, libraries and programming in health, education and community these subdivisions were supported as an answer for these lower income families.

The house lots generally had a frontage of 40 feet and depth of 100 feet. Lots were included for a church, community hall (Figure 11) and what would become Victory Park. The street names reflected the war effort including Lancaster Avenue for the Lancaster bomber.



Figure 11 – 3091 Victory Crescent, Malton Victory Hall (Photo: RHC 2017).

4.3 Victory Park Cultural Heritage Landscape

The 1942 survey of these lands (Figures 12 and 13) shows the layout of streets and lots as well as a block for a school or church and a block designated as park space which became known as Victory Park. The streets were given wartime theme names such as Churchill (for Winston Churchill), Lancaster (for the Lancaster Bomber) and McNaughton (for Lieutenant-General Andrew McNaughton, Commanding Officer for the Canadian Army during World War II). Fourteen lots face onto Lancaster Avenue with 40-foot frontages and 100 feet depth. According to *The Victory Wartime Housing Cultural Landscape in the Malton Neighbourhood in the City of Mississauga's Cultural Landscape Inventory, Section L-RES-5*:

According to local sources, one in four of the houses was moved from Bramalea Road when the airport was expanded in 1950. The relocated houses and lots sold for \$2,500.00 each.

Lancaster Avenue has many mature and maturing trees on lawns and the grass boulevards shading the sidewalks that run down both sides of the roadway as well as a number of original wartime houses on either side. Alterations and additions to many of the remaining original houses was noted including variety of different exterior

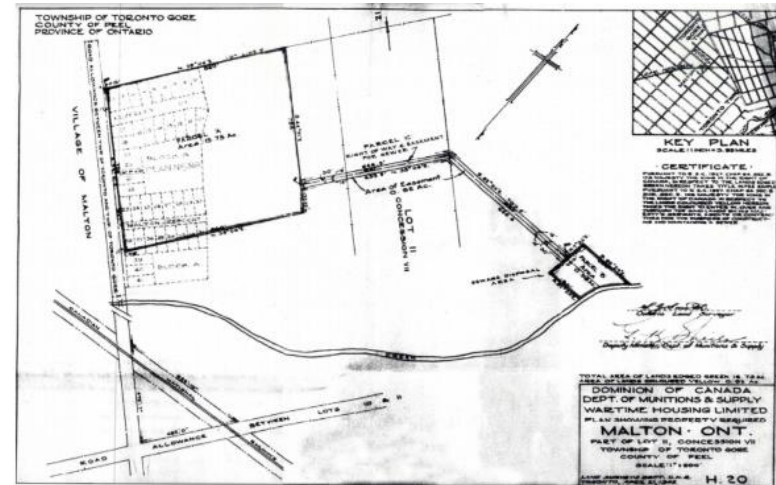


Figure 12 - Wartime Housing Limited, Survey H-20 (1942).

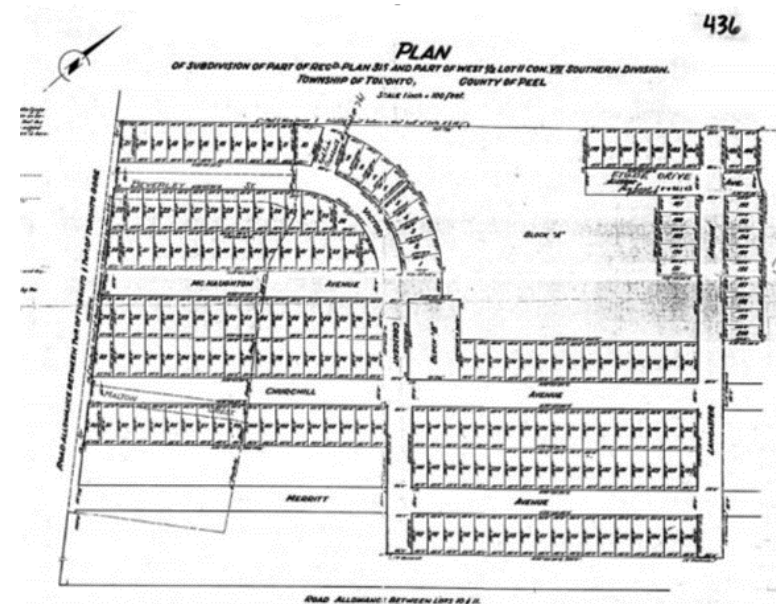


Figure 13 - Registered Plan 436 (Peel Land Registry Office).

claddings, windows, doors and the addition of sheds, carports and garages and a second storey added to at least one although the original form is discernible. Several wartime homes have been demolished and replaced with typical suburban 2-storey homes (Figures 22, 24 and 26).

A comparison of 2002 and 2017 air photos (Figures 14 and 15) shows that of the nine original wartime houses on the east side of Lancaster Avenue below Etude Drive, four have been demolished and replaced with what in RHC's opinion are unsympathetic and inappropriate residential designs that are not compatible with the heritage character of the Victory War Housing cultural landscape.



Figure 14 – Air photo 2002 (Image: City of Mississauga Online Mapping)

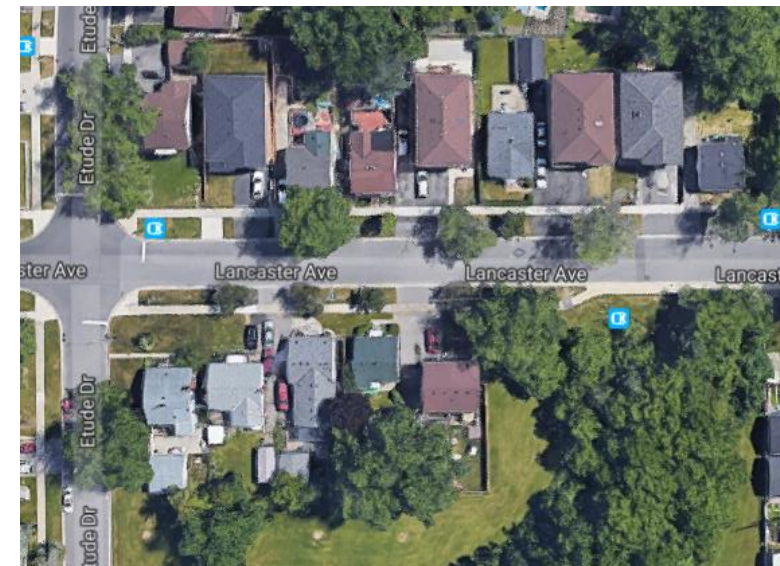


Figure 15 - Air photo 2017. (Image: Google Maps)

4.4 Cultural Heritage Landscape of the Subject Property

The modest wartime bungalow at 7177 Lancaster Avenue retains its original form as a side gable, three-bay, single storey house with a rear tail. It was built inexpensively from prefabricated panels as emergency rental housing during the Second World War on lands expropriated by the federal government in 1942.

This small and simple dwelling lacks any significant architectural features or craftsmanship on its own but in combination with the rest of the wartime houses in the Victory Wartime Housing Cultural Landscape contributes to the overall cultural heritage landscape. It is unlikely that 7177 Lancaster Avenue could be designated under Part IV of the Ontario Heritage Act on its own. Designation as a Cultural Heritage Conservation District under Part V of the Ontario Heritage Act as the Victory Wartime Housing Cultural Landscape would be required to capture the true historic value of the property. The **consistent scale of built features** is an identified attribute that distinguishes the Victory Wartime Housing Cultural Landscape. There are several variants of these bungalows from single storey to one and half storey gable dwellings. The low height, general massing and setbacks of the bungalows in the district is a simple but **recognizable pattern and style** when viewed en mass. When combined along with the common lands, layout of lots and streets and mature tree along grassy boulevards there is an identifiable character to the neighbourhood. The wartime houses have slight variations in setbacks but generally all relate to one another. Narrow driveways and single garages located behind the front façade of the building are also characteristic of this neighbourhood.

The challenge with 7177 Lancaster is that it is becoming interrupted and even isolated from the rest of the Victory Wartime Housing Cultural Landscape by the demolition and construction of new homes that do not relate to the balance of the Victory Wartime Housing Cultural Landscape in design, massing, heights or materials. The context has been severely impacted in this area of Lancaster Avenue with at least five newer homes being constructed. The set backs for the new construction is distinctly different from the wartime housing and include attached garages set in front of the house rather than at the side like the wartime housing. The increased height is also not in keeping with the neighbourhood yet the zoning for this area allows two storey dwellings to be built. The **recognizable pattern and style** is interrupted in this area of Lancaster Avenue.

The subdivision is a common experience all over the country so perhaps there is a tendency to look beyond the simple designs and overlook their contribution to our cultural heritage. What sets Victory Wartime Housing Cultural Landscape apart is its **direct association with a historic event** as a wartime response to the housing shortage and as an **important phase in Mississauga's development**. The Victory Wartime Housing Cultural Landscape is situated close to what was, in the early part of world war II, the new international airport and allied airplane factory. Housing was immediately required for 10,000 workers so the Wartime Housing Limited expropriated lands and had plans for simple dwellings that could be rapidly erected from prefabricated panels prepared. This was some of the first mass produced housing in the greater Toronto area and it along with other wartime and victory housing projects like it significantly altered the way we subdivide and develop property.

The value for 7177 Lancaster Avenue is as a contributing element to the Malton Wartime Housing Cultural Landscape. The impact of the new homes on the street to the contextual value of 7177 Lancaster Avenue is that the understanding of the wartime bungalow has been interrupted in this part of the street therefore lowered for the subject property and its remaining wartime houses on this section of Lancaster Avenue.

Overall the design for the proposed new house to replace the wartime bungalow at 7177 Lancaster Avenue is more in keeping with the newer homes on the street rather than the wartime bungalows. The nature of the narrow long lots precludes many designs of larger homes that were stylistically developed after the victory homes that may provide better transition to larger homes. There really is no two-storey variation that would truly fit in the cultural heritage landscape as it is the small, low gabled form that is a defining characteristic of the landscape. Unfortunately, with no urban design guidelines, one and half storey limitations on zoning heights, permissible large front garages, altered setbacks and wide driveways there is limited way to control the change in this area. The proposed new house for 7177 Lancaster Avenue has taken into account some of the recommendations of this report in the choice of materials and arrangement of windows in order to lower the impact and contrast to the remaining wartime streetscape but generally is more like the newer houses on the block in height, design and massing. The construction of new homes has meant the loss of the large front lawns, mature trees on the boulevard and set backs identified and features of the original wartime neighbourhood.

5.0 Historical Summary

5.1 Indigenous People

Malton was developed upon lands that had been occupied by various indigenous groups for thousands of years before the first French explorers and Jesuit missionaries came to this area in the 1600's. During the Woodland Period (100BC – AD 1650) this area was occupied by the Iroquoian people. During this period language developed between groups and the nomadic people began to settle and develop methods of agriculture.

The Mississauga people established themselves along the north shore of Lake Ontario during the early 1700's. The British Crown purchased much of the Mississauga people's lands for Loyalists to settle on between 1781 and 1800. The remaining lands were referred to as the "Mississauga Tract" but would again be reduced through a purchase outlined in Treaty 13-A in 1805 that was commonly referred to as the First Purchase. Several more treaties would follow until the Crown owned all the Mississauga's lands against the wishes of the Mississauga people. Peter Jones, an Ojibwa Methodist minister, translator and chief, arrived in the area in 1825 and assisted the Mississauga resulting in a village being established along side the Credit River on what is now Mississauga Road. By the 1840's the Mississauga people began to leave this area for the Six Nations Reserve area establishing the Mississauga's of the New Credit Reserve near Hagersville.

5.2 Village of Malton

The Village of Malton was established shortly after the first European settlers arrived in the area. One of this group was Richard Halliday, a blacksmith that had emigrated from Malton, North Yorkshire, England in 1819. Halliday was not registered as a land owner as he is not found in the land registry but he would give the name of his hometown to the new village. Joseph Tomlinson arrived with his wife Mary in August of 1820 to claim his 100-acre parcel of land located on the east half of Lot 10 Concession 7. He cleared and fenced five acres of land and built a cabin within the 18-month period to comply with the conditions of his land grant. Other early settlers include Samuel and

Margaret Shaw, Samuel Moore and Henry and Elizabeth Brocklebank. In the 1840's the Blanchard family cleared lands that the Village of Malton was established on beyond the original four corners - the commercial district in this largely agricultural community. A log school house was built in 1828 and replaced with a brick structure in 1858 and again in 1923.

In 1850, when Toronto Township was incorporated, Malton had a population of 350. The introduction of the Grand Trunk Railway in 1854, allowed better access to Toronto markets for local farmers and Malton thrived as a result. The village of Malton was subdivided in 1855. The population was 600 in 1864. Malton was chosen as the county seat in 1867, but Brampton contested the decision and was awarded the county seat a year later. Its economic prosperity declined, as did the population, to 200. The opportunity for advancement was dealt another blow when the Credit Valley Railway came to Dixie, Streetsville, Meadowvale and Churchville in 1879. Malton suffered with the resulting drop in shipping business.

The small rural farming village steadily grew and by 1850 Malton had a population of 350 and boasted a blacksmith shop, cobbler shop, wagon maker, carriage maker, saddlery, general store, school and a hotel. In 1854 when the Grand Trunk Railway arrived it brought more prosperity for the small the village as it became a hub for grain export to the Toronto markets drawing farmers from the surrounding agricultural lands. New businesses established and flourished providing goods and services for the farmers and their families. Malton was incorporated into a police village in 1914 before being ceded into Toronto Township in 1952 and incorporated into the Town of Mississauga in 1967 which then grew and became the City of Mississauga in 1974.

5.3 Malton during World War II

When Malton was chosen as the site for a new international airport in 1937 the City of Toronto purchased 13 farms along Derry Road that same year clearing the land for runways and associated buildings. With the World War II on the horizon the airport became a training centre and in 1938 the National Steel Company (Montreal) opened an aircraft factory to the east of the airport. The Avro Anson and Westland Lysander were built here which drew hundreds of employees. As part of the war effort, the federal government took over the site for the

production of allied aircraft production increasing the workforce to 10,000 resulting in a housing shortage that was remedied with the construction of a new subdivision just north of the factory that would be known as “Victory Wartime Housing Cultural Landscape”.

Wartime Housing Limited expropriated 15.75 acres from Frederick Codlin’s farm on Airport Road in April of 1942 as well as lands for easement for sewage disposal and a further 73.36 acres in October of the same year (Figure 12). In 1951 these lands were surveyed by H. C. Sewell into 200 lots that comprised Registered Plan 436 (Figure 13) which superseded an earlier plan of 1939.

At the end of the war the factory and its assets were sold off to one of the largest aeronautical companies in the world who developed the famed supersonic Avro Arrow fighter jet which was unveiled in October 1957 but then quickly saw production terminated in 1959 as a cost cutting measure by Prime Minister John Diefenbaker and sold in 1960 to the company that would become Boeing. The major expansion of the Lester B. Pearson airport occurred in 1963. Over the years of 2003 and 2004 Boeing demolished the original Victory aircraft buildings.

In 1946, the National Housing Act created the Central Mortgage and Housing Corporation (CMHC) and transferred much of the wartime housing assets to the CMHC to administer. By 1949 the remaining wartime housing units were transferred to CMHC who began to prepare the assets for individual sale as tenant’s either vacated or accepted the offer to purchase the property they were living in, eventually divesting itself of all housing assets and relinquishing its role as landlord.

6.0 Land Title and Ownership History

Table 1 - Entries from Title Abstract for Lot 194, Plan 436
(Also described as Part of W ½ Lot 11, Con 7) Township of Toronto

Number	Instrument	Inst Date	Reg Date	Grantor	Grantee	Consideration	Remarks
436	Plan	5 Feb 1952	5 Feb 1952	Central Mortgage & Housing Corp.			
74437	Notice	14 May 1953	1 June 1953	Re: Zoning Reg's. Malton Airport			
426	By-law	1 June 1954	9 June 1954	Re: Control			
120053	Notice	3 June 1959	15 June 1959	Re-Zoning Regs Malton Airport			
853US	Grant	23 Feb 1961	14 Jan 1966	Central Mortgage & Housing Corp.	Gordon F. Meager & Elizabeth H. Meager, as joint tenants	3,575.75	All
854VS	Grant	29 Nov 1965	14 Jan 1966	Gordon F. Meager & Elizabeth H. Meager	Robert R. Williams	8,000.00	All
855VS	Mortgage	29 Nov 1965	14 Jan 1966	Robert R. Williams	Gordon F. Meager & Elizabeth H. Meager	6,000.00	All
248789VS	Notice	30 Nov 1972	12 Feb 1973	Amendment of Airport Zoning Regulations			

(Information source: Peel Land Registry Office, Brampton.)

7.0 Property Description

A detail from a surveyor's plan of 7177 Lancaster Avenue (Lot 194, Registered Plan 436) is presented in Figure 16. The real property measures 12.19 metres in width and 30.4 in depth.

The front façade of the house is setback from the Lancaster Avenue sidewalk a distance of 3.10 metres. Between the sidewalk and the street is a grass boulevard. The trunk of a large, mature silver maple tree rises from the grass boulevard with its crown extending over the subject property just past the front façade of the house.

The left side lot line has a wood board fence. An asphalt driveway is also along the left side lot line and extends to well past the rear of the house and the wood frame deck on the rear of the house.

The rear of the lot has a small, wooden shed near the back right corner, beneath several large, rear yard trees of the neighbouring properties.

The plan footprint of 7173 Lancaster Avenue is a mirror image the subject building. Both houses have the same front yard setback.

The existing interior arrangement of 7177 Lancaster Avenue is presented in floor plan drawings in Figure 17. No significant interior architectural features were found in the site visit. Photos of the current interior are presented in Figures 39-46.

The ground floor room partitions appear to be in what are likely their original locations.

Additional bedroom spaces have been created in the basement.

The existing form of 7177 Lancaster Avenue is presented in elevation drawings in Figures 18 and 19.

With the exception of the half-glass, rear exterior door at and the 3-pane woos sash basement windows, all other doors and windows have been replaced.

7.1 Property and Site Plan Area

The 1942 survey of these lands (Figure 10) shows the layout of streets and lots as well as a block for a school or church and a block designated as park space which became known as Victory Park. The streets were given wartime theme names such as Churchill (for Winston Churchill), Lancaster (for the Lancaster Bomber) and McNaughton (for Lieutenant-General Andrew McNaughton, Commanding Officer for the Canadian Army during World War II). Fourteen lots face onto Lancaster Avenue with 40-foot frontages and 100 feet depth. According to The Victory Wartime Housing Cultural Landscape in the Malton Neighbourhood in the City of Mississauga's *Cultural Landscape Inventory, Section L-RES-5*:

According to local sources, one in four of the houses was moved from Bramalea Road when the airport was expanded in 1950. The relocated houses and lots sold for \$2,500.00 each.

Lancaster Avenue has many mature and maturing trees on lawns and the grass boulevards shading the sidewalks that run down both sides of the roadway as well as a number of original wartime houses (Figure 11) on either side. Alterations and additions to a many of the remaining original houses was noted including variety of different exterior claddings, windows, doors (Figure 12) and the addition of sheds, carports and garages and a second storey added to at least one although the original form is discernible. Several wartime homes have been demolished and replaced with typical suburban 2-storey homes (Figures 13, 14 and 15).

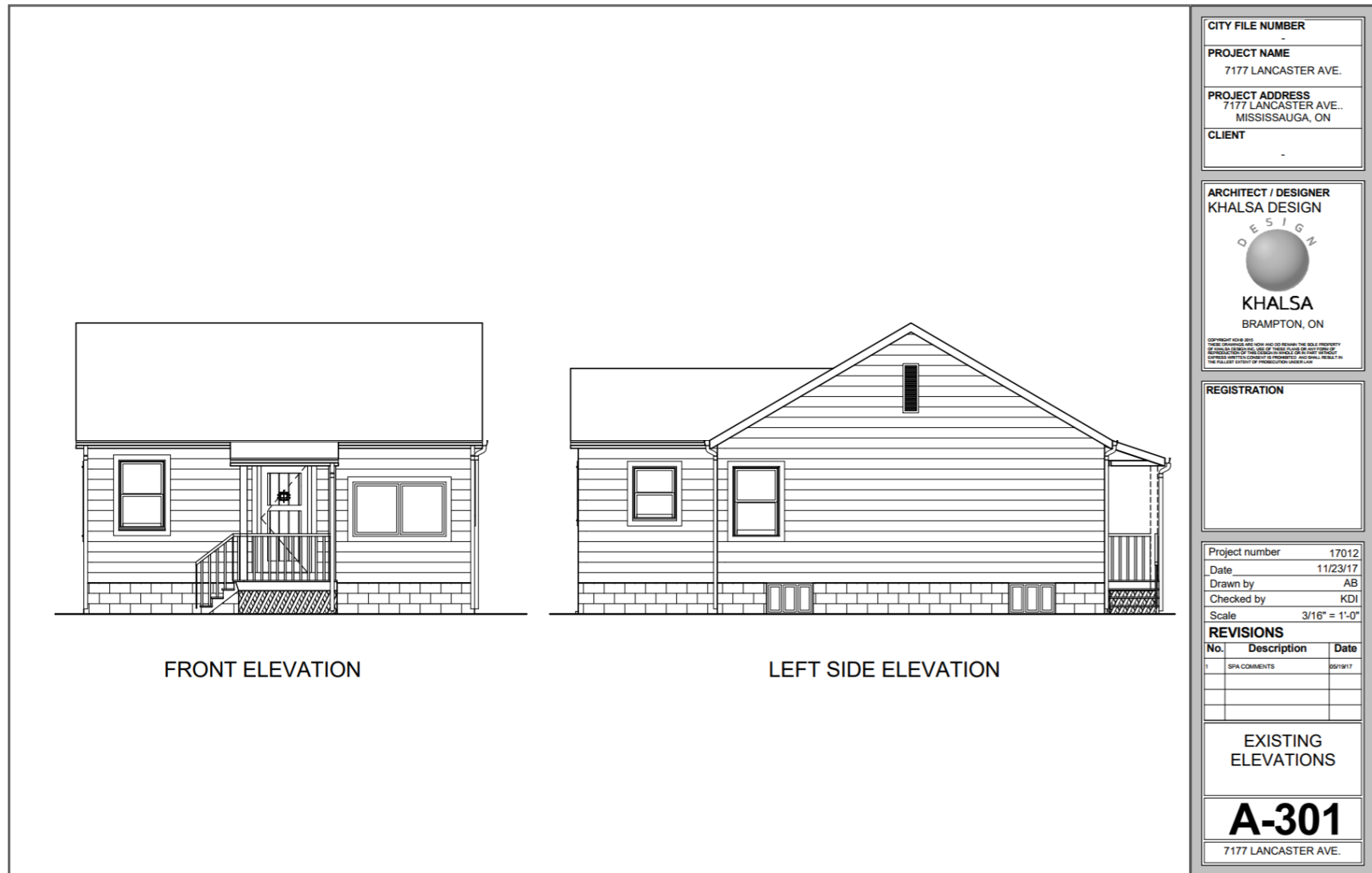


Figure 18 –7177 Lancaster Avenue. Existing front and left elevations. (Image: Khalsa Design, Nov 23, 2017)

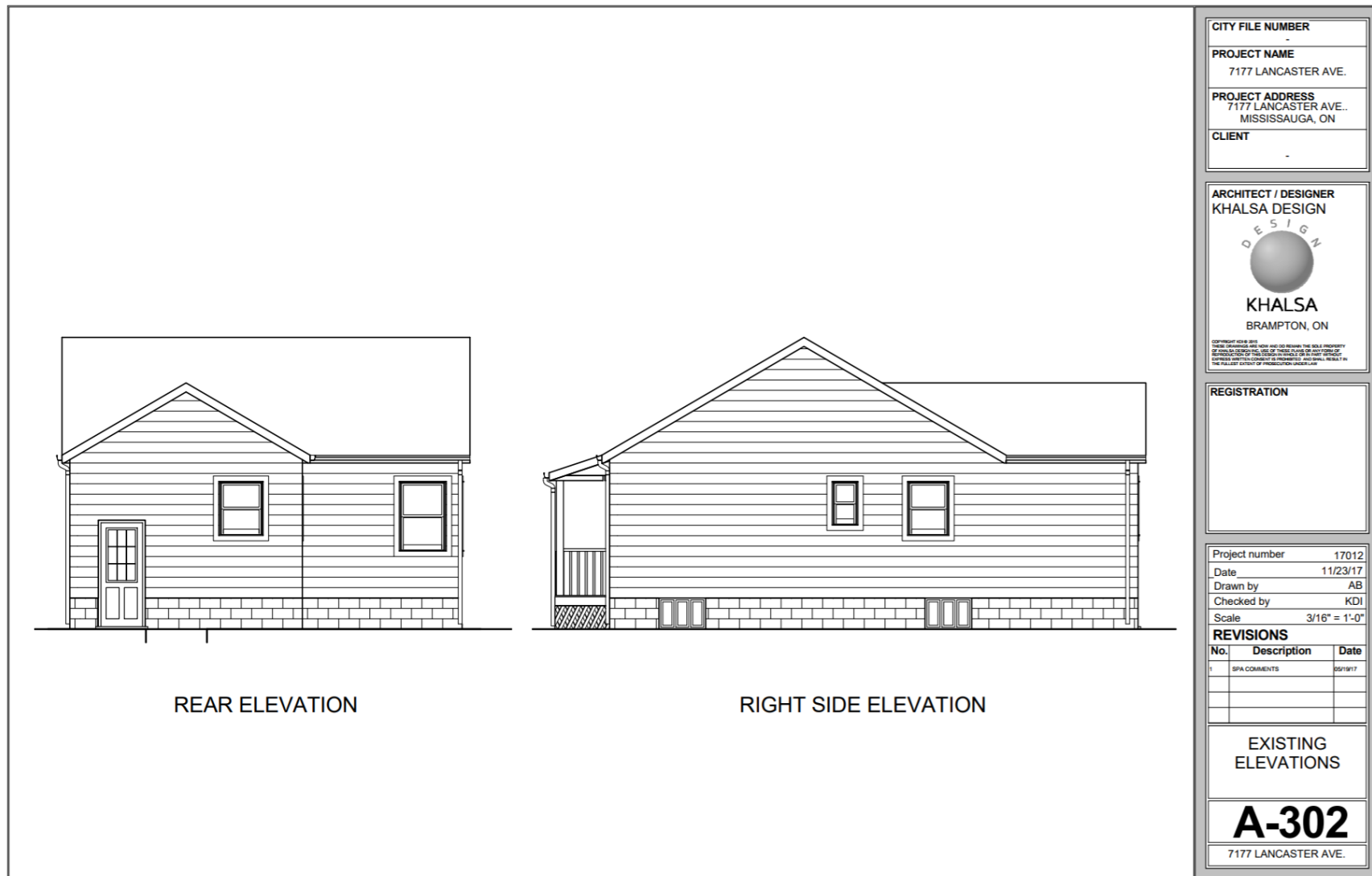


Figure 19 –7177 Lancaster Avenue. Existing rear and right elevations. (Image: Khalsa Design, Nov 23, 2017)



Figure 20 - 7185, 7181 and 7177 Lancaster Avenue (Photo: RHC 2017)



Figure 21 - 7185 Lancaster Avenue (Photo: RHC 2017)



Figure 22 - 7181 Lancaster Avenue (Photo: RHC 2017)



Figure 24 - 7169 Lancaster Avenue (Photo: RHC 2017)



Figure 23 - 7177 and 7173 Lancaster Avenue (Photo: RHC 2017)



Figure 25 - 7165 Lancaster Avenue (Photo: RHC 2017)



Figure 26 - 7161 and 7157 Lancaster Avenue (Photo: RHC 2017)



Figure 28 - 7170, 7174, 7178 Lancaster Avenue (Photo: RHC 2017)



Figure 27- 7153 Lancaster Avenue (Photo: RHC 2017)



Figure 29 - 7182 and 7186 Lancaster Avenue (Photo: RHC 2017)

7.2 Building Description

The original house at 7177 Lancaster Avenue is in many ways similar to the Wartime Housing Limited's "H22" design (Figure 29) with a medium side gable roof and a 3-bay front with a centre door. The main block footprint of the single-storey building measures roughly 24' x 24'. The house has concrete block foundation walls and a full height basement.

The WHL dwellings were built with modular, prefabricated 4' x 10' floor and wall sections. The side gable roof has a slope of roughly 8/12 and the remains of the original, red brick chimney stack rises from near the centre of the roof ridge. The attic is ventilated by a narrow, louvered, rectangular opening near the peak of both side gable walls. At 7177 Lancaster, this feature has been subsequently covered by what appears to be a decorative, aluminum window shutter – likely added when the building was re-clad with the existing siding.

Although it is difficult to be certain without closer inspection by a building materials expert, most of the existing cladding appears to be asbestos-cement shingles with some infill done with (non-asbestos)



Figure 30 - 7177 Lancaster Avenue. (Photo: RHC 2017)



Figure 31 - Design H22 used by Wartime Housing Limited. (Image: National Film Board, 1942)

fibre-cement product.⁴ A comparison of the same window in 7177 Lancaster and 3216 Merritt Avenue (Figure 20) reveals that the existing siding on 7177 Lancaster Avenue has been applied over the original cladding making the existing siding almost flush with the window frame.

The only extant original window sashes are in the three-pane, wood sashes at the top of the basement walls. All other windows and doors have been replaced.

The front elevation has three bays with a centre door (Figure 18). The front left window opening position is original but the front right window opening has been altered to become a horizontally oriented, 2-pane unit attempting to achieve a wider “picture” window for the living room – a feature that became popular in mid-20th century residential design. The left window on the right (south) side elevation has been filled in with matching siding. The standard WHL designs (e.g. H22) had a window in this location but it may be that a decision as made to fill these in – in such a way that would make it easy to uncover the opening in future.

⁴ https://inspectapedia.com/exterior/Asbestos_Cement_Woods_Amy.pdf

https://inspectapedia.com/exterior/Asbestos_Cement_Siding.php



Figure 32 – Window comparison: (left) 7177 Lancaster Ave; (right) 3216 Merritt Drive. (Photos: RHC 2017)



Figure 33 - 3216 Merritt Drive. (Photo: RHC 2017)

A basic comparison of 7177 with another Victory CL house (3216 Merritt Avenue) shows how these houses often underwent similar changes over the years. 3216 Merritt appears to have retained its original wood clapboard siding and, like 7177 Lancaster Ave, the front right window has been enlarged to become a “picture window” with larger panes.

The left window on the right side elevation of both houses has been filled in with matching siding (Figure 22). The standard H22 design had a window in this location but it may be that home owners decided to fill these in in such a way that would make it easy to uncover the opening in future.

The northeast corner of the rear has a typical early Modernist architectural feature of the era with a window on both angles of the rear corner (Figure 23). At the rear of the house is an extension at the southeast corner with a back door leading to a split stairway leading to the kitchen area or the basement (Figures 24 and 26).



Figure 34 – 7177 Lancaster Ave, Covered south window. (Photo: RHC 2017)



Figure 35 – Rear. (Photo: RHC 2017)



Figure 36 – Rear tail and entrance door. (Photo: RHC 2017)



Figure 37- Front facade. (Photo: RHC 2017)



Figure 38 – View between 7177 and 7173 Lancaster Avenue, showing trees and simple shed at the rear of the subject property. (Photo: RHC 2017)

8.0 Statement of Significance

The subject property meets the following criteria for determining cultural heritage value or interest under Regulation 9/06 of the Ontario Heritage Act.

8.1 Heritage Attributes

- single-storey, side gable massing
- 3-bay front elevation

8.2 Design Value or Physical Value

7177 Lancaster Avenue does not have significant architectural or design value on its own but rather contributes, along with other original World War II period homes and boulevard trees, to the overall heritage character and cultural heritage landscape of the Victory Wartime Housing Cultural Landscape in the Malton neighbourhood as planned by Wartime Housing Limited.

The house has merit as being representative of a new mass-produced construction method of its time.

8.3 Historical Value or Associative Value

7177 Lancaster Avenue has historical value in that it is associated with wartime events and the development of mass produced housing by Wartime Housing Limited in Malton during the Second World War and contributes to an important phase in Mississauga's development.

8.4 Contextual Value

The existing house at 7177 Lancaster Avenue contributes along with other original World War II period homes and boulevard trees and park spaces to the overall heritage character and cultural heritage landscape of the Victory Wartime Housing Cultural Landscape neighbourhood as an important form of urban planning carried out in Malton by Wartime Housing Limited.

9.0 Proposed Development

The owner of the wartime house located at 7177 Lancaster Avenue is proposing to demolish the structure and build a 2 storey, hip roofed modern suburban home with a brick veneer, an interior 2-car garage and large paired and tripled windows in the upper storey on the front façade. (Figures 47-56)

The brick proposed for 7177 Lancaster, Brampton Brick's "Aurora" clay brick (Figure 48).

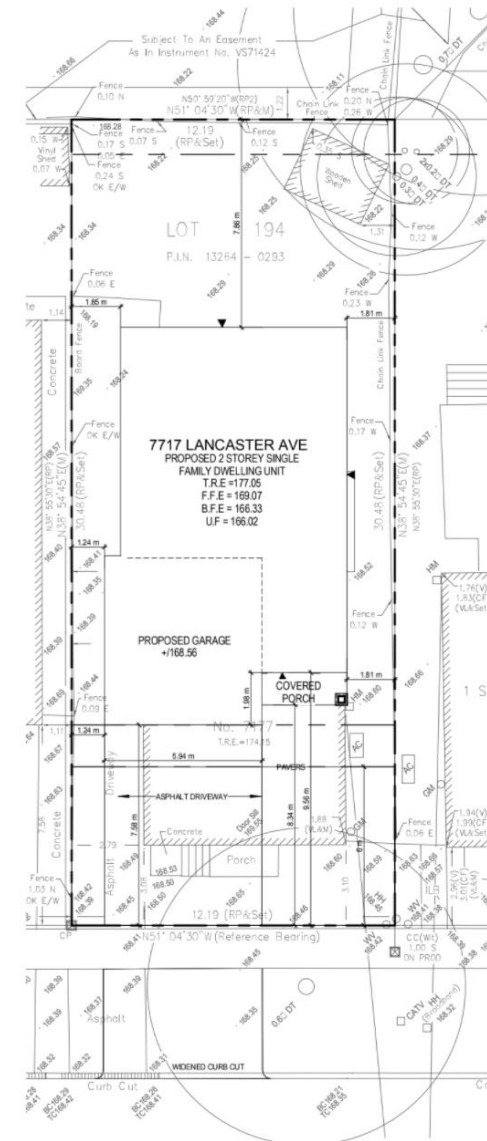


Figure 47 – Detail from Proposed Site Plan A-002. (Image: Khalsa Design)

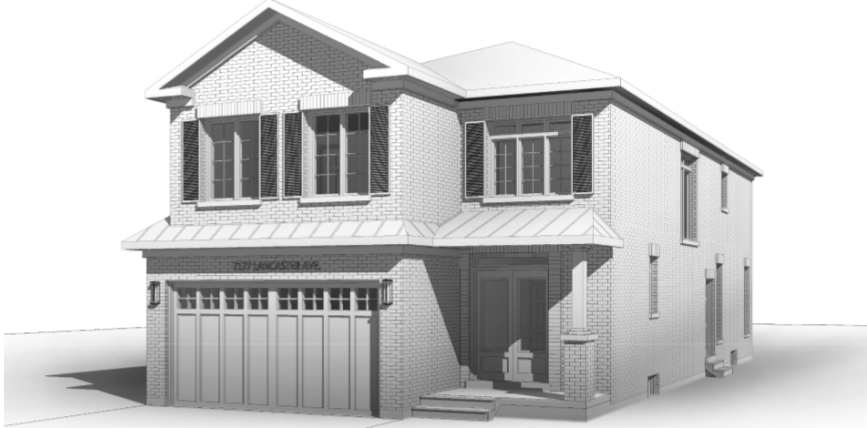


Figure 48 – Proposed front elevation design for 7177 Lancaster Ave, compared with current front elevations of 7181 and 7173 Lancaster Avenue.
(Image: Khalsa Design)

DESIGN SET

DRAWLING LIST

- A-000 TITLE PAGE
- A-001 SITE PLAN
- A-002 ZONING PLAN
- A-101 BASEMENT FLOOR PLAN
- A-102 GROUND FLOOR PLAN
- A-103 SECOND FLOOR PLAN
- A-104 ROOF PLAN
- A-301 FRONT ELEVATION
- A-302 REAR ELEVATION
- A-303 LEFT SIDE ELEVATION
- A-304 RIGHT SIDE ELEVATION



AURORA

SB-12 ENERGY EFFICIENCY DESIGN MATRIX

PERSCRPTIVE COMPLIANCE SB-12 (SECTION 3.1.1)		
PACKAGE A1		
SPACE HEATING FUEL		
<input checked="" type="checkbox"/> GAS <input type="checkbox"/> OIL <input type="checkbox"/> ELECTRI <input type="checkbox"/> PROPANE <input type="checkbox"/> EARTH <input type="checkbox"/> SOLID FUEL		
BUILDING COMPONENT	REQUIRED	PROPOSED
INSULATION RSI (R) VALUE		
CEILING W/ ATTIC SPACE	10.56 (R60)	10.56 (R60)
CEILING W/O ATTIC SPACE	5.46 (R31)	5.46 (R31)
EXPOSED FLOOR	5.46 (R31)	5.46 (R31)
WALLS ABOVE GRADE	3.87 (R24)	3.87 (R22)
BASEMENT WALLS	3.52 ci (R20 ci)	3.52 ci (R20 ci)
BELOW GRADE SLAB ENTIRE SURFACE >600mm BELOW GRADE	-	-
EDGE OF BELOW GRADE SLAB < 600mm BELOW GRADE	1.76 (R10)	1.76 (R10)
HEATED SLAB < 600mm BELOW GRADE	1.76 (R10)	1.76 (R10)
WINDOWS & DOORS		
WINDOWS SLIDING GLASS DOORS (MAX U-VALUE / MIN. ER)	1.6	1.6
SKYLIGHTS (MAX. U-VALUE)	2.8	2.8
APPLIANCE EFFICIENCY		
SPACE HEATING EQUIP. (AFUE%)	96%	96%
HRV. EFFICIENCY (%)	75%	75%
DHW HEATER (EF)	0.8	0.8

AREA CALCULATIONS

GROUND FLOOR AREA	989 sq ft
SECOND FLOOR AREA	1313 sq ft
SUBTOTAL	2302 sq ft
DEDUCT ALL OPEN AREAS	63 sq ft
TOTAL NET AREA	2219 sq ft (206.15 sq m)
FINISHED BSMT. AREA	0 sq ft
COVERGE W/OUT PORCH	1355 sq ft (125.86 sq m)
COVERGE WITH PORCH	1397 sq ft (129.79 sq m)

CITY FILE NUMBER

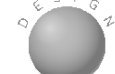
PROJECT NAME

7177 LANCASTER AVE.

PROJECT ADDRESS

7177 LANCASTER AVE.
MISSISSAUGA, ON

CLIENT

ARCHITECT / DESIGNER
KHALSA DESIGNKHALSA
BRAMPTON, ON

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REGISTRATION

Project number 17012

Date 10/17/2017

Drawn by AB

Checked by KDI

Scale 1/2" = 1'-0"

REVISIONS

No.	Description	Date

TITLE PAGE

A-000

7177 LANCASTER AVE.

Figure 49 - Zoning Plan A-000 (Image Khalsa Design)

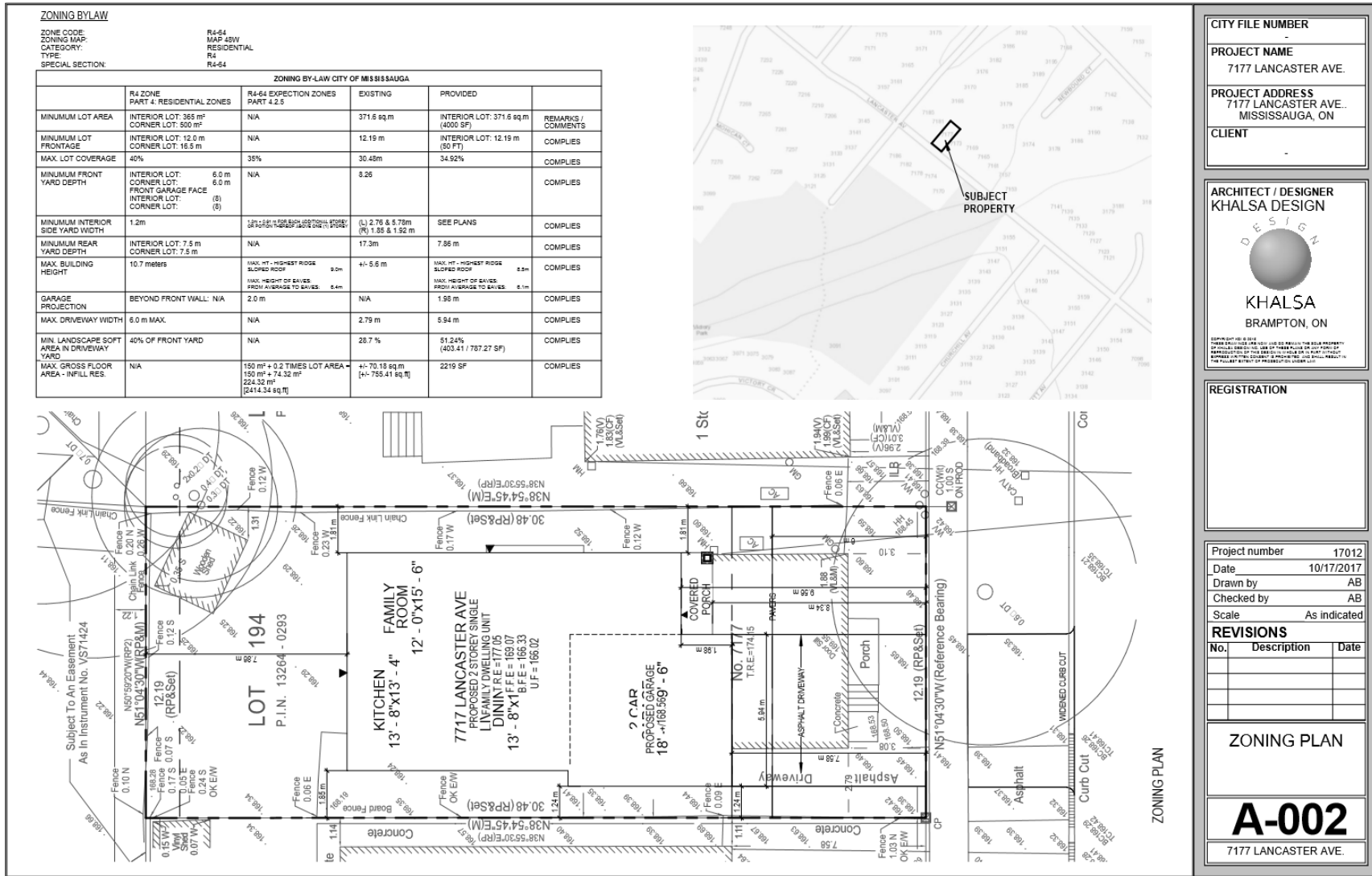


Figure 50 – Zoning Plan A-002. (Image: Khalsa Design)

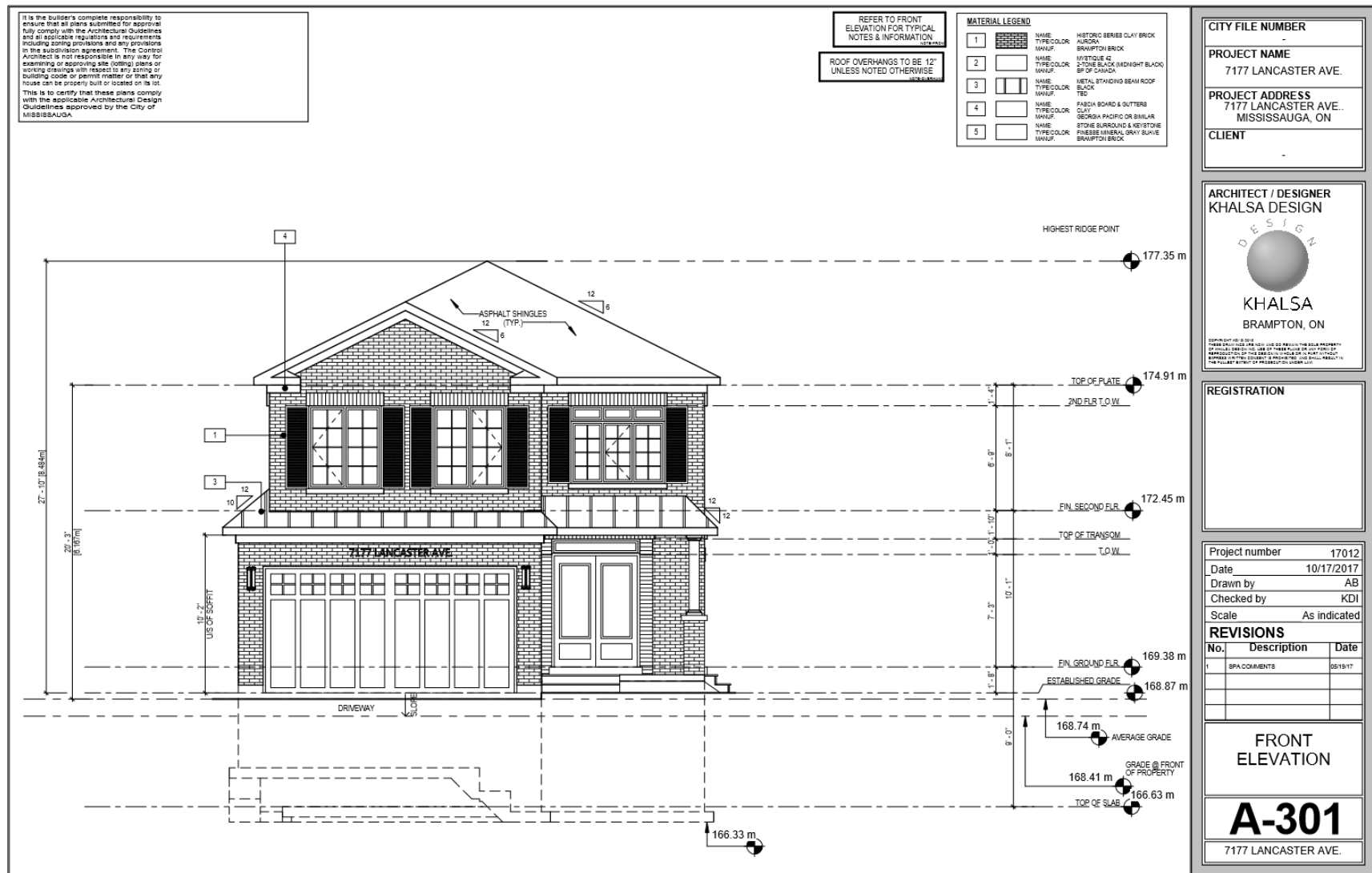


Figure 53 – Front Elevation A-301. (Image: Khalsa Design)

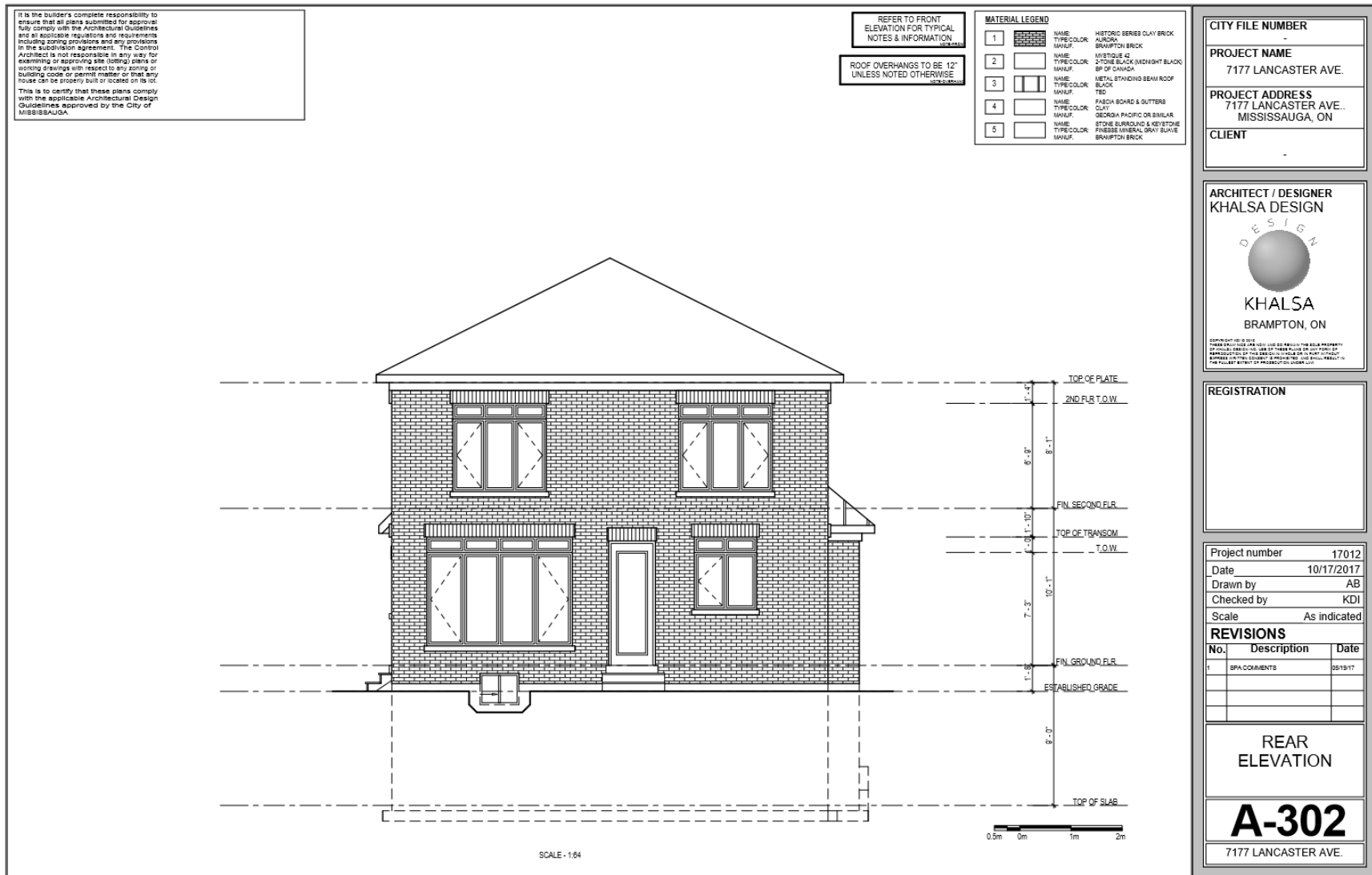


Figure 54 – Rear Elevation A-302. (Image: Khalsa Design)

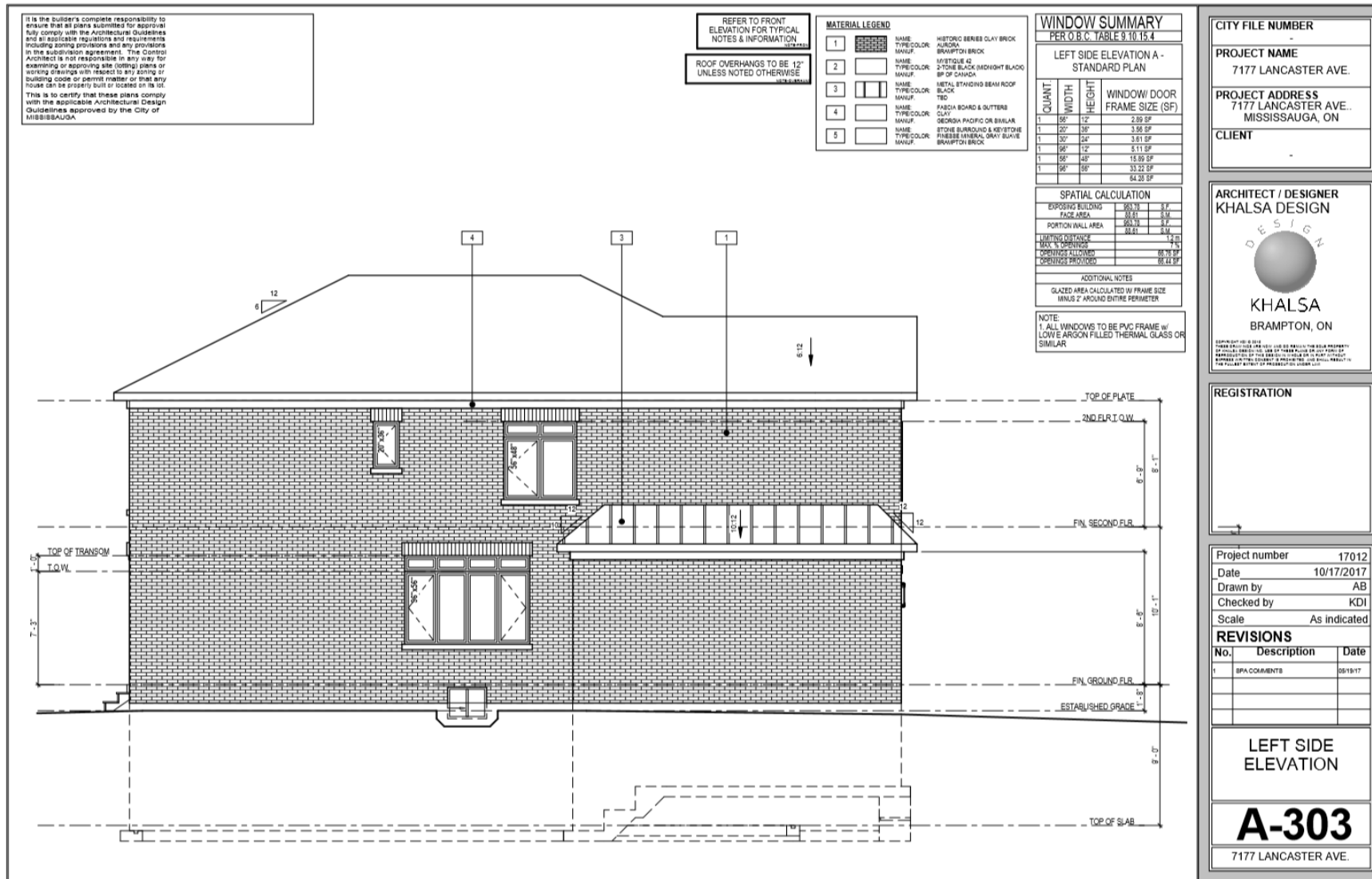


Figure 55 – Left (north) Side Elevation A-303. (Image: Khalsa Design)

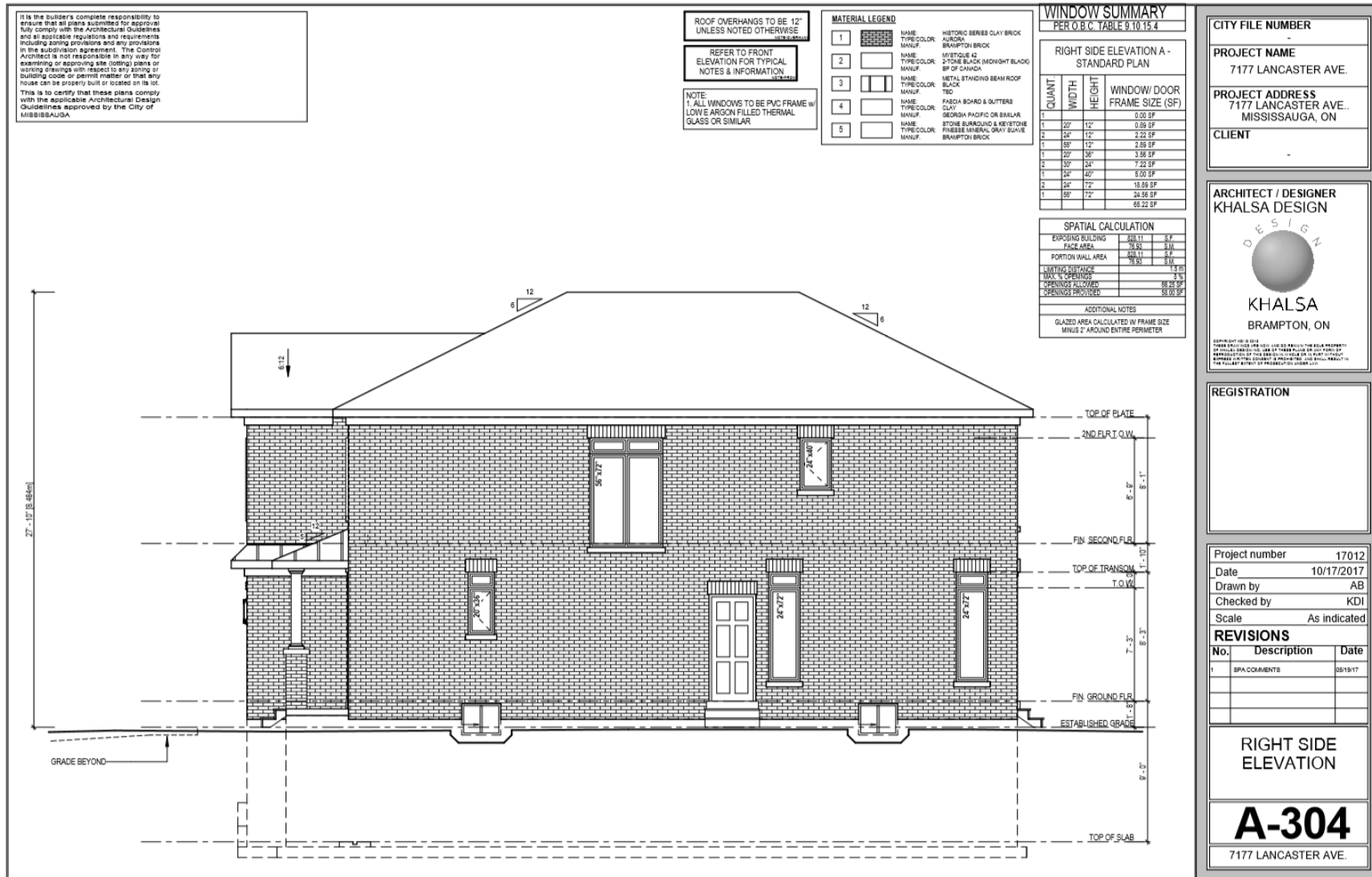


Figure 56 – Right (south) Side Elevation A-304. (Image: Khalsa Design)

10.0 Impacts and Recommended Mitigation

7177 Lancaster Avenue is one of the remaining wartime homes that line this part of Lancaster Avenue and one of the homes that make up the Victory Wartime Housing Cultural Landscape. The demolition of the wartime houses and construction of new modern dwellings that do not relate to the balance of the cultural heritage landscape in design, massing, heights or materials is having a negative impact on the integrity of the cultural heritage landscape. The context has been severely impacted in this area of Lancaster Avenue with at least five newer or altered dwellings being constructed with little or no relationship to the cultural heritage landscape.

The set backs for the new construction is distinctly different from the wartime housing and include attached garages set in front of the house rather than at the side like the wartime housing. The **recognizable pattern and style** is interrupted in this area of Lancaster Avenue. The ultimate mitigation is to limit the new building's height to 1 and 1½-storeys however this is not supported by current zoning by laws. It is only through the establishment of a heritage conservation district and associated design guidelines that true protection can occur for the Victory Wartime Housing Cultural Landscape. At this time, on this particular area of Lancaster Avenue, it may be too late to save the contextual value of the remaining wartime homes. There is very little addition or alteration that these small homes can sustain before losing the character defining elements of their form and the spacious feeling of the lots. The following is an example of the type of design that would more closely fit into the landscape than what has replaced other wartimes homes in this area.

The home at 57 Albert Street in the City of Waterloo (Figure 57) is roughly contemporary to the subject dwelling and could be seen as the type of house that a successful wartime worker might have aspired to move up to when they could afford it. In his suggestions for mitigation of the proposed design in the new house built at 7181 Lancaster Avenue (next door to the subject property) (Figure 22), heritage consultant and planner Paul Dilse (Dilse 2013) had recommended the emulation of 57 Albert Street in the City of Waterloo. The full 2-storey side gable design is mitigated by using light coloured brick on the ground floor and a lighter weight horizontal siding in the upper floor. This design expands the single-storey, three-bay form of the typical “H1” house design. The garage is set back from the house front so that the front door remains prominent. Any expansion of the original form will reduce the sense of spaciousness of the lots that is a defining feature of the cultural heritage landscape the stepped back portion helps mitigate this.

The proposed new dwelling for 7177 Lancaster Avenue has applied some of the recommendations of this report in the choice of lighter coloured brick and arrangement of windows in order to lower the impact with the remaining wartime streetscape but generally is more like the newer houses on the block in height, design and massing.

Further mitigation of the height of the proposed dwelling may be attained by changing the second storey cladding to a wood clapboard-like material in a colour lighter (but complementary to) the brick on



Figure 57 – 57 Albert St., Waterloo. (Image: Martin Chiropractic Clinic, Waterloo; accessed through Google maps, 2017)

the first floor keeping contrasting colour to a minimum. A gable roof form would be more appropriate as well as using coupled 6-over-6 window arrangements and a 3 bay main body of the house stepping back the garage instead of projecting.

However, no matter the design of this particular dwelling it will not serve to mitigate the effect of the other new dwellings on this part of Lancaster Avenue. Given the lack of protection currently in place and the precedent set by the other new dwellings it is recommended that efforts go into the study and creation of a plan for the Victory Wartime Housing Cultural Landscape that protects and provides clear design guidelines for alterations, additions and replacement in this area. New boundaries may need to be determined for the protected area in order to focus on relatively intact portions of the cultural heritage landscape.

11.0 Conclusion

The WWII wartime house located at 7177 Lancaster Avenue does not have significant architectural merit on its own but rather contributes along with the other original homes, setbacks, boulevards, trees and park spaces to the overall heritage character and cultural heritage landscape of the Victory Wartime Housing Cultural Landscape neighbourhood as planned by Wartime Housing Limited.

There is a clear erosion of the Victory Wartime Housing Cultural Landscape cultural landscape as these wartime houses continue to be significantly altered or demolished as discussed in previous reports of neighbouring properties (Dilse 2013; Gillespie 2014). The original homes of the Victory Wartime Housing Cultural Landscape are being replaced with much larger suburban houses that have had no obvious design guidelines so that they have inconsistencies in design, setbacks and materials and no connection or compatibility with the cultural landscape. These small homes and the overall cultural landscape are particularly at risk with no heritage protection through a heritage district as buyers seem to prefer larger homes and the large lots are appealing for buyer to demolish a rebuild upon. The designation of a heritage district under Part V of the Ontario Heritage Act is the only effective way to protect the heritage character of an area at risk. An excellent example of the success in this regard is the St. Mary's Heritage Conservation District of victory homes in Kitchener, Ontario.

The loss of the nearby historical aerospace buildings through demolition (2003, 2004) also removed some of the context for the reason for this neighbourhood's existence. It is recommended that the City of Mississauga Heritage Planning department consider a study to look at: the remaining properties that are representative of the wartime homes and in particular any rows or continuous groups; and review existing protocol to see if there is a way to better inform both staff and the public and encourage retention or sympathetic additions over wholesale demolition; review the Zoning By-law and the Official Plan policies to determine if there are site specific bylaws, site plan controls, zoning restrictions or policies that could aid in the retention of these wartime houses; and develop urban design guidelines to assist with change to the housing stock but also to the tree canopy and open spaces that are character defining elements of the neighbourhood.

It is recommended that the suggested mitigation outlined in section 10.0 be applied to the design of the new house proposed for 7177 Lancaster Avenue to better integrate the replacement infill into the existing neighbourhood.

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13.0 Qualifications of the Author



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=

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Certificate in Heritage Planning, University of Waterloo (1999)
M. A., Canadian Art & Architectural History, Concordia University Montreal (1992)
B. A., University of Toronto and Sheridan College, Oakville (1986)
Senior Heritage Planner, Planning Services, City of Guelph (July 2009 to present)
Cultural Heritage Co-ordinator, Cultural Services, Department of Recreation & Culture, City of Vaughan (March 2005 to June 2009)
Heritage Inventory Researcher, Planning Department, City of Brantford (June 2001 to Feb 2005)

Tracie Seedhouse

Certificate in Heritage Planning, University of Waterloo (1999)
Diploma, Construction Engineering Technology Program, Conestoga College (1993)
Member, Heritage Kitchener, Municipal Heritage Committee (1996-1999)



Robinson Heritage Consulting

Project History

 2017

Little Folks Building, 24 Carlton Place, Elora, Township of Centre Wellington – Heritage Impact Assessment and Conservation Plan
 Client: Elora South Inc., September 2017

“The Gore”, 266 and 280 Northumberland Street, Ayr, Township of North Dumfries – Heritage Impact Statement
 Client: Engel Developments, April 2017

6830 Main Street West, Town of Milton – Heritage Impact Assessment
 Client: Paul De Battista, March 2017

 2016

22 Shade Street, Cambridge – Heritage Impact Assessment
 Client: Salvation Army, August 2016

Reid Farmhouse, 20 Stokes Trail (Campbellville), Milton - Heritage Impact Assessment
 Client: Carson Reid Homes, August 2016

Dickson Public School, 65 St. Andrews Street, Cambridge – Heritage Impact Assessment
 Client: Summerco Properties, May 2016

St. Agnes Anglican Church, 69 Long Branch Boulevard and 24 Marina Avenue, Toronto - Heritage Impact Statement
 Client: Gil Shcolyar, March 2016

4908 Highway 7 (Woodbridge), Vaughan - Heritage Impact Assessment
 Client: Camelot on 7 Inc., January 2016

 2015

Huron Regional Centre, 700 Memorial Avenue, Orillia - Heritage Impact Assessment
 Client: Infrastructure Ontario (represented by MHPM Development Solutions Inc. and DST Consulting Engineers Inc.) December 2015

**Chatham Provincial Courthouse and Walkway, 21 Seventh Street, Chatham -
Heritage Impact Assessment**

Client: Infrastructure Ontario (represented by MHPM Development Solutions Inc. and DST Consulting Engineers Inc.) December 2015

**Cassidy Farmhouse at St. Thomas Psychiatric Hospital, 467 Sunset Drive
- Heritage Impact Assessment**

Client: Infrastructure Ontario; (represented by MHPM Development Solutions Inc. and DST Consulting Engineers Inc.) December 2015

York Detention Centre, 354 George Street, Toronto - Heritage Impact Assessment

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Employer: City of Guelph, October 2015

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Client: City of Brampton, June 2015

Lot 22 Concession 9 Bridge, Township of Windham (Norfolk County) – Cultural Heritage Evaluation Report

Client: County of Norfolk, and G. Douglas Valee Limited, March 2015

Fergus High School, 680 Tower Street, (Fergus) Township of Centre Wellington - Scoped Heritage Impact Assessment and Conservation Plan

Client: Emmanuel Christian High School, February 2015

2 William Street, Elmira - Heritage Impact Assessment

Client: Scott and Libby Playford, January 2015

2014

Herb & Elsie Crawford Farm, Brampton – Heritage Impact Assessment

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Silvercreek Farm, Caledon – Review of Reasons for Designation

Client: Town of Caledon, August 2014

111 Mary Street, Milton - Heritage Impact Assessment

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New Toronto Hydro Substation, 124 Birmingham Street, Toronto - Heritage Impact Statement

Client: 5th Essential Inc., April 2014

2013**150 King Street South, Waterloo – Heritage Impact Assessment**

Client: ABA Architects Inc., December 2013

58 Richmond Street, Richmond Hill - Cultural Heritage Impact Statement

Client: Alex Boros Planning + Design Associates, December 2013

Bob Devereaux Bridge, County of Brant – Cultural Heritage Evaluation Report

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Client: County of Norfolk, and G. Douglas Valee Limited, July 2013

“Heritage Square” Condominium, Fergus – Heritage Impact Assessment

Client: Jennark Homes Ltd., May 2013

1683 Huron Road, Kitchener – Conservation Plan

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2012**9307 Union Drive, Strathroy-Caradoc – Heritage Impact Assessment**

Client: Canadian Solar Developers and Exp Inc., Renewal Energy Approval, September 2012

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1216 Penetanguishene Road, Township of Springwater – Heritage Impact Assessment

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Employer: City of Guelph, June 2012

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Client: Evans Planning, February 2012

2011

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Dolby Garage, 6009 Regional Road 25, Milton – Heritage Impact Assessment

Client: Regional Municipality of Halton, April 2011

2485 Conservation Road, Milton - Heritage Impact Assessment

Client: K. Strobele, February 2011

2010

5761 First Line, Milton - Heritage Impact Assessment

Client: Scrap Metal Depot Inc., November 2010

61 Usher Street, Brantford - Heritage Impact Assessment

Client: First Home Construction Inc., July 2010

Alexandra School, 1525-7th Ave. E., Owen Sound – Cultural Heritage Property Evaluation

Client: Bluewater District School Board, May 2010

124 Birmingham Street, Toronto – Heritage Impact Assessment

Client: City of Toronto Economic Development Corporation, March 2010

2009

8656 Creditview Road, Brampton - Heritage Research Report

Client: Phillip H. Carter Architect, December 2009

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Client: Glen Schnarr & Associates, November 2009

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Client: Loedige (Canada) Limited, October 2009

8763 Bayview Avenue, Richmond Hill - Heritage Impact Assessment

Client: Signature Developments Inc., July 2009

1524 Countryside Drive, Brampton - Heritage Impact Assessment

Client: City of Brampton, July 2009

418 Glasgow Street, Kitchener - Heritage Impact Assessment

Client: Doug Cornwell, June 2009

7435 Ninth Line, Mississauga - Heritage Impact Statement

Client: ProLogis Canada and Erin Mills Development, April 2009

340 Oak Street, Milton - Heritage Impact Assessment

Client: 52457 Ontario Limited, April 2009

501 and 511 John Street, Burlington – Heritage Impact Assessment

Client: Carriage Gate Group Inc., and Millington & Associates, February 2009

11859 Hurontario Street, Brampton – Heritage Impact Assessment

Client: Dinesh Patel, January 2009

2008

47-49 Alice Street, Guelph – Expert Witness Statement and Testimony provided for Conservation Review Board Hearing (CRB0816)

Client: City of Guelph, December 2008

1571 Fisher Hallman Road, Kitchener – Salvage Documentation Report

Client: Mattamy Homes Ltd., November 2008

Branningham Grove, 2010 16th Street East – Cultural Heritage Property Evaluation

Client: City of Owen Sound, October 2008

12 Henderson Avenue, Brampton – Heritage Impact Assessment

Client: 1753849 Ontario Inc., October 2008

318 Guelph Avenue, Cambridge – Heritage Assessment

Client: Doug Craig, Mayor of Cambridge, June 2008

48 George Street North, Cambridge – Heritage Impact Assessment

Client: Maison Canada Holdings Ltd., May 2008

27-31 Cambridge Street, Cambridge – Heritage Impact Assessment

Client: Techno Steel Canada, April 2008

1120 Bovaird Drive West, Brampton – Heritage Impact Assessment

Client: Weston Consulting Group Inc., March 2008

2007

St. Mary's High School – Heritage Documentation Report

Client: Bruce Grey Catholic District School Board and SRM Architects Inc., December 2007

Fergus High School – Heritage Impact Assessment

Client: Reid's Heritage Homes, December 2007

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33 Southwood Drive, Cambridge – Heritage Impact Assessment

Client: Geoffrey Reid, September 2007

Carnegie Public Library, Owen Sound – Reasons for Designation

Client: City of Owen Sound, September 2007

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Client: City of Owen Sound, September 2007

1683 Huron Road, Kitchener – Heritage Impact Assessment

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Preston Meadows, 633 Margaret Street, Cambridge – Heritage Impact Assessment

Client: Reid's Heritage Homes, in collaboration with Stantec Consulting, April 2007

443 Dover Street North, Cambridge – Heritage Impact Assessment

Client: Carl Csanits, January 2007

2006

Barber Paper Mill, Town of Halton Hills – Heritage Impact Assessment

Produced in collaboration with The Ventin Group Architects

Client: Everlast Restoration, December 2006

806 Gordon Street, Guelph – Heritage Documentation Report

Client: Mar-Cot Homes Ltd., November 2006

Revue Theatre, Roncesvalles Avenue, Toronto – Heritage Documentation Report

Client: Chris McQuillan, September 2006

Interpretive Plaque Project on Queen Street, Cambridge (Hespeler)

Client: Heritage Cambridge, July 2006

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Queen's Hotel, Owen Sound – Reasons for Designation

Client: City of Owen Sound, April 2006

299 & 313 Plains Road W., Burlington – Heritage Impact Assessment

Client: Recchia Developments Inc., and Greg Poole & Associates, February 2006

246 Crawley Road, Guelph – Heritage Impact Assessment

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Client: Wilfrid Laurier University and Brant Historical Society, 2006

2005**148 Crawley Road, Guelph – Heritage Impact Assessment**

Client: Royal-LePage Commercial, June 2005

Brantford Heritage Inventory

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Employer: Brantford Planning Department, June 2001 to February 2005

2004**63-67 Woolwich Street, Guelph – Heritage Documentation Report**

Client: Wellington Catholic District School Board, February 2004

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Assessment of heritage bridges within the Grand River watershed

Client: Grand River Conservation Authority, 2004

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produced with Tracie Seedhouse for the *Keys to History* series

Client: Guelph Civic Museum / McCord Museum, Montreal, April 2004

2003**Brant Arts, Culture & Heritage Centre (BACH Project)**

Heritage assessments for Roger Jones & Associates and The Ventin Group Architects

Client: BACH Steering Committee, September 2003

340 Clair Road, Guelph – Heritage Documentation Report

Produced in association with The Ventin Group Architects

Client: Reid's Heritage Homes, July 2003

1471 Gordon Street, Guelph – Heritage Documentation Report

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341 Forestell Road, Guelph – Heritage Documentation Report

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Client: City of Guelph, September 2002

Heritage Sampler and An Interactive Guide to Tremaine's Map of County of Waterloo, 1861

Client: Waterloo Regional Heritage Foundation, 2002 (compact disc)

City of Mississauga
Corporate Report



Date: December 7, 2017

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
 Community Services

Originator's files:

Meeting date:
 2018/01/09

Subject

Request to Demolish a Heritage Listed Property: 1775 Thorny Brae Place (Ward 8)

Recommendation

That the property at 1775 Thorny Brae Place, which is listed on the City's Heritage Register, is not worthy of heritage designation, and consequently, that the owner's request to demolish proceed through the applicable process.

Background

Section 27.3 of the Ontario Heritage Act states that structures or buildings on property listed on the City's Heritage Register cannot be removed or demolished without at least 60 days' notice to Council. This legislation allows time for Council to review the property's cultural heritage value to determine if the property merits designation.

The owner of the subject property has submitted a heritage permit application to demolish and replace the existing detached dwelling. The subject property is listed on the City's Heritage Register as it forms part of the Mississauga Road Scenic Route Cultural Landscape. This cultural landscape is significant due to its scenic and visual quality as the road traverses a variety of topography and land use, from old established residential neighbourhoods to new industrial and commercial uses. Its landscape is of archaeological, design, technological interest as well as having historical interest and associations, illustrating important phases of Mississauga's history and displaying a consistent scale of built features.

Comments

The owner of the subject property has requested permission to demolish the existing structure. The applicant has provided a Heritage Impact Assessment, attached as Appendix 1. The consultant has concluded that the structure at 1775 Thorny Brae Place is not worthy of designation. Staff concurs with this finding.

Financial Impact

There is no financial impact.

Conclusion

The owner of 1775 Thorny Brae Place has requested permission to demolish a structure on a property that is listed on the City's Heritage Register. The applicant has submitted a documentation report which provides information which does not support the building's merit for designation under the Ontario Heritage Act. Staff concurs with this finding.

Attachments

Appendix 1: Heritage Impact Assessment



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: P. Wubbenhorst, Senior Heritage Coordinator

City Of Mississauga
HERITAGE IMPACT ASSESSMENT
FOR
1745, 1765 and 1775 THORNY-BRAE PLACE
Mississauga, Ontario



Credit: RN design, 2016

SBA Project No. A15094

FINAL November 16th 2017



Stevens Burgess Architects Ltd.

40 St. Clair Avenue East, Ste. 301, Toronto, ON. M4T 1M9 T: 416-961-5690 F: 416-972-6417 E-mail: office@sba.on.ca

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APPENDICES:

Appendix A: *City of Mississauga Heritage Impact Assessment Terms of Reference, dated October 2014*

Appendix B: *City of Mississauga Cultural Landscape Heritage Impact Assessment (HIA Terms of Reference, undated*

Appendix C: Qualifications of the Author

Appendix D: Architectural Drawings provided by Armstrong Planning and Project Management

Appendix E: Landscape Plan provided by Armstrong Planning and Project Management

Heritage Impact Assessment

1745, 1765 and 1775 Thorny-Brae Place, Mississauga

1.0 BACKGROUND

In November 2015 Stevens Burgess Architects Ltd. (SBA) was contacted by Amanda Kosloski of Armstrong Planning and Project Management on behalf of Peter Sciavilla of Pace Developments to prepare a Heritage Impact Assessment (HIA) for 1745, 1765 and 1775 Thorny-Brae Place in Mississauga.

The scope of the heritage assessment included the proposed demolition of the two existing houses at 1765 and 1775 as well as the redevelopment of the site. No building exists on the property associated with 1745. As per the City of Mississauga's Property Information website the follow is applicable for each address:

- **1745 Thorny Brae Place** is listed as part of the Credit River Corridor under on the Heritage Register but is not designated.
- **1765 Thorny Brae Place** is not listed on the Heritage Register.
- **1775 Thorny Brae Place** is listed as part of the Mississauga Road Scenic Route and Credit River Corridor under on the Heritage Register but not designated.

On February 23rd, 2016 a visual and photographic review of the exterior of the two existing houses and site was conducted by SBA. The HIA was prepared in accordance with the Mississauga Heritage Impact Assessment Terms of Reference, dated October 2014 (**refer to Appendix A**) and Cultural Landscape Heritage Impact Assessment Terms of Reference, undated (**refer to Appendix B**)



Figure 1: Location Map

Credit: Google Maps 2016, Annotated by SBA

Heritage Impact Assessment1745, 1765 and 1775 Thorny-Brae Place, Mississauga

2.0 HERITAGE IMPACT ASSESSMENT**2.1 Site History including List of Owners¹**

Until the 1960s, the property at 1745 – 1775 Thorny Brae Place was rural, with a long laneway leading from Mississauga Road to a building complex. An orchard graced the land between the buildings and the road. Eglinton Avenue was a dead end right-of-way and there was no bridge across the Credit River at this location. **Refer to Figure 2: Aerial Photography dated 1954.**

In 1938 Arthur Dinning sold the property to Albert C. Parker and Lelia E. Parker as joint tenants. The Parkers held the property (jointly until 1950 and Lelia Parker until 1958) and presumably farmed it until 1958. In 1956 a City of Mississauga by-law was passed permitting “Re-Subdiv.” and two lots; 1765 and 1775 were created and sold to Rudolf Kreppner.

In the 1960s the Eglinton Avenue bridge was constructed over the Credit River and the property was subdivided into single family lots, surrounding a short cul-de-sac on the alignment of the former farm land. Four houses were constructed between 1958 and 1966; including 1765 and 1775 Thorny-Brae Place, and a fifth house was located at the end of the cul-de-sac in 1966. **Refer to Figure 3: Aerial Photography dated 1966.**

By 1985, the orchard that occupied the lands south of the subject property had been replaced by the Roman Catholic Church of Croatian Martyrs with its large asphalt parking lot on the table land above the river. **Refer to Figures 4 and 5: Aerial Photography dated 1985 and 1989 respectively.**

The Eglinton Avenue bridge and the street had been widened to six lanes by 1995. **Refer to Figure 6: Aerial Photography dated 1995.** Lands north of Eglinton had ceased to be agricultural and were developed as residential as were those across Mississauga Road (opposite the subject property) in the intervening ten years.

1765 Thorny-Brae was owned by Alfred Bentley from 1956-1961 and subsequently the Gizzarellis’ from 1961-2005. 1775 Thorny-Brae was owned by the Chouinards from 1964-1981, the Travers from 1981-1983 and the Cusciannas until 2005.

Since 1995 little has changed except that the development at the easterly end of the subject property at the end of the cul-de-sac bulb, has been demolished and that portion of the site is vacant.

Since 2006, **(Refer to Figure 7: Aerial Photography dated 2006)**, the property has transferred owners multiple times in what is assumed to be the acquisition of 1745-1775 for future development.

¹ Site history compiled from Heritage Impact Assessment 1745-1775 Thorny-Brae Place, Mississauga, prepared by The Landplan Collaborative Ltd., June 22, 2009 and Land Registry Research, 2015.

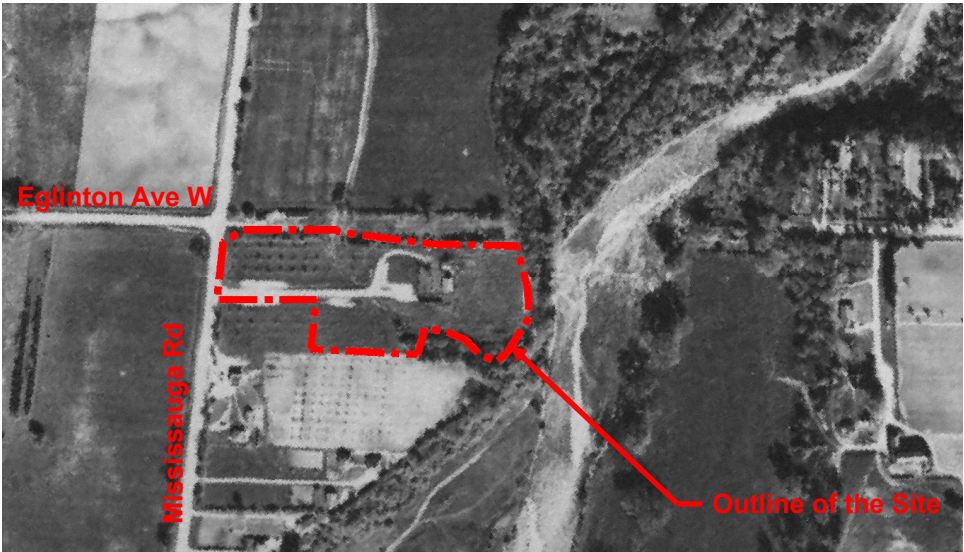


Figure 2:
1954 Aerial Photography
Credit: City of Mississauga, eMaps

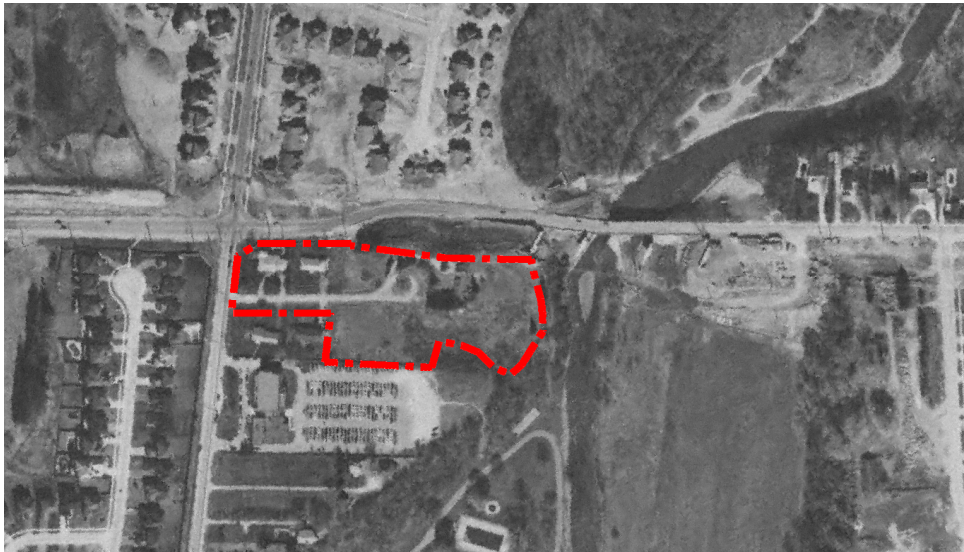


Figure 5:
1989 Aerial Photography
Credit: City of Mississauga, eMaps

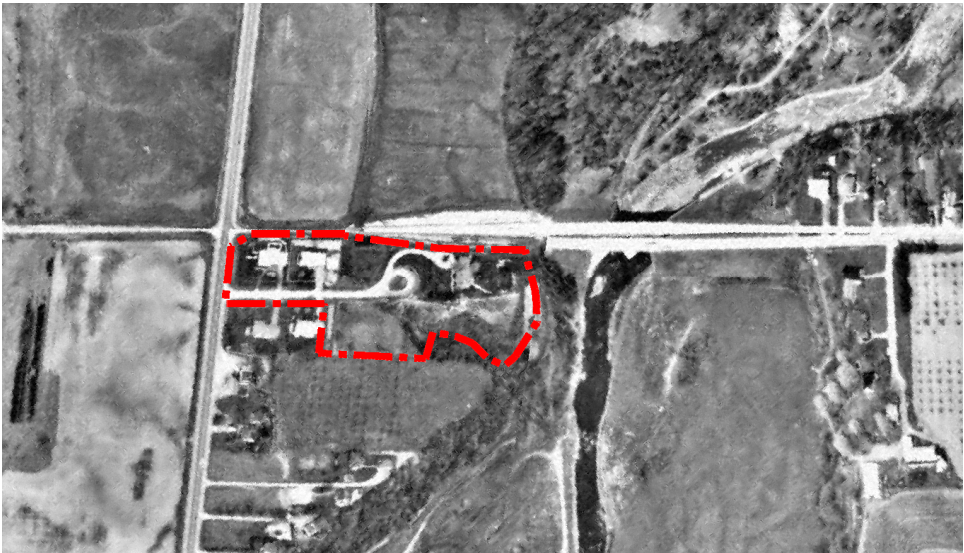


Figure 3:
1966 Aerial Photography
Credit: City of Mississauga, eMaps

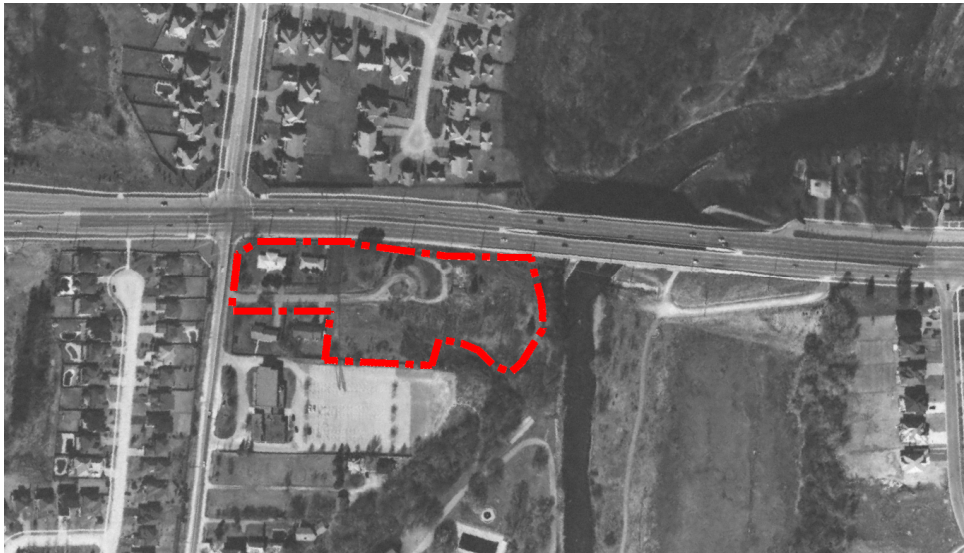


Figure 6:
1995 Aerial Photography
Credit: City of Mississauga, eMaps

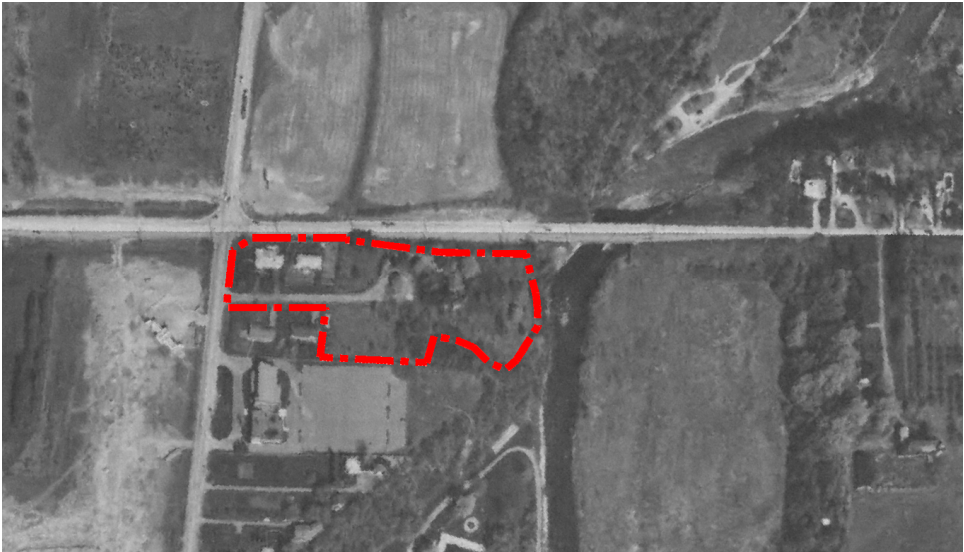


Figure 4:
1985 Aerial Photography
Credit: City of Mississauga, eMaps



Figure 7:
2006 Aerial Photography
Credit: City of Mississauga, eMaps

Heritage Impact Assessment

1745, 1765 and 1775 Thorny-Brae Place, Mississauga

List of Owners* (1765 Thorny-Brae Place: Lt 2, PL 498, 13383-023 (LT))	
Date	Owner *
2015 to present	Information not provided to conform with the Freedom of Information and Protection of Privacy Act
2015	Diversified Capital Inc.
2010	Hush Homes Inc.
2006	Berkley Homes (Mississauga Rd.) Inc.
2006	DLNS Investments Inc. DLNS Holdings (Mississauga) Inc. Gennaro Di Santo Maria Mancuso
2005	Michael Gizzarelli
1961	Michael and Antonietta Gizzarelli
1956	Alfred Bentley
1956	Rudolf Kreppner
1956	Re-Subdiv. Control By-Law
1938	Albert C. Parker and Lelia E. Parker
prior to 1938	Arthur Dinning
*List of Owners compiled from Land Registry Office at 1 Gateway Blvd, Brampton, ON L6T 4X2	

Figure 8: List of Owners, 1765 Thorny Brae Place: Lt 2, PL 498, 13383-023 (LT))
Credit: Land Registry Office, 2015

List of Owners* (1775 Thorny-Brae Place: Lt 1, PL 498, 13383-022 (LT))	
Date	Owner *
2015 to present	Information not provided to conform with the Freedom of Information and Protection of Privacy Act
2015	Diversified Capital Inc.
2010	Hush Homes Inc.
2006	Berkley Homes (Mississauga Rd.) Inc.
2005	DLNS Investments Inc. DLNS Holdings (Mississauga) Inc. Gennaro Di Santo Maria Mancuso
1983	Michael and Angelou Cuscianna
1981	Kevin and Donna Travers
1964	Emer and Eleanor Chouinard
1956	Rudolf Kreppner
1956	Re-Subdiv. Control By-Law
1938	Albert C. Parker and Lelia E. Parker
prior to 1938	Arthur Dinning
*List of Owners compiled from Land Registry Office at 1 Gateway Blvd, Brampton, ON L6T 4X2	

Figure 9: List of Owners, 1775 Thorny Brae Place: Lt 1, PL 498, 13383-022 (LT))
Credit: Land Registry Office, 2015

Heritage Impact Assessment

1745, 1765 and 1775 Thorny-Brae Place, Mississauga

2.2 Written Description of Site and Heritage Resources

The site is located at the south east corner of Eglinton Ave. West and Mississauga Road. Residential development (1990s) exists north and northwest of the property across Eglinton Avenue. To the west of the property and across Mississauga Road the 1980's residential development is partially obscured by fencing flanking the east side of Mississauga Road. The south side of Thorny-Brae Place is flanked by two poorly maintained 1960's bungalows and the Roman Catholic Church of the Croatian Martyrs.

The site is bordered on the east side by the Credit River Valley (part of the Credit River Corridor Cultural Landscape) and on the west side by Mississauga Road (part of the Mississauga Road Scenic Route Cultural Landscape).

Any subsequent site development must take into consideration both the heritage resources of the Credit River Corridor and the Mississauga Road Scenic Route Cultural Landscapes.



Figure 10: Credit River Corridor and Mississauga Road Scenic Route Cultural Landscapes

Credit: Base Image, Heritage Impact Statement 1745 – 1775 Thorny-Brae Place, Mississauga, prepared by The Landplan Collaborative Ltd. June 2009, Additional annotations by SBA, 2016

Heritage Impact Assessment

1745, 1765 and 1775 Thorny-Brae Place, Mississauga

2.3 Written Description of Structures and Heritage Resources

There are two existing structures on the property; namely 1765 and 1775 Thorny-Brae Place, with the remaining area of the site being vacant. Both houses are 1960s bungalows, neither house is included on the City's Heritage Register or within a Heritage Conservation District, and both residences do not exhibit elements of heritage significance.

Each bungalow and associated property is overgrown, neglected and 1775 Thorny-Brae Place especially appears to have become dumping grounds for bags of garbage overflowing from its open garage door as well as an assortment of debris on its front, side and rear lawns. Subsequent to the site visit in February 2016 a fire occurred at 1775 Thorny-Brae in March 2016 and the house was deemed unsafe by the fire department as per the Fire Incident Report dated March 21, 2016. The house has been boarded up for safety until a demolition permit is obtained. Due to the fire and the inability to access the interior of this structure, interior photographs are not available.

As 1765 Thorny-Brae Place is not listed on the Heritage Register, interior photographs were not requested nor reviewed.

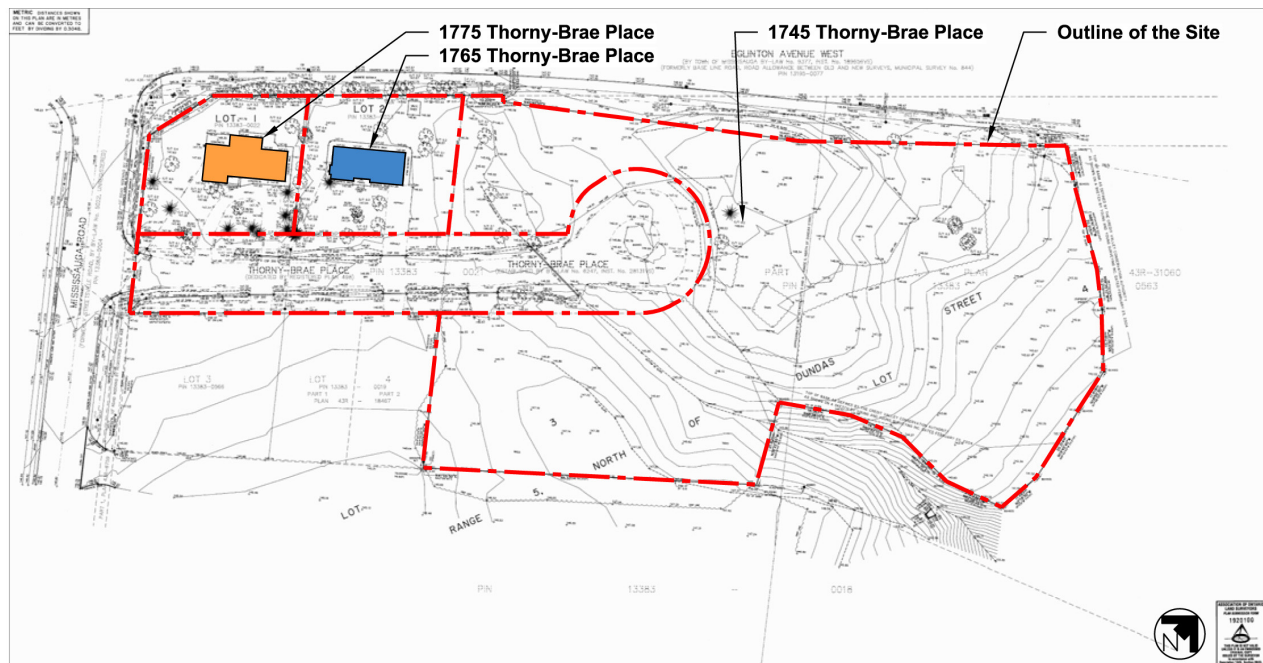


Figure 11: Survey of Site

Credit: Provided by Armstrong Planning and Project Management, dated July 21, 2015, Annotated by SBA, 2016

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Figure 12: Front Elevation of 1765 Thorny-Brae Place
Credit: SBA, 2016



Figure 13: Front Elevation of 1775 Thorny-Brae Place (prior to fire)
Credit: SBA, 2016



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2.4 Outline of the Proposed Development and Impact on Heritage Attributes

The Heritage Impact Assessment was prepared for the demolition of the two remaining structures on the property, namely the two 1960s bungalows in anticipation of a proposed redevelopment of the property. Neither structure has heritage attributes to merit their conservation.

The development proposal consists of one 2 storey detached home, located at the corner of Mississauga Road and Eglington Ave. West, and a series of 3 blocks (Blocks 1 to 3) of three storey townhouses fronting Eglington Ave. West. The proposed internal street pattern follows the current street alignment with two blocks (Blocks 4 and 5) of 2 storey townhouses fronting the cul-de-sac bulb and 2 additional blocks (Blocks 6 and 7) of 2 storey townhouses fronting Thorny-Brae Place. Refer to **Figure 15**.

Residential development (1990s) exists both north and northwest of the property across Eglington Ave. West. To the west of the property and across Mississauga Road the 1980's residential development is partially obscured by fencing flanking the east side of Mississauga Road. The south side of Thorny-Brae Place is flanked by two poorly maintained 1960's bungalows (signed for future redevelopment) and the Roman Catholic Church of the Croatian Martyrs. The proposed massing, masonry veneer and asphalt shingled roofs are in keeping with the neighbouring residential developments.

The site is bordered on the east side by the Credit River Valley (part of the Credit River Corridor Cultural Landscape) and on the west side by Mississauga Road (part of the Mississauga Road Scenic Route Cultural Landscape). The townhouses are set well back from the Credit River Valley and will not be visible from the valley. The front entrance and covered porch of the detached corner unit is located on Mississauga Road.



Figure 15: Development Site Plan overlaid on Google Map, 2017
Credit: Concept Plan; RN design, 2017, Additional Annotations; SBA, 2017

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Figure 16: Proposed Development, 2016
Credit: RN design, 2016



Figure 17: View from corner of Eglinton Ave. West and Mississauga Road
Credit: RN design, 2016



Figure 18: Measured Streetscape along Mississauga Road
Credit: RN design, September 2017

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Figure 19: View from corner of Eglinton Ave. West and Mississauga Road looking north on Eglinton
Credit: SBA, 2016



Figure 20: Residential area on north side of Eglinton Ave. West
Credit: SBA, 2016

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Figure 21: Proposed Development, Mississauga Road Elevation (East side of Road)
Credit: RN design, 2016



Figure 22: Corner of Eglinton Ave. West and Mississauga Road looking south on Mississauga Road
Credit: SBA, 2016



Figure 23: West Side of Mississauga Road
Credit: SBA, 2016

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Figure 24: Credit River Valley (site beyond trees)
Credit: SBA, 2016



Figure 25: View/Steps into Credit River Valley
Credit: SBA, 2016

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2.5 Assessment of Proposed Development and Mitigation Measures

The **Mississauga Road Scenic Route Study** provides policies related to adjacent development. Those that apply to this site are noted below²:

- *Direct frontage lots with direct access or flanking lots with front doors facing Mississauga Road will be encouraged.*

The proposed development places the residential buildings to address the street with pedestrian connection to front doors and landscaped front yards. This will also enhance the scenic quality of Mississauga Road and Eglinton Ave. West.

- *Existing lot frontages in the range of 15m (49 ft) to 33 m (108 ft) on residential lands abutting Mississauga Road as determined through the existing zoning standards shall be retained.*

The single dwelling unit facing Mississauga Road provides a lot frontage of over 40m including an elevation that is consistent with neighbouring residential properties.

- *Building setback from Mississauga Road including garages should be consistent with buildings on surrounding lots.*

The single dwelling unit facing Mississauga Road is consistent with neighbouring residential properties.

- *Projecting garages will be discouraged.*

Projecting garages are not included in the proposed development.

- *Alternative on-site turn arounds such as hammerhead driveways will be encouraged to reduce reverse movements and the number of driveway entrances.*

A cul-de-sac is provided within the proposed development. This is consistent with the original 1960's cul-de-sac style driveway.

² Mississauga Road Scenic Route Study, Prepared by the Study of Mississauga Planning and Building Department, May 1997.

The site plan illustrates the proposed development at 1000 Eglinton Avenue West. The plan includes several building footprints, each labeled with the number of units it contains: Block 1 (6 units), Block 2 (6 units), Block 3 (7 units), Block 4 (3 units), Block 5 (3 units), Block 6 (5 units), Block 7 (4 units), Block 8 (5 units), and Block 9 (4 units). The buildings are arranged around a central circular area, which is likely a courtyard or a roundabout. The plan also shows the location of stone pillars, a black decorative metal fence, a black vinyl chain link fence, and a 10m LTSSL buffer. The surrounding area includes Eglinton Avenue West, Thorny Brae Place, and the Credit River Corridor. The plan also shows the location of the top of the bank and the Credit River Corridor.



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The **Credit River Corridor Cultural Landscape Inventory** provides criteria in which the proposed development must address and demonstrate how the features/criteria of the specific cultural landscape will be conserved. Those that apply to this site (Credit River Corridor) are noted below³:

Landscape Environment

- *Scenic and Visual Quality*

This quality may be both positive (resulting from such factors as a healthy environment or having recognized scenic value) or negative (having been degraded through some former use, such as a quarry or an abandoned, polluted or ruinous manufacturing plant). The identification is based on the consistent character of positive or negative aesthetic and visual quality. Landscapes can be visually attractive because of a special spatial organization, spatial definition, scale or visual integrity.

Currently, the river bank adjacent to the site is an almost vertical shale cliff of considerable height with dense foliage along the edge. These natural features contribute to the positive aesthetic and visual quality of the Credit River Corridor in which the consistent character of natural landscape is maintained along the river edge and river banks and associated setbacks. As per the Concept Landscape Plan provided, the natural lands will be undisturbed. To promote and increase the visual quality and character, new native deciduous and coniferous trees are proposed within the buffer area.

- *Natural Environment*

Natural history interest can include such features as the remnants of glacial moraines, shoreline features of former water courses and lakes, and concentrations of distinct features such as specific forest or vegetation types or geological features. Remnants of original pre-settlement forests would fall into this category.

The back portion of the property contributes to the Credit River Corridor. As illustrated in **Figure 24 and 25**, rising from the river's edge is an almost vertical shale cliff of considerable height with dense foliage along the edge which then dissipates to an open field. It is assumed that these features contribute to its natural environment. As per the Concept Landscape Plan provided, the natural lands will be undisturbed.

- *Landscape Design, Type and Technological Interest*

This includes complete landscapes that were designed for a specific use or single purpose. These landscapes are characterized by their design intent or urban function i.e. stormwater management. These landscapes are valued in the community by association of use and/or contribution to the visual quality of the community.

The proposed development includes a generous setback from the top of the bank that encompasses the natural lands. Setbacks vary based on the proposed development from a minimal distance of $\pm 16.9\text{m}$ to $\pm 23.3\text{m}$. **Refer to Figure 15**. The river bank adjacent to the site is an almost vertical shale cliff of considerable height with dense foliage along the edge. As per the Concept Landscape Plan provided, the natural lands will be undisturbed.

³ Cultural Landscape Inventory, Prepared by The Landsplan Collaborative Ltd. in association with Goldsmith Borgal & Company Ltd., Architects, North South Environmental Inc. and Geodata Resources Inc., January 2005.

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Historical Association

- *Direct Association with Important Person or Event*

Some sites are rather simple or prosaic in nature. However, great events can happen in a field or in a hut. Famous persons may inhabit or major events may happen in unexpected locations. Preservation of such sites is important to the public's understanding of history and of itself.

There are two remaining structures on the site. Neither structure is associated with the original owner of the site, original orchards or farming.

- *Illustrates Important Phase in Mississauga's Social or Physical Development*

A site may be evocative or representative of a phase or epoch in the development of the City. Such remnants provide context for an on-going understanding of the development of the community.

Both structures are reflective of the 1960s typical suburban development and do not illustrate an important phase in Mississauga's social or physical development.

Other

- *Historical or Archaeological Interest*

Cultural heritage resources associated with pre-historical and historical events.

Research of the land and building owners did not identify any associations with pre-historical or historical events.

- *Outstanding Features/Interest*

A one-of-a-kind feature that is set apart from other similar landscapes or features because of its context or some other special quality i.e. the first of its kind or the acknowledged best of its kind.

The landscape of almost vertical shale cliff rising to a dense cliff of foliage is a similar landscape along the Credit River Corridor and should be conserved. As per the Concept Landscape Plan provided, the natural lands will be undisturbed.

- *Significant Ecological Interest*

Having value for its natural purpose, diversity and educational interest.

The back portion of the property significantly contributes the Credit River Corridor and the natural purpose, diversity and education interest that is associated with it. These features should be conserved. As per the Concept Landscape Plan provided, the natural lands will be undisturbed maintaining the natural purpose, diversity and educational interest that is associated with it.

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2.6 Summary of Conservation Principles and How They will be Used

The *Standards and Guidelines for the Conservation of Historic Places in Canada (Standards and Guidelines)* is a key reference document for the development of heritage impact assessments. The *Standards and Guidelines* describe the principles and practices that encourage the long-term conservation of Canada's historic places based on sound, practical guidance. A second purpose of the Standards and Guidelines was to develop a Canadian set of Standards and Guidelines that could be adopted by federal, provincial, territorial or other authorities as a benchmark for assessing proposed conservation interventions. The objective for the conservation of a historic place is to meet functional goals while respecting the site/building heritage value and character-defining elements. This 'minimal intervention' approach is the foundation of good conservation practice.

The *Standards and Guidelines* establishes three conservation treatments: preservation: the action or process of protecting, maintaining and stabilizing the existing form, materials, and integrity of an historic place, or of an individual component, while protecting its heritage value; rehabilitation: the action or process of making possible a continuing or compatible contemporary use of an historic place through repair, alterations, and/or additions, while protecting its heritage value; and thirdly restoration: the action or process of accurately revealing, recovering or representing the state of a historic place or of an individual component, as it appeared at a particular period in its history, while preserving its heritage value.

In addition to the above, the *Eight Guiding Principles in the Conservation of Historic Properties, Ontario Ministry of Culture* are ministry statements in the conservation of built heritage properties and are based on international charters which have been established over the century. These principles provide the basis for all decisions concerning good practice in heritage conservation around the world. Principles explain the "why" of every conservation activity and apply to all heritage properties and their surroundings.

The above standards, guidelines and principles were developed to address the conservation of heritage structures however the two bungalows are not considered significant.

2.7 Proposed Demolition/Alterations Explained

Demolition of the two 1960's vacant bungalows is being proposed. Neither structure is considered to have historic significance.

Due to the fire that occurred at 1775 Thorny-Brae in March 2016 and the follow up Fire Incident Report dated March 21, 2016, the house has been boarded up for safety until a demolition permit is obtained.

2.8 Alternatives for Salvage Mitigation

Alternatives for salvage mitigation not applicable.

Heritage Impact Assessment1745, 1765 and 1775 Thorny-Brae Place, Mississauga

3.0 SUMMARY STATEMENT AND CONSERVATION RECOMMENDATIONS

There are no heritage structures on the property. The cultural heritage significance is the location of the property along the Mississauga Road Scenic Route Cultural Landscape as well as portion of the site within the Credit River Corridor Cultural Landscape. Accordingly the demolition of the two structures has no heritage impact to the site.

The proposed development does not impact on the cultural heritage resources (Mississauga Road Scenic Route and Credit River cultural Landscape). As per the urban design guidelines outlined in the Mississauga Road Scenic Route Study, the following mitigation measures are included in the proposed development:

- Direct frontage lots, front facades and landscaping that addresses both Mississauga Road and Eglinton Ave. West.
- Lot frontages consistent with the existing residential lot pattern along Mississauga Road;
- Retaining the historic farm laneway/later street as the main vehicular access to the project⁴;
- Providing building articulation and massing compatible with the existing neighbourhood buildings; and
- Maintaining the river corridor in its natural state inclusive of a significant setback with captures the existing shale cliff and mature dense foliage.

⁴ Heritage Impact Assessment 1745-1775 Thorny-Brae Place, Mississauga, prepared by The Landplan Collaborative Ltd., June 22, 2009 and Land Registry Research, 2015.

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4.0 MANDATORY RECOMMENDATION

1. *Does the property meet the criteria for heritage designation under the Ontario Regulation 9/06, Ontario Heritage Act?*

Evaluation as per Ontario Regulation 9/06 of the Ontario Heritage Act:

1. The property has design value or physical value because it,

- i) *is a rare, unique, representative or early example of a style, type, expression, material or construction method,*
The two houses are typical one storey, brick clad, asphalt shingled bungalows within 1960's residential subdivisions. They are not rare, unique, representative or early examples of a style, type, expression, material or construction method.
- ii) *displays a high degree of craftsmanship or artistic merit, or*
The two simple one-storey houses are typical of residential housing within the 1960's and do not display a high degree of craftsmanship or artistic merit.
- iii) *demonstrates a high degree of technical or scientific achievement.*
The two simple one-storey brick clad bungalows are typical of residential housing within the 1960's and do not demonstrate a high degree of technical or scientific achievement.

2. The property has historical value or associative value because it,

- i) *has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,*
The bungalows are not associated with the original orchards, farming and owners of the site.
- ii) *yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or*
The bungalows are not associated with the original orchards and farming on the site and have no potential to yield information that contributes to an understanding of the community.
- iii) *demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.*
Both structures were designed and built by a residential developer. The fifth bungalow at the end of the cul-de-sac has already been demolished and presently redevelopment signage on the south side of Thorny-Brae suggests that the third and forth 1960's bungalows will also be demolished for imminent site development.

3. The property has contextual value because it,

- i) *is important in defining, maintaining or supporting the character of an area,*
Both structures are reflective of the 1960's developments and do not define, maintain or support the original orchards or farming on the site.
- ii) *is physically, functionally, visually or historically linked to its surroundings, or*
The site is located within the Mississauga Road Scenic Route Cultural Landscape as well a portion of the site is within the Credit River Corridor Cultural Landscape but neither structure is a contributing factor.
- iii) *is a landmark. O. Reg. 9/06, s. 1 (2).*
The visibility of the north and west sides of 1775 Thorny-Brae is obscured by vegetation and fencing while the rear of 1765 Thorny-Brae is likewise obscured. Neither house would be considered a landmark.

The property does not meet the criteria for heritage designation under the Ontario Regulation 9/06, Ontario Heritage Act.

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2. *If the subject property does not meet the criteria for heritage designation then it must be clearly stated as to why it does not.*

Most of the property has been vacant for many years. The two existing bungalows are not considered significant.

3. *Regardless of the failure to meet criteria for heritage designation, does the property warrant conservation as per the definition in the Provincial Policy Statement:*

Conserved: *means the identification, protection, use and/or management of cultural heritage and archaeological resources in such a way that their heritage values, attributes and integrity are retained. This may be addressed through a conservation plan or heritage impact assessment.*

The site is located within the Mississauga Road Scenic Route Cultural Landscape and the proposed development addresses the policies of the Mississauga Road Scenic Route Study.

A portion of the site is within the Credit River Corridor Cultural Landscape and this land is to be preserved in its natural state. The proposed development will have no negative impact to the landscape and is not visible from the valley.

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5.0 QUALIFICATIONS

The Heritage Impact Assessment (HIA) for the demolition of 1765 and 1775 Thorny-Brae Place was prepared in accordance with the City of Mississauga's Heritage Impact Assessment Terms of Reference, dated October 2014. **Refer to Appendix A.**

As a requirement of the above noted guidelines, the HIA was prepared by a member of the Canadian Association of Heritage Professionals (CAHP); namely Kelly Gilbride OAA, P.Eng., CAHP, LEED AP of Stevens Burgess Architects Ltd. (SBA). **Refer to Appendix B for resume.** SBA is an OAA licensed architectural practice complete with six licensed architects, three members of the Canadian Association of Heritage Professionals (CAHP), three LEED accredited professionals and a staff specializing in architecture, heritage conservation, interior design and master planning.

Since 1988 when SBA was retained to assist the Trustees of The Old Stone Church in Beaverton Ontario to restore the 1840's stone church, SBA has worked on over twenty recognized or designated heritage properties and many more listed or eligible to be listed buildings. Following internationally recognized preservation principles as inscribed in the charters, SBA's involvement with projects range from research and documentation to production of Heritage Significance Evaluations, Building Condition Assessments, Intervention Guidelines, Conservation Master Plans, Feasibility Studies, Heritage Impact Statements, Building Conservation, Retrofit and/or Reuse and Monitoring and Maintenance Plans.

Appendix A:

City of Mississauga's Heritage Impact Assessment
Terms of Reference, October 2014



CITY OF MISSISSAUGA HERITAGE IMPACT ASSESSMENT

TERMS OF REFERENCE

1. Background: The Mississauga Official Plan

The City's Official Plan introduces cultural heritage resources in the following manner:

Mississauga's cultural heritage resources reflect the social, cultural and ethnic heritage of the city and, as such, are imperative to conserve and protect. Cultural heritage resources are structures, sites, environments, artifacts and traditions that are of cultural, historical, architectural, or archaeological value, significance or interest.

In compliance with the City's policy 7.4.1.12, as stated below, the City of Mississauga seeks to conserve, record, and protect its heritage resources:

*7.4.1.12: The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a **Heritage Impact Statement**¹, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction.*

A Heritage Impact Assessment is a study to determine the impacts to known and potential heritage resources within a defined area proposed for future development. The study would include an inventory of all heritage resources within the planning application area. The study results in a report which identifies all known heritage resources, an evaluation of the significance of the resources, and makes recommendations toward mitigation measures that would minimize negative impacts to those resources. A Heritage Impact Assessment may be required on a Designated or individually Listed property on the City's Heritage Register or where development is proposed adjacent to a known heritage resource. The requirement may also apply to unknown or recorded heritage resources which are discovered during the development application stage or construction.²

¹ At time of the writing of these Terms of Reference, the 2014 Official Plan Amendments supporting updated heritage definitions has not yet been enacted.

² For the definition of "development," please refer to the Mississauga Official Plan.

The City's Heritage Register includes properties that comprise cultural landscapes. Cultural landscapes include neighbourhoods, roadways and waterways. Individual properties within these landscapes may or may not have cultural heritage value independent of the landscape. Heritage Impact Assessments are required to ascertain the property's cultural heritage value and to ensure that any development maintains the cultural landscape criteria, available at http://www5.mississauga.ca/pdfs/Cultural_Landscape_Inventory_Jan05.pdf

To determine the specific heritage status of a particular property visit <http://www.mississauga.ca/portal/services/property>. Submit the desired address and click on the "Heritage" tab. Further information is available by clicking the underlined "INV#." This last tab explains the reason why the property is listed or designated.

2. The following minimum requirements will be requested in a Heritage Impact Assessment:

- 2.1 A detailed site history to include a listing of owners from the Land Registry Office, and a history of the site use(s). However, please note that due to the Freedom of Information and Protection of Privacy Act, current property owner information must not be included. As such, Heritage Planning will request that current property owner personal information be redacted to ensure the reports comply with the Act.
- 2.2 A complete listing and full written description of all existing structures, natural or man-made, on the property. Specific mention must be made of all the heritage resources on the subject property which include, but are not limited to: structures, buildings, building elements (like fences and gates), building materials, architectural and interior finishes, natural heritage elements, landscaping, and archaeological resources. The description will also include a chronological history of the structure(s) developments, such as additions, removals, conversions, alterations etc.

The report will include a clear statement of the conclusions regarding the significance and heritage attributes of the cultural heritage resource.

A location map must be provided, with indications of existing land use, zoning, as well as the zoning and land use of adjacent properties.

- 2.3 Documentation of the heritage resource will include current legible photographs, from each elevation, and/or measured drawings, floor plans, and a site map, at an appropriate scale for the given application (i.e. site plan as opposed to subdivision), indicating the context in which the heritage resource is situated. Also to include historical photos, drawings, or other archival material that may be available or relevant. For buildings, internal and external photographs and floor plans are also required. Please note that due to the Freedom of Information and Protection of Privacy Act, photographs should not contain people or highlight personal possessions. The purpose of the photographs is to capture architectural features and building materials.

The applicant must provide a description of all relevant municipal or agency requirements which will be applied to the subject property, and when implemented may supplement,

supersede and/or affect the conservation of heritage resources (i.e. Building Code requirements, Zoning requirements, Transportation and Works requirements.)

- 2.4 An outline of the proposed development, its context and how it will impact the heritage resource and neighbouring properties will be provided. This may include such issues as the pattern of lots, roadways, setbacks, massing, relationship to natural and built heritage features, recommended building materials, etc. The outline should address the influence of the development on the setting, character and use of lands on the subject property and adjacent lands.

Note: An architectural drawing indicating the subject property streetscape with properties to either side of the subject lands must be provided. The purpose of this drawing is to provide a schematic view of how the new construction is oriented and integrates with the adjacent properties from a streetscape perspective. The drawing must therefore show, within the limits of defined property lines, an outline of the building mass of the subject property and the existing neighbouring properties, along with significant trees or any other landscape or landform features. A composite photograph may accomplish the same purpose with a schematic of the proposed building drawn in.

- 2.5 Full architectural drawings, by a licensed architect or accredited architectural designer, showing all four elevations of the proposed development must be included for major alterations and new construction.
- 2.6 An assessment of alternative development options and mitigation measures that should be considered in order to avoid or limit the negative impact on the cultural heritage resources. Methods of minimizing or avoiding negative impact on a cultural heritage resource as stated in the Ontario Heritage Tool Kit (InfoSheet #5, Ministry of Culture) include, but are not limited to:
- Alternative development approaches
 - Isolating development and site alteration from the significant built and natural heritage features and vistas
 - Design guidelines that harmonize mass, setback, setting and materials
 - Limiting height and density
 - Allowing only compatible infill and additions
 - Reversible alterations

These alternate forms of development options presented in the Heritage Impact Assessment must be evaluated and assessed by the heritage consultant writing the report as to the best option to proceed with and the reasons why that particular option has been chosen.

- 2.7 A summary of conservation principles and how they will be used must be included. The conservation principles may be found in publications such as: Parks Canada – *Standards and Guidelines for the Conservation of Historic Places in Canada*; *Eight Guiding Principles in the Conservation of Historic Properties*, Ontario Ministry of Culture. (Both publications are available online.)

- 2.8 Proposed demolition/alterations must be explained as to the loss of cultural heritage value interests in the site and the impact on the streetscape and sense of place.
- 2.9 When a property cannot be conserved, alternatives will be considered for salvage mitigation. Only when other options can be demonstrated not to be viable will options such as relocation, ruinification, or symbolic conservation be considered.

Relocation of a heritage resource may indicate a move within or beyond the subject property. The appropriate context of the resource must be considered in relocation. Ruinification allows for the exterior only of a structure to be maintained on a site. Symbolic conservation refers to the recovery of unique heritage resources and incorporating those components into new development, or using a symbolic design method to depict a theme or remembrance of the past.

All recommendations shall be as specific as possible indicating the exact location of the preferred option, site plan, building elevations, materials, landscaping, and any impact on neighbouring properties, if relevant.

3. Summary Statement and Conservation Recommendations

The summary should provide a full description of:

- The significance and heritage attributes of the cultural heritage resource, including the reference to a listing on the Heritage Register, or designation by-law if it is applicable
- The identification of any impact that the proposed development will have on the cultural heritage resource
- An explanation of what conservation or mitigative measures, or alternative development, or site alteration approaches are recommended
- Clarification as to why conservation or mitigative measures, or alternative development or site alteration approaches are not appropriate

4. Mandatory Recommendation

The consultant must write a recommendation as to whether the subject property is worthy of heritage designation in accordance with the heritage designation criteria per Regulation 9/06, *Ontario Heritage Act*. Should the consultant not support heritage designation then it must be clearly stated as to why the subject property does not meet the criteria as stated in Regulation 9/06.

The following questions **must** be answered in the final recommendation of the report:

- Does the property meet the criteria for heritage designation under the Ontario Regulation 9/06, *Ontario Heritage Act*?
- If the subject property does not meet the criteria for heritage designation then it must be clearly stated as to why it does not

- Regardless of the failure to meet criteria for heritage designation, does the property warrant conservation as per the definition in the Provincial Policy Statement:

Conserved: means the identification, protection, use and/or management of cultural heritage and archaeological resources in such a way that their heritage values, attributes and integrity are retained. This may be addressed through a conservation plan or heritage impact assessment.

Please note that failure to provide a clear recommendation as per the significance and direction of the identified cultural heritage resource will result in the rejection of the Heritage Impact Assessment.

5. Qualifications

The qualifications and background of the person completing the Heritage Impact Assessment will be included in the report. The author must be a qualified heritage consultant by having Professional standing with the Canadian Association of Heritage Professionals (CAHP) and/or clearly demonstrate, through a Curriculum Vitae, his/her experience in writing such Assessments or experience in the conservation of heritage places. The Assessment will also include a reference for any literature cited, and a list of people contacted during the study and referenced in the report.

6. Approval Process

Three hard copies of the Heritage Impact Assessment, along with a PDF version, will be provided to the Heritage Coordinator. Hard copies must be single sided and pages must be no larger than 11 x 17 inches. Staff will ensure that copies are distributed to the Planning and Building Department and relevant staff and stakeholders within the Corporation. The Heritage Impact Assessment will be reviewed by City staff to determine whether all requirements have been met and, if relevant, to evaluate the recommendations presented by the Heritage Consultant on the alternative development options. The applicant will be notified of Staff's comments and acceptance, or rejection of the report.

All Heritage Impact Assessments will be sent to the City's Heritage Advisory Committee for information or review. As of September 2014, Heritage Impact Assessments will no longer be published online. However, these documents will be made available to the public by appointment with Heritage Planning staff.

An accepted Heritage Impact Assessment will become part of the further processing of a development application under the direction of the Planning and Building Department. The recommendations within the final approved version of the Heritage Impact Assessment will be incorporated into development related legal agreements between the City and the proponent at the discretion of the municipality.

7. References

Applicants looking for professional assistance may wish to refer to the Canadian Association of Heritage Professionals. website: <http://www.cahp-acecp.ca/>

For more information on Heritage Planning at the City of Mississauga, visit us online at [http:// www.mississauga.ca/heritageplanning](http://www.mississauga.ca/heritageplanning)

Interpretation Services: <http://www.mississauga.ca/portal/cityhall/languages>

Appendix B:

City of Mississauga Cultural Landscape Heritage Impact Assessment
(HIA Terms of Reference), undated



Cultural Landscape Heritage Impact Assessment (HIA) Terms of Reference

1. Introduction

The City of Mississauga adopted a Cultural Landscape Inventory in 2005. Cultural landscapes include neighbourhoods, roadways, waterways and more. The Cultural Landscape Inventory is available online at http://www5.mississauga.ca/pdfs/Cultural_Landscape_Inventory_Jan05.pdf.

All of the properties listed on the Cultural Landscape Inventory are listed on the City's Heritage Register. In compliance with the City's policy 7.4.1.12, as stated below, the City of Mississauga seeks to conserve, record, and protect its heritage resources:

*7.4.1.12: The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a **Heritage Impact Statement**¹, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction.*

These cultural heritage resources include properties identified on the City's Heritage Register as being part of Cultural Landscapes.

A Heritage Impact Assessment is a study to determine the impacts to known and potential heritage resources within a defined area proposed for future development. The study would include an inventory of all heritage resources within the planning application area. The study results in a report which identifies all known heritage resources, an evaluation of the significance of the resources, and makes recommendations toward mitigation measures that would minimize negative impacts to those resources. A Heritage Impact Assessment will be required on a property which is listed on the City's Heritage Register, a property designated under the *Ontario Heritage Act*, or where development is proposed adjacent to a known heritage resource. The requirement may also apply to unknown or recorded heritage resources which are discovered during the development application stage or construction.²

¹ At time of the writing of these Terms of Reference, the 2014 Official Plan Amendments supporting updated heritage definitions has not yet been enacted.

² For the definition of "development," please refer to the Mississauga Official Plan.

2. General Requirements include:

- A location map
- A site plan of existing conditions, to include buildings, structures, roadways, driveways, drainage features, trees and tree canopy, fencing, and topographical features
- A written and visual inventory (legible photographs – we suggest no more than two per page) of all elements of the property that contribute to its cultural heritage value, including overall site views. For buildings, internal and external photographs and floor plans are also required. Please note that due to the Freedom of Information and Protection of Privacy Act, photographs should not contain people or highlight personal possessions. The purpose of the photographs is to capture architectural features and building materials.
- A site plan and elevations of the proposed development
- For cultural landscapes or features that transcend a single property, a streetscape plan is required, in addition to photographs of the adjacent properties
- Qualifications of the author completing the report
- Three hard copies and a PDF

The City reserves the right to require further information, or a full HIA. These terms of reference are subject to change without notice.

3. Addressing the Cultural Landscape or Feature Criteria

Cultural Heritage Landscape Inventory Heritage Impact Assessments must demonstrate how the proposed development will conserve the criteria that render it a cultural heritage landscape and/or feature. Each cultural heritage landscape and feature includes a checklist of criteria. The Heritage Impact Assessment need only address the checked criteria for the pertinent cultural heritage landscapes or features. (Please note: some properties constitute more than one cultural heritage landscape.) Criteria include the following:

Landscape Environment

- scenic and visual quality
- natural environment*
- horticultural interest
- landscape design, type and technological interest

Built Environment

- aesthetic/visual quality
- consistent with pre World War II environs
- consistent scale of built features
- unique architectural features/buildings
- designated structures

Historical Associations

- illustrates a style, trend or pattern
- direct association with important person or event

- illustrates an important phase of social or physical development
- illustrates the work of an important designer

Other

- historical or archaeological interest**
- outstanding features/interest
- significant ecological interest
- landmark value

Descriptions of these criteria are available in the Cultural Landscape Inventory document (pages 13 to 16).

*For cultural landscapes or features noted for their **natural environment** (i.e. checked off in the Cultural Landscape Inventory document), and when also required as part of the Planning process, a copy of a certified arborist's report will be included as part of the scope of the Heritage Impact Assessment.

For cultural landscapes or features noted for their **archaeological interest (i.e. checked off in the Cultural Landscape Inventory document), and when also required as part of the Planning process, a stage 1 archaeological assessment is required.

4. Property Information

The proponent must include a list of property owners from the Land Registry office. Additional information may include the building construction date, builder, architect/designer, landscape architect, or personal histories. However, please note that due to the Freedom of Information and Protection of Privacy Act current property owner information must NOT be included. As such, Heritage Planning will request that current property owner personal information be redacted to ensure the reports comply with the Act.

5. Impact of Development or Site Alteration

An assessment identifying any impact the proposed development or site alteration may have on the cultural heritage resource(s). Negative impacts on a cultural heritage resource(s) as stated in the *Ontario Heritage Tool Kit* include, but are not limited to:

- Destruction of any, or part of any, significant heritage attributes or features
- Removal of natural heritage features, including trees
- Alteration that is not sympathetic, or is incompatible, with the historic fabric and appearance
- Shadows created that alter the appearance of a heritage attribute or change the viability of an associated natural feature, or plantings, such as a garden
- Isolation of a heritage attribute from its surrounding environment, context or a significant relationship

- Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features
- A change in land use where the change in use negates the property's cultural heritage value
- Land disturbances such as change in grade that alter soils, and drainage patterns that adversely affect cultural heritage resources

The proponent must demonstrate how the new proposed built form reflects the values of the identified cultural landscape and its characterizations that make up that cultural landscape.

6. Mitigation Measures

The Heritage Impact Assessment must assess alternative development options and mitigation measures in order to avoid or limit the negative impact on the cultural heritage resources. Methods of minimizing or avoiding negative impact on cultural heritage resources, noted by the Ministry of Culture, include but are not limited to the following:

- Alternative development approaches
- Isolating development and site alteration from the significant built and natural heritage features and vistas
- Design guidelines that harmonize mass, setback, setting and materials
- Limiting height and density
- Allowing only compatible infill and additions
- Reversible alterations

These alternate forms of development options presented in the Heritage Impact Assessment must be evaluated and assessed by the heritage consultant writing the report as to the best option to proceed with and the reasons why that particular option has been chosen.

7. Qualifications

The qualifications and background of the person completing the Heritage Impact Assessment will be included in the report. The author must be a qualified heritage consultant by having professional standing with the Canadian Association of Heritage Professionals (CAHP) and/or clearly demonstrate, through a Curriculum Vitae, experience in writing such Assessments or experience in the conservation of heritage places. The Assessment will also include a reference for any literature cited, and a list of people contacted during the study and referenced in the report.

8. Recommendation

The heritage consultant must provide a recommendation as to whether the subject property is worthy of heritage designation in accordance with the heritage designation criteria per Regulation 9/06, *Ontario Heritage Act*. Should the consultant not support heritage designation then it must be clearly stated as to why the subject property does not meet the criteria as stated in Regulation 9/06.

The following questions must be answered in the final recommendation of the report:

- Does the property meet the criteria for heritage designation under Ontario Regulation 9/06, *Ontario Heritage Act*?
- If the subject property does not meet the criteria for heritage designation then it must be clearly stated as to why it does not
- Regardless of the failure to meet criteria for heritage designation, does the property warrant conservation as per the definition in the Provincial Policy Statement:
“Conserved: means the identification, protection, use and/or management of cultural heritage and archaeological resources in such a way that their heritage values, attributes and integrity are retained. This may be addressed through a conservation plan or heritage impact assessment.”

Please note that failure to provide a clear recommendation as per the significance and direction of the identified cultural heritage resource will result in the rejection of the Heritage Impact Assessment.

9. Approval Process

Three copies of the Heritage Impact Assessment will be provided to Heritage staff, along with a PDF version. Hard copies must be single sided and pages must be no larger than 11 x 17 inches. Staff will ensure that copies are distributed to the Planning and Building Department and relevant staff and stakeholders within the Corporation. The Heritage Impact Assessment will be reviewed by City staff to determine whether all requirements have been met and to evaluate the preferred option(s). The applicant will be notified of Staff’s comments and acceptance, or rejection of the report.

All Heritage Impact Assessments will be sent to the City’s Heritage Advisory Committee for information or review. As of September 2014, Heritage Impact Assessments will no longer be published online. However, these documents will be made available to the public by appointment with Heritage Planning staff.

An accepted Heritage Impact Assessment will become part of the further processing of a development application under the direction of the Planning and Building Department. The recommendations within the final approved version of the Heritage Impact Assessment will be incorporated into development related legal agreements between the City and the proponent at the discretion of the municipality.

10. References

Applicants seeking professional assistance may wish to refer to the Canadian Association of Heritage Professionals website: <http://www.cahp-acecp.ca/>

Interpretation Services: <http://www.mississauga.ca/portal/cityhall/languages>

For more information on Heritage Planning at the City of Mississauga, visit us online at www.mississauga.ca/heritageplanning.

Appendix B:
Qualifications of the Author



Kelly Gilbride OAA, P. Eng., CAHP, LEED AP
Partner

EDUCATION	Bachelor of Architecture (Honours), 1991, McGill University Bachelor of Engineering (Honours), 1987, Concordia University	
PROFESSIONAL EXPERIENCE	2001 to date	Stevens Burgess Architects Ltd., Toronto
	1998 to 2001	White and Gilbride Architects Inc., Deep River
	1997 to 1998	Turczyn White + Gilbride Architects, Pembroke
	1996 to 1998	Kelly Gilbride Architect, Deep River
	1991 to 1996	Greer Galloway Architects and Engineers, Pembroke
PROFESSIONAL ASSOCIATIONS	Ontario Association of Architects, OAA Association of Professional Engineers of Ontario, PEO Canadian Association of Heritage Professionals, CAHP	
AWARDS	CAHP Heritage Planning Award of Merit 2011, Dundurn National Historic Site CAHP Craftsmanship Award 2010, Restoration of Gore Park Fountain Grand Valley Construction Association Award of Excellence, 2008 SMATH Rebuild Robertson Building Systems 1994, Master Builder Award American Institute of Architects (AIA) Scholarships, 1989-1990; 1990-1991 Concordia University 1987, Building Studies Medal	

Kelly's architectural training is complemented by her building engineering degree. Shortly after joining SBA in 2001, Kelly became a partner and was able to work hand in hand with Jane Burgess in developing an expertise within the heritage field. Initially focused on conservation work, Kelly has been able to expand her experience to include heritage policy, conservation plans, impact assessments, and heritage evaluations and inventories. Kelly is the managing partner in-charge of SBA's Heritage Vendor of Record Contracts and, accordingly, has developed expertise in working with municipal, government and private clients on challenging heritage projects

SELECTED HERITAGE PROJECTS (+ indicates award winning)

Old Galt Post Office – Idea Exchange, Cambridge (*Designated, National Historic Site*)

§ Heritage Architect for Adaptive Reuse and Restoration

Holy Name Church, Toronto, ON

§ Renovations and Accessibility Upgrades

St. Michael's Hospital, Toronto, ON

§ Restoration of Chapel Stained Glass Windows

Redemptorists of Toronto and Edmonton, 141 McCaul St Monastery, Toronto (*Designated*)

§ Study to determine feasibility of conversion to self-contained residential suites.

§ Conservation of the building envelope, interior retrofit and accessibility improvements.

City of Hamilton: Jimmy Thompson Memorial Pool

§ Feasibility Study to develop Heritage Intervention Guidelines

+City of Hamilton - Dundurn National Historic Site, Hamilton (*Museum, National Historic Site, Designated*)

▪ Re-use Study for the adaptation of five significant outbuildings to augment the museum experience.

City of Toronto, Young Peoples Theatre, Toronto (*Designated*)

§ Heritage Window Conservation Feasibility Study and subsequent conservation of wood and metal windows.

+City of Hamilton, Gore Park Fountain, Hamilton (*Designated*)

§ Disassembly, Restoration and Re-assembly/Conservation of Gore Park Fountain

City of Toronto, Zion Schoolhouse Renovation, Toronto (*Designated*)

- § Renovation of Zion Schoolhouse
- § Building Condition Assessments

City of Hamilton, Gage Park Fountain and Watercourse, Hamilton (*Designated*)

- § Restoration of Historic Masonry, Gage Park Fountain and Watercourse

Infrastructure Ontario, St. Thomas Psychiatric Hospital Site (*Provincial Heritage Property of Provincial Significance*)

- § St. Thomas Psychiatric Hospital Demolition and Decommissioning Plan for site and sixteen heritage buildings

Infrastructure Ontario, Thunder Bay District Courthouse, Thunder Bay (*Ontario Government Heritage Inventory*)

- § Heritage Inventory and Evaluation of heritage fixtures, fittings, and furniture

Infrastructure Ontario, Sir James Whitney School, Belleville (*Ontario Government Heritage Inventory*)

- § Heritage Conservation Plan and Capital Plan for this 96 acre site and five designated buildings.

Infrastructure Ontario, Century Manor, Hamilton Psychiatric Hospital, Hamilton (*Designated*)

- § Adaptive Re-use Study of Century Manor Building at Former Hamilton Psychiatric Hospital
- § Century Manor, Phase Two Design Development and Construction Documents and Contract Administration for roofing – Central Block

+University of Guelph, Macdonald Institute, Guelph (*Heritage Inventory*)

- +▪ Renovation to 1903 Italianate load bearing masonry building, reconstruction of Parapet, Terrace and Portico
- § Renovation of MINS 300 Lecture Hall MINS 300 to an IT lecture theater while conserving the heritage elements

Ontario Realty Corporation, Whitney Block and Tower, Toronto (*Ontario Government Heritage Inventory*)

- § Heritage Conservation Plan; a maintenance and capital plan for all interior and exterior heritage features.

Ontario Realty Corporation, Three Properties on ORC Heritage Inventory, Markham (*ORC Heritage Inventory*)

- § Condition Assessment for Adaptive Re-use of Three Properties

Infrastructure Ontario - Century Manor, Hamilton Psychiatric Hospital, Hamilton (*Designated*)

- Adaptive Reuse Study to convert building use to office, museum, and half-way house

SNC Lavalin/ProFac, W. Ross Macdonald School, Brantford (*Designated*)

- § Sardargarh House, Design Development, Construction Documents for repairs to front porch/rear porch and window restoration (heritage attributes)
- § Intermediate and Deaf/Blind Residences, Notice of Violation-Liaison with authorities to protect heritage attributes

Ontario Realty Corporation, Hamilton Psychiatric Institute, Hamilton

- § Grove Hall – ORC Class EA Consultation & Documentation Report for steel window restoration

Huron Provincial Parks - Sainte Marie Among the Hurons, Midland (*Museum, Ont. Gov. Heritage Inventory*)

- +▪ Conservation of the chapel and reconstruction of blacksmith shop, carpentry shop and palisade.

Ministry Of Environment – Islandview and O.T. Workshop, Old Kingston Psychiatric Hospital, Kingston

- § Feasibility Study for reusing a collection of heritage buildings as a showcase sustainable office complex.

Ontario Realty Corporation, Leslie M. Frost Centre, Haliburton

- § Cultural heritage inventory and evaluation of approximately 20 buildings as part of an ORC Class EA

Ministry of Environment, Office Relocation to the Old Kingston Psychiatric Hospital Site: Kingston

- § Heritage Significance Study, Condition Assessment for Islandview Building (1880) and the Industrial Building.
- § Design and Feasibility Study for adaptive reuse of the buildings within a modern leading edge sustainable complex.

Ontario Realty Corporation, Brockville Registry Office, Brockville

- § Brockville Registry Office, Coordination of Construction Specifications and liaison with André Scheinman for site works

Appendix D:
Architectural Drawings
provided by Armstrong Planning and Project Management



Pace Developments

Corner of Eglinton and Mississauga Road

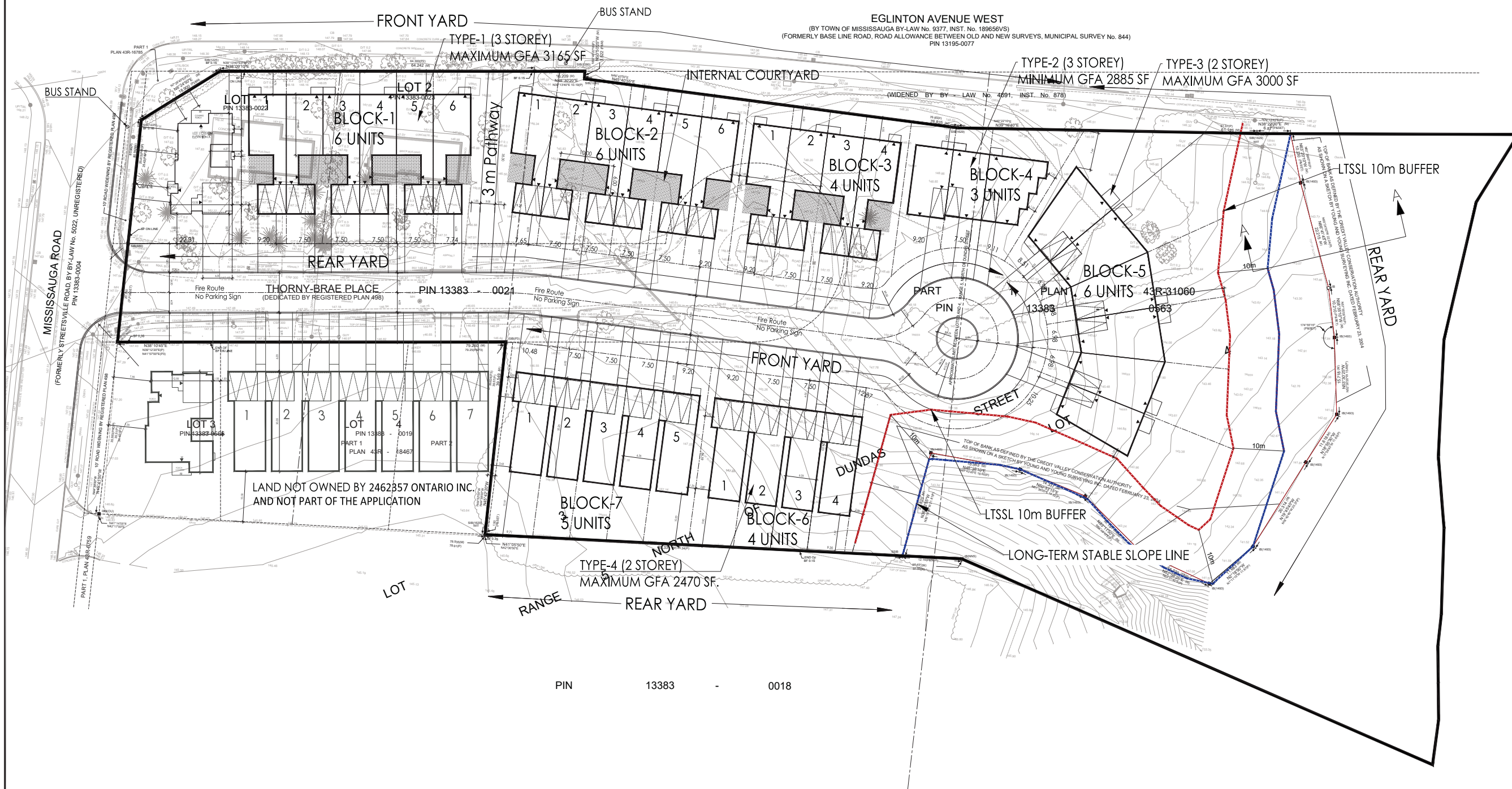


Zoning	Required	Preliminary Proposed
Single Detached (RA)		
Lot Coverage	40%	40%
Min. Lot Frontage	16.5 m	21.31 m
Min. Lot Area	500 sq. m.	612.7 sq. m.
Min. Front Yard	6.0 m	6.0m
Ext. Side Yard	4.5 m	6.0 m
Rear Yard	7.5 m	4.55
Height	13.7 m	10.7 m
Street Towns (RMS)		
Min. Lot Frontage	6.8 m	7.5 m
Min. Lot Area	200 sq. m.	255 sq. m.
Min. Front Yard	4.5m	6.0m
Ext. Side Yard	4.5 m	N/A
Int. Side Yard		
Attached Side	0	0 m
Unattached Side	1.5 m	1.5 m
Attached Garage - Unattached Side	1.2 m	1.2 m
Rear Yard	7.5 m	4.5 m
Height	13.7 m	11 m
Maximum GFA - Residential	0.75 times of the lot area	1.1 times of the lot area

KEY MAP

N.T.S.

7.8 - 54



ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR PRIOR TO COMMENCEMENT OF ANY WORK. ANY DISCREPANCIES MUST BE REPORTED DIRECTLY TO SRN ARCHITECTS INC.

PROJECT CONSULTANTS

[illegible]

Ample • Ample • Ample **RN DESIGN LTD.**

8395 JANE STREET, SUITE 203
VAUGHAN, ONTARIO. L4K 5Y2
T:905-738-3177 | F: 905-738-5449



CLIENT

2462357 ONTARIO INC

PROJECT/LOCATION
1745, 1765 and 1775 Thorny Brae
Place, Mississauga ON

DRAWING

CONCEPT
PLAN

DATE NOV 14 2017	SCALE 1:400
DRAWN BY MA	CHECKED BY MA
PROJECT NUMBER 16041	DRAWING NUMBER SP100



GROUND FLOOR ELEV. 'A'	
Floor Area:	1275.2
Open To Below:	0.0
Total Area (not incl. o/s):	1275.2
Coverage (with Porch):	1752.6
Coverage (without Porch):	1706.7

SAMPLE PLAN 1

SECOND FLOOR ELEV. 'A'	
Floor Area:	1706.7
Open To Below:	0.0
Total Area (not incl. other):	1706.7

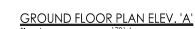


GROUND FLOOR ELEV. 'A'	
Floor Area:	1195.7
Open To Below:	0.0
Total Area (not incl orb):	1195.7
Coverage (With Porch):	1652.4
Coverage (Without Porch):	1622.8

SAMPLE PLAN 2

SECOND FLOOR ELEV. 'A'

Floor Area:	1622.8
Open To Below:	0.0
Total Area [net inc totl]:	1622.8



Floor Area:	1781.6
Open To Below:	0.0
Total Area (noted totl):	1781.6
Coverage (With Patch):	2568.7
Coverage (Without Patch):	2182.5

SECOND FLOOR PLAN ELEV. 'A'

Floor Area: 2156.7
Open To Below: 0.0
Total Area (not incl obj): 2156.7

Pace
Developments

project

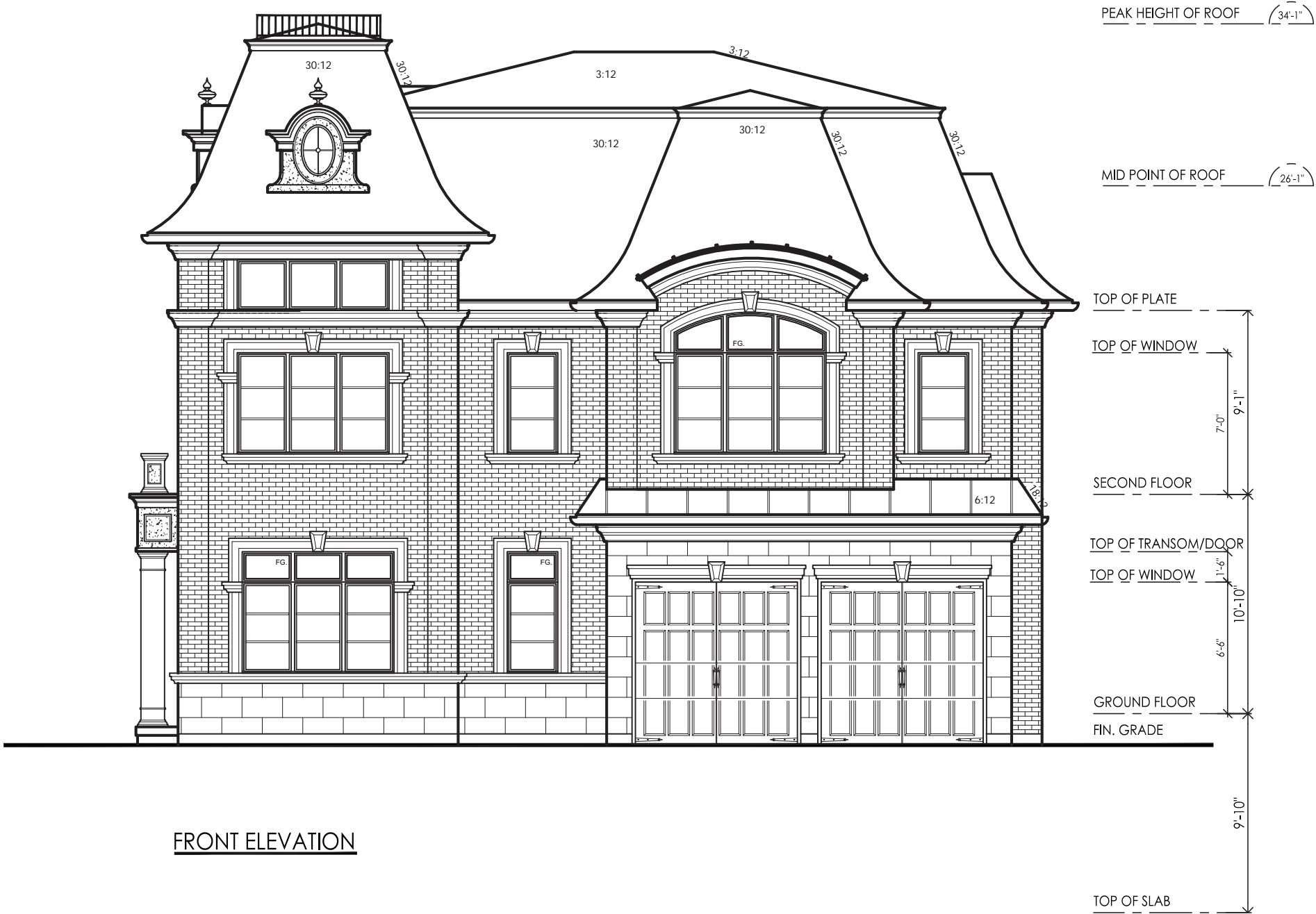
Thorny Brae
Towns
Mississauga

model SAMPLE RLT

project # 16041

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3938 SF
(includes 0 sf otb)

client
Pace
Developments

project
Thorny Brae
Towns
Mississauga

model
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project #
16041

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1	ISSUED FOR REVIEW	SP	29-JUNE-2016



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REAR ELEVATION

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(includes 0 sf otb)

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Developments**

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**Thorny Brae
Towns
Mississauga**

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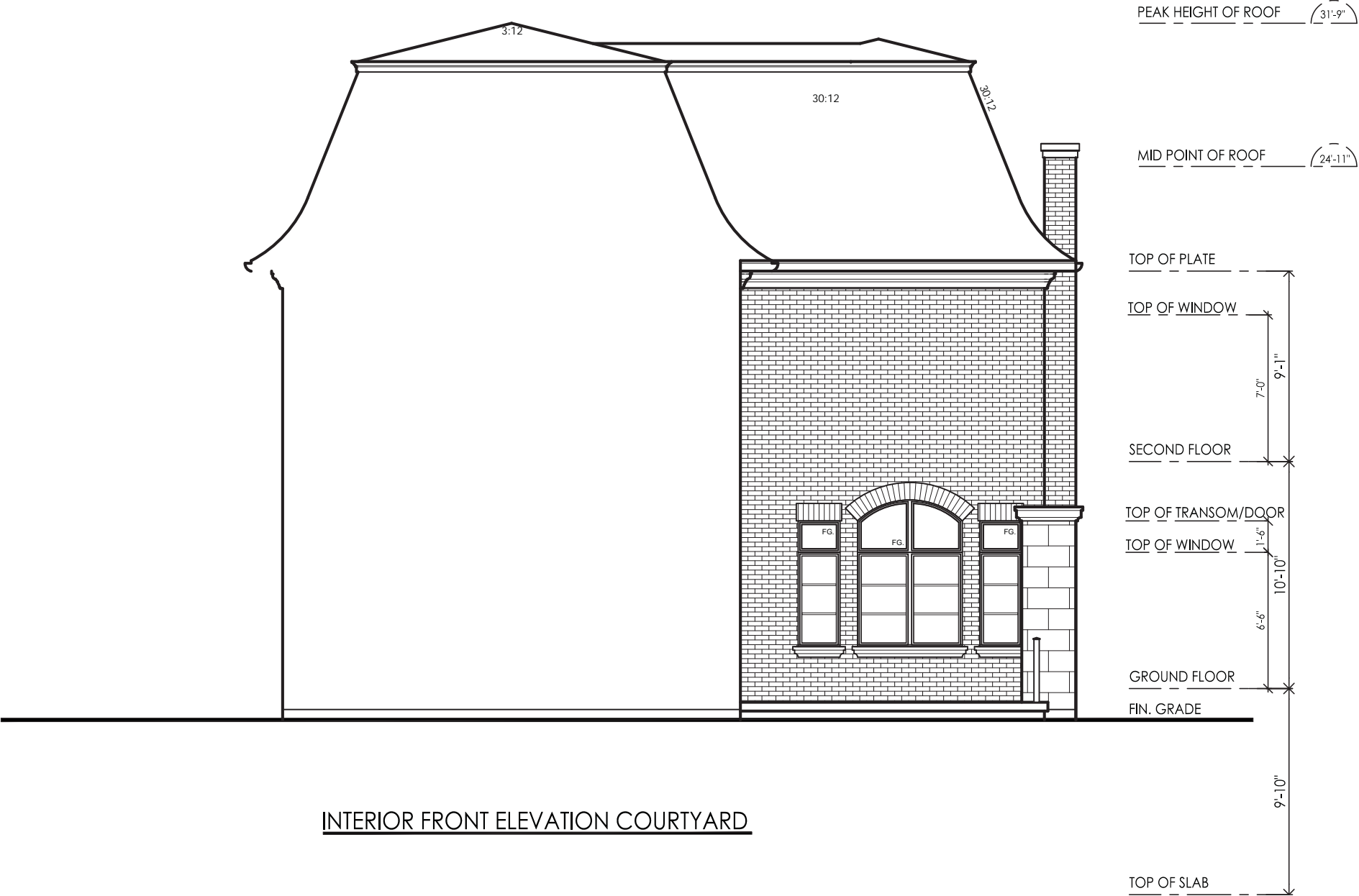
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INTERIOR FRONT ELEVATION COURTYARD

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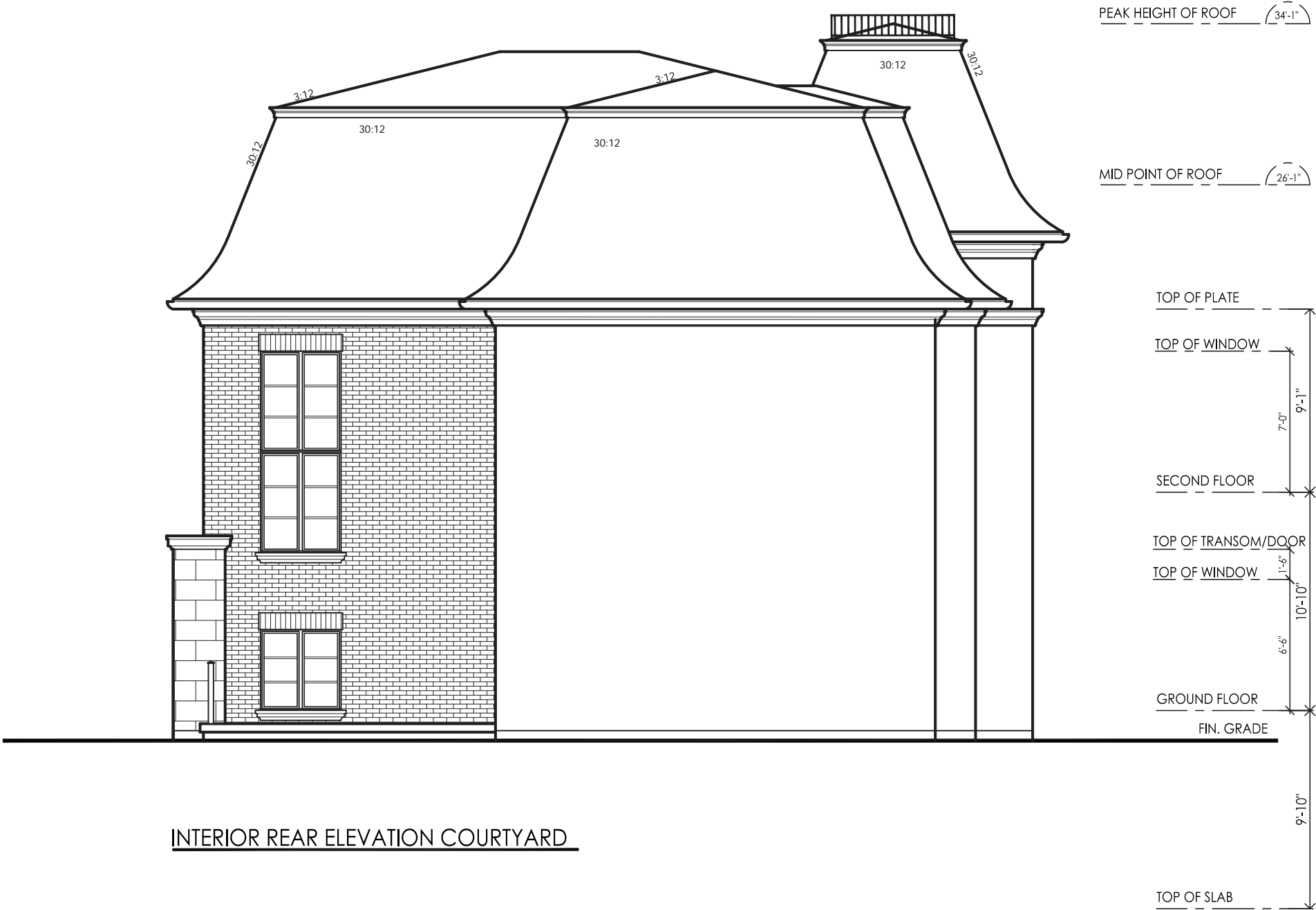
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INTERIOR REAR ELEVATION COURTYARD

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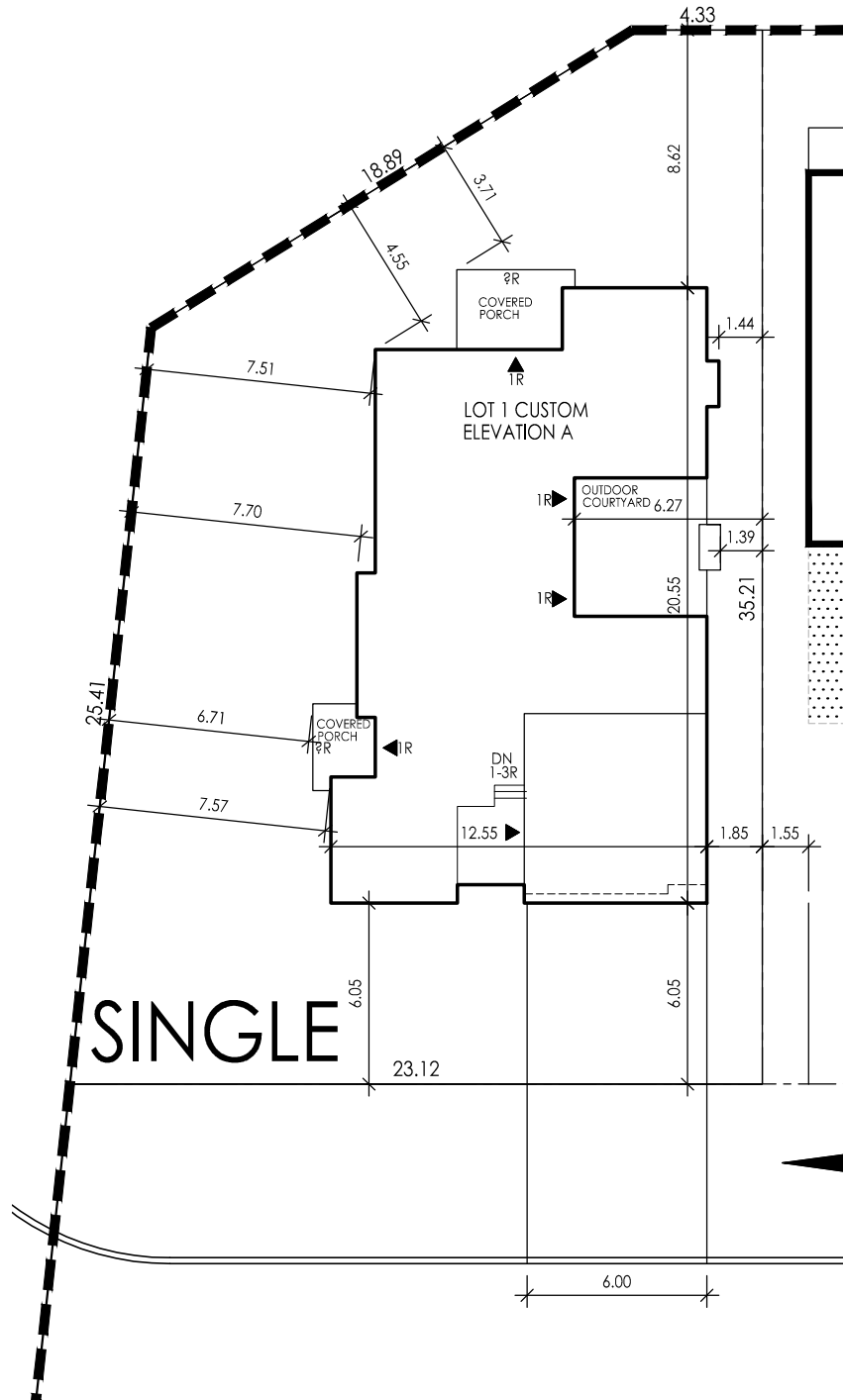
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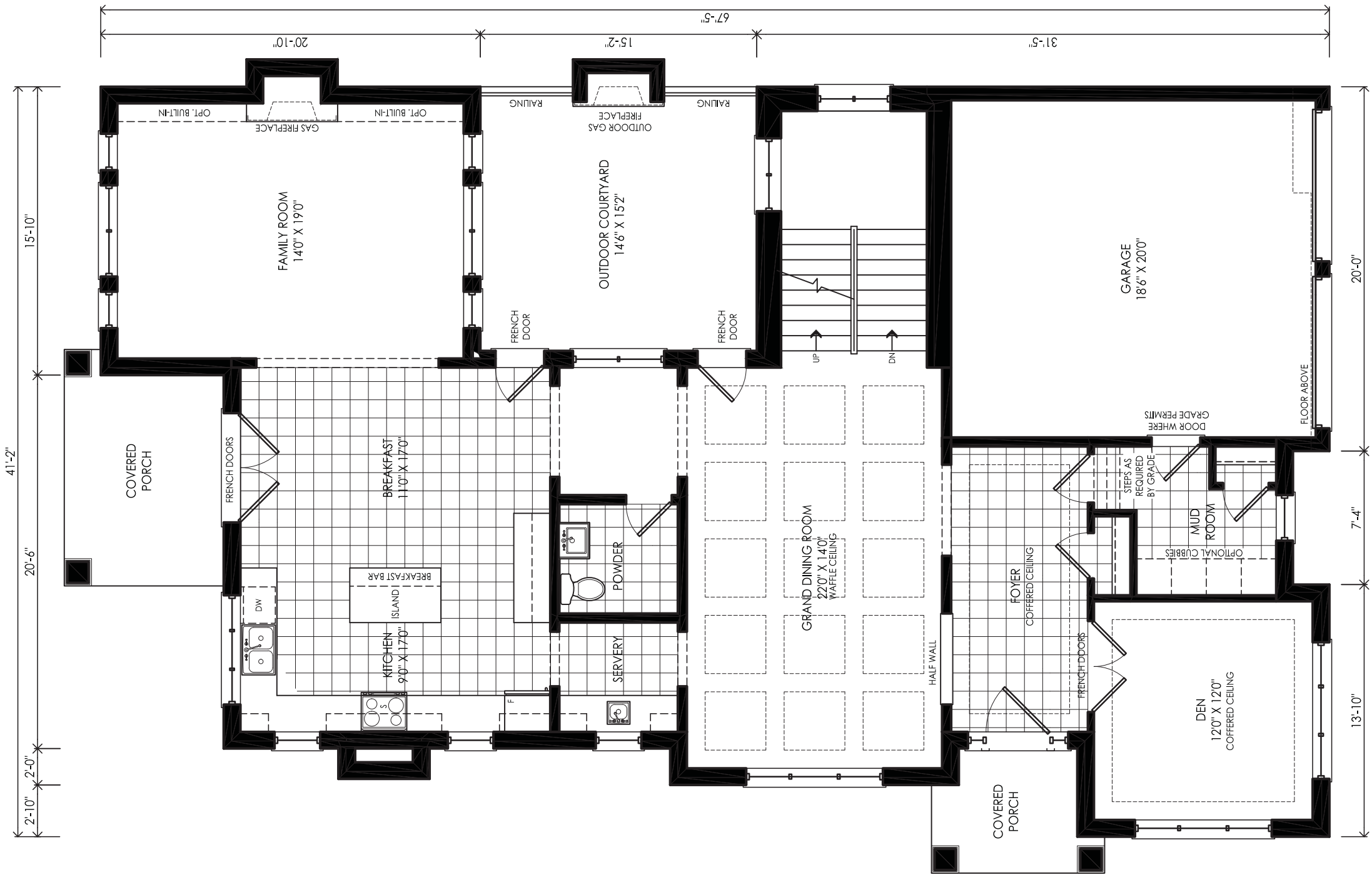
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GROUND FLOOR PLAN ELEV. 'A'

Floor Area:	1781.6
Open To Below:	0.0
Total Area (incl. otb):	1781.6
Coverage (With Porch):	2568.7
Coverage (Without Porch):	2182.5

revisions		initials	date
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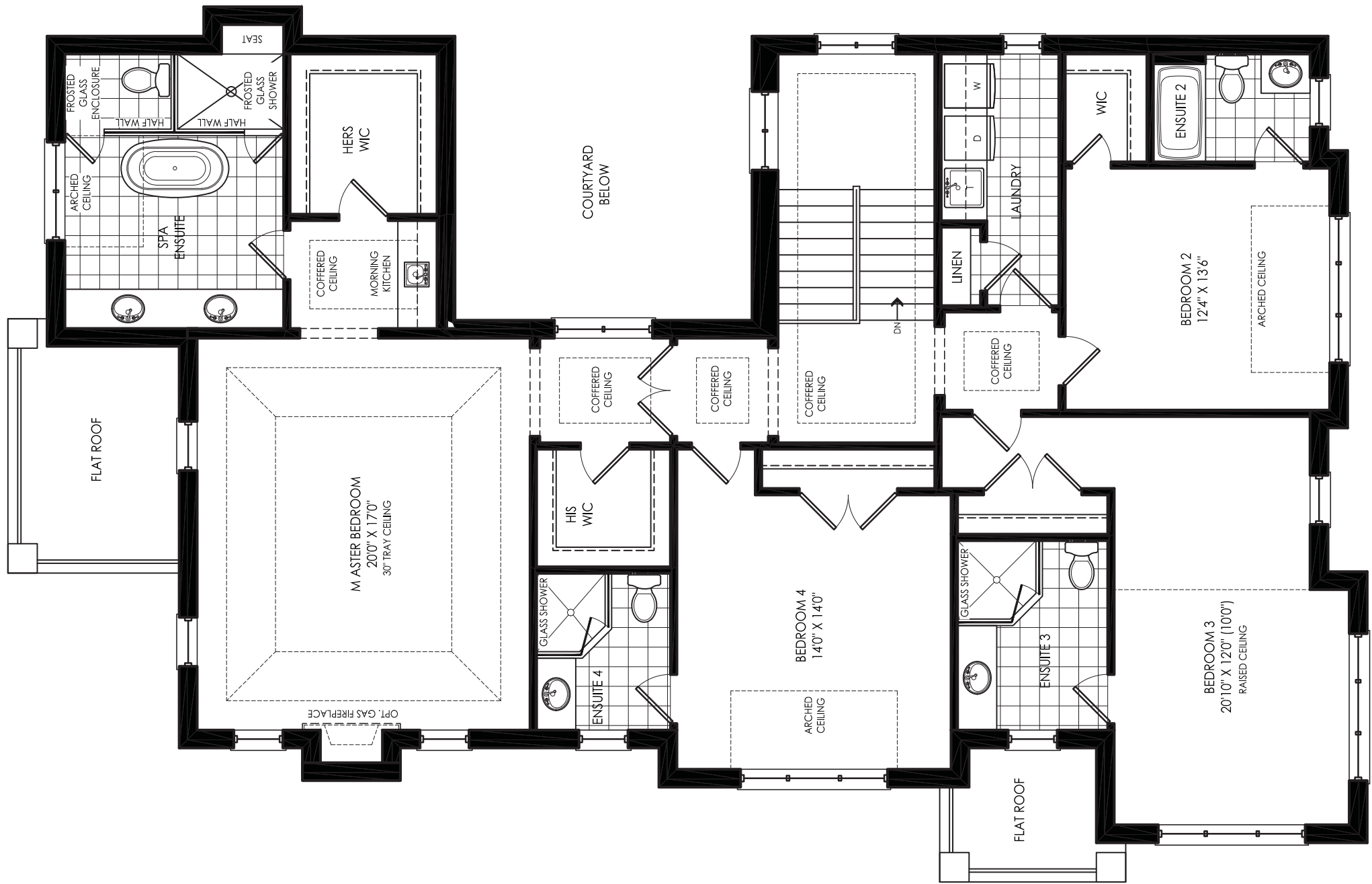
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Towns
Mississauga

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LOT1

project #
16041

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SECOND FLOOR PLAN ELEV. 'A'

Floor Area: 2156.7
Open To Below: 0.0
Total Area (not incl otb): 2156.7

revisions		initials	date
1	ISSUED FOR REVIEW	SP	29-JUNE-16

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(includes 0 sf otb)

client
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Developments

project
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Towns
Mississauga

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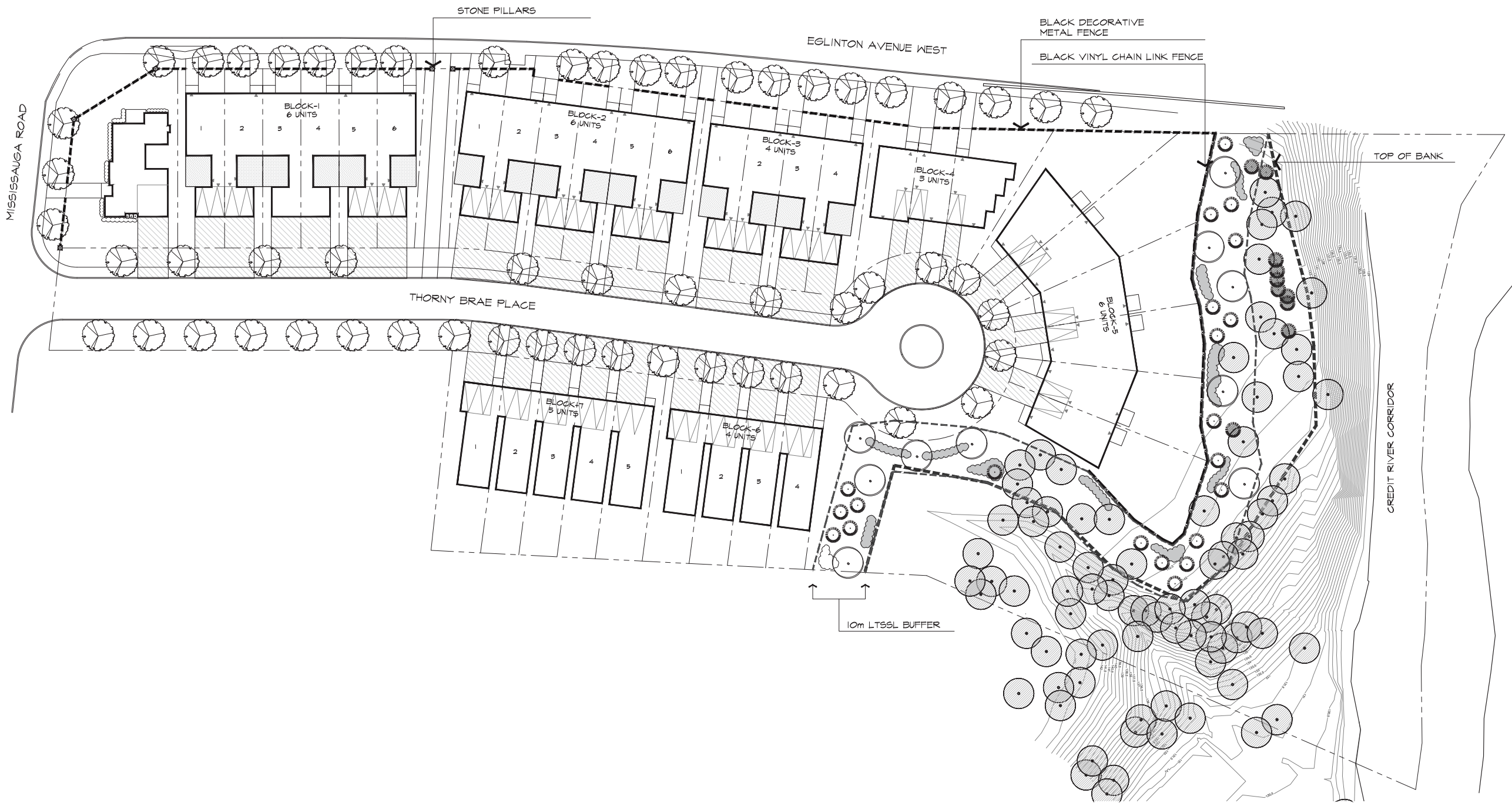
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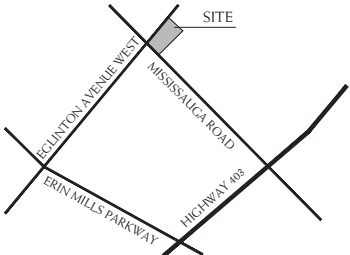
Appendix E:

Landscape Plan

provided by Armstrong Planning and Project Management



KEY PLAN



LEGEND

- EXISTING TREES TO BE PRESERVED
- PROPOSED NATIVE DECIDUOUS TREES IN THE NATURALIZED BUFFER AREA
- PROPOSED NATIVE DECIDUOUS TREES ALONG THE BOULEVARD
- PROPOSED NATIVE CONIFEROUS TREES IN THE NATURALIZED BUFFER AREA
- PROPOSED NATIVE SHRUBS IN THE NATURALIZED BUFFER AREA

NOTE: LOCATION OF PROPOSED TREES AND SHRUBS ARE CONCEPTUAL AND SUBJECT TO CHANGE THROUGH COORDINATION OF THE S.P.A. PROCESS AND DETAILED ENGINEERING DESIGN.

BASE INFORMATION OBTAINED ELECTRONICALLY FROM ARMSTRONG PLANNING & PROJECT MANAGEMENT (TEL. 416-444-3300)

0.	NOV 14 2017	ISSUED FOR HIA REPORT	LK
no.	date	version	by

All information hereon to be checked and verified at the site and any discrepancies must be reported to and clarified by the landscape architect before commencing work. All drawings, specifications, details, digital information, etc., prepared by the landscape architect are instruments of service and as such are his property and must be returned at his request.



ALEXANDER BUDREVICS
ASSOCIATES LIMITED
LANDSCAPE ARCHITECTS
895 Don Mills Road, Second Tower, Suite 212
Toronto, Ontario, Canada, M3C 1W3
416.444.5201
416.444.5208
www.budrevics.com

project
THORNY BRAE PLACE
1745, 1765, 1775 THORNY BRAE PLACE
MISSISSAUGA, ONTARIO
2462357 ONTARIO INC.
C/O PACE DEVELOPMENTS INC.
drawing
**LANDSCAPE
CONCEPT PLAN**

date	NOVEMBER 13, 2017	drawn	LK
scale*	1:400	file	3178.L1 R0-171114
direction			
	project no.	3178	
	sheet no.	L1	

*NOTED SCALE IS APPLICABLE ONLY WHEN PRINTED ON ARCH D (24"x36") SIZE SHEET

City of Mississauga
Memorandum



Date: 2017/12/05
To: Chair and Members of Heritage Advisory Committee
From: Paul Damaso, Director, Culture Division
Meeting Date: 2018/01/09
Subject: 29 Stavebank Road

This memorandum and its attachment are presented for HAC's information only.

The City received a request to demolish the heritage listed property at 21 Park Street East earlier this year. The property is adjacent to the Port Credit Cenotaph (now called "Vimy Park"), which is designated under the Ontario Heritage Act.

As per section 7.4.1.12 of the Mississauga Official Plan, "The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a Heritage Impact Assessment, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction."

The Heritage Impact Assessment addressed the redevelopment proposal and its adjacency to the heritage designated property. The report is available under item 7.1 here: https://www7.mississauga.ca/documents/committees/heritage/2017/2017_05_09_HAC_Agenda.pdf.

The City received a rezoning application (OZ 17 13) for a 15-storey residential condominium at 21, 25, 27 and 29 Park Street East in September. As per the Heritage Advisory recommendation 033-2017, the development proposal is attached for information.

Attachments

Appendix 1: Drawings

A handwritten signature in black ink, appearing to read 'Paul Damaso', written over a horizontal line.

Paul Damaso
Director, Culture Division
Prepared by: P. Wubbenhorst, Senior Heritage Coordinator



21-29 PARK STREET EAST,
PORT CREDIT, MISSISSAUGA,
ONTARIO

EDENSHAW PARK
DEVELOPMENTS LTD.

DEVELOPMENT PROPOSAL

DRAWING LIST

- A.01 - DRAWING LIST & RENDERING
- A.02 - SURVEY PLAN
- A.03 - SITE PLAN & PROJECT STATS
- A.04 - P3 PARKING LEVEL
- A.05 - P2 PARKING LEVEL
- A.06 - P1 PARKING LEVEL
- A.07 - GROUND FLOOR PLAN
- A.08 - 2 FLOOR PLAN
- A.09 - 3-6 FLOOR PLAN
- A.10 - 7 FLOOR PLAN
- A.11 - 8-10 FLOOR PLAN
- A.12 - 11 FLOOR PLAN
- A.14 - 15 FLOOR PLAN
- A.15 - ROOF PLAN
- A.16 - BUILDING SECTION
- A.17 - BUILDING SECTION
- A.18 - BUILDING ELEVATION
- A.19 - BUILDING ELEVATION
- A.20 - BUILDING ELEVATION
- A.21 - BUILDING ELEVATION

REVISIONS

NO.	DATE	DESCRIPTION

REZONING SUBMISSION

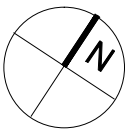
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DRAWING NAME

SCALE :	
DATE :	SEPTEMBER 07, 2017
JOB NUMBER :	109186

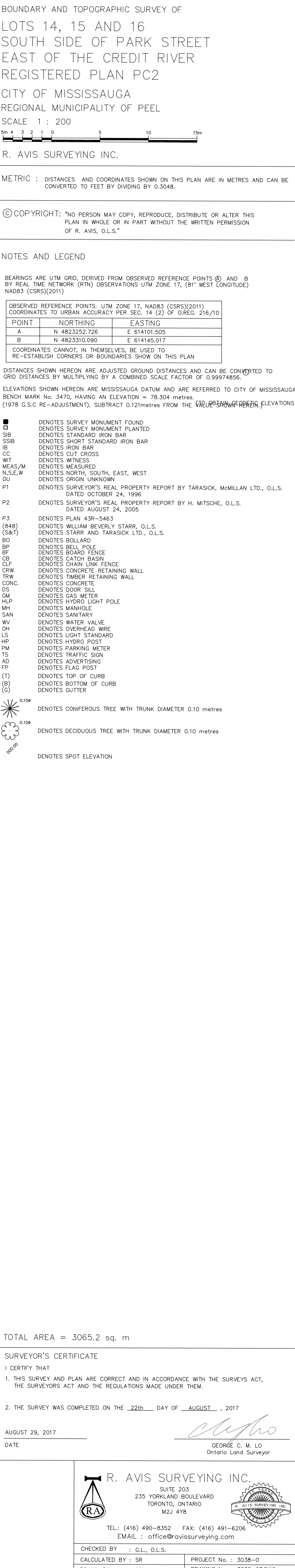


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A.01

DWG NO:



EDENSHAW PARK
DEVELOPMENTS LTD.

DEVELOPMENT PROPOSAL

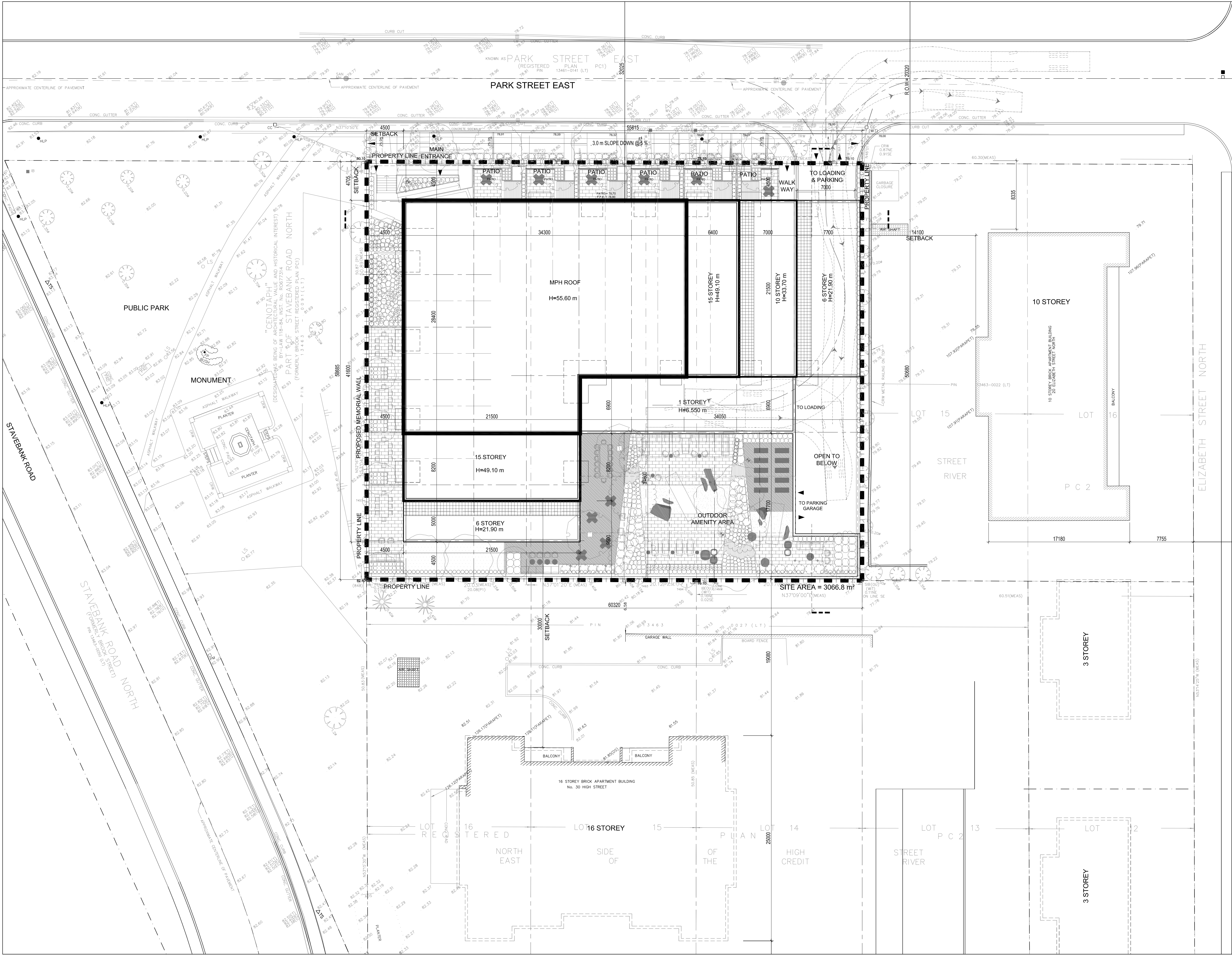
REVISIONS		
NO.	DATE	DESCRIPTION

SURVEY PLAN

SCALE :	1:300
DATE :	SEPTEMBER 07, 2017
JOB NUMBER :	109186



DWG NO:



21-29 PARK STREET EAST,
PORT CREDIT, MISSISSAUGA,
ONTARIO

EDENSHAW PARK
DEVELOPMENTS LTD.

DEVELOPMENT PROPOSAL

PROJECT STATISTICS

SITE AREA = 3,067 m² / 0.757 acres
TOTAL GFA = 19,471 m²
PROJECT FSI = 6.35

UNITS BREAKDOWN:

1 BEDROOM = 5 UNITS
1 BEDROOM + DEN = 51 UNITS
2 BEDROOM = 54 UNITS
2 BEDROOM + DEN = 67 UNITS
3 BEDROOM = 30 UNITS
TOTAL NUMBER OF UNITS = 207

AMENITY AREAS

REQUIRED: 5.6 m² per UNIT=1,159 m²
PROVIDED: 4.8 m² per UNIT= 995 m²
INCLUDING:
INDOOR AMENITY @ GRADE = 473 m²
OUTDOOR AMENITY @ GRADE = 523 m²

LANDSCAPED AREA
1,132.8 m²

PARKING SPACES PROVIDED

@ 3 BELOW GRADE LEVELS
INCLUDING 7 HC SPACES = 219

BICYCLE PARKING PROVIDED

@ P1 LEVEL=80 BICYCLE STACKERS

REVISIONS

NO.	DATE	DESCRIPTION

REZONING SUBMISSION

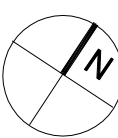
SITE PLAN &
PROJECT STATISTICS

DRAWING NAME

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DATE : SEPTEMBER 07, 2017
JOB NUMBER : 109186

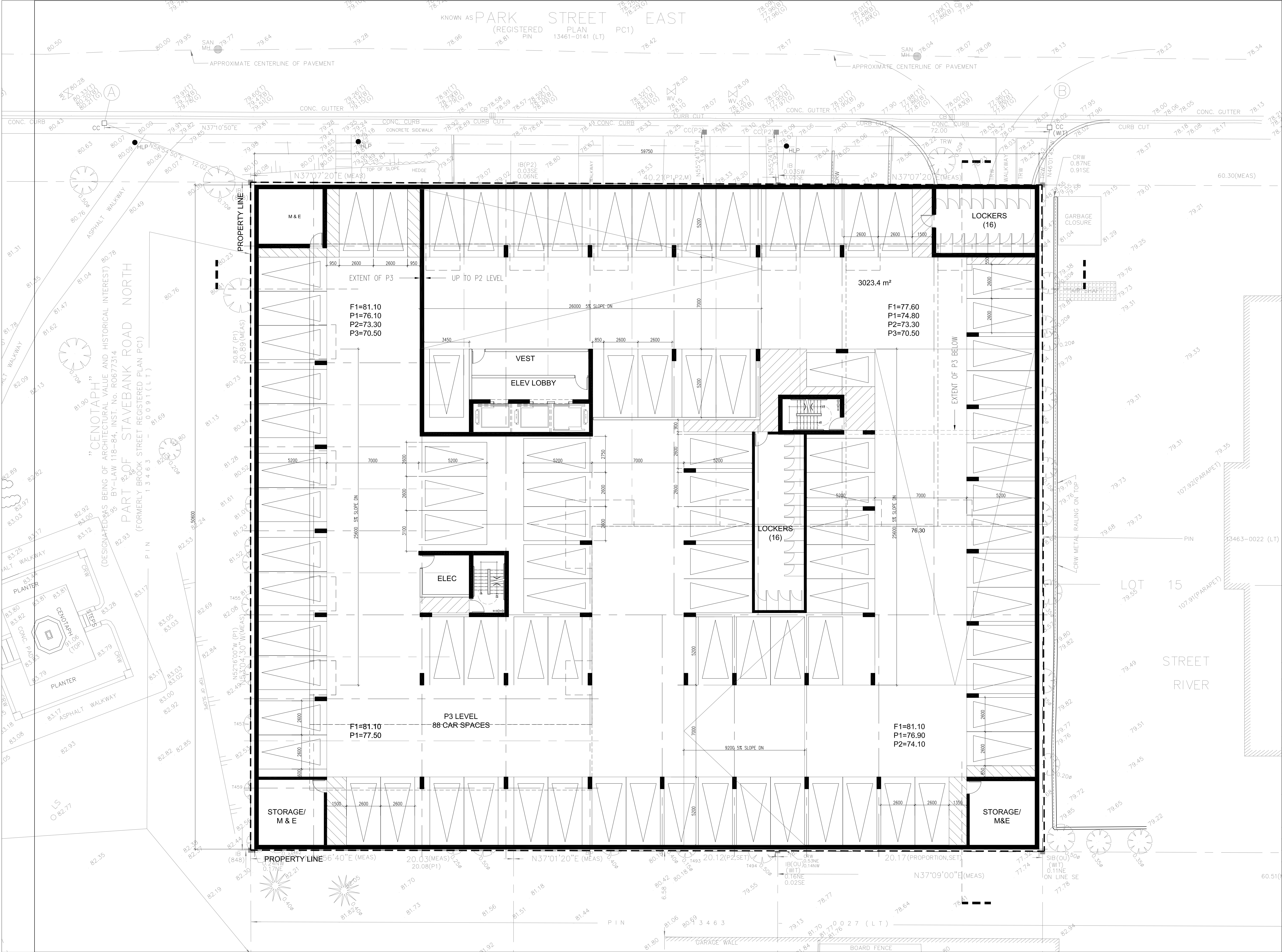


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ONTARIO

EDENSHAW PARK
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DEVELOPMENT PROPOSAL

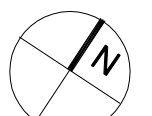
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NO.	DATE	DESCRIPTION

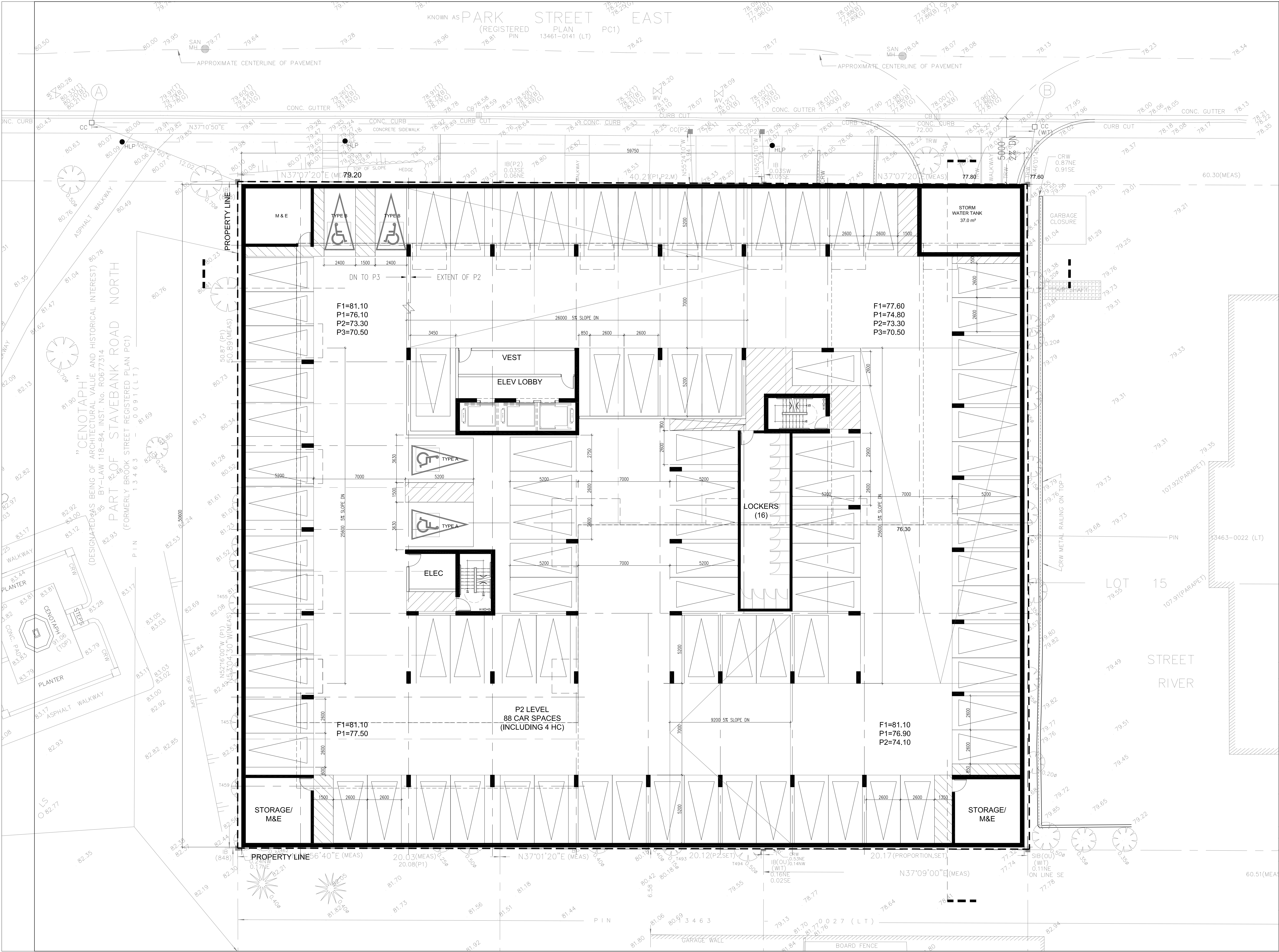
REZONING SUBMISSION

P3 PARKING LEVEL
DRAWING NAME

SCALE: 1:125
DATE: SEPTEMBER 07, 2017
JOB NUMBER: 109186

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 **A.04**
DWG NO:



21-29 PARK STREET EAST,
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ONTARIO

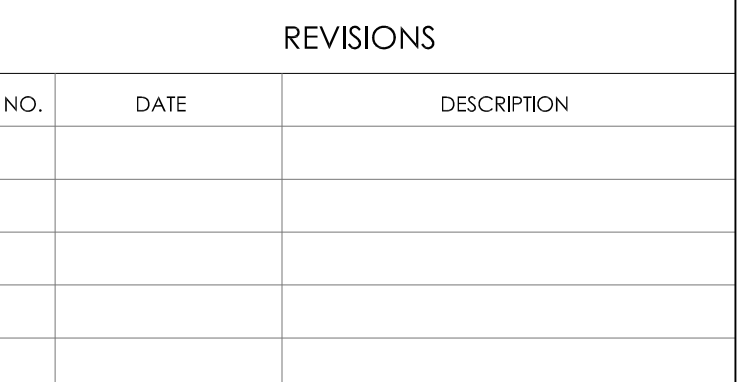
EDENSHAW PARK
DEVELOPMENTS LTD.

DEVELOPMENT PROPOSAL

REVISIONS		
NO.	DATE	DESCRIPTION
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P2 PARKING LEVEL		
DRAWING NAME		
SCALE :		1:125
DATE :		SEPTEMBER 07, 2017
JOB NUMBER :		109186
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DWG NO:		

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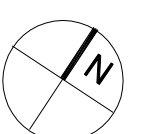
REZONING SUBMISSION

P1 PARKING LEVEL

DRAWING NAME	
SCALE :	1:125
DATE :	SEPTEMBER 07, 2017
JOB NUMBER :	109186

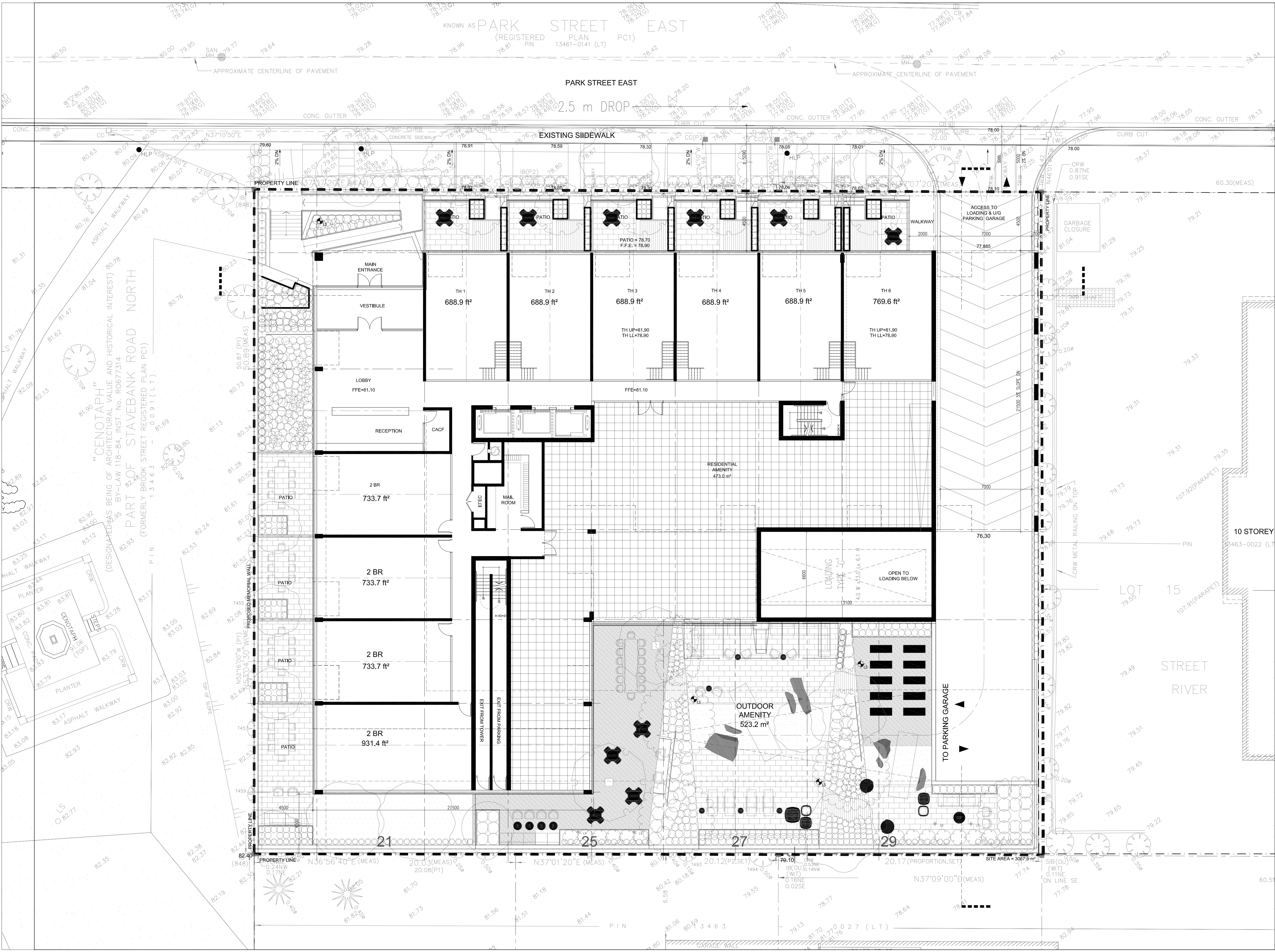


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A.06

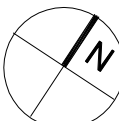
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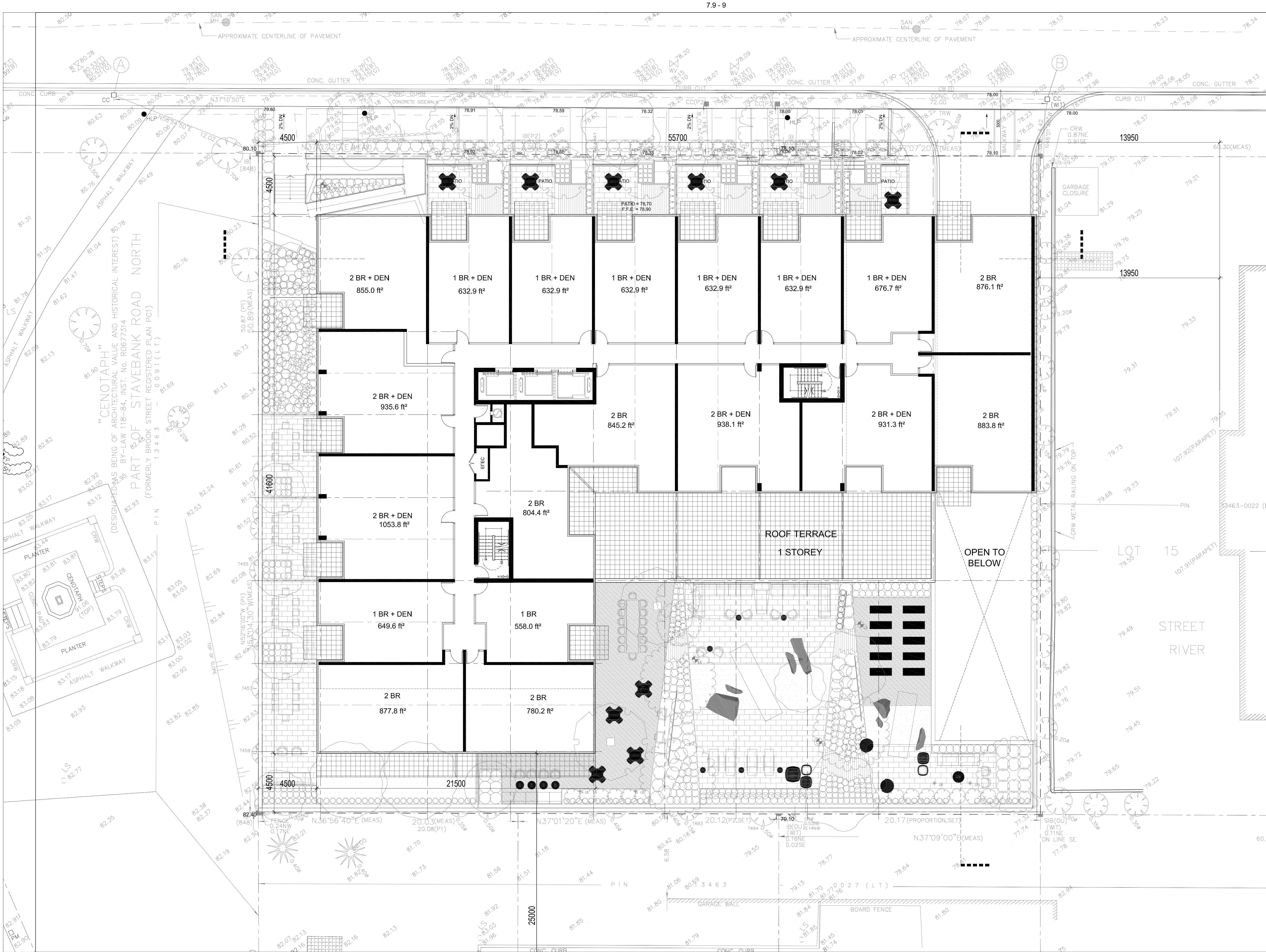


21-29 PARK STREET EAST,
PORT CREDIT, MISSISSAUGA,
ONTARIO

EDENSHAW PARK
DEVELOPMENTS LTD.

DEVELOPMENT PROPOSAL

REVISIONS		
NO.	DATE	DESCRIPTION
REZONING SUBMISSION		
GROUND FLOOR PLAN		
DRAWING NAME		
SCALE :	1:125	
DATE :	SEPTEMBER 07, 2017	
JOB NUMBER :	109186	
<div><div><div>IBI</div><div>IBI GROUP ARCHITECTS (CANADA) INC. 7th Floor, 55 St. Clair Avenue West Toronto ON , M4V 2Y7 Canada tel 416 596 1930 fax 416 596 0644 ibigroup.com</div></div></div>		
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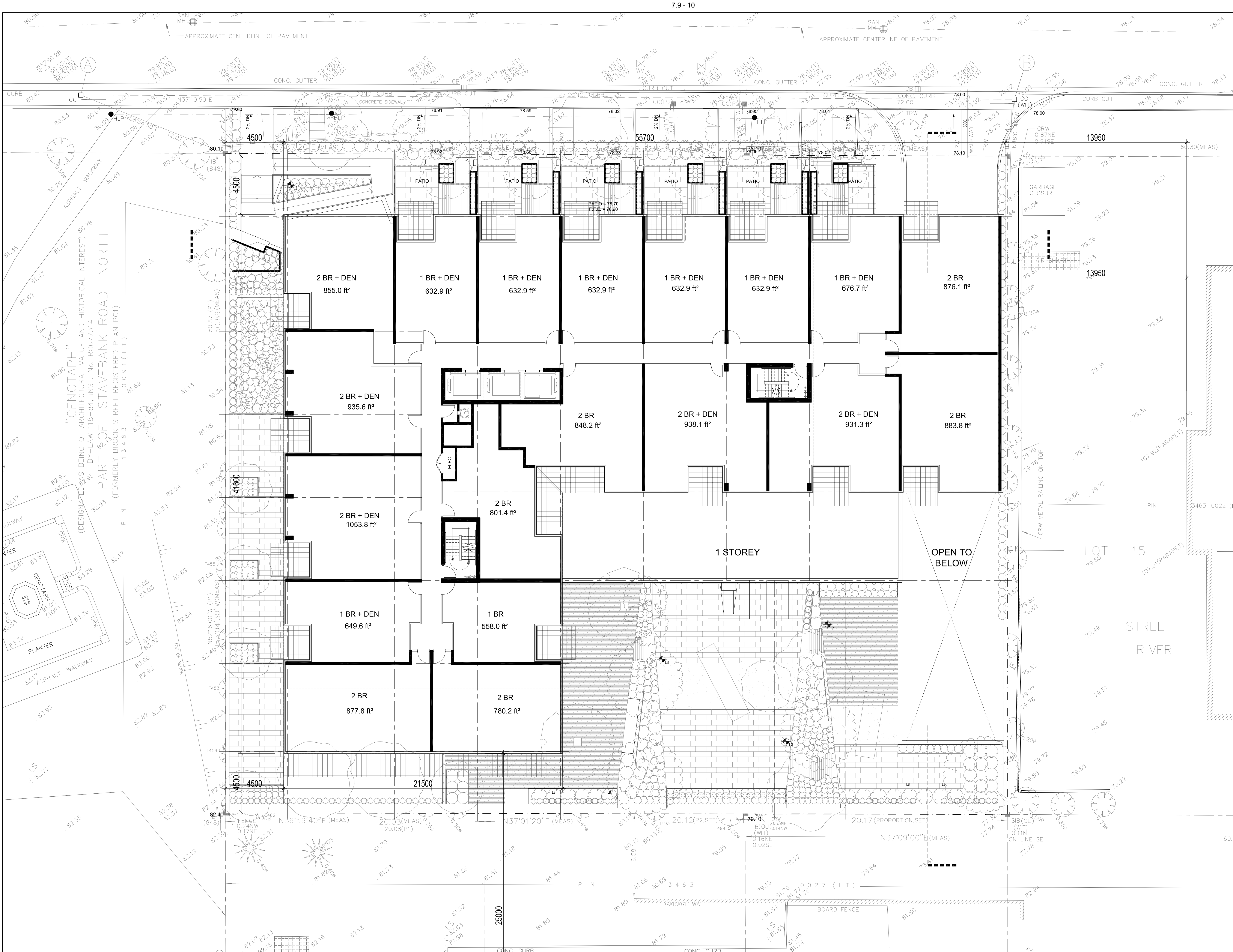


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ONTARIO

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DEVELOPMENTS LTD.

DEVELOPMENT PROPOSAL

REVISIONS		
NO.	DATE	DESCRIPTION
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2 FLOOR PLAN		
DRAWING NAME		
SCALE :	1:125	
DATE :	SEPTEMBER 07, 2017	
JOB NUMBER :	109186	
<div><div><div>IBI</div></div><div><div>IBI GROUP ARCHITECTS (CANADA) INC.</div><div>7th Floor, 55 St. Clair Avenue West</div><div>Toronto ON , M4V 2Y7 Canada</div><div>tel 416 596 1930</div><div>fax 416 596 0644</div><div>ibigroup.com</div></div></div>		
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NO.	DATE	DESCRIPTION

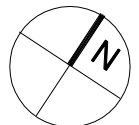
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3 - 6 FLOOR PLAN
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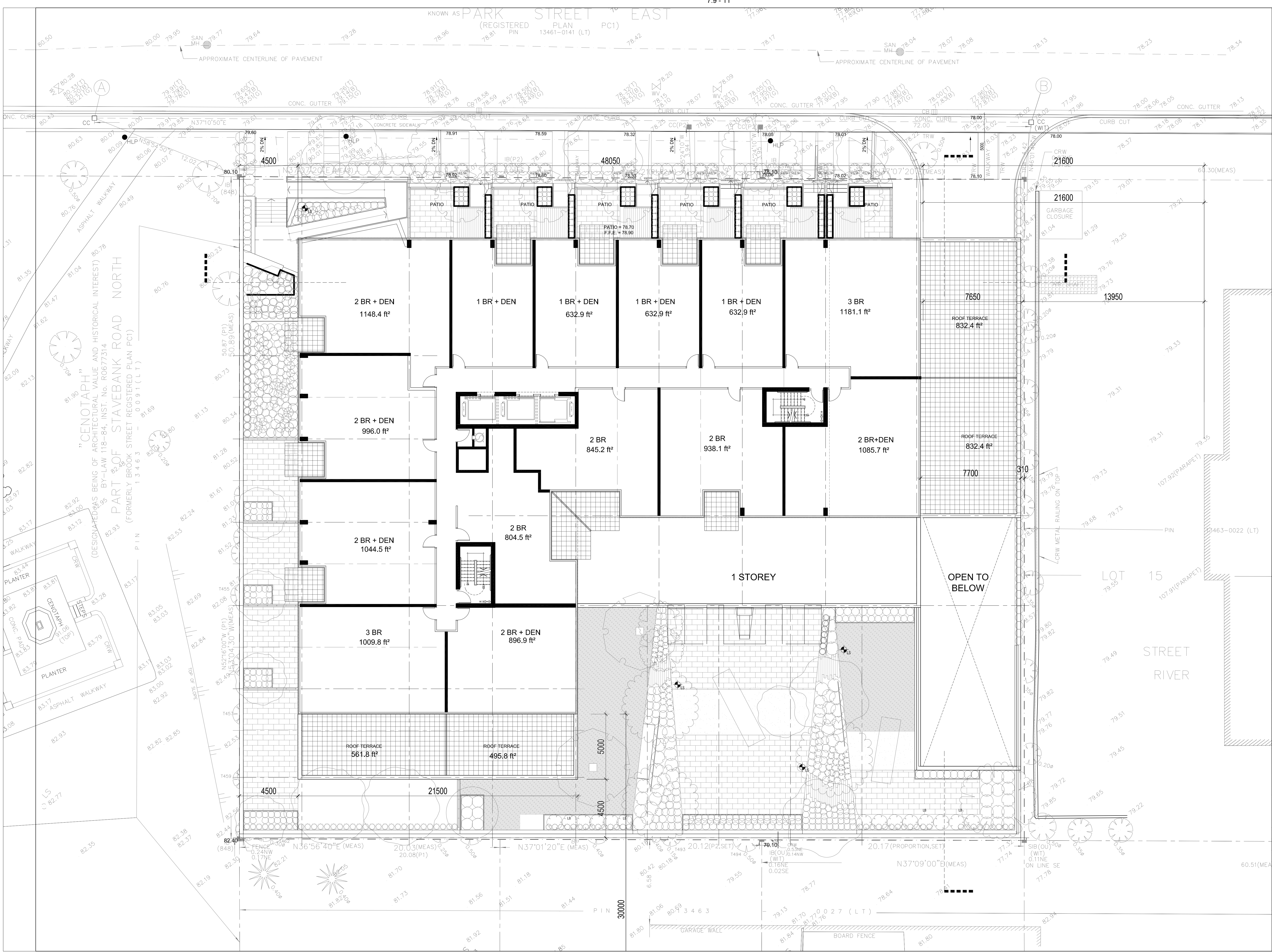
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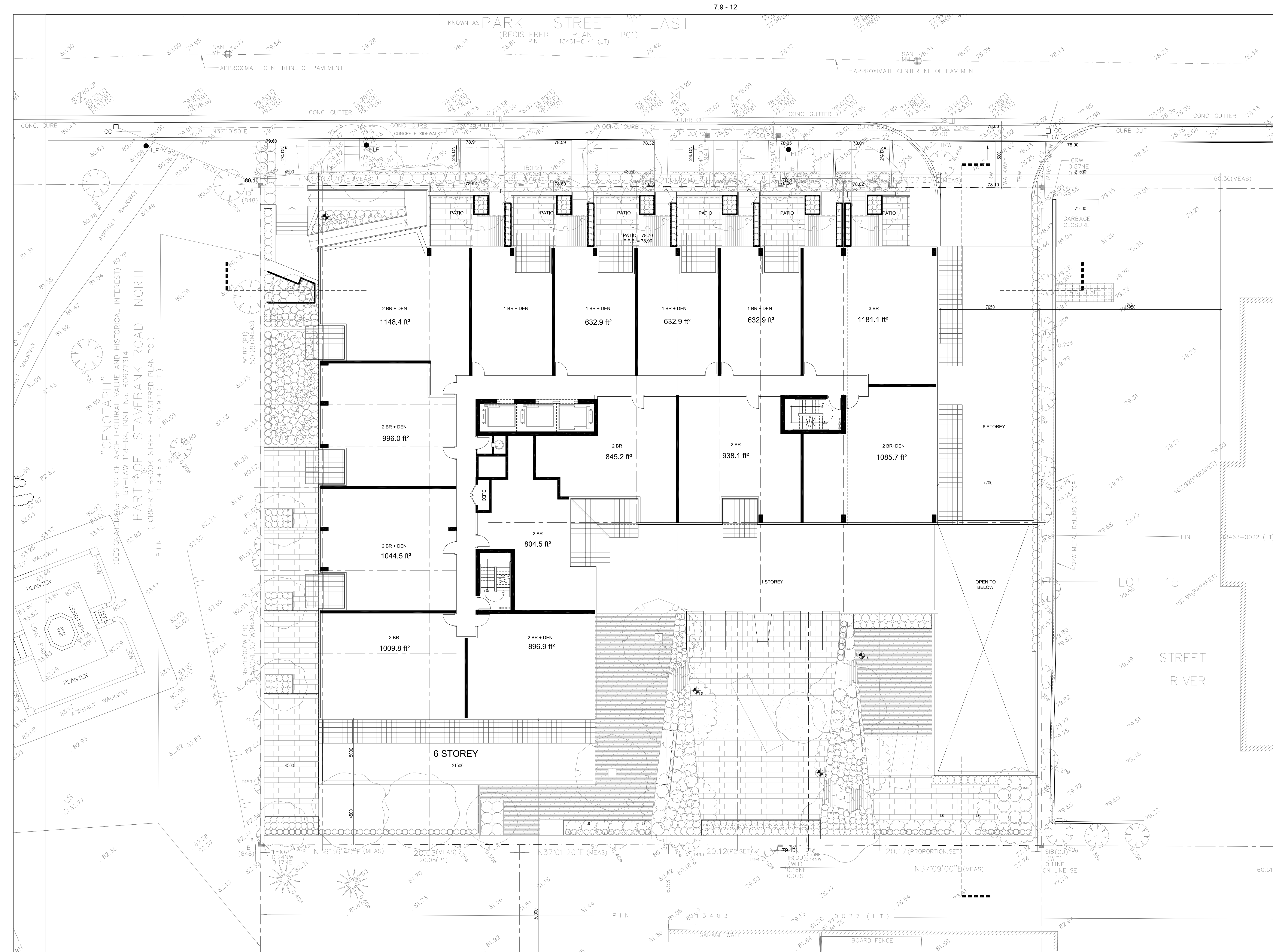


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DEVELOPMENT PROPOSAL

REVISIONS		
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REZONING SUBMISSION		
7 FLOOR PLAN		
DRAWING NAME		
SCALE :		1:125
DATE :		SEPTEMBER 07, 2017
JOB NUMBER :		109186
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		DWG NO:



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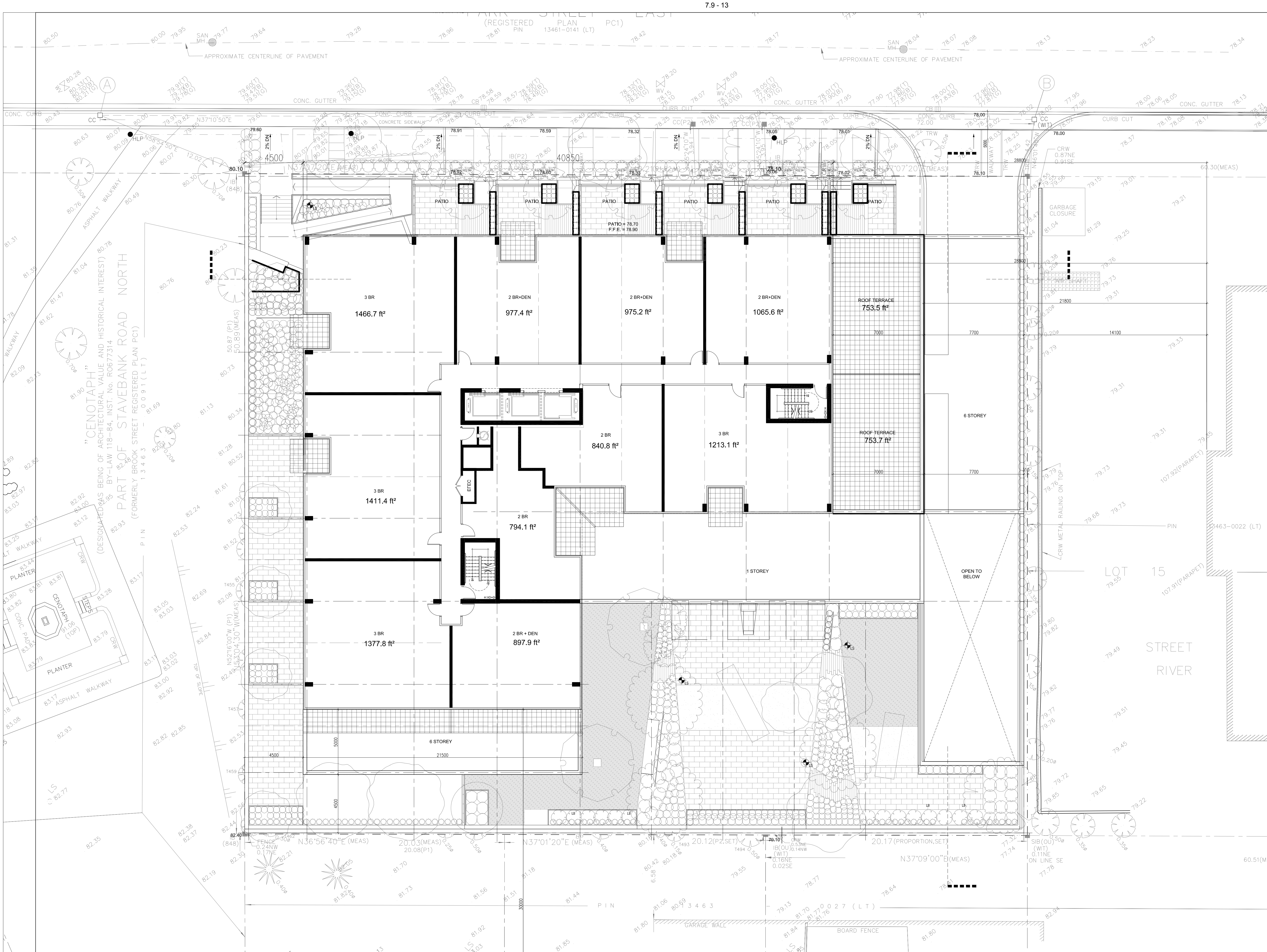
REVISIONS		
NO.	DATE	DESCRIPTION

REZONING SUBMISSION

8 - 10 FLOOR PLAN	
DRAWING NAME	
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DATE :	SEPTEMBER 07, 2017
JOB NUMBER :	109186



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REVISIONS		
NO.	DATE	DESCRIPTION

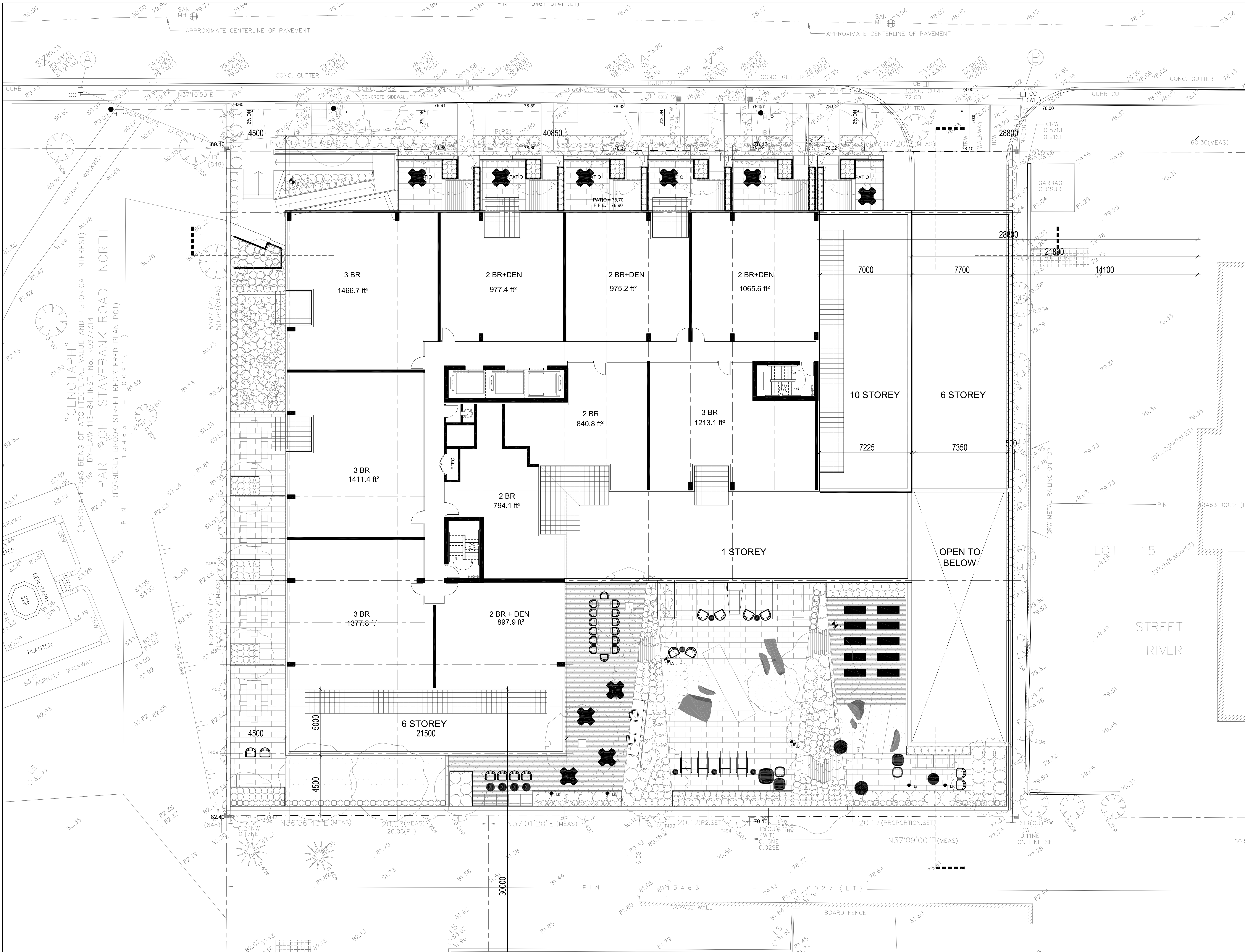
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11 FLOOR PLAN
DRAWING NAME

SCALE: 1:125
DATE: SEPTEMBER 07, 2017
JOB NUMBER: 109186

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A.12
DWG NO:

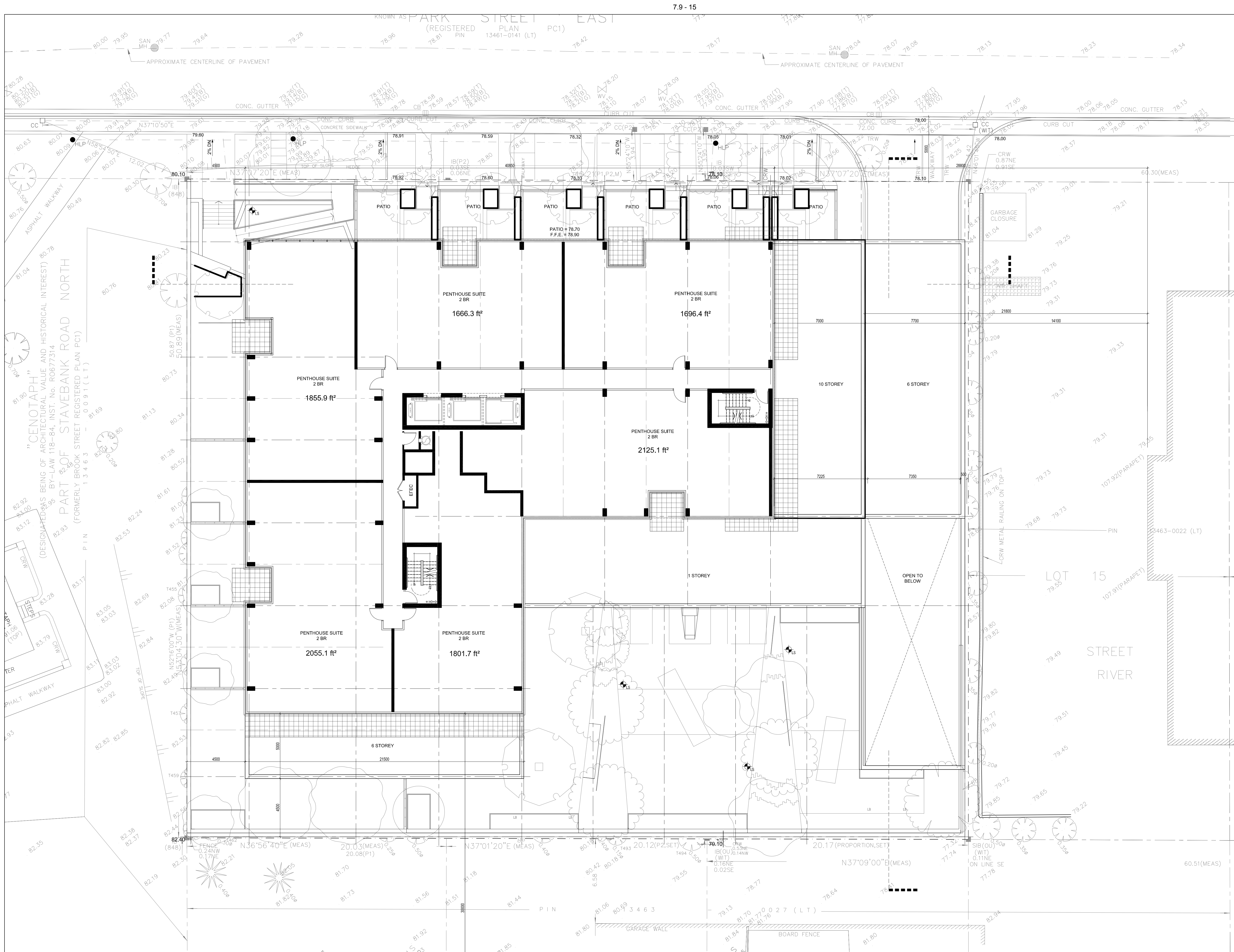


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DEVELOPMENT PROPOSAL

REVISIONS		
NO.	DATE	DESCRIPTION
REZONING SUBMISSION		
12-14 FLOOR PLAN DRAWING NAME		
SCALE:	1:125	
DATE:	SEPTEMBER 07, 2017	
JOB NUMBER:	109186	
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REVISIONS		
NO.	DATE	DESCRIPTION

REZONING SUBMISSION

15 FLOOR PLAN

DRAWING NAME

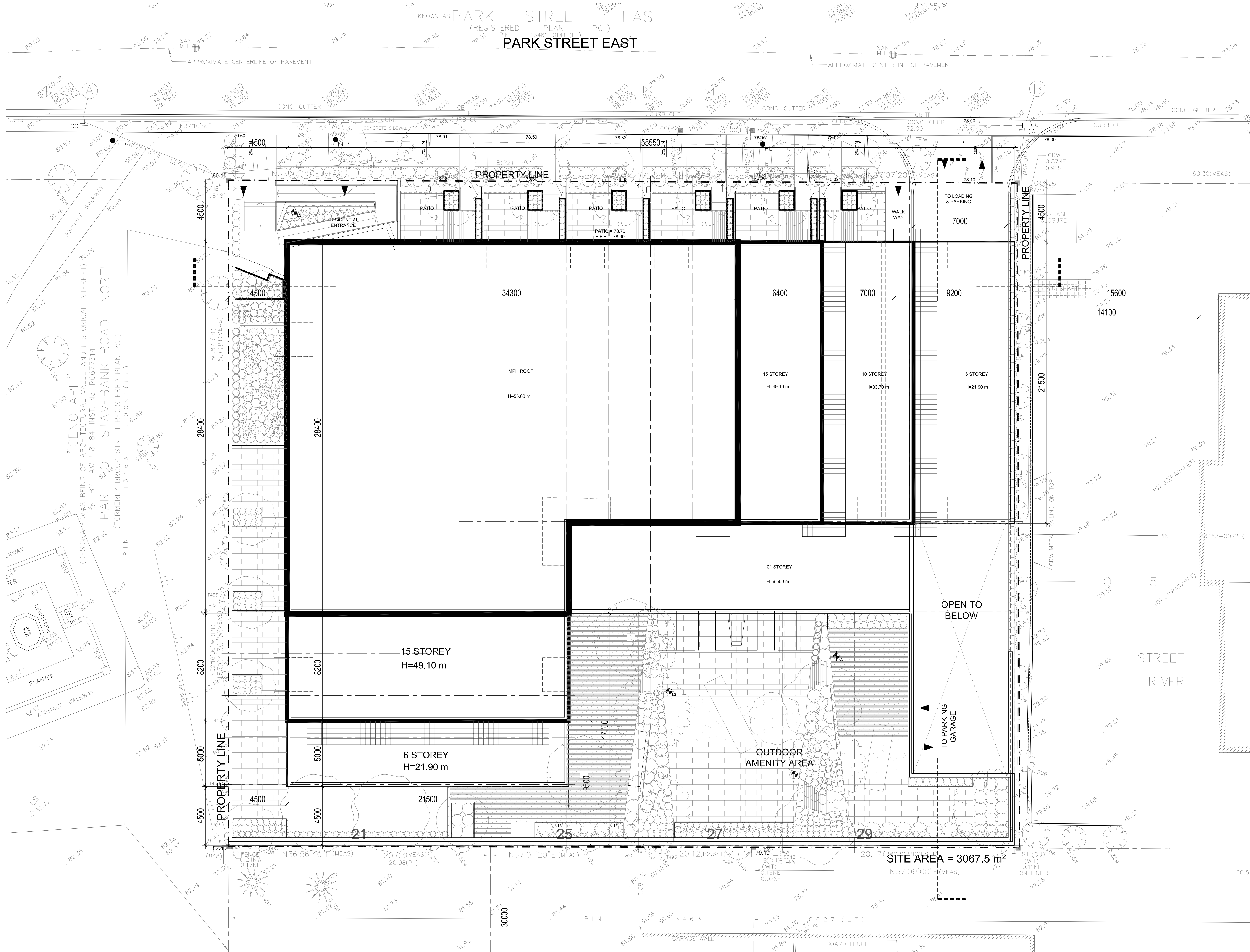
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DATE: SEPTEMBER 07, 2017

JOB NUMBER: 109186

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DEVELOPMENT PROPOSAL

REVISIONS		
NO.	DATE	DESCRIPTION

REZONING SUBMISSION

ROOF PLAN
DRAWING NAME

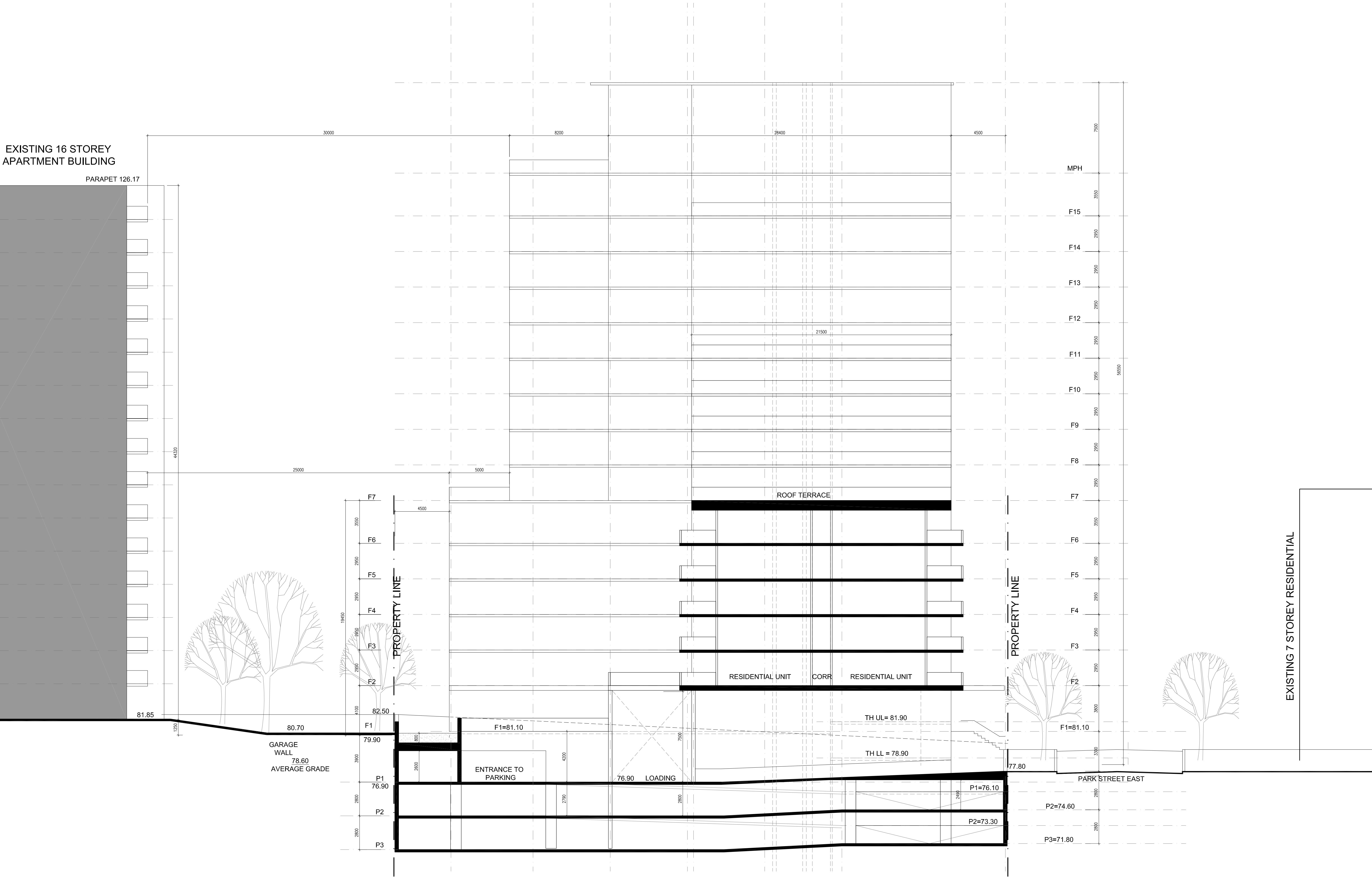
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JOB NUMBER : 109186

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A.15
DWG NO:

EXISTING 16 STOREY
APARTMENT BUILDING

PARAPET 126.17



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ONTARIO

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DEVELOPMENT PROPOSAL

REVISIONS		
NO.	DATE	DESCRIPTION

REZONING SUBMISSION

NORTH-SOUTH BUILDING SECTION LOOKING WEST	
DRAWING NAME	
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DATE :	SEPTEMBER 07, 2017
JOB NUMBER :	109186

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A.16

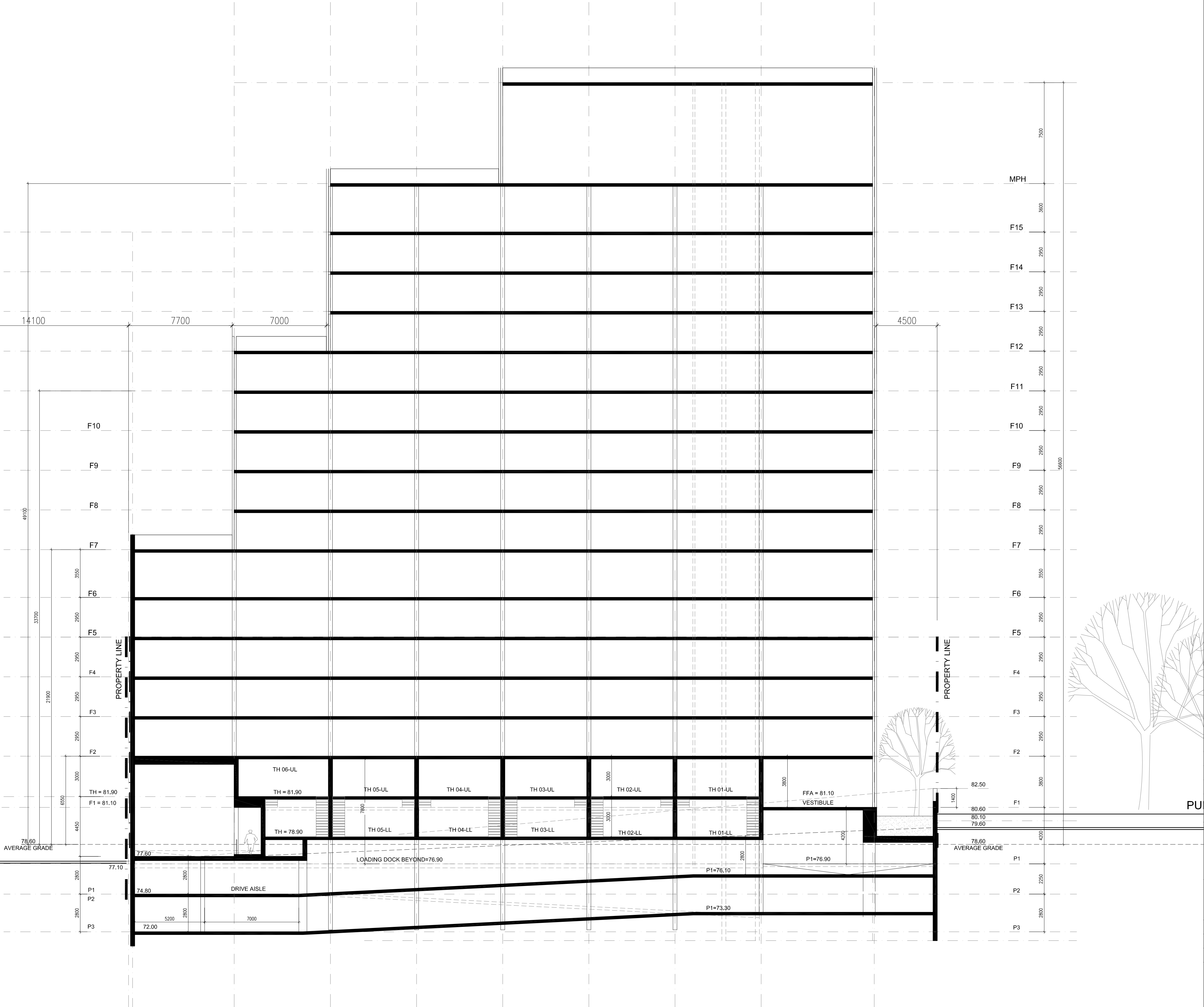
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EXISTING 10 STOREY
BUILDING



REVISIONS		
NO.	DATE	DESCRIPTION

REZONING SUBMISSION

EAST-WEST
BUILDING SECTION LOOKING SOUTH
DRAWING NAME

SCALE : 1:150

DATE : SEPTEMBER 07, 2017

JOB NUMBER : 109186

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REVISIONS

NO.	DATE	DESCRIPTION

REZONING SUBMISSION

BUILDING ELEVATION

DRAWING NAME

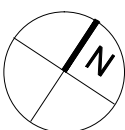
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JOB NUMBER : 109186



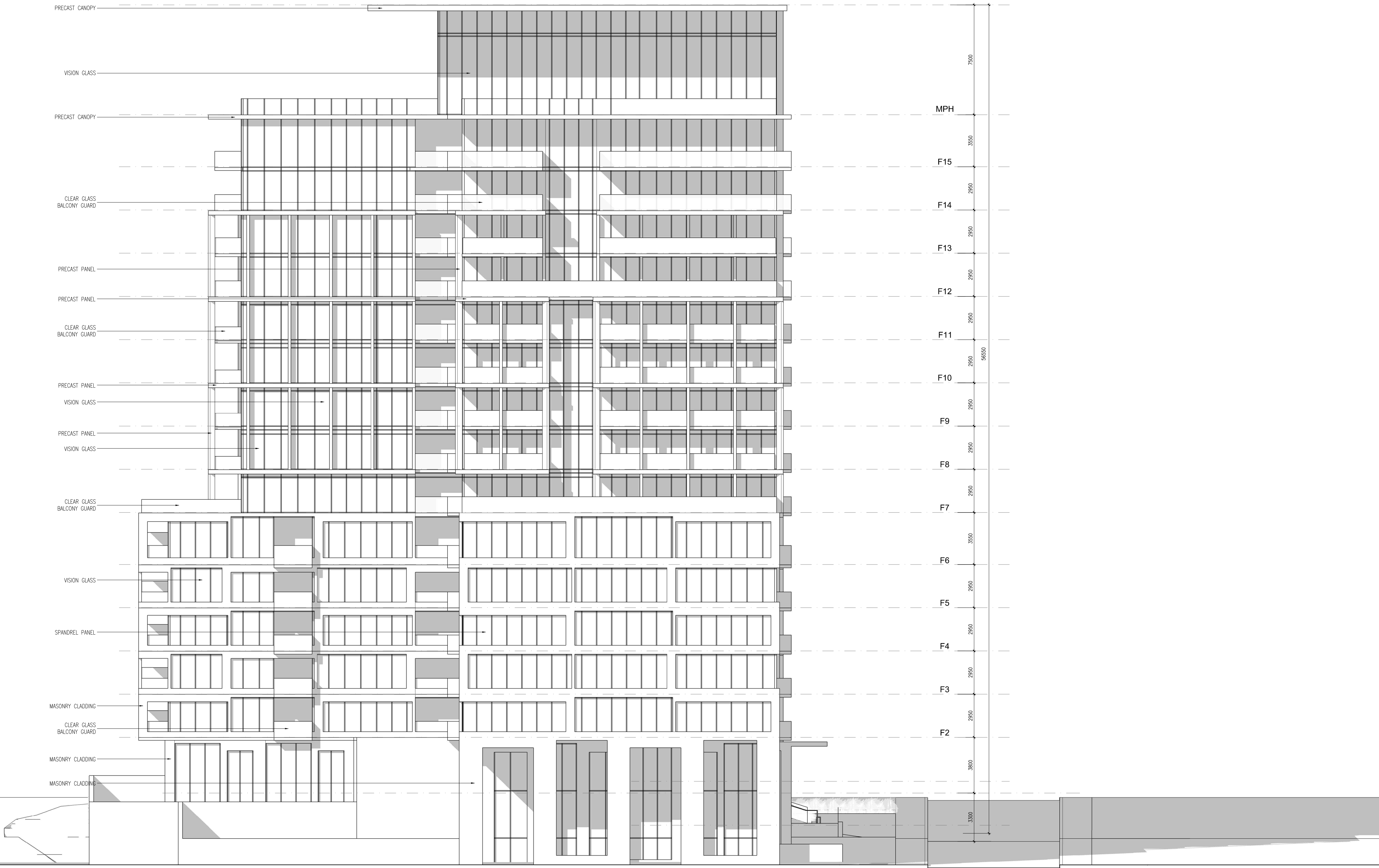
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REVISIONS

NO.	DATE	DESCRIPTION

REZONING SUBMISSION

BUILDING ELEVATION

DRAWING NAME

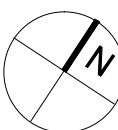
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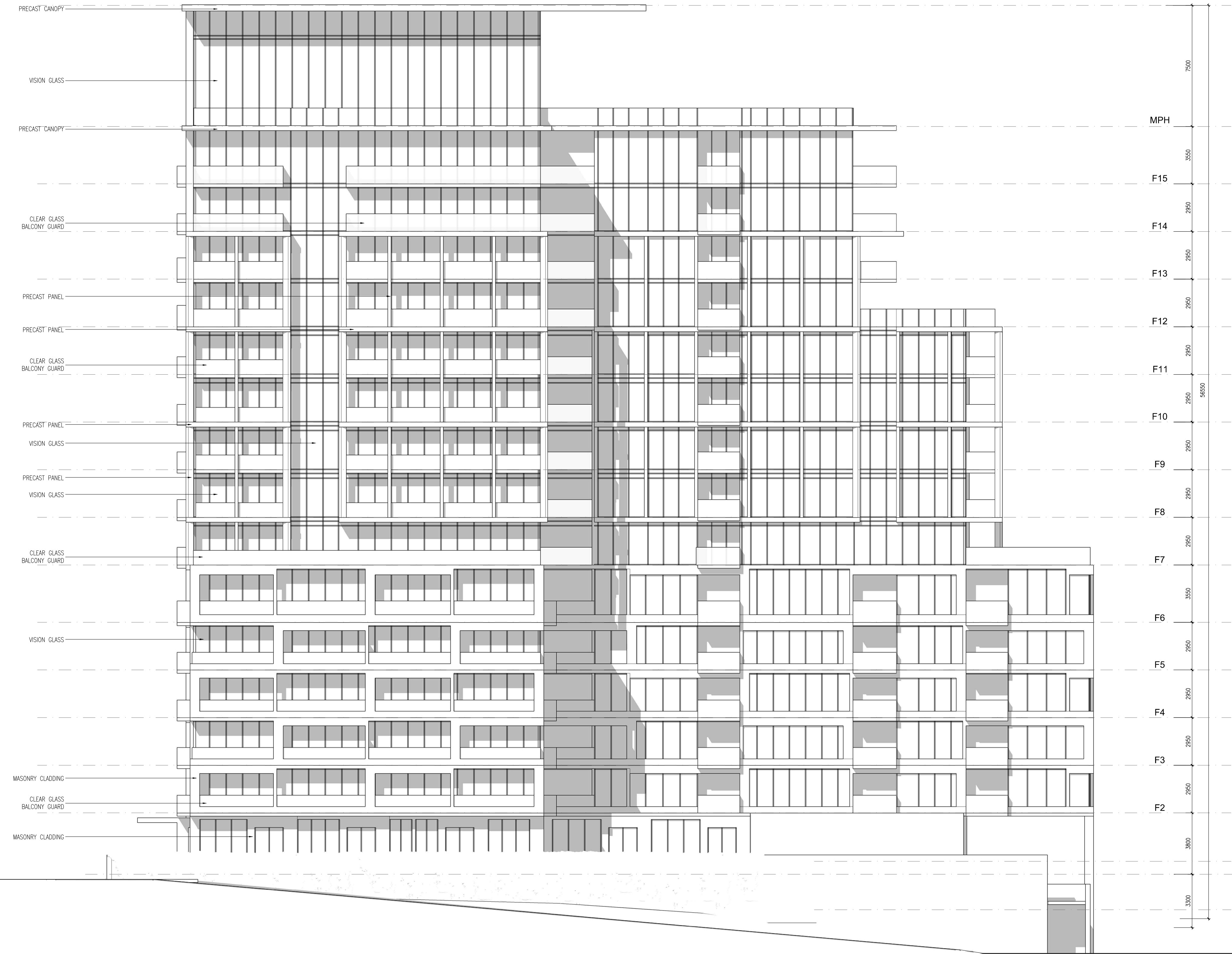
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DWG NO:

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REVISIONS

NO.	DATE	DESCRIPTION

REZONING SUBMISSION

BUILDING ELEVATION

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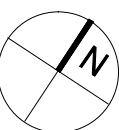
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JOB NUMBER : 109186



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1 NORTH ELEVATION FACING PARK ST. E

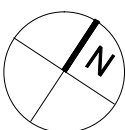
REVISIONS		
NO.	DATE	DESCRIPTION

REZONING SUBMISSION

BUILDING ELEVATION	
DRAWING NAME	
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DATE :	SEPTEMBER 07, 2017
JOB NUMBER :	109186



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A.21

DWG NO:

City of Mississauga
Memorandum



Date: 2017/11/23
To: Chair and Members of Heritage Advisory Committee
From: Paul Damaso, Director, Culture Division
Meeting Date: 2018/01/09
Subject: Credit River Lakeshore Railway Bridge

This memorandum and its attachment are presented for HAC's information only.

The subject property is listed on the City's Heritage Register. As per section 7.4.1.12 of the Mississauga Official Plan, "The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a Heritage Impact Assessment, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction." As such, the Cultural Heritage Evaluation Report and Heritage Impact Assessment are attached for your reference.

Attachments

Appendix 1: Cultural Heritage Evaluation Report
Appendix 2: Heritage Impact Assessment

A handwritten signature in black ink, appearing to read 'Paul Damaso', written in a cursive style.

Prepared by: Paul Damaso, Director, Culture Division

Final Cultural Heritage Evaluation Report:

Credit River Bridge

For



Prepared by:



Reviewed by:



MORRISON HERSHFIELD



Gannett Fleming

Gannett Fleming Project No. 060277

Metrolinx Electrification Project

Contract No. QBS-2014-IEP-002

Submittal Date: September 2017

METROLINX GO RAIL ELECTRIFICATION

Quality Assurance Document Release Form

Name of Firm: Archaeological Services Inc. (ASI)

Document Name: Final Cultural Heritage Evaluation Report: Credit River Bridge Rev.3

Submittal Date: September 8, 2017

Discipline: Environmental Assessment

Prepared By: Johanna Kelly and Lindsay Graves Date: January 16, 2017

Reviewed By: Annie Veilleux Date: September 8, 2017

Approved By: Rebecca Sciarra Date: September 8, 2017
Project Manager

The above electronic signatures indicate that the named document is controlled by GF Canada ULC, and has been:

1. Prepared by qualified staff in accordance with generally accepted professional practice.
2. Checked for completeness and accuracy by the appointed discipline reviewers and that the discipline reviewers did not perform the original work.
3. Reviewed and resolved compatibility interfaces and potential conflicts among the involved disciplines.
4. Updated to address previously agreed-to reviewer comments, including any remaining comments from previous internal or external reviews.
5. Reviewed for conformance to scope and other statutory and regulatory requirements.
6. Determined suitable for submittal by the Project Manager.

REVISION HISTORY

Revision	Date	Comments
0	July 27, 2016	Initial Release to Metrolinx
1	December 23, 2016	Revised report reflecting new information, corrected information, client review comments, etc.
2	January 16, 2017	Revised report reflecting new information, corrected information, client review comments, etc.
3	September 8, 2017	Final Report to Metrolinx reflecting new template and finalization

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FINAL CULTURAL HERITAGE EVALUATION REPORT: CREDIT RIVER BRIDGE



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APPENDIX A: CULTURAL HERITAGE LANDSCAPE INVENTORY EXCERPT

APPENDIX B: COMPLETED QUESTIONNAIRES

APPENDIX C: COMPARATIVE ANALYSIS

REPORT DISCLAIMER

NOTWITHSTANDING the results and recommendations presented in this study, Archaeological Services Inc. notes that no cultural heritage assessment, no matter how thorough or carefully completed, can necessarily identify every property and/or structure that has not been previously identified as a known or potential cultural heritage resource. Cultural heritage assessments for transportation related projects are limited to the public right-of-way, and as such, potential cultural heritage resources on private property may be screened from view by vegetation and/or other barriers. In the event that a potential cultural heritage resource is found during subsequent construction activities, the consultant cultural heritage specialist and approval authority should be immediately notified.

Executive Summary

ASI was contracted by Morrison Hershfield on behalf of Metrolinx to conduct a Cultural Heritage Evaluation Report (CHER) and Cultural Heritage Evaluation Recommendation Report (CHERR) of the Credit River Bridge on the Lakeshore West rail corridor as part of the GO Rail Network Electrification Transit Project Assessment Project (TPAP). Metrolinx is undertaking a TPAP study under *Ontario Regulation 231/08 - Transit Projects and Metrolinx Undertakings* for electrification of the GO Rail Network. The Credit River Bridge was identified as a Potential Provincial Heritage Property as part of the Cultural Heritage Screening Report completed for the GO Rail Network Electrification TPAP.

The Credit River Bridge is located at Mile 13.27 of the GO Transit Lakeshore West rail corridor, and is owned by Metrolinx. The bridge, built in 1903 and twinned in 2008, carries three tracks of rail traffic in an east and west direction across the Credit River, between Stavebank Road and Mississauga Road, in the City of Mississauga.

Part 1 of this CHER provides a description of the property, including a summary of its historical and current context (Section 1), a description of methodology and sources (Section 2), existing heritage recognition of the resource (Section 3), a description of adjacent lands (Section 4), summary of previous archaeological assessment (Section 5), community input (Section 6), and discussion of cultural heritage value (Section 7). A data sheet is provided in Section 8 and figures, including mapping and photographs, are provided in Section 9. Part 2 of this CHER contains the Recommendations Report which presents the evaluation tables outlining criteria set out in Ontario Regulations 9/06 and 10/06 and recommended outcome of the evaluation, including the draft statement of cultural heritage value and recommended list of heritage attributes (as appropriate).

The CHER was conducted by Lindsay Graves, Cultural Heritage Specialist and Assistant Manager of the Cultural Heritage Division, ASI. Information from the CHER was used in the decision making for the CHERR.

1 Introduction

ASI was contracted by Morrison Hershfield on behalf of Metrolinx to conduct a Cultural Heritage Evaluation Report (CHER) and Cultural Heritage Evaluation Recommendation Report (CHERR) of the Credit River Bridge on the Lakeshore West rail corridor as part of the GO Rail Network Electrification Transit Project Assessment Project (TPAP). Metrolinx is undertaking a TPAP study under *Ontario Regulation 231/08 - Transit Projects and Metrolinx Undertakings* for electrification of the GO Rail Network. The Credit River Bridge was identified as a Potential Provincial Heritage Property as part of the Cultural Heritage Screening Report completed for the GO Rail Network Electrification TPAP.

The objective of this CHER is to provide evidence about reasons why the subject resource may be of cultural heritage value or interest, and identify the physical elements that contribute to its heritage value. Research for this CHER was conducted under the senior project management of Lindsay Graves, Assistant Manager of the Cultural Heritage Division, ASI.

1.1 Description of Property

The Credit River Bridge is located at Mile 13.27 of the GO Transit Lakeshore West rail corridor, and is located in the City of Mississauga (Figure 1-1 and Figure 1-2). The bridge, built in 1903 and twinned in 2008, carries three tracks of rail traffic in an east and west direction across the Credit River, between Stavebank Road and Mississauga Road. While rail traffic travels in an east-west direction, it should be noted that at this segment of the rail corridor, the bridge and corridor is on a northeast-southwest alignment, and the Credit River flows northwest to southeast under the bridge. The Credit River Bridge is located within Metrolinx-owned parcel PIN 13456-0580.

1.2 Historical Summary

The Credit River Bridge is located in part of Lot 6, Concession II in the historic Township of Toronto South in the former County of Peel. The bridge is located in the historic village of Port Credit, which developed at the mouth of the Credit River between Hurontario Street and Mississauga Road. The Credit River Bridge was built in 1903 to the designs and specifications of the Grand Trunk Railway Company, and it was constructed by the Canadian Bridge Company Limited of Walkerville, Ontario.

1.3 Current Context

The Credit River Bridge is located in Port Credit, Mississauga. The general area around the Credit River Bridge is mixed residential and recreational. The residential areas primarily consist of single family homes with some multi-dwelling buildings. The recreational areas feature a combination of historic mixed use recreational space and more recently constructed recreational businesses.

The properties adjacent to the bridge include: the Port Credit Memorial Park to the east; a single family residential building to the north and to the west, and the Royal Canadian Legion (Port Credit Branch) and parking area to the south. All of these adjacent features appear to have been constructed more than 40 years ago.

Figure 1-1: Location of the Credit River Bridge study area in the City of Mississauga, Ontario (Open Street Map)

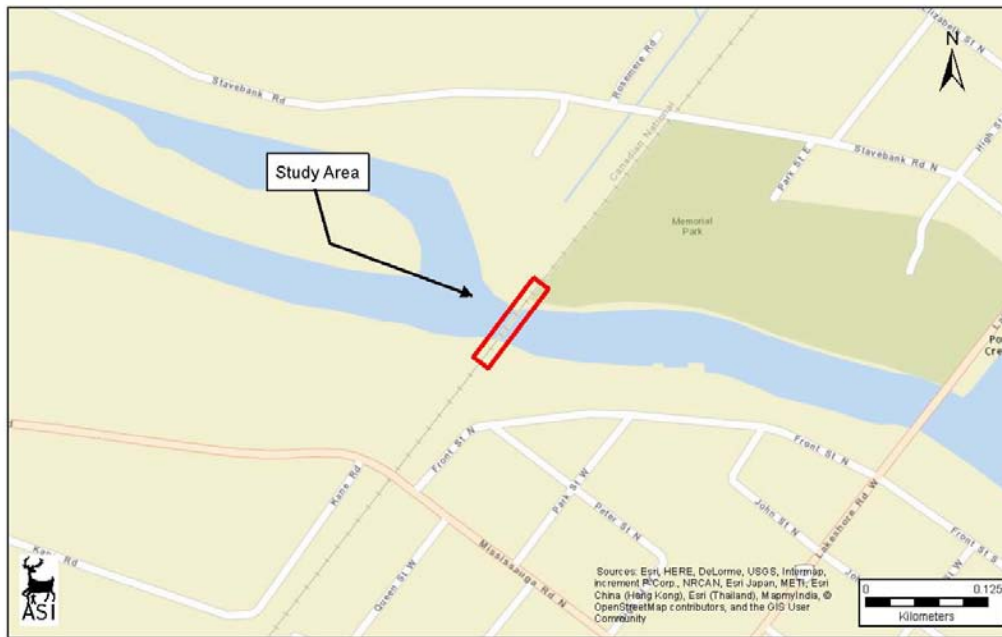


Figure-1-2: South elevation of the Credit River Bridge in the City of Mississauga, Ontario



2 Methodology and Sources

2.1 Legislation and Policy Context

This cultural heritage screening considers cultural heritage resources in the context of improvements to specified areas, pursuant to *Ontario Regulation 231/08: Transit Projects and Metrolinx Undertakings (Transit Projects Regulation)* and the *Ontario Environmental Assessment Act* (EAA 1990). Pursuant to the *Environmental Assessment Act*, applicable infrastructure projects are subject to assessment so as to determine related impacts on above ground cultural heritage resources (MTO 2006). Infrastructure projects have the potential to impact cultural heritage resources in a variety of ways such as loss or displacement of resources through removal or demolition and the disruption of resources by introducing physical, visual, audible or atmospheric elements that are not in keeping with the resources and/or their setting.

When considering cultural heritage resources in the context of improvements to specified areas, a 40 year old threshold is used as a guiding principle when identifying cultural heritage resources. While identification of a resource that is 40 years old or older does not confer outright heritage significance, this threshold provides a means to collect information about resources that may retain heritage value. Similarly, if a resource is slightly younger than 40 years old, this does not preclude the resource from retaining heritage value.

The TPAP is defined in sections 6-17 in *Ontario Regulation 213/08: Transit Projects and Metrolinx Undertakings*, and provides a series of relevant provisions and definitions. The TPAP Guide (January 2014) includes provisions to consider when the proposed project may have a negative impact on a matter of provincial importance, which is defined as follows (2014: 2):

“...a matter of provincial importance that relates to the natural environment or has cultural heritage value or interest...”

The TPAP Guide further notes that identification and assessment of potentially impacted built heritage resources, cultural heritage landscapes, and protected heritage properties are relevant in determining if a matter is of ‘provincial importance’ (2014: 10). It should be noted that the TPAP Guide acknowledges that a built heritage resource, cultural heritage landscape, or protected heritage property does not necessarily need to meet criteria set out under *Regulation 10/06* of the *Ontario Heritage Act* to be considered of ‘provincial importance’.

The analysis used throughout the cultural heritage resource assessment process addresses cultural heritage resources under other various pieces of legislation and their supporting guidelines:

- *Environmental Assessment Act* (R.S.O. 1990, Chapter E.18)
 - *Guideline for Preparing the Cultural Heritage Resource Component of Environmental Assessments* (MCC – MOE 1992)
 - *Guidelines on the Man-Made Heritage Component of Environmental Assessments* (MCR – MOE 1981)
- *Ontario Heritage Act* (R.S.O. 1990, Chapter O.18) and a number of guidelines and reference documents prepared by the Ministry of Tourism and Culture (MTC):
 - *Standards and Guidelines for the Conservation of Provincial Heritage Properties* (MTC 2010)
 - *Ontario Heritage Tool Kit* (MCL 2006)
- *Planning Act* (R.S.O. 1990, Chapter P.13) and the 2014 *Provincial Policy Statement*

This assessment was also guided by the *Metrolinx Interim Cultural Heritage Management Process* (Metrolinx 2013b), and the *Draft Terms of Reference for Consultants: Cultural Heritage Evaluation Report and Cultural Heritage Evaluation Report Recommendations* (Metrolinx 2014).

2.2 Approach to Cultural Heritage Evaluation Report

The scope of a CHER is guided by the Ministry of Tourism, Culture and Sport’s *Ontario Heritage Toolkit* (2006) as well as the Metrolinx *Draft Terms of Reference for Consultants: Cultural Heritage Evaluation*

Report and Cultural Heritage Evaluation Report Recommendations (2014). Generally, CHERs include the following components:

- A general description of the history of the study area as well as a detailed historical summary of property ownership and building(s) development;
- A description of the cultural heritage landscape and built heritage resources;
- Representative photographs of the exterior and interior of a building or structure, and character-defining architectural details;
- A cultural heritage resource evaluation guided by the *Ontario Heritage Act* criteria;
- A summary of heritage attributes;
- Historical mapping, photographs; and
- A location plan.

A site visit was conducted by John Sleath, Cultural Heritage Assistant, ASI, on 22 June 2016, to conduct photographic documentation of the subject resource. While the Credit River Bridge was accessible from the public right-of-way, it was only accessible from the south. As such, the bridge abutments and the north elevation of the bridge were not viewed during the site visit.

Using background information and data collected during the site visit, the cultural heritage resource is evaluated using criteria contained within Ontario Regulations 9/06 and 10/06 of the *Ontario Heritage Act*. The two criteria sets share a requirement to fully understand the history, design and associations of all cultural heritage resources of the property. The following differences between the two sets of criteria should be noted (Metrolinx 2014: 12):

- Regulation 9/06 requires a consideration of the community context
- Regulation 10/06 requires a consideration of the provincial context

2.2.1 List of Key Sources and Research Limitations

Key Sources

Background historical research, which includes the consultation of primary and secondary source documents, photos, and historic mapping, was undertaken to identify early settlement patterns and broad agents or themes of change in a study area. In addition, on-site archival research was undertaken at the following libraries and archives to build upon information gleaned from other primary and secondary materials:

- Canadiana Room at the City of Mississauga's Central Library;

- Mississauga Heritage Foundation; and
- Archives of Ontario.

Where available, comprehensive bridge inventories were consulted for comparative analysis purposes to determine the potential design value of the subject bridge. The Metrolinx Master Bridge List (August 31, 2015) recording information such as bridge name, location, construction date, material, bridge type, number of spans and overall bridge length, was provided by Metrolinx and utilized for comparative purposes. Additional sources were considered for comparative analysis where relevant.

Available federal, provincial and municipal heritage inventories and databases were also consulted to obtain information about the property. These included:

- The City of Mississauga's Heritage Register;
- The Ontario Heritage Trust's *Provincial Plaque Program* database;
- Park's Canada's *Directory of Federal Heritage Designations*, a searchable on-line database that identifies National Historic Sites, National Historic Events, National Historic People, Heritage Railway Stations, Federal Heritage Buildings, and Heritage Lighthouses; and
- Park's Canada's *Canada's Historic Places* website: a searchable on-line register that provides information on historic places recognized for their heritage value at the local, provincial, territorial and national levels.

Previous consultant reports associated with potential above-ground cultural heritage resources and archaeological resources within and/or adjacent to the GO Rail Network Electrification TPAP included the following:

- Cultural Heritage Screening Report: GO Rail Network Electrification TPAP (ASI 2016)

A full list of references consulted can be found in Section 11 of this CHER.

Research Limitations

No research limitations were identified.

2.3 Consultation

Consultation with the Ontario Heritage Trust, the Ministry of Tourism, Culture, and Sport (MTCS), and heritage staff at the City of Mississauga regarding the subject property took place as part of the Cultural Heritage Screening Report (ASI 2016). However, given that the Credit River Bridge in Mississauga is identified as retaining municipal heritage recognition, additional consultation with heritage staff was undertaken as part of this CHER.

Paula Wubbenhorst, Senior Heritage Coordinator at the City of Mississauga was consulted on June 9, 2016, via email. Ms. Wubbenhorst confirmed that the Credit River Bridge is listed on the City of Mississauga's Cultural Landscape Inventory (2005), and that there is no additional information about the bridge on file at the municipality.

3 Heritage Recognitions

3.1 Municipal

The subject resource retains heritage recognition at the municipal level for the following reasons:

- The property is listed on the City of Mississauga's Cultural Landscape Inventory (2005), and as such it is listed on the City's Municipal Heritage Register.¹ In the Cultural Landscape Inventory, it is identified as F-SLF-4 and called the CN Bridge over Credit River. It is recognized as an unusual bridge type (See Appendix A for the Inventory Description).

3.2 Provincial

The subject resource does not retain heritage recognition at the provincial level for the following reasons:

- The property is owned by Metrolinx, however, it has not previously been identified as a Provincial Heritage Property; and
- The property has not been commemorated by the Ontario Heritage Trust.

3.3 Federal

The subject resources do not retain heritage recognition at the federal level for the following reasons:

- The property does not contain a Federal Heritage Building; and
- The property is not a National Historic Site.

4 Adjacent Lands

The Credit River Bridge is adjacent² to the following known heritage properties:

- The Credit River Corridor is listed on the City of Mississauga's Cultural Landscape Inventory (2005, L-NA-2);

¹ This property is flagged in the building or development application process. Should an application be made for demolition, it would require a heritage permit and 60 days notice to Council. The 60 days is legislated by the Province of Ontario to allow time for Council to consider heritage designation.

² Adjacent land means those lands contiguous to a protected heritage property.

- The Mineola Neighbourhood, located north of the subject bridge, is listed on the City of Mississauga's Cultural Landscape Inventory (2005, L-RES-6); and
- The Royal Canadian Legion, located south of the subject bridge at 35 Front Street North, is listed on the City of Mississauga's Heritage Register.

As per the City of Mississauga's Official Plan (2015) regarding municipal obligations triggered by development on a property adjacent to a listed property or cultural heritage landscape on the City's Heritage Register, Section 7.4.1.12 reads as follows (City of Mississauga 2015:7-7):

"The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a Heritage Impact Assessment, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction."

5 Summary of Archaeological Assessments

The Stage 1 Archaeological Assessment for the GO Rail Network Electrification TPAP is currently underway (ASI, in progress). Once completed, this report will be on file with Metrolinx and will provide information about archaeological potential in the study area.

6 Community Input

A number of stakeholder groups were contacted and asked to complete a questionnaire to collect any information relating to the Credit River Bridge along the Lakeshore West Corridor. See Appendix B for questionnaire responses received and Table 6-1 for a list of organizations contacted and a description of information received. At the time of writing, responses were received from the Mississauga Heritage Foundation and the Port Credit Village Project. These responses indicate that there is an interest in the cultural heritage value of the bridge, and in the conservation of the bridge.

A review of various online sources did not reveal any interest from the community in the potential heritage value of the Credit River Bridge.

Table 6-1: Results of Community Consultation for Bridge in Mississauga

Contact	Organization	Contact Information	Date(s) of Communications	Description of Information Received
n/a	Port Credit BIA	info@portcredit.com	June 3, 2016	No response received to date
John McKinnon	Credit Reserve Association	jmckinnon@credit-reserve.com	June 3, 2016	No response received to date
Matthew Wilkinson	Mississauga Heritage Foundation	history@heritagemississauga.org	June 3, 2016	Response received June 14 and 24 (email and questionnaire).

				Matthew provided historic photographs of the bridge and a history of railways in Port Credit, and indicated that the bridge has cultural heritage value.
n/a	Mississauga South Historical Society	missysouth@rogers.com	June 3, 2016	No response received to date
Deborah Greenfield and Jim Danahy	Town of Port Credit Association	TOPCA@topca.net	June 3, 2016	No response received to date
Andrew Beattie	Port Credit Village Residents Association	andrew.beattie@rogers.com	June 3, 2016	No response received to date
Dorothy Tomiuk	Viva Port Credit	dtomiuk@sympatico	June 3, 2016	No response received to date
Janis Alton	Port Credit Village Project	janis.alton@sympatico.ca	June 3, 2016	Response received June 3, 2016.
n/a	Credit Valley Conservation Authority	June 3, 2016	June 3, 2016	No response received to date

7 Discussion of Cultural Heritage Value

7.1 Discussion of Historical or Associative Value

7.1.1 Settlement History

Township of Toronto South

In 1788, the County of Peel was part of the extensive district known as the “Nassau District.” After the province of Quebec was divided into Upper and Lower Canada in 1792, the Nassau District became known as Home District. The same year, Upper Canada was subdivided into nineteen counties by its first Lieutenant Governor, Colonel John Graves Simcoe, and by 1852, the Home District was replaced by the Counties of York, Ontario and Peel (Robb 2003; Gibson 2002).

The Township of Toronto was originally surveyed in 1806 by Mr. Wilmot, Deputy Surveyor. The first settler in this Township, and also the County of Peel, was Colonel Thomas Ingersoll. The whole population of the Township in 1808 consisted of seven families scattered along Dundas Street. The number of township inhabitants gradually increased until the War of 1812 broke out, which gave considerable check to its progress. When the war was over, Toronto Township continued to grow and the rear part was surveyed and called the “New Survey.” Along the lakeshore, the pre-existing trail was widened and improved as a public road by 1798, but bridges were lacking. By 1826, a regular stagecoach

service ran between York and Niagara. The Toronto Road Company purchased the Lakeshore Road in 1850, turning it into a toll road (Robb 2003; Gibson 2002).

A review of nineteenth-century historic mapping reveals that the subject rail bridge was located on the northern fringe of the village of Port Credit (Figures 9-1 and 9-2). Lakeshore Road, to the south of the bridge, is depicted as the commercial hub of Port Credit, with densely surveyed residential lots located north and south of the thoroughfare. Major north-south roads include Mississauga Road and Stavebank Road, which are depicted to the west and east of the subject bridge crossing. Larger residential property parcels were developed to the north of the bridge along these roads, while institutional, commercial and public spaces are depicted along the south of the tracks. Much of the land around the bridge was low and occupied by marshland until the late twentieth century.

A review of twentieth-century historic mapping (Figures 9-3 to 9-6) demonstrates that the village of Port Credit was well established and densely settled by the early 1900s. Little changed in the next 50 years, with the exception of an increase in density through housing infill. To the north of the railway tracks, residential properties along Mississauga Road and Stavebank Road had appeared, and the neighbourhood of Mineola to the northeast had been established. By 1975, changes to the former marshland to the south and east of the bridge had begun to take place, which included construction of a municipal library and arena, and flood control through construction of gabion retaining walls along the edge of the Credit River. This was followed by construction of Memorial Park which is comprised of a network of pathways, open green space, and recreational spaces that link to the marina and downtown Port Credit.

The Credit River

The Credit River itself was named “*Mis.sin.ni.he*” or “*Mazinigae-zeebi*” by the Mississaugas. The surveyor Augustus Jones said that this signified “the trusting creek,” although a better translation is “to write or give and make credit.” This is said to refer to the fur trading period, when the French or British would meet with the Aboriginal peoples here “extending credit for supplies until the following spring if the Indians did not have sufficient furs to pay in full.” It is said that the French military engineer, Chaussegros de Lery, suggested that a trading post be established at the Credit in 1749. The French name for this place, when the river was first mapped in 1757, was “*Riviere au Credit*” (Jameson 1923:73-74; Smith 1987:255-257; Rayburn 1997:84; Scott 1997:182; Gibson 2002:177; Robb 2003:6).

Lieutenant Governor Simcoe and his wife, Elizabeth, stopped at the mouth of the Credit River on June 16, 1796. The Simcoes walked along the Credit, and explored the river by canoe about as far upstream as Streetsville. Mrs. Simcoe noted that “the banks were high one side covered with pines & pretty piece of open rocky country on the other.” She also wrote that the river provided a multitude of salmon. Mrs. Simcoe sketched and painted the first known view of the Credit at this time (Robertson 1911:328-331; Gibson 2002:177).

Port Credit

Around 1804, Col. Ingersoll, the first settler, built a trading store. At around the same time, a Government Inn was established on the east bank of the river to accommodate and direct new settlers. Port Credit was officially surveyed and established as a village in 1834. The land on the west side of the Credit River was the first to be surveyed and developed. However, a disastrous fire in 1855 halted its growth. In 1856, a survey of the land on the east side of the river was undertaken, and surveyed lots between the lakefront and the railway were quickly occupied. Port Credit attained status as a police village by 1909, and in 1961, it was incorporated as a town. In 1974, Port Credit amalgamated with the City of Mississauga (Hicks 2007: 3; Gibson 2002:188).

7.1.2 Significant Themes, Events and/or People

Railway Development

The Lakeshore West rail corridor follows the tracks initially laid in the mid 1850s from Toronto to Hamilton by the Great Western Railway (GWR), who were leasing the land from the Hamilton & Toronto Railway Company (H&TRC). The H&TRC was established by Sir Allan MacNab and a number of other investors, with additional financial support from England, and a charter was granted in 1852. Construction on the line began in 1853 and it was completed in 1855. The line was initially leased to the Great Western Railway (GWR), who in turn supplied railway stations along the corridor and constructed the GWR branch between Hamilton and Toronto (Paterson & George 1988:13). Given that the GWR was headquartered in Hamilton, mileage started in Hamilton. Extending from Hamilton, the first train stations were as follows (Reynolds 2011):

- Hamilton, Stuart St. (Mile 0.00);
- Bronte (Mile 13.33);
- Oakville (Mile 17.57);
- Clarkson (Mile 22.82);
- Lorne Park (Mile 23.89)
- Port Credit (Mile 25.84);
- Mimico (Mile 32.26); and
- Sunnyside (Mile 35.18).

In Port Credit, local teamsters were hired by the railway as labourers to build the rail bridge spanning the Credit River, to clear the land, build and level the roadbed, and lay the track through this part of the township. The first train to come through Port Credit Station, which at that time was located on the east side of Stavebank Road, took place on December 3, 1855. The first bridge at the Credit River was a wooden trestle bridge mounted on red brick piers (Clarkson 1967:108; Hicks 2007: 53). It is reported that this trestle dipped lower than the mainline, which caused accidents where rail cars being switched off at Port Credit could roll back down the track towards the bridge (Reynold 2011). Unfortunately, no images of this bridge are known to exist.

The establishment of the railway through Port Credit brought great change to the village. Prior to the 1850s, much of Port Credit's prosperity was reliant on the Credit River as the village served primarily as a shipping port. Mills and farms to the north used the Credit River to access the port at Port Credit, though the arrival of the railroad and the construction of the trestle bridge, ended easy access to the port (Reynolds 2011). The village continued to prosper, however, as it shifted to an emphasis on the railway which linked Port Credit to larger economic centres, brought daily mail, provided more efficient transportation, and attracted people, business and industry to the village.

By the 1870s, there were five trains running daily between Toronto and Hamilton (Hicks 2006). Locomotives were now powered by coal rather than wood and air brakes had been developed which allowed for trains to attain greater speeds. By 1872, iron rails were being replaced by the more resilient steel rails, greatly improving safety standards and reducing expenses. It was also around this time that the H&TR was absorbed into the GWR and the single track between Hamilton and Toronto became known as the Toronto Branch. Other lines constructed by, or purchased by, the GWR included: the Galt & Guelph Railway; the London & Port Sarnia Railway; and the Canada Air Line Railway (Reynolds 2011).

In 1882, the Grand Trunk Railway (GTR) merged with the GWR. Track mileage was reversed at this time, with Union Station in Toronto now at Mile 0.00. In the 1880s and 1890s, a plan was developed by the GTR to fix the 'Dip' at the Credit River, in which the tracks would be raised by 12 feet. At the same time, the Toronto Branch rail corridor was doubled and to accommodate the new track and the raised roadbed, the old wooden trestle spanning the Credit River was replaced by the existing metal bridge in 1903 (Figures 9-7 to 9-10). In about 1900, the location of the Port Credit GTR Station (Mile 12.81) was moved from Stavebank Road easterly, closer to Hurontario Street near the present GO Station (Clarkson 1967; Reynolds 2011; see Figure 9-11).

Due to financial difficulty, control of the GTR was assumed by the Canadian Government in 1919 and by 1923, the GTR was amalgamated with Canadian National Railways (CNR) (Andreae 1997). The CNR continued to operate freight and passenger trains along the Lakeshore West rail corridor on a regular basis, making this one of the busiest rail corridors in Canada. By the 1950s, automobiles and highways were replacing trains and railways as the preferred mode of transportation, which meant that it was becoming economically unviable for the CNR to continue passenger services. The following decades saw the introduction of GO Transit commuter rail service and the creation of VIA Rail Canada by the federal government to ensure the continuity of intercity passenger train services (VIA Rail n.d.).

GO Transit service began in May 1967, and the old train station at Port Credit was demolished to make way for parking for the new GO station.

In the early 2000s, increase rail traffic on the Lakeshore West rail corridor necessitated the addition of a third track. Triple tracking was completed by 2008 and consisted of more than 29 miles (48 kilometres)

of new track, 15 interlockings, and 25 bridges (AECOM n.d.). Work on a new bridge to carry the third track over the Credit River, on the north side of the existing bridge, began in 2007. The concrete substructure was completed in the fall of 2007 and work on the deck truss span began in spring of 2008 (Figure 9-12). The new bridge was lowered into place on August 9th/10th, 2008, between 1:00 am and 8:00 am. According to Reynolds (2011):

“The building of this bridge is a unique undertaking as this is the first time a railway bridge has been built using this method. The main span was constructed on the west side of the river and on the north side of the mainline. It is a Deck Truss Span and measures 20 feet wide and 22 feet high, and 143 feet long. The span weighs 330 tons.

When completed it was moved by equipment rollers and beams onto four heavy equipment flat cars on the mainline. It was then rolled out onto the existing bridge and again using beams and the equipment rollers it was moved between two towers constructed for the installation at each end. Once moved between the towers it was attached to them and lowered into place on the base. Concrete trays were then placed across the top, a total of 24, each weighing 20 thousand pounds. The rails will then be laid across these trays.”

Joseph Hobson and the Grand Trunk Railway

Drawings for the “Double Track Bridge over Credit River, Toronto Branch, Port Credit” were drawn in 1901 by the GTR and signed by Joseph Hobson, Chief Engineer. These included details regarding the repairs necessary to the existing abutments, and demonstrated how the new bridge would raise the base of the rail by 12 feet.

Joseph Hobson began his career as a land surveyor and worked as a provincial land surveyor on the GTR between Toronto and Guelph in the mid 1850s. Hobson became county engineer for the County of Waterloo in 1858. In 1866, he relocated to Guelph and then Hamilton in 1875, working as assistant engineer for the GWR in various capacities. Over the next few decades, he continued up the ranks at the GWR and later the GTR, becoming chief engineer of the GTR west of Toronto in 1882, and finally chief engineer at the entire GTR in 1896. He is attributed to several significant engineering works in Ontario, including the St. Clair Tunnel (1881-1891), the International Bridge at Fort Erie, and the Victoria Bridge in Montreal. He retired as chief engineer in 1906, although continued to consult until his death in Hamilton in 1917 (Irwin 2009).

The Canadian Bridge Company Limited, Walkerville ON

The Canadian Bridge Co. Ltd. of Walkerville, Ontario, fabricated the steel for the deck truss span and the four 30 foot single track deck girder spans forming two double track spans. All steelwork was completed to the specifications of the GTR (dated November 22, 1900).

The Canadian Bridge Co. Ltd. was founded by Francis McMath, a third-generation civil engineer from St. Louis, Missouri. He worked with the Detroit Bridge & Iron Works before establishing the Canadian Bridge Company in 1900 in Walkerville, Ontario. He remained president until 1922, with Willard Pope serving as vice president and chief engineer. In 1923, the company became a subsidiary of the United States Steel Corporation, and in 1937, it was sold to Dominion Steel and Coal Corporation (DOSCO). It operated as a division of DOSCO until 1962, when it was dissolved. Under the direction of McMath and Pope, the Canadian Bridge Company fabricated steel road and rail bridges across Canada including the notable Quebec Bridge in 1917 (a joint venture with the Dominion Bridge Company), the Lethbridge Viaduct, the St. Louis Bridge in Saskatchewan, the Little Current Swing Bridge, and the High Level Bridge in Edmonton (Disher & Smith 2001: 123).

7.2 Discussion of Design and Physical Value

7.2.1 Physical Characteristics

The following description of the Credit River Bridge is based on the available bridge drawings (9-13 to 9-18), site visit (Figures 9-19 to 9-27), inspection reports, and bridge inventory. The following drawings were available for review:

- Plan of Abutments, Credit River Bridge, GTR, GW Division, 1883;
- Plan of Piling for Credit River Bridge Abutments, engineers office, 1883;
- Cross section of Credit River at 25.5 MP, Shewing [showing] position of abutments for iron superstructure, 1884;
- Supplementary Abutments, Credit River Bridge, by Joseph Hobson, Chief Engineer, 1901
- Double Track Bridge over Credit River, Toronto Branch, Port Credit. Joseph Hobson, Chief Engineer, G.T.Ry., Montreal, 1901, 16th District, GTR.
- One (1) 208'-0" double track deck truss span and four (4) 30'-00" single track deck girder spans forming two double track spans, Credit River Bridge, Port Credit, Ont., Grand Trunk Railway, Toronto Branch, 1903, The Canadian Bridge Co. Ltd. of Walkerville Ont. (Series of drawings; note: Specifications for the steel bridge are attributed to the GTR, Nov 22. 1900)
- 1951 – "Credit River Bridge, Precast Reinforced Concrete Slabs for backwalls of east and west abutments" & reinforced concrete slabs for bridge seats... (repairs) CNR, office of bridge engineer, Toronto.
- 1998, Feb 27: View of structure showing the position of the temporary work platform. Credit River Bridge. Oakville Sub.
- Bridge over Credit River, M.13.27 Oakville Subdivision, Mississauga, CN Office of the Chief Engineer. Bridge Widening Design, 2002

Although local history books claim that the subject bridge was built in about 1898 (Hicks 2007; Reynolds 2011) it would appear that the Credit River Bridge was built in 1903 to the designs and specifications of the GTR. The Credit River Bridge was constructed 1903 to carry two tracks of the GTR's Toronto Branch over the Credit River at Port Credit, Ontario. The three-span bridge features a middle deck truss span with steel beam approach spans. The abutments are masonry and according to the drawings, the original bridge abutments for the former wooden bridge were altered to accommodate the new steel bridge.

The middle deck truss is unusual. It was fabricated by the Canadian Bridge Company as a single 208 ft (63 m) double track deck truss span. It is comprised of a ten panel inverted bowstring deck truss with diagonal members forming a Warren truss configuration. The curved lower chord forms a distinctive arch shape. The bridge also features multiple-types of connections; the bridge is riveted, however, the curved lower chord features massive eyebar bundles. To have both a riveted and pin-connected steel truss bridge is unusual and the reasoning behind this particular design is unknown. It was also observed that the steel construction of this truss span is very robust. Unfortunately, a review of the *Canadian Engineer*, and the *Annual Report of the Department of Railways and Canals* did not find any articles about this bridge, and Annual Reports for the Canadian Bridge Company are not available.

As part of the design for the new double track bridge at the Credit River, new masonry piers were constructed to accommodate the new steel deck truss span and the four single track 30 ft (9 m) approach deck girder spans (forming double track spans at each approach).

Modifications

In 1951, the CNR undertook repairs to the bridge abutments and bridge seats. This work involved precast reinforced concrete slabs for backwalls of the east and west abutments. In 2007-8, the bridge was widened with the addition of a three-span bridge to the north side of the existing 1903 bridge, to accommodate a third track. This new steel structure rests on concrete piers and abutments and features a middle deck truss span with two approach deck plate girder spans.

Existing Conditions

According to a 2013 Bridge Inspection Report (Metrolinx 2013), the 1903 structure carrying Track 2 and 3 of Oakville Subdivision of the Lakeshore West rail corridor, is generally in fair condition. The bridge deck is in poor condition, and the report indicates that at that time, the ties were in poor to bad condition overall. The superstructure was in fair condition overall, and the substructure consisting of masonry stone abutments and piers was in fair condition and the abutments showed evidence of minor movement. Of particular note is that the approaches were identified as being 'low', and that the abutment bearings for spans 1 and 3 consisted of welded pedestals and layers of shims which were in poor condition. It recommended that the approaches be lifted and the deck replaced.

7.2.2 Comparative Analysis

The three-span, 1903 Credit River Bridge is comprised of a central deck truss bridge of unusual design with two approach beam spans. The individual span lengths are 30 ft, 210 ft, and 30 ft (9 m, 63, 9 m) for an overall length of 270 ft (82 m). Deck truss railway and road bridges were introduced to the Ontario landscape in the late nineteenth century and become more popular in the early part of the twentieth century, when steel was becoming more affordable and available (Cuming 1984). This type of bridge is typically used to span waterways and/or valleys, where overhead clearance is not necessarily an issue.

According to a review of the Metrolinx Bridge Inventory (2015), there are five other deck truss railway bridges owned by Metrolinx. These are summarized as follows:

- Five-span Rouge River Bridge, Toronto, Lakeshore East Corridor, which included a 140 ft (43 m) double track central deck truss span (b.1898) with four double track approach spans (b.1902), with a total bridge length of 236 ft (72 m);
- Five-span, Sixteen Mile Creek Bridge, Oakville, Lakeshore West Corridor, built 1900-1902, 98 ft (30 m) span length, and 490 ft (149 m) total bridge span length;
- Six-span Bronte Creek, Oakville, Lakeshore West Corridor, built 1900-1902, 93 ft (28 m) span length, and 558 ft (170 m) total bridge length;
- Single-span Mimico Creek Bridge, Toronto, Lakeshore West Corridor, built 1902, measuring 100 ft (31m); and
- Two-span Etobicoke Creek Bridge, Toronto, Lakeshore West Corridor, built 1903, with a 92 ft (28m) span length, and total bridge length of 184 ft (56 m).

According to the bridge inventory maintained by www.historicbridges.org, there are over twenty deck truss road highway bridges in Ontario. A summary of these is provided in Appendix C. It is important to note that this list should not be considered exhaustive, as a complete inventory of bridges in Ontario is not available. In addition, an inventory of rail bridges currently owned/maintained by the CNR or CPR is not available for further comparison.

Based on this review, the subject bridge is not considered to be the oldest example of a deck truss, or significant in terms of individual span length or overall bridge length. However, the inverted bowstring arch design appears to be unique to Ontario, and possibly rare in North America. A significant example of this bridge type is found in the Little Hell Gate spans of the Hell Gate Bridge in New York, which was built in 1916 to the designs of engineer Gustav Lindenthal and built by the American Bridge Company of New York (Holth 2013).

The images included in Section 9.5 of this report illustrate the Rouge River Bridge on the Lakeshore East rail corridor on the Toronto/Pickering border, and Little Hell Gate Bridge in New York. The Rouge River

Bridge is a typical example of a deck truss bridge (which is not arched) for comparison purposes (Figure 9-28), while the Little Hell Gate Bridge (Figure 9-29) is similar to the Credit River Bridge.

7.3 Discussion of Contextual Value

7.3.1 Description of Setting and Character of the Property and Surroundings

The Credit River Bridge is located on the outer fringe of the historic village of Port Credit in Mississauga, Ontario. The Credit River is predominantly used for recreational purposes, and is a significant waterway in the City of Mississauga and an important part of its history and development. Upriver, the Credit River is bounded by residential properties that front Mississauga Road or Stavebank Road. The QEW Credit River Bridge, a concrete open spandrel deck arch bridge built in 1934, is located approximately 1.5 km upriver (northwest) from the subject bridge. The QEW Credit River Bridge is a Provincial Heritage Property.

Downriver, from the Credit River Bridge to where it opens into Lake Ontario, the land use adjacent to the river is a combination of commercial and recreational. On the southwest side of the river is the Royal Canadian Legion (Branch 82), the Mississauga Canoe Club, the Don Rowing Club of Mississauga, and some open green space and parking areas. The Port Credit Memorial Park is located on the northeast side of the river and is comprised of paved pathways, passive and active parkland, a library, an arena, and interpretive plaques providing information on the history of the river and Port Credit. The commercial core of Port Credit spreads out from the river along Lakeshore Road, with a marina and additional residential properties, parkland, and commercial parcels to the south of Lakeshore Road.

Another bridge in the vicinity of the subject bridge is the Lakeshore Road West Bridge over the Credit River. Located approximately 42 metres downstream (southeast) of the subject bridge, the Lakeshore Road West Bridge is a steel beam bridge built in 1960 (Figure 9-30). Previous bridges at this location include the two-lane concrete bowstring arch bridge built in 1919 (Figure 9-31), the 1895 iron through truss bridge built by the Peterborough Bridge Engineering Company, and pre-1895 there were at least two wooden trestle bridges which each were destroyed by storms and flooding (Mississauga Library System n.d.).

The Port Credit GO Station is located approximately 50 m northeast of the Credit River Bridge along the rail corridor. Another bridge, located approximately 15 m southwest of the subject bridge along the rail corridor, was built in 1923 to carry the rail lines over Mississauga Road. This is a deck plate girder bridge with stone abutments which was widened in 2008.

In summary, the character of the general vicinity of the Credit River Bridge is strongly tied to the natural and recreational elements of the Credit River, to the mouth of the Credit River where it meets Lake

Ontario, and to the village of Port Credit. Further, the bridge is well-proportioned and fits easily into the landscape.


7.3.2 Community Landmark

Significant views upriver and downriver from the Credit River Bridge are noted, as well as views to the bridge from Lakeshore Road West and Memorial Park in particular. Given its unusual design, size, prominent location, association with the rail corridor and the Credit River, it is considered to be a community landmark. In particular, given that it dates to the early 1900s, it can be considered to be a cornerstone structure in the Port Credit community given that so much of its surrounding landscape has changed over the past 100 years, yet the bridge remains intact and relatively unaltered. Finally, it is listed on the City of Mississauga's Heritage Register.

FINAL CULTURAL HERITAGE EVALUATION REPORT: CREDIT RIVER BRIDGE



8 Data Sheet

	
Property Name:	Credit River Bridge
Municipal Address:	43.3436742, -79.80512282, Datum WGS84
Municipality:	City of Mississauga
Metrolinx/GO Transit Rail Corridor:	GO Transit Lakeshore West rail corridor
PIN:	13456-0580
Ownership:	Metrolinx
Date of Construction:	1903 (Metrolinx Bridge Inventory; Structural Drawings)
Date of Significant Alterations:	2008 (expanded); Air photo review
Architect/Designer/Builder:	GTR, Canadian Bridge Co. Ltd. (Structural Drawings)
Previous Owner(s)/Occupant(s):	GTR, CNR
Current Function:	Railway Bridge
Previous Function(s)	Railway Bridge
Heritage Recognition/Protection:	Listed on Mississauga's Heritage Inventory
Local Heritage Interest:	Listed on Mississauga's Heritage Inventory
Adjacent Lands:	Port Credit Memorial Arena (By-law 40-2011)

9 Figures

9.1 Historic Map Review

Figure 9-1: View of the study area on 1859 historic mapping (Tremaine 1859)

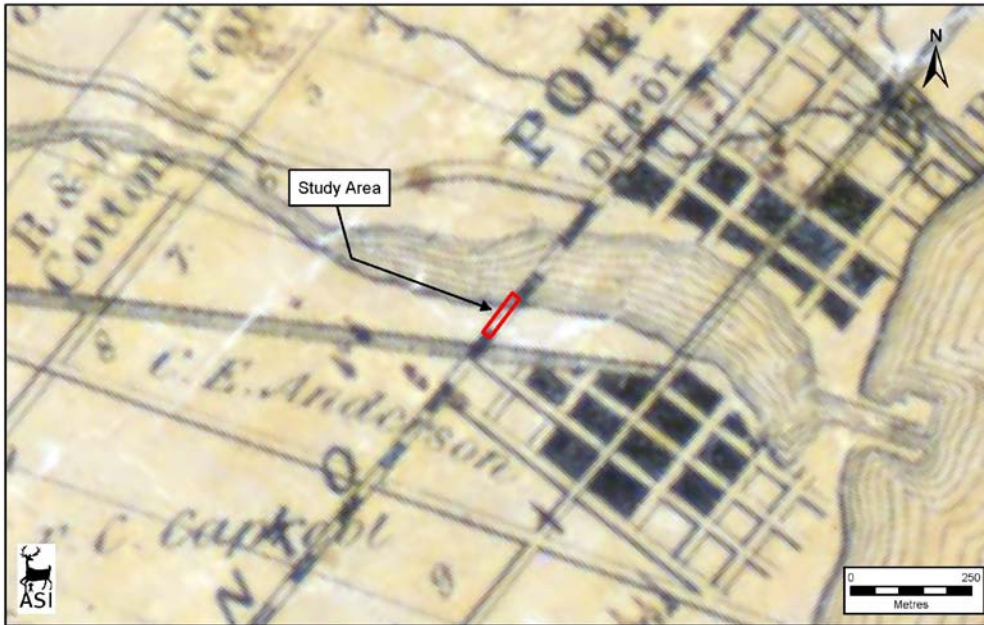


Figure 9-2: View of the study area on 1877 historic mapping (Walker & Miles 1877)

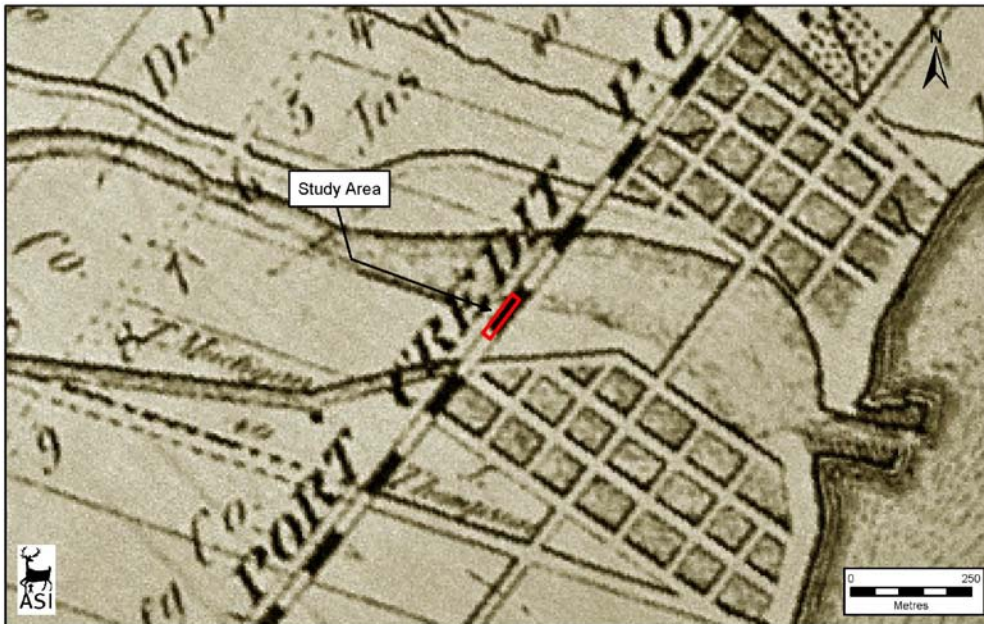


Figure 9-3: View of the study area on 1909 Topographic Map (Ministry of Militia and Defence, 1909)

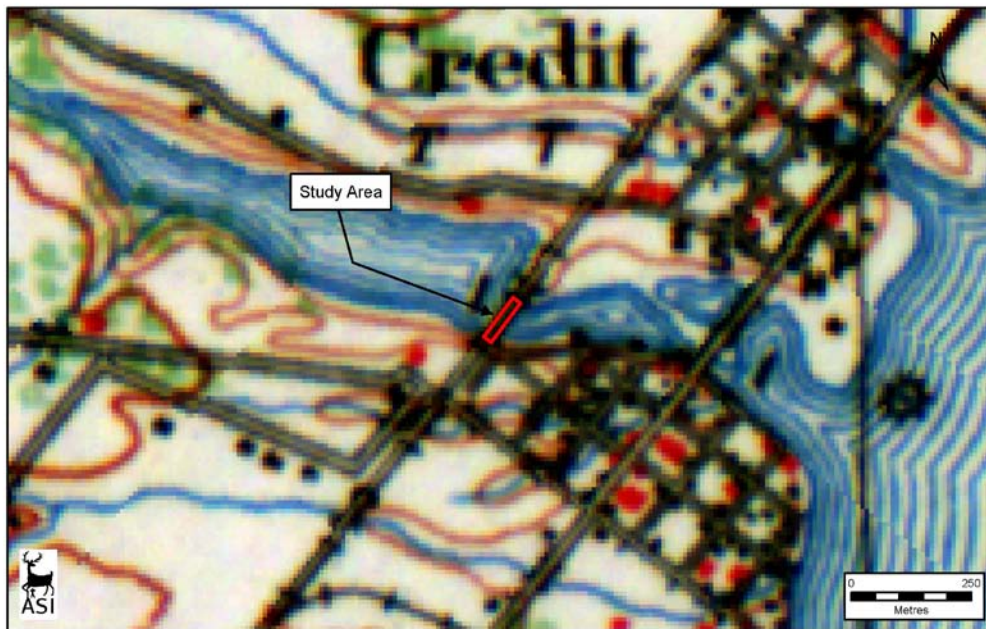


Figure 9-4: View of the study area on 1954 aerial photograph (City of Mississauga, Online Maps)

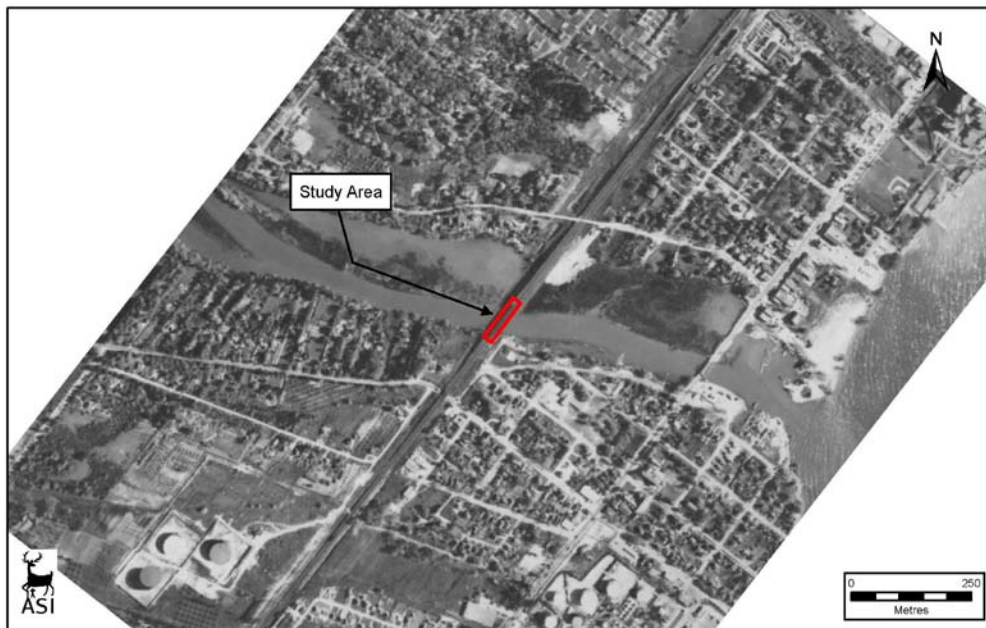


Figure 9-5: View of the study area on 1975 aerial photograph (City of Mississauga, Online Maps)

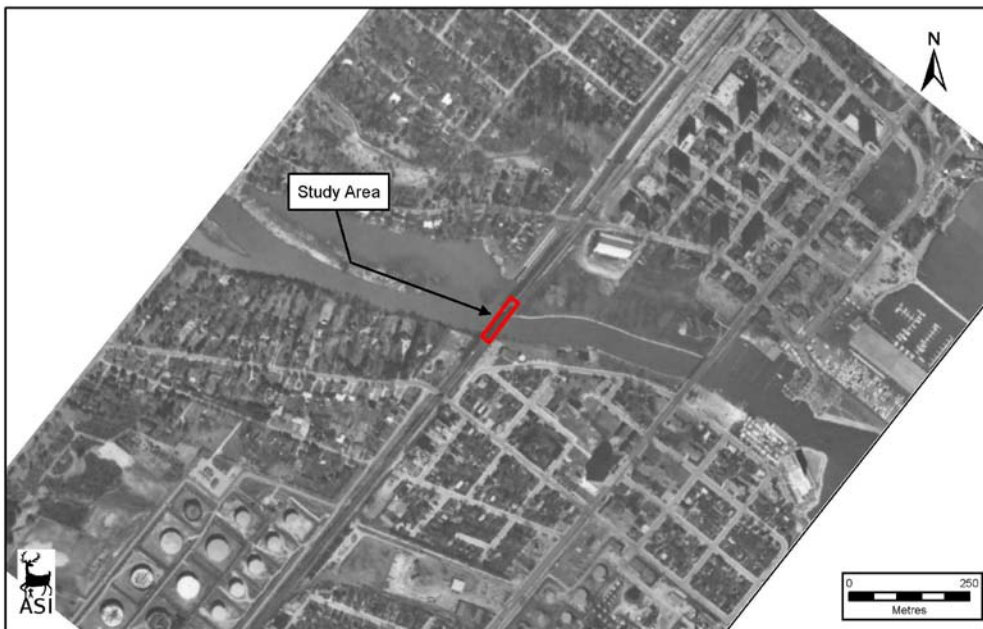


Figure 9-6: View of the study area on 2010 aerial photograph (City of Mississauga, Online Maps)



9.2 Historical Photographs

Figure 9-7: View of the new iron bridge at Port Credit, looking north, date unknown (provided by Mississauga Heritage Foundation)



Figure 9-8: View of the Credit River Bridge, looking south, c.1910 (provided by Mississauga Heritage Foundation)

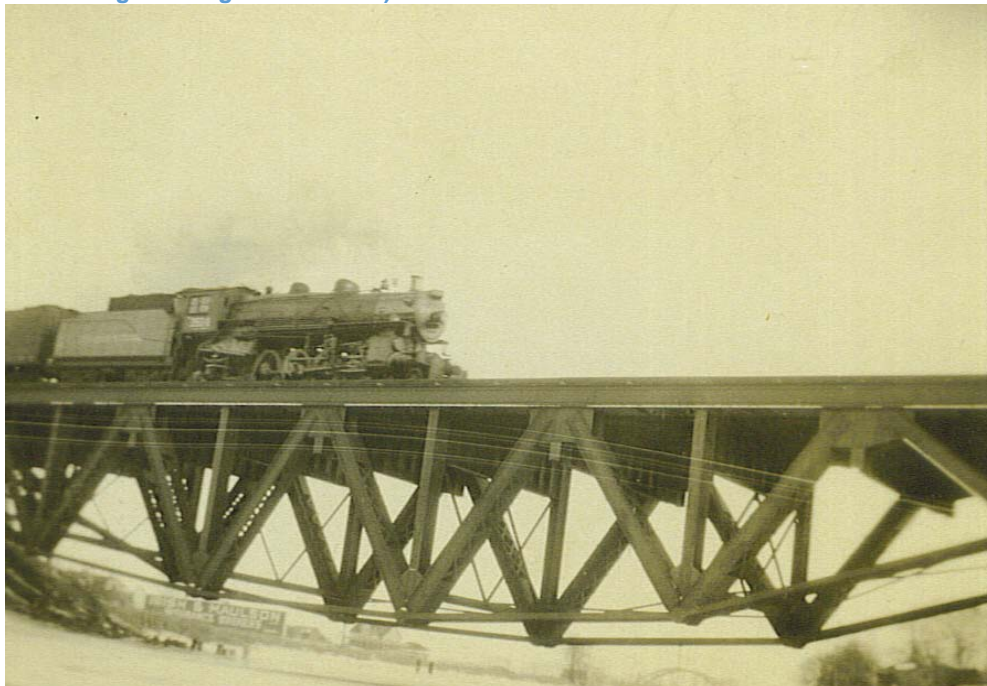


Figure 9-9: Mountain Type Locomotive 6060 crossing the Credit River, looking northeast, c.1950s (Reynolds 2011)



Figure 9-10: Credit River Bridge, looking north, 1978 (Adeney 1978)



Figure 9-11: View of the Port Credit Train Station, c.1950s (Reynolds 2011)



Figure 9-12: View of the new bridge to accommodate a third track, looking east, 2008 (Reynolds 2011)



9.3 Select Structural Drawings

Figure 9-13: Plan of Piling for Credit River Bridge Abutments, 1883 (GTR 1883)

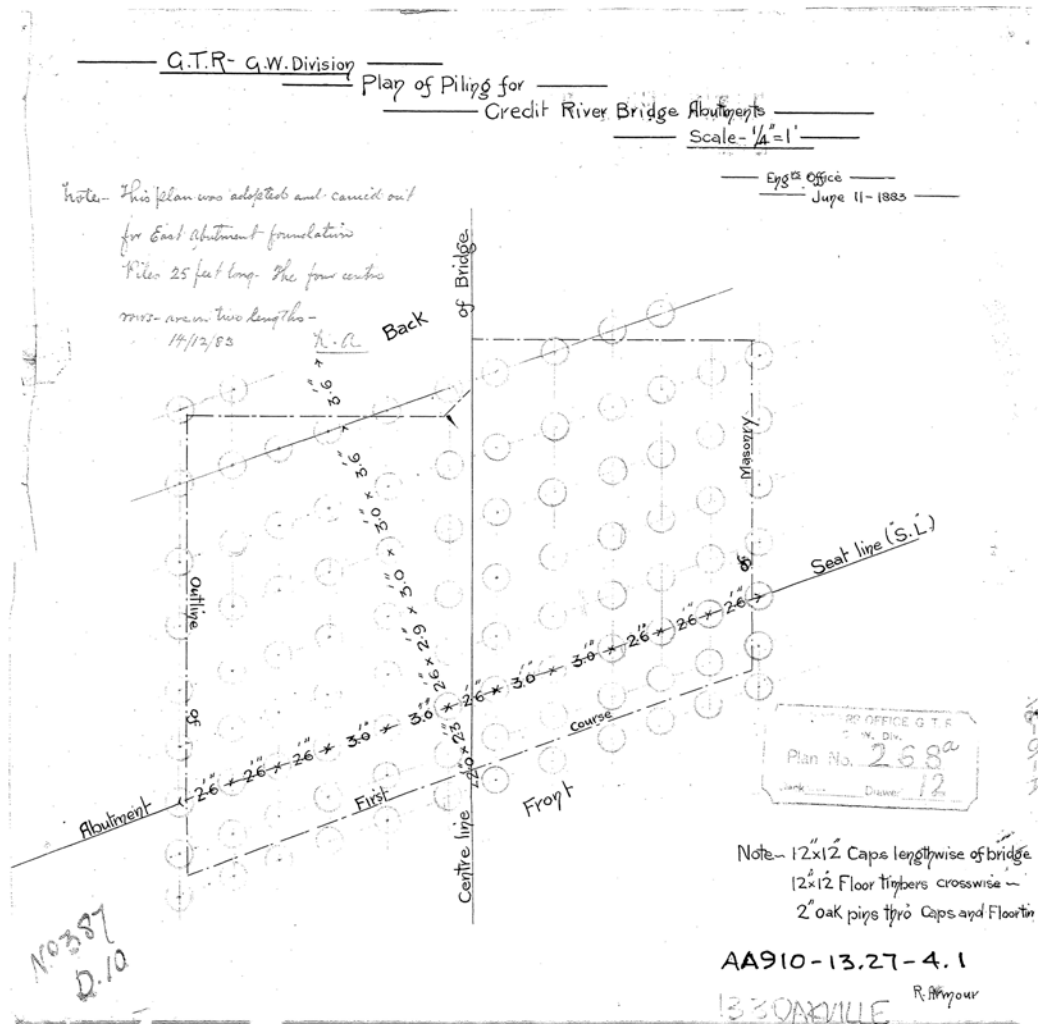


Figure 9-14: Cross Section of Credit River at 25 ½ M.P. showing position of Abutments for Iron Superstructure, 1884 (GTR 1884)

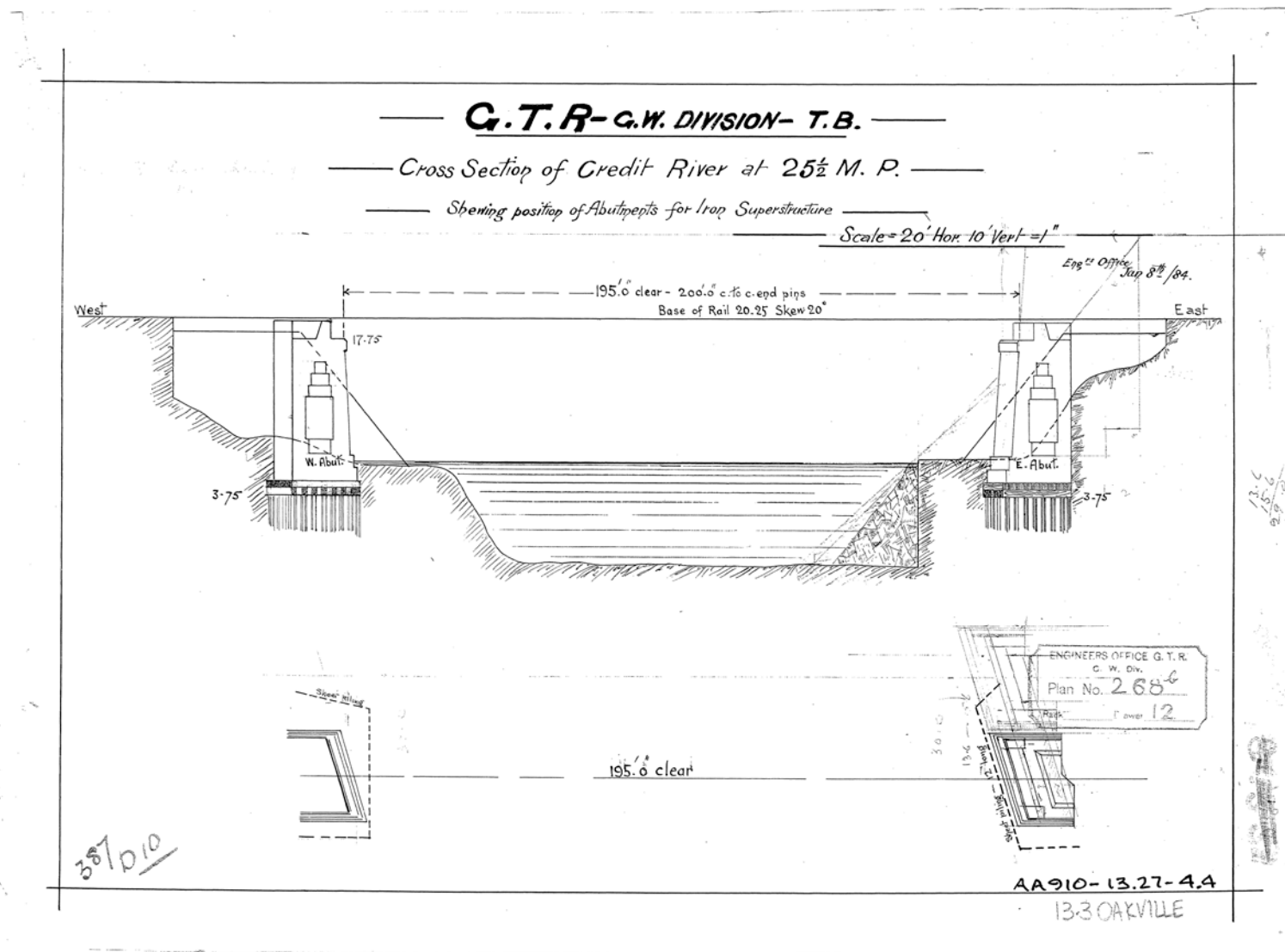


Figure 9-15: Abutment Piers – Credit River Bridge, 1901 (GTR 1901)

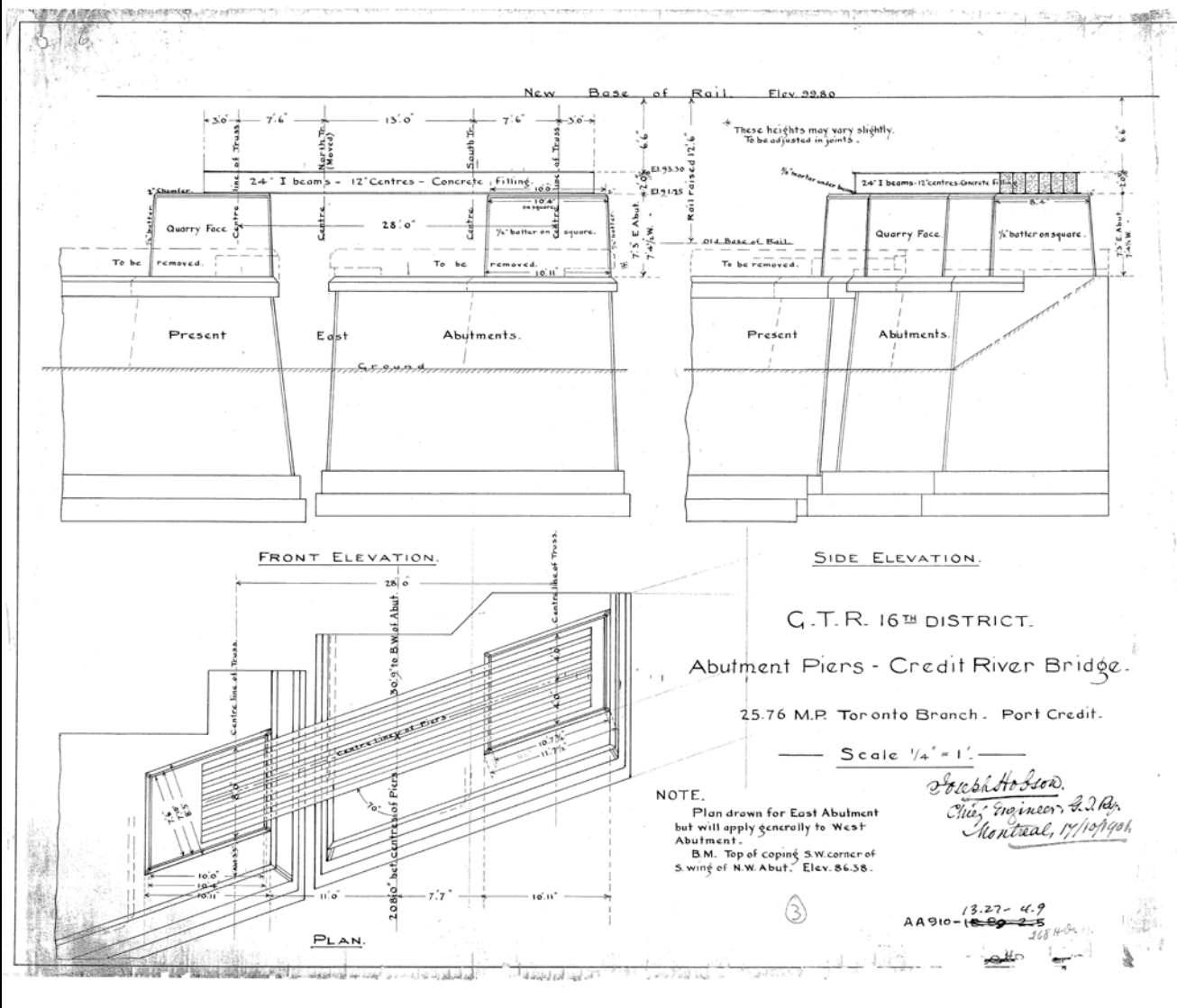


Figure 9-16: Span Diagram for Double Track Bridge over Credit River, 1901 (GTR 1901)

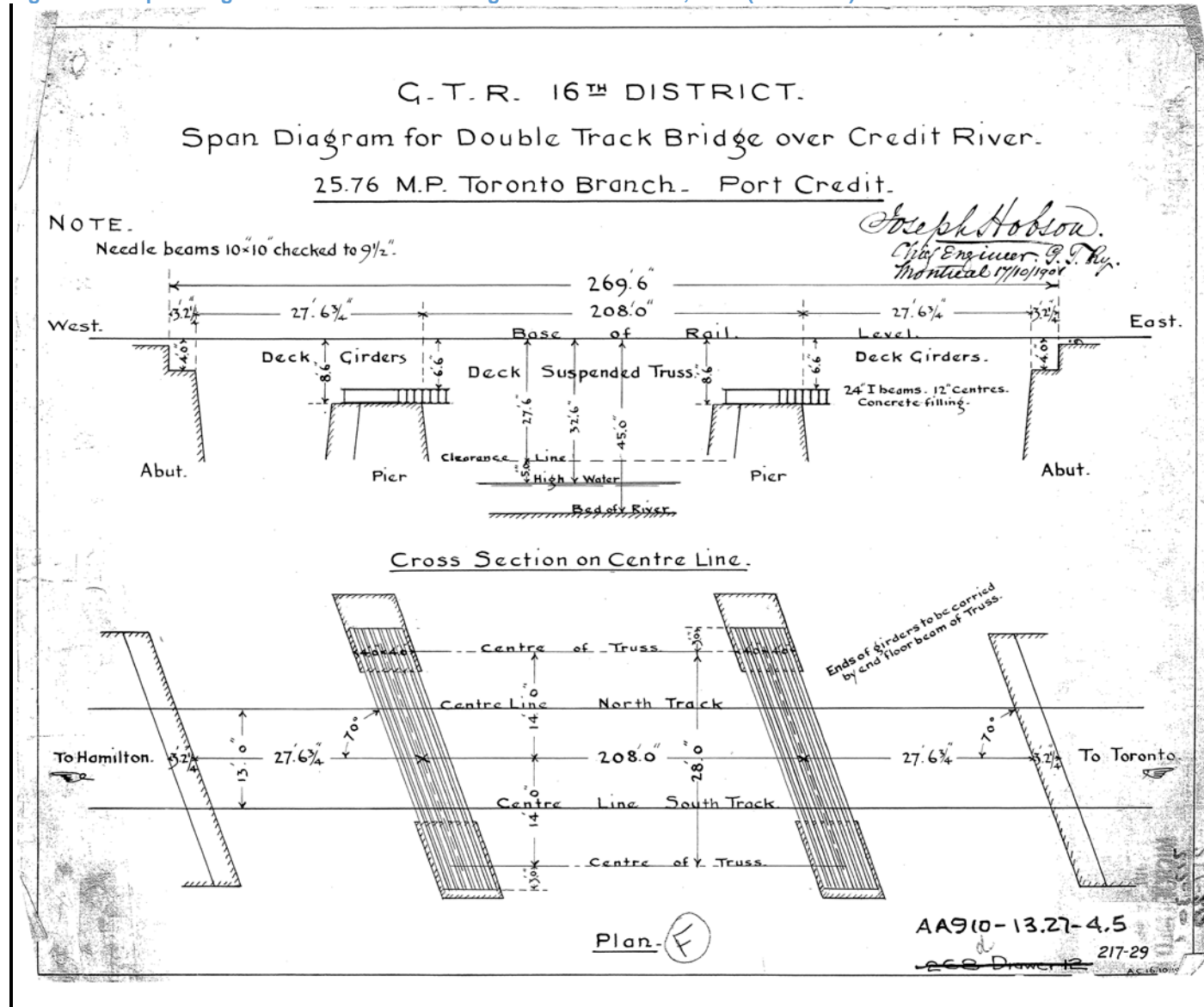


Figure 9-17: Credit River Bridge, 1903 (Canadian Bridge Co. Ltd 1903)

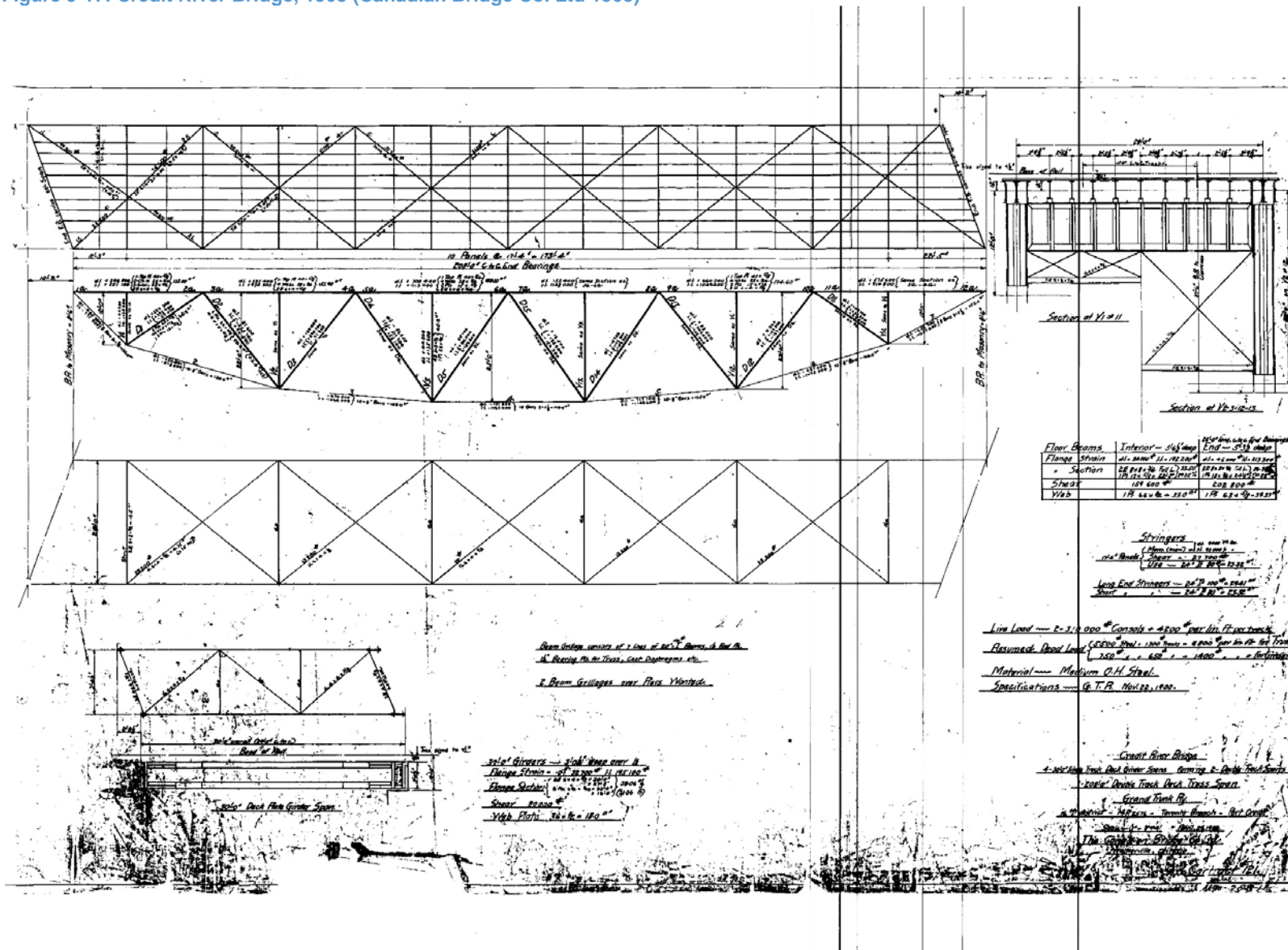
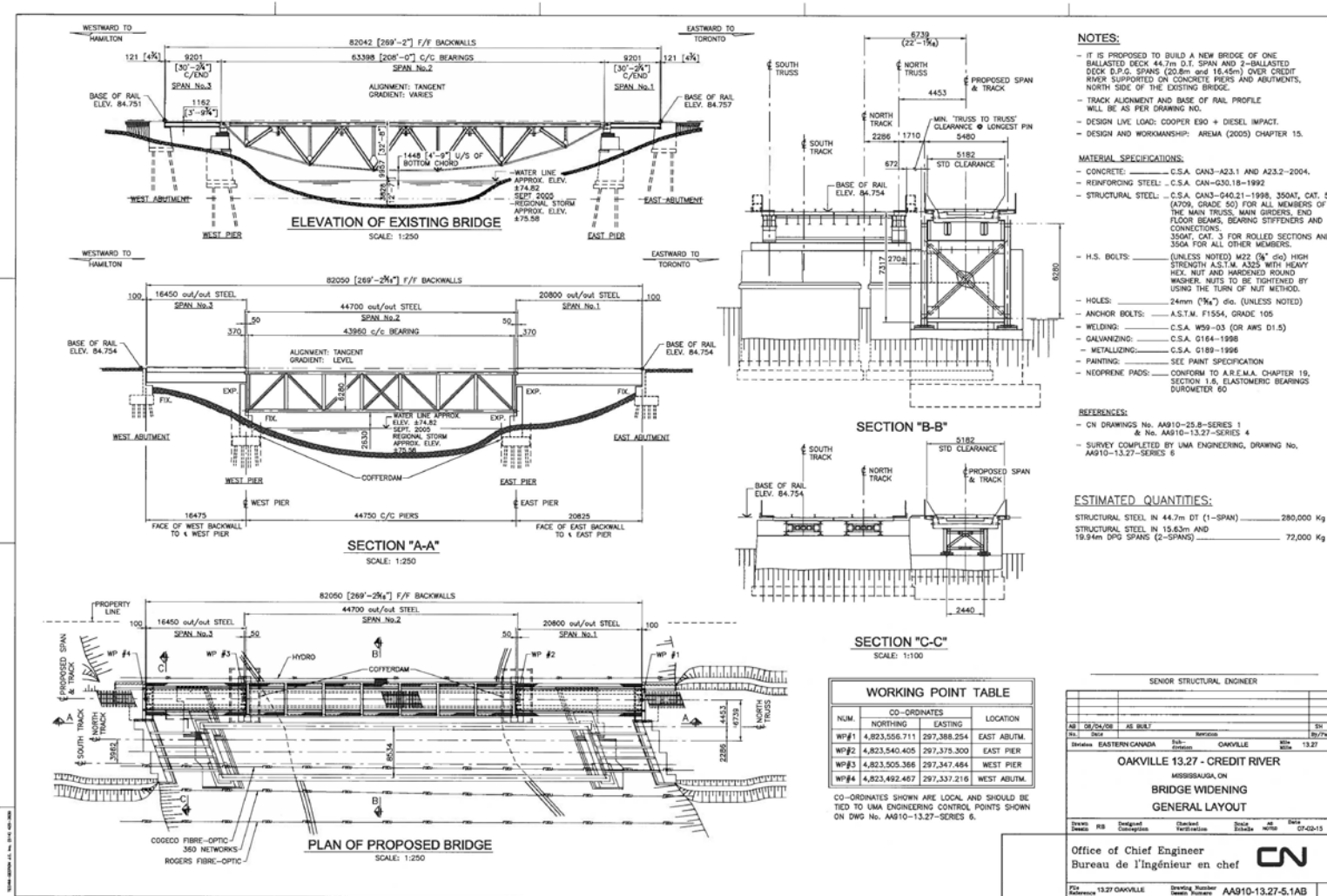


Figure 9-18: Credit River Bridge Widening, General Layout, 2008 (CN 2008)



9.4 Site Visit Photographs

Figure 9-19: View of south elevation, from west bank.



Figure 9-20: View of south elevation, from Lakeshore Road West



Figure 9-21: View of the south elevation, from east bank.



Figure 9-22: Detail of the central section of the deck truss, south elevation.



Figure 9-23: Detail of eastern-part of the deck truss, south elevation.



Figure 9-24: Detail of truss members at west pier, south elevation.



Figure 9-25: Detail view of eyebar bundles.



Figure 9-26: East pier of 2008 bridge addition, south elevation.



Figure 9-27: East pier of 1903 bridge, south elevation.



9.5 For Comparative Contextual Analysis

Figure 9-28: Rouge River Bridge, Lakeshore East rail corridor, City of Toronto, Ontario (ASI 2015)



Figure 9-29: Little Hells Gate Bridge, New York (Holth 2013)



Figure 9-30: View of the c.1960 Lakeshore Road West Bridge, north elevation (ASI 2016)



Figure 9-31: Birds Eye View of the Credit River, 1949, looking northwest (Mississauga Historic Images)



10 Chronology

Date	Event	Reference
1855	Completion of Lakeshore West rail corridor by the Great Western Railway (leased from the Hamilton and Toronto Railway Company (H&TRC)); First bridge built at Credit River on the rail line	Clarkson 1967:108; Hicks 2007: 53
1870s	H&TRC merged with GWR	Hicks 2007; Reynolds 2011
1882	GTR merged with the GWR	Clarkson 1967; Reynolds 2011
1903	Metal inverted bowstring arch bridge built	Mx Bridge Inventory; Bridge drawings
1923	GTR amalgamated with the CNR	Andreae 1997
1967	GO Transit service begins on Lakeshore West	Hicks 2007
2008	Bridge widened to accommodate a third track	AECOM n.d.; Reynolds 2011

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Blake Williams, MLitt
Geomatics Specialist

Report Reviewer:

Joel Konrad, PhD
Cultural Heritage Specialist

APPENDIX A: Cultural Heritage Landscape Inventory Excerpt



Cultural Landscape Inventory

CN Bridge Over Credit River

F-SLF-4

Location Located north of Lakeshore Road on the north side of Port Credit Memorial Park

Heritage or Other Designation None

Landscape Type Special Landscape Feature (Railway Bridge)

LANDSCAPE ENVIRONMENT

- ☒ Scenic and Visual Quality
- ☐ Natural Environment
- ☐ Horticultural Interest
- ☒ Landscape Design, Type and Technological Interest

HISTORICAL ASSOCIATION

- ☒ Illustrates Style, Trend or Pattern
- ☐ Direct Association with Important Person or Event
- ☐ Illustrates Important Phase in Mississauga's Social or Physical Development
- ☐ Illustrates Work of Important Designer

BUILT ENVIRONMENT

- ☒ Aesthetic/Visual Quality
- ☒ Consistent Early Environs (pre-World War II)
- ☒ Consistent Scale of Built Features
- ☒ Unique Architectural Features/Buildings
- ☐ Designated Structures

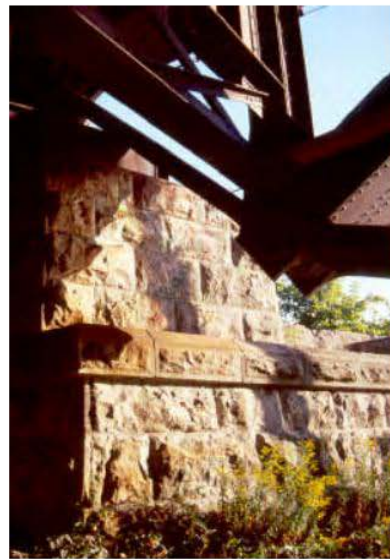
OTHER

- ☒ Historical or Archaeological Interest
- ☒ Outstanding Features/Interest
- ☐ Significant Ecological Interest
- ☒ Landmark Value



**CN Bridge Over Credit River****Cultural Landscape Inventory****F-SLF-4****SITE DESCRIPTION**

This unusual bridge is located just north of Port Credit Harbour on the Credit River. It is supported by two abutments constructed of massive cut and rock-faced stone. The main deck is supported by a reverse suspension structure made up of a series of steel links that support vertical columns attached to the deck. The lowest point of the supporting arch is approximately two to three metres (6-10') above the surface of the Credit River. This is identified as a significant cultural feature because there is no other bridge like it in the City of Mississauga and maybe no other like it in the country.



APPENDIX B: Completed Questionnaires

Cultural Heritage Evaluation Report Questionnaire

Subject *

Credit River Railway Bridge

Response By *

Matthew Wilkinson, Historian

Name of Organization *

Heritage Mississauga

Date *

MM DD YYYY

06 / 24 / 2016

1. Have you collected any historical information on the property? *

☒ Yes

☐ No

If yes, please provide a short description of this collection:

Some historic images, basic date information

2. Is there any local interest in the history of the property relating to (please check all that apply): *

☒ Historical or Associative Value

☒ Design or Physical Value

☒ Contextual Value

☐ Other: _____

If yes, please provide additional information regarding your selection(s) above:

The unique design of the bridge has been a community landmark for years, particularly visible from the south (looking north) from Port Credit Memorial Park. The new twinned bridge structure on the north side of the historic railway bridge obscures the view of the north side of the older bridge.

3. Do you know whether the lands where the property is located may be valued by a community, including First Nations? *

☒ Yes

☐ No

If yes, please provide a short description:

The bridge is built within what were once the Credit Indian Reserve lands, and the Credit River Valley was traversed by Native peoples for thousands of years prior to the treaty period (1805-1820) and the Port Credit survey (1835) and the sale of the Indian Reserve lands (1847). There have been aboriginal archaeological finds in close proximity in the past.

4. Please provide any additional comments you think are relevant.

The bridge is also in close proximity to the Mineola West Cultural Landscape, and of course is identified within the Credit River Corridor Cultural Landscape.

Cultural Heritage Evaluation Report Questionnaire

Subject *

Credit River Railway Bridge

Response By *

[Janis Alton

Name of Organization *

Port Credit Village Project

Date *

MM DD YYYY

06 / 04 / 2016

1. Have you collected any historical information on the property? *

☐ Yes

☒ No

If yes, please provide a short description of this collection:

2. Is there any local interest in the history of the property relating to (please check all that apply): *

- ☒ Historical or Associative Value
- ☐ Design or Physical Value
- ☐ Contextual Value
- ☐ Other: _____

If yes, please provide additional information regarding your selection(s) above:

This line is very old and represents the northern boundary of Port Credit. The bridge spans the Credit River itself a place of historical relevance to the peoples who have long used this waterway - the Mississaugas and later the settlers. The bridge links this community to our neighbouring and far-flung towns and cities.

3. Do you know whether the lands where the property is located may be valued by a community, including First Nations? *

- ☒ Yes
- ☐ No

If yes, please provide a short description:

Local citizens for a number of years now have requested a pedestrian link here to facilitate the link to the GO Station. Environmentally, this is to be applauded.

In reverse, the link from the Port Credit Go Station to Credit River/riverside- events (and there are many, and increasing) is a convenience.

4. Please provide any additional comments you think are relevant.

APPENDIX C: Comparative Analysis

The Information in this table is compiled from www.historicbridges.org	
16 Mile Creek Railway Bridge	<p>Railroad (Canadian National and Go Transit) Over 16 Mile Creek and Cross Avenue</p> <p>Oakville: Halton Region, Ontario Metal 8 Panel Rivet-Connected Warren Deck Truss, Fixed Length: 490 ft Main Spans: 5 Built By: Unknown This deck truss bridge noted for its very shallow trusses was more recently widened on one side with girders.</p>
Bayfield Bridge	<p>KH-21 (Bluewater Highway) Over Bayfield River</p> <p>Bayfield: Huron County, Ontario Metal Rivet-Connected Warren Deck Truss, Fixed Main Spans: 2 Built 1949 By: Unknown An attractive two-span deck truss with v-lacing hidden under an ugly deck.</p>
Bayview Avenue Bridge	<p>Bayview Avenue Over West Branch Don River</p> <p>Toronto: Toronto City, Ontario Metal 10 Panel Bolt-Connected Warren Deck Truss, Fixed and Approach Spans: Metal Deck Girder, Fixed Length: 900 ft Main Span: 155 ft Built 1929 By: Unknown and Margison and Babcock This multi-span deck truss bridge was widened with a very similar looking parallel set of trusses in 1960.</p>
Bracebridge Railway Bridge	<p>Railroad (Canadian National) Over North Branch Muskoka River</p> <p>Bracebridge: Muskoka District, Ontario Metal 6 Panel Pin-Connected Pratt Deck Truss, Fixed and Approach Spans: Metal Deck Girder, Fixed Main Spans: 1 Built By: Unknown This is a rare example of a pin-connected deck truss in Ontario.</p>
Burgoyne Bridge (Demolished)	<p>St. Paul Street Over 12 Mile Creek</p> <p>St. Catharines: Niagara Region, Ontario Metal 6 Panel Rivet-Connected Warren Deck Truss, Fixed and Approach Spans: Metal Deck Girder, Fixed Length: 1236 ft</p>

FINAL CULTURAL HERITAGE EVALUATION REPORT: CREDIT RIVER BRIDGE



	<p>Main Span: 120 ft Roadway: 30 ft Main Spans: 7 Built 1915 By: Dominion Steel and Coal Corporation and Sprague and Reppert of Pittsburgh, Pennsylvania</p>
Cayuga Railway Bridge	<p>Railroad (Abandoned Canadian National) Over Grand River</p> <p>Cayuga: Haldimand County, Ontario Metal 5 Panel Rivet-Connected Pratt Deck Truss, Fixed and Approach Spans: Metal Deck Girder, Fixed Main Spans: 5 Built By: Unknown An impressive multi-span deck truss with a pleasing trapezoid truss shape and stone piers.</p>
Dorchester Bridge	<p>Bridge Street Over South Branch Thames River</p> <p>Dorchester: Middlesex County, Ontario Metal 6 Panel Rivet-Connected Warren Deck Truss, Fixed Main Spans: 1 Built 1923 By: Unknown This bridge is an uncommon example of a highway deck truss in Ontario.</p>
Forks of the Credit Railway Bridge	<p>Railroad (Canadian Pacific) Over Erin Branch Credit River and Forks of the Credit Road</p> <p>Caledon (Near Belfountain): Peel Region, Ontario Metal 12 Panel Rivet-Connected Warren Deck Truss, Fixed and Approach Spans: Metal Deck Girder, Fixed Main Spans: 1 Built By: Unknown This bridge is located in a scenic area where railway and river wind through the Niagara Escarpment. Homer Lift Bridge</p>
Welland Canal Bridge #4 / Queenston Street Bridge	<p>Queenston Street Over Welland Canal</p> <p>St. Catharines: Niagara Region, Ontario Metal Rivet-Connected Pratt Deck Truss, Movable: Double Leaf Bascule (Rolling Lift) and Approach Spans: Metal Deck Girder, Fixed Main Spans: 1 Built By: Unknown A rare Ontario example of a deck truss bascule bridge, this is a rolling lift.</p>
Leaside Bridge / Confederation Bridge	<p>Millwood Road Over Don River, Don Valley Parkway, and Railroad</p> <p>Toronto: Toronto City, Ontario Metal 8 Panel Rivet-Connected Warren Deck Truss, Fixed Length: 1443 ft</p>

FINAL CULTURAL HERITAGE EVALUATION REPORT: CREDIT RIVER BRIDGE



	<p>Main Span: 125 ft Main Spans: 11 Built 1927 By: Unknown and Frank Barber of Toronto, Ontario This tall and long bridge was widened significantly in the late 1960s.</p>
London Thames River Railway Bridge	<p>Railroad (Canadian National) Over Thames River</p> <p>London: Middlesex County, Ontario Metal 8 Panel Rivet-Connected Warren Deck Truss, Fixed and Approach Spans: Metal Stringer (Multi-Beam), Fixed Main Spans: 2 Built By: Unknown Setting new standards for massive, this bridge has gargantuan members and connections.</p>
Constable Rick Hopkins Memorial Bridge	<p>KH-6 (Main Street) Over South Saugeen River</p> <p>Wellington North (Mt. Forest): Wellington County, Ontario Metal 10 Panel Bolt-Connected Pratt Deck Truss, Fixed Main Spans: 1 Built 1961 By: Unknown This is a rare example of a highway deck truss in this region of Ontario.</p>
Mt. Pleasant Road Bridge	<p>Mt. Pleasant Road Over Rosedale Ravine, Rosedale Valley Road, Castle Frank Brook</p> <p>Toronto: Toronto City, Ontario Metal 8 Panel Rivet-Connected Warren Deck Truss, Fixed Length: 440 ft Main Span: 100 ft Main Spans: 3 Built 1948 By: Unknown This is a later but uncommon example of a riveted deck truss bridge with good integrity.</p>
KH-11 Northbound Bridge	<p>KH-11 Northbound Over South Branch Muskoka River</p> <p>Bracebridge: Muskoka District, Ontario Metal 12 Panel Rivet-Connected Warren Deck Truss, Fixed and Approach Spans: Metal Stringer (Multi-Beam), Fixed Main Spans: 2 Built 1952 By: Unknown This deck truss crosses over an impressive gorge and waterfall.</p>
Paisley Railway Bridge	<p>Railroad (Rail-Trail) Over Teeswater River</p> <p>Paisley: Bruce County, Ontario Metal 10 Panel Rivet-Connected Warren Deck Truss, Fixed and Approach Spans: Metal Deck Girder, Fixed</p>

FINAL CULTURAL HERITAGE EVALUATION REPORT: CREDIT RIVER BRIDGE



	<p>Length: 607 ft Main Spans: 1 Built 1926 By: Unknown Although a nice looking high level deck truss, also noteworthy are remains of Phoenix columns under the bridge.</p>
Paris Railway Bridge	<p>Railroad (Canadian National) Over Grand River</p> <p>Paris: Brant County, Ontario Metal 8 Panel Rivet-Connected Warren Deck Truss, Fixed Main Spans: 5 Built By: Unknown This is a large high level deck truss that remains in heavy use by trains.</p>
Penetangore River Bridge / Huron Terrace Bridge	<p>Huron Terrace Over Penetangore River</p> <p>Kincardine: Bruce County, Ontario Metal 8 Panel Rivet-Connected Warren Deck Truss, Fixed and Approach Spans: Metal Deck Girder, Fixed Main Spans: 1</p> <p>Built 1934 By: Hamilton Bridge Company of Hamilton, Ontario</p> <p>This impressive deck truss retains excellent historic integrity and is a significant part of Canadian heritage and innovation.</p>
Port Credit Railway Bridge	<p>Railroad (Go, Canadian National) Over Credit River</p> <p>Mississauga: Peel Region, Ontario Metal 10 Panel Multiple-Type-Connected Inverted Bowstring Deck Truss, Fixed and Approach Spans: Metal Stringer (Multi-Beam), Fixed Length: 270 ft Main Spans: 1 Built By: Unknown This bridge's extremely unusual design gives it a very high level of heritage significance.</p>
Southampton Bridge	<p>KH-21 (Albert Street) Over Saugeen River</p> <p>Southampton (Saugeen Shores): Bruce County, Ontario Metal Continuous 14 Panel Bolt-Connected Warren Deck Truss, Fixed Main Spans: 3 Built 1959 By: Unknown and Reginald Arthur Blyth A rare, and relatively late example of continuous deck bridge construction.</p>
St. Thomas Canadian National Railway Bridge	<p>Railway (Canadian National) Over Kettle Creek</p> <p>St. Thomas: Elgin County, Ontario Metal 6 Panel Rivet-Connected Pratt Deck Truss, Fixed and Approach Spans: Metal Deck Girder, Fixed</p>

FINAL CULTURAL HERITAGE EVALUATION REPORT: CREDIT RIVER BRIDGE



	<p>Main Spans: 1 Built By: Unknown One of two large high level rail bridges in St. Thomas, this bridge appears to be built of imported German steel.</p>
Tansley Bridge / Dundas Street Bridge	<p>Dundas Street (RR-5) Over Bronte Creek</p> <p>Burlington: Halton Region, Ontario Metal Continuous 16 Panel Rivet-Connected Warren Deck Truss, Fixed Length: 700 ft Main Spans: 4 Built 1948 By: Unknown Although it has been widened on one side with ugly beams, the original part of this bridge is a beautiful example of a deck truss bridge.</p>
Thomas B. McQuesten High Level Bridge	<p>York Boulevard Over Desjardins Canal</p> <p>Hamilton: Hamilton City, Ontario Metal Cantilever Rivet-Connected Deck Truss, Fixed Roadway: 54 ft Main Spans: 3 Built 1932 By: Hamilton Bridge Company of Hamilton, Ontario and James, Proctor, and Redfern of Toronto, Ontario This highly decorated landmark bridge is very beautiful and also has a high level of heritage and technological significance.</p>
Wingham Railway Bridge	<p>Railroad (Wingham Community Rail-Trail, Former Canadian National) Over Maitland River</p> <p>Wingham (North Huron): Huron County, Ontario Metal 10 Panel Pin-Connected Warren Deck Truss, Fixed and Approach Spans: Metal 8 Panel Rivet-Connected Warren Deck Truss, Fixed Main Spans: 1 Built 1915 By: Unknown A variety of spans were relocated here in 1915 to construct this unique bridge, including a rare pin-connected Warren truss.</p>

Final Heritage Impact Assessment:

Credit River Bridge

For



Prepared by:



Reviewed by:



MORRISON HERSHFIELD



Gannett Fleming

Gannett Fleming Project No. 060277

Metrolinx Electrification Project

Contract No. QBS-2014-IEP-002

Submittal Date: September 2017

ISO 9001:2008
CERTIFIED

METROLINX GO RAIL ELECTRIFICATION

Quality Assurance Document Release Form

Name of Firm: Archaeological Services Inc. (ASI)

Document Name: Final Heritage Impact Assessment: Credit River Bridge V 3 _____

Submittal Date: September 18, 2017

Discipline: Environmental Assessment

Prepared By: Joel Konrad Date: September 13, 2017

Reviewed By: Rebecca Sciarra Date: September 15, 2017

Approved By: Rebecca Sciarra Date: September 18, 2017
Project Manager

The above electronic signatures indicate that the named document is controlled by GF Canada ULC, and has been:

1. Prepared by qualified staff in accordance with generally accepted professional practice.
2. Checked for completeness and accuracy by the appointed discipline reviewers and that the discipline reviewers did not perform the original work.
3. Reviewed and resolved compatibility interfaces and potential conflicts among the involved disciplines.
4. Updated to address previously agreed-to reviewer comments, including any remaining comments from previous internal or external reviews.
5. Reviewed for conformance to scope and other statutory and regulatory requirements.
6. Determined suitable for submittal by the Project Manager.

REVISION HISTORY

Revision	Date	Comments
0	April 3, 2017	Initial Release to Metrolinx
1	June 1, 2017	Revised report reflecting new information, corrected information, client review comments, etc.
2	August 3, 2017	Revised reflecting new information and finalization
3	September 18, 2017	Final Report to Metrolinx, addressing comments from Ministry of Tourism, Culture and Sport.

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Executive Summary

ASI was contracted by Morrison Hershfield on behalf of Metrolinx to conduct a Heritage Impact Assessment (HIA) for the Credit River Bridge along the Lakeshore West rail corridor, as part of the GO Rail Network Electrification Transit Project Assessment Process (TPAP). Metrolinx is undertaking a Transit Project Assessment study under Ontario Regulation 231/08 - *Transit Projects and Metrolinx Undertakings* for electrification of the GO Rail Network. The purpose of the Project is to convert the GO Rail Network from diesel to electric propulsion. The Credit River Bridge has been identified as a Metrolinx Provincial Heritage Property of Provincial Significance as it satisfied Ontario Regulation 10/06 of the Ontario Heritage Act. This assessment was made through a Cultural Heritage Evaluation Report (ASI, July 2016) and confirmed by the Metrolinx Heritage Committee (October 13, 2016). The bridge is listed on the City of Mississauga Heritage Register and also identified in the City's Cultural Landscape Inventory (2005).

This research was conducted under the project direction of Rebecca Sciarra, Partner and Director of the Business Services Division, ASI. The present HIA follows the Ministry of Tourism, Culture and Sports' (MTCS) *Ontario Heritage Toolkit* (2006), the *Standards and Guidelines for the Conservation of Provincial Heritage Properties* (2010), and Info Bulletin #3: Heritage Impact Assessments for Provincial Heritage Properties (Draft – July 26, 2016). The Credit River Bridge will be impacted through the attachment of electrification infrastructure.

Introduction of the Overhead Contact System (OCS) Attachments will result in alteration to the bridge and requires application of mitigation measures. However, these interventions are not expected to result in significantly adverse impacts on the bridge's identified heritage attributes. OCS attachments will be installed to the substructure or superstructure of the bridge. According to existing material, the intervention is reversible. However, the OCS Attachments do have the potential to alter the legibility of Credit River Bridge as a unique example of an inverted bowstring arch deck truss bridge constructed of steel and stone masonry. In addition, the placement of the OCS Attachments on the superstructure could result in the introduction of bracing/plates/structural supports. As such, the following mitigation measures should be undertaken:

1. The OCS Attachments should be installed to be compatible with the bridge's type and massing and to minimize material interventions. Mitigations may include sitting OCS attachments at the edges of the bridge and using materials and finishes that would make the new infrastructure physically and visually compatible with, but subordinate to and distinguishable from the bridge.
2. The number of connections used to attach electrification infrastructure to the Credit River Bridge should be minimized. It is anticipated that installation of OCS Attachments at abutments/piers would be bolted to the substructure whereas installation at the superstructure level may require introduction of additional bracing, plates, and or structural elements.

3. All interventions should be designed to be reversible. Where interventions are undertaken that will result in alterations to material and fabric, documentation should be undertaken in advance of installation activities. The purpose of documentation is to record existing conditions of the bridge at a level of detail for the purposes of implementing a program to reverse impact should OCS attachments be removed in future due to changes in technology or operational priorities.
4. Given that the subject bridge has been identified in the City of Mississauga's Cultural Landscape Inventory (2005) and listed on the City's Heritage Register, the subject report has been circulated to the City of Mississauga as of September 2017 for review and comment. Any updates to this HIA undertaken during detailed design will be undertaken in consultation with the MTCS and heritage staff at the City of Mississauga. As the Credit River Bridge is a Provincial Heritage Property of Provincial Heritage Significance, any request for demolition or transfer from provincial control will require Ministerial consent from the Ministry of Tourism, Culture and Sport (MTCS). It is noted that Federal heritage approvals are not anticipated to be required in relation to the proposed electrification modifications to the Credit River Bridge.
5. Detail Design and implementation of interventions at the Credit River Bridge should be guided by a qualified heritage professional who is a member of the Canadian Association of Heritage Professionals and who has demonstrated experience developing impact assessments and conservation plans for culturally significant road and rail bridges.

1 Introduction

ASI was contracted by Morrison Hershfield on behalf of Metrolinx to conduct a Heritage Impact Assessment (HIA) for the Credit River Bridge along the Lakeshore West rail corridor, as part of the GO Rail Network Electrification Transit Project Assessment Process (TPAP). Metrolinx is undertaking a Transit Project Assessment study under Ontario Regulation 231/08 - *Transit Projects and Metrolinx Undertakings* for electrification of the GO Rail Network. The purpose of the Project is to convert the GO Rail Network from diesel to electric propulsion. The Credit River Bridge has been identified as a Metrolinx Provincial Heritage Property of Provincial Significance as it satisfied Ontario Regulation 10/06 of the Ontario Heritage Act. This assessment was made through a Cultural Heritage Evaluation Report and confirmed by the Metrolinx Heritage Committee. As of September 2017, a Strategic Conservation Plan for the bridge had not been completed. The bridge is listed on the City of Mississauga Heritage Register and also identified in the City's Cultural Landscape Inventory (2005). It is anticipated that the Credit River Bridge will be impacted through the attachment of electrification infrastructure.

This research was conducted under the project direction of Rebecca Sciarra, Partner and Director of the Business Services Division, ASI. The present HIA follows the Ministry of Tourism, Culture and Sports' (MTCS) *Ontario Heritage Toolkit* (2006), the *Standards and Guidelines for the Conservation of Provincial Heritage Properties* (2010), and Info Bulletin #3: Heritage Impact Assessments for Provincial Heritage Properties (Draft – July 26, 2016).

1.1 Description of Property

The Credit River Bridge is located at Mile 13.27 of the GO Transit Lakeshore West rail corridor, and is located in the City of Mississauga (Figure 1-1). The bridge, built in 1903 and twinned in 2008, carries three tracks of rail traffic in an east and west direction across the Credit River, between Stavebank Road and Mississauga Road. While rail traffic travels in an east-west direction, it should be noted that at this segment of the rail corridor, the bridge and corridor is on a northeast-southwest alignment, and the Credit River flows northwest to southeast under the bridge. The Credit River Bridge is located within Metrolinx-owned parcel PIN 13456-0580.

The Credit River Bridge is located in Port Credit, Mississauga. The general area around the Credit River Bridge is mixed residential and recreational. The residential areas primarily consist of single family homes with some multi-dwelling buildings. The recreational areas feature a combination of historic, mixed use recreational space, such as trails and parkland, and more recently constructed recreational businesses.

The City of Mississauga's Cultural Heritage Inventory (2005) recognizes the Credit River Corridor as a culturally significant landscape within the City for its landscape environment, historical associations, and historical, archaeological, and ecological interests. The Credit River Bridge is identified as a significant feature within the Credit River Valley, valued for its design and visual quality as well as its historical associations and landmark value. In addition, the City of Mississauga's *Official Plan* (Office Consolidation

FINAL HERITAGE IMPACT ASSESSMENT: CREDIT RIVER BRIDGE



2017 recognizes the Credit River as a heritage corridor (Policy 7.4.1.18). The bridge is listed on the City's Heritage Register.

The properties adjacent to the bridge include: the Port Credit Memorial Arena and Memorial Park to the east; a single family residential building to the north and to the west, and the Royal Canadian Legion (Port Credit Branch) and parking area to the south.

Figure 1-1: Location of the Credit River Bridge. Source: ESRI 2016



2 Discussion of Cultural Heritage Value and Status

2.1 Credit River Rail Bridge

The following statement of Cultural Heritage Value was adopted by the Metrolinx Heritage Committee. Figure 2 identifies the location of the Credit River Bridge within its surrounding landscape.

Name: Credit River Bridge

Address: GO Lakeshore West rail corridor, Mile 13.27.

Heritage Components:

Credit River Bridge

Prepared By: ASI

09-18-2017

Author: Archaeological Services Inc.

2.1.1 Description of Historic Place

The Credit River Bridge is located at Mile 13.27 of the GO Transit Lakeshore West rail corridor, and is located in the historic village of Port Credit, in the City of Mississauga. The three-span railway bridge was built in 1903 to the designs and specifications of the Grand Trunk Railway Company, and it was constructed by the Canadian Bridge Company Limited of Walkerville, Ontario. The bridge features a central inverted bowstring arch deck truss with steel beam approach spans on either side. It was widened to the north in 2008 to accommodate a third track. The bridge carries three tracks of rail traffic in an east and west direction across the Credit River, between Stavebank Road and Mississauga Road. While rail traffic travels in an east-west direction, it should be noted that at this segment of the rail corridor, the bridge and corridor is on a northeast-southwest alignment, and the Credit River flows northwest to southeast under the bridge. The Credit River Bridge is located within Metrolinx-owned parcel PIN 13456-0580.

2.1.2 Cultural Heritage Value or Interest

The Credit River Bridge spans the Credit River, listed as a cultural heritage landscape by the City of Mississauga, in the village of Port Credit. The bridge is a landmark in Port Credit and it contributes significantly to the scenic character of the river and the community. Further, given the age of the bridge, proximity to Port Credit GO Station, and the role of the railway corridor in the community, this bridge retains significant physical, functional, visual and historical links to the Credit River and to Port Credit.

The Credit River Bridge is directly associated with the GTR's program to double track its route from Montreal to Sarnia in the late nineteenth and early twentieth century. The current bridge was built to replace the original wooden railway bridge at this location. This was a significant improvement to railway infrastructure in southern Ontario that contributed to economic and population growth, particularly in the Greater Toronto Area.

The Credit River Bridge is a unique example of an inverted bowstring arch deck truss bridge and is thought to be one-of-a-kind in Ontario. The low curved chord underneath the bridge gives a sense of floating above the water as it extends over the Credit River, for an unsupported 210 ft (63 m). The unique design, combined with the span of the deck truss, demonstrates that the Credit River Bridge has a high degree of technical achievement. Distinctive features of this style of bridge construction include: combination of pin and riveted connections; heavy duty steel ten panel truss with diagonal members forming a Warren truss configuration; lower curved chord composed of lighter, less robust, steel; and massive eyebar bundles.

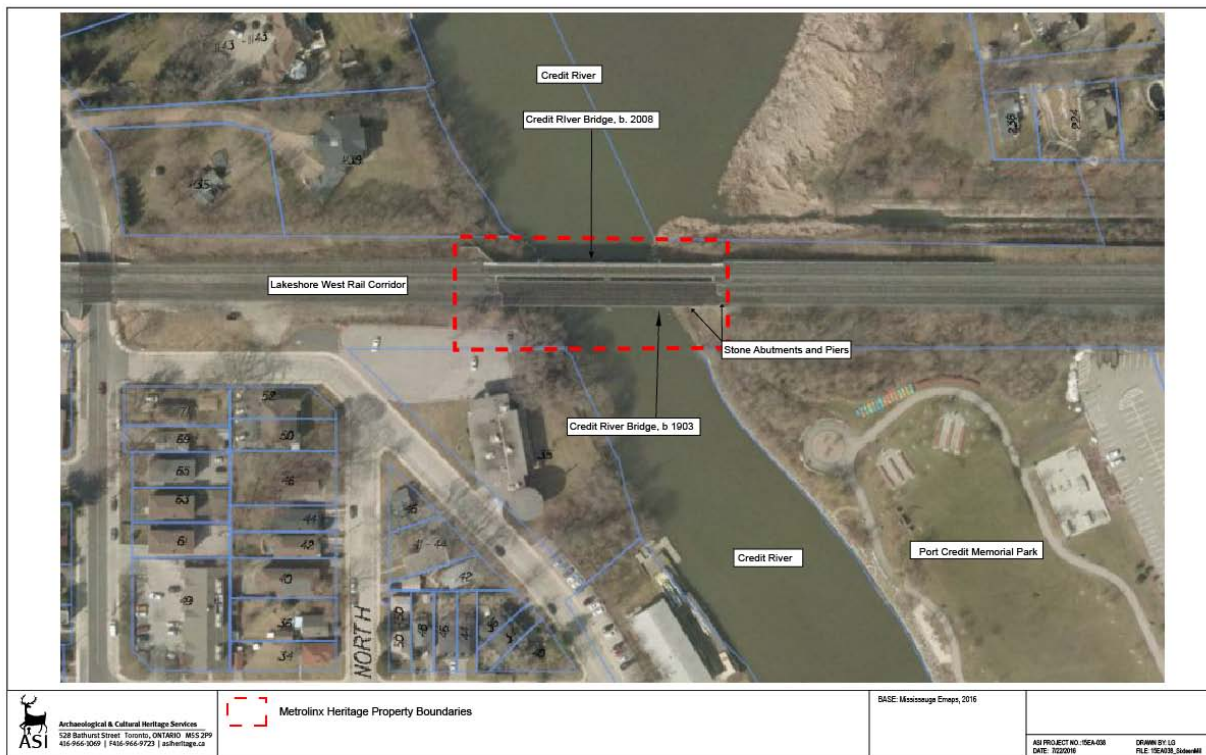
The Credit River Bridge was designed by Chief Engineer of the GTR, Joseph Hobson, and fabricated by the Canadian Bridge Company Limited of Walkerville, in 1903. Given its noted technical achievement, and unique design, the Credit River Bridge is considered to be a notable example of a bridge designed by Hobson, the GTR, and the Canadian Bridge Company Limited.

2.1.3 Description of Heritage Attributes

A list of heritage attributes that contribute to the cultural heritage value of the Credit River Bridge include its:

- Steel and masonry bridge design and construction (Plates 1-3);
- Stone masonry substructure;
- Three-span scale and dimension, including the 210 ft (63 m) central deck truss span and two steel beam approach spans (30 ft or 9 m each); Unique and unusual steel deck truss centre span with an inverted bowstring arch shape (Plates 4-6); and
- Combination of pin and riveted connections (Plate 7).

Figure 2-1: Map showing the location of the Credit River Bridge



3 Assessment of Site Conditions

A site visit was conducted by John Sleath, ASI, on 22 June 2016, to conduct photographic documentation of the Credit River Bridge and surrounding environs (Plates 1 – 8). The assessment was conducted from publicly-accessible areas. There were no limitations regarding access. This site visit was conducted as part of the TPAP for the GO Rail Network Electrification baseline conditions phase. It was determined that data collected during this site visit was sufficient for the purposes of completing the subject

Heritage Impact Assessment (HIA) report. It should be noted that a second site survey was not completed given both the minimal interval between the CHER and HIA reports and the sufficient level of recording completed for the CHER.

The Credit River Bridge is located on the outer fringe of the historic village of Port Credit in Mississauga, Ontario. The Credit River is predominantly used for recreational purposes, and is a significant waterway in the City of Mississauga and an important part of its history and development. Upriver, the Credit River is bounded by residential properties that front Mississauga Road or Stavebank Road. The Queen Elizabeth Way (QEW) Credit River Bridge, a concrete open spandrel deck arch bridge built in 1934, is located approximately 1.5 km upriver (northwest) from the subject bridge. The QEW Credit River Bridge has been classified as a Provincial Heritage Property of Provincial Significance.

Downriver, from the Credit River Bridge to where it opens into Lake Ontario, the land use adjacent to the river is a combination of commercial and recreational. On the southwest side of the river is the Royal Canadian Legion (Branch 82), the Mississauga Canoe Club, the Don Rowing Club of Mississauga, and some open green space and parking areas. The Port Credit Memorial Park is located on the northeast side of the river and is comprised of paved pathways, passive and active parkland, a library, an arena, and interpretive plaques providing information on the history of the river and Port Credit. The commercial core of Port Credit spreads out from the river along Lakeshore Road, with a marina and additional residential properties, parkland, and commercial parcels to the south of Lakeshore Road.

Another bridge in the vicinity of the subject bridge is the Lakeshore Road West Bridge over the Credit River. Located approximately 42 metres downstream (southeast) of the subject bridge, the Lakeshore Road West Bridge is a steel beam bridge built in 1960. Previous bridges at this location include the two-lane concrete bowstring arch bridge built in 1919, the 1895 iron through truss bridge built by the Peterborough Bridge Engineering Company, and pre-1895 there were at least two wooden trestle bridges which each were destroyed by storms and flooding (Mississauga Library System n.d.).

The Port Credit GO Station is located approximately 50 m northeast of the Credit River Bridge along the rail corridor. Another bridge, located approximately 15 m southwest of the subject bridge along the rail corridor, was built in 1923 to carry the rail lines over Mississauga Road. This is a deck plate girder bridge with stone abutments which was widened in 2008.

In summary, the character of the general vicinity of the Credit River Bridge is strongly tied to the natural and recreational elements of the Credit River, to the mouth of the Credit River where it meets Lake Ontario, and to the village of Port Credit. Further, the bridge is well-proportioned and fits easily into the landscape.

FINAL HERITAGE IMPACT ASSESSMENT: CREDIT RIVER BRIDGE

Plate 3-1: View of south elevation, from west bank.



Plate 3-2: View of south elevation, looking north.



Plate 3-3: View of the south elevation, from east bank.



Plate 3-4: Detail of the central section of the deck truss, south elevation.



Plate 3-5: Detail of eastern-part of the deck truss, south elevation.



Plate 3-6: Detail of truss members at west pier, south elevation.



Plate 3-7: Detail of eyebar bundles



Plate 3-8: East pier of 2008 bridge addition, south elevation.



4 Description and Purpose of Proposed Activity

The Credit River Bridge is located on the Lakeshore West rail corridor and is being assessed as part of the GO Rail Network Electrification TPAP. The purpose of the GO Rail Network Electrification project is to convert six GO rail corridors from diesel to electric propulsion including: Union Station Rail Corridor, Lakeshore West Rail Corridor, a portion of the Kitchener Rail Corridor, Barrie Rail Corridor, Stouffville Rail Corridor, and Lakeshore East Rail Corridor. Once electrification is implemented, the system will operate with a mixed fleet of diesel and electric trains, as not all tracks on all corridors will be electrified.

The population of the Greater Toronto Hamilton Area is increasing, and with it, traffic congestion. As part of *Moving Ontario Forward*, Metrolinx is committed to electrifying the GO Transit system to bring 15-minute, two-way electrified service to core parts of the network through the Regional Express Rail (RER) program. A component of the regional transportation plan, *The Big Move*, this program supports Metrolinx's goal of transforming the GO system into a comprehensive regional rapid transit network. Electrification of the GO network is a key component of the RER program. Electrification, combined with other RER initiatives such as building new tracks, new stations, etc. makes it possible to increase service levels and offers several other benefits (compared to diesel service).

The subject bridge is one of several rail overpass bridges along the rail corridor to be electrified, as outlined in the GO Rail Network Electrification Transit Project Assessment Process Draft Environmental Project Report Volume 1 – Scope and Detailed Project Description (2017). As a result, the Credit River Bridge will require the following modifications:

- Installation of Overhead Contact System (OCS) attachments to allow for grounding and bonding required for electrification:
 - to prevent damage from flashovers to the bridge structures
 - to prevent step and touch potential from exceeding permissible limits as defined in the applicable standards

Rail overpasses (such as the Credit River Bridge) that are greater than 60m in length will require OCS wires and/or portal structures to be attached to the structure because the OCS requires support in order to control the vertical height of the OCS over the tracks, as well as to ensure the OCS (horizontally) stays on top of the pantograph during operation.

Of the four design options for the OCS Attachments outlined in the Draft EPR, only the following is currently considered for the Credit River Bridge:

- 1) Install OCS support structures on rail overpass structures

The intervention will consist of fastening the OCS Attachment to a portion of the superstructure or substructure of the Credit River Bridge and possibly require integration of additional barriers (Figures 4-8).¹ Electrification infrastructure will be bolted to the structure and, if required, may utilize additional stabilizing members to ensure safety. Provided that pier-to-pier distance is sufficient, OCS Attachments will be placed on the abutments or piers of the bridge (substructure) in an effort to avoid impacting the superstructure. However, if the distance between piers and abutments is insufficient, additional bracing, plates or other structural elements may be required to reinforce the bridge pending a structure analysis. Grounding and bonding infrastructure might also be constructed to allow current to flow from metallic

¹ It should be noted that this report contains only existing available information to illustrate potential proposed interventions to the Credit River Bridge.

objects that are not intended to carry current and thus ensure public safety. However, inclusion of this infrastructure is not expected to result in significantly adverse visual alterations to the bridge.

Figure 4-1: Visualization of new OCS infrastructure at the Rouge River Crossing. Note that this visualization has been included for comparative and illustrative purposes. Source: Metrolinx 2017



Figure 4-2: Plans for typical OCS Attachment. Source: Metrolinx 2017

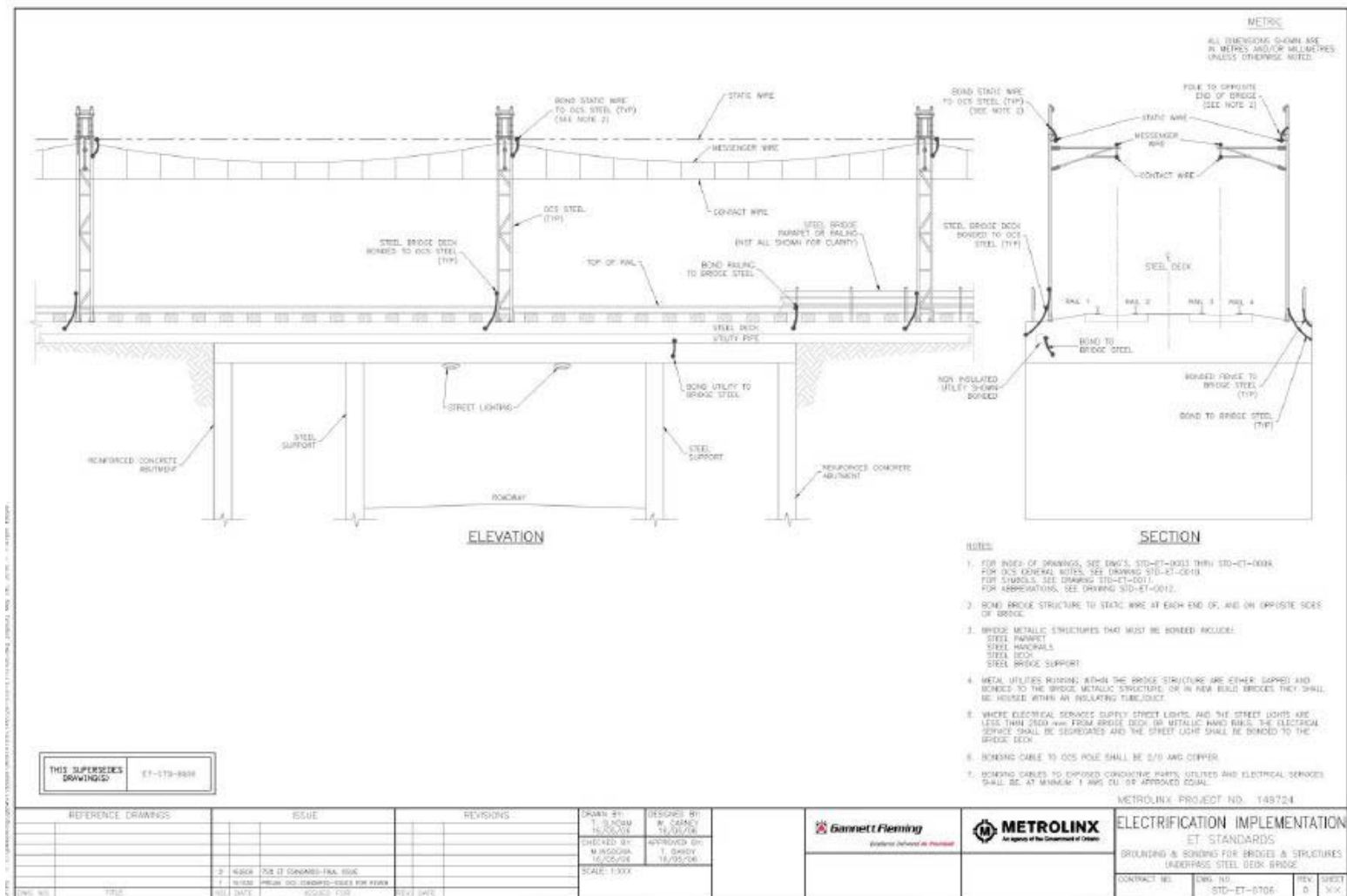


Figure 4-3: Detailed plan for the OCS Attachment on the Rouge River Bridge. Source: Metrolinx 2017

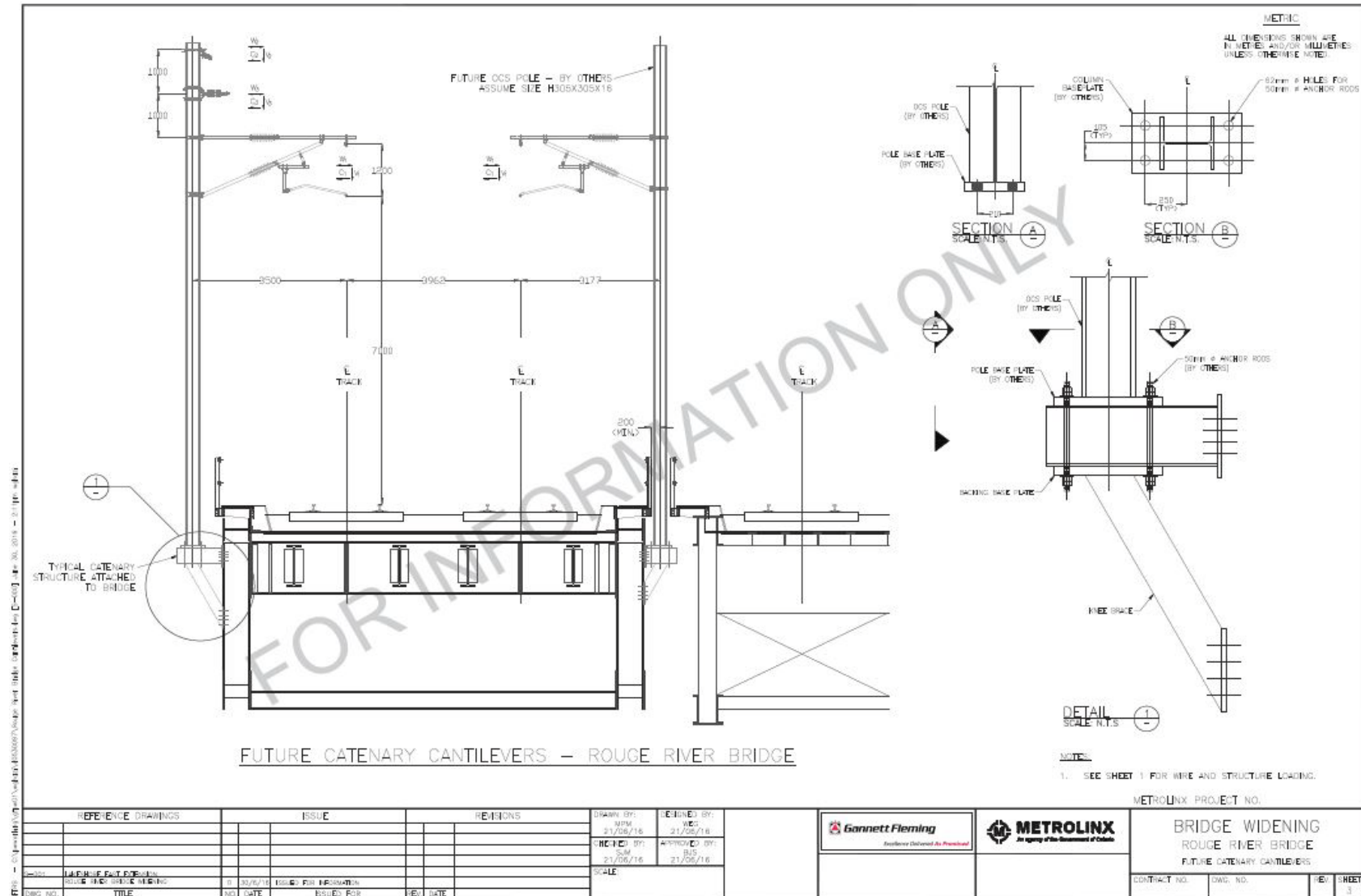


Figure 4-4: Detailed plan for the OCS Attachment on the Rouge River Bridge. Source: Metrolinx 2017

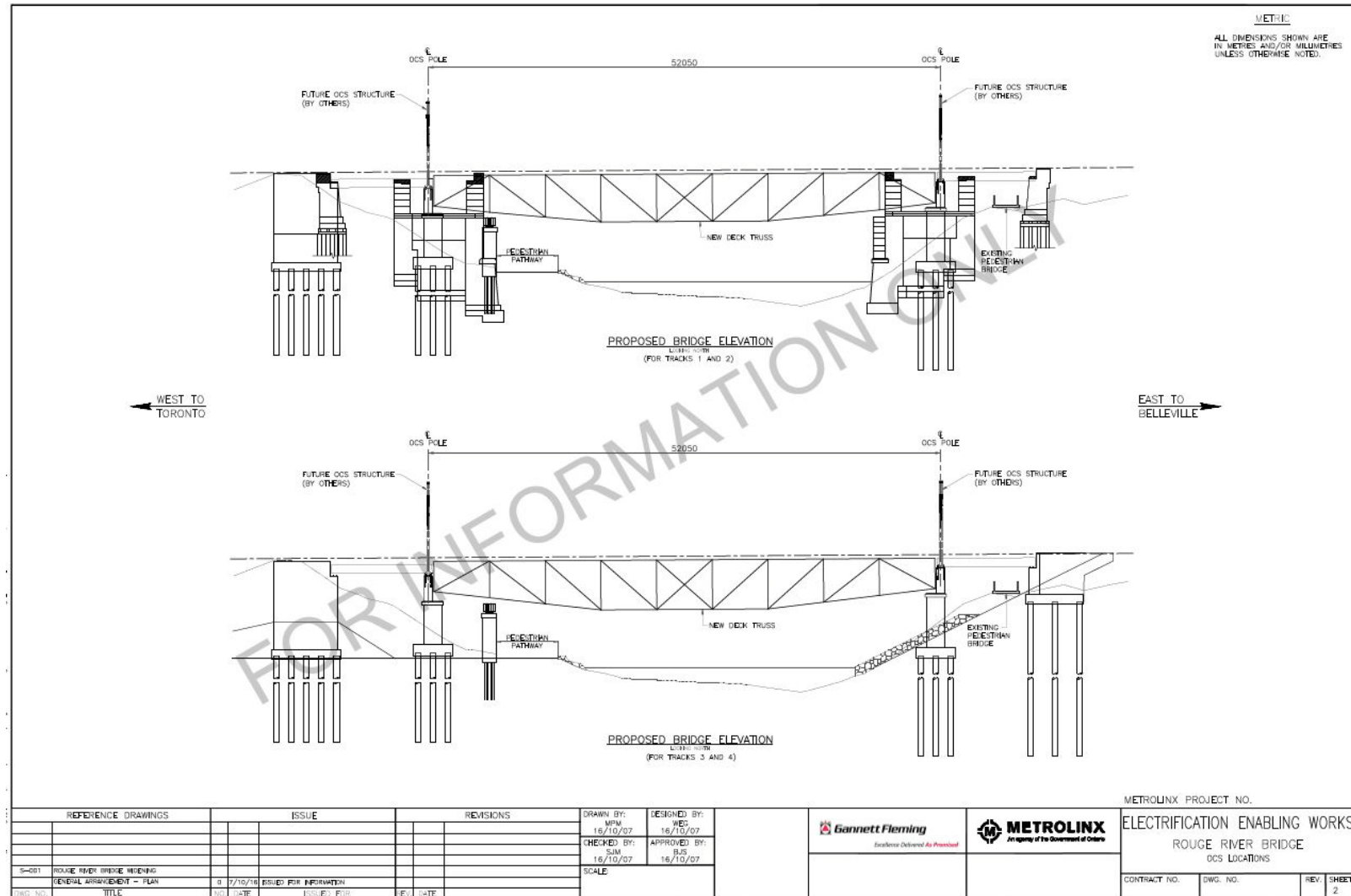


Figure 4-5: Detailed plan for a Gateway Portal on the Rouge River Bridge. Source: Metrolinx 2017

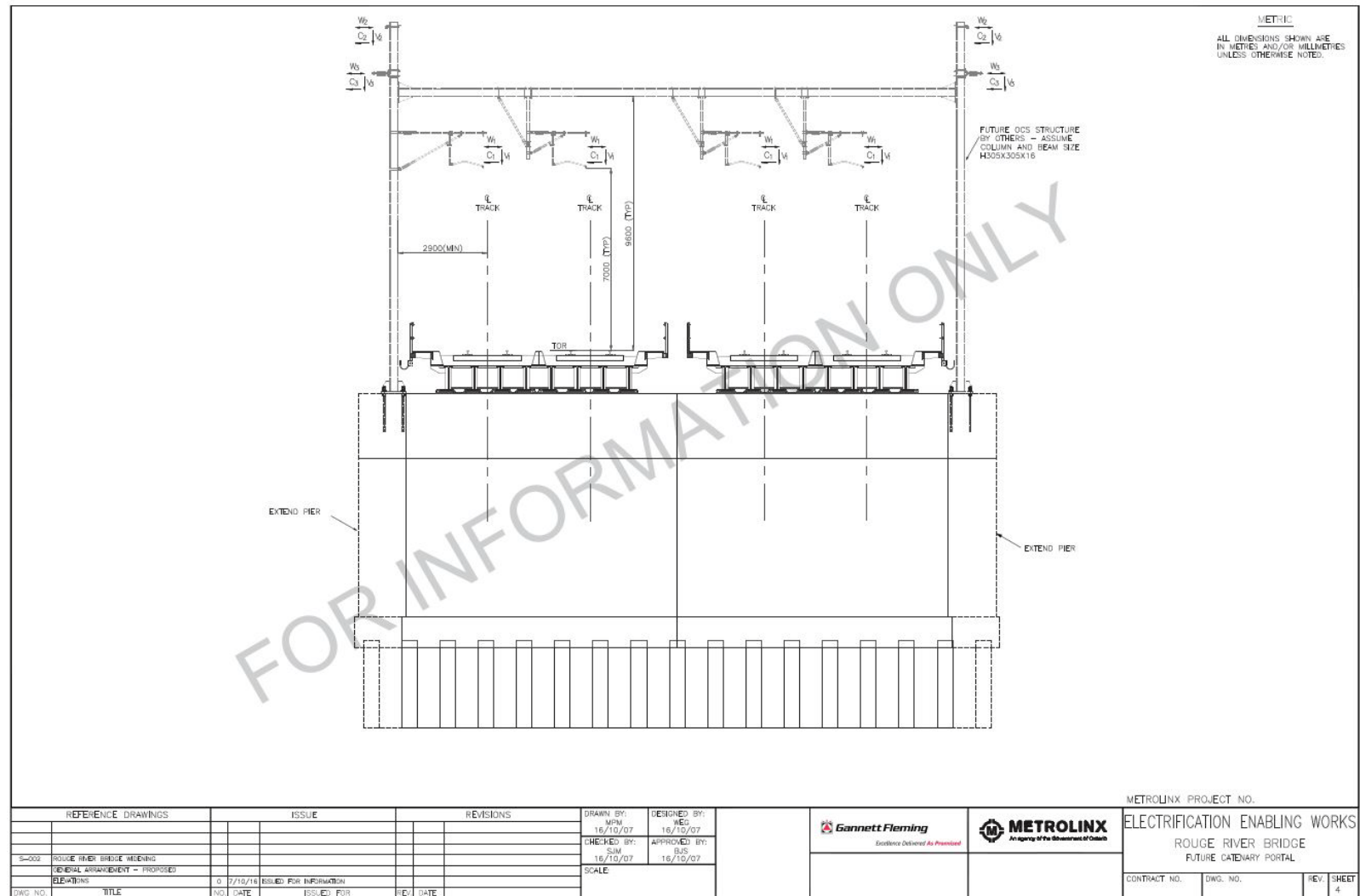


Figure 4-6: Example of anchor and bolt attachment. Source: Gannet Fleming 2017



5 Impact Assessment

To assess the potential impacts of the proposed works required as part of converting the GO network from diesel to electric propulsion, on the cultural heritage value of the Credit River Bridge, the identified heritage attributes were considered against a range of possible impacts as outlined in the *Ontario Heritage Tool Kit* (2006), and which include:

- Destruction of any, or part of any, significant heritage attribute or feature.
- Alteration which means a change in any manner and includes restoration, renovation, repair or disturbance.
- Shadows created that alter the appearance of a heritage attribute or change the visibility of a natural feature of plantings, such as a garden.
- Isolation of a heritage attribute from its surrounding environment, context, or a significant relationship.
- Direct or indirect obstruction of significant views or vistas from, within, or to a built and natural feature.
- A change in land use such as rezoning a battlefield from open space to residential use, allowing new development or site alteration to fill in the formerly open spaces.
- Soil Disturbance such as a change in grade, or an alteration of the drainage pattern or excavation.

As part of the analysis of impacts, factors such as scale, severity, and reversibility are also considered.

In addition, the Outline for a Heritage Assessment for a Provincial Heritage Property created by the MTCS was consulted to ensure this assessment was compliant with MTCS standards guiding Heritage Impact Assessments for Provincial Heritage Properties of Provincial Significance.

The following table presents the results of impact assessment based on the general design (January 2017). It considers possible direct adverse impacts, indirect adverse impacts, and positive impacts. See section 2.1.3 for a description of the cultural heritage attributes identified for the Credit River Bridge.

Table 1: Impact Assessment	
Impact	Description of Impacts on Heritage Attributes of the Credit River Bridge
Destruction, removal or relocation	The proposed addition of electrification components will not result in the destruction, removal or relocation of all or part of any heritage attribute.

FINAL HERITAGE IMPACT ASSESSMENT: CREDIT RIVER BRIDGE



Table 1: Impact Assessment	
Impact	Description of Impacts on Heritage Attributes of the Credit River Bridge
Alteration	<p>As a result of the proposed undertaking, the Credit River Bridge will be subject to alteration due to introduction of OCS attachments to the substructure or superstructure of the bridge and possible integration of additional barriers. Electrification infrastructure will be bolted to the structure and, if required, may utilize additional stabilizing members to ensure safety. Provided that pier-to-pier distance is sufficient, OCS Attachments will be placed on the abutments or piers of the bridge (substructure) in an effort to avoid impacting the superstructure. However, if the distance between piers and abutments is insufficient, additional bracing, plates or other structural elements may be required to reinforce the bridge pending a structural analysis. Grounding and bonding infrastructure might also be constructed to allow current to flow from metallic objects that are not intended to carry current and thus ensure public safety.</p> <p>As the attachment of this infrastructure to the substructure will require fewer attachments and structural elements, this approach would result in a less severe material impact to the bridge fabric. In addition, the fewer contact points will result in a greater degree of reversibility or reduce the likelihood of permanent damage to the structure. Finally, attachments on the substructure are expected to be sited at the abutments, therefore limiting visual interventions by concentrating them at lateral edges. While the introduction of OCS attachments are not expected to result in significantly adverse impacts on identified heritage attributes, these impacts should be minimized through application of appropriate mitigation measures.</p>
Shadows	Given the limited size of the proposed OCS Attachments and associated infrastructure, no significant shadows are anticipated.
Isolation	The proposed inclusion of electrification infrastructure to the Credit River Bridge is not expected to isolate identified heritage attributes from their surrounding environment, context, or a significant relationship.

Table 1: Impact Assessment	
Impact	Description of Impacts on Heritage Attributes of the Credit River Bridge
Direct or indirect obstruction of significant views	No views were identified as significant in the Statement of Cultural Heritage Value.
A change in land use	The proposed inclusion of electrification infrastructure will not result in a change in land use.
Soil disturbance	The proposed inclusion of electrification infrastructure is not believed to result in soil disturbance within the property.

6 Considered Alternatives and Mitigation Measures

The Credit River Bridge is located on the Lakeshore West rail corridor and is being assessed as part of the GO Rail Network Electrification Transit Project Assessment Process. The subject bridge is one of several rail overpass bridges along the rail corridor to be electrified, as outlined in the Metrolinx Electrification Draft EPR (2017).

The fixing of OCS Attachments to the Credit River Bridge is proposed as part of the electrification of the Lakeshore West Corridor. This development will consist of fastening the upright elements of the OCS Attachments directly to the superstructure or substructure of the bridge using bolts. In some cases, bracing elements may be required for additional safety.

Provided that pier-to-pier distance is sufficient, OCS Attachments will be placed on the abutments or piers of the bridge (substructure) in an effort to avoid impacting the superstructure. However, if the distance between piers and abutments is insufficient, additional bracing, plates or other structural elements may be required to reinforce the bridge pending a structure analysis. Grounding and bonding infrastructure might also be constructed to allow current to flow from metallic objects that are not intended to carry current and thus ensure public safety. However, inclusion of this infrastructure is not expected to result in significantly adverse visual alterations to the bridge.

According to available documentation, the intervention is reversible. However, care should be taken to ensure that no portions of the superstructure or substructure are permanently altered through the attachment of electrification infrastructure. Any plans for placing electrification equipment on the substructure or superstructure should attempt to limit the number of connections and ensure that all interventions are reversible.

Where possible, the electrification infrastructure fastened to the bridge should be designed to maintain the architectural form or type of the bridge. Significantly adverse impacts to the bridge's form, type and massing may be mitigated by minimizing the number of OCS Attachments, and using materials that will make the OCS Attachments physically and visually compatible with, subordinate to, and distinguishable from the Credit River Bridge.

7 Summary of Community Engagement

Consultation with the Ontario Heritage Trust, the Ministry of Tourism, Culture, and Sport (MTCS), and heritage staff at the City of Mississauga regarding the subject property took place as part of the Cultural Heritage Screening Report (ASI 2016). However, given that the Credit River Bridge in Mississauga is identified as retaining municipal heritage recognition, additional consultation with heritage staff was undertaken as part of the GO Electrification Rail Electrification Transit Project Assessment Process (TPAP).

Paula Wubbenhorst, Senior Heritage Coordinator at the City of Mississauga was consulted on June 9, 2016, via email. Ms. Wubbenhorst confirmed that the Credit River Bridge is listed on the City of Mississauga's Cultural Landscape Inventory (2005), and that there is no additional information about the bridge on file at the municipality. The City also confirmed that the bridge is listed on their Heritage Register. Appendix A provides correspondence received from and submitted to the City of Mississauga.

As part of the GO Rail Electrification TPAP, Metrolinx completed Round #1 of the project's public meetings between February 16 and March 22 of 2016. A total of 15 meetings were held, with two devoted to the Lakeshore West Corridor, held in Oakville and Burlington. High-level information on the cultural heritage was addressed at this meeting, though the Credit River Bridge was not specifically identified in the presentation boards.

A second round of public meetings was held between November 7 and November 29, 2016. During this period a meeting was held at the First United Church at the City of Mississauga on November 29, 2016. Story Boards for this meeting did not directly address the Credit River Bridge, though they did note that impacts to bridges identified as having heritage value will be assessed through a Heritage Impact Assessment. The Summary of Participant Feedback supplied by Metrolinx indicated that the impacts to the heritage value of the Credit River Bridge were not identified as primary feedback during the meeting and in written forms. However, there was some discussion on the visual and design impacts of the project on heritage bridges, but no specific heritage concerns appear to have been raised in relation to the Credit River Bridge.

A third round of public meetings was held in July 2017. Although there was some discussion on the impacts of the project on heritage bridges generally, no specific concerns were raised in relation to the Credit River Bridge.

A number of stakeholder groups were asked to complete a questionnaire during the completion of the CHER (ASI 2016)² with the purpose of collecting any information relating to the Credit River Bridge along the Lakeshore West Corridor. The results of this exercise are presented in the completed CHER. As of September 2017, this report is being circulated to the City of Mississauga for review and comment.

8 Recommendations

Introduction of the OCS Attachments is not expected to result in significantly adverse impacts on the bridge's identified heritage attributes. OCS attachments will be installed to the substructure or superstructure of the bridge. According to exiting material, the intervention is reversible. However, the OCS Attachments do have the potential to alter the legibility of Credit River Bridge as a unique example of an inverted bowstring arch deck truss bridge constructed of steel and stone masonry. In addition, the placement of the OCS Attachments on the superstructure could result in the introduction of bracing/plates/structural supports. As such, the following mitigation measures should be undertaken:

1. The OCS Attachments should be installed to be compatible with the bridge's type and massing and to minimize material interventions. Mitigations may include sitting OCS attachments at the edges of the bridge and using materials and finishes that would make the new infrastructure physically and visually compatible with, but subordinate to and distinguishable from the bridge.
2. The number of connections used to attach electrification infrastructure to the Credit River Bridge should be minimized. It is anticipated that installation of OCS Attachments at abutments/piers would be bolted to the substructure whereas installation at the superstructure level may require introduction of additional bracing, plates, and or structural elements.
3. All interventions should be designed to be reversible. Where interventions are undertaken that will result in alterations to material and fabric, documentation should be undertaken in advance of installation activities. The purpose of documentation is to record existing conditions of the bridge at a level of detail for the purposes of implementing a program to reverse impact should OCS attachments be removed in future due to changes in technology or operational priorities.
4. Given that the subject bridge has been identified in the City of Mississauga's Cultural Landscape Inventory (2005) and listed on the City's Heritage Register, the subject report has been circulated to the City of Mississauga as of September 2017 for review and comment. Any updates to this HIA undertaken during detailed design will be undertaken in consultation with the MTCS and heritage staff at the City of Mississauga. As the Credit River

² Communication with stakeholder groups occurred on 3 June 2016

Bridge is a Provincial Heritage Property of Provincial Heritage Significance, any request for demolition or transfer from provincial control will require Ministerial consent from the Ministry of Tourism, Culture and Sport (MTCS). It is noted that Federal heritage approvals are not anticipated to be required in relation to the proposed electrification modifications to the Credit River Bridge.

5. Detail Design and implementation of interventions at the Credit River Bridge should be guided by a qualified heritage professional who is a member of the Canadian Association of Heritage Professionals and who has demonstrated experience developing impact assessments and conservation plans for culturally significant road and rail bridges.

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FINAL HERITAGE IMPACT ASSESSMENT: CREDIT RIVER BRIDGE



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2nd level should be same blue color & font size as 1st level, but not BOLD.

- For bulleted lists, please start under 1st letter of the heading, as shown. If it's a multi-line bullet, text should line up as shown in this example.
- The bullets should be single spaced with a 10 pt space between each new bullet. This can be found in Paragraph settings as shown in pic below:

10 Project Personnel

<i>Project Manager:</i>	Annie Veilleux, MA Manager, Cultural Heritage Division
<i>Cultural Heritage Specialist:</i>	Joel Konrad, PHD Cultural Heritage Specialist
<i>Project Coordinator:</i>	Sarah Jagelewski, Hon. BA Staff Archaeologist Assistant Manager, Environmental Assessment Division
<i>Project Administration:</i>	Carol Bella, Hon. BA Research Archaeologist Administrative Assistant
<i>Report Preparation:</i>	Joel Konrad
<i>Graphics:</i>	Jonas Fernandez, MA Geomatics Specialist
<i>Senior Advisor and Report Reviewer:</i>	Rebecca Sciarra, MA Partner and Director of Business Services

APPENDIX A: Correspondence Received from and Submitted to the City of Mississauga

Kirstin Geissler

From: Electrification <Electrification@metrolinx.com>
Sent: Thursday, September 7, 2017 4:39 PM
To: Evie Przybyla; paula.wubbenhorst@mississauga.ca
Cc: James Hartley; Morayo Olagunju; Amber Saltarelli; patricia.staite@HydroOne.com; Sanzo, Adam (MOECC); Dan Beare; Rupesh Udash
Subject: Re: Metrolinx Electrification TPAP - Draft EPR for Comment - Responses
Attachments: GO Rail Network Elect TPAP_Credit River Bridge_HIA_03August2017_Clean.pdf; Mx Electrification TPAP_City of Mississauga_Draft EPR Responses_Issued_23June17.pdf; 170907 - E.Przybyla P.Wubbenhorst Credit River Bridge HIA_Metrolinx Electirifcation TPAP_Final.pdf

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Ms. Przybyla / Ms. Wubbenhorst:

As a follow up to Metrolinx's response to the City of Mississauga's Draft Environmental Project Report (EPR) Comment #9 (attached for reference) issued June 23, 2017, a Heritage Impact Assessment (HIA) has been undertaken for the Credit River Bridge (situated along the GO Lakeshore West Rail Corridor). This HIA will be appended to the Final EPR, which will be made available for a 30-day public review period upon issuing the TPAP Notice of Completion on October 11, 2017. An advance copy of the HIA has been enclosed with this correspondence for the City's information/reference.

Should you have any questions, please do not hesitate to reach out to us.

Sincerely,

GO Rail Network Electrification Project Team

Electrification@metrolinx.com

www.gotransit.com/electrification/

From: Evie Przybyla <Evie.Przybyla@mississauga.ca>
Sent: Tuesday, August 15, 2017 1:13 PM
To: Electrification
Cc: James Hartley; Morayo Olagunju; ASaltarelli@morrisonhershfield.com; patricia.staite@HydroOne.com; Sanzo, Adam (MOECC); Dan Beare
Subject: RE: Metrolinx Electrification TPAP - Draft EPR for Comment - Responses

Good Afternoon,

I received a voicemail from Allison requesting that I confirm receipt of the email below.
We do not have any additional questions or concerns at this time.

Best,
Evie



Evie Przybyla

Transportation Planner, Transportation Planning
T 905-615-3200 ext.5857
evie.przybyla@mississauga.ca

City of Mississauga | Transportation & Works Department,
Transportation & Infrastructure Planning Division



How will we get around in the next 25 years?

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From: Electrification [mailto:Electrification@metrolinx.com]

Sent: 2017/07/11 2:28 PM

To: Evie Przybyla; Susan Tanabe

Cc: James Hartley; Morayo Olagunju; ASaltarelli@morrisonhershfield.com; patricia.staite@HydroOne.com; Sanzo, Adam (MOECC); Dan Beare

Subject: RE: Metrolinx Electrification TPAP - Draft EPR for Comment - Responses

Dear Ms. Przybyla:

Thank you for providing comments on the Draft Environmental Project Report for the GO Rail Network Electrification Project. On June 23rd, 2017 the Project Team provided an email (below) with the attached table summarizing how each comment was considered and additional clarification and/or information was provided, where applicable.

Please note, the materials from the third round of Metrolinx public meetings, held from June 26th-July 5th, are now available online at: <http://www.gotransit.com/electrification/en/default.aspx>

Should you have any additional questions, please do not hesitate to reach out to us.

Sincerely,

GO Rail Network Electrification Project Team

Electrification@metrolinx.com

www.gotransit.com/electrification/

From: Electrification

Sent: June-23-17 5:00 PM

To: evie.przybyla@mississauga.ca; susan.tanabe@mississauga.ca

Cc: James Hartley; Morayo Olagunju; ASaltarelli@morrisonhershfield.com; patricia.staite@HydroOne.com; Sanzo, Adam (MOECC)

Subject: Metrolinx Electrification TPAP - Draft EPR for Comment - Responses

Dear Ms. Przybyla:

Thank you for providing comments on the Draft Environmental Project Report for the GO Rail Network Electrification Project. The Project Team has completed their review of the comments provided. The attached table has been prepared to summarize how each comment was considered and additional clarification and/or information has been provided, where applicable.

Should you have any additional questions, please do not hesitate to reach out to us.

James Hartley
Manager, Environmental Programs and Assessment
Metrolinx – GO Transit
20 Bay Street, Suite 600
Toronto, ON M5J 2W3
Tel: 416-202-4894 Email: electrification@metrolinx.com
www.gotransit.com/electrification

From: Evie Przybyla <Evie.Przybyla@mississauga.ca>
Sent: Wednesday, March 1, 2017 3:46 PM
To: Electrification
Cc: Susan Tanabe
Subject: Electrification - City of Mississauga - Comments

Good Afternoon,

Please find attached to this email comments on the Draft Environmental Project Report (EPR) for the GO Rail Network Electrification Transit Project Assessment Process (TPAP) as well as supporting documentation.

Thank you for the opportunity to comment on this exciting project. Should you have any questions or require clarification please do not hesitate to contact me.

Best Regards,
Evie



Evie Przybyla

Transportation Planner, Transportation Planning
T 905-615-3200 ext.5857
evie.przybyla@mississauga.ca

City of Mississauga | Transportation and Works Department,
Transportation and Infrastructure Planning Division

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GO Rail Network Electrification TPAP – Draft EPR Comment/Responses

Item No.	Issue	Comment/Issued Raised by Review Agency	How Comment was Considered by Metrolinx
City of Mississauga			
1	General Comment – Kitchener Corridor	Kitchener Corridor: There are currently six (6) watercourse erosion control projects forecasted on the City’s 10-yr Capital Works programming plan; where the upstream or downstream limits of construction about the Kitchener GO rail corridor. The projects have tentative construction start dates ranging from 2018 to 2025 which are subject to change on a yearly basis. Please review “2017-2026 Watercourse and Storm Drainage Improvements” Map provided, and consider the timing of electrification activities that will traverse the watercourses proposed for erosion control works. The City should be notified to coordinate/mitigate any potential conflicts (contractor, staging, timing etc.).	<p>Noted. As the City’s 10-year Capital Works programming plan progresses with respect to the forecasted watercourse erosion control projects, it is recommended that the City keep Metrolinx well apprised. In addition to the provided map of timelines, any additional information on details of the works should be provided to Metrolinx for overlap areas.</p> <p>Continued coordination with local municipalities regarding the project’s design and construction schedule will be undertaken during detail design.</p>
2	General Comment – Lakeshore West Corridor	Lakeshore West Corridor: There are currently five (5) watercourse erosion control projects forecasted on the City’s 10-yr Capital Works programming plan; where the upstream or downstream limits of construction about the Kitchener GO rail corridor. The projects have tentative construction start dates ranging from 2020 to 2026 which are subject to change on a yearly basis. Please review “2017-2026 Watercourse and Storm Drainage Improvements” Map provided, and consider the timing of electrification activities that will traverse the watercourses proposed for erosion control works. The City should be notified to coordinate/mitigate any potential conflicts (contractor, staging, timing etc.).	Please see response to Comment #1.
3	Stormwater Management	<p>Please see Appendix K – SWM Assessment Report</p> <p>It is noted that there are no Tap/TPS or SWS stations proposed for construction within the City of Mississauga. Should the location of any proposed Tap/TPS or SWS stations (or any appurtenances) be relocated to within Mississauga city-limits, please ensure these plans are circulated to Environmental Services Section for review from a stormwater management perspective.</p>	<p>Acknowledged.</p> <p>No relocation of Traction Power Facilities to within the City of Mississauga limits is anticipated. Should this change consultation with the City and stakeholders will be undertaken as noted in Environmental Project Report (EPR) Volume 5 as part of a Transit Project Assessment Process (TPAP) addendum.</p>
4	Stormwater Management	<p>Re: Page 22-23 – Culvert Capacity Criteria</p> <p>Should relief culverts be required at watercourse crossing locations within Mississauga city-limits, please circulate design summaries/plans to Environmental Services Section for review.</p>	As noted in EPR Appendix K, no footprint impacts to the culverts or watercourses are anticipated to result from the installation of overhead contact system (OCS) within the existing corridor above the culverts. No changes to the track design are proposed, as such no additional culverts will be required as part of the Electrification Project.
5	Visual Impact	There is concern regarding the visual impact of the overhead system. In the report, it is noted that most areas in Mississauga along the corridor will experience a negligible to low impact associated with the infrastructure given existing vegetation. However, it is noted that in the area between Port Credit and Clarkson that there is an increase in visual impact. Mitigation (e.g. vegetative screening) is noted as being explored in the next phase (detail design); however, Metrolinx should provide additional details on who they will be working with (e.g. City, Property Owners) to determine the appropriate mitigation measures.	<p>Visual impact mitigation strategies for OCS will be identified and incorporated into the design process. These strategies will address the range of visual conditions, area allocations, and mitigation needs that will be found along the corridor. Areas of ‘high’ visual impact will be identified and specific design measures will be incorporated to mitigate visual impacts of OCS.</p> <p>Metrolinx will work with affected parties during detailed design as required.</p>
6	Natural Environment – Tree/Vegetation Removal	Please be advised that trees located within the GO Transit Right of Way are exempt from the City By-Law’s with regard to removal and replacement however trees on private property are not. It is the City’s expectation that Metrolinx/GO Transit will act as a liaison between the owners of the land on which the tree in question is located and facilitate the removals process as per City policy. The pertinent City of Mississauga By-Laws, standards and information have been provided as attachments to this memo. The information and processes that were provided to the HuLRT team will apply to this project as well.	<p>Acknowledged. Metrolinx is establishing a Vegetation Compensation Protocol for Metrolinx RER projects and vegetation that is removed will be compensated for in accordance with the provisions of this protocol.</p> <p>For Municipal/Private Trees: Metrolinx will work with each municipality to develop a municipality-wide streamlined tree permitting /compensation approach for municipal and private trees. The goal is to reduce administrative permitting burden for trees along long stretches of rail corridor.</p> <p>For Trees within Metrolinx property: Metrolinx is developing a methodology to compensate for trees located within Metrolinx’s property. This will involve categorizing trees community types/ ecological value and establishing the appropriate level of compensation. Metrolinx will be looking to partner with Conservation Authorities and municipalities to develop the final compensation plan.</p>

Item No.	Issue	Comment/Issued Raised by Review Agency	How Comment was Considered by Metrolinx
			<p>Conservation Authorities: For vegetation removals within conservation authority lands where required, applicable removal and restoration requirements will be followed.</p> <p>Federal lands: For vegetation removals within Federally-owned lands where required, applicable removal and restoration requirements will be followed.</p> <p>Tree End Use: We will develop options for the end use of trees removed from Metrolinx property e.g reuse/recycling options.</p> <p>Metrolinx is continuing to work towards the Protocol and will follow up with stakeholders that have been engaged and participated to date and provide a draft for review. The final EPR will contain commitments to the Protocol which will be made publicly available once final.</p>
7	Archaeological Impacts	Appendix D- Archaeological Assessment Report: The recommendations on p.122 and 133 do not include information for lands within the City of Mississauga. P. 43 of the report appears to indicate that no further archaeological work will be required. Please clarify what are the recommendations for lands within the study area that fall within the City of Mississauga.	<p>Lands within the City of Mississauga were included and assessed as part of the Stage 1 Archaeological Assessment (AA) Report. As no Traction Power Facilities are proposed within the City of Mississauga, impacts will be limited to those proposed along the existing Lakeshore West (LSW) corridor Right of Way (ROW) in association with the OCS footprint and Tree/Vegetation Clearing Zones.</p> <p>As noted in Sections 4.2.6 (pg. 122) and Section 5.2 (pg. 133) the LSW OCS footprint has been assessed. The property inspection confirmed that the OCS footprint includes an active GO Rail Corridor on disturbed lands. Both sections note archaeological potential has been removed, and no further archaeological assessment will be required.</p>
8	Archaeological Impacts	Archaeology in general: Heritage Planning, requests to be forwarded a copy of all archaeological reports and corresponding Ministry of Tourism, Culture and Sport letters for comment pertaining to lands assessed within the City of Mississauga	Archaeological Assessment reports prepared for the GO Rail Network Electrification Project were provided as Appendix D of the draft EPR circulated to key stakeholders for review and comment in January 2017. The Stage 1 AA report has been submitted to the City of Mississauga, Indigenous Communities, as well as Ministry of Tourism, Culture and Sport (MTCS) for review/comment.
9	Cultural Heritage	<p>Appendix C- Part C 2: The Port Credit Arena and Credit River Bridge have been included in the report.</p> <p>It is noted that no project activities are foreseen for the Port Credit Arena at 40 Stavebank Road. However, construction impacts are noted under Avoidance/Mitigation/compensation column on Table 6-1. The property is designated part IV of the OHA and therefore a heritage permit may be required in the event that alterations are proposed.</p> <p>Credit River Bridge: The bridge is listed in the City of Mississauga’s heritage register. As such, proposed demolitions require a heritage permit. Furthermore, the property is subject to the Standards and Guidelines for Conservation of Provincial Heritage Properties as it is owned by Metrolinx. A detailed Heritage Impact Assessment and Conservation Plan may be required to be submitted for review and approval to the Ministry of Tourism Culture and Sport depending on the proposed alterations to the bridge. The commitment to complete an HIA is noted in the report. Heritage Planning requests to be kept up to date with regards to proposed works and submissions to the Ministry (MTCS).</p>	<p>Noted.</p> <p>Modifications to the Credit River Bridge are proposed in order to accommodate electrification infrastructure as described in EPR Volume 3 and Appendix C.</p> <p>Demolition of the structure is not proposed. A Heritage Impact Assessment (HIA) is in progress to assess the proposed modification and will be shared with the City of Mississauga and MTCS for review and comment. In addition, this assessment will be summarized and appended to the Final EPR for the 30-day public review period upon issuing the TPAP Notice of Completion.</p> <p>Continued coordination with local municipalities regarding the project’s design, including impacts to heritage properties will be undertaken as required during detail design.</p>
10	Cultural Heritage	Credit River Bridge CHER: p. 25, 27 ad 28 images are not showing. It is not clear what heritage attributes are proposed by the consultant as part of the identification of the bridge as having provincial significance	Errors with the images noted will be reviewed and corrected. Heritage attributes of the structure are identified in the Statement of Cultural Heritage Value (SCHV) provided in Appendix M6.
11	Natural Environment – Tree/Veg Removal	We’ve added a list of the acceptable trees for planting in the City of Mississauga. Another key component that we would require would be an inventory of all of the trees being removed and protected along any	Noted. The list of acceptable trees for planting in Mississauga will be considered as the Metrolinx Vegetation Compensation Protocol is developed.

Item No.	Issue	Comment/Issued Raised by Review Agency	How Comment was Considered by Metrolinx																																																
		construction corridor. This way we can adjust our inventory as needed and have an accurate idea of what we needs to be replaced as a result of removals and the linked construction activities.	Please note that a conservative quantification of areas of removal within and outside the Metrolinx ROW was provided in EPR Volume 3 and Appendix A2 based on ELC analysis. Refer to Tables 4-12, 4-14, 4-16 in EPR Volume 3 for vegetation removal areas expressed in hectares. During detail design, more detailed Tree Inventories will be completed.																																																
		<p>CITY OF MISSISSAUGA</p> <p>ACCEPTABLE STREET TREE SPECIES</p> <ul style="list-style-type: none">70% of planting should be selected from the proven performing list30% of planting should be selected from the notable performing list <table><tr><th colspan="3">TABLE 1: PROVEN PERFORMING FULL FORM TREES</th></tr><tr><th>Botanical Name</th><th>Common Name</th><th>Characteristics</th></tr><tr><td><i>Acer saccharinum</i></td><td>Silver Maple</td><td>Develops large crown, plant in locations with adequate space</td></tr><tr><td><i>Aesculus glabra</i></td><td>Ohio Buckeye</td><td>Showy flower spikes followed by seed husks covered in soft spines. Less susceptible to leaf scorch and leaf blotch than horsechestnut</td></tr><tr><td><i>Acer x freemanii</i> 'Autumn Blaze'</td><td>Autumn Blaze Maple</td><td>Sensitive to desiccation from winter winds. 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City of Mississauga
Memorandum



Date: 2017/11/23
To: Chair and Members of Heritage Advisory Committee
From: Paul Damaso, Director, Culture Division
Meeting Date: 2018/01/09
Subject: Alterations to a Heritage Listed Property: 1576 Dundas Street West

This memorandum and its attachment are presented for HAC's information only.

The subject property is listed on the City's Heritage Register. As per section 7.4.1.10 of the Mississauga Official Plan, "Applications for development involving heritage resources will be required to include a Heritage Impact Assessment prepared to the satisfaction of the City and other appropriate authorities having jurisdiction." As such, the Heritage Impact Assessment is attached for your reference.

Attachments

Appendix 1: Heritage Impact Assessment

A handwritten signature in black ink, appearing to read 'Paul Damaso', written in a cursive style.

Prepared by: Paul Damaso, Director, Culture Division

HERITAGE IMPACT ASSESSMENT

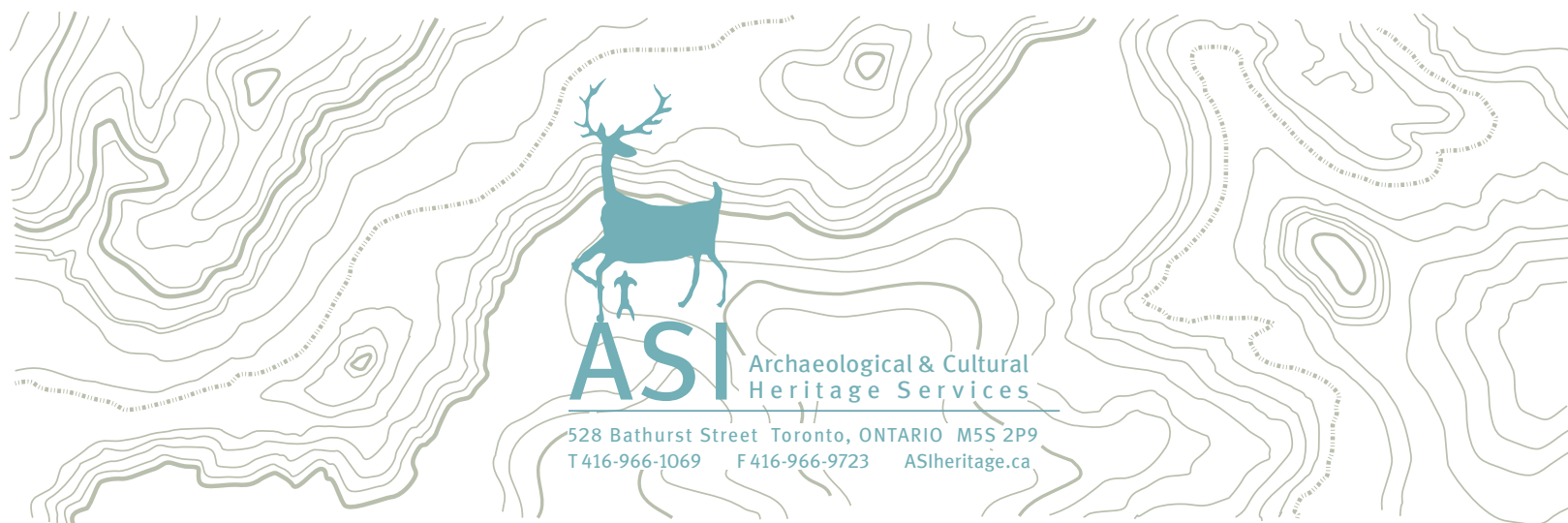
**1576 DUNDAS STREET WEST, ERINDALE
CITY OF MISSISSAUGA, ONTARIO**

Prepared for:

Weiss Architecture and Urbanism Ltd.
95 Pelham Avenue
Toronto Ontario
M6N1A5

ASI File: 17CH-114

October 13, 2017



HERITAGE IMPACT ASSESSMENT

**1576 DUNDAS STREET WEST, ERINDALE
CITY OF MISSISSAUGA, ONTARIO**

EXECUTIVE SUMMARY

ASI was contracted by Weiss Architecture and Urbanism Ltd to prepare a Heritage Impact Assessment (HIA) of the property at 1576 Dundas Street West in the City of Mississauga, Ontario. This HIA is structured to provide an evaluation of the impacts of the proposed development at 1576 Dundas Street West on surrounding heritage resources. The property includes a listed building known as the Taylor-Froebel House (1584 Dundas Street West), and designated property known as the Erindale Presbyterian Church is adjacent to the east. In addition, the property is located in the Erindale Village Cultural Heritage Landscape (EVCHL).

The proposed planning application involves the construction of a three storey rear addition, consisting of a classrooms, student lounge, cafeteria and multi-purpose room, to the Erindale Academy. The proposed addition has been designed in a sympathetic manner and is set back significantly from the Taylor-Froebel House. As such, it is anticipated that the proposed addition will have no impact on the Taylor-Froebel House and the adjacent Erindale Presbyterian Church. In addition, the addition will have no impact on the attributes of the EVCHL.

The report makes the following recommendation:

1. That improvements should be made to the streetscape along Dundas Street West specifically focusing on improving the visibility of the Taylor-Froebel House from the street.

PROJECT PERSONNEL

<i>Senior Project Manager:</i>	Annie Veilleux, MA, CAHP Senior Heritage Specialist Manager, Cultural Heritage Division
<i>Project Manager:</i>	James Neilson, MES Cultural Heritage Specialist
<i>Project Administrator</i>	Carol Bella, Hon. BA Executive Assistant - Operations Division
<i>Historical Research:</i>	James Neilson
<i>Field Review</i>	James Neilson
<i>Report Preparation:</i>	James Neilson
<i>Graphics Preparation:</i>	Blake Williams, MLitt Staff Archaeologist and Geomatics Specialist
<i>Report Reviewer:</i>	Annie Veilleux

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1.0 INTRODUCTION

ASI was contracted by Weiss Architecture and Urbanism Ltd. to prepare a Heritage Impact Assessment (HIA) for a proposed addition to the building at 1576 Dundas Street West, located on Plan TOR 7 ER 1 PT Lot 2 & 3 in the City of Mississauga, Ontario. This HIA is part of the proposed undertaking to construct a three storey rear addition to the Erindale Academy.

The subject property at 1576 Dundas Street West is located on the south side of Dundas Street West, midway between Jarvis Street and Robinson Street. The property also contains the Taylor-Froebel House (1584 Dundas Street West), a listed building on the City of Mississauga's Heritage Inventory. In addition, the subject property is adjacent to the Erindale Presbyterian Church (1560 Dundas Street West), a Victorian Gothic church constructed in 1877, which was designated in 1985 (By-law #247-85). The property is also located within the Erindale Village Cultural Heritage Landscape (EVCHL).

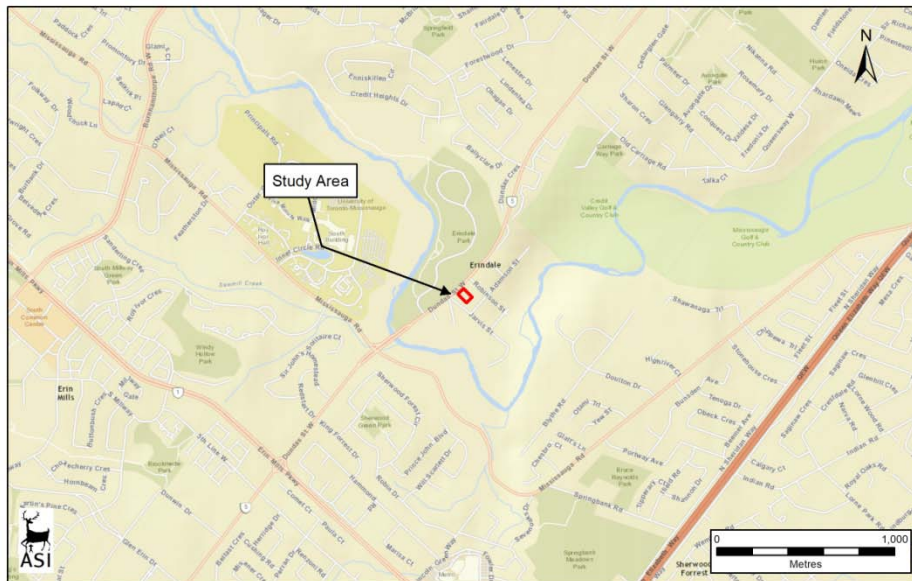


Figure 1: Location map of 1576 Dundas Street West (Base Map: Open Street Maps)

The research, analysis and site visit was conducted by James Neilson under the project direction of Annie Veilleux, Manager of the Cultural Heritage Division, ASI. The present heritage impact assessment follows the Ministry of Tourism, Culture and Sports' *Ontario Heritage Toolkit* (2006), the City of Mississauga Cultural Heritage Impact Assessment Terms of Reference (2014) and the *Standards and Guidelines for the Conservation of Historic Places in Canada* (2010). Research was completed to investigate, document and evaluate the cultural heritage resources within and adjacent to the study area.

This document will provide:

- a description of the cultural heritage resources, including location, a detailed land use history of the site and photographic documentation;
- a description of the site's cultural heritage value based on archival research, site analysis, and municipally accepted criteria for establishing cultural heritage significance; and
- an assessment of impacts of the proposed undertaking.

1.1 Location and Study Area Description

The subject property consists of a two-storey school known as the Erindale Academy (1576 Dundas Street West) built in 1990 and a two-storey Second Empire building known as the Taylor-Froebel House (1584 Dundas Street West) built in 1877 (Figure 2 and Figure 3). Both buildings are located on the same property located on the south side of Dundas Street West, midway between Jarvis Street and Robinson Street. The property is accessed from Dundas Street West.



Figure 2: Aerial photo of the Subject Property



Figure 3: 1576 Dundas Street West (left) and 1584 Dundas Street West (right)

The surrounding area consists of sparsely populated commercial route along Dundas Street West. To the south is Erindale Village, a neighbourhood of single detached residential buildings. The property is located within the Erindale Village Cultural Heritage Landscape, which is described as a “small residential enclave...defined by mature trees and a common scale of structures” (City of Mississauga 2005).

1.3 Policy Framework

The authority to request this heritage assessment arises from the *Ontario Heritage Act*, Section 2(d) of the *Planning Act*, the *Provincial Policy Statement* (2014), and the City of Mississauga’s *Official Plan* (Policies 22-25).

The *Ontario Heritage Act* (OHA) enables designation of properties and districts under Part IV and Part V, Sections 26 through 46 and also provides the legislative basis for applying heritage easements to real property.

The *Planning Act* (1990) and related *Provincial Policy Statement* (PPS 2014) make a number of provisions relating to heritage conservation. One of the general purposes of the *Planning Act* is to integrate matters of provincial interest in provincial and municipal planning decisions. In order to inform all those involved in planning activities of the scope of these matters of provincial interest, Section 2 of the *Planning Act* provides an extensive listing. These matters of provincial interest shall be regarded when certain authorities, including the council of a municipality, carry out their responsibilities under the *Act*. One of these provincial interests is directly concerned with:

- 2 (i) the conservation of features of significant architectural, cultural, historical, archaeological or scientific interest.

The *PPS* indicates in Section 4 - Implementation/Interpretation, that:

- 4.7 The official plan is the most important vehicle for implementation of this Provincial Policy Statement. Comprehensive, integrated and long-term planning is best achieved through official plans.

Official plans shall identify provincial interests and set out appropriate land use designations and policies. To determine the significance of some natural heritage features and other resources, evaluation may be required.

Official plans should also coordinate cross-boundary matters to complement the actions of other planning authorities and promote mutually beneficial solutions. Official plans shall provide clear, reasonable and attainable policies to protect provincial interests and direct development to suitable areas.

In order to protect provincial interests, planning authorities shall keep their official plans up-to-date with this Provincial Policy Statement. The policies of this Provincial Policy Statement continue to apply after adoption and approval of an official plan.



Those policies of particular relevance for the conservation of heritage features are contained in Section 2, *Wise Use and Management of Resources*, in which the preamble states that “Ontario’s long-term prosperity, environmental health, and social well-being depend on protecting natural heritage, water, agricultural, mineral and cultural heritage and archaeological resources for their economic, environmental and social benefits.”

Accordingly, in subsection 2.6, *Cultural Heritage and Archaeology* makes the following relative provisions:

- 2.6.1 Significant built heritage resources and significant cultural heritage landscapes shall be conserved.
- 2.6.3 Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.

This provides the context not only for discrete planning activities detailed in the *Planning Act* but also for the foundation of policy statements issued under Section 3 of the *Planning Act*.

The following policies, outlined in the City of Mississauga’s *Official Plan* (dated March 13, 2017), direct the undertaking of Heritage Impact Assessment within the City:

- 7.4.1.12 The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a Heritage Impact Statement, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction.

Furthermore, the City of Mississauga’s *Official Plan* provides policy direction for development on or adjacent to cultural heritage resources. These policies include:

- 7.4.1.2 Mississauga will discourage the demolition, destruction or inappropriate alteration or reuse of cultural heritage resources
- 7.4.1.3 Mississauga will require development to maintain location and settings for cultural heritage resources that are compatible with and enhance the character of the cultural heritage resource.
- 7.4.1.11 Cultural heritage resources designated under the Ontario Heritage Act, will be required to preserve the heritage attributes and not detract or destroy any of the heritage attributes in keeping with the Ontario Heritage Tool Kit, the Ontario Ministry of Culture, and the Standards and Guidelines for the Conservation of Historic Places in Canada, Parks Canada.
- 7.4.2.3 Development adjacent to a cultural heritage property will be encouraged to be compatible with the cultural heritage property.



- 9.5.1.15 Development in proximity to landmark buildings or sites, to the Natural Areas System or cultural heritage resources, should be designed to:
- a. respect the prominence, character, setting and connectivity of these buildings, sites and resources; and
 - b. ensure an effective transition in built form through appropriate height, massing, character, architectural design, siting, setbacks, parking, amenity and open spaces.

The Subject Property is located within Erindale, a neighbourhood that is included within the *Official Plan*. The property is part of Special Site 1 and the following heritage-related policies have been outlined:

- 16.9.2.1.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:
- b) any additions or alterations of existing buildings will be sensitive to the village theme of the area, and will be largely confined to the rear of the property;
 - g) buildings should have a minimum of two storeys and a maximum of three storeys in height.

The property is currently listed on the City of Mississauga's Heritage Register as part of the Erindale Village Cultural Heritage Landscape (EVCHL). The EVCHL is recognized for containing the following cultural heritage attributes:

- Landscape Environment
 - Scenic and Visual Quality
 - Horticultural Interest
- Historical Association
 - Illustrates Important Phase in Mississauga's Social or Physical Development
- Built Environment
 - Consistent Scale of Built Features
 - Designated Structures
- Other
 - Historical or Archaeological Interest
- Site Description

This small residential enclave has a wonderful visual appearance and special landscape character defined by mature trees and a common scale of structures. Most prominent are the rows of Norway spruce, remnants of the former agricultural fields, which predate the housing development. The preservation of these trees through the sensitive siting of housing and roads has created a unique and wonderful residential environment similar to other neighbourhoods straddling the Credit River Valley. The street pattern and scattered heritage properties are the remnants of this nineteenth century village.

1.4 Project Consultation

The following organizations, websites, online heritage documents, online heritage mapping tools were reviewed to confirm the level of significance of the subject property, the location of additional previously identified cultural heritage resources adjacent to the study area, and to request additional information generally:



- City of Mississauga Heritage Property Search Interactive Map [Accessed 21 August, 2017] at <https://www.mississauga.ca/portal/services/property?DPSLogout=true>
- City of Mississauga Cultural Heritage Landscapes Inventory (January 2005);
- Canadian Register of Historic Places [Accessed 21 August, 2017] at <http://www.historicplaces.ca/en/pages/about-apropos.aspx>;
- Parks Canada website (national historic sites) [Accessed 21 August, 2017] at <http://www.pc.gc.ca/eng/progs/lhn-nhs/index.aspx>;
- Ontario Heritage Trust *Ontario Heritage Plaque Guide*, an online, searchable database of Ontario Heritage Plaques [Accessed 21 August, 2017] at <http://www.heritagetrust.on.ca/Resources-and-Learning/Online-Plaque-Guide.aspx>;
- Federal Heritage Buildings Review Office (FHBRO) [these properties are recognized under the Treasury Board Policy on the Management of Real Property (TBPMRP)];
- Toronto Reference Library;
- Region of Peel Land Registry Office; and
- Historical and genealogical records at Ancestry.com

2.0 HISTORICAL RESEARCH

A review of available primary and secondary source material was undertaken to produce a contextual overview of the study area, including a general description of Euro-Canadian settlement and land-use, and the development of transportation infrastructure. Land use records were obtained from the Peel Region Land Registry Office, but did not provide early ownership records of the property. As such, the historical background of the property is based primarily on secondary sources. The following section provides the results of this research.

The subject property is located in TOR 7 ER 1 PT Lot 2 & 3 in the historical Village of Erindale and Toronto Township, and modern-day City of Mississauga, Ontario. The property consists of two buildings: a two-storey school known as Erindale Academy and the two-storey Second Empire building known as the Taylor-Froebel House. The property is bounded by Dundas Street West to the north, Erindale Presbyterian Church to the east, a residential building to the west and residential buildings to the south. Dundas Street West is a historical thoroughfare and the main street of the historical Village of Erindale.

2.1 Township and Settlement History

Village of Erindale

The Village of Erindale was established in 1822 after Thomas Racey constructed a sawmill on the Credit River, just south of Dundas Street. By 1824, a village site was laid out, first called Toronto, and then Credit, Springfield, Springfield-on-the-Credit, and finally Erindale in the early 1900s (Heritage Mississauga 2009). The village was a stopping place for stagecoach travelers between Dundas and York (now Hamilton and Toronto), along Dundas Street (Figure 4 to Figure 6). Early settlers included Emerson Taylor, who operated the Royal Exchange Hotel; John McGill, the first flour miller; Dr. Beaumont Dixie, an early physician, Duncan Turpel, a blacksmith, notary, and stagecoach operator; John Barker, the postmaster and storekeeper; and Edwin Turner and Christopher Boyes, who were prominent merchants; and General Peter Adamson, who held early Anglican church services in his home until St. Peter's Anglican Church was built in 1826. This was the only Anglican Church west of Toronto, later rebuilt in



1887, and still stands today. The village saw a period of decline when it was bypassed by the Great Western Railway, despite the Credit Valley Railway station being built in 1879. In the early 1900s Erindale was the centre of a large hydroelectric project which brought growth in the village until a devastating fire in 1919 (Figure 7). Erindale amalgamated with other villages in Toronto Township in 1968 to form the Town of Mississauga. The town became the City of Mississauga in 1974 (Heritage Mississauga 2009).



Figure 4: Erindale Village c.1885 (Mississauga Library HA0047)



Figure 5: Erindale Village c.1915 (Heritage Mississauga)



Figure 6: Erindale Village c.1915 (Heritage Mississauga)



Figure 7: Erindale Village fire of 1919 (Heritage Mississauga)

Toronto Township

The Township of Toronto was originally surveyed in 1806 by Mr. Wilmot, Deputy Surveyor. The first settler in this Township, and also the County of Peel, was Colonel Thomas Ingersoll. The whole population of the Township in 1808 consisted of seven families scattered along Dundas Street. The number of inhabitants gradually increased until the war erupted in 1812, which gave considerable check to its progress. When the war was over, the Township's growth revived and the rear part of the Township was surveyed and called the "New Survey." The greater part of the New Survey was granted to a colony of Irish settlers from New York City who suffered persecution during the war.

The Credit River runs through the western portion of the Township, and proved to be a great source of wealth to its inhabitants, as it was not only a good watering stream, but there were endless mill privileges along the entire length of the river.

In 1855, the Hamilton and Toronto Railway completed its lakeshore line. In 1871, the railway was amalgamated with the Great Western Railway, which in turn, was amalgamated in 1882, with the Grand Trunk Railway, and then in 1923, with Canadian National Railway (Andreae 1997:126-127). Several villages of varying sizes had developed by the end of the nineteenth century, including Streetsville, Meadowvale, Churchville, Malton and Erindale. A number of crossroad communities also began to grow by the end of the nineteenth century. These included Britannia, Derry, Frasers Corners, Palestine, Mt Charles, and Grahamsville.

2.2 Land Use History: 1576 Dundas Street West

The subject property at 1576 Dundas Street West is TOR 7 ER 1 PT Lot 2 & 3 in the historical Village of Erindale and Toronto Township, and modern-day City of Mississauga, Ontario. Following the 1805 Mississauga Purchase, the lot was originally within the Credit Indian Reserve (Figure 11). The land containing the subject property was surrendered in 1819-20 as part of the Second Purchase and was considered particularly important as it would permit Dundas Street to continue westward. The lot was given to Captain Thomas Racey to establish a village and mill, with the land known as the Racey Tract. According to Hicks (2009:XIV), Racey lost his property in 1827 when he fell behind in his payments (Hicks 2009). Attorney General John Beverley Robinson was given the task of granting and selling the acreage.

The 1830 *Plan of the Town of Toronto* shows the lots and roads of the village (Figure 12). No ownership information is provided for Lots 2 and 3. On the 1860 Tremaine Map, the village (now known as Springfield) is well developed with many buildings along Dundas Street (Figure 13). While the south side of Dundas Street is largely developed, Lot 2 does not contain a building. Emerson Taylor, the builder of the Taylor-Froebel House is noted as owning Lots 1-4 in Range Two, just south of the subject property.

By 1877, the *Illustrated Atlas of Peel* shows the village (now known as Credit) though does not depict any buildings (Figure 14). Emerson Taylor's stature in the village warranted inclusion within the 1877 *Illustrated Atlas*' biographical notices. Taylor was an immigrant from Pennsylvania who came to the village in 1837 (Walker and Miles 1877). For nearly thirty years Taylor owned the Royal Exchange Hotel, which is described as one of the best hotels in Peel. He also served as postmaster and as a Magistrate (Walker and Miles 1877), a member of Town Council for five years (The Globe 1893), and donated land for the Erindale Presbyterian Church (Adamson 1978). This property was adjacent to the subject property where he built the Taylor-Froebel House between 1875 and 1878. The basis for this date is unconfirmed, though Hicks recounts a story told in the Port Credit Weekly in 1955 by Emerson Taylor's grandson Taylor Statton. Statton writes, "I was born in the hotel, but when I was two years old he built a house on the lane opposite to the hotel and he and grandmother and my parents moved into it" (Hicks 2009:38). Research into Mr. Statton's life reveals that he was born in 1882 and he, his family, and his grandparents (including Emerson Taylor) are living together when the 1891 census was compiled (1891 Census, Toronto Peel Roll:T-6361, Family No. 117).

The 1909 Topographic Map depicts the Taylor-Froebel House as a brick blacksmith shop at the centre of Erindale Village (Figure 15). The 1901 Census notes that Lewis Pope was the only blacksmith in the community at this time (1901 Census, Toronto Peel Page 10, Family No. 107). Across the street is a hotel



and post office, and the Presbyterian Church is noted to the east. Along with the Presbyterian Church, the property is one of the few brick buildings in the area. A low quality photographic image of the house represents the sole image from the early decades of the house. Due to the condition, it is difficult to determine specific structural features, though the Second-Empire style and dormer windows are clearly evident (Figure 8). In 1938, the building remains described as a blacksmith shop, though by 1942, the property is no longer noted for such purposes (Figure 16 and Figure 17).



Figure 8: Taylor-Froebel House c.1919 (Heritage Mississauga)

The 1954 and 1966 aerial photos show the Taylor-Froebel House and the original building at 1576 Dundas Street West (Figure 18 and Figure 19). The original buildings on site are clearly visible in a photo from c.1967-1970, which shows a one storey structure with a gable roof adjacent to the Taylor-Froebel House (Figure 9). The Taylor-Froebel House appears the same as it does today, though a rear porch is evident where the sunroom exists today, which suggests that the sunroom is likely an enclosed porch. An additional photo from 1978 reveals that the one-storey structure is a wooden garage located on the existing parking lot (Figure 10). This garage likely serviced the Taylor-Froebel House as a staircase lead from the garage to the house. The appearance of the Taylor-Froebel House in 1978 is identical to its contemporary appearance. The 1989 and 1992 aerials show the changes to 1576 Dundas Street West as the original garage was torn down for the construction of the existing Erindale Academy school, which was constructed in 1990 (Figure 20 and Figure 21).



Figure 9: Aerial photo of the Taylor-Froebel House c.1967-1970. (UTM Library Archives Sec. 2-1, Box 274 File 0087-10)



Figure 10: Taylor-Froebel House c.1978 (Mississauga Library B529)

Historical Mapping



Figure 11: Patent Map. Subject Property in Red (Ontario Archives 1851)

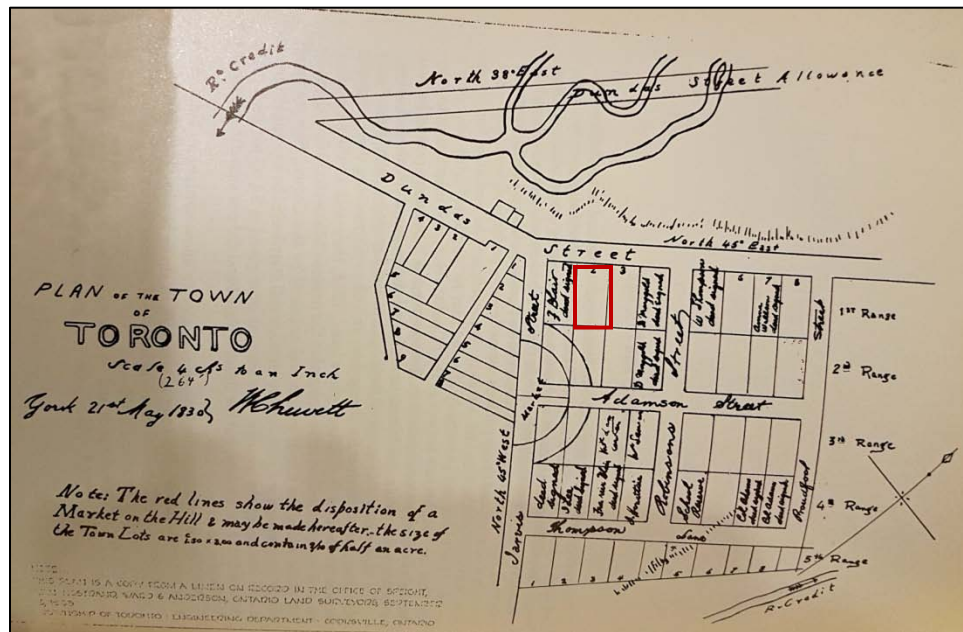


Figure 12: 1830 Plan of the Town of Toronto. Subject Property in Red (Heritage Mississauga)



Figure 13: 1860 Tremaine Map. Subject property in Red (Tremaine 1860)



Figure 14: 1877 Illustrated Atlas map. Subject Property in red (Walker and Miles 1877)

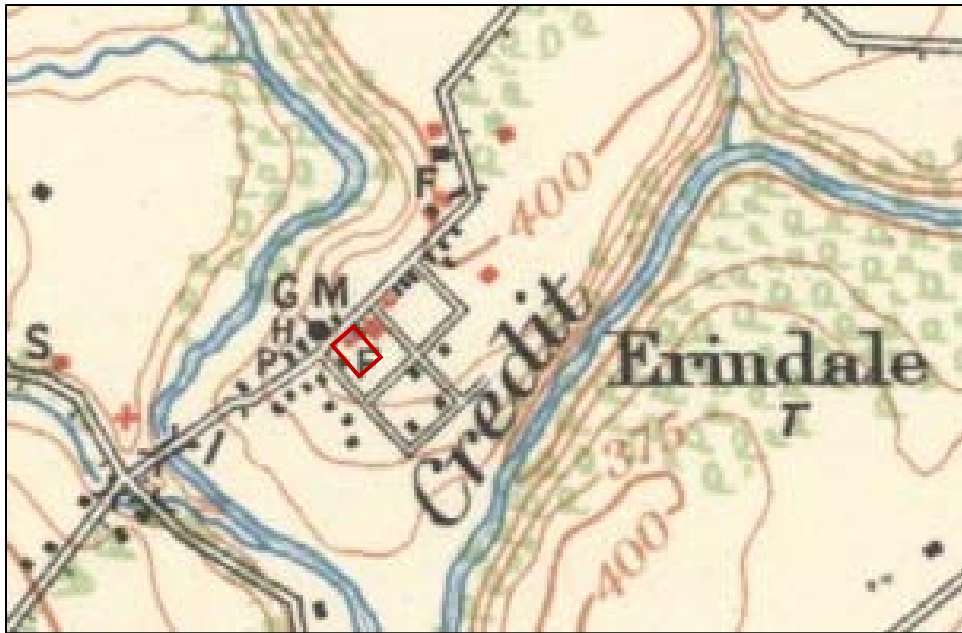


Figure 15: 1909 Topographic Map. Subject Property in red (Department of Militia and Defence 1909)



Figure 16: 1938 Topographic Map. Subject Property in Red (Department of National Defence 1938)



Figure 17: 1942 Topographic Map. Subject Property in Red (Department of National Defence 1942)

Historical Aerial Photos



Figure 18: 1954 Aerial (City of Mississauga)



Figure 19: 1966 Aerial (City of Mississauga)



Figure 20: 1989 Aerial (City of Mississauga)



Figure 21: 1992 Aerial (City of Mississauga)

3.0 EXISTING CONDITIONS

3.1 Introduction

A field review was conducted by James Neilson of ASI on August 23, 2017 to survey and document the study area and environs.

3.2 1576 Dundas Street West

3.2.1 Exterior

The structure at 1576 Dundas Street West is a two-storey institutional brick building, inspired by Victorian architecture. The hipped asphalt shingle roof features a dentil cornice, faux central bell tower, and gables on all four sides (Figure 14 and Figure 15). The building incorporates quoining throughout (Figure 12). The building faces west with an entrance with sidelights and an awning located in the middle of three bays (Figure 8). The entrance is flanked by large flat-headed windows with brick soldier courses and pre-cast concrete sills. The second storey features large flat-headed windows with brick soldier courses and pre-cast concrete sills in each bay. A central gable with a circular vent below is located above the second storey. South and north wings each contain a single flat-headed window with brick soldier courses and pre-cast concrete sills.

The north (Figure 10) and south (Figure 9) elevations are nearly identical with two entrances with sidelights. The north facade features arched brickwork above the door (Figure 13), while the south elevations contain brick soldier courses. The second storey on both elevations contains three flat-headed windows with pre-cast concrete sills. A circular vent is located beneath the gable. The east elevation contains a series of six flat-headed windows with brick soldier courses and pre-cast concrete sills (Figure 11). The second storey features a row of ten windows of various sizes with pre-cast concrete sills.



Figure 22: West elevation



Figure 23: South elevation



Figure 24: North elevation



Figure 25: East elevation

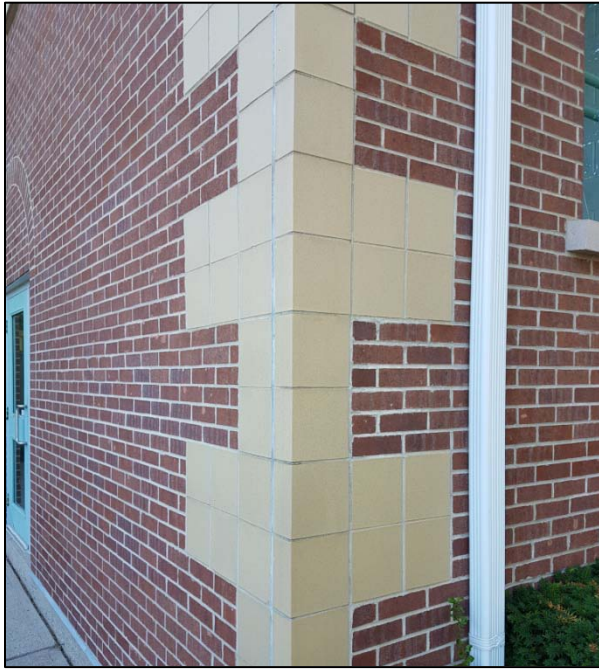


Figure 26: Quoining



Figure 27: Brick arch above the north elevation doors



Figure 28: Dentil cornice



Figure 29: Gable with circular vent

3.2.3 Landscape Features

The subject property at 1576 Dundas Street West is accessed from a driveway on Dundas Street West. The driveway leads to a parking lot, which fills the entirety of the space between 1576 Dundas Street West and the neighbouring building at 1584 Dundas Street West. The front yard consists of large hedges/bushes, two trees and a path linking Dundas Street West to the front of the building. The western boundary of the property is sparsely vegetated with six trees. A small playground is located behind the modern school building. The southern edge of the property contains a wooden gazebo and a small shed. The east side of the property contains a fenced in area with three trees, bushes along the wall of the building, and picnic tables. The east side lacks the landscaping that is present in the front yard. The perimeter of the property is marked by a wooden fence.

3.3 1584 Dundas Street West

3.3.1 Exterior

The structure at 1584 Dundas Street West is a two-storey Second Empire building with a rear addition. The building has been stuccoed and the mansard roof is covered in asphalt shingles. The building is accessed from an entry on the east elevation within one of the rear additions (Figure 16). The original entrance along Dundas Street West remains (Figure 17 and Figure 18), though there is limited space between the door and the row of vegetation between the building and the street. The first floor of the north elevation is divided into three bays with two-over-two double hung wooden windows with wooden storm windows and shutters. The centre bay contains the original entrance with “The Froebel House” on a carved wooden name plate above the door. The upper storey dormers on the second storey feature gables and pseudo four-centered wooden arches (Figure 19). The windows appear to be aluminum.



Figure 30: East elevation



Figure 31: North elevation



Figure 32: North elevation



Figure 33: Second storey dormer

The east elevation contains two distinct parts of the building: the original Second Empire building and a rear/side addition (Figure 20). The original portion of the building has a pair of double hung wooden windows with wooden storm windows and shutters. The centre bay features a large brick chimney (Figure 22). The upper storey contains a pair of dormers similar to those found on the north and west elevations.

The one-storey rear/side addition contains a set of wooden French doors with glass divided into three-by-five panes (Figure 21). Above the door is a wooden awning with shingles held up by stylized brackets (Figure 23). A second side door is located on the east elevation of the addition beneath a porch roof. The east elevation also contains a wooden double-hung window and shutters which is consistent with the windows found on the original building. The west elevation contains a single two-over-two double hung wooden window with shutters and a segmental arch (Figure 24). The upper storey contains two dormers that are identical to those on the north elevation.



Figure 34: East elevation



Figure 35: East elevation entrance



Figure 36: East elevation second storey



Figure 37: Wooden awning above the side entrance.



Figure 38: West elevation

The rear elevation contains at least two additions (Figure 25 and Figure 26). One addition consists of a sunroom with a shed roof with a set of five windows spanning the rear elevation, and a rear sliding-door entrance flanked by an additional window. The second rear addition contains a gable roof and an entrance with sidelights beneath an aluminum awning along with two vinyl windows (a double hung window and a slider window). The west elevation of the second rear addition features two windows: an aluminum slider window and a one-over-one double hung aluminum window both with small wooden awnings, wooden sills, and wooden mouldings.



Figure 39: Rear addition



Figure 40: Rear addition



Figure 41: West elevation of rear addition



Figure 42: Rear addition

3.3.2 Landscape Features

1584 Dundas Street West sits atop a ridge above Dundas Street West. The front yard is limited in space, with vegetation, grass, and a white wooden fence lining the ridge between the residence and the street. This ridge of vegetation limits views of the heritage building from Dundas Street West. Aggregate pavers have been laid around the perimeter of the house and gravel/dirt/stone has been used as fill around the foundation. Both the front door and the side door have small cement porches leading to the entrance. On the west elevation is a wooden fence separating the property from the adjacent property. On the east elevation, a white steel fence is located along the ridge providing a barrier due to the change in elevation between the residence and the adjacent parking lot. Access to the residence is provided by a pre-cast concrete staircase, which has been built in to the precast concrete retaining wall. A large parking lot is located on its east side and services the adjacent school, while likely also providing parking for the residence. The back yard features a wooden porch and interlocking bricks and a wooden fence.

4.0 PROPOSED DEVELOPMENT

4.1 Proposed Work

ASI has evaluated the plans and drawings by Weiss Architecture and Urbanism Ltd. dated September 19, 2017 (see Appendix B). The proposed development involves the construction of a three-storey rear addition to the building at 1576 Dundas Street West. The proposed addition contains a rectangular shaped floor plan running east-west across the southernmost portion of the property. The proposed addition is sited approximately twenty metres behind the existing listed building at 1584 Dundas Street West.



Figure 43: Rendering of the proposed addition (Weiss Architecture and Urbanism Ltd.)

The proposed addition is constructed of brick, which is complimentary to the existing building at 1576 Dundas Street West in both style and materials. The north elevation features two archways with recessed glass on the first floor. The design of these arches will be reflected in a new canopy for the existing school building. An additional half arch is filled in with a dogs-toothed brick pattern. The second and third storeys are divided into four bays with three windows similar in size and shape to those found on the existing building. The windows contain stone sills and lintels and a dogs-toothed brick pattern between the second and third storey. An additional set of three windows is located on the third floor above the existing building. The southern end of the hipped roof of the existing building will be extended to meet the facade of the proposed addition.

The west elevation contains a servicing door and secondary entrance. The second and third storeys each contain a set of three windows that are identical to those found on the north elevation. An additional single window is found on each floor. The east elevation is nearly identical to the west elevation apart from the first storey where three single windows are proposed and a basement entrance. Finally, the first floor arches found on the north elevation are replicated on the south elevation, with three archways. The glass curtain wall contains a single entrance that provides access to the rear of the property. The second and third storeys contain five sets of three windows that are identical to those found throughout the upper storeys of the proposed building.

With regards to landscaping, a new paved walkway will line the perimeter of the entire building and a new raised planter and bench will be installed in front of the south wing of the existing building.

To accommodate the removal of the existing parking, a new parking configuration is proposed. A new parking lot with six spaces is proposed for the front lawn, in front of the existing building at 1576 Dundas Street West. This parking lot will include a paved walkway which will lead to the sidewalk on Dundas Street West. The existing concrete stairs and retaining wall will be removed and new landscaping will be incorporated. Additional trees and landscaping enhancements will be planted in a new boulevard in front of the property.

4.2 Impact Assessment

4.2.1 Official Plan

The proposed addition to the building at 1576 Dundas Street West is consistent with the City of Mississauga's *Official Plan*. The proposed addition is restricted to the rear of the site and is three-storeys tall, which meets the requirements of the Erindale neighbourhood. Furthermore, the addition is consistent in appearance with the existing building, which has been on the site for nearly thirty years, and as such, should have no impact on cultural heritage resources in the area.

4.2.2 Impact on 1584 Dundas Street West

The proposal involves the construction of a three-storey addition to the existing building at 1576 Dundas Street West. The proposed development is approximately twenty metres south of the listed building at 1584 Dundas Street West. It is anticipated that despite the difference in height between the addition and the heritage building, the generous setback from the heritage building and the significant change in elevation from the street will prevent the addition from being visible above the roofline of the existing heritage building. The addition will be visible from Dundas Street West only directly from the north where the existing parking lot will provide an uninterrupted view of the addition from the street. Given the setback of the addition from the street and the limited range from which the addition will be visible, this view should not have any significant impacts.

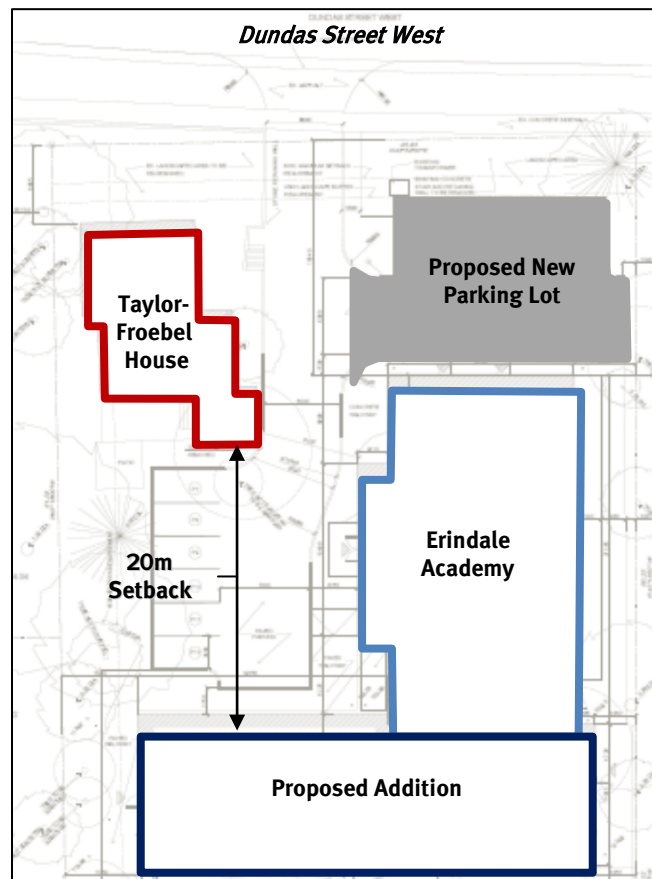


Figure 44: Plan of proposed development

4.2.3 Impact on the Adjacent Heritage Building at 1560 Dundas Street West

The adjacent Erindale Presbyterian Church is designated under Part IV of the Ontario Heritage Act. The building was constructed in 1877 and, along with the building at 1584 Dundas Street West, is one of the few remnant buildings from the historic Village of Erindale. Mitigating the impacts of proposed development on land adjacent to heritage buildings is required as part of the *Provincial Policy Statement* and the City of Mississauga *Official Plan* (see Section 1.3 of this report).

The proposed addition to 1576 Dundas Street West is located approximately 35-40 metres from the closest point of Erindale Presbyterian Church. The majority of the proposed addition will be obscured by the existing building at 1576 Dundas Street West and it is anticipated that due to the height of the church and the existing building at 1576 Dundas Street West, the proposed addition will not be visible from Dundas Street in the vicinity of the church. As such, it is anticipated that the proposed addition will have no impact on the Erindale Presbyterian Church.

4.2.4 Impact on the Erindale Village Cultural Heritage Landscape (EVCHL)

The proposed addition is located within the Erindale Village Cultural Heritage Landscape (EVCHL) and is required to adhere to the City of Mississauga's *Official Plan* policies that promote the respect for the prominence, character setting and connectivity of the cultural heritage landscape. The site description of the EVCHL describes the primarily residential nature of the cultural heritage landscape. It states:

This small residential enclave has a wonderful visual appearance and special landscape character defined by mature trees and a common scale of structures. Most prominent are the rows of Norway spruce, remnants of the former agricultural fields, which predate the housing development. The preservation of these trees through the sensitive siting of housing and roads has created a unique and wonderful residential environment similar to other neighbourhoods straddling the Credit River Valley. The street pattern and scattered heritage properties are the remnants of this nineteenth century village.

The subject property at 1576 Dundas Street West is not part of the small residential enclave described in the EVCHL's site description, which is primarily located to the south of the subject property. The property is located along Dundas Street West, where the character of the street displays the remnants of the Village of Erindale's main street. The subject property was constructed in 1990 and was designed in a manner consistent with other buildings along Dundas Street West. The proposed addition is set back significantly from the street and as such will not be visible. Furthermore, the character of Adamson Street should not be affected by the siting of the proposed addition. Overall, it is anticipated that the proposed addition will have no negative impacts on the EVCHL.

EVCHL Attribute	Contribution of Proposed Development to EVCHL	Impact of Proposed Development to EVCHL
Landscape Environment: ○ Scenic and Visual Quality ○ Horticultural Interest	○ The proposed development is set back significantly from the street and will not have any impact on the scenic and visual quality of Dundas Street West. ○ No trees of significant	• The proposed development will have limited visibility from the street and as such, will have no impact on the scenic or visual quality of the EVCHL. The proposed parking lot in front of



	importance, such as Norway Spruces will be removed as part of the proposed development.	the existing Erindale Academy is not atypical for properties on Dundas Street West, and will be obscured by vegetation and landscaping enhancements.
Built Environment <ul style="list-style-type: none"> Consistent Scale of Built Features Designated Structures 	<ul style="list-style-type: none"> The scale of the proposed addition is consistent with the existing building on the property. 	<ul style="list-style-type: none"> The scale of the proposed development will have no impact on the EVCHL or the adjacent Taylor-Froebel House.
Historical Association <ul style="list-style-type: none"> Illustrates Important Phase in Mississauga's Social or Physical Development 	<ul style="list-style-type: none"> The proposed development has no impact on the EVCHL's contribution to Mississauga's social or physical development. 	<ul style="list-style-type: none"> No impact on the historical associations of the EVCHL.
Other <ul style="list-style-type: none"> Historical or Archaeological Interest 	<ul style="list-style-type: none"> N/A 	<ul style="list-style-type: none"> N/A

5.0 CONCLUSION

The proposed addition to the subject property has been designed and sited in a sensitive manner that will have no impact on heritage resources in the area including the Taylor-Froebel House at 1584 Dundas Street West, the Erindale Presbyterian Church at 1560 Dundas Street West, and the Erindale Village Cultural Heritage Landscape.

5.1 Recommendations

- That improvements should be made to the streetscape along Dundas Street West specifically focusing on improving the visibility of the Taylor-Froebel House from the street.

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2005 *Provincial Policy Statement*
- Ministry of Tourism and Culture
2006 *Ontario Heritage Toolkit*
- Smart, Susan



2011 *A Better Place: Death and Burial in Nineteenth-Century Ontario*. Toronto: Dundurn Press.

Tremaine, George C.

1860 *Tremaine's Map of Ontario County, Upper Canada*. Toronto, Ontario.

Walker & Miles

1877 *Illustrated Historical Atlas of the County of Peel, Ont.* Toronto, Ontario,

APPENDIX A: 1584 Dundas Street West – Listing Information

PROPERTY HERITAGE DETAIL

[View Another Property](#)

Property Heritage Detail

Address: 1584 DUNDAS ST
Type: RESIDENTIAL

Area: ERINDALE
Reason: ARCHITECTURAL/CULTURAL
LANDSCAPE

Style: VERNACULAR - SECOND EMPIRE

 [Print Friendly Page](#)

Images



History

Formerly a blacksmith shop, the structure features horizontal shiplap siding and red brick originally, now stuccoed over. The structure is used today as a private school, or for offices related to the private school. The structure has a mansard roof with projecting dormers. ERINDALE VILLAGE RESIDENTIAL NEIGHBOURHOOD: This small residential enclave has a wonderful visual appearance and special landscape character defined by mature trees and a common scale of structures. Most prominent are the rows of Norway Spruce, remnants of the former agricultural fields, which predate the housing development. The preservation of these trees through the sensitive siting of housing and roads has created a unique and wonderful residential environment similar to other neighbourhoods straddling the Credit River Valley. The street pattern and scattered heritage properties are the remnants of this nineteenth century village.

APPENDIX B: Proposed Development





THE ERINDALE ACADEMY SCHOOL ADDITION

1584 DUNDAS STREET WEST, MISSISSAUGA

DRAWING LIST

A000	COVER SHEET
A101	SITE PLAN
A102	SITE SURVEY
A201	BASEMENT PLAN
A202	FIRST FLOOR PLAN
A203	SECOND FLOOR PLAN
A204	THIRD FLOOR PLAN
A205	ROOF PLAN
A206	EXISTING BUILDING 2 PLANS
A301	BUILDING ELEVATIONS - NORTH AND WEST
A302	BUILDING ELEVATIONS - SOUTH AND EAST
A303	STREET ELEVATION

General Notes

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2	HIA (Heritage Impact Assessment)	17/10/12
1	COORDINATION	17/09/26
No.	Revision	Date

Weiss

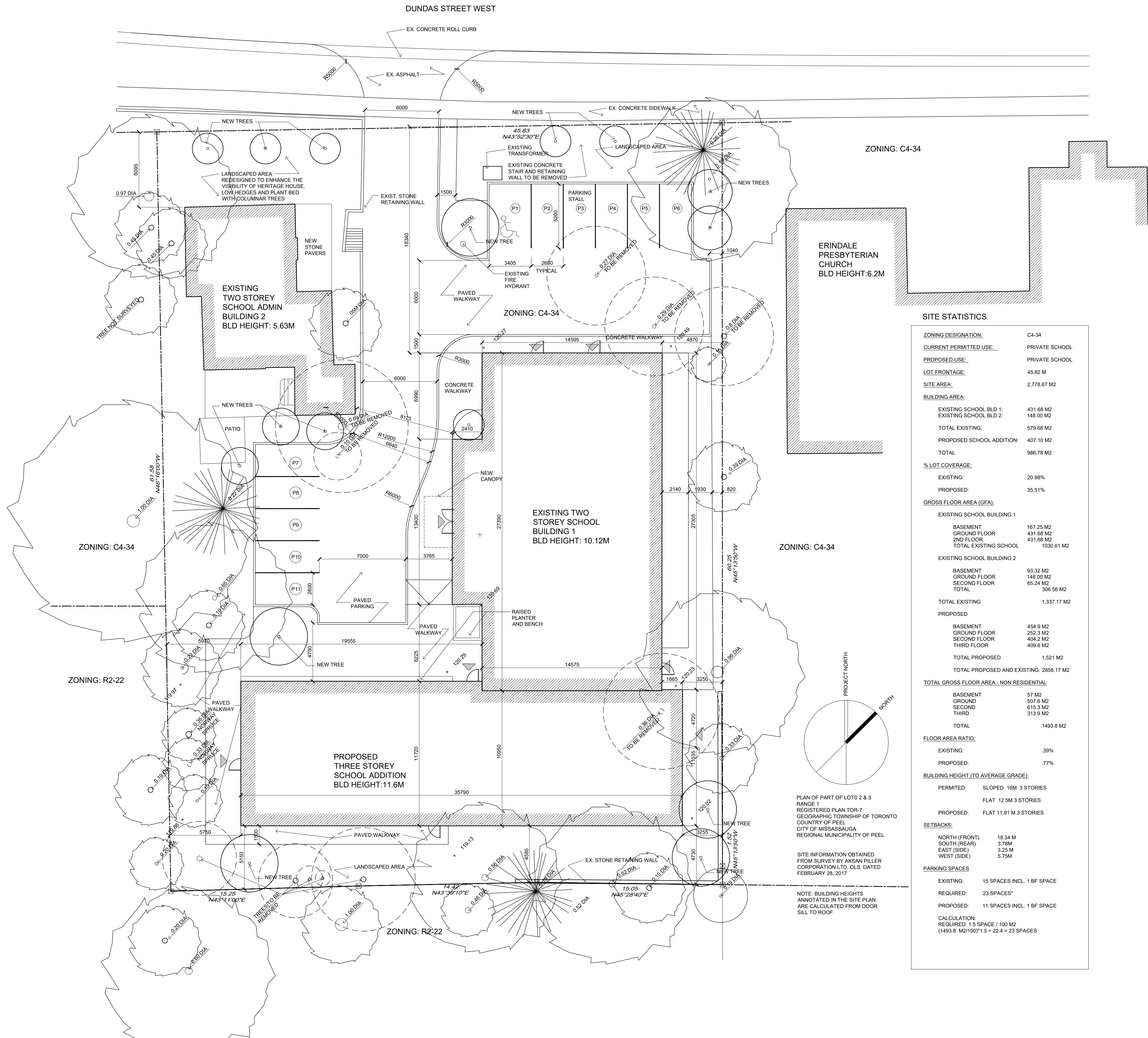
Architecture & Urbanism Limited

95 Pelham Avenue
Toronto Ontario
M6N 1A5
T 647 494 9829
F 647 494 9816

The Erindale Academy
School Addition
1584 Dundas Street West
Mississauga, Ontario

COVER SHEET

Project No. 1711	Sheet No. A000
Date:	
Scale:	



General Notes

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2	HIA (Heritage Impact Assessment)	17/10/12
1	COORDINATION	17/09/26
No.	Revision	Date

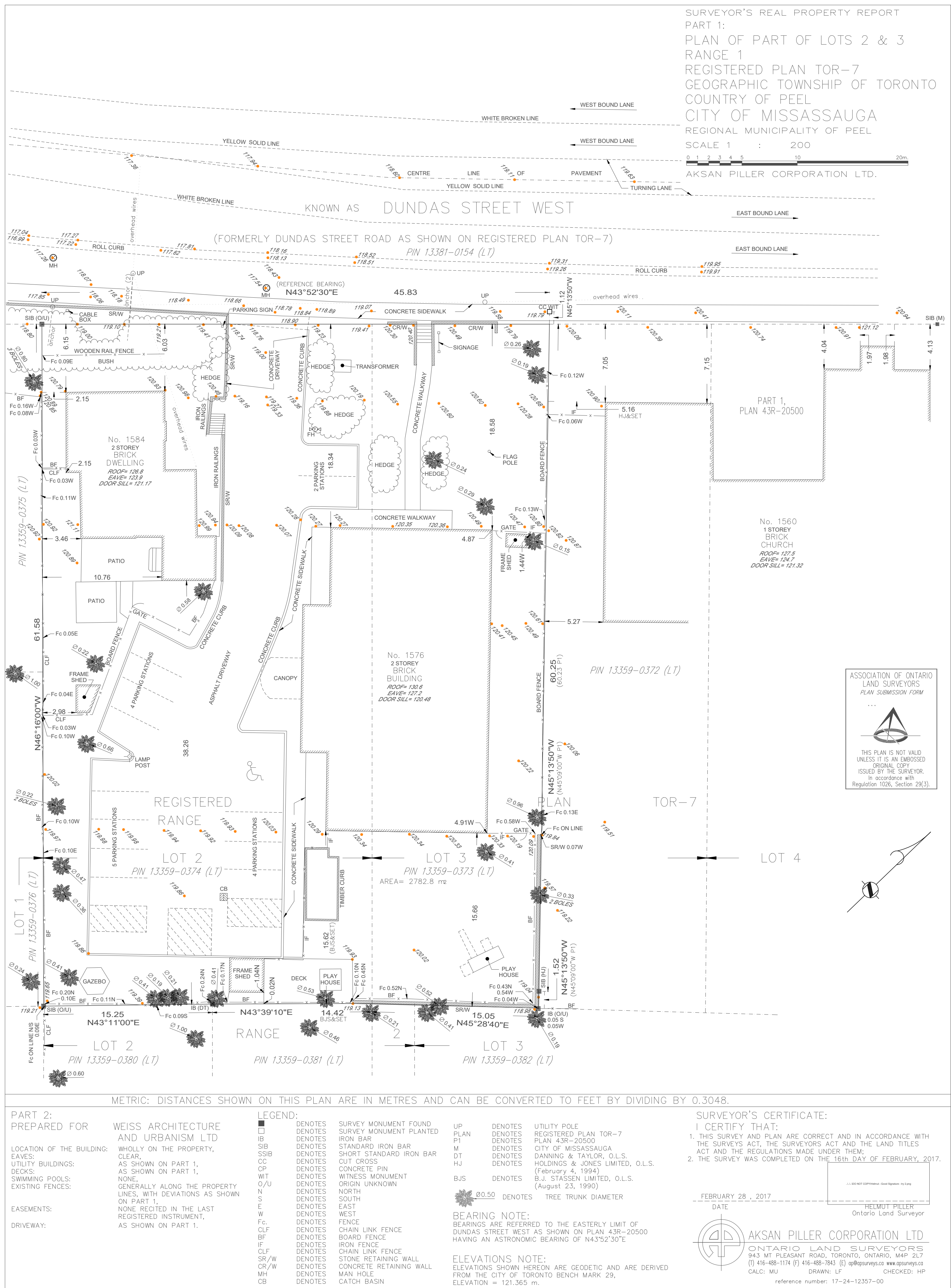
Weiss
Architecture & Urbanism Limited

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F 647 494 9816

The Erindale Academy
School Addition
1584 Dundas Street West
Mississauga, Ontario

Schematic Design
Site Plan

Project No. 1711	Sheet No.
Date:	A101
Scale: 1:150	



General Notes

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1	COORDINATION	17/09/26
No.	Revision	Date

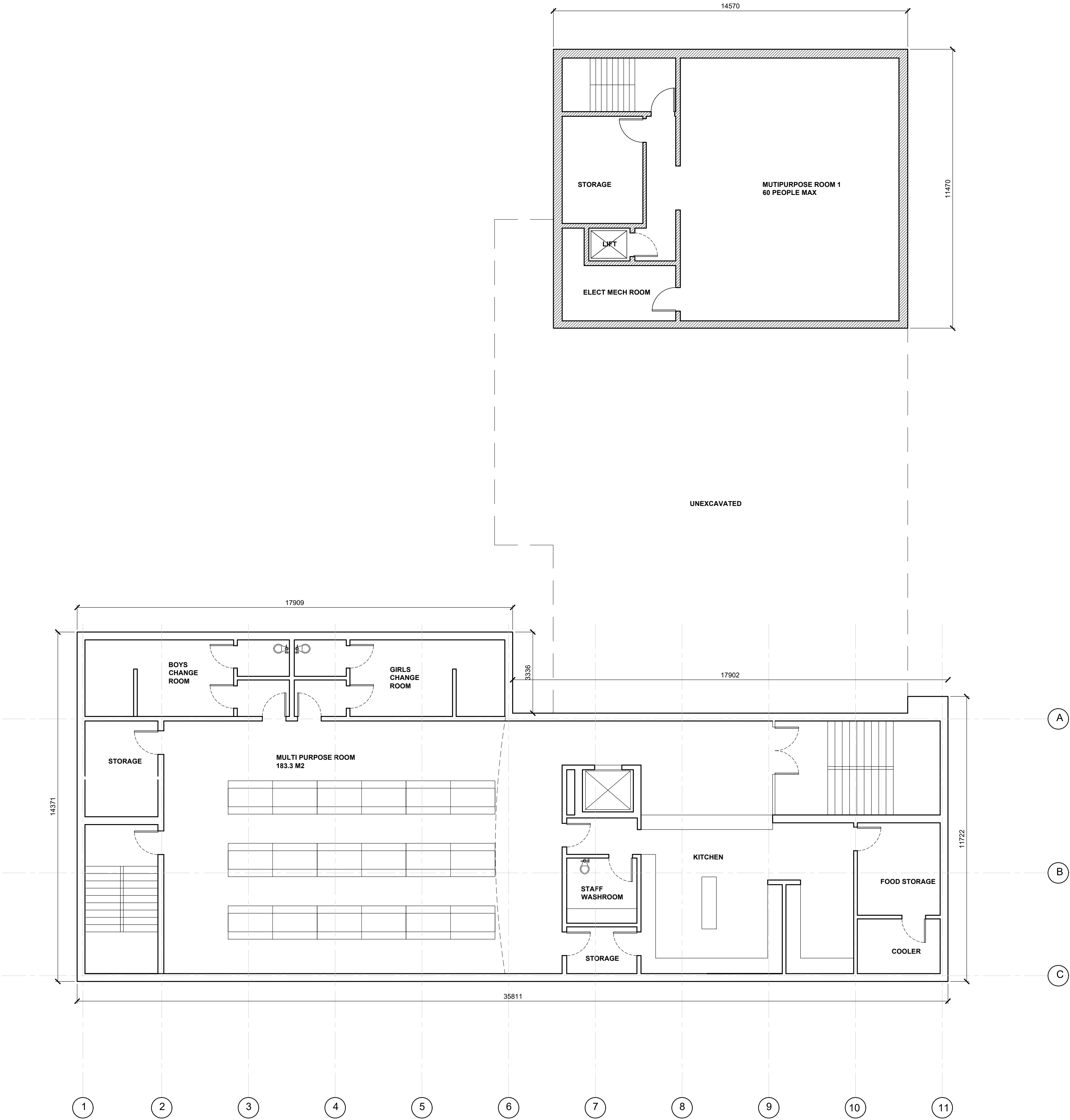
Weiss
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The Erindale Academy
School Addition
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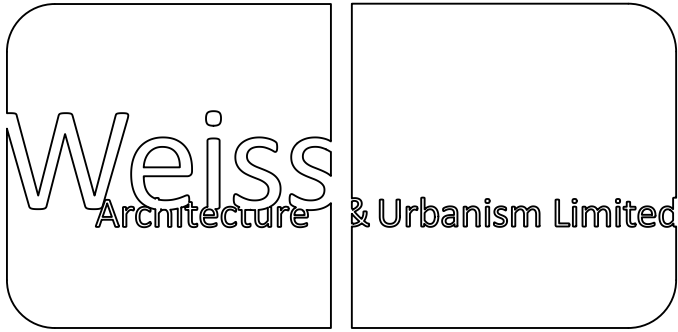
SITE SURVEY

Project No. 1711	Sheet No.
Date:	A102
Scale: 1:150	



General Notes

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No.	Revision	Date

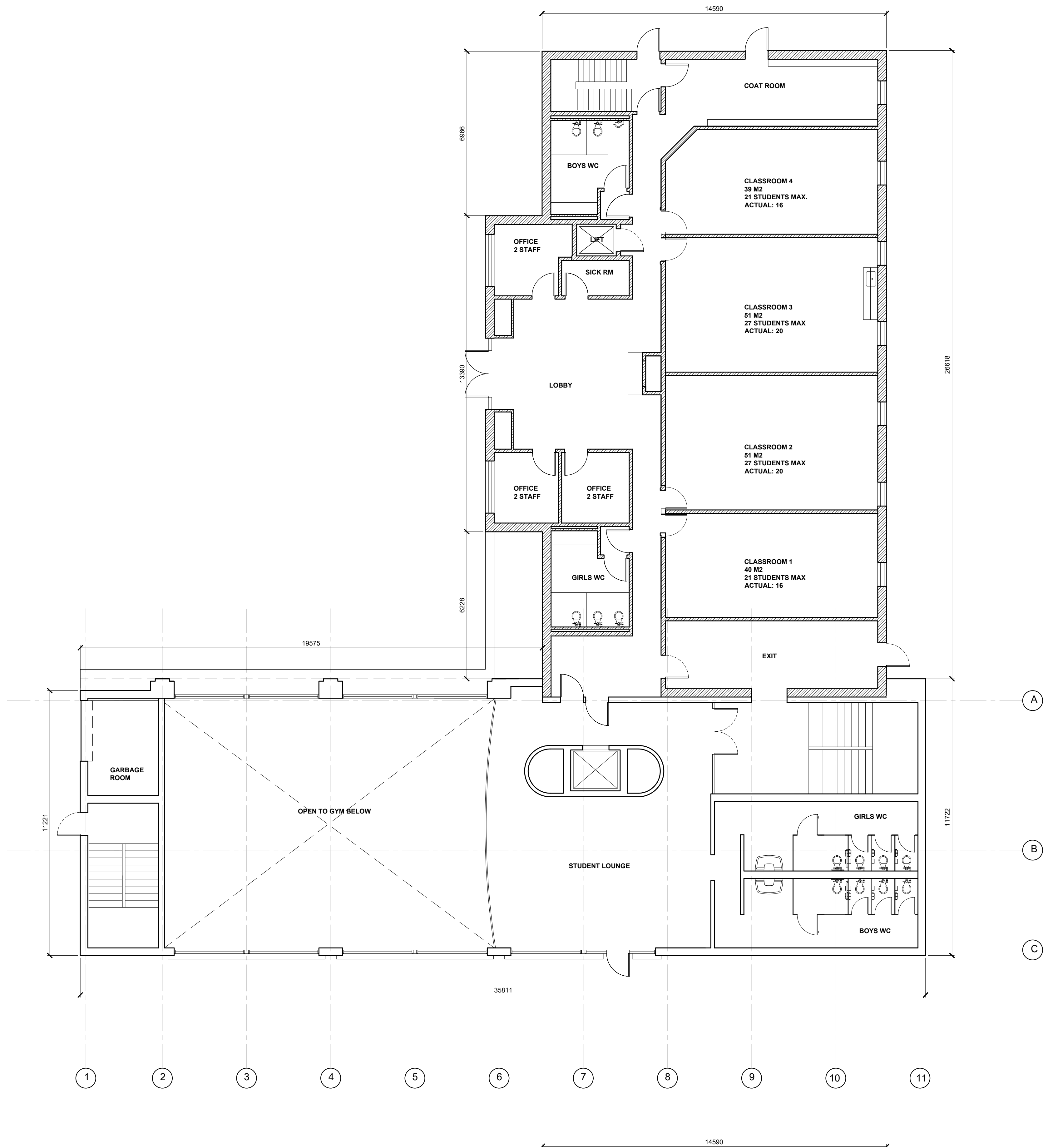


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The Erindale Academy
School Addition
1584 Dundas Street West
Mississauga, Ontario

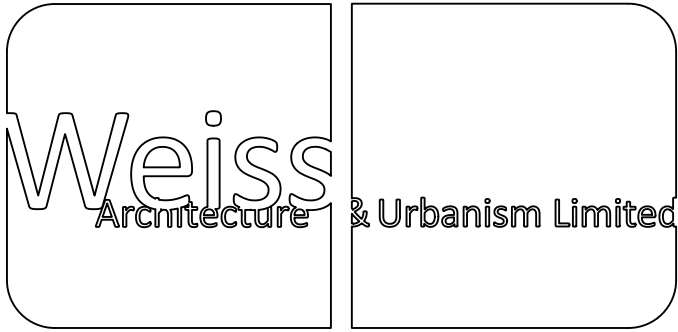
Schematic Design
Basement Floor Plan

Project No. 1711	Sheet No. A201
Date:	
Scale: 1:100	



General Notes

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2	HIA (Heritage Impact Assessment)	17/10/12
1	COORDINATION	17/09/26
No.	Revision	Date



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School Addition
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Mississauga, Ontario

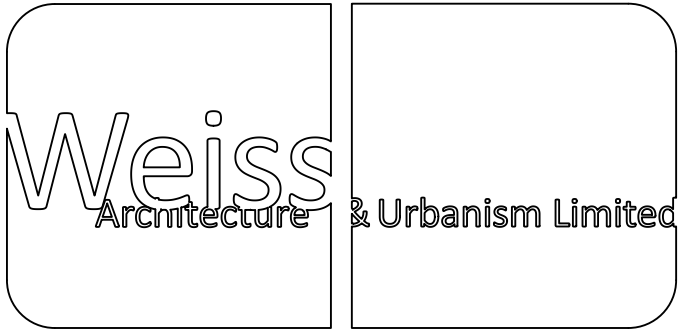
Schematic Design
Ground Floor Plan

Project No. 1711	Sheet No. A202
Date:	
Scale: 1:100	



General Notes

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1	COORDINATION	17/09/26
No.	Revision	Date

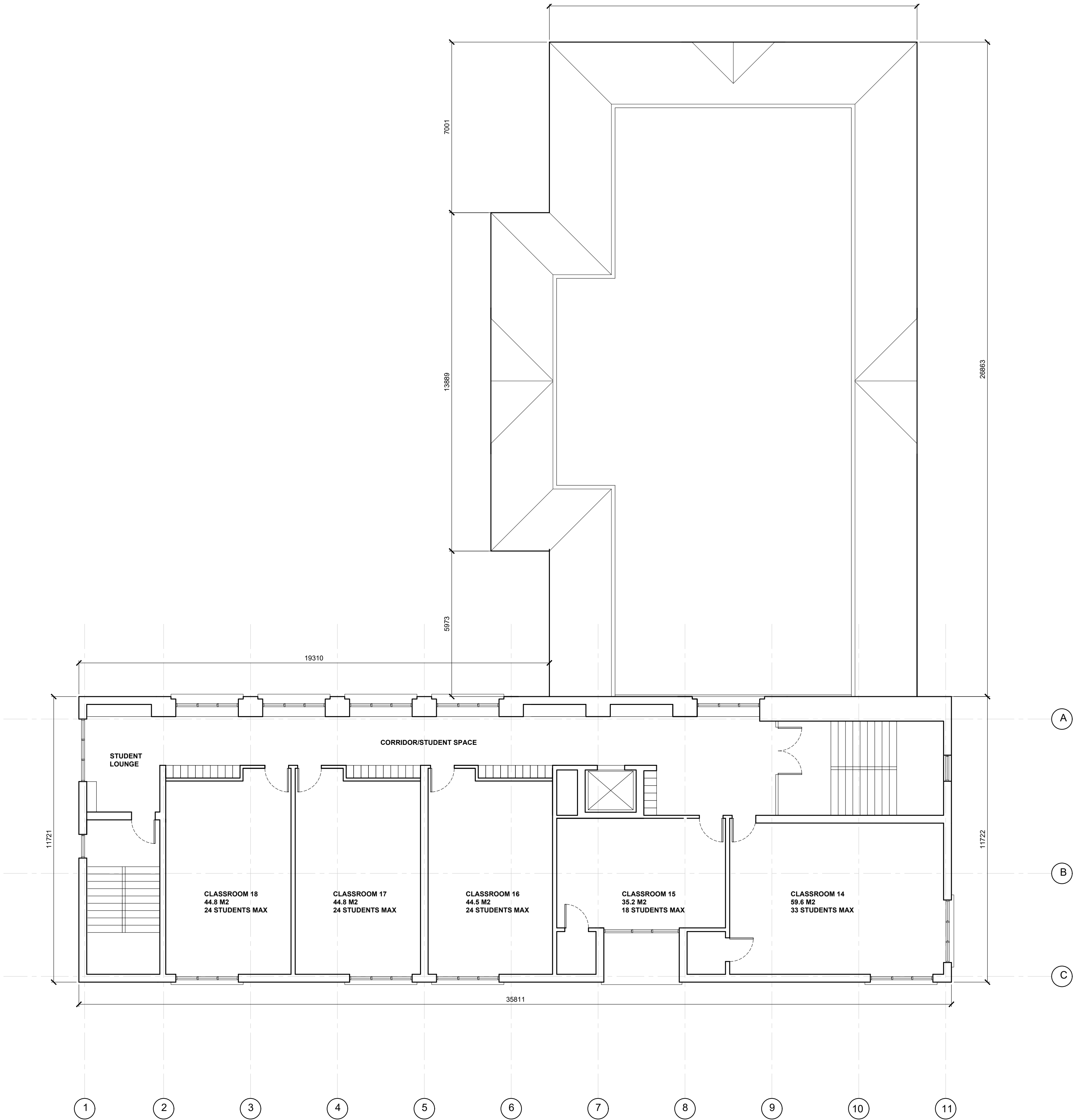


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M6N 1A5
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The Erindale Academy
School Addition
1584 Dundas Street West
Mississauga, Ontario

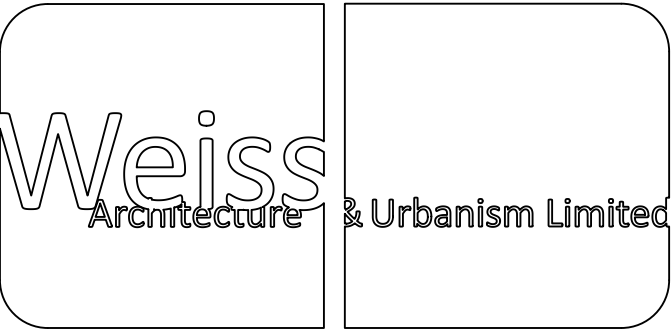
Schematic Design
Second Floor Plan

Project No. 1711	Sheet No. A203
Date:	
Scale: 1:100	



General Notes

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1	COORDINATION	17/09/26
No.	Revision	Date

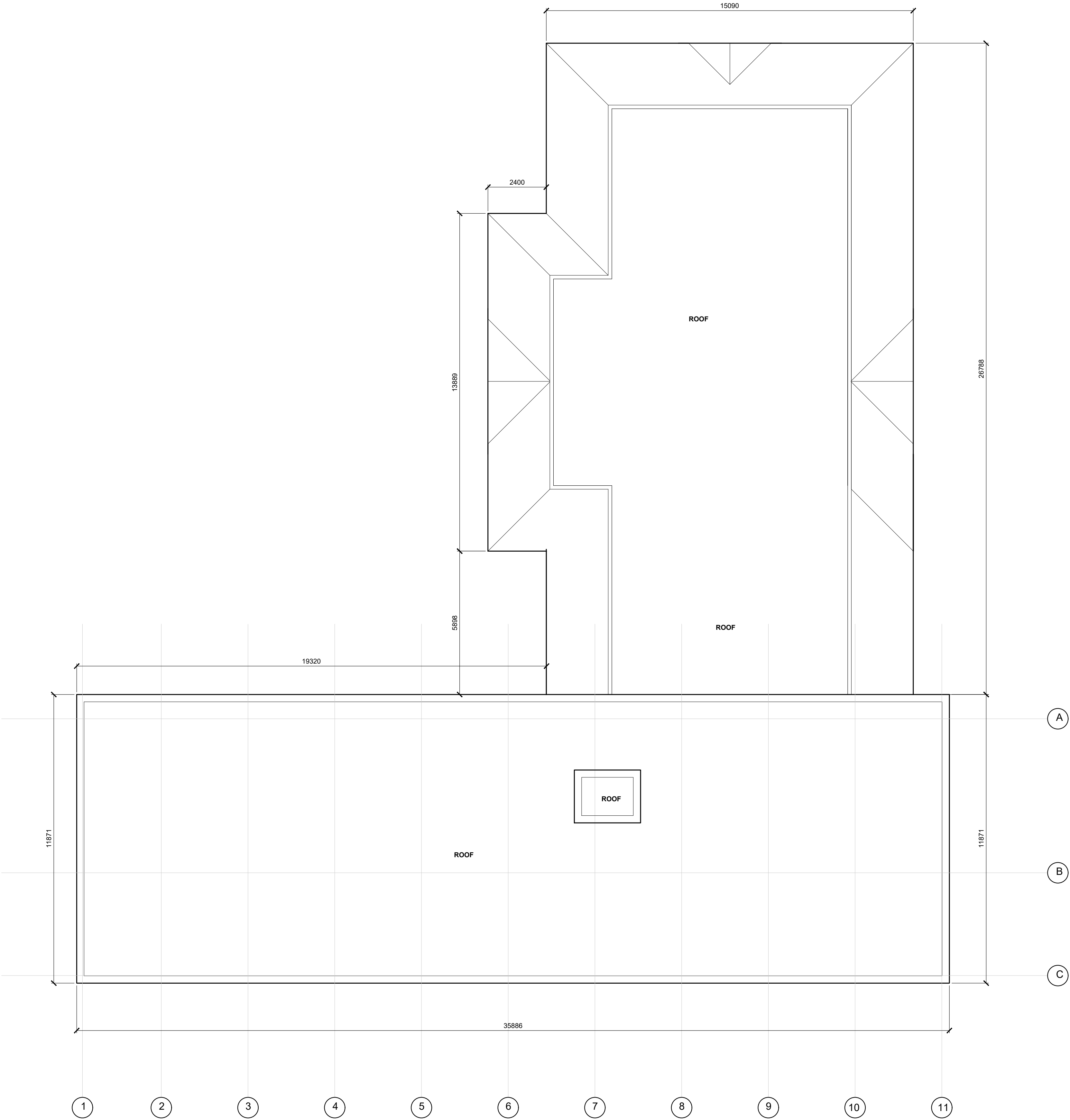


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The Erindale Academy
School Addition
1584 Dundas Street West
Mississauga, Ontario

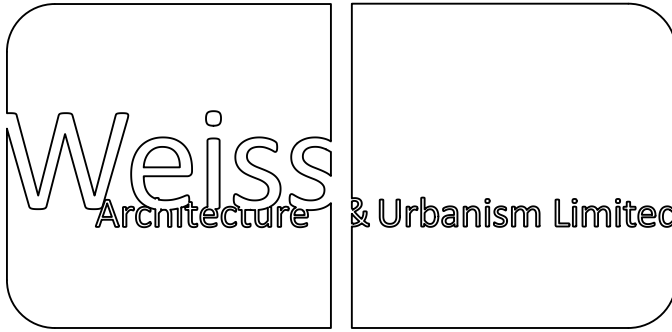
Schematic Design
Third Floor Plan

Project No. 1711	Sheet No. A204
Date:	
Scale: 1:100	



General Notes

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2	HIA (Heritage Impact Assessment)	17/10/12
1	COORDINATION	17/09/26
No.	Revision	Date

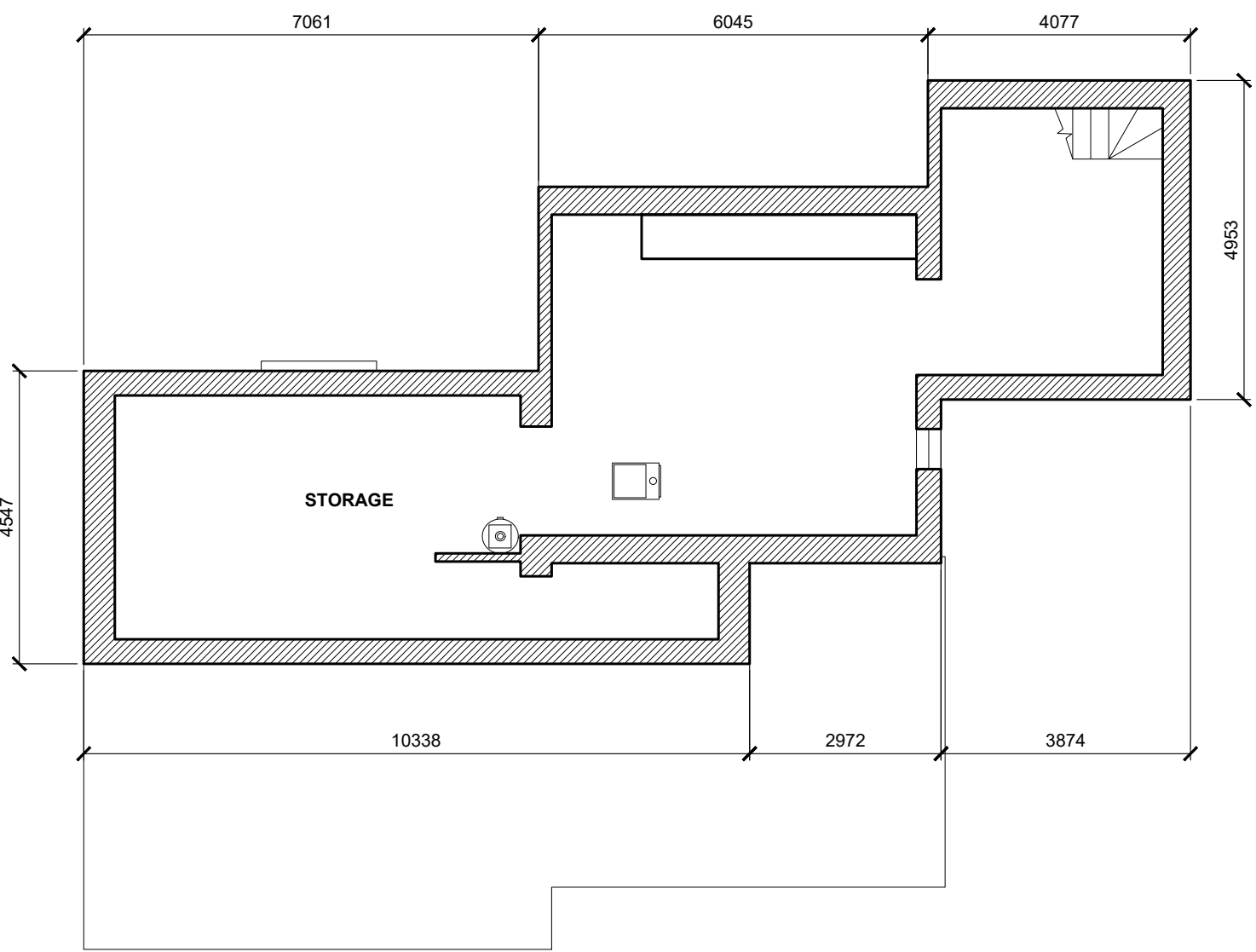


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Toronto Ontario
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F 647 494 9816

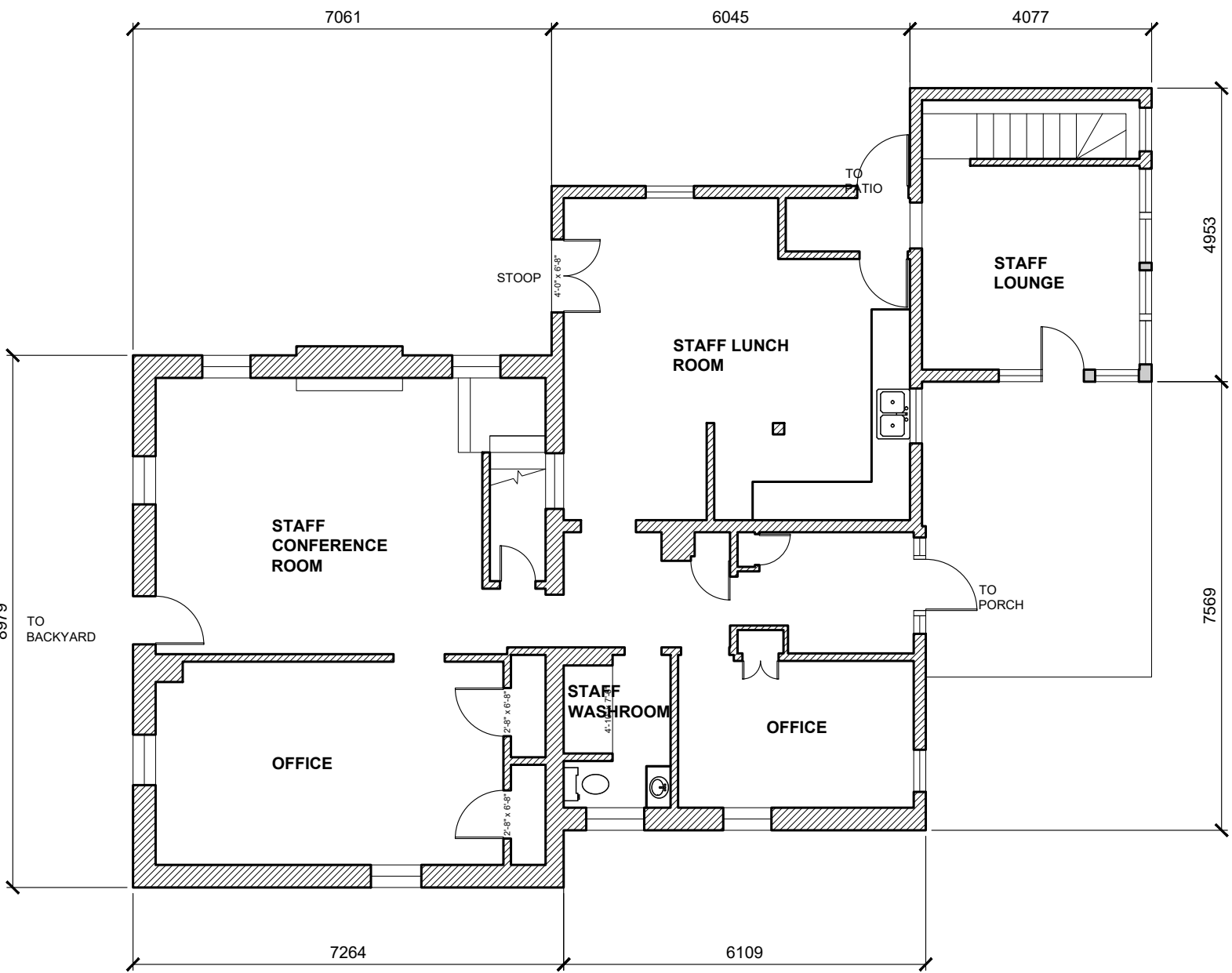
The Erindale Academy
School Addition
1584 Dundas Street West
Mississauga, Ontario

Schematic Design
ROOF Plan

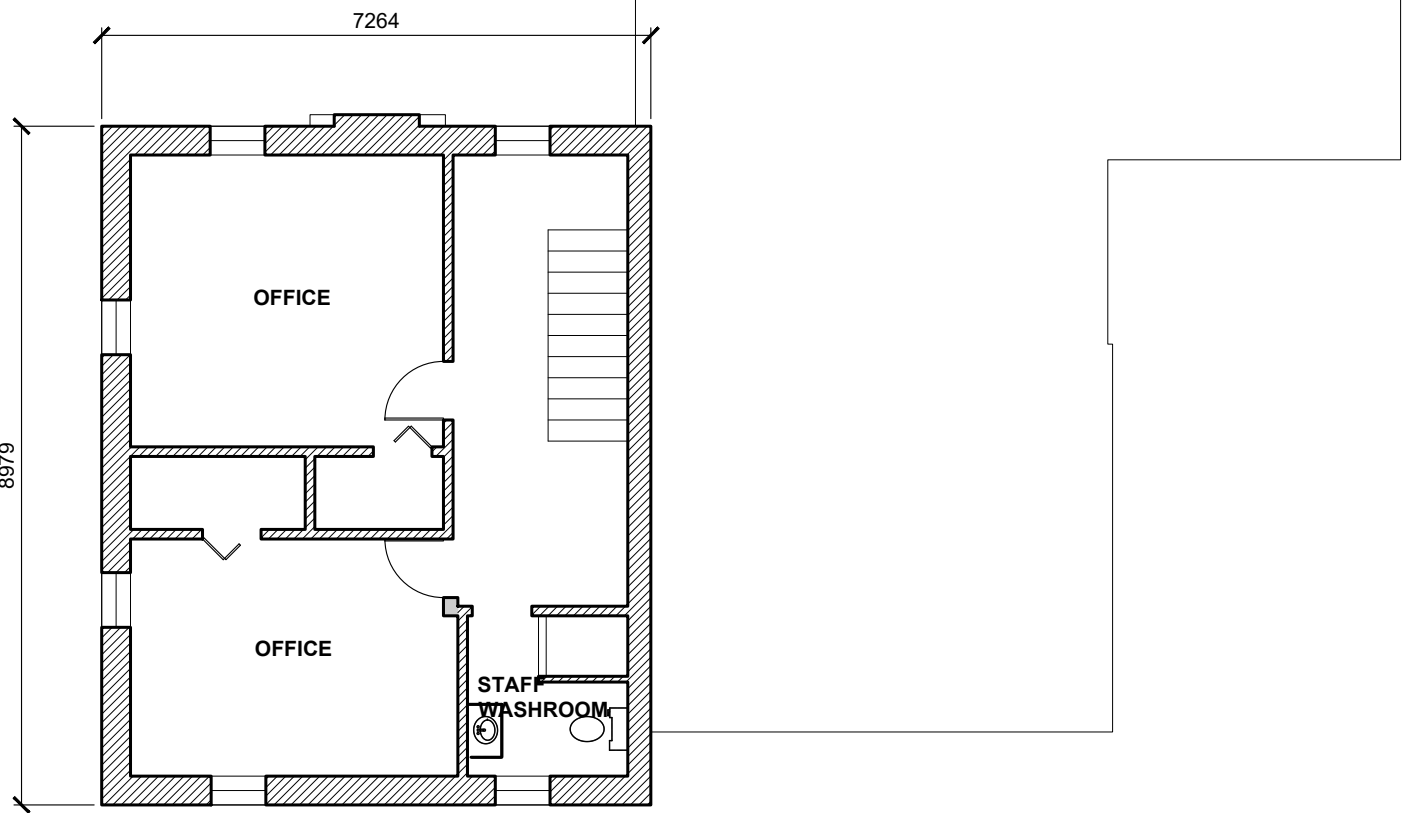
Project No. 1711	Sheet No. A205
Date:	
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EXISTING BASEMENT PLAN



EXISTING GROUND FLOOR PLAN



EXISTING SECOND FLOOR PLAN

General Notes		
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2	HIA (Heritage Impact Assessment)	17/10/12
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No.	Revision	Date

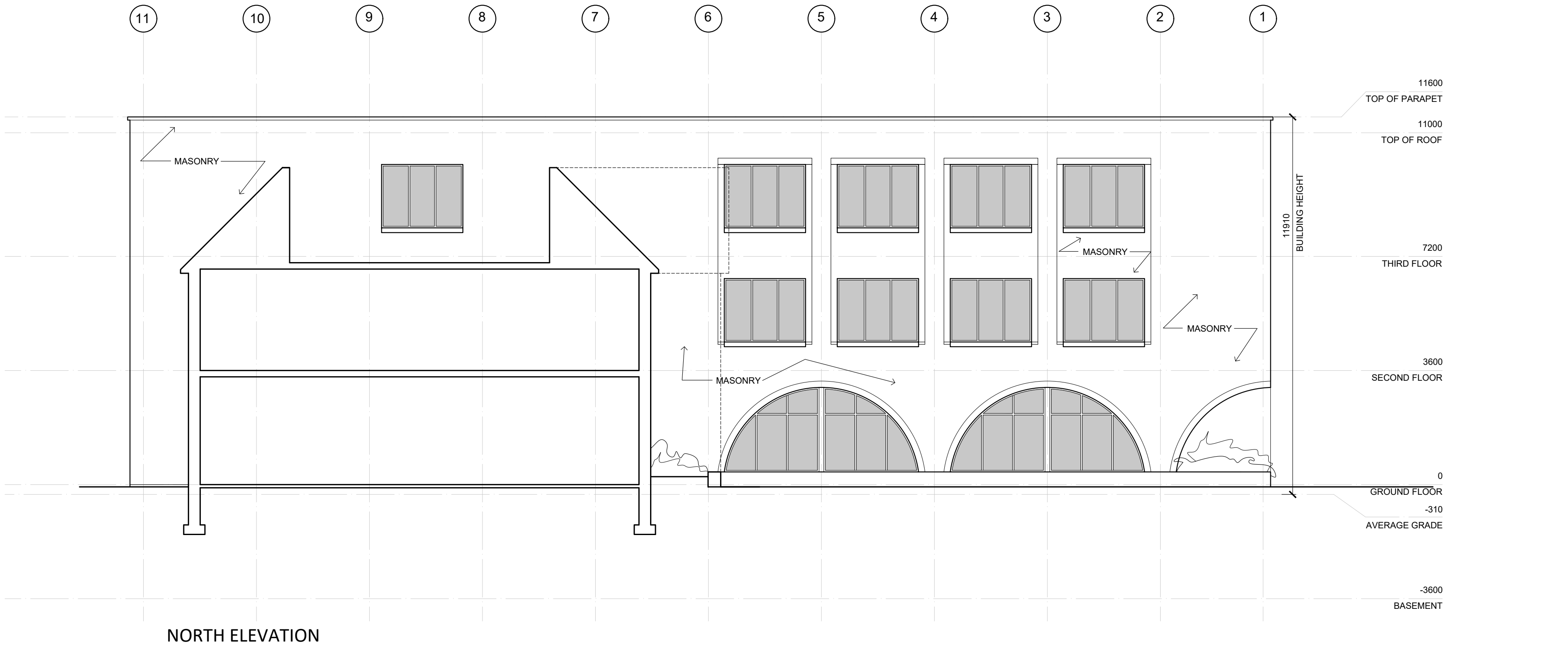
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Architecture & Urbanism Limited

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School Addition
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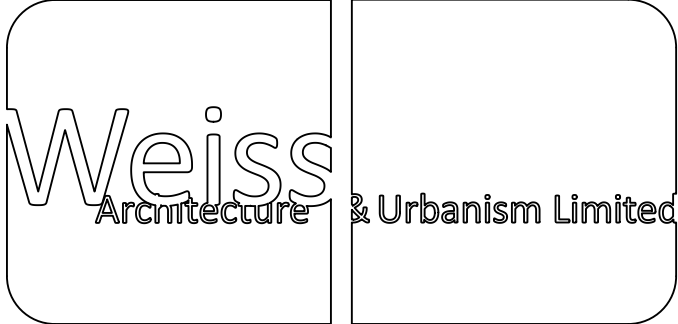
Schematic Design
Existing School Admin Building 2

Project No. 1711	Sheet No. A206
Date:	
Scale: 1:100	



General Notes

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2	HIA (Heritage Impact Assessment)	17/10/12
1	COORDINATION	17/09/26
No.	Revision	Date

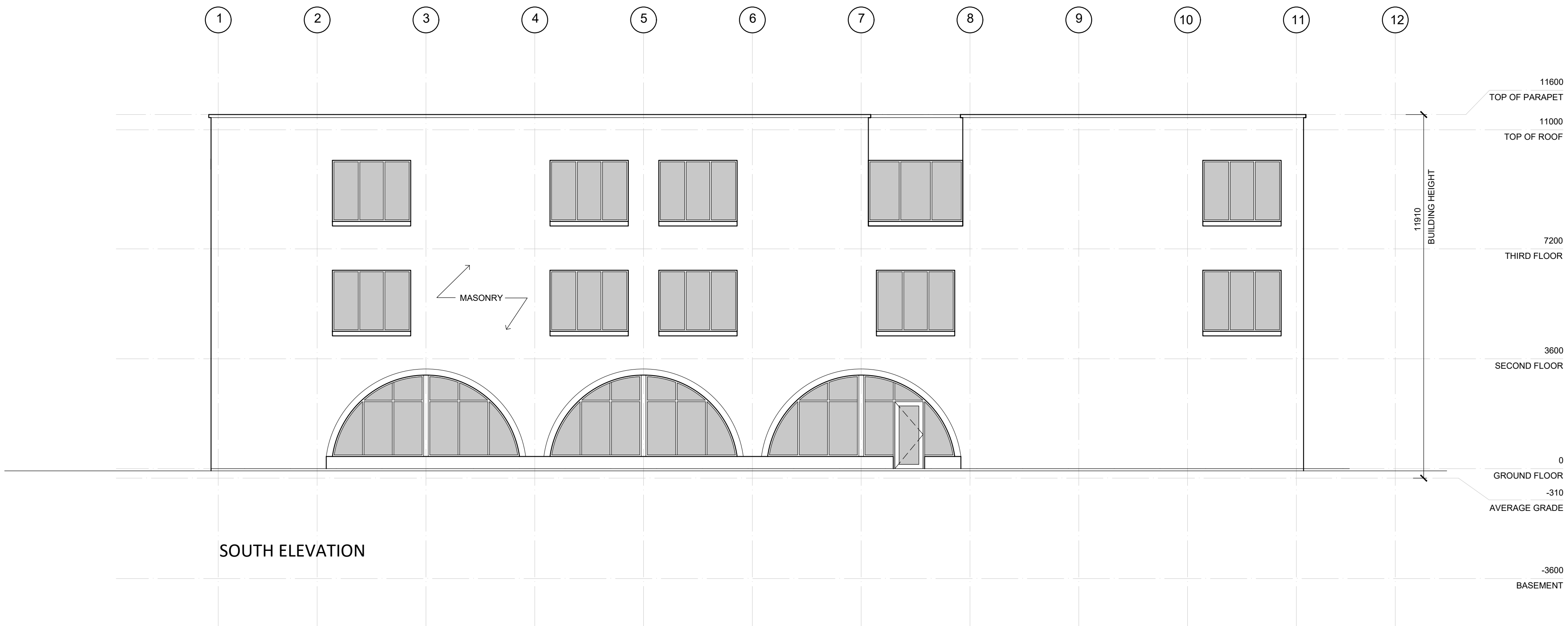


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The Erindale Academy
School Addition
1584 Dundas Street West
Mississauga, Ontario

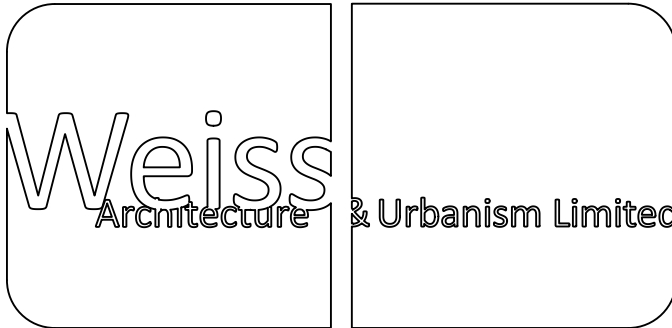
Schematic Design
North Elevation
West Elevation

Project No. 1711	Sheet No. A301
Date:	
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General Notes

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1	COORDINATION	17/09/26
No.	Revision	Date



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The Erindale Academy
School Addition
1584 Dundas Street West
Mississauga, Ontario

Schematic Design
East Elevation
South Elevation

Project No. 1711	Sheet No. A302
Date:	
Scale: 1:100	



ERINDALE PRESBYTARIAN CHURCH
1560 DUNDAS ST WEST

ERINDALE ACADEMY AND TAYLOR HOUSE
1576-1584 DUNDAS ST WEST

HOUSE
2585 JARVIS ST

TAYLOR HOUSE LANDSCAPE IMPROVEMENTS
RENOVATION OF EXISTING LANDSCAPE TO CREATE
A BETTER ADDRESS TO DUNDAS ST. WITH
TRIMMED LOW HEDGES AND NEW TREES.

EXISTING SCHOOL LANDSCAPE IMPROVEMENTS:
NEW TRIMMED LOW HEDGE TO CREATE SCREEN
TO PARKING AND NEW TREES

NOTE: BUILDING HEIGHTS ANNOTATED IN THIS
DRAWING ARE CALCULATED FROM DOOR SILL TO
ROOF PARAPETS.

General Notes		
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2	HIA (Heritage Impact Assessment)	17/10/12
1	COORDINATION	17/09/26
No.	Revision	Date

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The Erindale Academy
School Addition
1584 Dundas Street West
Mississauga, Ontario

STREET ELEVATION

Project No. 1711	Sheet No. A303
Date:	
Scale:	

APPENDIX C: Qualifications



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asiheritage.ca
aveilleux@asiheritage.ca

7.11 - 50

Annie Veilleux

Curriculum Vitae

EDUCATION

MA, Interdisciplinary Studies, York University, 2012
Diploma, Collections Conservation and Management, Sir Sanford Fleming College, 2006
BA, Honours Archaeology, University of Toronto, 2002

POSITION

Manager, Cultural Heritage Division, Archaeological Services Inc., 2014-present

PROFESSIONAL AFFILIATION

Canadian Association of Heritage Professionals
National Trust for Canada
Association of Critical Heritage Studies
Ontario Association for Impact Assessment
Ontario Archaeological Society

PROFESSIONAL EXPERIENCE

2014 - present	Manager – Cultural Heritage Division, ASI
2010 – 2013	Cultural Heritage Specialist and Project Manager, Built Heritage and Cultural Heritage Landscape Planning Division, ASI
2006 – 2009	Staff Archaeologist and Field Director, Stage 3-4 Division, ASI
2001 – 2004	Project Archaeologist, Field Archaeologist, and Laboratory Assistant, ASI

PROFILE

My education and experience in cultural landscape theory, historical research, archaeology, and collections management provide me with an excellent grounding in the area of cultural heritage planning and management. With over fifteen years of experience in this field, my work has focused on the identification and evaluation of cultural heritage resources, both above and below ground. I have served as Project Manager and Cultural Heritage Specialist on numerous built heritage and cultural heritage landscape assessments, heritage recordings and evaluations, and heritage impact assessments as required for Environmental Assessments and Planning projects throughout the Province of Ontario. I have extensive experience leading and conducting research for large-scale heritage planning studies, heritage interpretation programs, and have assisted in a number of projects requiring Indigenous consultation and engagement. I am fully bilingual in English and French and have served as a French language liaison on behalf of ASI.

AREAS OF EXPERTISE

- Heritage survey techniques
- Cultural heritage evaluation and impact assessment
- Consultation with the MTCS and heritage stakeholders
- Management of large scale heritage planning projects
- Identification and assessment of cultural heritage landscapes
- Thematic, archival, and oral historical research
- First Nations consultation and engagement programs

SELECT CULTURAL HERITAGE RESEARCH AND REPORTS

Large Scale Cultural Heritage Resource Planning Studies

Project Manager and/or Cultural Heritage Specialist for:

- Queen Elizabeth Way Lion Monument Strategic Conservation Plan, Sir Casimir Gzowski Park, City of Toronto, 2016 – Ongoing;
- City of Kawartha Lakes Heritage Conservation District Studies, City of Kawartha Lakes, 2015-2016;
- Official Plan Review (Heritage Policies), City of Brampton, 2016;
- Woodbridge Heritage Conservation District Urban Design Streetscape Plan Study, 2015-2016;
- Cave Springs Conservation Area Management Plan Cultural Heritage Inventory and Planning Study, Regional Municipality of Niagara (2015);
- Historic Yonge Street Heritage Conservation District Study and Plan, City of Toronto (2013-2015).

Cultural Heritage Resource Assessment (CHRA)/Cultural Heritage Screening Report (CHSR)

Senior Project Manager and Cultural Heritage Specialist for:

- Bayfront Industrial Area Renewal Strategy Phase 2, City of Hamilton, 2017 – Ongoing;
- Metrolinx Barrie Rail Corridor Expansion Cultural Heritage Screening, 2015 - Ongoing;
- Columbia Street – Lexington Road Improvements, City of Waterloo, 2017;
- Trent River Bridge Crossing, Campbellford, Northumberland County, 2015;
- Port Lands and South of Eastern Transportation and Servicing Master Plan Class Environmental Assessment, 2013-2016.

Cultural Heritage Evaluation Reports (CHER)/Heritage Impact Assessments (HIA)

Senior Project Manager and Cultural Heritage Specialist for:

- 2000 Stavebank Road Cultural Heritage Evaluation Report, City of Mississauga, 2017;
- Correctional Workers' Monument Heritage Impact Assessment, Whitney South Plaza, Queen's Park Complex Provincial Heritage Property, City of Toronto, 2016- Ongoing;
- Metrolinx Barry Rail Corridor Expansion Cultural Heritage Evaluations, 2016 – Ongoing;
- East Humber River Tributary Cultural Heritage Evaluation and Impact Assessment, City of Vaughan, 2014.



Heritage Documentation/Salvage Monitoring

Senior Project Manager and Cultural Heritage Specialist for:

- 5598 King Street Cultural Heritage Documentation Report, Town of Lincoln, 2014;
- Lingelbach United Church Cultural Heritage Documentation Report, Perth East, 2014.

Heritage Bridge Assessments

Senior Project Manager for:

- Frederick G. Gardiner Expressway West Heritage Impact Assessment, City of Toronto, 2017;
- Wyville Bridge Cultural Heritage Evaluation, Grey County, 2017.

ABORIGINAL ENGAGEMENT/TRADITIONAL ECOLOGICAL KNOWLEDGE PROJECTS

Research Assistant for:

- Highway 69 Four Laning and Bridge Crossing, French River, 2009;
- Humber River Shared Path, City of Toronto, 2009;
- Archaeological and First Nations Policy Study for the City of Vaughan Official Plan Review, 2009.

OTHER (INCLUDING FILM, TELEVISION, MEDIA AND PUBLIC DISPLAYS, HERITAGE EVENTS)

- Developed and assisted in the French translation of text for interpretive plaques along the Huron-Wendat Trail, City of Toronto (Heritage Toronto, 2012);
- Gave a presentation in French on the history of Indigenous people in Toronto to the FrancoGO Network of the Government of Ontario, 2015;

PUBLICATIONS

Articles

Veilleux, A.V.

2011 Toronto Landscapes: The Carrying Places. In *Profile: Newsletter of the Toronto Chapter of the Ontario Archaeological Society*. Volume 28(2).

Veilleux, A.V., and R.F. Williamson

2005 A Review of Northern Iroquoian Decorated Bone and Antler Artifacts: A Search for Meaning. In *Ontario Archaeology*. Volume 79/80.

PRESENTATIONS

Veilleux, A.

2017 Toronto Carrying Place: Exploring the Spaces in Between. Paper presented at the Understanding Toronto Through Archaeology: A Public Symposium, Heritage Toronto, Toronto, Canada.

Konrad, J., H. Schopf, R. Sciarra, and A. Veilleux



2015 The Niagara Escarpment: Exploring Bioregional Approaches to Cultural Heritage Landscape Management. Paper presented at the Cultural Landscapes and Heritage Values Conference, University of Massachusetts, Amherst, USA.

Veilleux, A.

2015 Knowing Landscapes: Living, Discussing, and Imagining the Toronto Carrying Place. Paper presented at the Toronto Carrying Place: A Shared Legacy Symposium, Etobicoke Historical Society, Toronto, Canada.

Konrad, J., R. Sciarra, and A. Veilleux

2014 Regional Borders and Cultural Heritage Landscapes in Ontario. Paper presented at the Borders in Globalization Conference, Carleton University, Ottawa, Canada.

Grant, T., Mackie, K., Mathias, C.A., Monahan, V., and Veilleux, A.

2006 Leather Clothing Components in Canada: From the Prehistoric to the Historic Period. Paper presented at the Canadian Association for Conservation 32nd Annual Conference, Toronto, Canada.





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James Neilson

Curriculum Vitae

EDUCATION

MES (Planning), Environmental Studies, York University, 2010
BA, Politics, Trent University, 2007

POSITION

Cultural Heritage Specialist, Archaeological Services Inc., 2017-present

PROFESSIONAL EXPERIENCE

2017 - present	Cultural Heritage Specialist –Cultural Heritage Division, ASI
2015 – 2017	Heritage Planner, ERA
2013 – 2015	Assistant Planner, Heritage Preservation Services, City of Toronto
2013	Secretary-Treasurer, Committee of Adjustment, Town of Aurora
2010	Heritage Planning Intern, Town of Oakville

PROFILE

My experience in the public and private sector has provided me with an excellent understanding of issues facing the cultural heritage industry and best practices in the field. Having prepared and reviewed cultural heritage evaluations and heritage impact assessments for projects ranging from small residential renovations to large-scale, high profile mixed-use developments, I am comfortable measuring impacts and providing a high-calibre of research and analysis that addresses municipal and provincial legislation and policy. As an urban planner, I have a particular interest in how cultural heritage resources can be conserved to create better communities.

AREAS OF EXPERTISE

- Heritage Planning Policy and Implementation
- Heritage Impact Assessments
- Cultural Heritage Evaluations
- Historical Research and Analysis
- Management of large scale heritage planning projects
- Stakeholder Engagement and Public Consultation

SELECT CULTURAL HERITAGE RESEARCH AND REPORTS

Cultural Heritage Evaluation Reports (CHER)/Heritage Impact Assessments (HIA)

- Cultural Heritage Evaluation Report: 33-47 & 91 Franklin Street, Newmarket On., Metrolinx Barrie Rail Corridor Expansion (May 2017)
- Cultural Heritage Evaluation Report/Heritage Impact Assessment: Union Station Rail Corridor - Yonge Street Bridge and Bay Street Bridge, Metrolinx (2016)
- Heritage Impact Assessment: Union Station Train Shed Electrification, Metrolinx (2016)
- Heritage Impact Assessment: 45/141 Bay Street, Toronto (2016)
- Cultural Heritage Evaluation Report/Heritage Impact Assessment: Glen Abbey Golf Course, Oakville On. (2016)
- Heritage Impact Assessment: Havergal College, Toronto On. (2016)
- Heritage Impact Assessment: 34-50 King Street East & 2 Toronto Street, Toronto On. (2016)
- Cultural Heritage Evaluation Report/Heritage Impact Assessment: 874 Yonge Street, Toronto On. (2016)
- Heritage Impact Assessment: 475 Yonge Street, Toronto On. (2016)
- Heritage Impact Assessment: 601 Sherbourne Street, Toronto On. (2016)
- Heritage Impact Assessment: 89-105 Church Street, Toronto On. (2016)
- Heritage Impact Assessment: Elora Mill South Bank, Elora On. (2015)
- Heritage Impact Assessment: 170 Spadina Avenue, Toronto On. (2015)
- Heritage Impact Assessment: 642 King Street West, Toronto On. (2015)
- Heritage Impact Assessment and Cultural Heritage Landscape Assessment: Homewood Health Centre, Guelph On. (2015)
- Cultural Heritage Evaluation Report: 183 King Street, London On. (2015)

PUBLICATIONS & ARTICLES

Neilson, James

“Heritage Tourism – Enhancing the Viability of Oakville’s Heritage Resources” *Ontario Planning Journal*. Vol 25, No. 6, Nov-Dec 2010, p. 21-22.

Neilson, James

“Promoting Oakville’s Heritage Resources” *Community Heritage Ontario*, October 2010, p. 4-5.



City of Mississauga
Memorandum



Date: 2017/11/23
To: Chair and Members of Heritage Advisory Committee
From: Paul Damaso, Director, Culture Division
Meeting Date: 2018/01/09
Subject: 1234 Old River Road

This memorandum and its attachment are presented for HAC's information only.

The City is in receipt of a site plan application for 36 Cotton Drive. This property is adjacent to the Robert Cotton House, 1234 Old River Road, which is designated under the Ontario Heritage Act. As per section 7.4.1.12 of the Mississauga Official Plan, "The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a Heritage Impact Assessment, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction." As such, the Heritage Impact Assessment is attached for your reference.

Attachments

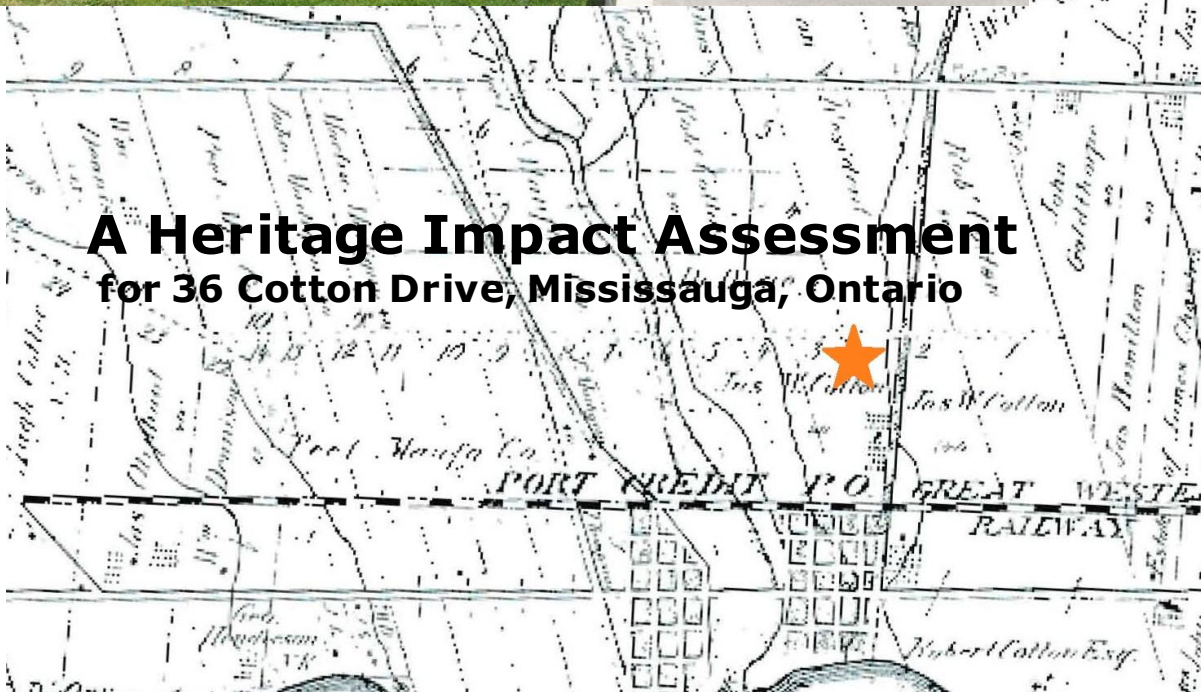
Appendix 1: Heritage Impact Assessment

A handwritten signature in black ink, appearing to read 'Paul Damaso', written in a cursive style.

Prepared by: Paul Damaso, Director, Culture Division



A Heritage Impact Assessment for 36 Cotton Drive, Mississauga, Ontario



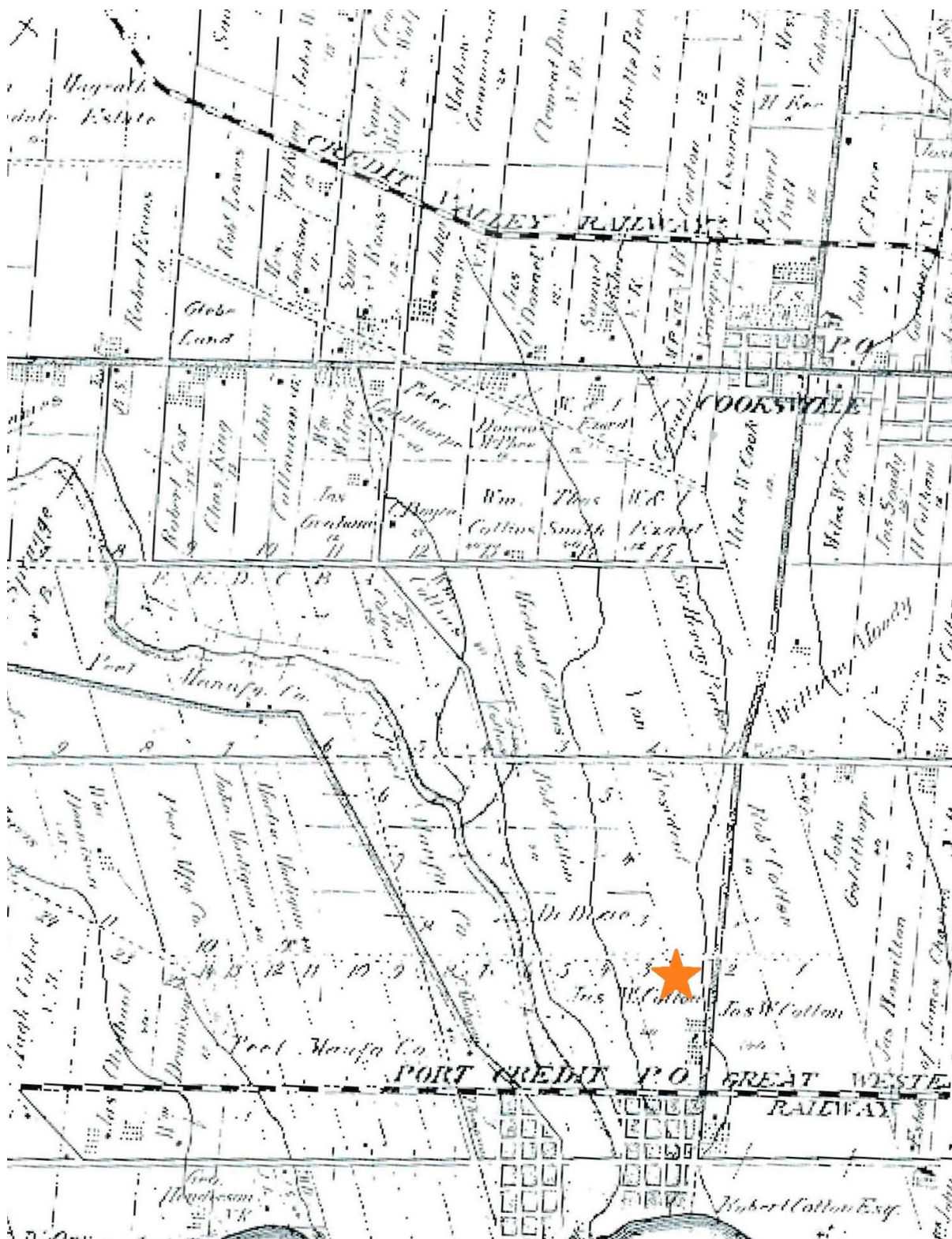


TABLE OF CONTENTS:

- 1.0 Development Proposal and Report Objective**
- 2.0 Location Plan of Subject Property**
- 3.0 Description of the Property**
 - 3.1 Description of the Present House**
 - 3.2 Floor Plans of the Existing House**
- 4.0 Historical Research**
- 5.0 Statement of Significance of the Property**
- 6.0 Summary of relevant municipal/agency requirements**
- 7.0 Proposed redevelopment Plan**
- 8.0 Impact on the Cotton-Hawksworth House**
- 9.0 Mitigation Measures**
- 10.0 Recommendation**

APPENDICES:

- A1. Sources of Information and References**
- A2. Selected Reference Documents**
- A3. Complete Drawings the proposed new house for 36 Cotton Drive (by Accredited Architectural Designer)**
- A4. Curriculum Vitae of Author**

Researched and authored by: **James Bailey Architect**
49 Melbourne Avenue
Toronto, M6K 1K6 jbarch@sympatico.ca
Tel. (416) 537-4140 Fax. (416) 537-0405

This study has been commissioned by its current owner in support of an application for redeveloping 36 Cotton Drive. It specifically addresses the architectural resources on the property and the relationship of the property to the cultural landscape in which it is located. The conclusions represent the independent opinions of the author.

1.0 Design Proposal and Report Objective

In May of 2016 this property on which was located an existing, vacant home was purchased by a new Owner. The intent of the purchaser is to construct a new home in sympathy with the kind of redevelopment that the Mineola West neighborhood has been experiencing.

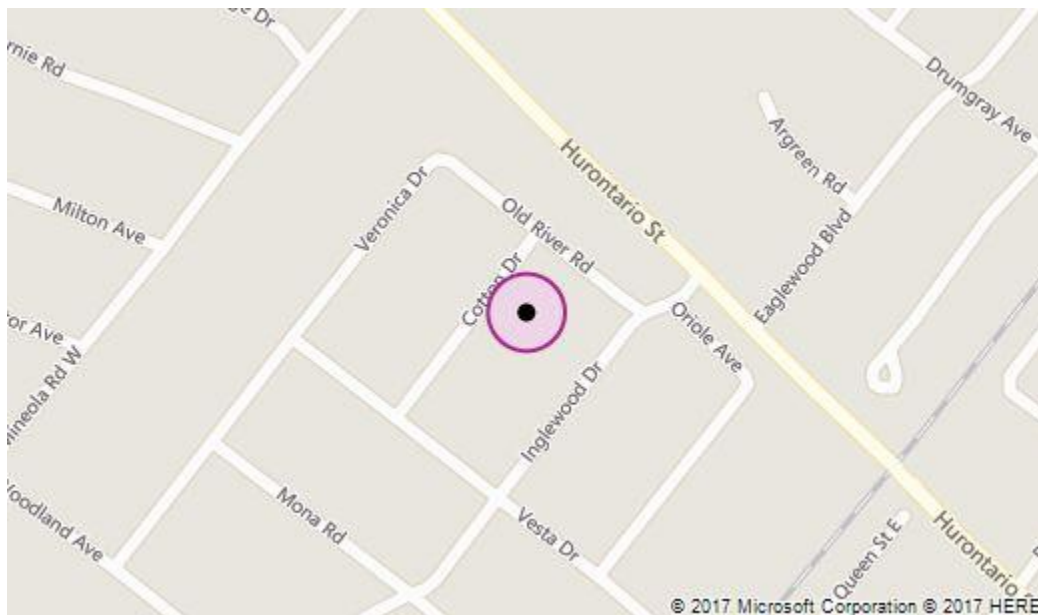
When this study was begun, 36 Cotton Drive was part of a Cultural Landscape Inventory approved by the City of Mississauga in 2005. On June 21, 2017, the Owner was informed that the high level of planning activity in Mineola has caused Council, in consultation of the City's Heritage Advisory Committee, to reduce the scope of the Heritage Landscape to those properties which abut Stavebank Road, as well as those individually listed on the Heritage Register (appendix 2 of a Memorandum of May 2, 2017 prepared by Paula Wubbenhorst, Senior Heritage Coordinator).

James Bailey, OAA, MRAIC, CAHP was retained by the owner to carry out this investigation as a Heritage Consultant. This report is a summary of his findings.

These findings are based on a documentary search, a site review by the Heritage Consultant carried out on June 16, 2017, a property title search carried out at the Ontario Land Registry Office on July 20, 2017.

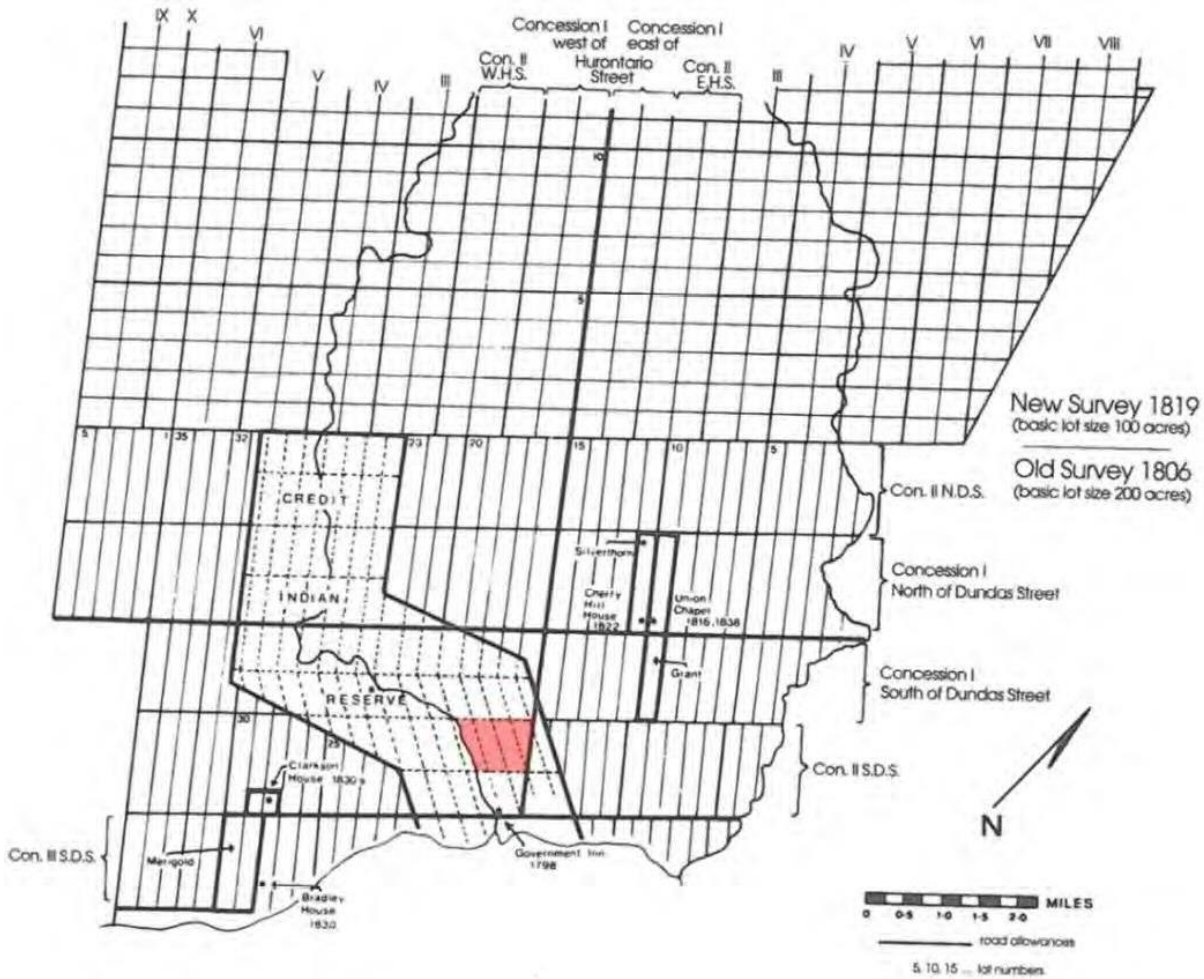
2.0 Location Plan of Subject Property

32 Cotton Drive is located in the Mineola West neighborhood. It is south east of the Queen Elizabeth Expressway and just west of Hurontario Street.



The lot is situated on land that was the last to be surveyed within the region. When Samuel Wilmot prepared his first survey of the area 1805-1806, a strip of land bordering the Credit

River remained in the hands of the Credit Indians. This area was not surveyed until 1821 and is known as the "Credit Indian Reserve"



Legal Description of the Property: **PLAN 323 PT BLK A RP 43R6925 PART 1**

Tax Roll Number: **21-05-010-016-08501-0000**

Zoning: R3-1

3.0 Description of the Property

At the time that this study began, this property was first listed on the City's Heritage Register due to its location in the Mineola Neighborhood, as part of a Cultural Landscape.

This is a neighborhood that was developed at a time before developers began the practice of taking a bulldozer to land to be subdivided to the point where natural topography and vegetation are all lost, and storm-water drainage is fully re-engineered. The subdivision was laid out in 1940 incorporating a road system and storm drainage system which respected the natural contours and drainage patterns. In this way the native vegetation was able to not

only survive but also regenerate. Lot sizes are generous and roadways follow the natural topography of the area. Because they were built to respect these irregularities, as well as the existing larger trees, the houses are often placed at unusual angles. There are no curbs and this provides a more natural transition from the roadways to the front lawns.

Much of the housing stock in Mineola is post World War II, although there are still a few homes dating from the 1920's. The older homes are distinguished by gum wood, stained glass and cedar shake shingles. Hurontario divides the neighborhood into East and West. The pricier of the two areas is the westerly half, in which this site is located.

As has already been indicated, just last month the concern with redevelopment of this property has moved from its contribution to the general fabric of the Mineola West neighborhood to some caution as to how this new home might impact the historic Cotton-Hawsworth at 1234 Old River Road (to the south-east of the proposed new home).

The lot measures 720.51 square meters, with a street frontage of 47.29 meters.

3.1 Description of the Present House

This particular house is a bungalow which, according to a building permit application on record, would seem to indicate that it was constructed in the fall of 1979.

36 Cotton Drive, stands somewhat out of proportion with its neighbors—for the most part one and one-half or two storeyed homes. The plan of the house is one that offers more depth than width to the street, which makes the house appear even smaller than it is. It sits on a full basement which is fully finished. The footprint of the house (with attached garage) is 186.5 sq. m. The total livable area of the house, on two levels, is approximately 253 sq. m.

A two-car garage is attached and faces the street. The remainder of the front elevation is a porch which fronts a large living room window and the main entrance doors.

The home is of conventional wood-frame construction with full brick veneer cladding. The hipped roof has at a relatively low slope (3:12) and is clad in asphalt shingles.

Interior finishes are undistinguished. The walls of the main floor areas are finished in painted gypsum wallboard. The floors in the living and bedroom areas are oak hardwood strip flooring (1 ¾"). The entrance foyer has a ceramic tile floor; bathroom and kitchen are floored in vinyl sheet. There is ceramic tiles on the bathroom walls.

In the basement, walls are clad in faux finish wood paneling. The basement floors, except for the bathroom are painted concrete. The bathroom has a ceramic tile floor and walls.

Other than the oak balustrade/handrail on the basement stairs, wood trim and doors appear to be in maple or cherry. The original kitchen cabinetry (i.e. 1979) is in a similar wood. Counters in the kitchen and on the bathroom vanities are in plastic laminate.

House Exterior/Interior Photographs



Front Façade



West façade (north end)



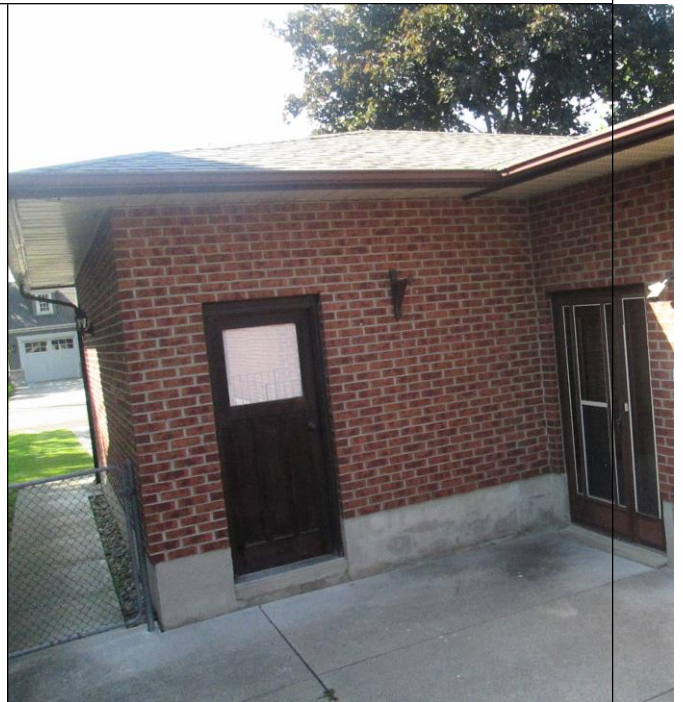
West Façade (south end)



Rear (south) façade



West wall



South wall of Garage

4.0 Historical Research

The focus of the archival research on this property has been to confirm when the existing house was built, if any construction had predated this house, and to confirm what the historical relationship of this property was, with the Cotton-Hawksworth at 1234 Old River Road. For this information there was not need to go any further than the Province of Ontario Land Registry.

The property is located on lands which were retained by the Credit Indian Reserve. The document, *Mississauga, Leading Today for Tomorrow*, compiled by the City Planning and Building Department, February 2004, describes Mineola as follows:

“From the late 17th century to the early 19th century the Credit River Valley was the exclusive domain for the Mississauga, a band of the Ojibway. They were nomadic hunters and fishers who travelled the entire length of the Credit River from Lake Ontario to Georgian Bay. In 1802 they relinquished most of their holdings to the British Government, with the caption of a strip of land one mile on each side of the Credit River—the Credit Indian Reserve, which now comprises part of Mineola, as we know it today. As settlement occurred, the Mississaugas sold most of the Credit Indian Reserve to the Crown in 1820.

Following deforestation, the lands in Mineola were used for agriculture up to the 1930's. Growth pressures of Port Credit, together with construction of the Queen Elizabeth Way, including Canada's first “clover leaf” interchange at Hurontario Street, provided the impetus for development.

Unlike many other subdivisions in Mississauga, Mineola appears to have been developed by several people in several parcels. Mineola underwent suburban residential development on these parcels of land throughout the 1940's and 1950's, and by 1950 older farmhouses lined Hurontario Street almost continuously from Port Credit to Cooksville.

Since that time, infill development has continued to take place, abetted by the widening of Hurontario Street, and the introduction of GO train services in 1967.”

The property which is the subject of this redevelopment proposal (demolition permit and building permit application) was severed from Block A which represented a more spacious property on which on which the Cotton-Hawksworth at 1234 Old River Road was located. A survey plan prepared by McConnell, Maughan Limited, consulting engineers and surveyors dated June, 1979 indicates that the rear (westerly) portion of the Cotton house was to be demolished, and two lots of 50' width, one to the back of the historic house, and one to its north, were to be created. We can only conclude that the owners of the Cotton house needed to generate some revenue to assist with maintaining their property.

Until recently this house, like all homes in Mineola West was considered to be part of a Cultural Landscape. In May of this year Council recognized that only certain properties within this neighborhood have been identified as being of concern. For the record, the Cultural Landscape Inventory L-RES-6 which applies to Mineola is included on the next page.



Cultural Landscape Inventory

Mineola Neighbourhood

L-RES-6

Location Located north of Lakeshore Road bounded by the Credit River on the west and Hurontario on the east

Heritage or Other Designation None

Landscape Type Residential (Neighbourhood)

LANDSCAPE ENVIRONMENT

- ☒ Scenic and Visual Quality
- ☒ Natural Environment
- ☐ Horticultural Interest
- ☒ Landscape Design, Type and Technological Interest

HISTORICAL ASSOCIATION

- ☒ Illustrates Style, Trend or Pattern
- ☐ Direct Association with Important Person or Event
- ☒ Illustrates Important Phase in Mississauga's Social or Physical Development
- ☐ Illustrates Work of Important Designer

BUILT ENVIRONMENT

- ☒ Aesthetic / Visual Quality
- ☐ Consistent Early Environs (pre-World War II)
- ☒ Consistent Scale of Built Features
- ☐ Unique Architectural Features / Buildings
- ☐ Designated Structures

OTHER

- ☐ Historical or Archaeological Interest
- ☐ Outstanding Features / Interest
- ☒ Significant Ecological Interest
- ☐ Landmark Value

A letter sent to the land-owner on June 21, 2017 appraised the owner that the entirety of Mineola West was no longer to be considered as part of a Cultural Landscape. As of the present time, only those along Stavebank Road and the Credit River, and several others identified on the Heritage Register of a Memorandum dated May 2, 2017 from Paula Wubbenhorst, Senior Heritage Coordinator.

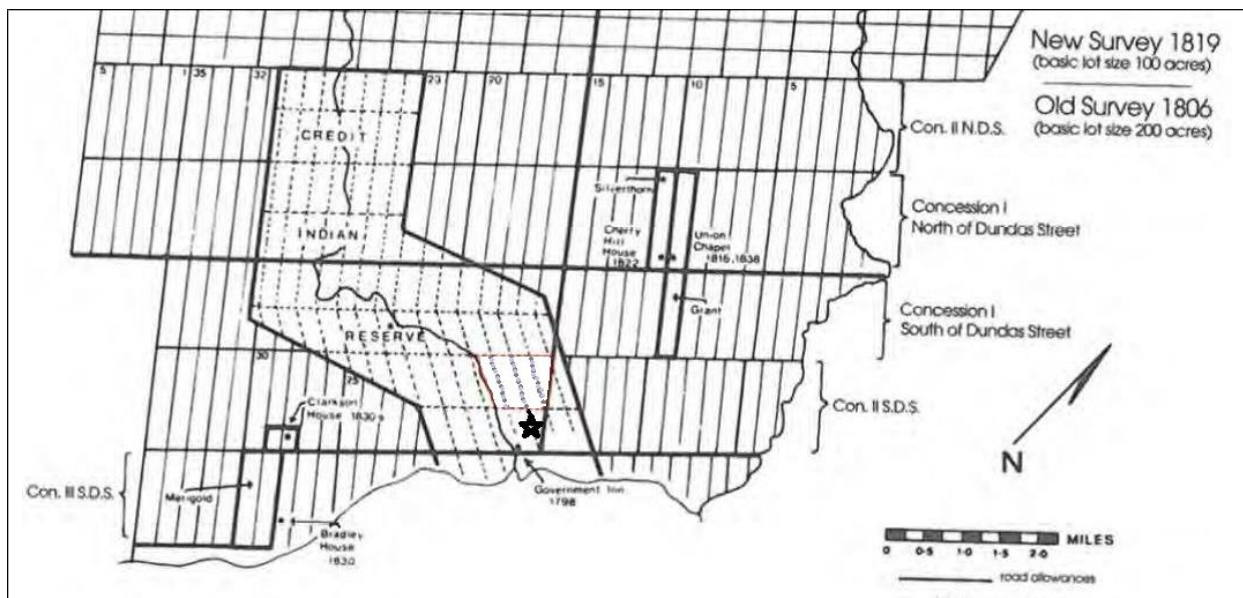
36 Cotton Drive, due to its proximity to the historic Cotton-Hawsworth House at 1234 Old River Road (just south of 36 Cotton Drive), was identified as one of these sensitive properties.

As no longer part of the Cultural Landscape, this study should rather comply with the Terms of Reference set out at:

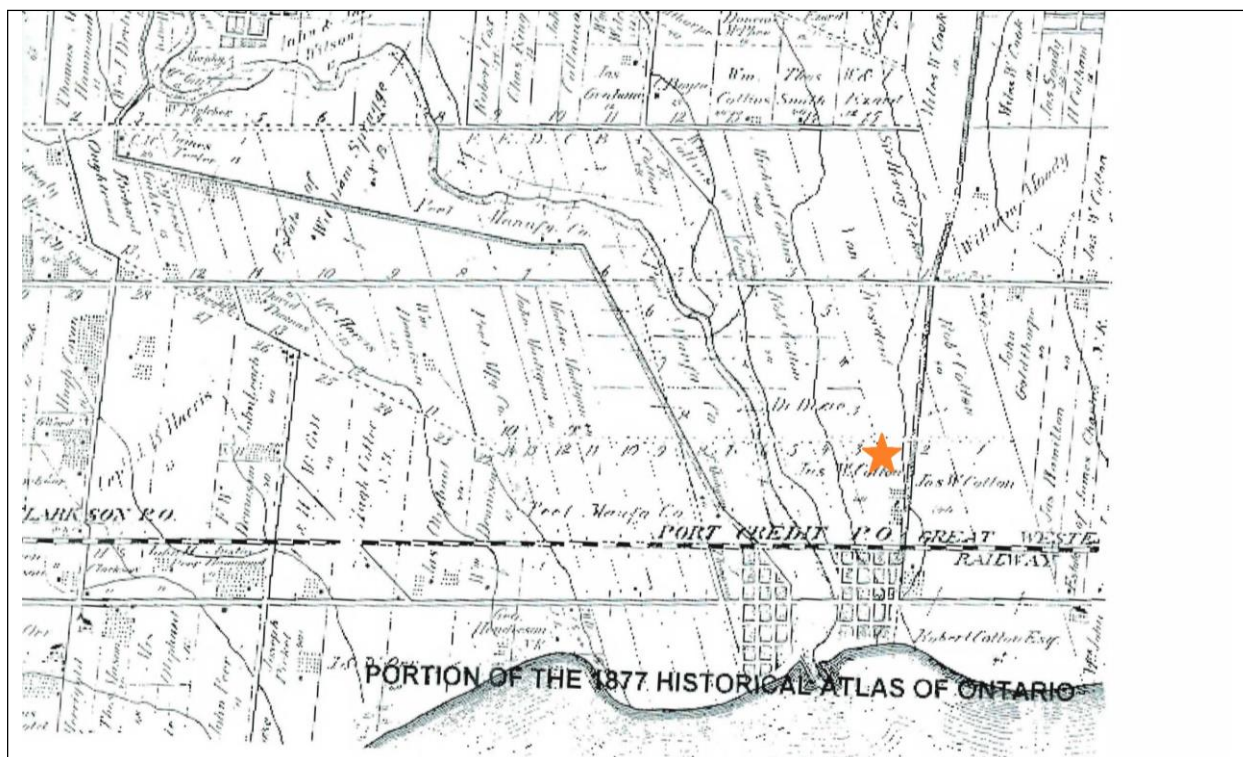
<https://www7.mississauga.ca/documents/culture/Heritage/HeritageImpactAssessment_TermsOfReference2017.pdf>

Land Ownership

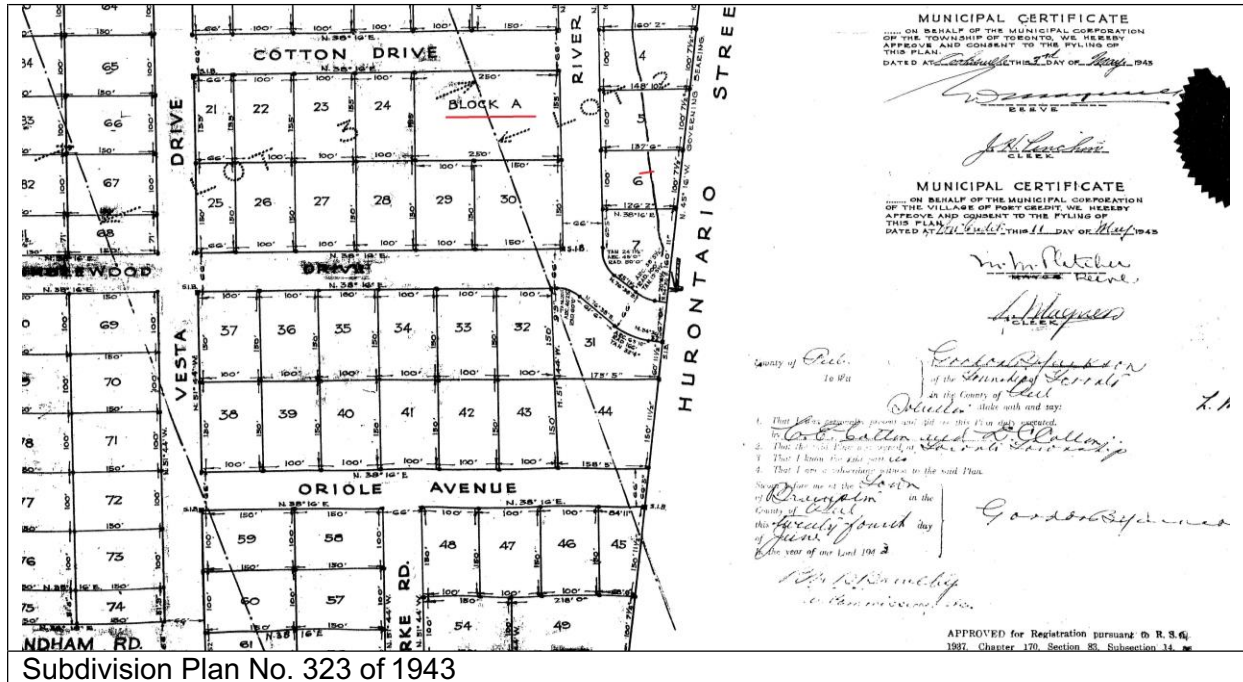
36 Cotton Drive is located on land which in the 1821 Credit Indian Reserve Survey was on a parcel known as "Block A" which straddled lots 2 and 3, of Range 1.



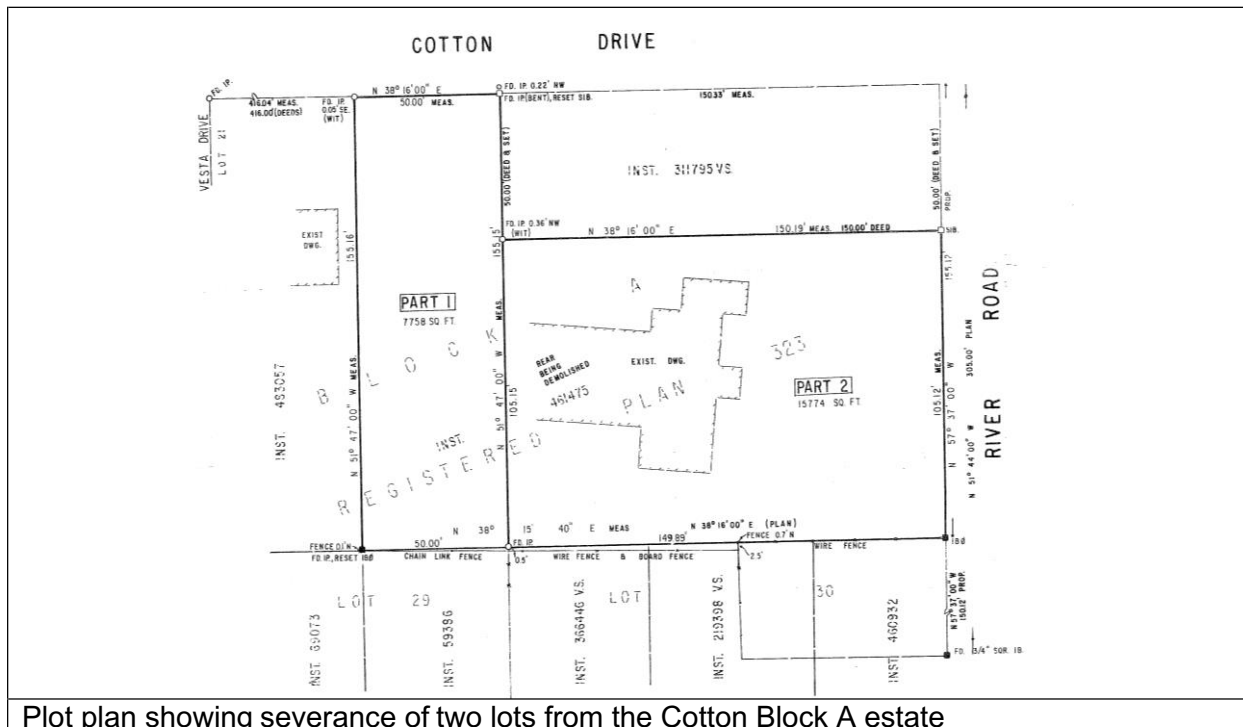
The historic maps which show Mineola include Tremaine's Map of the County of Peel, 1859; the 1877 Illustrated Historical Atlas of Peel; and a 1918 Guidal Map of the Township of Toronto.



The record indicates that Robert Cotton purchased part of Lot 2, CIR (Credit Indian Reserve) Range 1 in 1856. In 1943 the lot of which the Cotton house stood was considerably reduced when sub-division plan No. 323 was registered with the County of Peel. At that time, the last Cotton descendent to live in the house, Cyril E. Cotton, sold his interest.



What we know is that the house under review was built on land severed from the Cotton property in 1979.



The subject house was built for Michel Rose Minns, who sold it to the present owner.

5.0 Statement of Significance of the Property

Ontario Regulation 9/06 made under the Ontario Heritage Act establishes three criteria for determining Cultural Heritage Value or Interest.

1. The property has **design value or physical value** because it,
 - a) is a rare, unique, representative or early example of a style, type, expression, material or construction method,
 - b) displays a high degree of craftsmanship or artistic merit, or
 - c) demonstrates a high degree of technical or scientific achievement.
2. The property has **historical value or associative value** because it,
 - a) has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,
 - b) yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or
 - c) demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.
3. The property has **contextual value** because it,
 - a) is important in defining, maintaining or supporting the character of an area,
 - b) is physically, functionally, visually or historically linked to its surroundings, or
 - c) is a landmark.

We shall examine this property in relationship to each of these criteria, but as this property is listed as being part of a Cultural Landscape, we will put an emphasis on those specific criteria which were identified in L-RES-6 (included on page 13 of this report) as being significant for this site. Section 5.3 deals with these issues.

5.1 Design or physical value

The design of 36 Cotton Drive is not unlike many others constructed in the 1960's and 1970's. It is a comfortable home designed to accommodate a middle-class Canadian family, but from an architectural point of view it is undistinguished.

DESIGN OR PHYSICAL VALUE

We are unable to defend the preservation of this house on the merits of its design or construction.

5.2 Historical or associative value

While it has been built on property which was once part of the estate of a significant Port Credit family, the house itself has no historical associations of significance.

HISTORICAL OR ASSOCIATIVE VALUE

The present house has no historical associations of significance.

5.3 Contextual Value/ Landmark Status

It is how this proposed new home relates to the historic Cotton-Hawsworth House that has been flagged as being the major concern. The aerial photograph below (thanks to GoogleEarth) indicates the close proximity of the two properties. Historical and current photos follow.

AERIAL VIEW OF SITE

Historic photograph of Cotton House



Cotton-Hawsworth House as it appears today



View of Cotton-Hawsworth House from side and rear of 36 Cotton Drive (cream colour)

In terms of the Cotton Drive frontage, the current home has a very poor relationship to those either side.





View of existing house(from realtor's listing. House obscured in summer pictures)



Neighboring house to the right (west)



Neighboring house to the left (east)

CONTEXTUAL VALUE

Due to the hefty property values in Mineola, the neighborhood housing stock is being rejuvenated at a scale somewhat larger than what was originally constructed. This is a difficult trend to fight unless an existing home has some special merit, either architecturally or historically. The current home has neither of these, and also cannot be considered to have landmark status.

6.0 Summary of relevant municipal/agency requirements

As has been already stated, the current zoning of the lot is R3-1. It falls well within a neighborhood of exclusively single family homes.



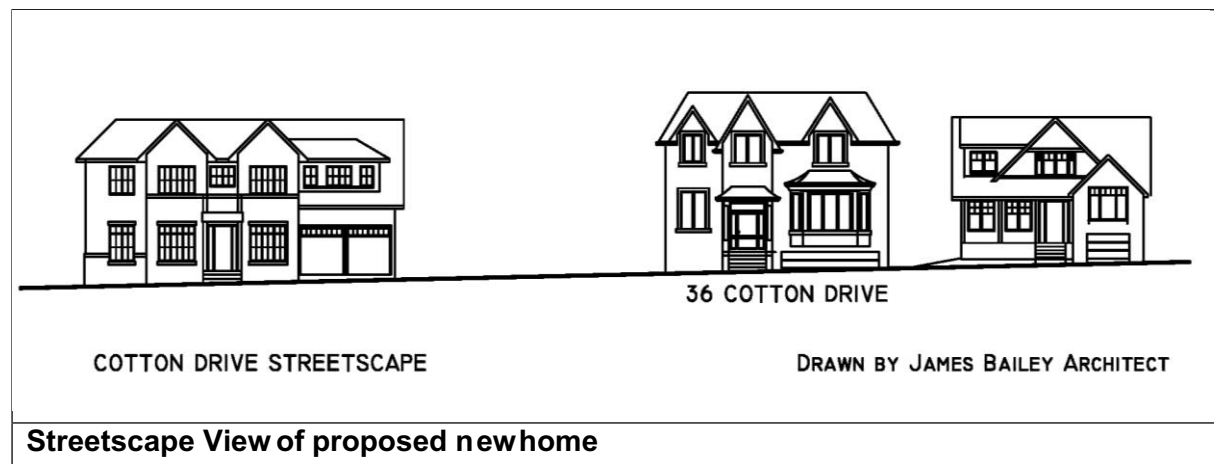
As part of a designated culture landscape, development is also controlled by Ontario Heritage Act, Regulation 9/06, which is addressed by this study. This property is listed under Cultural Landscape L-RES-6.

The City of Mississauga has also developed Design Guidelines and Site Plan Requirements for New Dwellings, Replacement Housing and Additions (May 2010) which were intended to provide some guidance to homeowners wishing to upgrade their properties.

7.0 Proposed redevelopment plan

What follows on the following pages is a site plan and elevations of a new home which has been proposed by the Owner for the Site. The house is substantially larger than the present home, however the front façade of the new house has attempted to follow the alignment established by neighboring homes to either side.

In order to get a sense of the relative scale of the proposed new home, as it relates to its neighbors, we have prepared the following streetscape view.



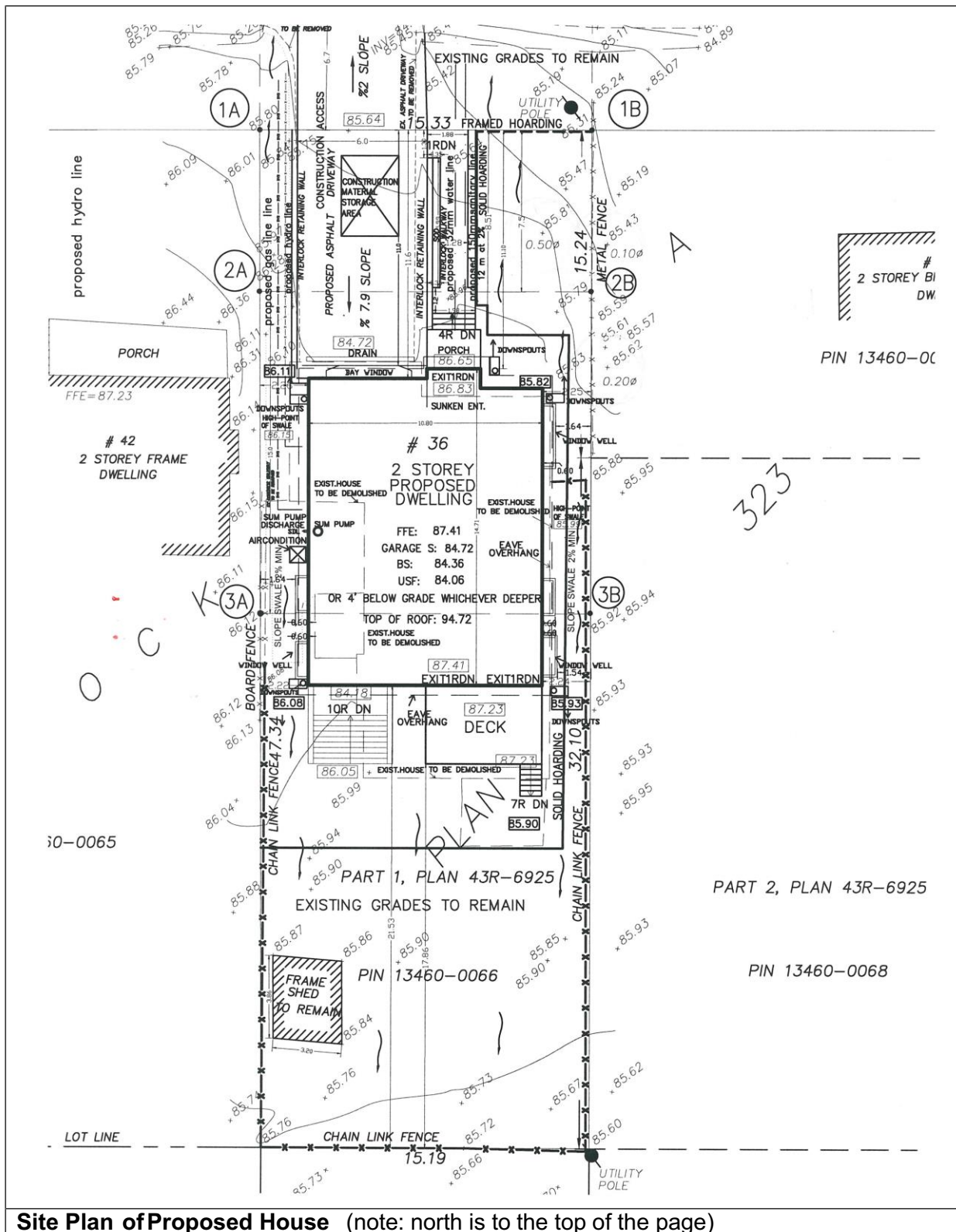
While historically many of the streetscapes in West Mineola were characterized by single floor height, bungalow designed homes, current property values and development pressures have increased the number of one and half and two story residential structures.

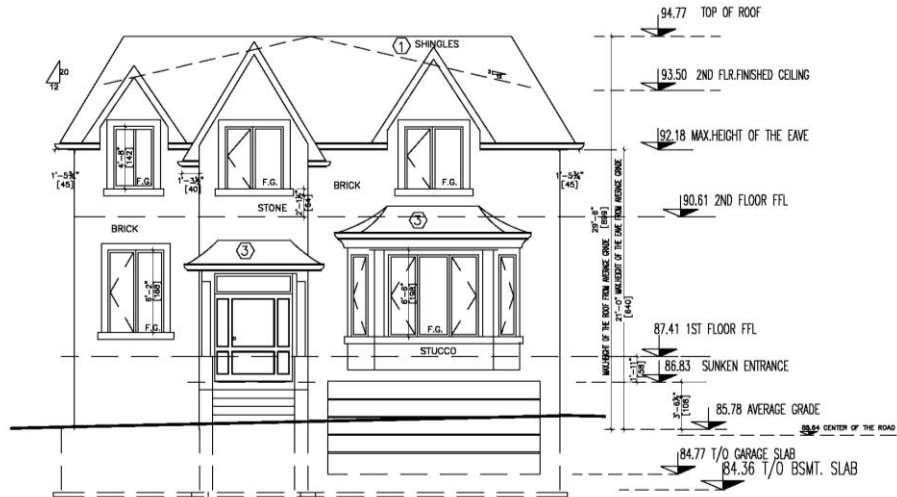
At 50 feet of width the site left no option other than to have a street-facing garage. Like its neighbor to the west, the Owner has resorted to a reverse-grade driveway. Less than 50% of the front hard is allocated to driveway.

The architectural design of the house is quite typical of the larger homes that are being built on tighter lots in the Toronto area. The mass has been broken by the use of three finish materials: brick, stone and stucco. A bay window and a porch bring down the scale of the two story height to better match that of neighboring homes.

In terms of the relationship of the proposed new home to the Cotton-Hawksworth House, the rear wall of the new house is 3 meters (10 feet) tighter to the front property line. This provides a greater buffer between the two homes, and the possibility of further plantings.

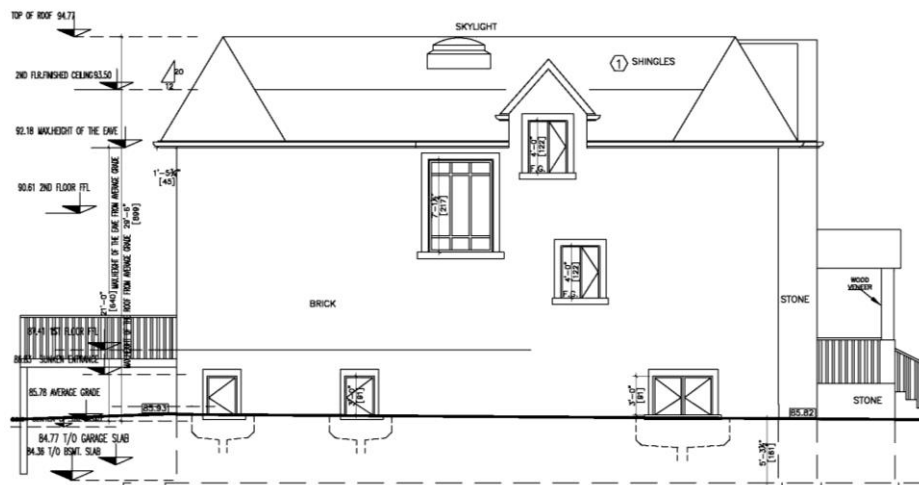
A framed garden shed currently located in the rear yard of 36 Cotton Drive is to remain.



**Front Elevation****West Side Elevation**



Rear Elevation



East Side Elevation

8.0 Impact on the Cotton-Hawsworth House

In our view the current house at 36 Cotton Drive holds no cultural heritage value. The concern is, above all, how redevelopment will impact the 1856 Cotton-Hawsworth home which abuts the easterly property line (1234 Old River Road).

There is no need for this report to establish the importance of this neighboring house, built and occupied by one of the earliest residents and entrepreneurs in Toronto Township. The following block plan, indicates the relationship of the existing house footprint to the Cotton-Hawsworth House.



We might offer regret that that house had not been protected on a larger parcel of property, but the costs of preservation being what they are, an earlier owner was forced to sell off the property on which 36 and 28 Cotton Drive now stand.

In terms of the relationship to the neighboring historic home, the proposed redevelopment of 36 Cotton Drive does two things to improve the neighborhood:

- 1) It increases the physical separation of old and new by some 10 feet.
- 2) It provides a more integrated streetscape along Cotton Drive.

9.0 Mitigation Measures

As has already been suggested, the deeper rear yard, which abuts the Cotton-Hawkworth house offers the possibility of more planting which might protect the “microenvironment” of the historic home.

The architectural designer of the proposed new home might take a few more “hints” from the homes either side which lean towards an “arts and crafts” style of architecture.

10.0 Recommendation

The author of this study recommends that the current home at 36 Cotton Drive be replaced by the proposed new home.

APPENDIX A1 Sources of Information

City of Mississauga Services Online: Property Information

Mississauga, Leading Today for Tomorrow”, the City Planning and Building Department, February 2004

Mississauga Planning Department: Mineola District, April 2010

Government of Ontario Land Registry Office, Peel Region: Land ownership records

1858 Tremaine Map

1877 Illustrated Historical Atlas of Peel, edited by Walker and Miles

Guidal Map of the Township of Toronto, 1918, published by the Map and Advertising Co.

The City of Mississauga has also developed Design Guidelines and Site Plan Requirements for New Dwellings, Replacement Housing and Additions (May 2010)

Google Earth

APPENDIX A2 Selected Reference Documents

- a) **Heritage Listing for 36 Cotton Drive, Mississauga**
- b) **Teranet Land Registry**

7.12 - 26

Property Information

Mississauga's heritage, which extends over 10,000 years, includes archaeological resources, numerous residential, commercial and industrial buildings, views, vistas, ridge lines, scenic routes and a variety of natural heritage properties.

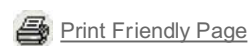
To view details about the inventory item, please click on the INV # link.

Property Details	Zoning Information	Building Permits	Development Applications	Committee of Adjustment	Heritage	Map It
------------------	--------------------	------------------	--------------------------	-------------------------	-----------------	--------

PROPERTY HERITAGE DETAILS[View Another Property](#)

Please contact the Planning and Heritage office at **905 - 896-5382** for further information.

Address: 36 COTTON DR
Legal Description: PLAN 323 PT BLK A RP 43R6925 PART 1
Roll Number: 21-05-010-016-08501-0000

**Heritage Status**

Status: PREVIOUSLY LISTED BUT NOW DEMOLISHED\UNLISTED
Conservation District:
Bylaw:
Bylaw Date:

Inventory Item

INV #	Property Name	Constructed	Decade	Demolished	Year Demolished
<u>1</u>	Mineola Neighbourhood			N	

Designation Statement

Designation Statement not available

LAND
REGISTRY
OFFICE #43

13460-0066 (LT)

PAGE 1 OF 1
PREPARED FOR JAMES BAILEY
ON 2017/06/05 AT 14:17:19

teranet express

PROPERTY DESCRIPTION: PT BLK A, PL 323 , PART 1 , 43R6925 ; MISSISSAUGA

PROPERTY REMARKS:

ESTATE/QUALIFIER:

FEE SIMPLE
LT CONVERSION QUALIFIED

OWNERS' NAMES

RECENTLY:

RE-ENTRY FROM 13460-0200

CAPACITY SHARE
ROWN

PIN CREATION DATE:

1998/02/23

* CERTIFIED IN ACCORDANCE WITH THE LAND TITLES ACT * SUBJECT TO RESERVATIONS IN CROWN GRANT *

REG. NUM.	DATE	INSTRUMENT TYPE	AMOUNT	PARTIES FROM	PARTIES TO	CERT/ CHKD
EFFECTIVE 2000/07/29	THE NOTATION OF THE	"BLOCK IMPLEMENTATION DATE" OF 1997/03/18 ON THIS PIN				
WAS REPLACED WITH THE	"PIN CREATION DATE" OF 1998/02/23					
** PRINTOUT INCLUDES ALL DOCUMENT TYPES (DELETED INSTRUMENTS NOT INCLUDED) **						
** RANGE SPECIFIED FOR SEARCH: FROM 1976/06/02 TO 2017/06/02 **						
**SUBJECT, ON FIRST REGISTRATION UNDER THE LAND TITLES ACT, TO:						
** SUBSECTION 44 (1) OF THE LAND TITLES ACT, EXCEPT PARAGRAPH 11, PARAGRAPH 14, PROVINCIAL SUCCESSION DUTIES *						
** AND ESCHEATS OR FORFEITURE TO THE CROWN .						
** THE RIGHTS OF ANY PERSON WHO WOULD, BUT FOR THE LAND TITLES ACT, BE ENTITLED TO THE LAND OR ANY PART OF						
** IT THROUGH LENGTH OF ADVERSE POSSESSION, PRESCRIPTION, MISDESCRIPTION OR BOUNDARIES SETTLED BY						
** CONVENTION .						
** ANY LEASE TO WHICH THE SUBSECTION 70 (2) OF THE REGISTRY ACT APPLIES .						
**DATE OF CONVERSION TO LAND TITLES: 1998/02/24 **						
43R6925	1979/06/12	PLAN REFERENCE				C
PR2954271	2016/07/22	TRANSFER	\$1,110,000			C
	REMARKS: PLANNING ACT STATEMENTS .					
PR2954272	2016/07/22	CHARGE	\$854,500	HSBC BANK CANADA		C

7.12 - 27

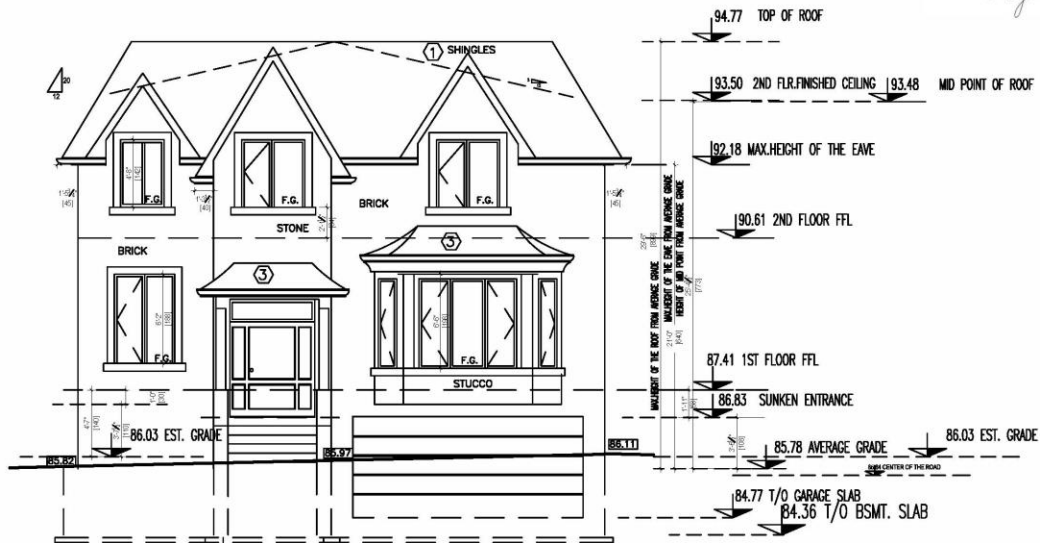
**APPENDIX A3: Complete Drawings the proposed new house for 36 Cotton Drive
 (by Accredited Architectural Designer)**

MURAT OZGUR DESIGN BUILD

BCIN # 107211

MURAT OZGUR

BCIN# 104649



BRICK: RED/BURGUNDY
STONE: INDIANA LIMESTONE
WINDOWS: VINYL, COMMERCIAL BROWN
EAVES/FASCIA/DOWN SPOUTS: COMMERCIAL BROWN
EXTERIOR DOOR: OAK - DARK WALNUT
SHINGLES: KO CAMBRIDGE DRIFTWOOD

SITE PLAN APP.NO:		
SPI 17-114 W1		
NO.	REVISIONS	DATE

JOIST SCHEDULE
 $\frac{3}{4}$ " T&G PLYWOOD
 (GLUED & NAILED)
 J1-11 $\frac{7}{8}$ " LPI-20 PLUS@18" oc
 J2-11 $\frac{7}{8}$ " LPI-20 PLUS@18" oc

**LAMINATED BEAM
SCHEDULE**
E=2.0 x 10⁶ PSI

S1= 1-~~3~~ X 11 7/8 LM.
S2= 2-~~3~~ X 11 7/8 LM.
S3= 1-~~3~~ X 11 7/8 LM.
S4= 2-~~3~~ X 8 1/2 LM.
S5= 1-~~3~~ X 8 1/2 LM.

ITEM NO.	ITEM DESCRIPTION
01. 1.000000	1.000000
02. 1.000000	1.000000
03. 1.000000	1.000000
04. 1.000000	1.000000

WF = G 40 21-50 W
 MIN. BOL. TENSILE CAPACITY = 1,000 LBS

C1 = 1000 4 X 1/2 X 1/2
 TOP PLATE SHGWS WELDED TO ST BEAM ABOVE
 BOLT PLATE SHGWS - 3/8" X 1/2 X 1/2
 C2 = 1000 4 X 1/2 X 1/2
 TOP PLATE SHGWS WELDED TO ST BEAM ABOVE
 BOLT PLATE SHGWS WELDED TO ST BEAM BELOW
 P1 = 1000 4 X 1/2 X 1/2
 P2 = 1000 4 X 1/2 X 1/2

NOTES

TWO-STEP TEST METHOD FOR PRELIMINARY SCREENING OF SOLID WASTE

SAMPLE ALL DISPOSITIONS OF SOLIDS, INCLUDING ON-DISPOSAL TO THE APPROPRIATE FACILITY

DISPOSALS ARE APPLICABLE

38 COTTON DRIVE
MISSISSAUGA,
L5G 2H4

SINGLE FAMILY DWELLING ON
36 COTTON DRIVE
MISSISSAUGA.

DWG TITLE:
A-200
FRONT NORTH
ELEVATION

SCALE: 1/100

DATE
15.01.2017

JOB. NO:

100

CHECKED BY:

DWG. NO:

6 OF 11

MURAT OZGUR DESIGN BUILD

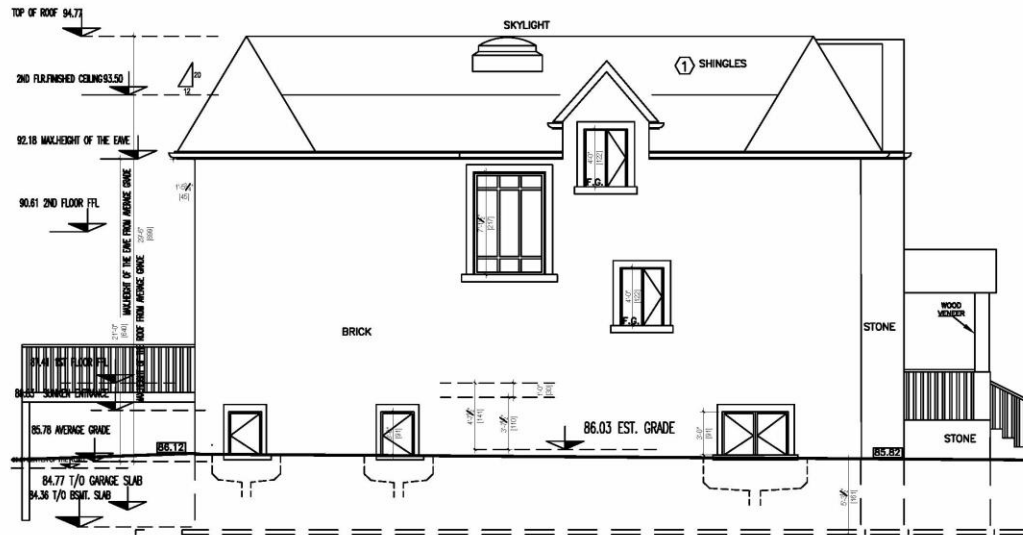
BCIN # 107211

М. О. Бук

MURAT OZGUR

BCIN# 104649

М.О.Дж



BRICK: RED/BURGUNDY
STONE: INDIANA LIMESTONE
WINDOWS: VINYL, COMMERCIAL BROWN
EAVES/FASCIA/DOWN SPOUTS: COMMERCIAL BROWN
EXTERIOR DOOR: OAK, DARK WALNUT
SHINGLES: KO CAMBRIDGE DRIFTWOOD

[illegible]

APPENDIX A4: Curriculum Vitae of the Author

CURRICULUM VITAE: James R. Bailey

EDUCATION

- Bachelor of Architecture, Carleton University, Ottawa. 1975. General Scholarship in Architecture, 1971.
- Premier Degré Superier de Musique (Orgue), Schola Cantorum, Paris, 1978

PROFESSIONAL ASSOCIATIONS

- Ontario Association of Architects
- Royal Architectural Institute of Canada
- Canadian Association of Heritage Professionals (CAHP)
- Royal Canadian College of Organists

PROFESSIONAL WORK HISTORY

- 1992-present Principal of James Bailey Architect, Toronto
- 1989-1992 Managing Architect with C.A. Ventin Architect Ltd., in charge of the firm's Toronto office and projects
- 1988-1989 Managing Architect with Lambur Scott Architects Inc., in charge of the firm's Toronto office and projects
- 1987-1988 Senior Architect with Douglas J. Cardinal Architect Limited, in charge of the firm's Newmarket, Ontario, office
- 1983-1987 Senior Architect with Douglas J. Cardinal Architect Limited, in Edmonton, then in Ottawa (after office move)
- 1981-1983 Senior Architectural Designer with Briskie Kasian Architects, Edmonton
- 1978-1981 Project Coordinator with Douglas J. Cardinal Architect Limited, Edmonton
- 1975-1977 Architectural Designer, ARCOP Associates, Ottawa
- 1975 Architectural Designer, Urbanetics, Ottawa



James Bailey
Architect

49 MELBOURNE AVE
TORONTO, ONTARIO
M6K 6K6

VOICE: [416] 537-4140

FAX: [416] 537-0405

E-MAIL:

jbarch@sympatico.ca

James R. Bailey
O.A.A., M.R.A.I.C., C.A.H.P.
Albena Bakalov
O.A.A., M.R.A.I.C.

COMMUNITY INVOLVEMENTS & TEACHING

- Royal Canadian College of Organists, National President, 2014-16
- Canadian Association of Heritage Professionals, Member of the Board, 2003-2005.
- Sheridan College, Professor of Architecture, 2001-2012
- Public Complaints Committee, Ontario Association of Architects, 1998-2001.
- Standing Committee for the administration of the Fabric Fund of Holy Trinity Church, Toronto, 1995-2003.
- Advisory Board, St. Chad's Anglican Church, Toronto, 1993-1995, Long-Range Planning Committee Chairman.

AWARDS

- Special Recognition Award, Heritage Mississauga, 2012
- Award of Merit, Heritage Toronto, The Allstream Centre, 2010
- Award of Distinction for Envelope Design, Allstream Centre, The Ontario Building Envelope Council, 2009
- Best of the Best Award, Toronto Construction Association, The Allstream Centre. 2009

SELECTED ARCHITECTURAL PROJECTS

Museums/Art Galleries

- *Edmonton Space Science Centre
- *Canadian Museum of Civilization, Hull
- *St. Albert Municipal Museum, Alberta
- Macaulay Church Museum, Picton

Performing Arts Centres

- *Arden Theatre, St. Albert, Alberta
- *Aurora Theatre, Spruce Grove, Alberta
- *Theatre of the CMC, Hull, Quebec
- Capitol Theatre, Port Hope, Ontario
- Artword Theatre, Toronto

Building Envelope Upgrades

- Buddies in Bad Times Theatre, Toronto
- **Peel Court House, Brampton
- **Metro West Detention Centre, Etobicoke
- **Locke Memorial Library, Toronto
- Allstream Centre, Toronto
- St. John's United Church, Alliston
- Food Building, Exhibition Place
- Princes' Gates, Exhibition Place

Barrier-free Access/Life-safety Upgrades

- **Toronto Old City Hall

- **Yorkville Branch Library, Toronto
- **Main Street Branch Library, Toronto
- Locke Memorial Library, Toronto
- Trinity Church, Port Credit
- St. John's United Church, Alliston
- Trace Manes Community Centre, Toronto

Offices

- *St. Albert Civic and Cultural Centre
- Public Service Commission, Ottawa
- Globe & Mail, Toronto
- Hitchman & Sprigings, Toronto
- *York Administrative Centre, Newmarket

Housing/ Live-Work Facilities

- **Armagh House, Mississauga
- Garden Residence, Aurora
- Webb Residence, Toronto
- Looney Residence, Toronto
- Kersey Residence, Toronto
- Hamilton Studio, South Mountain
- Dubil Residence, Toronto
- 81 Portland Live/work Studios, Toronto
- Hitchman Residence, Toronto
- Rose Residence, Port Credit
- O'Leary Residence, Toronto

Schools: New/Renovations

- Bowmore Road School, Toronto
- **Charles Fraser P.S., Junior, Toronto

- George Harvey High School, City of York
- Jones Ave Adult Education Centre, Toronto
- Wycliffe College, U. of T., Toronto

Schools: Exterior Upgrades

- Davisville Public School, Toronto
- North Toronto CI, Toronto
- Pape Public School, Toronto
- Palmerston Public School, Toronto
- John Ross Robertson Public School, Toronto

Public Library Renovations

- Parkdale Branch Library, Toronto
- Bloor Gladstone Branch Library, Toronto
- Yorkville Branch Library, Toronto
- Locke Memorial Library, Toronto
- Runnymede Branch Library, Toronto
- College Shaw Library, Toronto
- Saunderson Branch Library, Toronto
- Pape Danforth Library, Toronto
- Gerrard Ashdale Library, Toronto
- St. Clair/Silverthorn Library, Toronto
- Palmerston Branch Library, Toronto

Renovations for Exhibition Place, Toronto

- General Services Building
- Direct Energy Centre
- Allstream Centre (Automotive Building)
- Princes' Gates

Churches: New/Renovations/Additions

- Danish Lutheran Church, Toronto
- Devon Alliance Church, Alberta
- St. Luke's Church, East York
- St. Cuthbert's Church, Toronto
- St. John's Church, Whitby
- Faith United Church, Courtice
- Grace Ev. Lutheran Church, Oshawa
- Trinity Church, Port Credit
- St. Luke's Lutheran, Port Credit
- St. John's United Church, Alliston
- St. Paul's United Church, Bowmanville
- All Saints' Anglican Church, King City
- Knox Presbyterian Church, Burlington
- St. Andrew's Memorial P.C., Port Credit
- Trinity United, Peterborough
- St. John's Church, York Mills
- Don Valley Bible Chapel, Toronto
- Cathedral Church of St. James
- 3 MacDowell Street, Toronto
- Allstream Centre, Exhibition Place
- Rackus Studio, Clarkson
- Boulder Villa, Clarkson
- Princes' Gates, Exhibition Place
- Macaulay Church Museum, Picton
- St. Clair Reservoir Valve House and Portal Building
- Harrison Waterworks Reservoir
- Church of the Holy Trinity, Toronto
- St. James' Cathedral, Toronto
- Scarborough Museum
- The Guild Monument Relocation

* Work undertaken while in the employ of Douglas J. Cardinal Limited, Ottawa

** Work undertaken while Managing Architect, and Architect-of-Record, with Carlos Ventin Architect

Laboratories

- I-Fire Technology, Toronto
- Hemosol, Toronto
- Therapure, Mississauga

Historic Restorations

- **Peel County Court House, Brampton
- **St. John's Anglican Church, Ancaster
- St. John's Church, West Toronto
- St. John's United Church, Alliston

STUDIES (Condition Studies/Heritage Impact Studies/Barrier-Free Access Studies)

- Building Condition Assessment for 12 Alexander St., for the City of Toronto Heritage and Museums, 2017
- Heritage Statement for 75 Yorkville Avenue, for the Yorkville Ratepayer's Association, 2015
- Heritage Impact Study of 1109 and 1115 Clarkson Road North, Mississauga, 2014
- Building Assessment Study for the Scarborough Historic Museum, Toronto, 2014
- Building Assessment Study for the Coliseum Complex and the Direct Energy Centre, Exhibition Place, 2013
- Building Code Conformance Study, Tarragon Theatre, Toronto, for the management and Board of Directors of Tarragon Theatre, 2013
- Heritage Impact Study for 1183 Dufferin Street, Toronto, for We Care Homes, 2012
- Heritage Impact Study for 1380 Glenwood Drive, Mississauga, Ontario, for Dr. Mehri Habib, 2012
- Building Code/Condition Assessment, Fulford Preparatory College, Merrickville, Ontario for Mr. Francois Bernard, Owner, 2012
- Heritage Impact Study for 149 Tremaine Road, Milton, for Royal Park Homes, 2011
- Heritage Impact Study for 94 Peru Road, Milton, for Humphries Planning Group, 2011
- Long-range Planning Study, Tarragon Theatre, Toronto, for the management and Board of Directors of Tarragon Theatre, 2010
- Heritage Impact Study for 5514 Fifth Line Road, Milton, for Sempronia Estates Inc., 2010
- Heritage Impact Study for 1336 Britannia Road, and 5553 Fourth Line Road, Milton, for Orianna Glen Home Corp., 2009
- Heritage Impact Study for 6390 Fifth Line Road, Milton, for Milton 7-5 Holdings Inc., 2009
- Heritage Impact Study for the Davis-Minardi House, 3079 Neyagawa Boulevard, Oakville for the Davis-Minardi Corporation, 2009
- Heritage Impact Study for 2554 Mississauga Road, Mississauga prepared for Dr. Alaa Al Tamimi, 2008
- Heritage Impact Study for 1207 Lorne Park Road, Mississauga, prepared for Mr. Claudio Prosocco, 2008
- Heritage Impact Study for 3051 Victory Crescent, Mississauga prepared for Nirmal Sidhu, 2007
- Heritage Impact Study for 1190 Kane Road, Mississauga, prepared for Gemini Urban Design, 2006
- Heritage Impact Studies for 1998 and 2030 Lakeshore Road, Mississauga, prepared for Gemini Urban Design, 2006

- Heritage Impact Statement, 2301/2303 Stanfield Road, Mississauga, prepared for Moldenhauer, 2006
- Automotive Building Condition Audit, prepared for the Board of Governors, Exhibition Place, Toronto, 2005
- A Masterplan Study for Knox Presbyterian Church, Burlington, Prepared for the Long-range Planning Committee, 2004
- Heritage Impact Statement, Horticulture Building, prepared for Musik Clubs Inc. who wished to use the Beaux Arts Exhibit Hall as a night club, 2004.
- A Condition Audit for Rosedale Presbyterian Church, Prepared for the Property Committee, Toronto, 2003
- Arts Centre Feasibility Study, prepared for the Town of Leamington, Ontario, 2003.
- A Barrier-Free Access Study for St. John's United Church, Alliston, prepared for the Property Committee, 2003.
- A Barrier-Free Access Study for Kimbourne Park United Church, Toronto, prepared for the Accessibility Committee, 2002.
- A Feasibility Study for the conversion of the Truax Lumber Building into a Performing Arts Centre for the Town of Leamington, Ontario, 2002.
- Long Range Redevelopment Master Plan for Christ Church, Stouffville", for the Rector and Wardens of the Parish of Christ Church Anglican Church, Stouffville.
- Heritage Impact Statement, Two Georgian style, Pre-Confederation Houses, located at 72/74 Elm Street, Toronto, prepared for Toronto Hospital for Sick Children, 2000.
- A Barrier-Free Access Study for St. John's Church, York Mills, prepared for the Rector and Wardens of St. John's Church, York Mills, Toronto, 1999.
- A Space Needs Study and a Space Utilization Survey for the Parishes of St. John's Church, W.T. & St. Paul's Church, Runnymede for the Amalgamation Property Committee.
- Condition Surveys of three Branch Libraries: Deer Park, Northern District and Sanderson Branches for the Toronto Public Library, 1998.
- "A Feasibility Study for developing additional Residential Spaces at Wycliffe College, University of Toronto" prepared for the Property Committee of the College, 1997.
- "A Study of the Implications of accommodating Garbage Trucks inside the Food Building, Exhibition Place", prepared for the Capital Works Department of the CNE, 1997.
- "A Condition Survey of St. Bartholomew's Church", prepared for the Rector and Wardens of the Church of St. Bartholomew, Toronto, 1996.

- "A Building Study of Powell's House, Appleby College", prepared for the Board of Appleby College, Oakville, Ontario, 1995.
- "Condition Study of the Church of the Transfiguration, Toronto", prepared for the Rector and Wardens of the Church of the Transfiguration, Toronto, 1995
- "Pre-Engineering & Costing Study for Building Envelope Repairs & Barrier-free Access Improvements to the Food Building & Halls of Fame Building, Exhibition Place, Toronto", prepared for the Capital Works Department of Exhibition Place, 1995.
- "Condition Survey of the Buildings of the Parish of Georgina in the Diocese of Toronto", prepared for the Diocese of Toronto, 1994.
- "Building Code Impact Study related to the development of a Night Club in the MacLean Hunter Building, North York", prepared for Luxor Entertainment Corporation, 1994.

REPORT 4-2017

To: CHAIR AND MEMBERS OF HERITAGE ADVISORY COMMITTEE

The Meadowvale Village Heritage Conservation District Advisory Sub-Committee presents its fourth report for 2017 and recommends:

MVHCD-0005-2017

That the request to alter the property at 1155 Willow Lane, as described in the Memorandum dated November 6, 2017 from Mark Warrack, Manager, Culture and Heritage Planning, and the attached drawings, be approved.

MVHCD-0006-2017

That the request to alter the property at 1059 Old Derry Road, as described in the Memorandum dated November 6, 2017 from Mark Warrack, Manager, Culture and Heritage Planning, and the attached drawings, be approved.

MVHCD-0007-2017

That the request to alter the property at 1036 Old Derry Road, as described in the Memorandum dated November 7, 2017 from Mark Warrack, Manager, Culture and Heritage Planning, and the attached drawings, be approved.

MVHCD-0008-2017

That the Memorandum dated November 8, 2017 from Mark Warrack, Manager, Culture and Heritage Planning, with respect to a request to alter 1066 Old Derry Road be received, and that the following be approved:

1. That the existing sliding glass doors at the rear of the dwelling, be approved.
2. That the original type of garage door be installed, as previously approved.
3. That the landscaping be completed as soon as possible and, in the meantime, steps be taken to place a barrier to prevent parking on the grassed area in the front of the dwelling.
4. That a gravel drive way be recommended instead of the originally approved interlocking stone drive way.

MVHCD-0009-2017

That the resignation dated November 7, 2017 from Colleen Newmarch, be received.