
Heritage Advisory Committee

Date

2017/07/11

Time

9:30 AM

Location

Civic Centre, Council Chamber - Second Floor,
300 City Centre Drive, Mississauga, Ontario, L5B 3C1 Ontario

Members

Councillor George Carlson, Ward 11 (Chair)
Rick Mateljan, Citizen Member (Vice-Chair)
Councillor Carolyn Parrish, Ward 5
Michael Battaglia, Citizen Member
Elizabeth Bjarnason, Citizen Member
Robert Cutmore, Citizen Member
David Dodaro, Citizen Member
Lindsay Graves, Citizen Member
James Holmes, Citizen Member
Cameron McCuaig, Citizen Member
Melissa Stolarz, Citizen Member
Matthew N. Wilkinson, Citizen Member

Contact

Mumtaz Alikhan, Legislative Coordinator, Legislative Services
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mumtaz.alikhan@mississauga.ca

NOTE: To support corporate waste reduction efforts the large
appendices in this agenda can be viewed at:
<http://www.mississauga.ca/portal/cityhall/heritageadvisory.ca>

Find it Online

<http://www.mississauga.ca/portal/cityhall/heritageadvisory>

-
1. CALL TO ORDER
 2. APPROVAL OF AGENDA
 3. DECLARATION OF CONFLICT OF INTEREST
 4. MINUTES OF PREVIOUS MEETING
 - 4.1. Approval of HAC Minutes - June 13, 2017
 5. DEPUTATIONS
 6. PUBLIC QUESTION PERIOD - 15 Minute Limit *(In accordance with Section 43 of the City of Mississauga Procedure By-law 0139-2013, persons who wish to address the Heritage Advisory Committee about a matter on the Agenda may ask their question limiting it to 5 minutes, as the public question period total limit is 15 minutes.)*
 7. MATTERS TO BE CONSIDERED
 - 7.1. Request to Alter a Heritage Designated Property: 915 North Service Road (Ward 1)
 - 7.2. Request to Alter a Heritage Designated Property: 929 Old Derry Road West (Ward 11)
 - 7.3. Request to Alter a Heritage Designated Property: 7080 Gaslamp Walk (Ward 11)
 - 7.4. Request to Demolish a Heritage Listed Property: 3098 Merritt Avenue (Ward 5)
 - 7.5. Request to Demolish a Structure on a Heritage Listed Property: 1695 Dundas Street West (Ward 6)
 - 7.6. An information report on the removal of the heritage properties located on Clarkson Road North and a review of the Heritage Permit process.
 8. SUBCOMMITTEE UPDATES
 - 8.1. Heritage Designation Sub-Committee
 - 8.2. Public Awareness Sub-Committee
 9. INFORMATION ITEMS
 10. OTHER BUSINESS
 11. DATE OF NEXT MEETING - September 5, 2017
 12. ADJOURNMENT

City of Mississauga
Minutes



Heritage Advisory Committee

Date

2017/06/13

Time

9:30 AM

Location

Civic Centre, Council Chamber,
300 City Centre Drive, Mississauga, Ontario, L5B 3C1 Ontario

Members Present

Councillor George Carlson, Ward 11 (**Chair**)
Rick Mateljan, Citizen Member (**Vice-Chair**)
Councillor Carolyn Parrish, Ward 5
Michael Battaglia, Citizen Member
Elizabeth Bjarnason, Citizen Member
Robert Cutmore, Citizen Member
James Holmes, Citizen Member (left at 10:42)
Cameron McCuaig, Citizen Member
Melissa Stolarz, Citizen Member
Matthew N. Wilkinson, Citizen Member

Members Absent

David Dodaro, Citizen Member
Lindsay Graves, Citizen Member

Staff Present

Mark Warrack, Manager, Culture and Heritage Planning
Paula Wubbenhorst, Senior Heritage Coordinator, Culture Division
Cecilia Nin Hernandez, Heritage Coordinator, Culture Division
Mumtaz Alikhan, Legislative Coordinator

Find it online

<http://www.mississauga.ca/portal/cityhall/heritageadvisory>

1. CALL TO ORDER – 9:33 am
2. APPROVAL OF AGENDA
APPROVED (Councillor C. Parrish)
3. DECLARATION OF CONFLICT OF INTEREST – Nil.
4. MINUTES OF PREVIOUS MEETING
- 4.1. Approval of Minutes of Meeting held on May 9, 2017
APPROVED (R. Cutmore)
5. DEPUTATIONS
- 5.1. Old Port Credit Village Heritage Conservation District (HCD) Plan Review - Peter Stewart, George Robb Architect

Peter Stewart, George Robb Architect, and Nick Bogaert, MHBC Planning, provided an overview of the project to update the Old Port Credit Village Heritage Conservation District Plan (HCD Plan) which guides changes in the area to ensure the character is maintained. Mr. Stewart said the review will look at an expansion of the HCD area and provide clear and visually accessible design guidelines to improve the current heritage permit application process, provide guidelines for green initiatives, First Nations recognition, public realm improvements, and increased promotion to identify Old Port Credit Village as a place of interest. Mr. Bogaert spoke to a proposed recommendation to extend the HCD boundary to include the Port Credit Marina and the City owned lands on the east side, tax incentives, and building inventory. He outlined the next steps will include more public consultations prior to a presentation of the draft HCD Plan to the Committee in early 2018 and Council in Spring 2018.

The Committee commented as follows:

- public realm lighting strategy;
- with the proposed expansion of the boundary, look at the possibility of combining the original land use by First Nations with the European influence;
- keep the older photographs for people to see exactly how the area has evolved;
- extend Mississauga Road Scenic Route down to the Lake;
- ensure transition between both sides of the Mississauga Road Scenic Route;
- that the proponent of the Port Credit West Village development (70 Mississauga Road South) present the heritage aspects of their proposal to the Committee.

RECOMMENDATION

HAC-0044-2017

1. That the deputation by Peter Stewart, George Robb Architect, with respect to the Old Port Credit Village Heritage Conservation District Plan Review to the Heritage Advisory Committee dated June 13, 2017, be received;

2. That the proponent of the Port Credit West Village development (70 Mississauga Road South) present the heritage aspects of their proposal to the Heritage Advisory Committee.

RECEIVED (R. Cutmore)

6. PUBLIC QUESTION PERIOD – Nil.
Lilia D'Ovidio spoke with respect to Item 5.1. noting her concern that the west side of the proposed Port Credit West Village development is beautifully buffered and asked that the east side be similarly buffered. Councillor Carlson said that there is a lot of work to be done and advised Ms. D'Ovidio to contact her local Councillor regarding her concerns.

7. MATTERS TO BE CONSIDERED

- 7.1. Request to Alter a Heritage Designated Property: 29 Port Street West (Ward 1)

The Committee felt that a simplified approach would be more appropriate as the current proposal makes the massing of the building incongruous and not typical of the neighbourhood. Ms. Cecilia Nin Hernandez responded that the spectrum to interpretation is wide in the Heritage District Plan which is a guideline, not a by-law, and therefore would not be defensible if the request was denied.

The Committee requested the Owner, Mr. Tyler Goss, to consider working with staff to reduce the visual impact by lowering the height of the roof ridge. Mr. Goss expressed his willingness to do so.

RECOMMENDATION

HAC-0045-2017

1. That the proposed alteration to 29 Port Street West, as per the Corporate Report from the Commissioner of Community Services, dated May 18, 2017, be approved.
2. That if any changes result from other City review and approval requirements, such as, but not limited to, building permit, Committee of Adjustment or site plan approval, a new heritage permit application may be required. The applicant is required to contact Heritage Planning at that time to review the changes prior to obtaining other approvals and commencing construction.
3. That the Heritage Advisory Committee review the Committee of Adjustment application once it is submitted to the City by the applicant.
4. That the Owner be requested to work with staff to reduce the visual impact of the side addition by considering to lower the height of the roof ridge and change the side gable roof to a hip roof.

APPROVED (M. Wilkinson)

7.2. Request to Alter a Heritage Designated Property: 39 Peter Street South (Ward 1)

Cecilia Nin Hernandez, Heritage Coordinator, distributed Appendix 2, an Application for Minor Variance which was missing from the agenda for the Committee's information.

RECOMMENDATION

HAC-0046-2017

1. That the proposed alteration to 39 Peter Street South, as per the Corporate Report from the Commissioner of Community Services, dated May 18, 2017, be approved.
2. That if any changes result from other City review and approval requirements, such as, but not limited to, building permit, Committee of Adjustment or site plan approval, a new heritage permit application may be required. The applicant is required to contact Heritage Planning at that time to review the changes prior to obtaining other approvals and commencing construction.

APPROVED (R. Mateljan)

7.3. Request to Alter 1723 Birchwood Drive (Ward 2)

Corporate Report dated May 18, 2017 from the Commissioner of Community Services.

RECOMMENDATION

HAC-0047-2017

That the request to install a shed at 1723 Birchwood Drive, as per the report from the Commissioner of Community Services, dated May 18, 2017, be approved with the caveat that the potentially impacted trees continue to be maintained.

APPROVED (R. Cutmore)

7.4. Request to Alter the City Boulevard in front of 111 Lakeshore Road West

Corporate Report dated May 18, 2017 from the Commissioner of Community Services.

RECOMMENDATION

HAC-0048-2017

That the request to alter the City boulevard in front of 111 Lakeshore Road West, as per the report from the Commissioner of Community Services, dated May 18, 2017, be approved.

APPROVED (M. Stolarz)

7.5. Correction to Heritage Register Changes Pertaining to Reduction of Mineola Cultural

Landscape (Ward 1)

Ms. Wubbenhorst spoke to the minor corrections and that the residents will be notified in consultation with Ward 1 Councillor.

RECOMMENDATION

HAC-0049-2017

That the corrections to the Heritage Register pertaining to the Reduction of the Mineola Cultural Landscape, as per the Corporate Report from the Commissioner of Community Services, dated May 31, 2017, be approved.

APPROVED (R. Mateljan)

8. SUBCOMMITTEE UPDATES

8.1. Heritage Designation Sub-Committee - Nil8.2. Public Awareness Sub-Committee - Nil

9. INFORMATION ITEMS

Ms. Wubbenhorst distributed and spoke to the Memorandum dated June 1, 2017 from Paul Damaso, Director, Culture Division.

RECOMMENDATION

HAC-0050-2017

That the Memorandum dated June 1, 2017 from Paul Damaso, Director, Culture Division, with respect to amending Subsection 89(8) of Council Procedure By-law 0139-2013, as amended, delegating summer and election recess authority to the Director of Culture Division (or designate) for specific matters under the *Ontario Heritage Act*, be received for information.

RECEIVED (Councillor C. Parrish)

10. OTHER BUSINESS

- (a) In response to Councillor Parrish with respect to formal submissions to Committee of Adjustment from the Committee or the Chair, Councillor Carlson responded that he will consult Legal Services for advice on this matter.
- (b) Ms. Wubbenhorst sought the Committee's consideration with respect to the design of the heritage designation plaques; members of the Committee felt that no changes were warranted at this time.

11. DATE OF NEXT MEETING – July 11, 2017, Hearing Room, 2nd Floor.

12. ADJOURNMENT – 11:20 am

DRAFT

City of Mississauga

Corporate Report



Date: 2017/06/19

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
Community Services

Originator's files:

Meeting date:
2017/07/11

Subject

Request to Alter a Heritage Designated Property: 915 North Service Road (Ward 1)

Recommendation

That the request to alter the heritage designated property located at 915 North Service Road as outlined in the Corporate Report dated June 19, 2017, from the Commissioner of Community Services entitled *Request to Alter a Heritage Designated Property: 915 North Service Road (Ward 1)* be approved, subject to the following conditions:

- a. That the approval is without prejudice to charges that are pending before the courts related to this property,
- b. That, prior to the issuance of the heritage permit for the subject proposal, the owner is to submit a heritage permit application for the conservation work to the Hedge farmhouse, accompanied by a detailed Heritage Management Conservation Plan, building permit drawings for the Hedge farmhouse and the proposed new detached garage shown on Appendix D of the amended HIA report submitted (Appendix 3),
- c. That staff send comments to the Committee of Adjustment noting that, if the severance is approved by the Committee of Adjustment, conditions be imposed and the City enter into appropriate agreements with the owner in order to ensure the following:
 - i. That prior to the approval of the severance the heritage designation by-law be amended to reflect the new property boundary and that the owner provide the City with a survey and land description of the new lot boundaries to this end,
 - ii. That the building permit drawings for the new lots be circulated to Heritage Planning for review and comment,
- d. That if any changes result from other City review and approval requirements, such as but not limited to building permit, committee of adjustment or site plan approval, a new heritage permit application will be required. The applicant is required to contact heritage planning at that time to review the changes prior to obtaining other approvals and commencing construction.

Report Highlights

- The property is designated under Part IV of the Ontario Heritage Act.
- The hedge farmhouse itself and its orientation/location are the main heritage attributes (refer to appendix 1)
- The property is currently vacant and in need of conservation work. There are pending charges at the courts related to this property. The information on this report, appendices and recommendation are without prejudice to the pending charges.
- The owner has submitted a heritage permit application to alter the property by demolishing two garage structures, subdividing it into three parcels and to construct two new detached homes facing Ribston Road, while conserving the Hedge Farmhouse in situ.
- The proposal should be approved with conditions as set out in the recommendations section of this report.

Background

The William Hedge Farmhouse was built on the subject property in 1928. The City designated the property under Part IV of the Ontario Heritage Act under by-law 021-2016. The heritage designation by-law is included as Appendix 1. In summary, the cultural value of the property lies in it being a rare example of the Craftsman Bungalow style, with buff, rough cut limestone cladding, and interior features of its time as outlined in the by-law. The house was designed by Port Credit Architect, Dixie Cox Cotton, supporting its design and historical value. The property is subject to the provisions of the Ontario Heritage Act.

Section 33 of the Act requires Council permission to alter. The owner of the subject property has submitted a heritage permit application to alter the property by demolishing two garage structures, subdividing it into three parcels and to construct two new detached homes facing Ribston Road, while conserving the Hedge Farmhouse in situ. The proposal is described in the submitted "Amendment to Heritage Impact Statement (2015), 915 North Service Road, City of Mississauga, by Megan Hobson, built heritage consultant" (Appendix 3), more specifically its appendix D.

The house is currently vacant. There are pending charges at the courts related to this property. The information on this report, appendices and recommendation are without prejudice to the pending charges. The owner and a heritage planning staff have met without prejudice on May 11, 2017 where the owner requested a phased approach of two separate heritage permit submittals in order to obtain approvals for the subject proposal.

Comments

The report submitted entitled "Amendment to Heritage Impact Statement (2015), 915 North Service Road, City of Mississauga, by Megan Hobson, built heritage consultant" (Appendix 3),

more specifically its appendix D, seeks to maintain and conserve the Hedge House in situ while developing the property into three residential lots. This is the owner's and heritage consultant's preferred option for development of the property. The alternative option A shown in the submitted amendment HIA, involves moving the house, which is not recommended as it would put the heritage resource at risk and alter its heritage attributes. Staff concurs with the preferred option to leave the house in situ. The house is currently vacant and in need of conservation work, a heritage permit is required for this work. In light of the phased approach for the heritage approvals requested by the owner as noted in the background section of this report, a separate heritage permit submittal accompanied by a Heritage Management Conservation Plan and building permit drawings is required to be submitted in the near future to review the details of the heritage conservation plans for the Hedge house and the proposed new detached garage (refer to Appendix D of Appendix 3 attached). It is recommended that a condition be placed for the owner to submit a new heritage permit application addressing the proposed work to the house just mentioned, prior to the issuance of a heritage permit for the subject request to alter.

A minor variance for a 2.5 metre reduction of the required rear set back is noted in the HIA as well (proposed 5 metre setback to the proposed rear lot line). The proposed severance and anticipated minor variance for the reduced setback does not detract from the heritage attributes of the property, as the house itself will remain within an ample lot and with the same orientation. The proposed demolition of the two rear garage structures does not detract from the cultural value of the property as set out in the designation by-law, as the structures are not identified as heritage attributes. The preferred proposal also includes the removal of mature trees and vegetation; however these features are not included in the designation by-law. The HIA submitted concluded that the trees and vegetation do not have cultural heritage value and suggests that new trees be planted along the new rear property lines to mitigate this loss. Heritage planning staff concurs with the proposal of planting new replacement trees. The owner is advised to obtain the required City permits prior to removal of the trees.

In order to ensure that the construction of the new homes proposed to front onto Ribston Road results in a sympathetic adjacent intervention to the Hedge farmhouse, conditions should be placed on the consent (severance), should the Committee of Adjustment decide to approve it. It is recommended that staff send comments reflecting a Heritage Advisory Committee recommendation requesting that conditions on the consent approval (severance) be imposed such that prior to the approval of the severance the heritage designation by-law be amended to reflect the new property boundary and that the owner provide the City with a survey and land description of the new lot boundaries to this end. Additionally, a condition should be requested of the consent application that the building permit drawings for the new lots be circulated to Heritage Planning for review and comment.

Financial Impact

Not applicable.

Conclusion

The proposal to demolish the two detached garages, and sever the property into three lots, construct two detached homes in the rear two lots to front onto Ribston Road, and reduction in rear yard setback to the Hedge farmhouse do not detract from the cultural heritage value as set out in the designation by-law; therefore they should be approved. Given that the Hedge farmhouse is currently vacant and in need of conservation work, prior to the issuance of a heritage permit for the subject proposal, the owner is to submit a heritage permit application for the conservation work to the Hedge farmhouse, accompanied by a detailed Heritage Management Conservation Plan, building permit drawings for the Hedge farmhouse and the proposed new detached garage shown on Appendix D of the amended HIA report submitted (Appendix 3). It is further recommended that the Committee of Adjustment be requested to impose conditions as set out in the recommendation section of this report should it decide to approve a future consent and variance application to achieve the proposal depicted in appendix D to Appendix 3 of this report.

Attachments

Appendix 1: Designation By-law

Appendix 2: Heritage Impact Assessment, 2015

Appendix 3: Amendment to Heritage Impact Assessment



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: Cecilia Nin Hernandez, Heritage Coordinator



THE CORPORATION OF THE CITY OF MISSISSAUGA

BY-LAW NUMBER 0021-2016

A By-law to designate the William Hedge Farmhouse
located at 915 North Service Road as being of cultural
heritage value or interest

WHEREAS the *Ontario Heritage Act*, R.S.O. 1990, C. O.18, as amended ("Heritage Act") authorizes the Council of a municipality to enact By-laws to designate real property, including all the buildings and structures thereon, to be of cultural heritage value or interest;

AND WHEREAS Council for The Corporation of the City of Mississauga (the "City") approved the designation of the property known as the William Hedge Farmhouse located at 915 North Service Road in the city of Mississauga (the "Property") as being of cultural heritage value or interest through Resolution 0196-2015;

AND WHEREAS in accordance with the requirements of the Heritage Act, a Notice of Intention to designate the Property was published and served and no notice of objection to its designation was received by the Clerk of The Corporation of the City of Mississauga;

NOW THEREFORE the Council of The Corporation of the City of Mississauga hereby ENACTS as follows:

1. That the property, including all the buildings and structures thereon, known as the William Hedge Farmhouse, located at what is municipally known as 915 North Service Road in the city of Mississauga and legally described in Schedule 'A' attached hereto (the "Property"), is hereby designated as being of cultural heritage value or interest under Part IV of the *Ontario Heritage Act*, R.S.O. 1990, C. O.18, as amended.
2. That the reasons for designating the Property are duly set out in Schedule 'B' attached hereto.
3. That the City Clerk is hereby authorized to cause a copy of this By-law to be served upon the owner of the Property and upon the Ontario Heritage Trust and to cause notice of this By-law to be published in a newspaper having general circulation in the city of Mississauga.
4. That the City Solicitor is hereby directed to register a copy of this By-law against the Property in the proper land registry office.
5. That Schedules 'A' and 'B' form an integral part of this By-law.

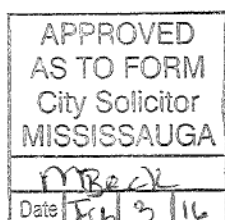
ENACTED AND PASSED this 10 day of February, 2016.

Bonnie Crombie

MAYOR

Crystal Green

CLERK



SCHEDULE 'A' TO BY-LAW 0021-2016

Summary: All of Block H, Registered Plan 481
Part of Lot 9, Concession 1, South of Dundas Street
(To be designated under the Ontario Heritage Act)

(Ward 1, City Zone 13, in the vicinity of Cawthra Road and North Service Road)

Legal Description: In the City of Mississauga, Regional Municipality of Peel, (Geographic Township of Toronto, County of Peel), Province of Ontario and being composed of all of Block H, Registered Plan 481 and Part of Lot 9, Concession 1, South of Dundas Street, of the said Township, as in Instrument RO1073948.



Alnashir Jeraj
Ontario Land Surveyor

SCHEDULE "B"

Reasons for Identification

Description of Property

The property known as 915 North Service Road is located on the North Side of North Service Road on Concession 1, Part of lot 9 in the City of Mississauga. It is located in the vicinity of Westfield Drive and North Service Road.

The property contains a single family house, a detached garage and a shed. The single family dwelling - the William Hedge farmhouse - is most easily identified by its one and half storey form, side gabled roof with two dormers, and buff limestone cladding extending to the top of the windows on the second floor. The front entrance faces south, it has two bay windows on the ground floor (south and west side), a front porch with thick stone columns. The house is well set back on the lot.

Statement of Cultural Heritage Value or Interest

The William Hedge Farmhouse's cultural heritage value lies in it being a rare example of the Craftsman Bungalow style within the City of Mississauga with buff, rough cut (rusticated) limestone cladding sourced in the vicinity of the municipality. It has interior features telling of the era including cabinetry and millwork, plumbing and heating fixtures. Built in 1928, its architectural form, style and detailing reflect the design work of a local architect: Port Credit born and raised, Dixie Cox Cotton.

The Cultural Heritage Value also lies in its historic association with this architect. He was the grandson of area pioneer Robert Cotton and the nephew of Dr. Dixie Beaumont Cotton, after whom the village of Dixie was named. Dixie Cox Cotton was active in the community: he was maintenance Engineer for the St. Lawrence Starch Co. (a major locally based Canadian Industry) for over twenty years and is attributed for the design of various buildings in the community, reflecting the mainstream architectural design ideas of the time. These were based on references to vernacular and classical architecture within the British Empire, high quality craftsmanship and design, and integration of the arts and architecture as expressed in the Craftsman Bungalow, Edwardian, and Institutional and Commercial Period Revival buildings. Design ideas were carried into interior elements of the house displaying attention to detail in interior design and craftsmanship such as stonework and millwork. The house therefore demonstrates his work, the work of a significant architect to the community. The William Hedge house also has the potential to yield information to the understanding of a community. The farmhouse was built prior to the existence of the Queen Elizabeth Way as a highway, and was retained by the family within the Applewood subdivision of 1953, maintaining its orientation of its original frontage on Queen Elizabeth Highway, known as Middle Road at the time the house was designed.

Description of Heritage Attributes

The property at 915 North Service Road has cultural heritage value as it satisfies the criteria for Determining Cultural Heritage Value or Interest set out in Regulation 9/06 of the Ontario Heritage Act. The following are the key exterior and interior attributes as a rare example of the Craftsman Bungalow style within the City of Mississauga and as a reflection of the work by D.C. Cotton, architect:

1. The property has design and physical value in its architectural value as a rare example of the Craftsman Bungalow style within the City of Mississauga. The house features recognizable design characteristics of the style, including:
 - a) 1 and half storey massing
 - b) almost square plan, with protruding bay windows on the south and west wall, protruding stout stone chimney on the west wall
 - c) relatively low floor to ceiling heights

- d) low-slung gabled roof with dormers
- e) front porch with thick stone columns
- f) rusticated buff limestone exterior building material, laid in a split course bond, mortar joints that accentuate the bond pattern of the wall.
- g) “punched” style masonry openings for windows, with segmental arch, key stone and straight cut voussoirs
- h) exterior stone extends to the top of the 2nd floor window level and in all facades of the original portion of the house
- i) stone is sourced from Milton
- j) wood three over one pane sash style windows arranged in a variety of compositions: singles, pairs or threes
- k) interior layout with centre hall plan with staircase in main hall
- l) original kitchen shaker style stained oak cabinetry, sink, plumbing fixtures and hardware of the style
- m) stained wood millwork such as wainscoting, mission style balustrade
- n) limestone fireplaces and built in book case found in the house designed in an integrated way with the fireplace wall
- o) orientation of the house on the lot

2. The house has associative and historical value because:

- a) It has direct associations with Dixie Cox Cotton, architect born and raised in Port Credit, who is native of Port Credit, Mississauga. He studied at the University of Toronto, and worked both in Toronto and his home town. He is a rare architect born and raised in the municipality known to the community that lived and produced work in the early 20th century in Mississauga, contributing to the building of the character of the municipality as we know it today.
- b) The house has the potential to yield information that contributes to the understanding of a community and culture because the house was built on farmland which was subdivided into suburban lots in the early 1950s. The Hedge family farmhouse stood in the family’s fruit farm originally run on the lands. The Hedge family presumably farmed the land since 1906. Hedge Drive in the subdivision was named after the family. The orientation of the house facing North Service Road as the front entrance is reflective of an earlier time, prior to the building of the Queen Elizabeth Way as a multilane highway in the 1950s. The incorporation of the William Hedge Farmhouse, within the 1953 subdivision and retention to today provides a tangible representation of the history of land use and urban design in the City of Mississauga and it can yield information as to the history of a community.
- c) The house demonstrates the work of Dixie Cox Cotton, an architect who is significant to the community. Dixie Cox Cotton is attributed with having designed a number of buildings in the community and Toronto, reflecting the mainstream architectural design ideas of the time, which were based on references to vernacular and classical architecture within the British Empire, high quality craftsmanship and design, and integration of the arts and architecture as expressed in the Craftsman Bungalow, Edwardian, and Institutional and Commercial Period Revival buildings. The ideas reflected in the execution of the interior of the house speak of innovations in middle class domestic architecture in order to achieve practicality while maintaining high quality craftsmanship. This is specially expressed in the kitchen cabinetry materiality and design, including the sink with interior plumbing (faucet) and millwork found throughout the house.

The physical/design attributes listed in point one are also the materialization of the historical and associative value. In addition to these attributes, the following lend the property its historical/associative value:

- Orientation of the front entrance towards North Service Road
- Siting within a large lot that is distinctive from the neighbouring properties



Heritage Impact Statement

915 North Service Road
Mississauga, Ontario

March, 2015

INTRODUCTION

It is a requirement for the City of Mississauga to request "Heritage Impact Statements" for proposed demolitions of homes listed as a cultural heritage resource. This report will review the subject property and its architectural style.

The property was the subject of Consent Applications to the City of Mississauga (Files B 49, 50 and 51/14) for the creation of three (3) new lots; 4 lots in total. Two fronting onto the North Service Road, and two fronting onto Ribston Road.

As a result of the requirement for the demolition of the existing house on the subject property, this Heritage Impact Statement has been prepared.

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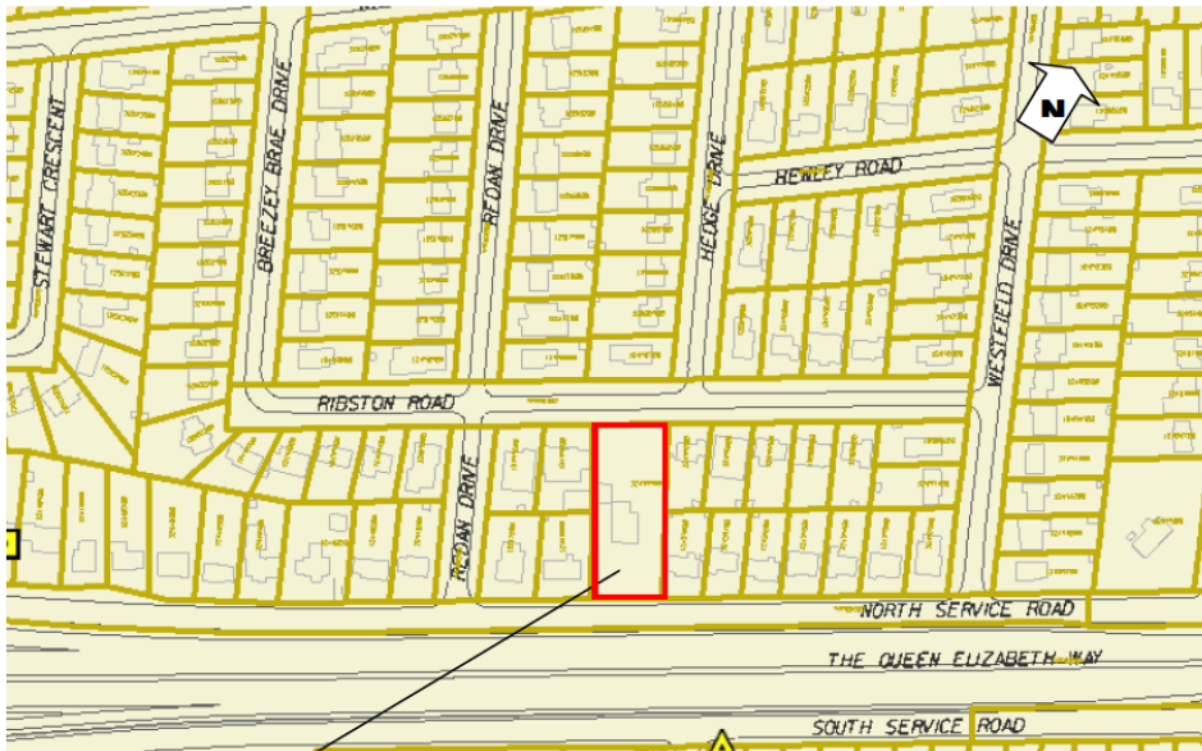
- 1.Context Map**
- 2.Location Map**
- 3.Plan of Survey**
- 4.Official Plan**
- 5.Zoning Map**
- 6.Aerial Photos**
- 7.Significant Cultural Landscape Designation**
- 8.Property History**
- 9.Peel Historical Atlas**
- 10.Existing site conditions**
 - a. Exterior Photos**
 - b. Floor Plans - Original Home**
 - c. Floor Plans - Addition**
 - d. Interior Photos**
- 11.Streetscapes**
- 12.Mandatory Recommendations**
- 13.About the Author**
- 14.References**
- 15.Appendix A; History of the QEW**

1. Context Map



The property is located on the north side of North Service Road, east of Cawthra Road.

2. Location Map

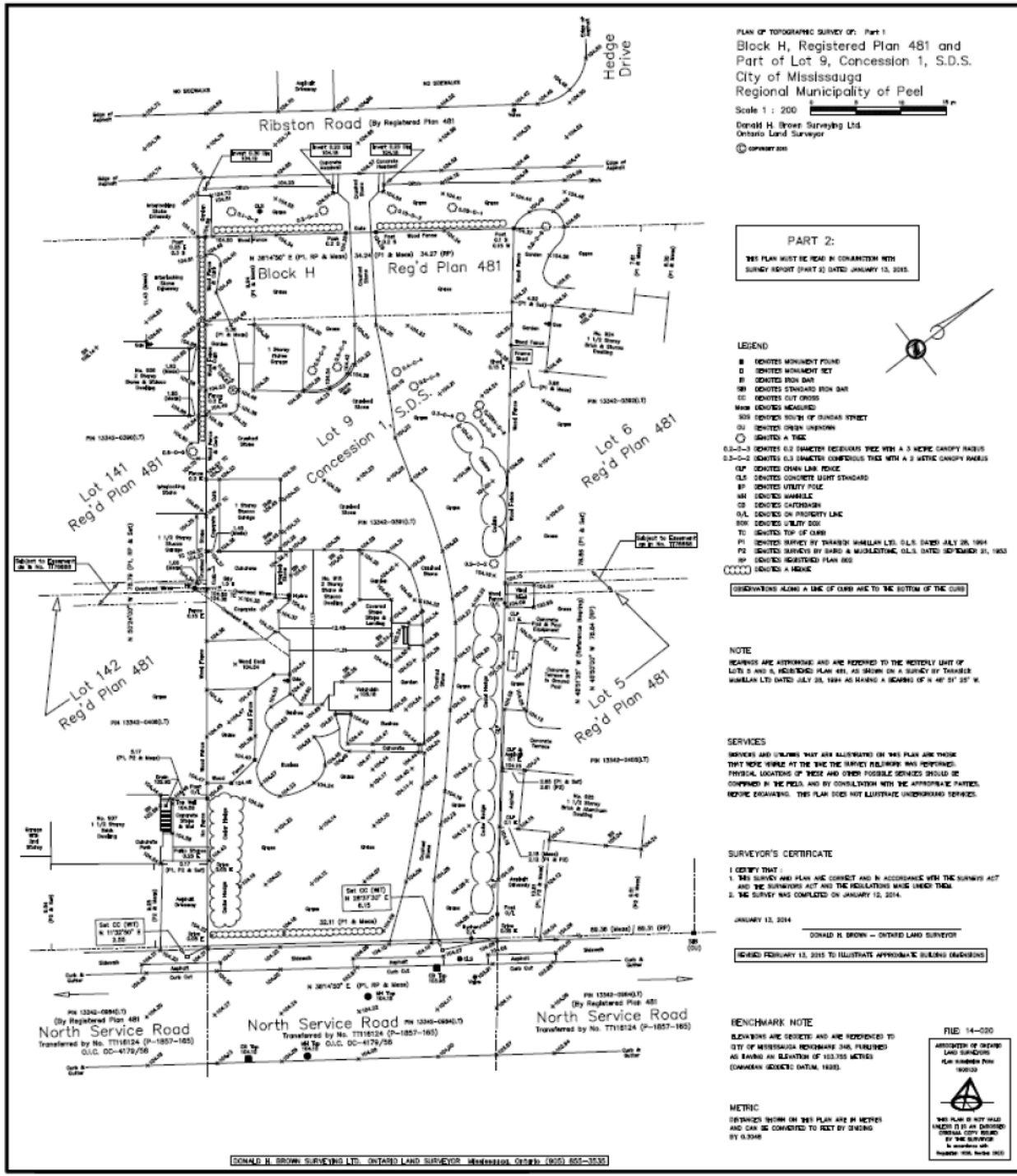


Subject Property

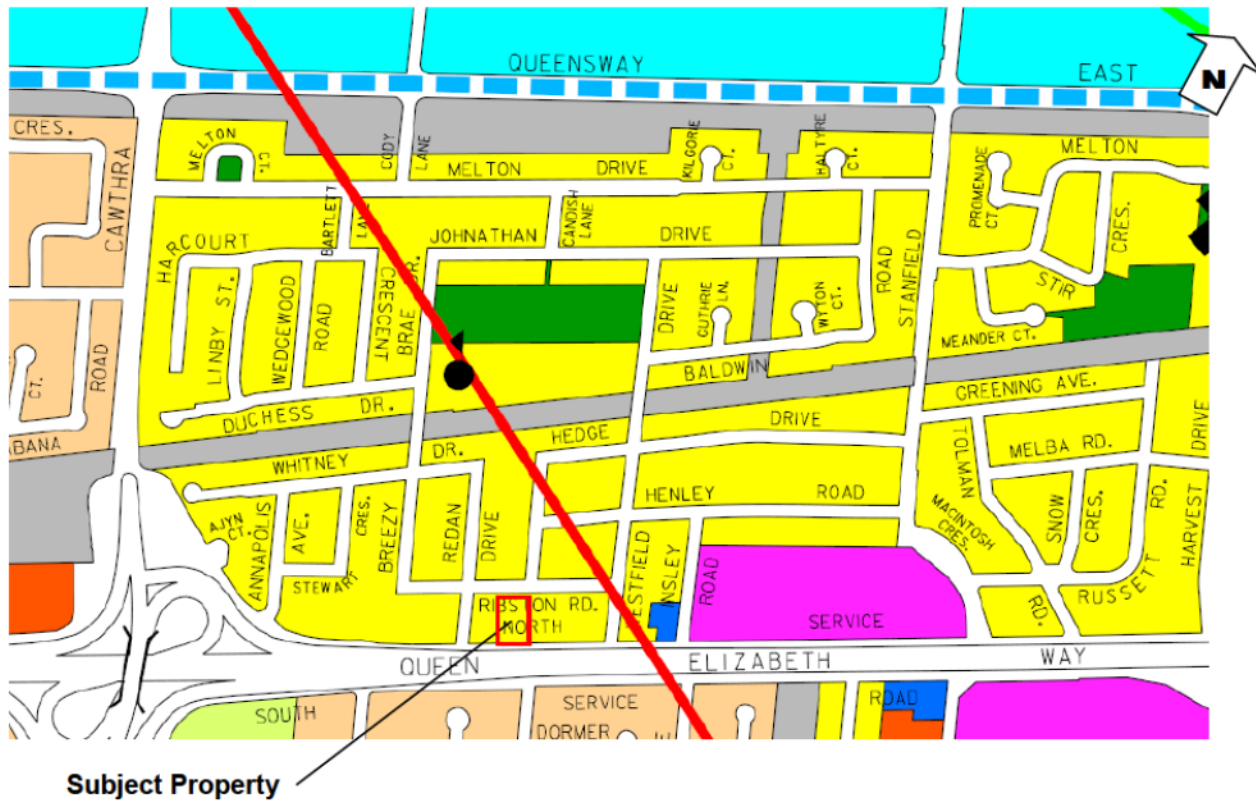
The property is located on the north side of North Service Road, east of Cawthra Road. It is a through lot that backs onto Ribston Road.

The creation of the four new lots will be in keeping with the existing lot fabric around the subject property.

3. Plan of Survey



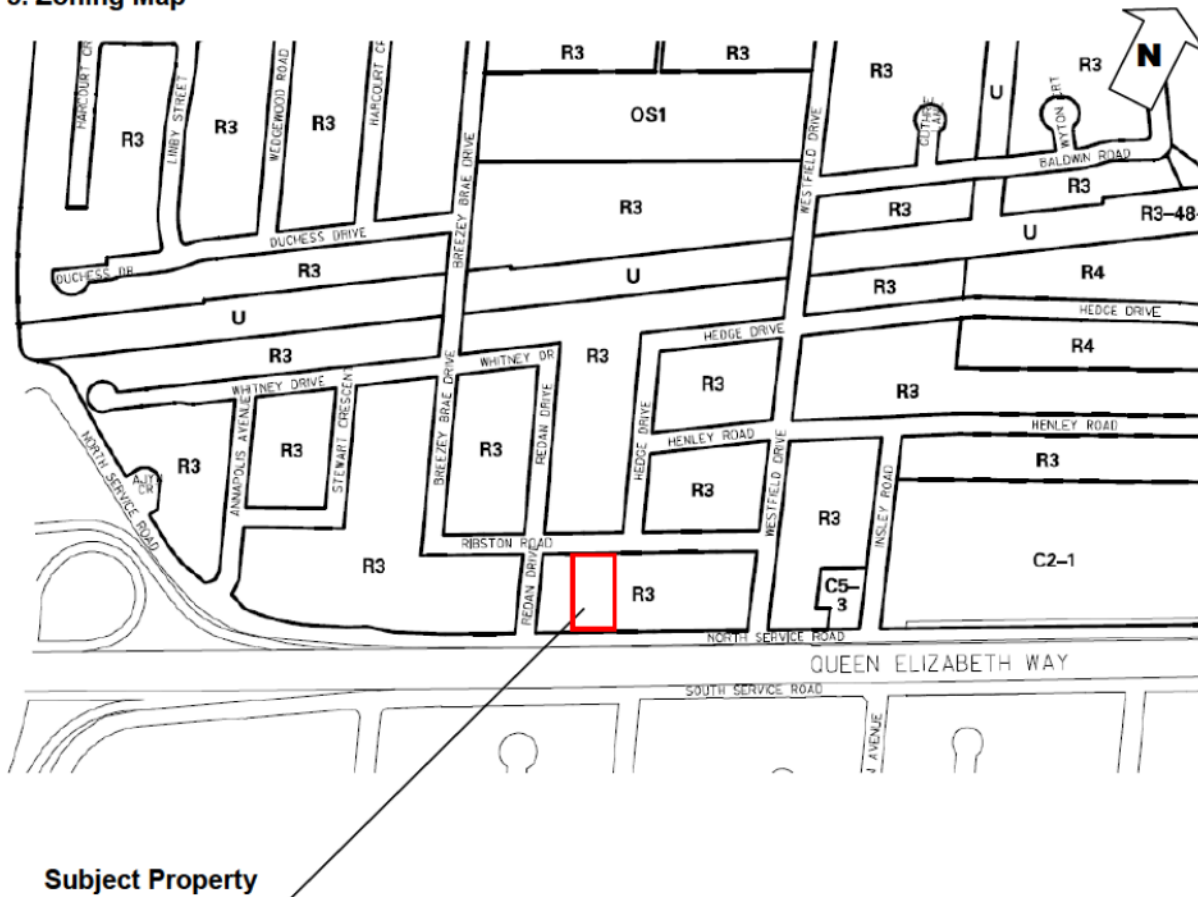
4. Mississauga Plan (Official Plan)



The subject property is designated Residential Low Density 1 in the Mineola District Policies of the Mississauga Plan.

The Residential Low Density 1 policies provide for single detached residential dwellings.

5. Zoning Map



The subject property is zoned R3 under the City of Mississauga Zoning By-law 225-2007, as amended.

The provisions of the R3, zoning permits single detached residential dwellings on lots having an area greater than 550 m².

6. Aerial Photos

The aerial photos demonstrate the development of the neighbourhood. The subject property is outlined in red in all of the photos.

The photo below is from 1954 and shows that the neighbourhood has been developed. The subdivision that surrounds the dwelling is the western portion of Applewood Acres, developed in 1953.

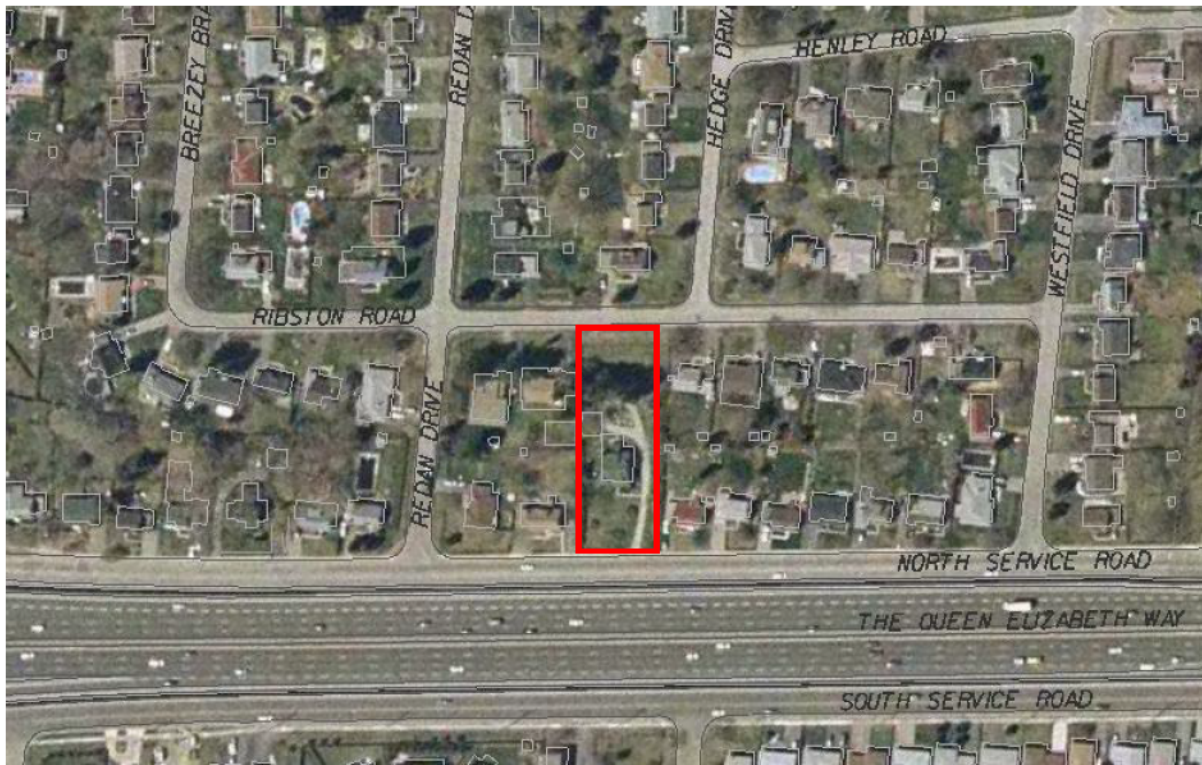


1954 Aerial Photo



1966 Aerial Photo

This image more clearly identifies the house on the property. Further, Westfield Drive has been developed since the aerial of 1954. It also shows the expansion of the QEW from a two-lane to a six lane highway.



1999 Aerial Photo

The photos through the 70's and 80's demonstrate that the neighbourhood is fully developed and unchanged.

The home looks unaltered from its original form.



2014 Aerial Photo - above, Google Streetview below.



7. Significant Cultural Landscape Designation

Address: 915 NORTH SERVICE RD
 Legal Description: CON 1 SDS PT LOT 9 PLAN 481 BLK H
 Roll Number: 21-05-070-054-20700-0000

 [Print Friendly Page](#)

Heritage Status

Status: LISTED ON THE HERITAGE REGISTER BUT NOT DESIGNATED
 Conservation District:
 Bylaw:
 Bylaw Date:

Inventory Item

INV #	Property Name	Constructed	Decade	Demolished	Year Demolished
<u>737</u>	FREDERICK HEDGE HOUSE	1928	1920	N	

Images



History

The 1.5 storey building is built of stone, with an elongated asphalt gable roof and dormers. There is a 3-bay, asymmetrical front facade, with a recessed front porch, and an internal stone chimney on the west side. Fenestration appears to be original on the building, and is comprised by grouped series of one-over-one paned windows. Ornamentation is practically non-existent, in keeping with the theme of simplicity inherent in the design of early Bungalows. Constructed in 1928 by Frederick Hedge (owner or architect?), according to an interview with Mr. Dave Cook. Note: Hedge family owned 871 North Service Road in same time period.

*City of Mississauga Cultural Landscape Inventory.

8. Property History (Title Chain)

This chain of title search was provided by Stephen Shaw Conveyancing.

November 20, 1807: The Crown to John Gage

April 20, 1808: John Gage to Mary & Joseph Horning

August 17, 1831: Mary & Joseph Horning to Frances Logan

June 6, 1840: Francis Logan to John McGhie

March 5, 1847: John McGhie to Andrew Graham and Estate Deazeley Graham

December 3, 1890: Andrew Graham and Estate Deazeley Graham to Jane Graham

April 24, 1886: Jane Graham to Annie M. Leaver

September 1, 1906: Annie M. Leaver to John S. Hedge

November 21, 1901: John S. Hedge to William Henry Hedge & Estate (Fred Hedge)

December 20, 1941: Fred Hedge to Sarah M. Hedge

Sarah M. Hedge includes Frederick M. Hedge and William L. Hedge on title

In 1953 Block H, Plan 481 is severed from the property and sold to Applewood Dixie Limited, the remaining parcel; Part Lot 9, Con 1, SDS remains with the Hedge family.

April 22, 1971: Estate of Sarah Hedge to Frederick McNally Hedge

August 29, 1994: Frederick McNally Hedge to George Jones and Barbara Fritz

The title search and subsequent research into the listed property owners do not indicate any owners of significance or relevance to the development of Mississauga.

However, Dave Cook, in his book *Apple Blossoms and Satellite Dishes*, provides a recollection by T.L. Kennedy (farmer, and famous Mississauga resident) suggesting that the Hedge's purchased the property directly from the Grahams, however this is contradicted by the title search, so perhaps, T.L. Kennedy's recollection was incorrect.

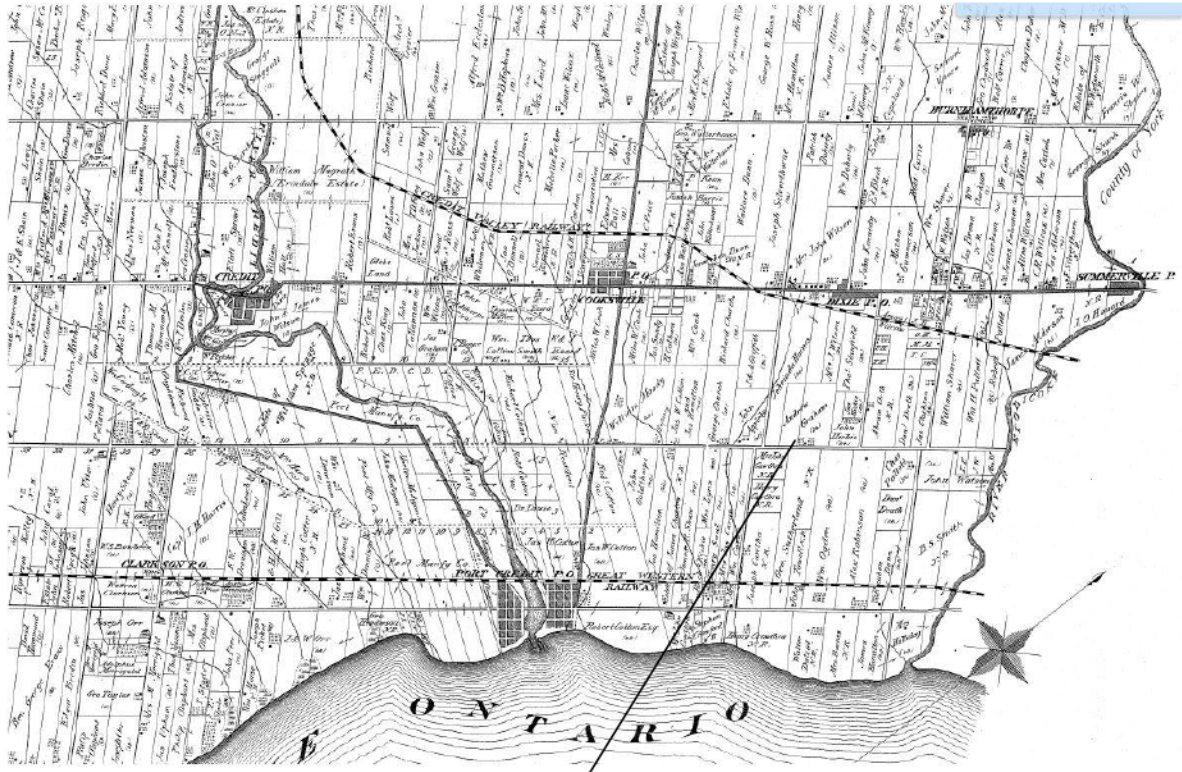
The title search indicates that the Leaver's owned the property after the Graham's and before the Hedge's. There is no significant information available on Annie M. Leaver with the exception that she was born about 1865 in England, married to a George Leaver, and arrived from England approx 1869 (Ancestry.com)

From page 22, *Apple Blossoms and Satellite Dishes*: Lt. Col. T.L. Kennedy says referring to the western portion of Applewood Acres

"The Churches held the land for some time. Then the Haines family bought 300 acres from the Churches. The Haines sold the property to the Grahams, who built the brick, two-storey house on what is now the Service Road. The house is now owned by Dr. R. J. Thompson. The Hedges and Stewarts each bought property from the Grahams at \$20 an acre. They were the ones who planted the fruit trees and apple orchards. Gordon Shipp bought the Hedge and Stewart farms which he built up into the Westacres section of Applewood Acres.

What remains today are 871 North Service Road (identified as inventory No. 486 and Hedge House, although this appears to be the original Stewart Farmhouse. Dr. R. J. Thomson says "at the time I bought the Stewart farmhouse (871 North Service Road) from Howard Stewart in 1953, my street address was 889 Queen Elizabeth Highway, RR1 Port Credit.....to my east was a bungalow belonging to Bill Hedge, his wife and two sons.(presumably now 905 North Service Road. Further east of that was the Hedge farm house (915 North Service Road) a gray stone building occupied by Mrs. Hedge and her other son, Fred."

9. Peel Historical Atlas 1877



Approximate location of subject property.

Indicates the property owned by Andrew Graham which corresponds to the title search documents.

10. Existing Site Conditions

The subject property is an interior lot situated on the north side of North Service Road, east of Cawthra Road. It is a large through lot that backs onto the street behind; Ribston Road. A one and a half storey single family dwelling is situated on the lot. The property is known as the Frederick Hedge House, constructed in 1928. The property contains a dwelling and two (2) detached garages.

Cedar bushes surround the majority of the property along the boundary lines and there are several mature trees at the rear of the property.

Driveway access is provided to both the front and the rear of the property from both North Service Road and Ribston Road.

Discussions with Jim Hedge revealed that his grandfather, William Henry Hedge constructed the home in 1928-1929. He recalls hearing that his grandfather cut the stone by hand and brought it down from Milton by wagon. He said constructing the home caused an early death (August 21, 1941) and that he never really had time to enjoy the home after it was built. William Hedge, born March 18, 1877, was a fruit grower who grew apples, cherries, peaches and plums. He married Sarah Mabel Winters (born May 31, 1880, died July 8, 1970) and together they had two children Frederick and William.

The existing heritage inventory record from the City of Mississauga indicates that the house was built by Frederick Hedge. However, this is incorrect. It was built by William Henry Hedge, father of Frederick Hedge.

The interior of the original dwelling remains unaltered since its construction (with the exception of the bathroom and kitchen appliances). The hardwood floors and vinyl in the kitchen remain, as do the kitchen cabinets and sink.

The walls are of plaster construction with oak baseboards, crown molding and details. The dining room is wainscoted in oak as well. The radiators for heating all remain, most with oak 'shelves' on top to utilize the space.

Windows are single pane, double hung, casement.

The bathroom was updated at some point, presumably when the addition was completed in 2003.

An extensive history of the neighbourhood was undertaken by Kathleen Hicks in her book *Dixie - Orchards to Industry*. There is one reference to the Hedge family house located at 915 North Service Road, and this can be found under her subtitle "Historic Homes of Dixie". Ironically no picture is included although one of less architectural merit is, 835 North Service Road.

Building permit records from the City of Mississauga are shown below.

Building Permits

1 Permit(s) found Page: 1 ▼ of 1

▫ App Number	▫ Address	▫ Scope	▫ <u>Issue Date</u>
▫ <u>App Date</u>	▫ Description	▫ Type Description	▫ Status
BP 9ALT 3 8548 2003-05-16	915 NORTH SERVICE RD ADDITION	ADDITION TO EXIST BLDG DETACHED DWELLING	2003-07-16 COMPLETED -ALL INSP SIGNED OFF

1 Permit(s) found Page: 1 ▼ of 1

The Committee of Adjustment applications are shown listed below. An application for severance was submitted in 1994 and subsequently withdrawn.

Committee of Adjustment Applications

4 Application(s) found Page: 1 of 1

Address/Description	App Number	App Date	Status
915 NORTH SERVICE RD	B-50/14	2014-08-08	APPROVED

The applicant requests the consent of the Committee to the conveyance of a parcel of land having a frontage of approximately 17.11 m (56.16 ft.) and an area of approximately 573.84 m² (6177.00 sq.ft.). The effect of the application is to create a new lot for residential purposes onto Ribston Road.

[View Decision / Certificate Text](#)

915 NORTH SERVICE RD	B-51/14	2014-08-08	APPROVED
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The applicant requests the consent of the Committee to the conveyance of a parcel of land having a frontage of approximately 17.11 m (56.16 ft.) and an area of approximately 573.84 m² (6177.00 sq.ft.). The effect of the application is to create a new lot for residential purposes onto Ribston Road.

[View Decision / Certificate Text](#)

915 NORTH SERVICE RD	B-49/14	2014-08-08	APPROVED
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The applicant requests the consent of the Committee to the conveyance of a parcel of land having a frontage of approximately 16.05 m (52.67 ft.) and an area of approximately 727.79 m² (7834.13 sq.ft.). The effect of the application is to create a new lot for residential purposes onto North Service Road.

[View Decision / Certificate Text](#)

915 NORTH SERVICE RD	B-55/94	1994-01-01	WITHDRAWN
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The applicant(s) request(s) consent to a conveyance of part of lot 9 Conc. 1 S.D.S. (Geo. Twp. of Toronto, County of Peel) in the City of Mississauga Region of Peel. The "severed" land, has a frontage of approximately 32.12m (105.38 ft.) and an area of approximately 1623.88m² (17,479.3 sq. ft.). The land is located on North Service Road. It is proposed that the "severed" land be used for single family dwelling purposes.

[View Decision / Certificate Text](#)

A). Exterior Photos



Front of dwelling





Front of dwelling



Rear Addition



Rear Addition



East Side



East Side



East Side



West Side Elevation



West Side Elevation



West Side Elevation

B) Floor Plans and Elevations- Original Dwelling

The original home was designed by D.C. (Dixie) Cotton, as indicated on the drawings. His parents were James William Cotton and Susan Amelia Barber. The Cotton family was quite well known in the history of Mississauga and Port Credit.

Dixie Cotton was the grandson of Robert Cotton "a farmer and merchant, who served as Postmaster for Port Credit from 1856 to 1885. He also ran the general store on the west bank of the river in Port Credit. This store was destroyed in the fire of 1855 and Cotton set up a second store on the east bank of the Credit, which was later bought by James Hamilton. Cotton held several offices in the community including that of Manager of the Port Credit Harbour Company. He died March 19, 1885 and is buried in the cemetery at St. Peter's Erindale. He had previously passed the homestead on to his son, James W. Cotton, shortly after James's marriage to Susan Barber, daughter of Robert Barber." City of Mississauga

Dixie Cotton is buried in Park Lawn Cemetery, Toronto.

This biography was provided from the www.dictionaryofarchitectsincanada.org/architects/view/970.

Biographical Dictionary of ARCHITECTS IN CANADA 1800 - 1950

Introduction	Sources & Methodology	Abbreviations	Acknowledgments	Appendix
Architect Search	Browse: <input type="text"/>	Search by Keyword: <input type="text"/>	<input type="button" value="Reset"/>	<input type="button" value="Submit >>"/>
Cotton, Dixie Cox COTTON, Dixie Cox (1882-1943) was born in Port Credit, Ont. on 19 September 1882 and articulated with Herbert G. Macklin from 1900 to 1907. In 1908 he formed a partnership with Macklin and operated an architectural firm called The Designing & Draughting Co. which can be credited with designs for several houses in the Rosedale and Forest Hill neighbourhoods of Toronto. When Macklin resigned in 1919 Cotton became the sole owner of the firm and moved his practice from Toronto to Port Credit (where he had been a life-long resident). Cotton later became a maintenance engineer for the St. Lawrence Starch Co. He died in Port Credit on 9 September 1943 (obit. Toronto Telegram, 10 Sept. 1943; inf. Ontario Assoc. of Architects) DESIGNING & DRAUGHTING CO. (works in Toronto unless noted) ADMIRAL ROAD, near Dupont Street, residence for Clement A. Hurndale, 1907 (Toronto b.p. 8327, 3 July 1907) ADMIRAL ROAD, near Bernard Avenue, residence for H.H. Snyder, 1907 (Toronto b.p. 8607, 26 July 1907) BLOOR STREET WEST, near Brunswick Avenue, store and dwelling for William Unser, 1908 (Toronto b.p. 12680, 15 Sept. 1908) CHESNUT PARK ROAD, near Roxborough Street, residence for Samuel C. Smoke, 1908 (Toronto b.p. 13127, 29 Oct. 1908) BALMORAL AVENUE, near Poplar Plains Road, residence for G.H. Smith, 1909 (Toronto b.p. 14407, 27 March 1909) ROWANWOOD AVENUE, residence for Robert C. Bustard, 1909 (inf. Carolyn Neal, Toronto) DUNVEGAN ROAD, near Heath Street West, residence for Fred Holt, 1911 (Toronto b.p. 15056, 23 Jan. 1911) ADMIRAL ROAD, near Bernard Avenue, residence for Benjamin Kent, 1911 (Toronto b.p. 25437, 3 March 1911) LYNTHURST AVENUE, near Nina Street, residence for Ernest A. Lock, 1911 (Toronto b.p. 25734, 20 March 1911) GLEN ROAD, near Elm Avenue, residence for John E. Hoare, 1911 (Toronto b.p. 25885, 23 March 1911) RUSSELL HILL ROAD, near St. Clair Avenue West, residence for Walter E. Berkinshaw, 1911 (Toronto b.p. 27041, 12 May 1911) ST. CLAIR AVENUE WEST, at Warren Road, residence for R. Thomson, 1911 (Toronto b.p. 27549, 27 May 1911) RUSSELL HILL ROAD, near Lonsdale Avenue, residence for Thomas H. Cooper, 1911 (Toronto b.p. 28578, 28 June 1911) WALMER ROAD, residence for M.L. Kent, 1911-12 (C.R., xxv, 29 Nov. 1911, 59, t.c.) RUSSELL HILL ROAD, near Heath Street West, residence for Fred Holt, 1912 (Toronto b.p. 32372, 24 Jan. 1912) TYNDALL AVENUE, near King Street West, apartment block for William B. Charlton, 1912 (Toronto b.p. 32971, 25 March 1912) D.C. COTTON ELLIOTT & SON LTD., Yonge Street at Dundonald Street, store and warehouse, 1912 (Toronto b.p. 37029, 3 Sept. 1912) BRACONDALE AUTO GARAGE, St. Clair Avenue West at Shaw Street, for J.B. Barrett, 1917 (Toronto b.p. 14775, 31 Oct. 1917) PORT CREDIT, ONT., factory for St. Lawrence Starch Co., 1919-20 (C.R., xxxiii, 12 Nov. 1919, 48) WINDSOR MILLS, QUE., factory for Kilgour Bros., 1923 (C.R., xxxvii, 25 April 1923, 61, t.c.) EDGEWOOD APARTMENTS, Kingston Road at Queen Street East, for W.S. Liscombe, 1924 (C.R., xxxviii, 30 April 1924, 68)				

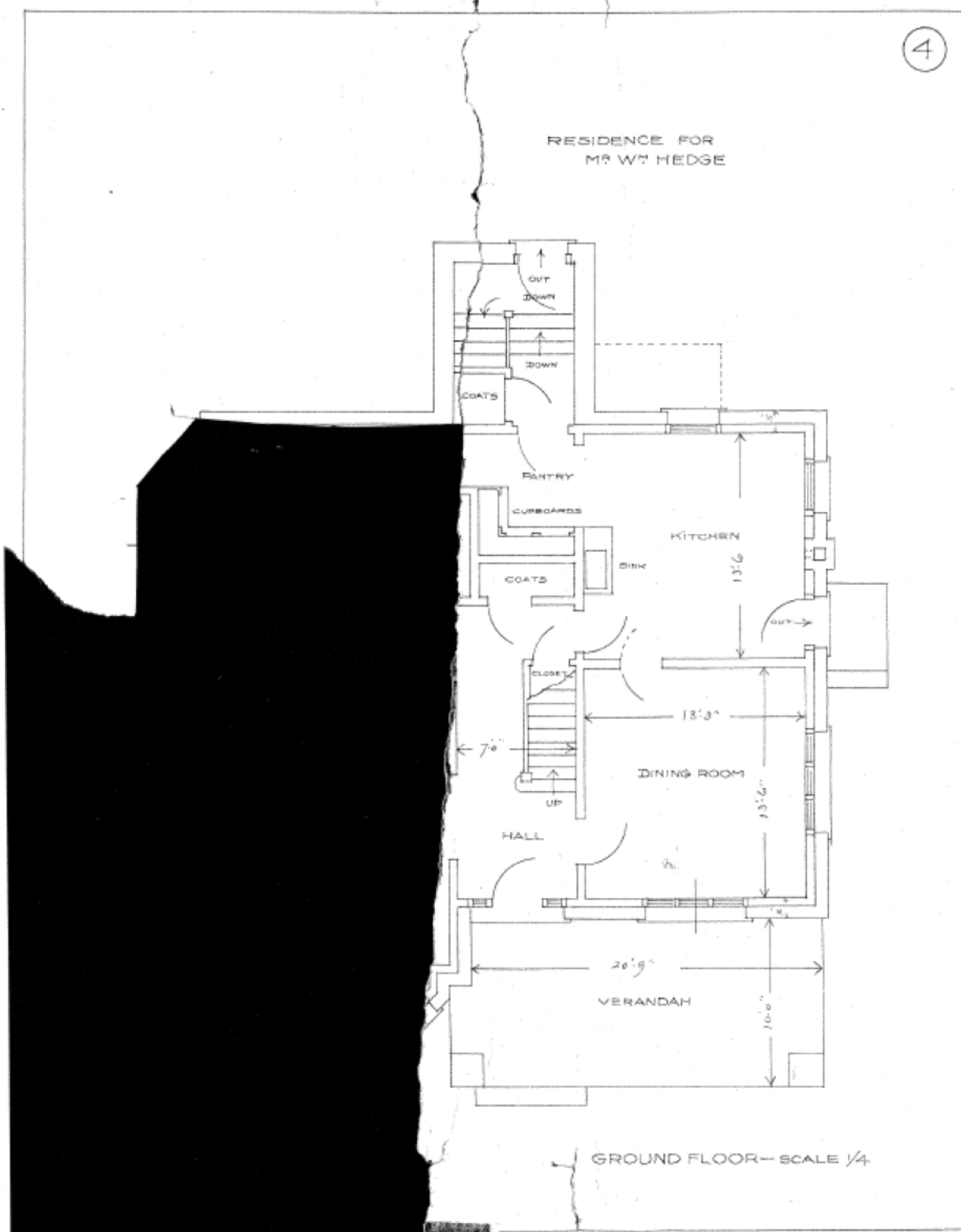
Perhaps of note, is that he ended up working at St. Lawrence Starch after he designed it.

The dwelling at 915 North Service Road is an Arts & Crafts style bungalow. This style of home became popular in Canada in the early 1910's and can be found in many older neighbourhoods and Cities. This home epitomizes many of the characteristics of bungalows and includes the following elements:

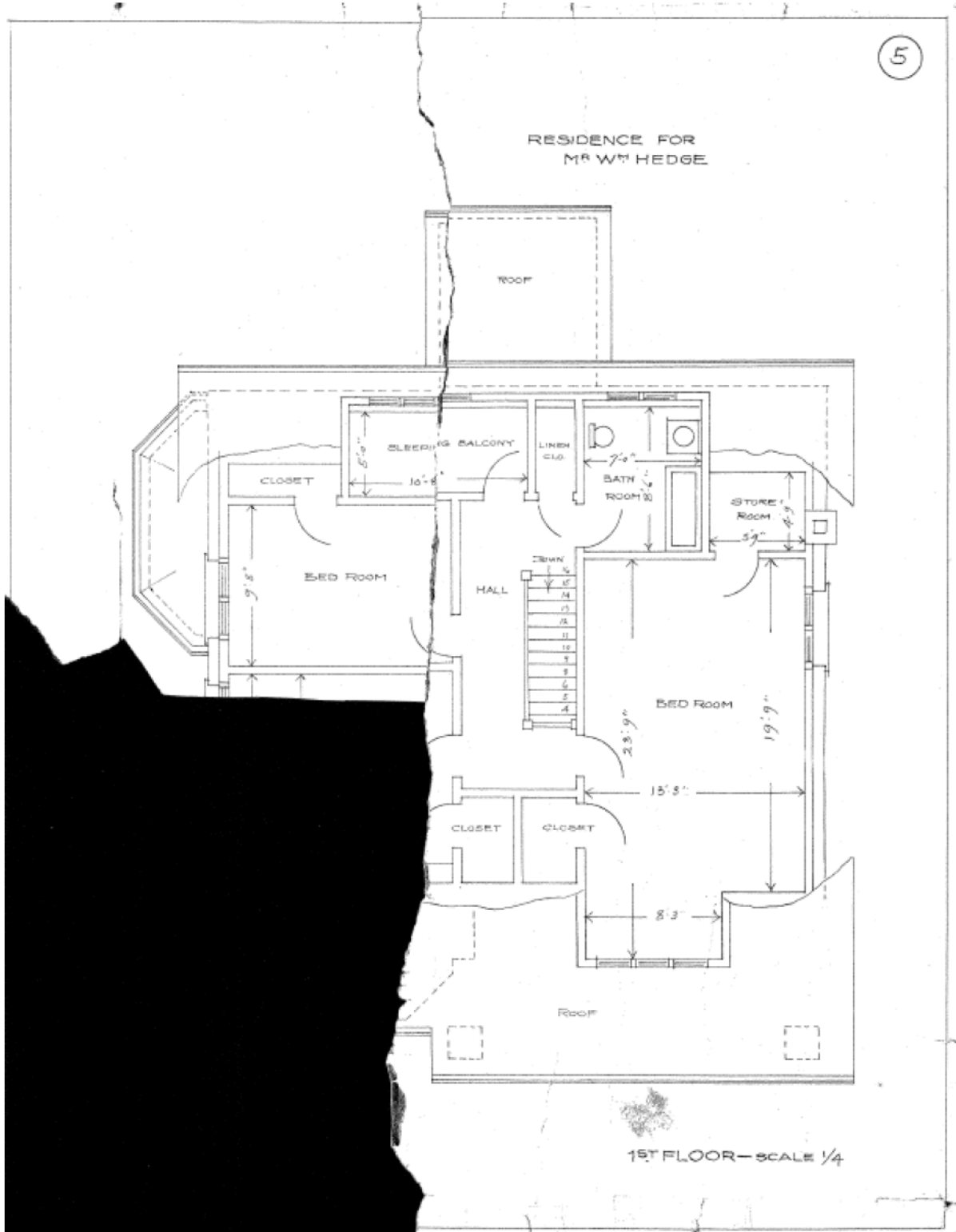
1. Low-pitched roof gable roof
2. Two (2) stories
3. Large porch covered by the overhanging roof and supported by substantial columns.
4. Large fireplace with built-in cabinetry (rear living room)
5. Built-in cabinetry (buffet in kitchen)
6. Simple wainscot in the dining room
7. Dormers
8. Double hung windows with simple wide casings.

[illegible]

W.E. Oughtred & Associates Inc.

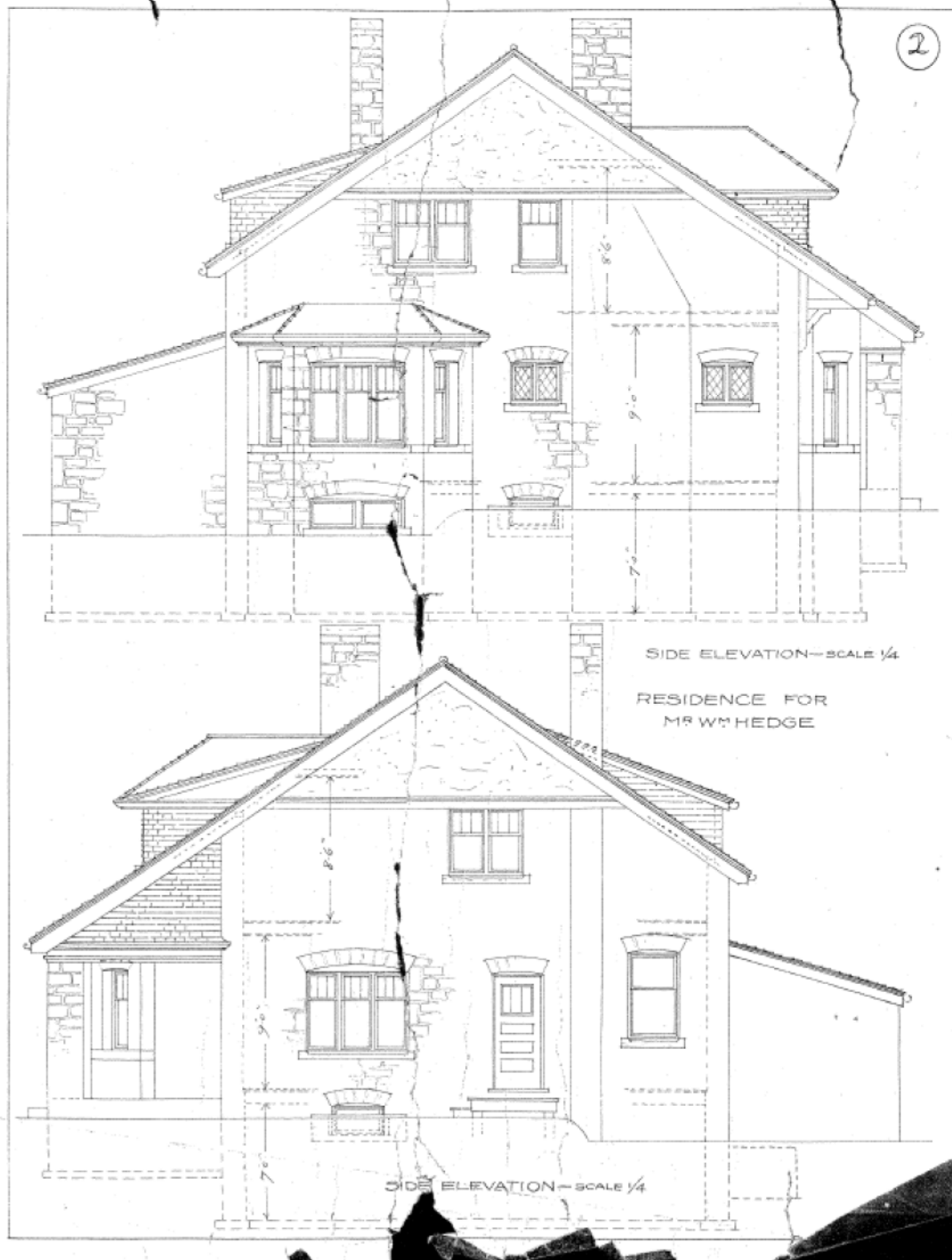


The missing portion of the blue print shows a living room and rear study. Both rooms contained fire places as evidenced in the interior photos. Further the kitchen sink was located on the rear wall below the window looking out over the rear yard.



The missing portion of the blue print shows another bedroom with the same dormer window as the bedroom on the opposite side of the house (at the front). The dormer window is slightly smaller than the one shown in the other front bedroom.

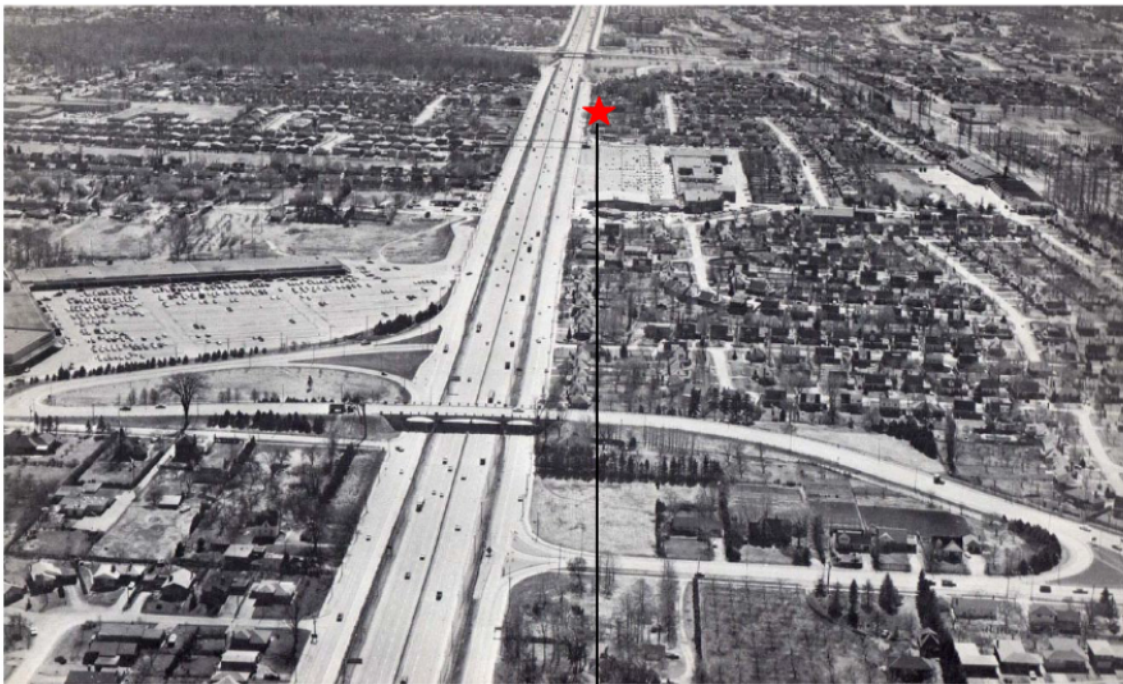




It is noted on the front/rear elevation that the *Plans of Residence for Wm. Hedge to be built on the Middle Road*. Middle Road became the QEW and then at some point in the 1950's, the North Service Road was created so that homes no longer directly accessed the highway. The North Service Road was constructed sometime between 1953 and 1966, as it is not shown in the photo dated 1953, but does appear in the other photo dated 1969. Further, it is shown on the aerial photo in 1966 and it is indicated on the Plan of Subdivision for Applewood Estates, but not yet constructed. Included in the appendices is a history of the QEW.



Resurfaced Queen Elizabeth Way near Port Credit (ca. 1953). (Photo from thekingshighway.ca)
 Photo from Cameron Bevers' Historical Photograph Collection - Original Photographer Unknown



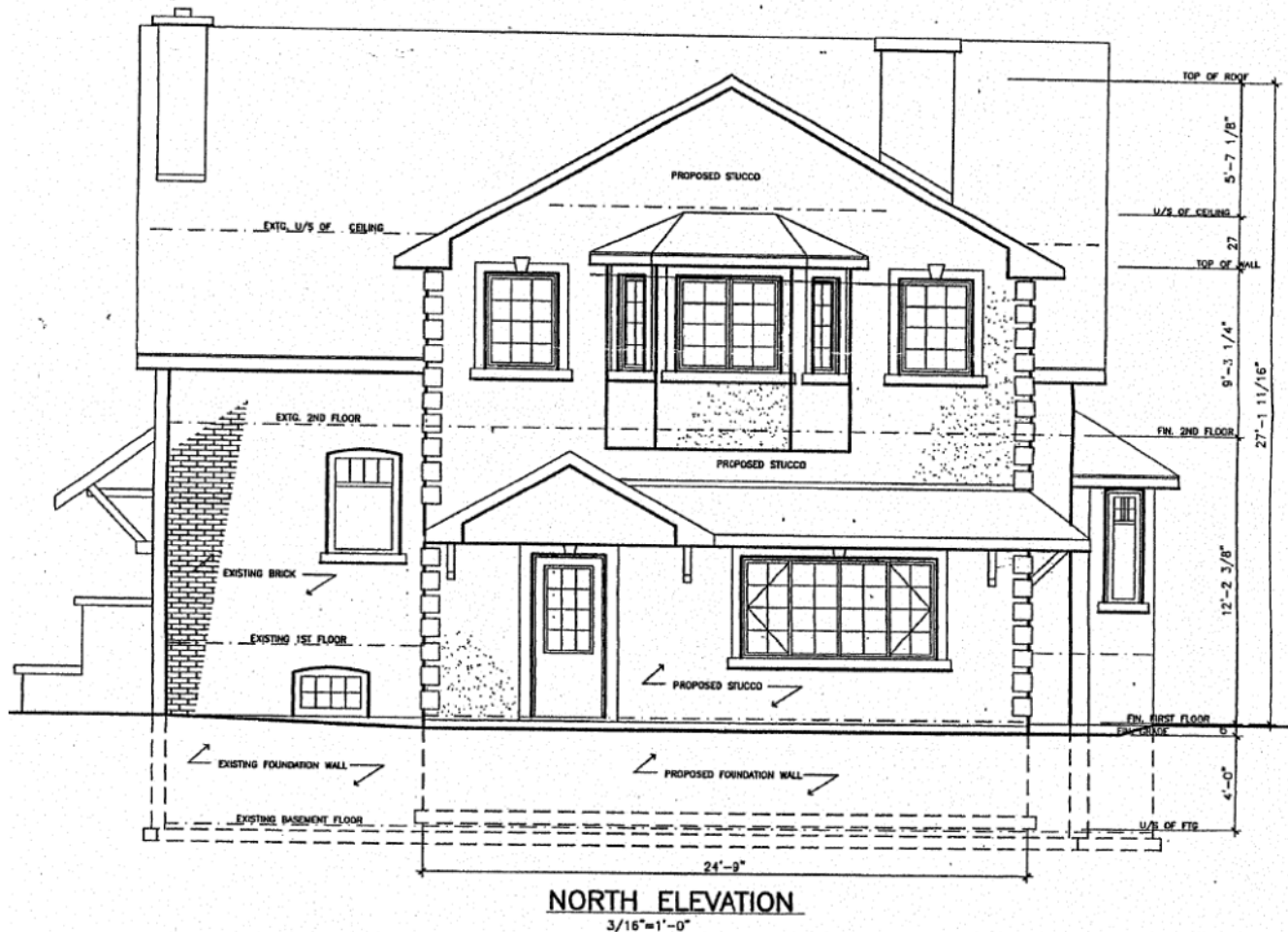
Approximate location of 915 North Service Road

Aerial view of the Dixie Road Interchange on the Queen Elizabeth Way in Mississauga, facing west.
 Photo taken in April, 1969. Photo courtesy of Lockwood Survey Corporation. (Photo from thekingshighway.ca).

A Plan of Subdivision for the expansion of Applewood Estates is included below. This land was purchased from

[illegible]

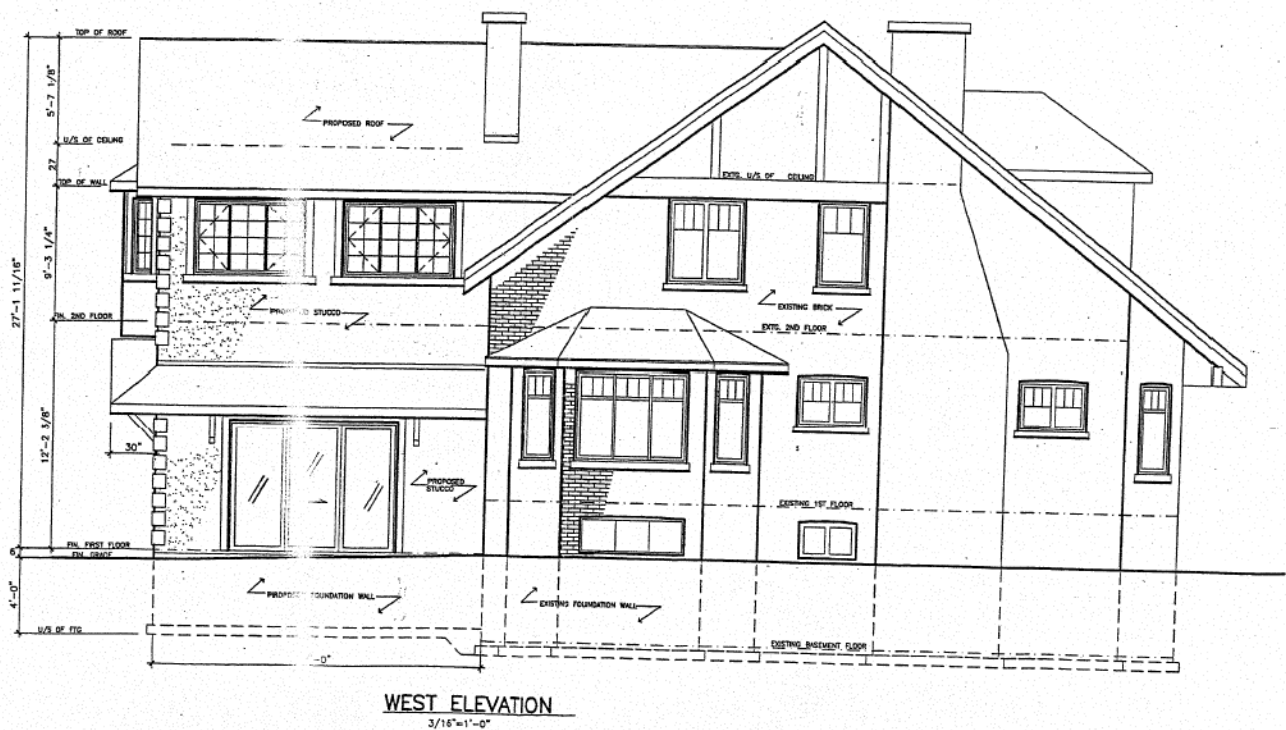
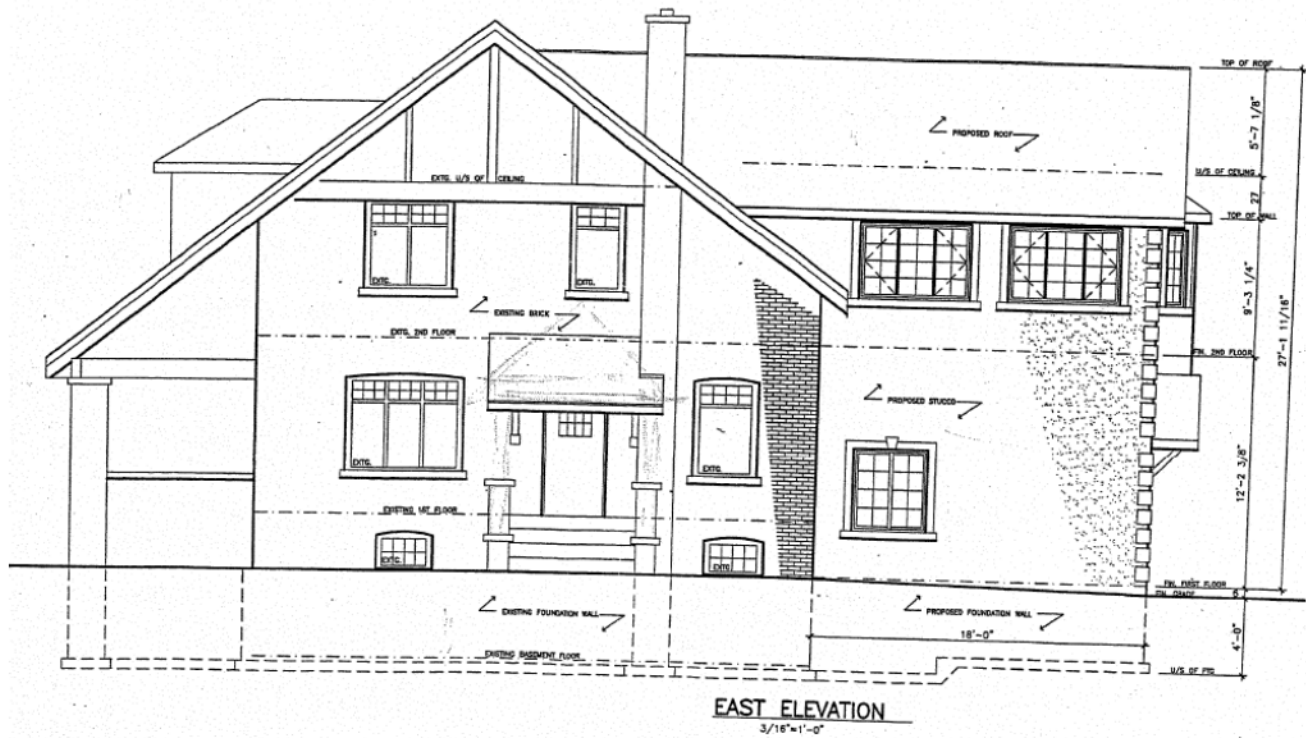
C) Floor Plans - Addition, 2003

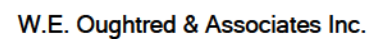


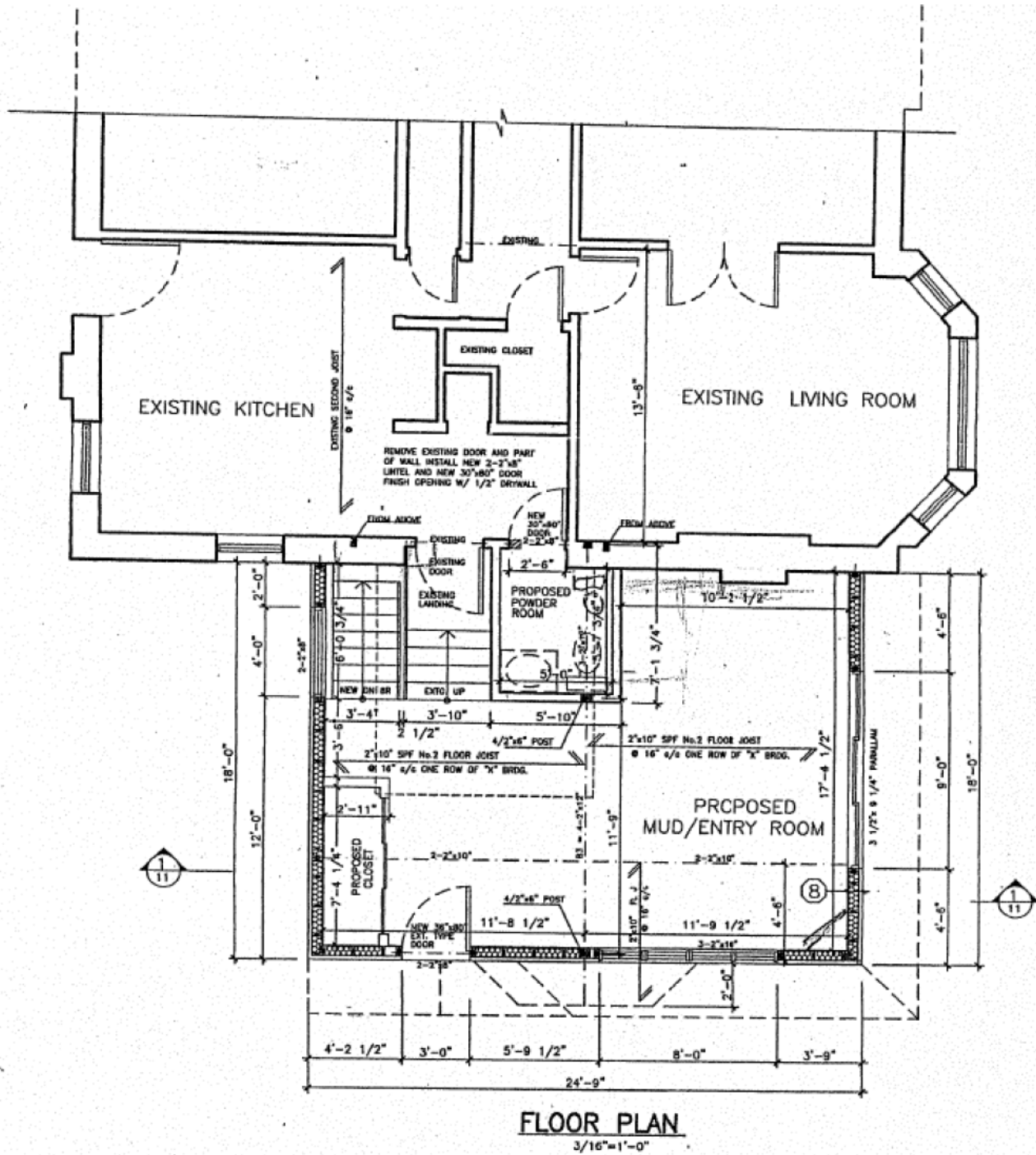
These plans were provided by the current homeowner (Jones/Fritz). These plans were prepared as part of a City of Mississauga Building Permit Application for the proposed rear addition in 2003. The permit application code was 03/8548.

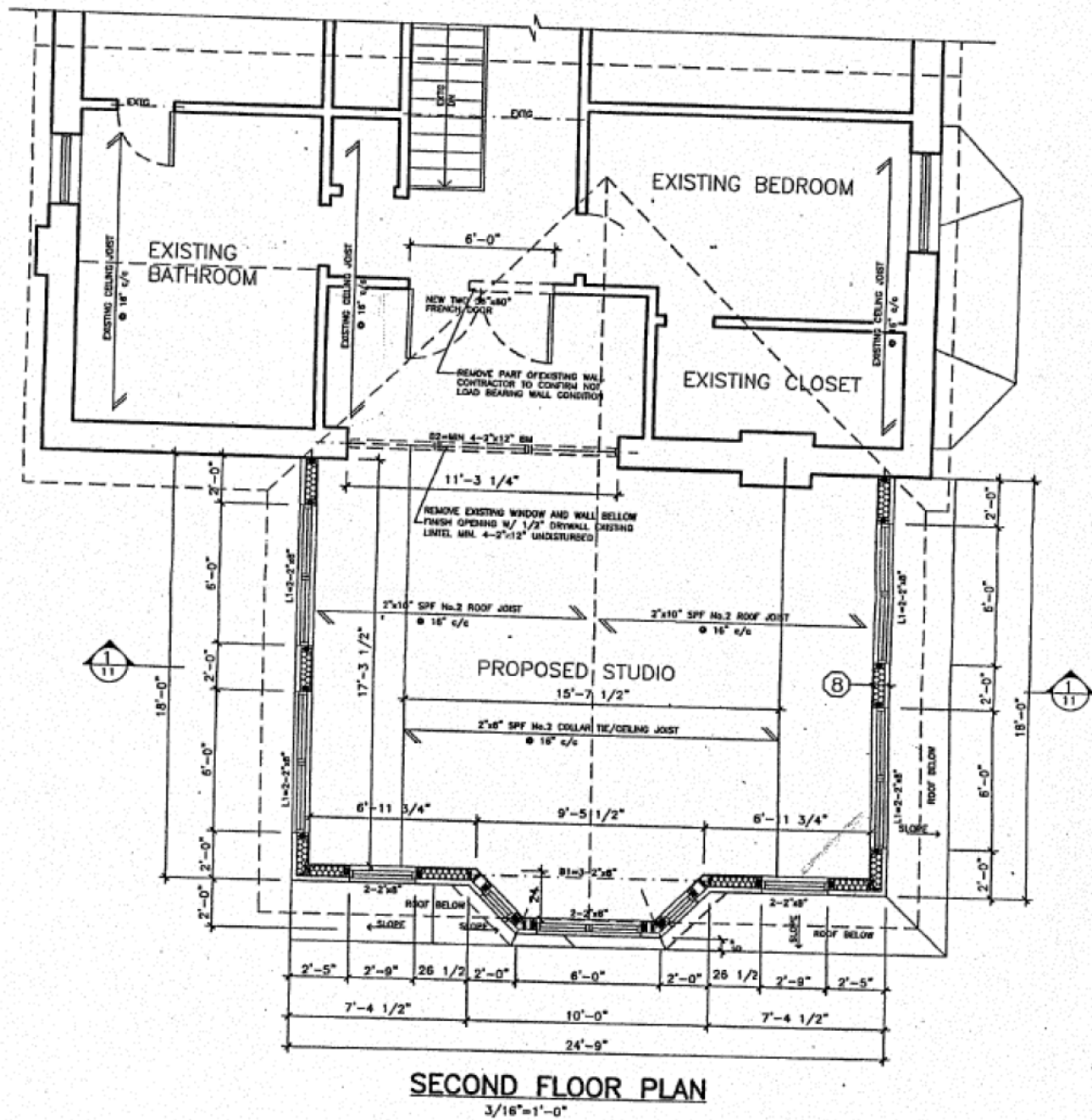
This is the rear (north) elevation, no changes were proposed to the existing south (front) elevation as part of the building permit application, and as such, no elevation was prepared. The front elevation was slightly modified from the original elevation as shown on the plans prepared by Dixie Cotton (reference drawing #1 - as indicated in upper right hand corner of drawing). Modifications to the front elevation include the change of the dormer window roof line from a flat roof to a peaked roof. It is unknown when the roofline was changed, however, it was presumably done when the rear addition and roof were redone in 2003. See image below.











D. Interior Photos



The kitchen - original cabinetry, sink and radiator.



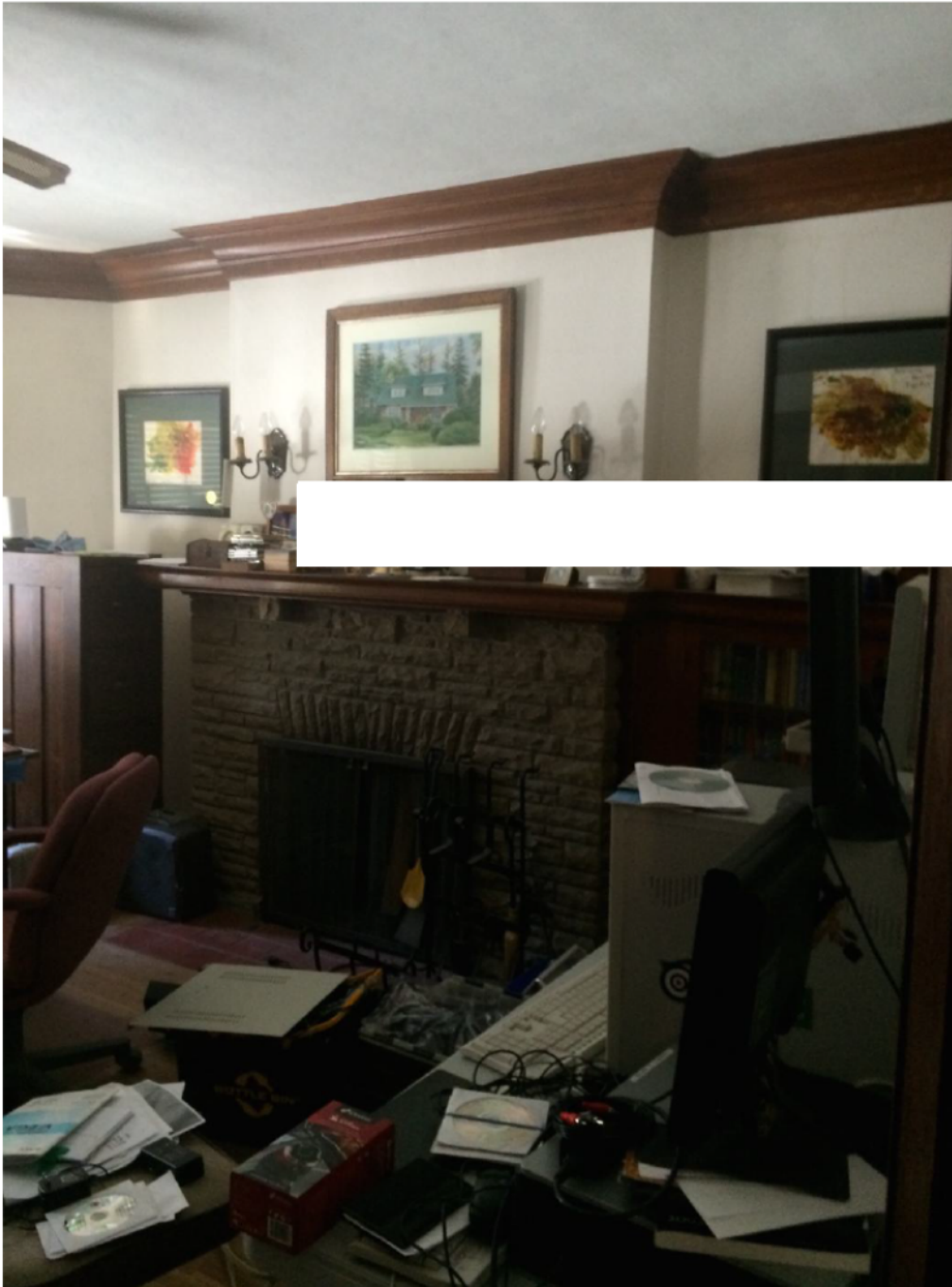
The kitchen - original cabinetry, countertop and vinyl floor.



The kitchen - original cabinetry, wainscoting and vinyl floor.



The kitchen - original cabinetry, sink and radiator.



Family room fireplace is west wall.



Built in bookcase beside fireplace in rear family room.



Main stair and hallway details.



Main stair and hallway details.



Main stair and hallway details. Door on left enters into family room, door at end of hallway is a closet and door under stairs enters into kitchen.



Main stair and upstairs hallway banister.



Living room bay window facing North Service Road.



Living room fireplace is located on the west wall.

Small bedroom at top of stairs. Trim has been painted in this room.

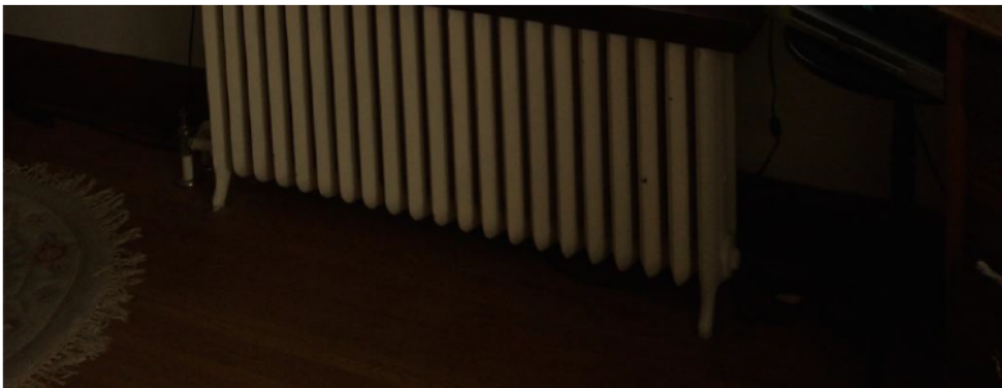
Bedroom #2 on west side front of dwelling. Note water damage to ceiling.



Bedroom #2 on west side front of dwelling. Trim is unpainted in this room.



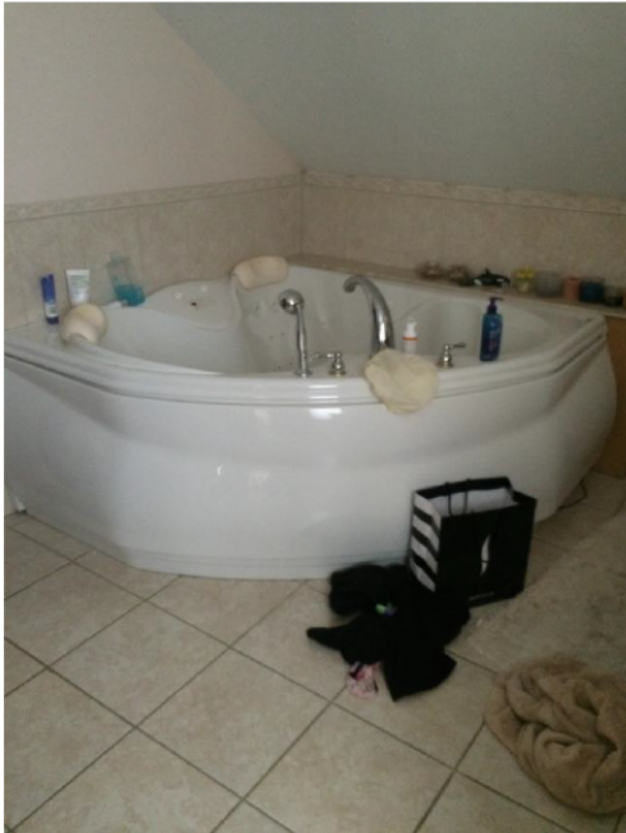
Bedroom #2 on west side front of dwelling. Door is to closet.



Master bedroom on east side of dwelling. This window faces east. Trim and baseboard are unpainted.



Master bedroom on east side of dwelling. This window faces south. Trim and baseboard are unpainted. Door on right is a closet.



Bathroom on second floor. This has been remodeled and updated, probably in 2003. It can be accessed from the master bedroom as well as from the second floor hallway.



Second story rear addition. Looking south (above), doors enter onto hallway and top of stairs. Looking north (below).





Details of stairs and moldings.



Window ledges found throughout the home on main floor. Crown molding (below).





Original telephone located in main hallway at front of dwelling. Radiator (below).





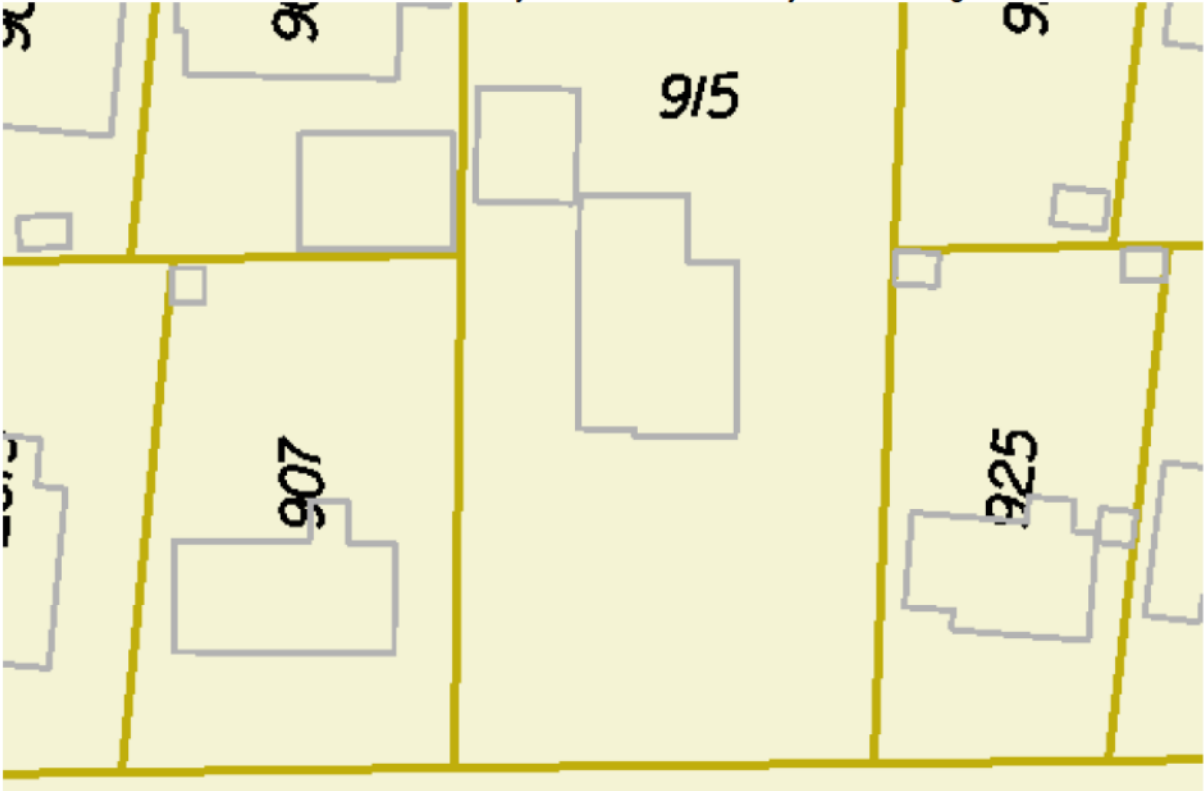
Details of stone work.



11. Streetscape - Existing

The existing dwelling faces south and is set well back from the road as evidenced by the City of Mississauga property mapping (below). Both houses on either side of the subject property appear to be Victory style housing built in the 1940's. The house to the west, 907 North Service Road has had a two-storey addition since original construction.

The new homes will be in situated with similar front yard setbacks to the adjacent dwellings.



12. Mandatory Recommendations

The subject property does not meet the criteria for heritage designation under Ontario Regulation 9/06 of the Ontario Heritage Act.

Subsection (2) sets out the criteria by which consideration is given in determining whether a property is of cultural heritage value or interest. It is our opinion that the property does not have cultural heritage value or interest as supported by the following points:

1. The property has design value or physical value

The home and detached garage were designed by D.C. Cotton and built by William Henry Hedge in and around 1928/29. The detached garage and addition were completed in 2003. Neither the home or addition achieve the standard established in any of the three criteria in the Ontario Heritage Act Regulation 9/06 for determining whether a property has design or physical value. It is not rare or unique. While it is representative of an early example of a style; Arts and Crafts bungalow; type, expression, material or construction method; the alterations and addition undertaken in 2003 have negated any value had by that representation. It does not display a high degree of craftsmanship or artistic merit. It does not demonstrate a high degree of technical or scientific achievement. The three criteria for determining design value or physical value are not met.

2. The property has historical value or associative value.

None of these three (3) criteria are met either. William Henry Hedge who built and lived in the dwelling is not known for any significant contribution to Mississauga. Although the home was designed by D.C. Cotton, who has some relevance to Mississauga, his biography only lists homes built in Toronto and that he eventually went to work at St. Lawrence Starch. The property does not have direct associations with a theme or event significant to a community; it does not yield information that contributes to an understanding of a community nor does it reflect the work of a builder who was significant to the community.

3. The property has contextual value.

The home is not a landmark, nor does it define the character of the area. Further, it is not visually or historically linked to its surroundings. The home does not meet any of the regulations three criteria for determining contextual value.

13. About the Author

William Oughtred of W.E. Oughtred & Associates Inc. is a development and land use consultant who has been practicing in the Mississauga and GTA area for over twenty-five years. Mr. Oughtred has worked in the land use planning field for over 20 years, specializing in the City of Mississauga. He is well versed in both Planning and Building procedures and the City of Mississauga Zoning By-law and The City of Mississauga Official Plan.

William was born, raised and attended school in Mississauga. He is a lifelong resident and has been very active in the Mississauga community through his other interests and pursuits including volunteering on the Spring Creek Cemetery Board.

William specializes in infill type development projects which typically require attendance before the Committee of Adjustment in connection with Applications for Consent or Minor Variance. His twenty years of experience has afforded him the opportunity to see the City evolve and be at the forefront of evolving trends and patterns in land development in Mississauga. William has been involved in the City of Mississauga's challenge in dealing with the pressures created by the infill housing that has occurred in the south part of Mississauga. His experience in shepherding development applications through the approval process and dealing with the community, City staff and the Members of Council provides an insight into the market for redevelopment that has focused its attention on this community.

Heritage Impact Statements have been completed for the following properties located in Mississauga:

- 276 Arrowhead Road
- 1510 Stavebank Road
- 1267 Mississauga Road
- 2701 Mississauga Road
- 123 Kenollie Avenue
- 1168 Mississauga Road
- 4077 Mississauga Road
- 92 Pinetree Way
- 169 Donnelly Drive
- 1445 Glenburnie Road

14. References

<http://www.ontarioarchitecture.com/>

<http://www.mississauga.ca>

Wikipedia

Canadiana Room, Mississauga Central Library

Dixie - Orchards to Industry, Kathleen Hicks

Apple Blossoms and Satellite Dishes, Dave Cook

Appendix A; History of the QEW (source: Wikipedia)

Route description

The Middle Road or Commissioners' Road,^[3] named so because of its location between what were then the highways between Toronto and Hamilton, [Dundas Street](#) and [Lake Shore Road](#), followed the same path taken by today's Queen Elizabeth Way (QEW), with little exception, beginning at the [Humber River](#) and travelling to Bronte Line (where today, the QEW veers south). Since the highway followed a [concession road](#),^[4] it was straight for almost its entire length, veering only at Browns Line (Highway 27), the [Credit River](#), and at the Oakville border. At the time of construction, this route was surrounded by farmland, as most of the towns in the area at the time lay along Dundas or Lake Shore, to the north and south respectively. However, the highway spurred rapid development of the surrounding land;^[citation needed] it did not take long for farmland to become subdivisions. Middle Road was also a concession created between 1st Concession South and 2nd Concession South during the early 1800s.



Bronte Creek Bridge, 1936. The old Middle Road bridge is visible in the foreground.

Most of the route was paved with concrete.^[5] The two roadways were separated by a [median](#) varying in width,^[Note 2] often filled with trees. Most of the rows of trees along the old country lane were incorporated into the median, and new ones were planted in the gaps, as equal focus was given to functionality as aesthetics.^[6] Over most of the bridges, the two roadways converged, undivided. These were, however, a great improvement from the bridges along the old lane, as seen in the picture at left. At Highway 10 (Hurontario Street) was Canada's first [cloverleaf interchange](#),^[7] opened in 1937.^{[8][Note 3]}

One of the elements of the Middle Road, to which engineers paid particular attention in future highway design, was [controlled access](#). While the section between the Humber River and Highway 27 was built without allowance for private driveways, and was thus controlled-access, many at-grade intersections lie along the Middle Road west of Highway 27, and many adjoining landowners built driveways to the highway for access, an issue that led to the creation of legislation to allow the designation of a road as 'controlled-access'.^[9]

Today, a small remnant of the pre-1930s Middle Road can be found spanning the Etobicoke Creek off of Sherway Drive at the Mississauga and Etobicoke border. The bridge, constructed in 1909, was part of Middle Road until 1932 and served as a farm lane until the section of the Queen Elizabeth Way into Toronto opened in 1940.^{[10][11]} The short section was later renamed Sherway Drive.



Middle Road in 1917.

The Middle Road was not the first highway between Toronto and Hamilton. In January 1914, a cement road known as the *Toronto–Hamilton Highway* was proposed.^[12] The highway was chosen to run along the [macadamized](#) old Lake Shore Road, instead of [Dundas Street](#) to the north, because of the numerous hills encountered along Dundas Street, which would have increased the cost of the road without improving accessibility. Middle Road, a dirt lane named because of its position between the two, was not considered since Lake Shore and Dundas were both overcrowded and in need of serious repairs.^[13] By November of that year, the proposal was approved,^[14] and work began quickly to construct the road known today as [Lake Shore Boulevard](#) and [Lakeshore Road](#) from Toronto to Hamilton. The road was finished by 1917, 18 feet (5.5 m) in width and nearly 40 mi (64 km) long, becoming the first concrete road in Ontario, as well as one of the longest stretches of concrete road between two cities in the world.^[15] The highway became the favourite drive of many motorists, and it quickly became a tradition for many families to drive it every Sunday.^[16]



The [cloverleaf interchange](#) at Middle Road and Highway 10 was the first controlled access interchange in Canada.

Over the next decade, vehicle usage increased monumentally; as early as 1920 the Lake Shore Road was once again bumper to bumper on weekends.^[17] In response, the Department of Highways once again sought out improving another road between Toronto and Hamilton. Middle Road, a continuation of Queen Street west of the [Humber River](#), was chosen to avoid delays on Dundas or Lake Shore. The road was to be more than twice the width of the Lake Shore Road, at 40 ft (12 m), and would carry two lanes of traffic in each direction.^[11] Construction on what was then known as the *Queen Street Extension* between [Highway 10](#) and [Highway 27](#) began in the spring of 1931,^[18] and between Highway 27 and the Humber River on November 1, 1931.^[19]



Middle Road in 1937, east of present day [Erin Mills Parkway](#), looking east towards Toronto.

Before the highway could be completed, the [1934 provincial elections](#) brought [Mitchell Hepburn](#) into office as premier and Thomas McQuesten was appointed the new minister of the Department of Highways.^[20] McQuesten in turn appointed Robert Melville Smith as deputy minister. Smith, inspired by the German [Autobahns](#) - new "dual-lane [divided highways](#)", separated by a depressed grass centre crossing short distances between major cities - modified the design for Ontario roads,^[21] and McQuesten ordered that the Middle Road be changed into this new form of highway.^{[22][23][24]} A [right-of-way](#) of 132 ft (40 m) was purchased along the Middle Road and construction began to convert the existing sections to a divided highway, as well as on Canada's first [cloverleaf interchange](#) at [Highway 10](#).^[11]

By the end of 1937, the Middle Road was open between Toronto and Burlington, where it connected with what was first known as the *Hamilton – Niagara Falls Highway*. It soon came time to name the new highway, and an upcoming visit by King [George VI](#) and [Queen Elizabeth](#) proved to be the focal point for a dedication ceremony. On June 7, 1939, the two royal family members drove along the highway (which now connected to Niagara Falls) and passed through a light beam at the Henley Bridge in St. Catharines. This caused two [Union Jacks](#) to swing out, revealing a sign which read *The Queen Elizabeth Way*.^[25]

However, the ceremony only designated the highway between St. Catharines and Niagara Falls. The remainder of the road was known by various names, including the *Toronto–Burlington/Hamilton Highway* and *The New Middle Road Highway*. At the formal opening of the highway between Toronto and Niagara Falls on August 23, 1940, the entire length was declared *The Queen Elizabeth Way* by Thomas McQueston.^[25]

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AMENDMENT TO HERITAGE IMPACT STATEMENT (2015) **915 NORTH SERVICE ROAD, CITY OF MISSISSAUGA**

PURPOSE

This addendum is an amendment to the *Heritage Impact Statement* report submitted by W.E. Oughtred & Associates Inc. in March 2015. The purpose of this amendment is to:

1. address changes in the heritage status of the property related to its Designation under Part IV of the Ontario Heritage Act in 2016,
2. provide an update on the current condition of the interior and document two non-historic outbuildings that the owner proposes to demolish, and
3. propose an alternative development option to allow for redevelopment of the site and conservation of heritage values as identified in *Designation By-law 0021-2016*.

BACKGROUND

On August 8, 2014, the Committee of Adjustment approved Consent Applications B49/14, B50/14 and B51/14, submitted by Michael Denham on behalf of the property owners, to allow creation of a new residential lot fronting on the North Service Road. This approval was subject to several conditions, including submission of a *Heritage Impact Statement (HIS)* because the property was listed on the Municipal Heritage Register.

An *HIS* was prepared by W.E. Oughtred & Associates and submitted in April 2015. That report concluded that structures located on the property, including the c. 1928 William Hedge farmhouse designed by architect Dixie Cotton Cox, did not meet criteria for Designation under the Ontario Heritage Act. The proposal for redevelopment of the site at that time involved demolition of the William Hedge House and subdivision of the lot into four parcels for new residential dwellings, 2 fronting on the North Service Road and 2 on Ribston Road.

Heritage staff reviewed this report and carried out their own research and evaluation and concluded that the property did meet criteria for Designation and recommended that the property be Designated under Part IV of the Ontario Heritage Act. At the June 2015 meeting of the Heritage Advisory Committee, W.E. Oughtred & Associates made a presentation and requested a deferral of the decision to allow for an on-site meeting to take place with heritage staff and members of the Heritage Committee. This on-site meeting took place on July 15, 2015 and alternative development options were discussed, specifically relocation of the house closer to the North Service Road and the west side property line.

On September 22, 2015 the owner submitted an application to sever the rear section of the lot and create two lots on Ribston Road (B53/16 and B54/16). Due to the location of the Hedge Farmhouse in the centre of the lot, this application required minor variances for the rear yard set-backs. Due to its heritage status as a Part IV

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Designated heritage property, the Committee of Adjustment has deferred all applications related to this property until a Heritage Permit has been issued.

During this period of uncertainty regarding potential re-development options, the house sat vacant and un-serviced. In March 2016, a water pipe burst in the 2nd floor bathroom. This leak was not detected right away and resulted in considerable damage to the interior. After the flooding, all of the interior plaster and lathe on the ground floor was removed.

In October 2016, the owner retained Megan Hobson to provide heritage consulting services. Megan Hobson met with the owner on site on October 14, 2016 to discuss a revised development plan that involved relocating the house on the site. Dan Myette, President of Danco Movers, was consulted on site to determine if relocation was feasible and to determine what architectural components would be impacted by the proposed methodology for moving the house. It was determined that relocation would impact the lower portion of the masonry walls and the base of the front porch because the house has a raised basement. These impacts could be fully mitigated but would require a Moving Plan, a Conservation Plan and specialized masonry expertise. After relocation and subdivision of the lot, the Hedge house would retain its orientation towards the North Service Road but it would no longer be sited on a large lot, one of the attributes included in the *Reasons for Designation*. This impact could be partially mitigated by installing some form of marker that denotes the current property lines, such as a low stone wall, hedging, or a row of apple trees but this would be difficult to implement across four properties in separate ownership. For these reasons, an alternative option has now been developed so that the house can remain *in situ* on a double sized lot that will retain the existing frontage on the North Service Road. The rear portion of the lot will be severed to create two new residential lots on Ribston Road for single-detached dwellings.

HERITAGE RESOURCE

See Appendix A: Designation By-law 0021-2016

William Hedge Farmhouse

The *Designation By-law* includes a *Statement of Heritage Value* and a *Description of Heritage Attributes* that is included as an Appendix to this Addendum. The subject property contains a c. 1928 stone house that has been identified as a rare example of a 'Craftsman Bungalow' in Mississauga. The physical/design values are the primary heritage values and these are expressed in the design and craftsmanship of the original farmhouse.

The house has historical associations with the Hedge family and with architect Dixie Cox Cotton. These associations have significance to the local community. The contextual value associated with the house, such as its location on the historic Middle Road (now the North Service Road) and its connection with agricultural land uses prior to the building of residential subdivisions in the 1950s, has been much altered and impacted by later development. The current lot size and configuration reflects the period after 1952 when the farmland associated with the William Hedge Farmhouse was subdivided and developed as a residential subdivision.



Hedge Farmhouse designed by Dixie Cox Cotton in 1928 for William Hedge. With the exception of the dormers that have been altered, the house retains its original features and is a good example of a Craftsman bungalow.

DOCUMENTATION

See Appendix B: Site Photos (October 14, 2016)

See Appendix C: As Found Drawings

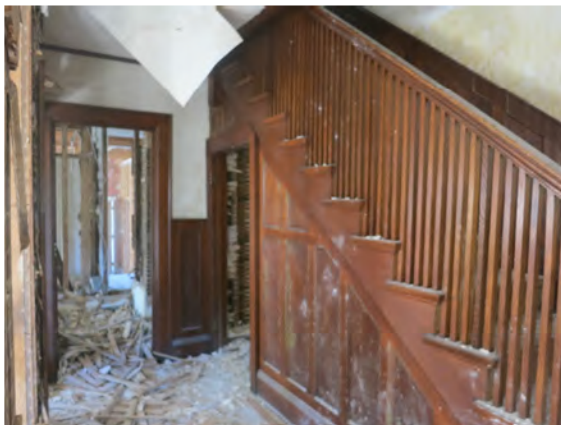
The HIS (2015) prepared by W.E. Oughtred & Associates provides a thorough history of the site and documentation of the original farmhouse built by William Hedge c. 1928, including the original architectural drawings by Dixie Cox Cotton.

This Addendum documents the current condition of the interior as well as 2 non-historic outbuildings to be demolished that were not included in the earlier report.

Current Condition of the Interior

In the winter of 2016 services were disconnected from the vacant house. In March, after several days of very cold weather, a water pipe froze and burst in the 2nd floor bathroom. The leak was not detected for several days and as a result there was a considerable amount of water that ran down and collected in the basement. After the flooding was discovered, water-soaked materials and materials susceptible to mold growth were removed from the interior as a precautionary measure, to prevent further mold growth occurring. With the exception of wood flooring in the bedroom adjacent to the bathroom where the pipe burst, the 2nd floor was not damaged and all original features are intact and in good condition. Extensive mold growth was observed on the drywall throughout the basement level.

On the 1st floor, all of the plaster and lathe has been removed from the walls and the interior wood framing is now exposed. The plaster ceilings are intact and all of the wood cornice trim is intact and in good condition. All wood baseboards have been removed and some wood trim around door and window openings has been removed. Interior wood doors, including wood paneled doors and French doors, have been removed but have been retained for re-use.



Left: Plaster and lathe has been removed from the 1st floor walls but the wood elements are intact

Right: Doors on the 1st floor have been removed but have been retained for re-use

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Wood paneling on the main staircase is intact and in good condition, with the exception of a small section at the base of the stairs that is badly warped and water damaged. Other than this localized damage, other components of the staircase, including the wood treads, risers and hand railing, are intact and generally in good condition. Wood paneling on the other side of the hallway has been removed.



Left: Water damage caused by flooding due to a burst pipe in the bathroom above the dining room

Right: A section of the paneling at the base of the stairs is warped and split and will require replacement

The stone fireplace in the front living room has had the wood mantelpiece and built-in cabinetry removed but the masonry is intact. The mantelpiece components have been retained so that it can be rebuilt.

The stone fireplace in the rear family room has been partially demolished. This fireplace does not appear on the original drawings. Although the masonry and millwork was carefully done to match the living room fireplace, the family room fireplace is not original to the house.



Left: Living Room fireplace – the mantle and integrated bookshelves have been removed.

Right: Family room fireplace – the mantle and some of the stone has been removed

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The kitchen has been gutted. Features included in Item I) of the List of Heritage Attributes in the Designation By-law have been removed, ie; "original shaker style stained oak cabinetry, sink, plumbing fixtures and hardware of the style". These elements were documented on pages 44 to 47 of the *Heritage Impact Assessment* prepared by William Oughtred & Associates in 2015.



The kitchen cabinetry and fixtures have been removed.

Outbuildings

There are two outbuildings located behind the William Hedge Farmhouse. The detached 2-car garage was built in 2003 when the rear addition was added. It is wood frame construction covered with modern stucco. There is a wooden shed that is an older building and may have been used as a garage prior to construction of the 2-car garage in 2003. Based on its simple framing, wood siding, wood doors and multi-pane wood window, it was probably built in the 1930s or 40s. It does not have any Craftsman style design features associated with the farmhouse.



Left: Detached garage constructed in 2003 does not have heritage value



Right: Utilitarian wooden shed located in the rear yard does not have heritage value

Context

As noted in the HIS (2015), the context of the William Hedge Farmhouse has undergone significant change since c. 1928 when the house was surrounded by farmland. When originally constructed the house fronted on the Middle Road, an historic route between Toronto and Hamilton that was half way between Dundas Street (now Highway 5) and the Lake Road (now Lakeshore Boulevard). In the early 1930s, in an effort to alleviate traffic congestion, the Middle Road was improved and was subsequently widened and paved to become the Queen Elizabeth Highway, the first divided highway in Canada.



Historic character of the Middle Road (c. 1940 photo taken east of Port Credit)

In the 1950s, this area was transformed by large-scale residential development when hundreds of acres of farmland on either side of the QEW between Cawthra and Dixie were purchased by local developer Gordon S. Shipp (1891-1981). The subject property is located in the Applewood Acres subdivision. When this subdivision was laid out some of the original apple trees were retained and Hedge Drive was named after the Hedge family. The Hedge Farmhouse was retained on a large through-lot with frontage on the North Service Road and Ribston Road. The surrounding land was laid out with new streets and small residential lots for bungalows. The lot retained for the Hedge Farmhouse is comparable in size to 4 lots in the new subdivision.

This massive post-War suburban development, that included large residential subdivisions and amenities such as schools and shopping centres, resulted in the creation of a new interchange at Dixie and construction of north and south service roads to improve traffic safety on the Queen Elizabeth Highway. With these changes, the William Hedge farmhouse now fronted directly on the North Service Road and today faces a massive barrier wall between the North Service Road and the Queen Elizabeth Highway, a 400 series 8-lane highway.

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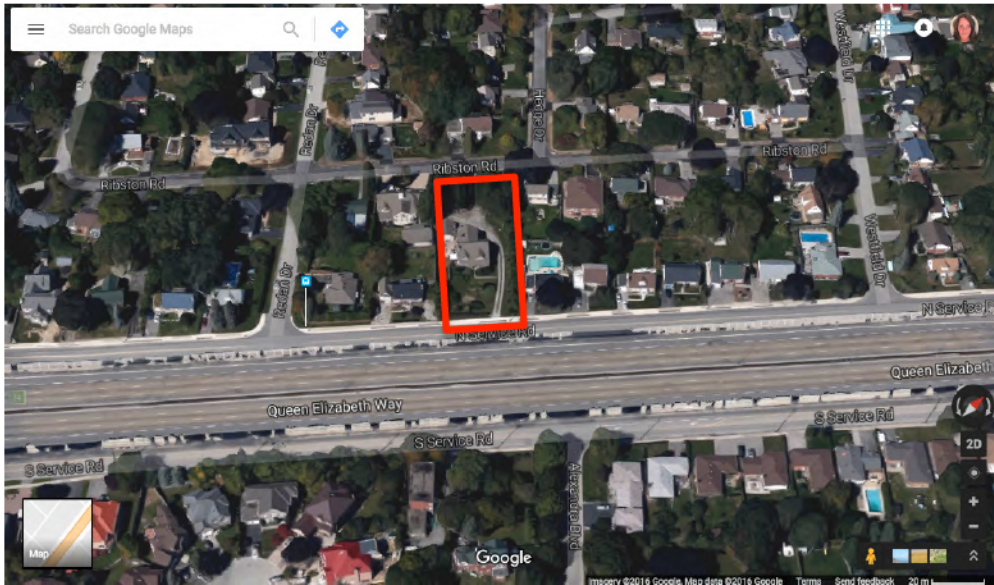
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Subject property fronts on the North Service Road



Subject property faces a high barrier wall between the North Service Road and the Queen Elizabeth Highway.

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PROPOSED DEVELOPMENT**See Appendix D: Proposed Development**

The original proposal for this site was to demolish the house and subdivide the land into 4 lots so that 4 new single-detached dwellings could be constructed, 2 fronting on the North Service road and 2 fronting on Ribston Road.

However, upon receipt of an application to demolish the listed heritage house, the City of Mississauga responded by Designating the property under Part IV of the Ontario Heritage Act. The Designation By-law includes a *Statement of Cultural Heritage Value* (SCHV) that states that the property primarily has value as a rare example of the Craftsman Bungalow style within the City of Mississauga and a good example of the work of local architect Dixie Cotton Cox.

Additionally, the SCHV states that the property contributes to an understanding of the development of Mississauga because it is a remnant of a larger farmstead that, since 1906 was owned by the Hedge family who operated a fruit farm here. The house was built for William Hedge in 1928 and was occupied by members of the Hedge family until 1994. The associated agricultural land was sold in 1953 to Applewood Dixie Ltd. Hedge Drive in the subdivision is named after the family.



Plans of Residence to be built on the Middle Road, Dixie, On. for Mr. Wm. Hedge, D.C. Cotton Architect (1928)

ALTERNATIVE OPTION A

Following the Designation, a revised proposal was presented informally to heritage staff and the heritage committee that involved relocation of the Hedge Farmhouse on the site so that the lot could be subdivided into 4 lots. This approach would conserve the historic house on a smaller lot on the North Service Road, and allow construction of 3 single-detached houses, one on the North Service Road and 2 on Ribston Road.

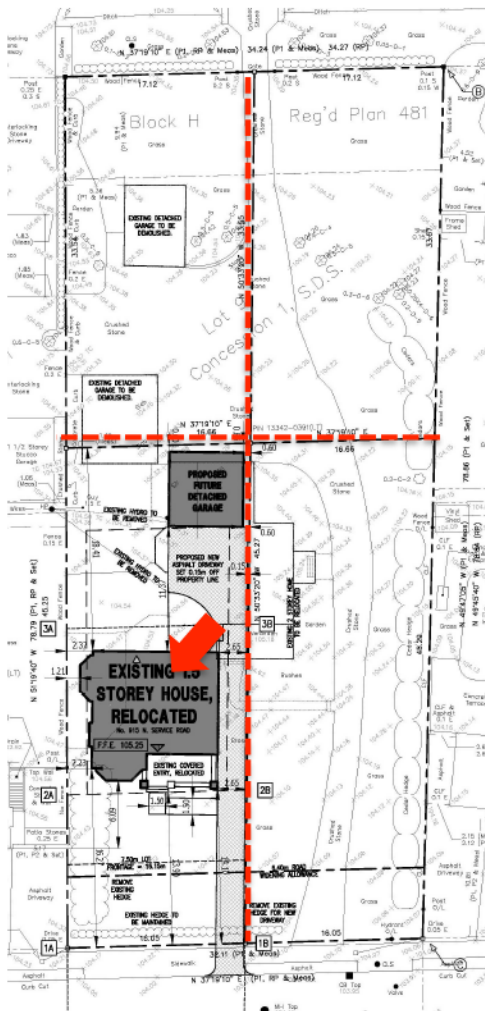


Figure 1: ALTERNATIVE OPTION A - Relocation of the William Hedge House on the site and subdivision of the property into 4 lots to create one new residential lot on the North Service Road and 2 new residential lots on Ribston Road.

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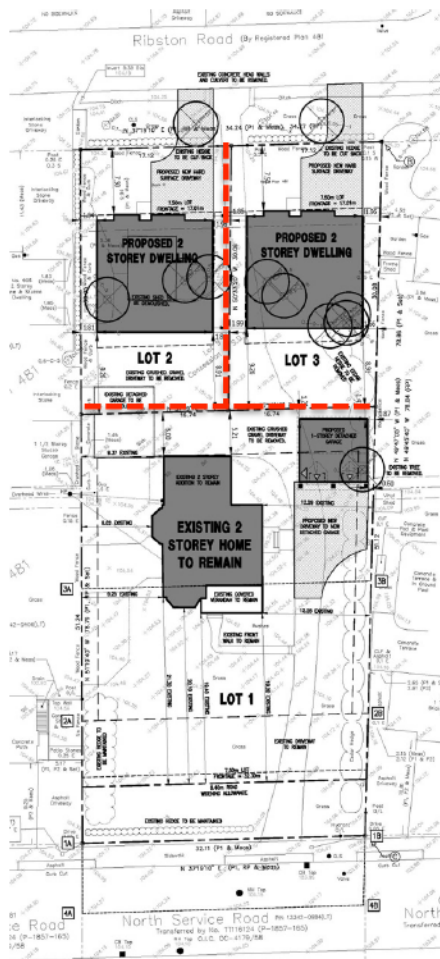
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ALTERNATIVE OPTION B (PREFERRED OPTION)

See Appendix D: Proposed Development

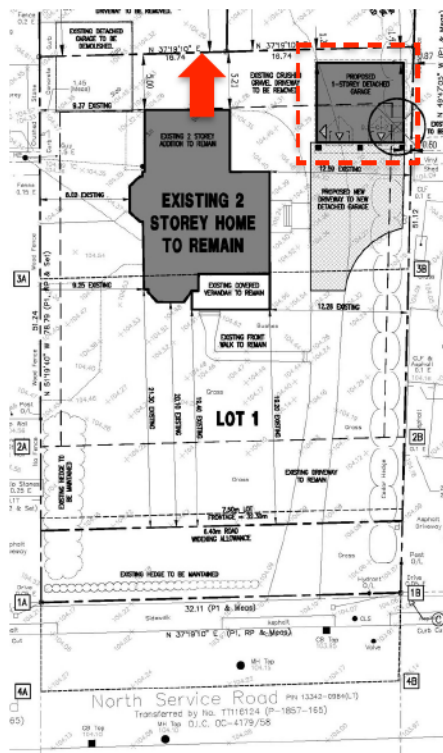
After further consultation with heritage staff, an alternative development option is being proposed that will not require relocation of the Hedge farmhouse and will retain the existing frontage on the North Service Road. The revised proposal is to sever the rear portion of the lot to create 2 new residential lots on Ribston Road. The Ribston Road lots will be approximately 17 x 30 m (55 x 98 ft), and will be similar in size and frontage to existing lots on Ribston Road and the Hedge House will be retained on a large double-sized lot, approximately 32 x 51 m (104 x 164 ft).



ALTERNATIVE OPTION B (PREFERRED OPTION) Retention of the Hedge House *in situ* on a double-lot and subdivision of the property into 3 lots to create 2 new residential lots on Ribston Road.

Hedge House (Lot 1)

The proposed severance will require a minor variance for the rear yard set-back behind the Hedge House because the yard behind the house will be reduced to 5.0 m deep, which is 2.5 m less than the minimum requirement for rear yard set backs. This is being proposed so that the Ribston Road lots can accommodate new homes without requiring any variances. This will be offset by the exceptionally large front yard and the large side yard on the east side. The side yard is large enough to accommodate a detached garage without have a negative impact on the Hedge House and the existing driveway form the North Service Road can be utilized with this configuration.



M. Hobson 915 North Service Road FINAL HIA Amendment June 2nd, 2017

Demolition of Non-historic Outbuildings

Two outbuildings located behind the Hedge House will be demolished. These structures are not included in the *Designation By-law*. The garage is a modern structure that was built in 2003 and has no heritage value. The wood shed is a utilitarian structure that is crudely constructed and in poor condition. Neither of these buildings contributes to heritage values associated with the site.

Photographic documentation of both structures is included in Appendix B of this report and no further mitigation is required.



The modern stucco garage (left) and c. 1940 wooden shed (right) to be demolished,

Tree Removals

There will be no impact to landscape elements in the front yard. One tree will be removed from the side yard to allow construction of the proposed detached garage for the Hedge House. There are 8 large conifers in the rear yard that will be removed to allow construction of two new houses on Ribston Road. These trees do not appear to be part of a designed landscape associated with the Hedge House and are not associated with circulation routes on the site. They appear to have been self seeded and are arranged in an irregular row that screens the back yard from the house.

A landscape plan for the new lots on Ribston Road could include planting trees along the rear property line to mitigate the loss of mature trees behind the Hedge House.



The rear portion of the lot to be severed.

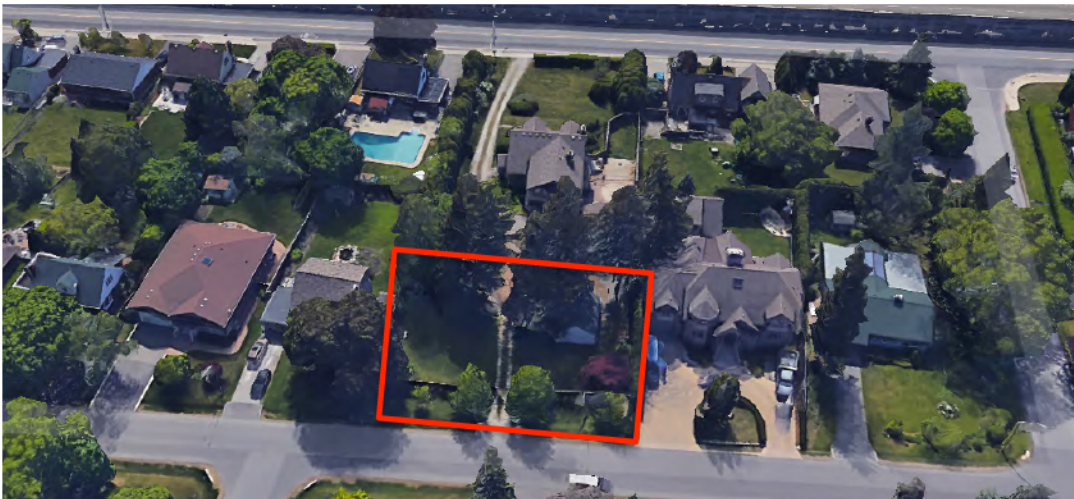
Construction of 2 New Houses on Ribston Road (Lots 2 & 3)

The construction of two new houses on Ribston Road will have very little impact on views of the Hedge House because the house is currently not visible from Ribston Road. Primary views of the Hedge House are from the North Service Road. The Hedge House is approximately 8.4 m tall to the ridge of the roof. The houses to be built behind it will be approximately 9.5 m tall to the ridge of the roof and located a sufficient distance away so that there will be no impact to the North Service Road streetscape.



North Service Road streetscape (ATA Architects). The new houses on Ribston Road will not impact views of the Hedge House from the North Service Road.

The 2 houses to be constructed on Ribston Road will be consistent with current zoning requirements in terms of building height, lot coverage and set-backs so that they will be compatible with the surrounding neighbourhood. This is a suburban neighbourhood with a mix of 1 and 2-storey single detached buildings constructed c. 1950 to the present in a variety of styles and materials. The size and configuration of the new lots being proposed and the orientation of the houses will be consistent with the existing pattern of development. In addition to appropriate massing and scale, the materials and architectural character being proposed for these houses will be compatible with the existing building stock in this area and with the historic Hedge House.



Aerial view showing the back portion of the Hedge Estate that will be severed to create two new residential lots on Ribston Road. The neighbourhood contains a mix of older housing stock and larger infill housing.

Compatibility of the new houses on Ribston Road

Drawings for the new houses to be built on Ribston Road have been included in this submission to ensure that they will be compatible with the existing neighbourhood and with the Hedge House. (See Appendix D – Proposed Development)

The height, scale and massing will be compatible with the current zoning in this area. The area is zoned R3-1 Exception and contains a mix of older 1 & ½ storey Post-War bungalows and new 2-storey homes. The maximum lot coverage allowed in this area is 35%. The proposed houses will be just under the maximum lot coverage and consistent with the minimum set-backs and the maximum roof height of 9.5 m so that no variances will be required.

Site Statistics

LOT 2	
Lot coverage	34.91 %
Roof height	9.5 m
Front yard set back	7.5 m
Side yard set back	1.81 m
Side yard set back	1.82 m
Rear yard set back	8.51 m
LOT 3	
Lot coverage	35.0 %
Roof height	9.43 m
Front yard set back	7.5 m
Side yard set back	1.82 m
Side yard set back	1.81 m
Rear yard set back	8.43 m

Architectural Character

The Ribston Road house designs are *Eclectic Neo-Traditional* in style with traditional cladding materials including buff stone veneer, lighter stone trim and cedar shingle roofs. The window to wall ratio is similar to historic masonry buildings and the windows have a traditional rectangular shape (taller than they are wide). The windows are anodized aluminum but are multi-paned sash and casement style windows similar to historic wood windows. Although generally box-like, the roof and front wall plane are somewhat articulated and there is an asymmetrical arrangement of elements on the façade similar to Queen Anne or Arts & Crafts style house designs. The materials and design are therefore complimentary to the stone construction and Arts & Crafts style of the Hedge House.

House proposed for Lot 2

The house proposed for Lot 2 is two storeys in height with a full basement and an integrated two-car garage. The garages are staggered and one has an arched opening to give visual interest and articulation to the main façade. The entry is flanked by slightly projecting bays with gable roofs. The roof is hipped with one small dormer in the centre with an arched top. Windows on the main elevation are a mix of arched and flat-topped openings and vary in size. The walls are clad with a buff stone veneer with lighter stone trim around door and window openings. There is a small balcony with wrought iron railings on the main elevation and two stone clad chimneys, one on the end wall and one in the rear.



House proposed for Lot 2 on Ribston Road; front elevation and ground floor plan.

House proposed for Lot 3

The house proposed for Lot 3 is two storeys in height with a full basement and an integrated two-car garage. The main entry and one of the garage bays project slightly to give articulation to the wall plane. The façade is generally symmetrical with multi-pane casement style windows of similar sizes and shapes. The hipped roof has two small dormers with arched tops and two sky-lights.



House proposed for Lot 2 on Ribston Road; front elevation and ground floor plan.

CONCLUSIONS

Alternative Option B is the preferred option. This approach will conserve the following physical/design values associated with this property:

- Example of an architectural work by Dixie Cox Cotton (1882-1943), who is an architect of significance to the community because he is a native of Port Credit and had a successful architectural practice in Toronto and the municipality of Mississauga.
- Rare example of a Craftsman Bungalow in Mississauga that displays a high degree of design and craftsmanship, specifically in the exterior masonry and the interior millwork.

This approach will conserve the following historical/associative values associated with this property:

- Association with Dixie Cox Cotton (1882-1943), grandson of area pioneer Robert Cotton (1809-1885) and nephew of Dr. Dixie Beaumont Cotton, after whom the village of Dixie was named.
- Association with the Hedge family, fruit growers who owned a larger farmstead that included this property in the period c. 1906, specifically William Henry Hedge (1877-1941) who commissioned Dixie Cox Cotton to design the house and is said to have built the house himself with stone from Milton.

This approach will conserve the following contextual values associated with this property:

- Orientation towards the North Service Road, a service road along the Queen Elizabeth Highway (QEW) that is historically associated with the Middle Road, a historic road between Toronto and Hamilton between Dundas Street (Highway 5) and Lakeshore Road.
- Contributes to an understanding of the history of land use and urban design in the City of Mississauga, specifically the farmland that predates the Applewood Acres subdivision that was created in the early 1950s and the relationship of the William Hedge Farmhouse to the Middle Road, prior to construction of the Queen Elizabeth Highway and the North Service Road.

RECOMMENDATIONS

The proposed development will retain heritage values associated with this site and will contribute to the long-term conservation of the Hedge farmhouse and its distinctive presence on the North Service Road. The following is therefore recommended:

1. Allow demolition of 2 outbuildings at the rear of the property that do not have heritage value.
2. Allow removal of 7 conifers and a cedar hedge in the rear yard that do not have heritage value.
3. Allow subdivision of the lot into three parcels, thus creating two new residential lots on Ribston Road and retaining the Hedge Farmhouse *in situ* on a double lot on the North Service Road.
4. Allow construction of the proposed 2-storey houses on Ribston Road.
5. Amend the Designation By-law after the severance has occurred to reflect the new lot boundaries.
6. Require the owner to come back to the Heritage Committee next month for a separate Heritage Permit related to interior renovations to the Hedge House, restoration and repair of damaged heritage features, and construction of the new garage. The second Heritage Permit Application will include a detailed Conservation Plan, building permit drawings for the Hedge House and the proposed new garage.

APPENDIX A – DESIGNATION BY-LAW



THE CORPORATION OF THE CITY OF MISSISSAUGA

BY-LAW NUMBER 0021-2016

A By-law to designate the William Hedge Farmhouse
located at 915 North Service Road as being of cultural
heritage value or interest

WHEREAS the *Ontario Heritage Act*, R.S.O. 1990, C. O.18, as amended ("Heritage Act") authorizes the Council of a municipality to enact By-laws to designate real property, including all the buildings and structures thereon, to be of cultural heritage value or interest;

AND WHEREAS Council for The Corporation of the City of Mississauga (the "City") approved the designation of the property known as the William Hedge Farmhouse located at 915 North Service Road in the city of Mississauga (the "Property") as being of cultural heritage value or interest through Resolution 0196-2015;

AND WHEREAS in accordance with the requirements of the Heritage Act, a Notice of Intention to designate the Property was published and served and no notice of objection to its designation was received by the Clerk of The Corporation of the City of Mississauga;

NOW THEREFORE the Council of The Corporation of the City of Mississauga hereby ENACTS as follows:

1. That the property, including all the buildings and structures thereon, known as the William Hedge Farmhouse, located at what is municipally known as 915 North Service Road in the city of Mississauga and legally described in Schedule 'A' attached hereto (the "Property"), is hereby designated as being of cultural heritage value or interest under Part IV of the *Ontario Heritage Act*, R.S.O. 1990, C. O.18, as amended.
2. That the reasons for designating the Property are duly set out in Schedule 'B' attached hereto.
3. That the City Clerk is hereby authorized to cause a copy of this By-law to be served upon the owner of the Property and upon the Ontario Heritage Trust and to cause notice of this By-law to be published in a newspaper having general circulation in the city of Mississauga.
4. That the City Solicitor is hereby directed to register a copy of this By-law against the Property in the proper land registry office.
5. That Schedules 'A' and 'B' form an integral part of this By-law.

ENACTED AND PASSED this 10 day of February, 2016.

Bonnie Crombie

MAYOR

Crystal Green

CLERK

APPROVED AS TO FORM City Solicitor MISSISSAUGA	
<u>M. Beck</u>	
Date	<u>Feb 3 16</u>

SCHEDULE 'A' TO BY-LAW 0021-2016

Summary: All of Block H, Registered Plan 481
Part of Lot 9, Concession 1, South of Dundas Street
(To be designated under the Ontario Heritage Act)

(Ward 1, City Zone 13, in the vicinity of Cawthra Road and North Service Road)

Legal Description: In the City of Mississauga, Regional Municipality of Peel, (Geographic Township of Toronto, County of Peel), Province of Ontario and being composed of all of Block H, Registered Plan 481 and Part of Lot 9, Concession 1, South of Dundas Street, of the said Township, as in Instrument R01073948.



Alnashir Jeraj
Ontario Land Surveyor

SCHEDULE "B"

Reasons for Identification

Description of Property

The property known as 915 North Service Road is located on the North Side of North Service Road on Concession 1, Part of lot 9 in the City of Mississauga. It is located in the vicinity of Westfield Drive and North Service Road.

The property contains a single family house, a detached garage and a shed. The single family dwelling - the William Hedge farmhouse - is most easily identified by its one and half storey form, side gabled roof with two dormers, and buff limestone cladding extending to the top of the windows on the second floor. The front entrance faces south, it has two bay windows on the ground floor (south and west side), a front porch with thick stone columns. The house is well set back on the lot.

Statement of Cultural Heritage Value or Interest

The William Hedge Farmhouse's cultural heritage value lies in it being a rare example of the Craftsman Bungalow style within the City of Mississauga with buff, rough cut (rusticated) limestone cladding sourced in the vicinity of the municipality. It has interior features telling of the era including cabinetry and millwork, plumbing and heating fixtures. Built in 1928, its architectural form, style and detailing reflect the design work of a local architect: Port Credit born and raised, Dixie Cox Cotton.

The Cultural Heritage Value also lies in its historic association with this architect. He was the grandson of area pioneer Robert Cotton and the nephew of Dr. Dixie Beaumont Cotton, after whom the village of Dixie was named. Dixie Cox Cotton was active in the community: he was maintenance Engineer for the St. Lawrence Starch Co. (a major locally based Canadian Industry) for over twenty years and is attributed for the design of various buildings in the community, reflecting the mainstream architectural design ideas of the time. These were based on references to vernacular and classical architecture within the British Empire, high quality craftsmanship and design, and integration of the arts and architecture as expressed in the Craftsman Bungalow, Edwardian, and Institutional and Commercial Period Revival buildings. Design ideas were carried into interior elements of the house displaying attention to detail in interior design and craftsmanship such as stonework and millwork. The house therefore demonstrates his work, the work of a significant architect to the community. The William Hedge house also has the potential to yield information to the understanding of a community. The farmhouse was built prior to the existence of the Queen Elizabeth Way as a highway, and was retained by the family within the Applewood subdivision of 1953, maintaining its orientation of its original frontage on Queen Elizabeth Highway, known as Middle Road at the time the house was designed.

Description of Heritage Attributes

The property at 915 North Service Road has cultural heritage value as it satisfies the criteria for Determining Cultural Heritage Value or Interest set out in Regulation 9/06 of the Ontario Heritage Act. The following are the key exterior and interior attributes as a rare example of the Craftsman Bungalow style within the City of Mississauga and as a reflection of the work by D.C. Cotton, architect:

1. The property has design and physical value in its architectural value as a rare example of the Craftsman Bungalow style within the City of Mississauga. The house features recognizable design characteristics of the style, including:
 - a) 1 and half storey massing
 - b) almost square plan, with protruding bay windows on the south and west wall, protruding stout stone chimney on the west wall
 - c) relatively low floor to ceiling heights

- d) low-slung gabled roof with dormers
- e) front porch with thick stone columns
- f) rusticated buff limestone exterior building material, laid in a split course bond, mortar joints that accentuate the bond pattern of the wall.
- g) "punched" style masonry openings for windows, with segmental arch, key stone and straight cut voussouirs
- h) exterior stone extends to the top of the 2nd floor window level and in all facades of the original portion of the house
- i) stone is sourced from Milton
- j) wood three over one pane sash style windows arranged in a variety of compositions: singles, pairs or threes
- k) interior layout with centre hall plan with staircase in main hall
- l) original kitchen shaker style stained oak cabinetry, sink, plumbing fixtures and hardware of the style
- m) stained wood millwork such as wainscoting, mission style balustrade
- n) limestone fireplaces and built in book case found in the house designed in an integrated way with the fireplace wall
- o) orientation of the house on the lot

2. The house has associative and historical value because:

- a) It has direct associations with Dixie Cox Cotton, architect born and raised in Port Credit, who is native of Port Credit, Mississauga. He studied at the University of Toronto, and worked both in Toronto and his home town. He is a rare architect born and raised in the municipality known to the community that lived and produced work in the early 20th century in Mississauga, contributing to the building of the character of the municipality as we know it today.
- b) The house has the potential to yield information that contributes to the understanding of a community and culture because the house was built on farmland which was subdivided into suburban lots in the early 1950s. The Hedge family farmhouse stood in the family's fruit farm originally run on the lands. The Hedge family presumably farmed the land since 1906. Hedge Drive in the subdivision was named after the family. The orientation of the house facing North Service Road as the front entrance is reflective of an earlier time, prior to the building of the Queen Elizabeth Way as a multilane highway in the 1950s. The incorporation of the William Hedge Farmhouse, within the 1953 subdivision and retention to today provides a tangible representation of the history of land use and urban design in the City of Mississauga and it can yield information as to the history of a community.
- c) The house demonstrates the work of Dixie Cox Cotton, an architect who is significant to the community. Dixie Cox Cotton is attributed with having designed a number of buildings in the community and Toronto, reflecting the mainstream architectural design ideas of the time, which were based on references to vernacular and classical architecture within the British Empire, high quality craftsmanship and design, and integration of the arts and architecture as expressed in the Craftsman Bungalow, Edwardian, and Institutional and Commercial Period Revival buildings. The ideas reflected in the execution of the interior of the house speak of innovations in middle class domestic architecture in order to achieve practicality while maintaining high quality craftsmanship. This is specially expressed in the kitchen cabinetry materiality and design, including the sink with interior plumbing (faucet) and millwork found throughout the house.

The physical/design attributes listed in point one are also the materialization of the historical and associative value. In addition to these attributes, the following lend the property its historical/associative value:

- Orientation of the front entrance towards North Service Road
- Siting within a large lot that is distinctive from the neighbouring properties

APPENDIX B: SITE PHOTOS (October 14, 2016)

Current Condition of Interior



Figure 1: GROUND FLOOR - Front stair hall, view toward front entrance.



Figure 2: MAIN FLOOR - View from living room into front stair hall.



Figure 3: MAIN FLOOR - Detail of wood paneling on main stairs.



Figure 4: MAIN FLOOR - Detail of front stair newel post and hand rail



Figure 5: 2ND FLOOR - Upstairs landing, view looking down to main entrance.



Figure 6: 2nd FLOOR - Detail showing water damaged wood flooring.



Figure 7: MAIN FLOOR - Living room fireplace.



Figure 8: MAIN FLOOR - Living room fireplace and mantle-piece components.



Figure 9: MAIN FLOOR - Living room cornice trim.



Figure 10: MAIN FLOOR - Living room bay window.



Figure 11: MAIN FLOOR - Living room cornice trim.



Figure 12: MAIN FLOOR - View from living room into family room.



Figure 13: MAIN FLOOR - View from kitchen into dining room.



Figure 14: MAIN FLOOR - Dining room.



Figure 15: MAIN FLOOR - Kitchen



Figure 16: MAIN FLOOR - Kitchen



Figure 17: GROUND FLOOR - Stairs to the rear addition and basement level.



Figure 18: GROUND FLOOR - Rear addition, view into the historic portion of the house.



Figure 19: GROUND FLOOR - Rear addition



Figure 20: GROUND FLOOR - Rear addition.



Figure 21: BASEMENT - Main room.



Figure 22: BASEMENT - Bathroom

DETACHED GARAGE & SHED TO BE DEMOLISHED



Figure 32: DETACHED GARAGE – front elevation



Figure 33: DETACHED GARAGE - side elevation



Figure 34: SHED – Front elevation.



Figure 35: SHED - rear elevation

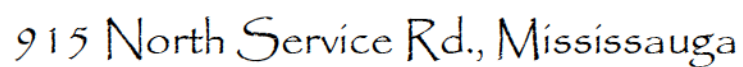


Figure 36: SHED - Side elevation.



Figure 37: SHED - Side elevation.

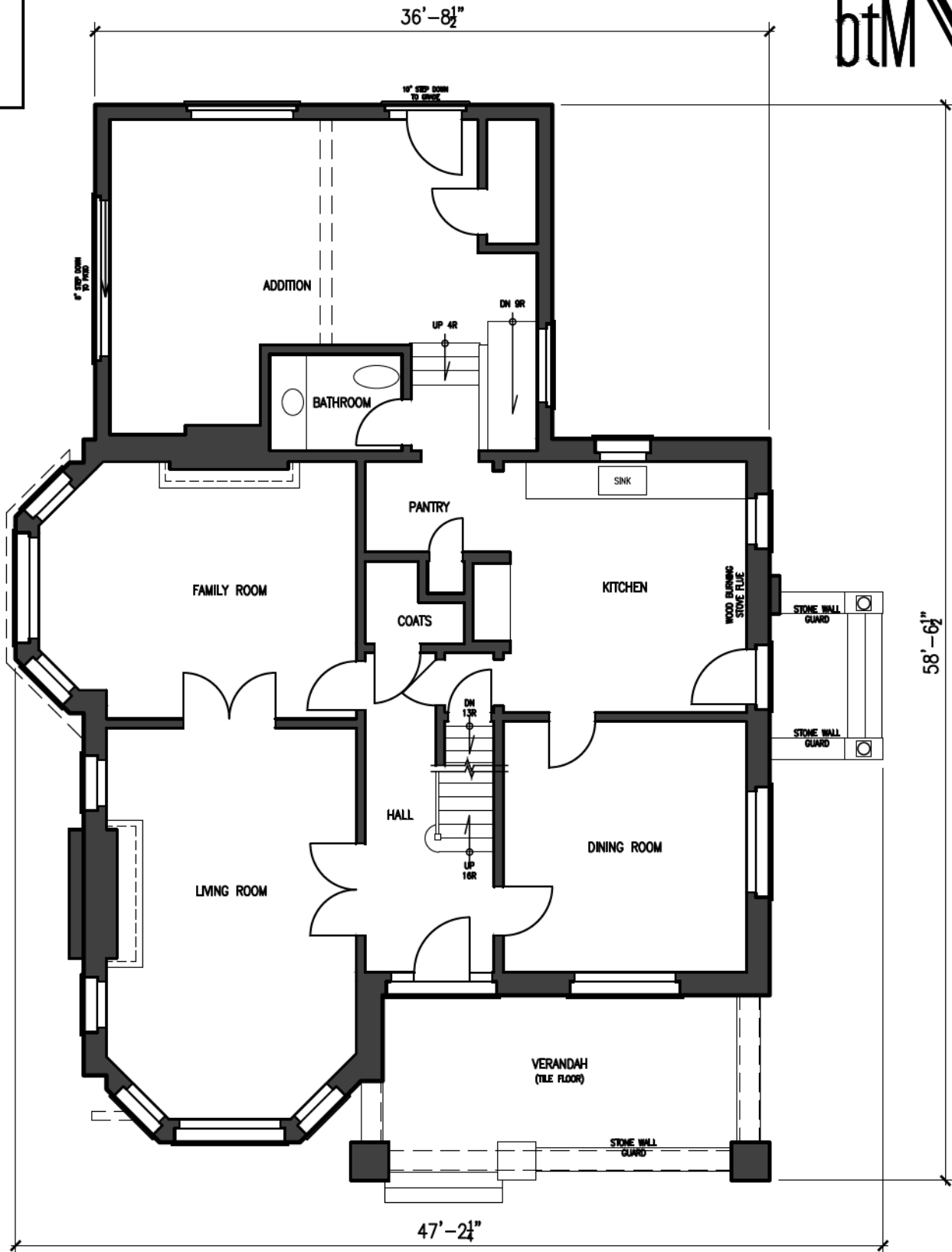
APPENDIX C – AS-FOUND DRAWINGS


$$1/8'' = 1'-0''$$

7.1 - 123



EXISTING DETACHED GARAGE



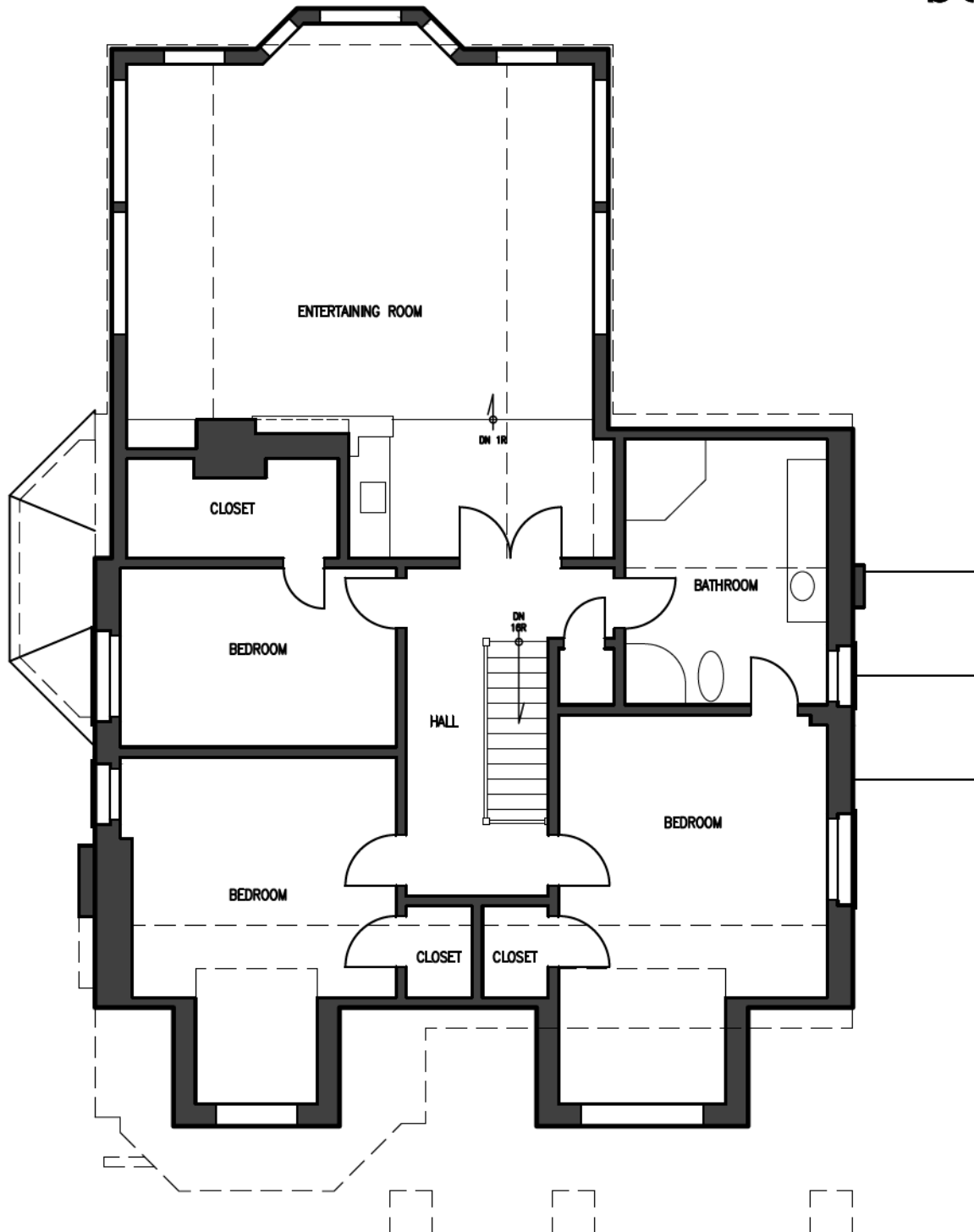
915 North Service Rd., Mississauga

JULY 18, 2016

174-15

EXISTING MAIN FLOOR PLAN
1,740 sq.ft.

1/8" = 1'-0"



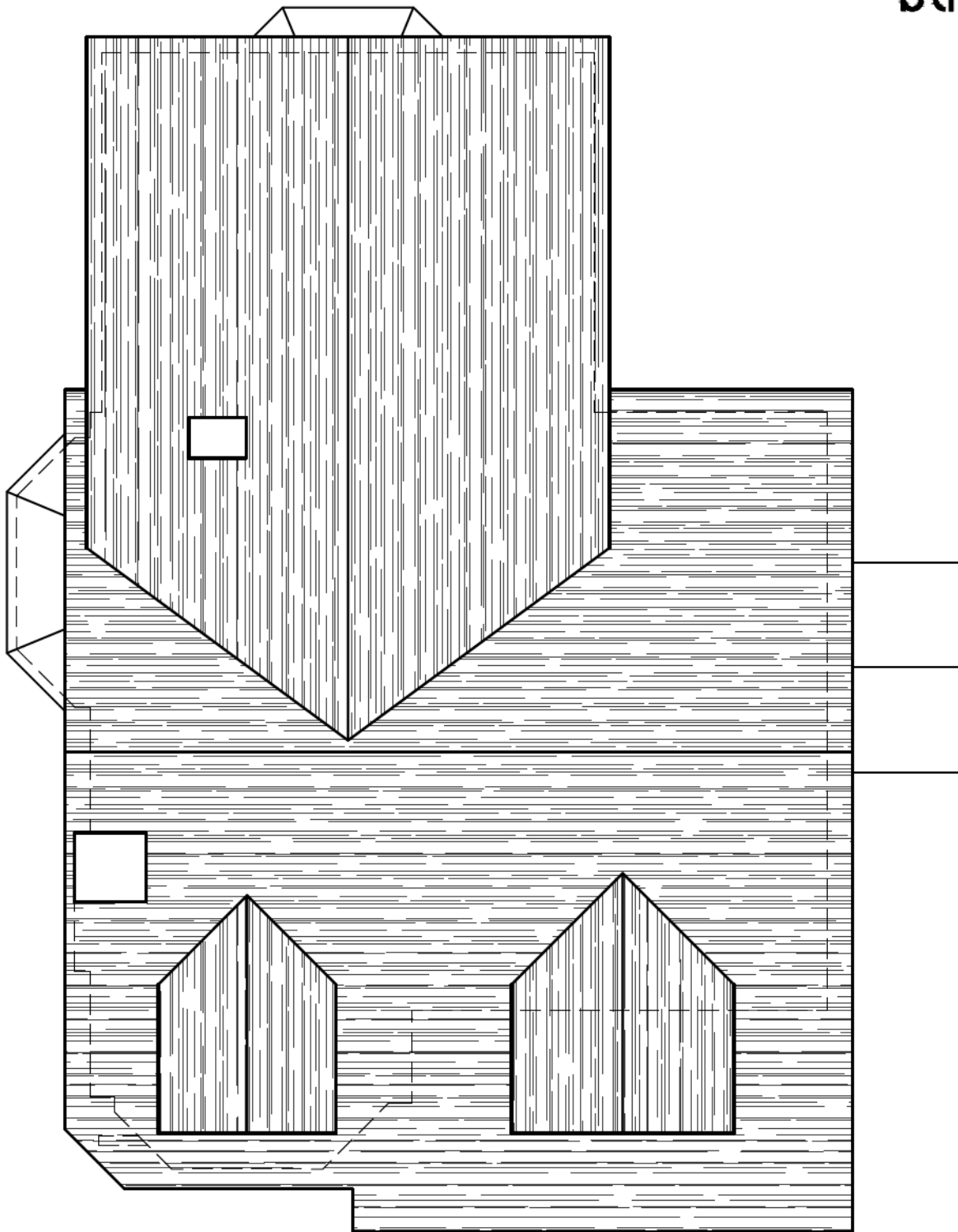
915 North Service Rd., Mississauga

JULY 18, 2016

174-15

EXISTING SECOND FLOOR PLAN
1,645 sq. ft.

1/8" = 1'-0"



915 North Service Rd., Mississauga

JULY 18, 2016

174-15

EXISTING ROOF PLAN

1/8" = 1'-0"



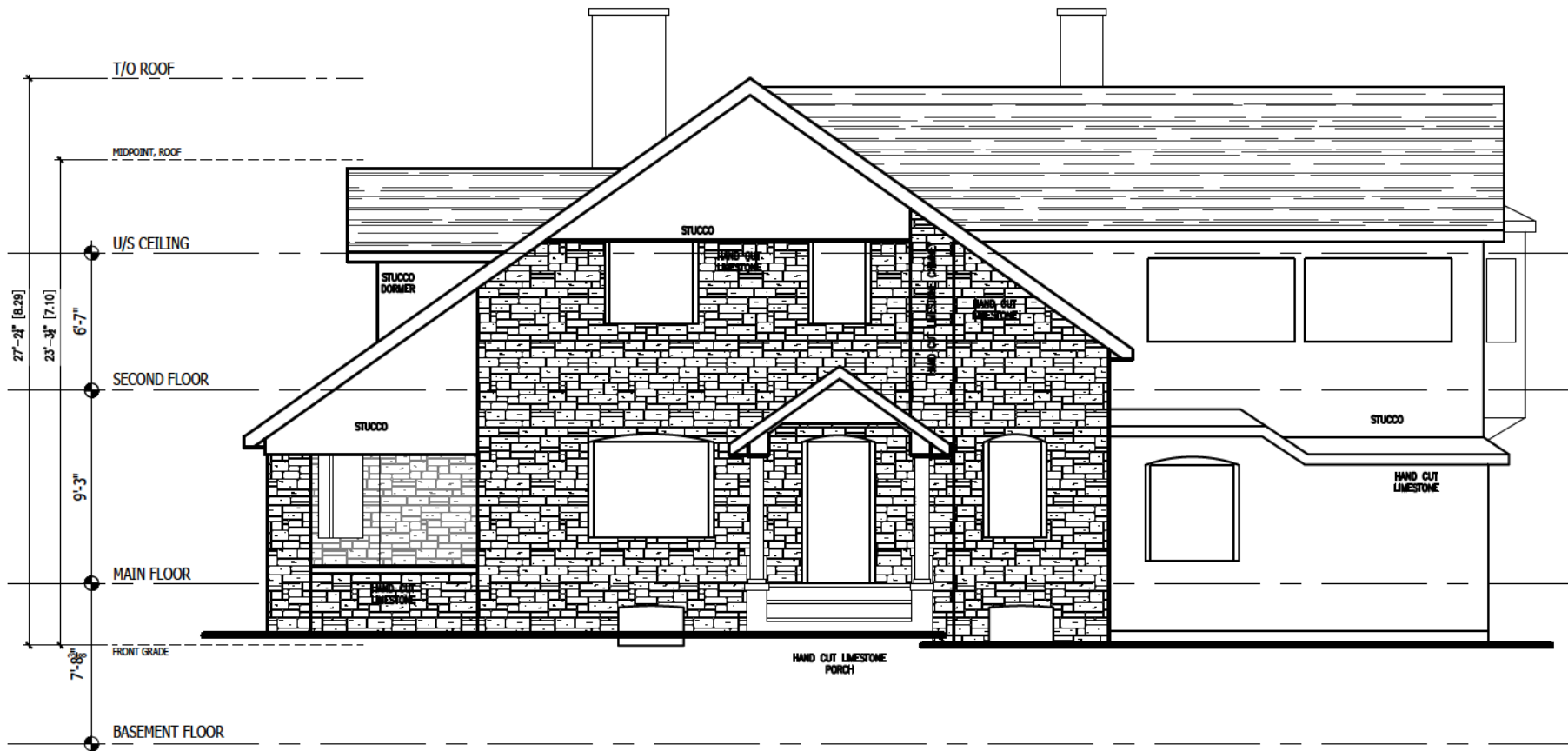
915 North Service Rd., Mississauga

JULY 18, 2016

EXISTING FRONT ELEVATION

174-15

1/8" = 1'-0"



915 North Service Rd., Mississauga

JULY 18, 2016

EXISTING RIGHT SIDE ELEVATION

174-15

1/8" = 1'-0"



915 North Service Rd., Mississauga

JULY 18, 2016

EXISTING REAR ELEVATION

174-15

1/8" = 1'-0"



915 North Service Rd., Mississauga

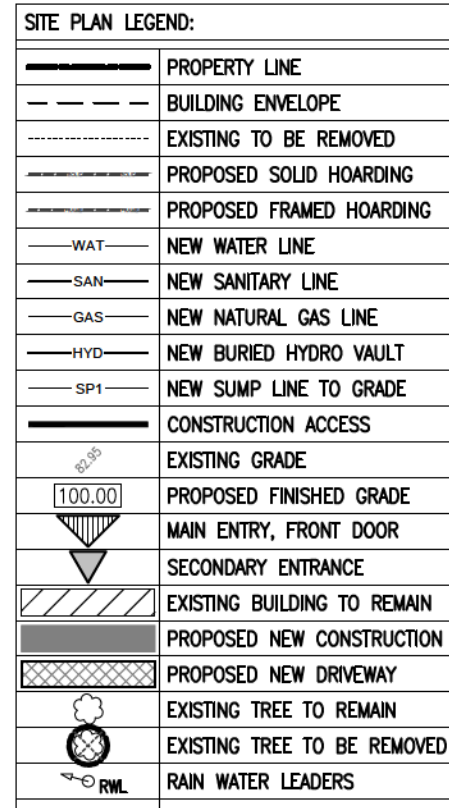
JULY 18, 2016

EXISTING LEFT SIDE ELEVATION

174-15

1/8" = 1'-0"

APPENDIX D – PROPOSED DEVELOPMENT



3 SITE STATISTICS

DRAWN: B.T.M.	
DATE: 2.16.2017	SCALE: 1 : 200
JOB NUMBER:	SHEET NUMBER:

GENERAL NOTES	
STANDARD NOTES	STRUCTURAL NOTES
DO NOT SCALE DRAWINGS. ALL DRAWINGS ARE THE PROPERTY OF THE ARCHITECT/DESIGNER AND SHALL NOT BE REPRODUCED IN WHOLE OR IN PART WITHOUT WRITTEN CONSENT FROM THE SAID ARCHITECT/DESIGNER. bM DRAFTING AND DESIGN MAKES EVERY EFFORT TO PROVIDE A COMPLETE AND ACCURATE CONSTRUCTION DRAWING PACKAGE. HOWEVER, IT IS THE RESPONSIBILITY OF THE BUILDER TO CHECK AND VERIFY ALL DIMENSIONS AND DETAILS BEFORE PROCEEDING WITH CONSTRUCTION. CONTRACTOR TO REPORT ANY DISCREPANCIES TO THE ARCHITECT/DESIGNER. FAILURE TO DO SO WILL CAUSE FORFEIT TO ANY CLAIM. BY COMMENCING CONSTRUCTION OF A BUILDING FROM THESE DRAWINGS, THE OWNER AND/OR BUILDER ACKNOWLEDGES THAT THE GENERAL NOTES HAVE BEEN READ AND UNDERSTOOD AS FOLLOWS. ALL CONSTRUCTION TO BE ACCORDING TO BEST COMMON PRACTICE AND TO CONFORM TO THE ONTARIO BUILDING CODE OR OTHER CODES HAVING JURISDICTION. GENERAL CONTRACTOR SHALL GUARANTEE ALL MATERIAL AND WORKMANSHIP FOR A PERIOD OF (1) ONE YEAR FOLLOWING SUBSTANTIAL COMPLETION. ALL OTHER MANUFACTURER'S GUARANTEES TO APPLY. GENERAL CONTRACTOR SHALL OBTAIN AND PAY FOR ALL PERMITS EXCEPT FOR THE BUILDING PERMIT, WHICH WILL BE OBTAINED BY THE OWNER. bM DRAFTING AND DESIGN SHALL NOT BE RESPONSIBLE FOR ANY CHANGES OR VARIANCES FROM THE FINAL CONSTRUCTION DRAWINGS AND SPECIFICATIONS, OR ADJUSTMENTS REQUIRED RESULTING FROM CONDITIONS ENCOUNTERED ON THE JOB SITE AND IS THE SOLE RESPONSIBILITY OF THE OWNER OR CONTRACTOR. GENERAL CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY SHORING AS REQUIRED DURING ANY CONSTRUCTION OR DEMOLITION OF STRUCTURAL WALLS, COMPONENTS OR SITE CONDITIONS. ANY AND ALL RESULTING DAMAGE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. NO SUBSTITUTIONS CAN BE MADE TO bM DRAFTING AND DESIGN'S DRAWINGS OR DOCUMENTS UNLESS APPROVED BY bM DRAFTING AND DESIGN IN WRITING AND FOLLOWING SUBSEQUENT DETAILS.	ALL SITE VISITS AND SHOP DRAWING REVIEW ARE TO BE INVOICED TO, AND PAID DIRECTLY BY THE OWNER. TRUSS SHOP DRAWING REVIEW (IF APPLICABLE) IS TO BE INCLUDED WITHIN INITIAL FEE PROPOSAL, ANY DEVIATION ON THE BUILDER'S PART TO CHANGE OR REVISE TRUSS PACKAGE WILL REQUIRE ADDITIONAL SHOP DRAWING REVIEW, AND SHALL BE INVOICED AS NOTED ABOVE. SEE S1.1 FOR ALL ADDITIONAL STRUCTURAL NOTES, INSTRUCTIONS AND DETAILS. FOOTINGS: CONTINUOUS STRIP FOOTINGS, w/ DOWELS INTO FOUNDATION WALL. INSTALL FOOTING BARRIER ATOP FOOTING BEFORE INSTALLATION OF FOUNDATION WALL FORMWORK. SEE DETAIL 1A3.1. DESIGNED SOIL BEARING CAPACITY IS MINIMUM 2,000 p.s.f. AND IS THE RESPONSIBILITY OF THE OWNER TO PROVIDE VERIFICATION OF SUCH. FOOTING DIMENSIONS MAY NEED TO BE REVISED IF BEARING CAPACITY IS NOT AS ASSUMED. PERMETUR WEEPING TILE DRAINAGE TO BE INSTALLED AS PER DETAIL 1A3.2. FOUNDATION WALL: POURED CONCRETE FOUNDATION WALL, 9" 0" TALL @ 10" THICK, HORIZONTAL REINFORCING AS PER DETAIL, ON S1.1. THE FLOOR JOISTS LATERALLY SUPPORTING THE TOP OF FOUNDATION WALL MUST BE INSTALLED PRIOR TO BACKFILLING. FOUNDATION WALLS SHALL NOT BE BACKFILLED UNTIL CONCRETE HAS REACHED ITS SPECIFIED 28 DAY STRENGTH, OR UNTIL ADEQUATELY BRACED AND APPROVED BY PROJECT ENGINEER. MOISTURE PROTECTION TO BE APPLIED TO EXTERIOR FACE AS PER CONSTRUCTION ASSEMBLY. WALLS ABOVE GRADE CONSTRUCTION: 2"x6" @ 16" o.c. S.P.F. WOOD WALL CONSTRUCTION, w/ 1/2" EXTERIOR GRADE PLYWOOD SHEATHING. FOR AIRMOISTURE PROTECTION, RAIN SCREEN AND INSULATION REFER TO EXTERIOR WALL FINISH IN CONSTRUCTION ASSEMBLIES. CEILING HEIGHTS: MAIN FLOOR: 10'-0" USING 116 2/3" STANDARD STUD. SECOND FLOOR: 8'-0" USING 92 5/8" STANDARD STUD. BASEMENT FLOOR CONSTRUCTION: 4" POURED CONCRETE SLAB, w/ 6"x6"-10 GAUGE, W.W.F. STEEL MESH. PROVIDE SLOPED TOP, MINIMUM 1% TO FLOOR DRAINS OR SUMP PITS. BUILT ATOP, 1" OF 3/4" CLEAR CRUSHED STONE ON UNDISTURBED GRADE. INSTALL 15mil CROSS LINKED POLYETHYLENE VAPOUR BARRIER BETWEEN STONE AND UNDERSIDE OF POURED SLAB. 4 LINKED V.B. JOINTS TO BE LAPPED NOT LESS THAN 12" AND SEALED w/ 3M FLASHING TAPE. WRAP 4 LINKED V.B. JOINTS MINIMUM 9" AND SEAL WITH 'BLUESKIN' PRIMER TO FOUNDATION WALL USING 6" WIDE STRIP. WOOD FLOOR CONSTRUCTION: ENGINEERED WOOD JOISTS AS NOTED ON STRUCTURAL DRAWINGS. 3/2" TONGUE AND GROOVE PLYWOOD, GLUED AND SCREWED TO JOISTS, 2"x2" CROSS BRIDGING AT MAXIMUM 7'-0" o.c. SOUNDPROOFING: INSTALL RESILIENT METAL CHANNELS @ 16" o.c. TO UNDERSIDE, AND RUNNING PERPENDICULAR TO FLOOR JOISTS. INSTALL SOUND-PROOFING PADDING TAPE ALONG CHANNEL FACE TO CONTACT DRYWALL. FOR ADDED SOUND PROOFING INSTALL 1/2" RIGID FOAM INSULATION BETWEEN STRIPS OF RESILIENT CHANNEL. 1/2" GYPSUM CEILING BOARD FINISH. NOTE: UNDER KITCHEN ISLANDS, FLOOR JOISTS ARE TO BE AT MAXIMUM 12" o.c. GARAGE FLOOR CONSTRUCTION: 4" CONCRETE SLAB, (MINIMUM DEPTH), 35 MPa CONCRETE, w/ 8% AIR ENTRAINMENT, 6"x6"x24 WIRE MESH SHEETS, SET IN MIDDLE OF SLAB. 10mil CROSS LINKED VAPOUR BARRIER, ATOP 6" CLEAR CRUSHED STONE. HIGH SIDE OF GARAGE FLOOR SLAB TO BE SET 4" BELOW TOP OF FOUNDATION WALL AND SLOPED DOWN MINIMUM 1% TOWARDS OVERHEAD DOOR. SAW CUT SLAB SURFACE 2 WAYS. FILL CUTS w/ POLYURETHANE CAULKING TO CONTROL CRACKING. CUTS SHOULD NOT BE MORE THAN 1/2 DEPTH OF CONCRETE SLAB. PORCH CONSTRUCTION: PORCH FLOOR AND SUPPORTING CONSTRUCTION AS WELL AS WATERPROOFING AS NOTED IN SPECIFICATION AND FURTHER DETAILED ON A3.2 AND S1.1 DETAILS. ROOF CONSTRUCTION: AS PER ONTARIO BUILDING CODE 2012, DIVISION B PART 9, SENTENCE 9.8.9.1.1 (a) THE DESIGN LOAD FOR STAIRS AND RAMPS SERVING A SINGLE DWELLING UNIT IS 1.9kPa (40 psf). STAIR CONSTRUCTION: AS PER ONTARIO BUILDING CODE 2012, DIVISION B PART 9, SENTENCE 9.8.9.1.1 (a) THE DESIGN LOAD FOR STAIRS AND RAMPS SERVING A SINGLE DWELLING UNIT IS 1.9kPa (40 psf).
INSTALLATION / CONSTRUCTION NOTES	
INTERCONNECTED SMOKE DETECTORS AND CARBON MONOXIDE ALARMS ARE TO BE INSTALLED ON EACH FLOOR LEVEL AND IN EACH BEDROOM AS PER ONTARIO BUILDING CODE REQUIREMENTS. 220 VOLT WIRING AND GAS LINES ARE TO BE ROUGHED IN FOR ALL STOVES AND DRYERS PROPOSED. INSTALLATION OF ELECTRICAL ITEMS MUST COMPLY WITH LOCAL ELECTRICAL CODES AND REGULATIONS AS WELL AS WITH LOCAL ELECTRIC POWER SUPPLIER'S REGULATIONS IN ALL RESPECTS. DOOR CHIME TO BE INSTALLED, COORDINATE WITH OWNER. ALARM SYSTEM IS TO BE COORDINATED WITH OWNER'S REQUEST AND SPECIFICATIONS. COMPLETE HOME AUTOMATION OR STRUCTURED CABLING IS TO BE COORDINATED WITH OWNER'S REQUEST AND SPECIFICATIONS. WINDOW AND EXTERIOR DOOR HEIGHTS ARE BASED ON STANDARD 'KOLBE' UNITS. SHOULD BUILDER OR OWNER CHANGE MANUFACTURER THEN ALL EXTERIOR WINDOW HEAD HEIGHTS ARE TO LINE UP WITH TYPICAL EXTERIOR DOOR HEIGHTS LOCATED ON THE SAME FLOOR. WHERE NO EXTERIOR DOORS ARE PRESENT, PLACE THEM AS DIMENSIONED ON ELEVATIONS WITHIN THIS DRAWING SET. WINDOW AND EXTERIOR DOOR SHOP DRAWINGS SHALL BE SUBMITTED TO ARCHITECT/DESIGNER FOR REVIEW BEFORE ORDERING. COAT AND CLOTHES CLOSETS SHALL HAVE (1) ONE HANGING ROD AND (1) ONE SHELF ABOVE. LINEN CLOSETS SHALL HAVE (5) FIVE ADJUSTABLE SHELVES INSTALLED. BROOM CLOSETS SHALL BE COMPLETE WITH (1) ONE HIGH SHELF.	
H.V.A.C. NOTES	
ALL SITE VISITS AND SHOP DRAWING REVIEW AND DETAIL(S) PRODUCTION ARE TO BE INVOICED TO, AND PAID DIRECTLY BY THE OWNER. HEATING, VENTILATION AND AIR CONDITIONING CALCULATIONS AND LAYOUTS ARE PROVIDED SEPARATELY FROM THIS SET OF DRAWINGS. INFORMATION BELOW, IS JUST FOR QUICK REFERENCE. GAS CONNECTION WILL REQUIRE SEPARATE PERMIT AND INSPECTION, AND SHALL BE THE RESPONSIBILITY OF THE BUILDER TO OBTAIN. NOTE: EACH FURNACE IS TO HAVE ITS OWN DEDICATED H.V. UNIT AND AIR CONDITIONER. PROJECT SPECIFIC COMPLIANCE PACKAGE IS LOCATED ON THE RIGHT SIDE OF THIS SHEET, REFER TO IT FOR ALL REQUIRED INSULATION VALUES AND EFFICIENCY REQUIREMENTS. REFER TO CONSTRUCTION ASSEMBLIES FOR INSULATION TYPES. FUEL BURNING APPLIANCES, INCLUDING FURNACES, FIREPLACES AND STOVES ARE TO BE PROVIDED WITH COMBUSTION AIR SUPPLY FROM EXTERIOR. DRYER(S) ARE TO BE VENTED DIRECTLY TO THE EXTERIOR OF THE HOUSE. ELECTRONICALLY PROGRAMMABLE THERMOSTATS ARE TO BE INSTALLED. ALL BATHROOMS ARE TO HAVE EXHAUST FANS INSTALLED DIRECTLY TO EXTERIOR. ALL COOKTOPS/RANGES ARE TO HAVE RANGE HOOD INSTALLED, MAXIMUM C.F.M NOT TO EXCEED 1,000 C.F.M. FAN UNIT TO BE INSTALLED ON EXTERIOR WALL.	
FURNACE QUANTITY:	1 FORCED AIR FURNACE
FURNACE LOCATIONS:	BASEMENT MECHANICAL ROOM
FURNACE FUEL SOURCE:	NATURAL GAS
FURNACE ZONES:	1-ALL BASEMENT, 1-ALL MAIN FLOOR, 1-ALL SECOND FLOOR
FIREPLACES:	SEE ARCHITECTURAL FLOOR PLANS FOR FUEL BURNING TYPE
IN-FLOOR HEATING:	BASEMENT FLOOR: NO
	MAIN FLOOR: NO
	SECOND FLOOR: NO

PROJECT SPECIFICATIONS, CONSTRUCTION ASSEMBLIES	
PROJECT SPECIFICATIONS ARE TO BE READ IN CONJUNCTION WITH DETAILS OUTLINED ON A3.2 AND S1.1.	
DAMP-PROOFING / WATERPROOFING	INTERIOR WALLS AND CEILINGS
<div><div>1001</div><div>FOUNDATION MOISTURE PROTECTION: DIRECTLY ATOP EXTERIOR FACE OF CONCRETE FOUNDATION WALL APPLY, 15-20mil DAMP-PROOF MATERIAL. AT TOP, EXTEND DAMPROOFING ABOVE GRADE TO POINT 4" BELOW TOP OF FOUNDATION WALL. AT BOTTOM, COVER FOOTING CORE & EXTEND DOWN VERTICAL FACE OF FOOTING A MINIMUM OF 4". ATOP DAMP-PROOF MATERIAL, INSTALL, 8mm GEO-COMPOSITE MEMBRANE SYSTEM, INSTALL AS PER MANUFACTURER'S RECOMMENDATIONS. AT TOP, STOP 1" BELOW PROPOSED FINISHED GRADE. AT BOTTOM, EXTEND BEYOND FOOTING FACE BY 1". AT TOP, WHERE FOUNDATION IS EXPOSED ABOVE GRADE APPLY SPRAY ON WATERPROOFING, "SIKA 740W" SPRAY APPLIED 12" STRIP FROM TOP OF FOUNDATION WALLS DOWN TO MEET GRADE.</div></div>	<div><div>1001</div><div>TYPICAL INTERIOR WALL NOTES: 1. UNLESS NOTED OTHERWISE ALL INTERIOR WALLS ARE TO BE CONSTRUCTED USING S.P.F. #12 (THICKNESS AS NOTED ON PLANS) @ 16" o.c., FINISH EACH SIDE OF WALL w/ 1/2" GYPSUM BOARD. 2. INSTALL 12" STRIP OF 6mil POLY VAPOUR BARRIER CENTRED ALONG DOUBLE TOP PLATE OF WALLS WHERE THEY INTERFACE ATTIC OR EXPOSED AREAS TO ALLOW OVERLAPPED AND SEALED JOINTS WITH MAIN VAPOUR BARRIER INSTALLATION. 3. ALL GAPS IN LUMBER FRAMING ARE TO BE ACOUSTICALLY SEALED ON INSIDE FACE PRIOR TO VAPOUR BARRIER AND DRYWALL INSTALLATION. 4. ATTIC FLOOR HEIGHT OR KNEE WALLS ARE TO BE CONSTRUCTED AS PARTITION WALLS USING 2"x6" @ 16" o.c. (Ø 12" o.c. FOR BEARING) AND INSULATED WITH MIN. R24 BATT INSULATION, ON INSIDE FACE OF STUD WALL, INSTALL 6mil POLY VAPOUR BARRIER (6" LAP, TAPE & SEAL ALL JOINTS), w/ 1/2" GYPSUM BOARD FINISH. 5. WASHROOM WALLS TO HAVE WATER RESISTANT GYPSUM BOARD THROUGHOUT ENTIRE. 6. SHOWER/SAUNA ENCLOSURES TO BE CLAD WITH "DURLOCK" CEMENT BOARD, JOINTS TO BE FILLED WITH LATEX FORTIFIED MORTAR w/ "DURLOCK" TAPE (MORTAR). 6.1. SHOWER CONSTRUCTION TO INCLUDE 'SCHLUTER' BASE AND WALL SYSTEMS. 7. PROVIDE POLY MOISTURE PROTECTION BELOW ALL WALLS ATOP OF CONCRETE FLOOR SLABS. 8. PROVIDE 'ROXUL, SAFE N SOUND' SOUND ABSORBENT INSULATION IN PARTITIONS AROUND WASHROOMS, ENSUITES, MECHANICAL ROOMS, POWDER ROOMS & LAUNDRY. 9. DRYWALL FINISHING, SQUARE METAL CORNER BEADS TO BE INSTALLED AT ALL LOCATIONS, ALL JOINTS TO BE TAPED WITH MIN. 2 COATS OF MUD AND SANDED. DRYWALL TO BE LEFT READY FOR PAINT. 10. ALL INTERIOR WALL FINISHES SHALL HAVE A SURFACE FLAME SPREAD LIMIT NOT EXCEEDING 150, PER O.B.C. 9.10.17.1(1).</div></div> <div><div>1001</div><div>INTERIOR FOUNDATION WALL INSULATION AND FINISH: "TYVEK MOISTURE WRAP" TO BE ATTACHED TO REAR FACE OF 2"x4"Ø16" o.c. LIGHT STEEL FRAMED WALL, STAPLE Ø 4" o.c., INSTALL SOLID WOOD BLOCKING AS NECESSARY FOR FUTURE SOLID BEARING POINTS. "TYVEK" TO BE CONTINUOUS, PROJECT 6" FROM U/S OF BOTTOM PLATE ATOP BASEMENT FLOOR SLAB. LEAVE MIN. 8" EXTRA AT TOP TO WRAP OVER SILL PLATE. CONSTRUCT LIGHT STEEL WALL HORIZONTALLY AND TIP INTO PLACE. ONCE IN PLACE, WRAP "TYVEK" OVER TOP PLATE AND STAPLE INTO PLACE. FILL LIGHT GAUGE FRAMED WALL w/ CLOSED CELL RIGID POLYURETHANE FOAM TO CREATE CONTINUOUS INSULATION COVERAGE. STOP INSULATION 8" BEFORE TOP OF BASEMENT SLAB, INSTALL SOLID BLOCKING AS STOP. SEPARATE, ADDITIONAL VAPOUR BARRIER IS NOT REQUIRED. 1/2" GYPSUM BOARD.</div></div> <div><div>1001</div><div>INTERIOR CEILING FINISH - UNDERSIDE ROOF: 6 MIL POLY VAPOUR BARRIER, INSTALL RESILIENT METAL CHANNELS Ø16" o.c. TO U/S CEILING FRAMING, RUNNING PERPENDICULAR. INSTALL SOUND-PROOFING, PADDING TAPE ALONG CHANNEL FACE TO CONTACT DRYWALL, FOR ADDED SOUND PROOFING, INSTALL 1/2" RIGID FOAM INSULATION BETWEEN RESILIENT CHANNEL, 1/2" GYPSUM, CEILING BOARD FINISH.</div></div>
EXTERIOR WALL FINISH	
ALL STUD WALL FRAMING, SHEATHING, INSULATION AND INTERIOR FINISH HAS BEEN NOTED IN STRUCTURAL NOTES	
<div><div>1001</div><div>AIR / MOISTURE BARRIER, RAINSCREEN AND INSULATION: ON EXTERIOR FACE OF EXTERIOR SHEATHING: 'HENRY VP100' POLYMERIC NON-WOVEN AIR/MOISTURE SHEET BARRIER (SELF ADHERING), 3oz/yd² OR EQUIVALENT, 1" THICK, "LOW PERMEATE BOARD" (KPS) 'RIGID' INSULATION w/ SHIP LAP HORIZONTAL JOINTS. INSTALLATION NOTES FOR (VP): 'DOW PERIMATE BOARD' COMES w/ DRAINAGE RIDGES ON THE BACK, AND IS TO BE INSTALLED WITH DRAINAGE RIDGES FACING THE AIR/MOISTURE BARRIER. DO NOT TAPE OR SEAL HORIZONTAL SHIP LAP JOINTS. IF APPLICABLE, VERTICAL BUTT-JOINTS ARE TO BE TAPED, IF VERTICAL JOINTS ARE SHIP LAP, DO NOT TAPE THEM EITHER. 'BENJAMIN OBDYKE' 'SLUCKER', CLASSIC RAINSCREEN, 10mm (MORTAR NET FOR BRICK/STONE). ON INTERIOR FACE OF EXTERIOR SHEATHING: INSTALL BATT INSULATION, MINIMUM R24 BETWEEN WOOD STUD FRAMING, 6mil POLY VAPOUR BARRIER, 6" LAP, TAPE AND SEAL ALL JOINTS. 1/2" GYPSUM BOARD FINISH.</div></div>	
<div><div>1001</div><div>STONE WALL ASSEMBLY (INCLUDE AIR/MOISTURE BARRIER ABOVE) 3/4" FULL BED STONE FACE VENEER, COLOUR AND TEXTURE TO BE APPROVED BY OWNER, 1" AIR SPACE BRICK TIES AS PER 9.20.9.5, BRICK TIES ATTACHED DIRECTLY TO STUDS BEHIND SHEATHING AND THROUGH EXTERIOR INSULATION IF APPLICABLE, (MIN. 2" PENETRATION INTO WALL STUD). 1"x3" Ø 16" o.c. HORIZONTAL STRAPPING. NAIL DIRECTLY TO STUDS BEHIND SHEATHING AND THROUGH EXTERIOR INSULATION IF APPLICABLE. AT BOTTOM, WEEP HOLES SPACED NOT MORE THAN 30" APART SHALL BE PROVIDED AT THE BOTTOM OF THE MASONRY WALL. PRE-FINISHED METAL FLASHING BENEATH WEEP HOLES IN MASONRY VENEER SHALL BE INSTALLED SO THAT IT EXTENDS FROM A POINT NOT LESS THAN 1/2" BEYOND THE OUTER FACE OF THE FOUNDATION WALL TO A POINT 6"(MIN) UP THE SHEATHING OF WOOD FRAME WALL, BRING AIR/MOISTURE BARRIER OVER VERTICAL LEG OF FLASHING AND SECURE WITH 3M FLASHING TAPE.</div></div>	
ROOF FINISH REQUIREMENTS	
TYPICAL ROOF FINISH NOTES: 1. EAVE PROTECTION: UNDERLAY ICE AND WATER SHIELD ATOP PLYWOOD SHEATHING AND BELOW ANY ADDITIONALLY REQUIRED MEMBRANES. EXTEND FROM EAVES TO A POINT MINIMUM 4"-0" BEYOND THE INSIDE FACE OF EXTERIOR WALLS. 10mil CROSS LINKED VAPOUR BARRIER, ATOP 6" CLEAR CRUSHED STONE. HIGH SIDE OF GARAGE FLOOR SLAB TO BE SET 4" BELOW TOP OF FOUNDATION WALL AND SLOPED DOWN MINIMUM 1% TOWARDS OVERHEAD DOOR. SAW CUT SLAB SURFACE 2 WAYS. FILL CUTS w/ POLYURETHANE CAULKING TO CONTROL CRACKING. CUTS SHOULD NOT BE MORE THAN 1/2 DEPTH OF CONCRETE SLAB. 2. ROOF SHEATHING: 1/2" EXTERIOR GRADE: SPRUCE PLYWOOD + METAL 1/4" CLIPS, ALL JOINTS STAGGERED, 3. ALL VALLEY FLASHING SHOULD BE CONSTRUCTED AS PER WALLS. WHERE INSTALLATION OF RIGID SHINGLES (WOOD, SLATE, METAL, ETC) THE SLOPE MUST BE 10:12 OR GREATER TO BE CONSTRUCTED AS CLOSED VALLEY.	
<div><div>1001</div><div>ROOF STRUCTURE: ENGINEERED ROOF TRUSSES, LAYOUT, SIZES AND SPACING AS PER TRUSS PACKAGE.</div></div>	<div><div>1001</div><div>ROOF STRUCTURE: 2"x12" Ø 16" o.c. ROOF JOISTS LAYOUT, SIZES AND SPACING AS PER STRUCTURAL DRAWINGS.</div></div>
<div><div>1001</div><div>ROOF FINISH (CEDAR SHINGLES): WESTERN RED CEDAR SHINGLES, # 1 GRADE 18" PERFECTION. INTERLAY 18" WIDE, 30 LB. FELT BUILDING PAPER OVER TOP PORTION OF EACH COURSE OF SHAKES. SHINGLES SHALL BE SPACED APPROXIMATELY 1/2" APART AND OFFSET AT THE JOINTS IN ADJACENT COURSES NOT LESS THAN 1/2" SO THAT THE JOINTS IN ALTERNATE COURSES ARE STAGGERED. FASTEN WITH (2) NAILS/STAPLES APPROXIMATELY 3/4" FROM THE SIDES OF THE SHINGLE AND 1/2" ABOVE THE EXPOSURE LINE.</div></div>	<div><div>1001</div><div>ROOF FINISH (PRE-FINISHED METAL): STANDING SEAM PRE-FINISHED METAL ROOFING, BY 'VIC-WEST' INSTALL 'VIC SYNTHETIC UNDER-LAYMENT, V.S.U.' OVER ENTIRE SURFACE TO BE CLAD WITH METAL ROOF. V.S.U. WILL SEAL ALL PUNCTURE HOLES FOR WATER TIGHT SHEATHING CONTINUITY, INSTALLER TO FOLLOW MANUFACTURER'S INSTALLATION INSTRUCTIONS.</div></div>
<div><div>1001</div><div>ROOF FINISH - HIGH "LOW SLOPE" ABOVE SLOPED ROOF: 2 PLY, SBS MODIFIED BITUMEN MEMBRANE ROOF SYSTEM, TORCH APPLIED TO PROTECTION BOARD, PROTECTION BOARD AND FASTENING TO SHEATHING BELOW AS PER MANUFACTURER'S SPECIFICATIONS. 3/4" EXTERIOR GRADE PLYWOOD SHEATHING, ALL JOINTS TO BE STAGGERED. 2"x LUMBER CUT FOR A MIN. 1.50, MAX. 1.4, SLOPE TO LOW SLOPE PERMITTER. ENSURE LOW END OF SLOPED FRAMING ALLOWS FOR 2x4" AIR FLOW CLEARANCE BELOW SHEATHING AND ABOVE STRUCTURAL FRAMING WHERE INSULATION IS REQUIRED BELOW. ENGINEERED FLAT ROOF JOISTS OR ENGINEERED TRUSSES, LAYOUT, SIZES AND SPACING AS PER STRUCTURAL DRAWINGS AND/OR TRUSS PACKAGE. MINIMUM ROOF FRAMING TO BE 110" DEEP. ENSURE THAT NO SHEATHING IS APPLIED TO THE FLAT TOP CHORD OF ROOF JOIST OR TRUSS. AIR MUST BE ALLOWED TO CIRCULATE FREELY TO UNDERSIDE OF LOW SLOPE ROOF SHEATHING.</div></div>	
INTERIOR FLOOR FINISH	
BASEMENT FLOOR FINISH: 3/4" ENGINEERED HARDWOOD, UNLESS SPECIFIED OTHERWISE. 1"EPAGE' SELF-LEVELING, EPOXY BASED, FLOOR LEVELER, 3/8" PLYWOOD SUB-FLOOR, 10MX AIRFLOW UNDERLAYMENT ATOP CONCRETE SLAB.	
DORMER FINISH	
CREZON PLYWOOD BACKING WITH FIBER-CEMENT TRIM WORK, ALL ROOF TO WALL FLASHING TO BE HIDDEN UNDER WOOD FINISH.	
CHIMNEY FINISH	
<div><div>1001</div><div>CHIMNEY CONSTRUCTION: CHIMNEY TO BE MASONRY BLOCK CONSTRUCTION w/ EXTERIOR FINISH AS NOTED BELOW. CHIMNEY TO BE WOOD STUD FRAMING w/ EXTERIOR FINISH AS NOTED BELOW.</div></div>	<div><div>1001</div><div>CHIMNEY FINISH: STONE VENEER, ROOF TO WALL FLASHING TO BE PREFINISHED METAL, BLACK.</div></div>
<div><div>1001</div><div>CHIMNEY CAP: PRECAST CONCRETE CHIMNEY CAP, CLAY CHIMNEY POT ATOP BY "SUPERIOR CLAY".</div></div>	

CLIENT INFORMATION				
PROJECT ADDRESS:		Ribston Road, Mississauga, ON		
OPENING DATE:		October 11, 2016		
CLIENT NAME:		Jade Estates		
CLIENT ADDRESS:		501 Danforth Avenue, Toronto, ON, M4K 1P5		
CLIENT PHONE NUMBER:		---		
CLIENT EMAIL:		---		

ENERGY COMPLIANCE PACKAGE				
COMPLIANCE PACKAGE SPECIFIED:		(NEW BUILD) 3.1.1.2.A, TYPE 'A5'		
WINDOW AREA : EXTERIOR WALL AREA		13.85%		
CEILING WITH ATTIC SPACE	R50	WINDOWS AND SLIDING GLASS DOORS, MAX. U-VALUE	1.6	
CEILING WITHOUT ATTIC SPACE	R31			
EXPOSED FLOOR	R35	SKYLIGHTS, MAX U-VALUE	2.8	
WALLS ABOVE GRADE	R19 +R5 C.I. R12 +R5 C.I.			
BASEMENT WALLS		SPACE HEATING EQUIPMENT, MINIMUM AFUE (FURNACE EFFICIENCY)	94%	
BELOW GRADE SLAB, MORE THAN 24" BELOW GRADE	N/A			
HEATED SLAB OR SLAB LESS THAN 24" BELOW GRADE	R10	H.R.V., SENSIBLE HEAT-RECOVERY EFFICIENCY (SRE)	70%	
EDGE OF BELOW GRADE SLAB, LESS THAN 24" BELOW GRADE	R10	HOT WATER HEATER, MINIMUM ENERGY FACTOR	0.80	

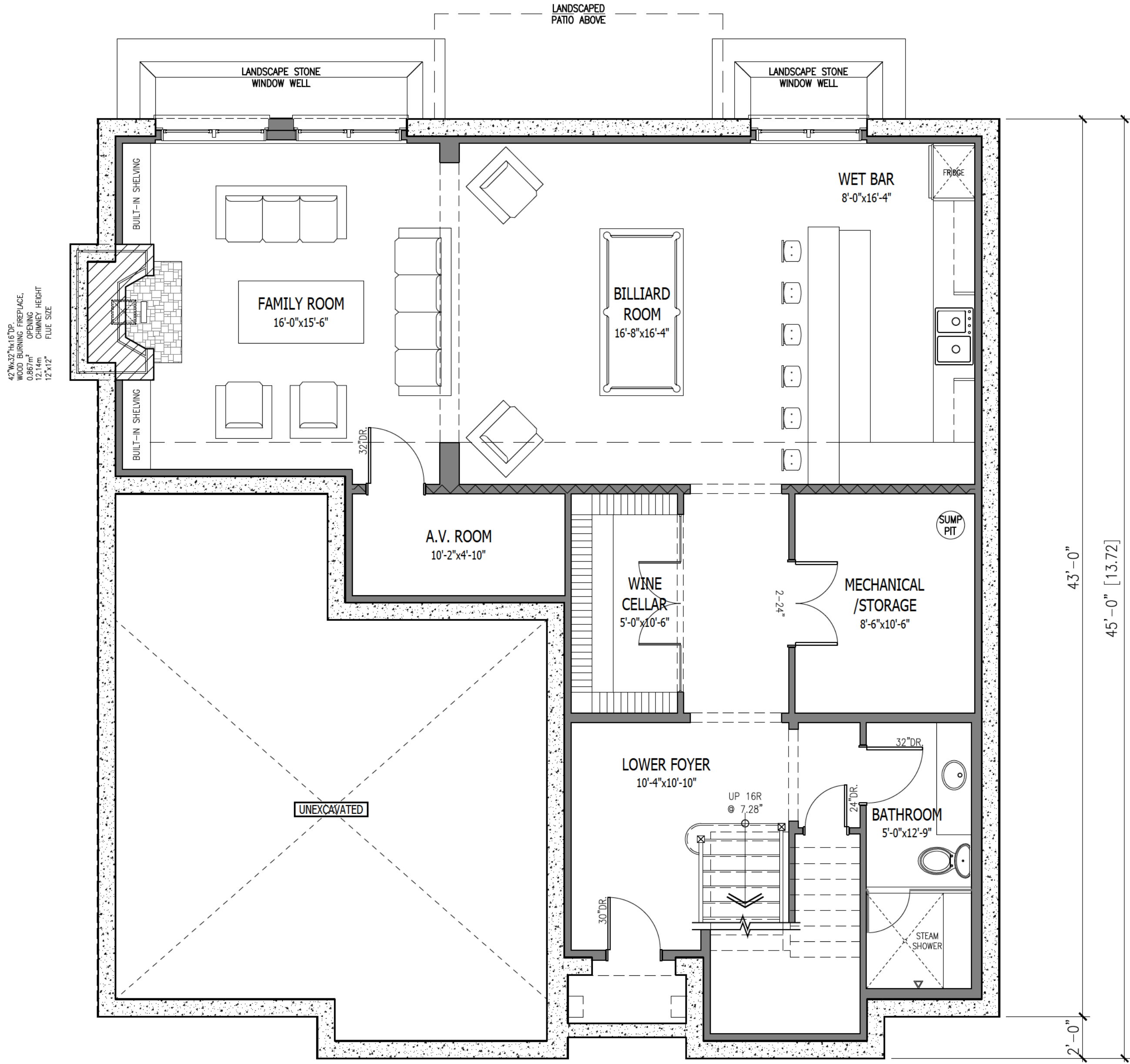
DATA MATRIX				
PROJECT NAME:		LOCATION:	LOT 2, RIBSTON ROAD,	
RIBSTON LOT 2		CITY:	MISSISSAUGA, ONTARIO	
FIRM NAME:	bTM Drafting & Design, Brian Matthews, BCIN 44237 32 Gilmour Place, Hamilton, ON, L8M 2Y2, (905) 393-8005			
	O.B.C. DATA MATRIX, Parts 3 & 9		O.B.C. Reference	
PROJECT DESCRIPTION:	<input checked="" type="checkbox"/> New <input type="checkbox"/> Addition	<input type="checkbox"/> Alteration <input type="checkbox"/> Change	<input type="checkbox"/> Part 11 (11.1-11.4) (2.1.1, 9.10.1.3)	
OCCUPANCY:	GROUP C, Residential Occupancy		9.10.2.	
BUILDING AREA:	Existing: 0.00	New: 176.98	Total: 176.98 (m²) 1.1.3.2.	
GROSS AREA:	Existing: 0.00	New: 335.84	Total: 335.84 (m²) 1.1.3.2.	
## OF STORIES:	Above Grade: 2 stories, Below Grade: 1 storey		2.1.1.3.	
CLASSIFICATION:	Facing (1) one street		9.10.4.	
CONST, Permitted:	<input checked="" type="checkbox"/> Combustible <input type="checkbox"/> Non-Combustible	<input type="checkbox"/> Non-Combustible <input type="checkbox"/> Both	9.10.6.	
CONST, Actual:	<input checked="" type="checkbox"/> Combustible <input type="checkbox"/> Non-Combustible	<input type="checkbox"/> Non-Combustible <input type="checkbox"/> Both		
OCCUPANT LOAD	(2) Two persons per sleeping room or area.		9.9.1.3.	
SPACIAL SEPARATION, CONSTRUCTION OF EXTERIOR WALLS			9.10.14.	
NO WALLS OF THIS PROJECT ARE REQUIRED TO HAVE A FIRE RESISTANCE RATING. EXTERIOR WALLS OF THIS PROJECT ARE ALLOWED TO BE OF COMBUSTIBLE CONSTRUCTION AND CLAD WITH COMBUSTIBLE MATERIAL. NON-COMBUSTIBLE CONSTRUCTION IS <u>NOT</u> REQUIRED.				
WALL DESIGNATION	AREA OF E.B.F. (m²)	LIMITING DISTANCE (m)	PERMITTED MAX. % OF OPENINGS	PROPOSED % OF OPENINGS
NORTH	83.68	1.82	8.85%	6.31% (5.28m²)
SOUTH	83.68	1.82	8.85%	6.07% (5.08m²)
EAST	84.00	8.51	56.00%	33.40% (28.05m²)
WEST	84.95	7.50	63.15%	35.36% (30.03m²)

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A2.1	ARCHITECTURAL SITE PLAN AND STATISTICS
A3.1.1	2012 ONTARIO BUILDING CODE, CONSTRUCTION NOTES
A3.1.2	2012 ONTARIO BUILDING CODE, CONSTRUCTION NOTES
A3.2	ARCHITECTURAL DETAILS AND WINDOW SCHEDULE
A4.1	BASEMENT FLOOR PLAN
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S2.3	SECOND FLOOR FRAMING PLAN
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S2.5	ROOF FRAMING PLAN

bTM Drafting & Design
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of. 905.393.8005
ce. 905.912.2337
em. btmddrafting@gmail.com

RIBSTON LOT 2

203-16
HERITAGE SET
May 29, 2017

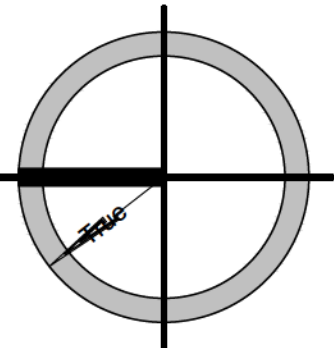


SCHEDULE 1: DESIGNER INFORMATION

I BRIAN MATTHEWS DECLARE THAT I REVIEW AND TAKE RESPONSIBILITY FOR THE DESIGN WORK AND AM QUALIFIED IN THE APPROPRIATE CATEGORY AS AN "OTHER DESIGNER" UNDER SUBSECTION 3.2.2.2. OF THE BUILDING CODE.
INDIVIDUAL BCIN: 41322
FIRM BCIN: 44237
SIGNATURE:

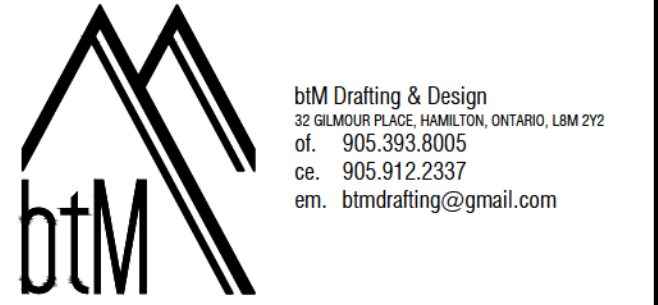
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TRUE & CONSTRUCTION NORTH



PRELIMINARY,
NOT FOR
CONSTRUCTION

1	05.29.17	ISSUED FOR HERITAGE REVIEW
NO	DATE	ISSUED/REVISED
REF.	DATE	DESCRIPTION:
REVISIONS / ISSUANCE:		

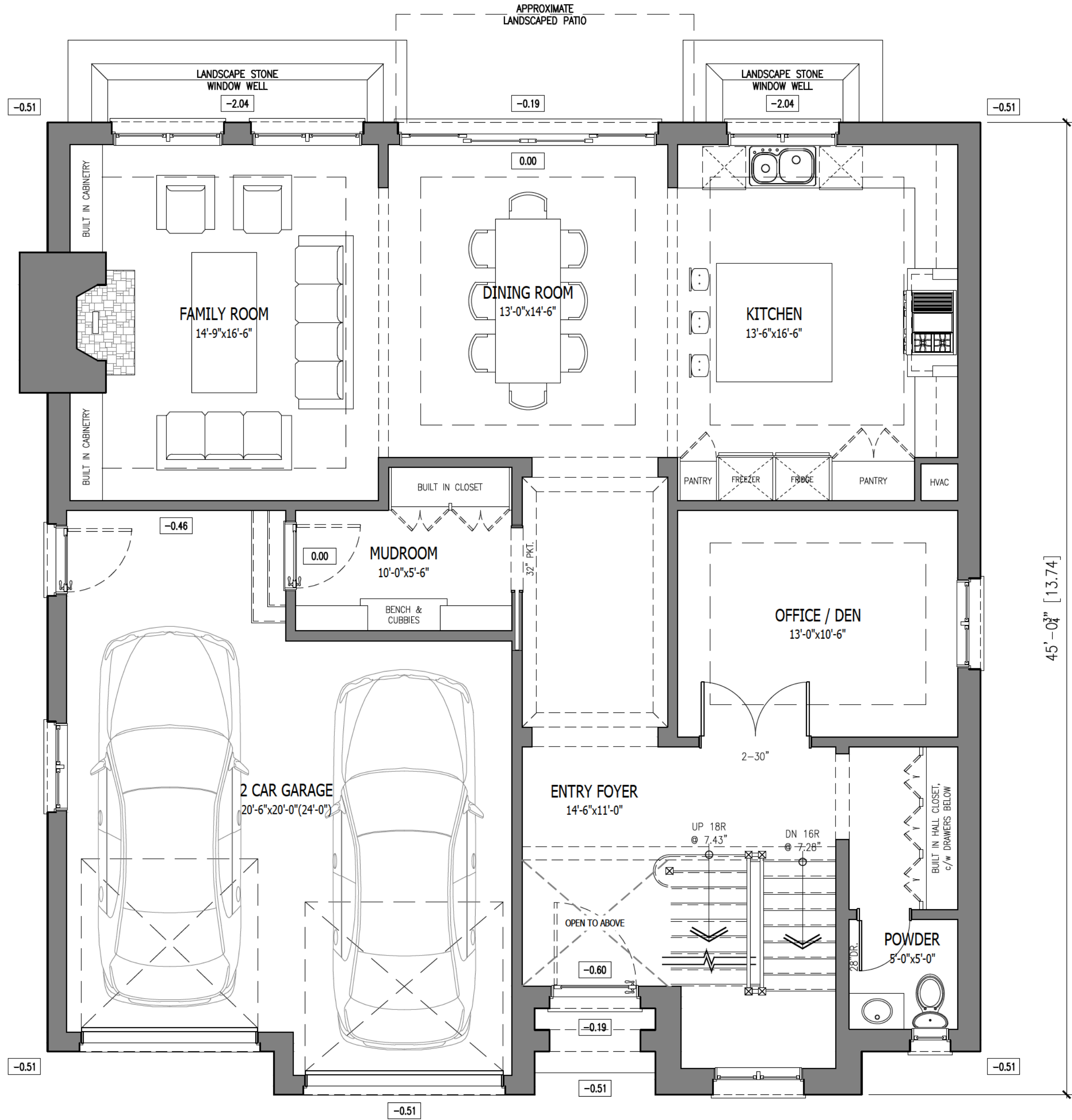


CLIENT:
RIBSTON LOT 2

ADDRESS: LOT 2, RIBSTON ROAD
CITY: MISSISSAUGA, ONTARIO

DRAWING TITLE:
BASEMENT PLAN

DRAWN: B.T.M.		
DATE: 2.16.2017	SCALE: 1/4" = 1'-0"	
JOB NUMBER: 203-16	SHEET NUMBER: A4.1	

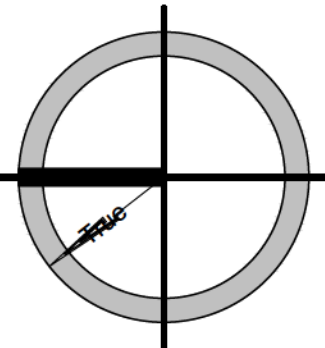


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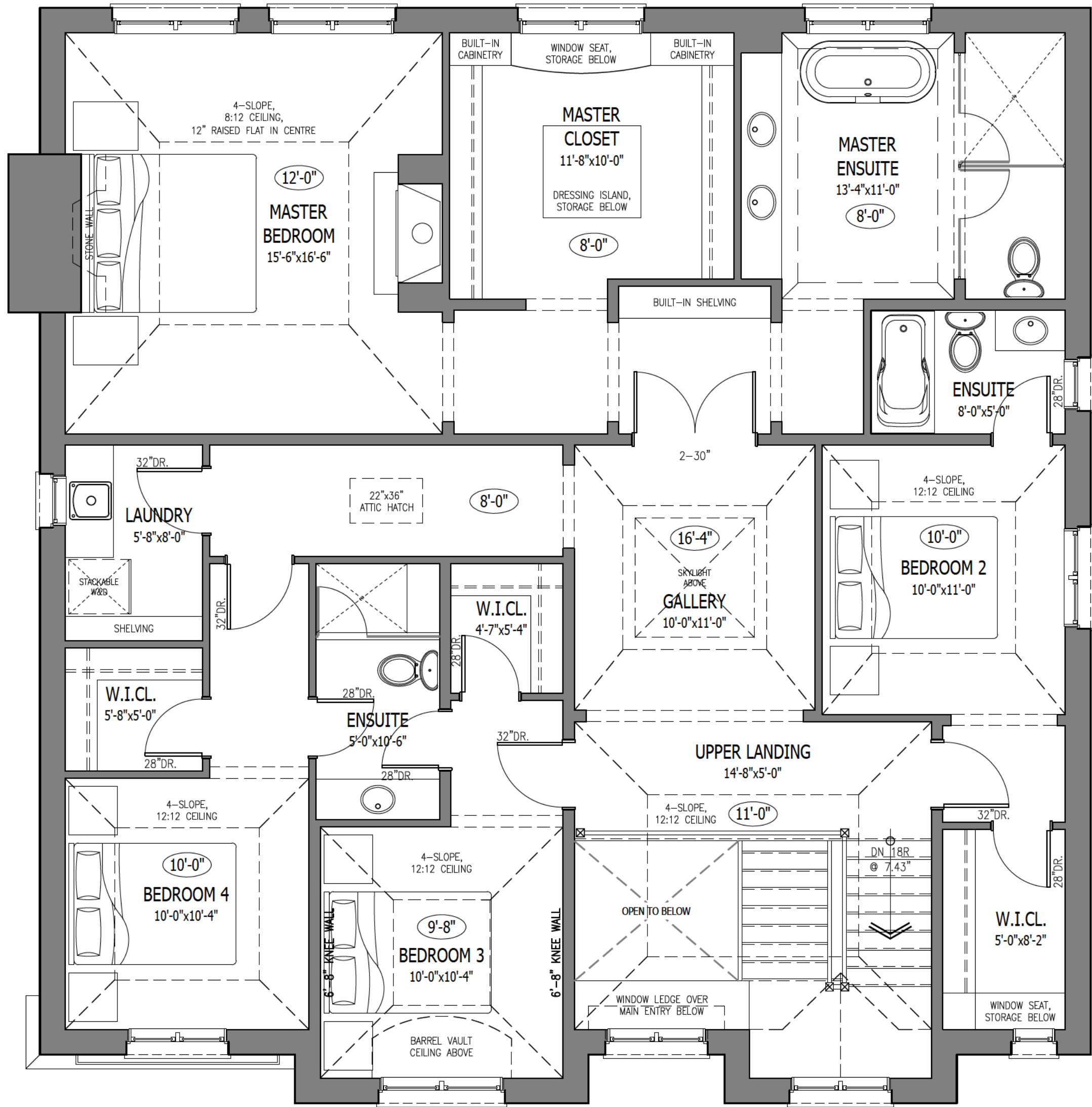
1	05.29.17	ISSUED FOR HERITAGE REVIEW
NO	DATE	ISSUED/REVISED
REF.	DATE	DESCRIPTION:
REVISIONS / ISSUANCE:		

btM Drafting & Design
32 GUNBAR PLACE, HAMILTON, ONTARIO, L8M 2Y2
tel. 905.353.9005
cell. 905.912.2337
email. btmdrafting@gmail.com

CLIENT:
RIBSTON LOT 2

ADDRESS: LOT 2, RIBSTON ROAD
CITY: MISSISSAUGA, ONTARIO
DRAWING TITLE:
MAIN FLOOR PLAN

DRAWN: B.T.M.		
DATE:	2.16.2017	SCALE: 1/4" = 1'-0"
JOB NUMBER:	203-16	SHEET NUMBER:
		A4.2

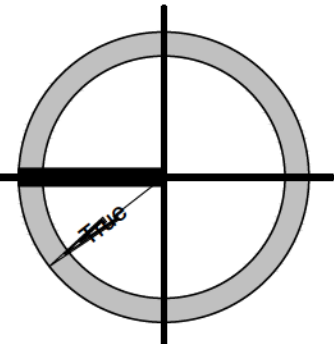


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INDIVIDUAL BCIN: 41322
FIRM BCIN: 44237
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REF.	DATE	DESCRIPTION:
REVISIONS / ISSUANCE:		

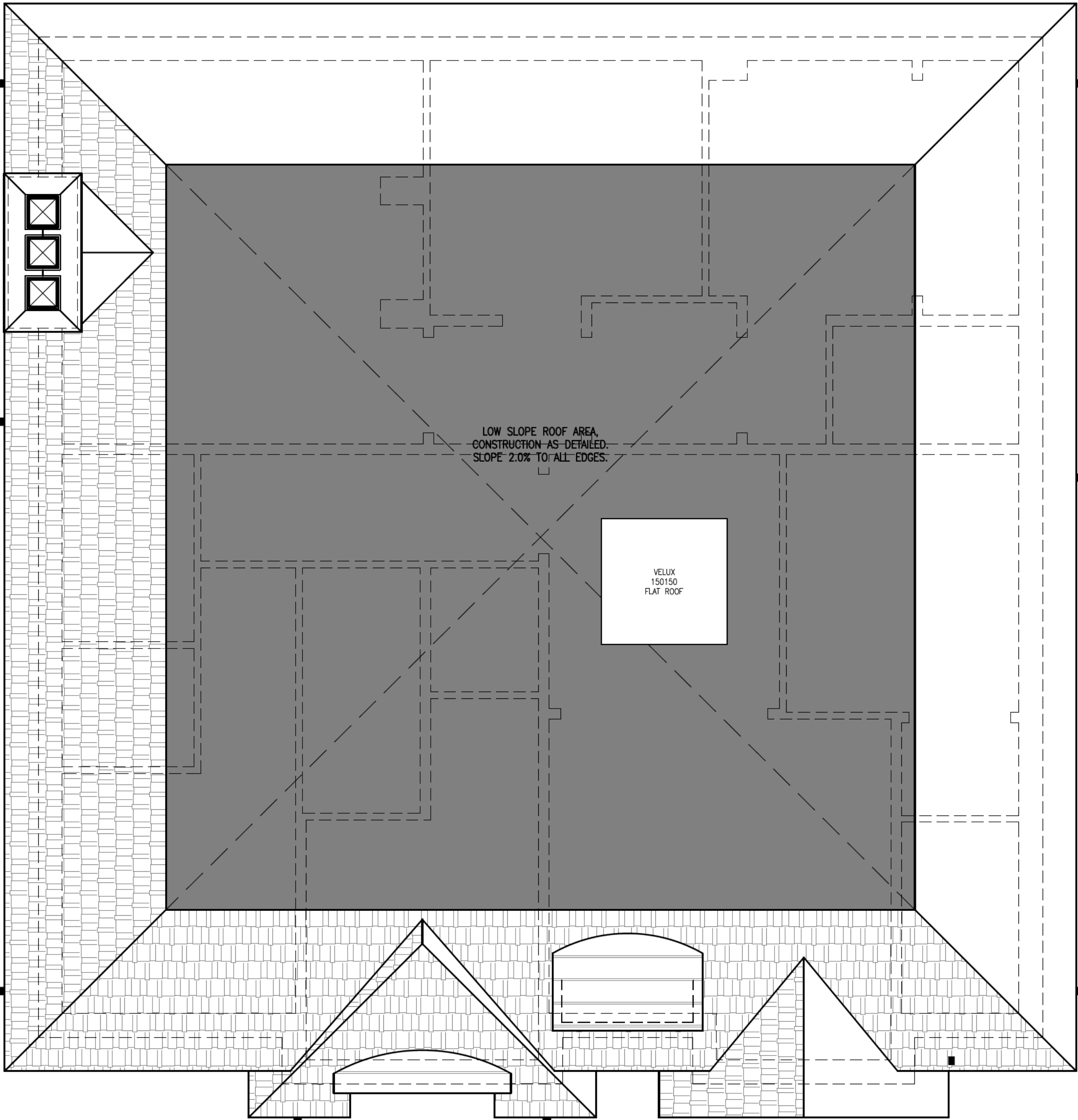


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tel. 905.353.9005
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email. btm.drafting@gmail.com


CLIENT:
RIBSTON LOT 2

ADDRESS: LOT 2, RIBSTON ROAD
CITY: MISSISSAUGA, ONTARIO
DRAWING TITLE:
SECOND FLOOR PLAN

DRAWN: B.T.M.		
DATE:	2.16.2017	SCALE: 1/4" = 1'-0"
JOB NUMBER:	203-16	SHEET NUMBER: A4.3



SCHEDULE 1: DESIGNER INFORMATION

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INDIVIDUAL BCIN: 41322
FIRM BCIN: 44237
SIGNATURE: 

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PRELIMINARY,
NOT FOR
CONSTRUCTION

STRUCTURAL DESIGN BY:



1	05.29.17	ISSUED FOR HERITAGE REVIEW
NO.	DATE	ISSUED/REVISED
REF.	DATE	DESCRIPTION:
REVISIONS / ISSUANCE:		

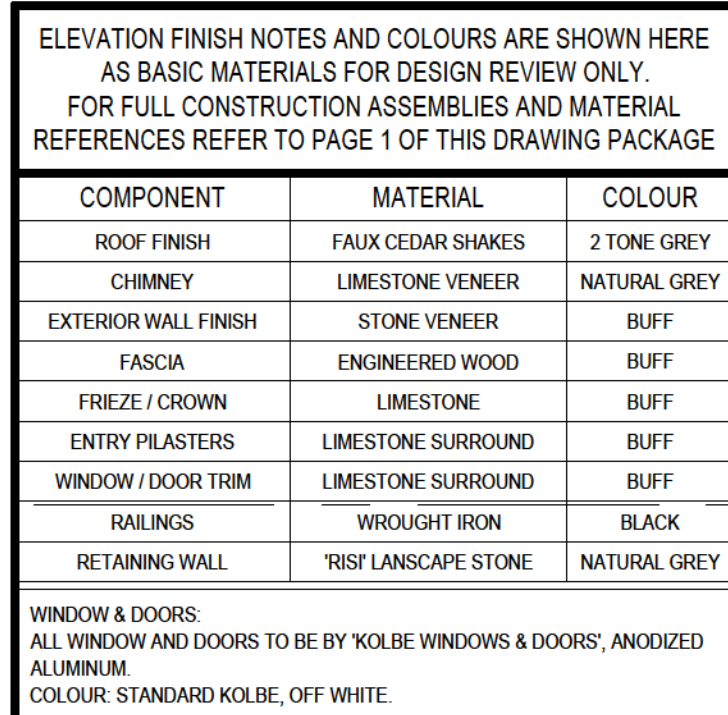


CLIENT:
RIBSTON LOT 2

ADDRESS: LOT 2, RIBSTON ROAD
CITY: MISSISSAUGA, ONTARIO

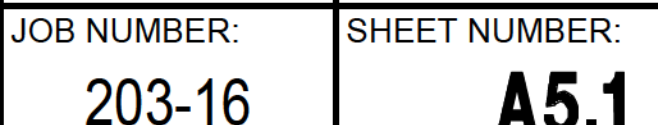
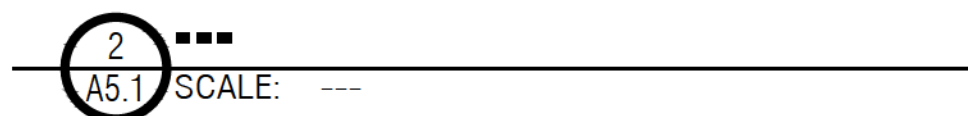
DRAWING TITLE:
ROOF FRAMING

DRAWN: B.T.M.		
DATE: 2.16.2017	SCALE: 1/4" = 1'-0"	
JOB NUMBER: 203-16	SHEET NUMBER: S2.5	

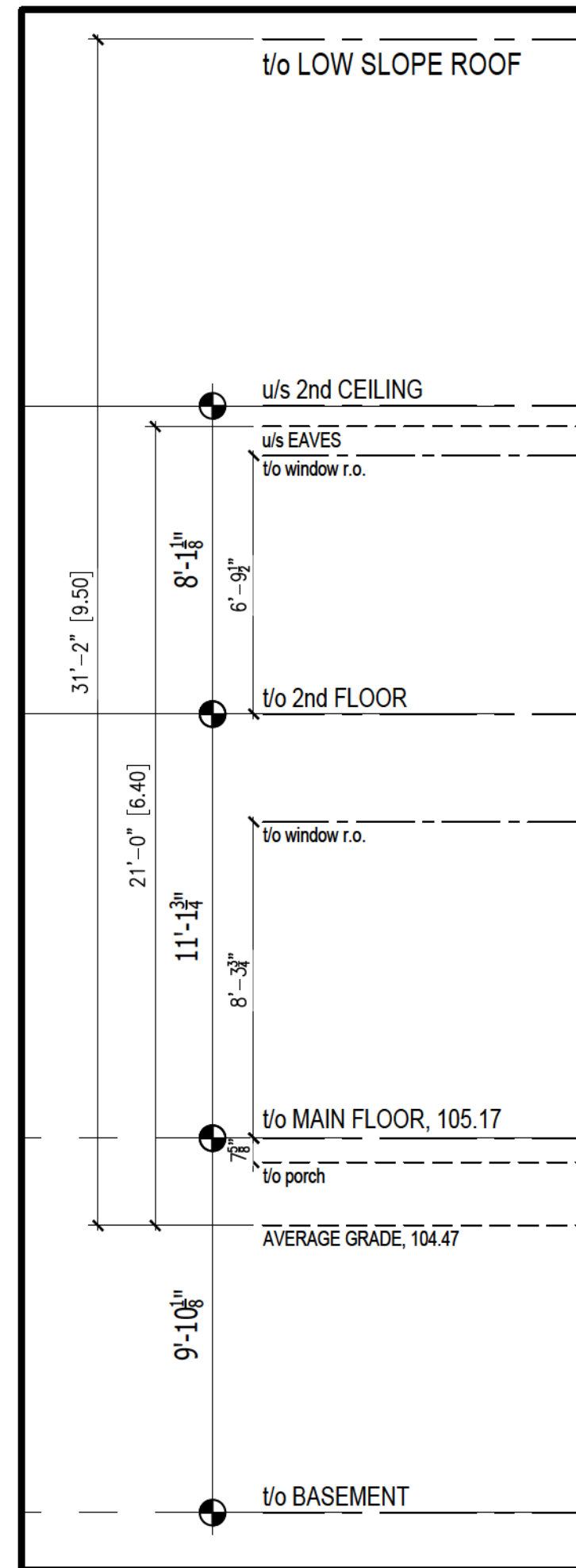


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CONSTRUCTION

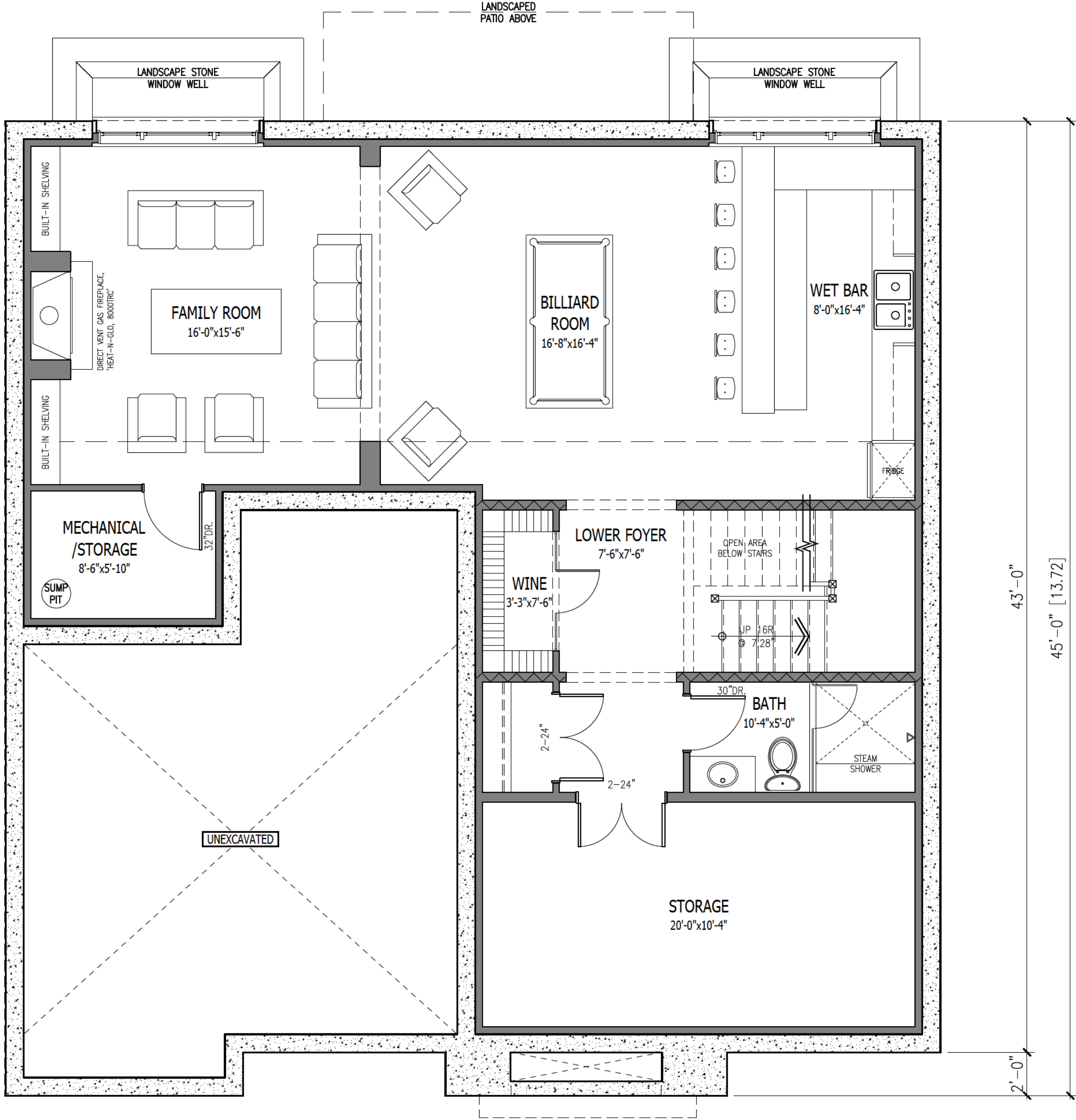


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DRAWN: B.T.M.	
DATE: 2.16.2017	SCALE: 1/4" = 1'-0"
JOB NUMBER:	SHEET NUMBER:

A5.2

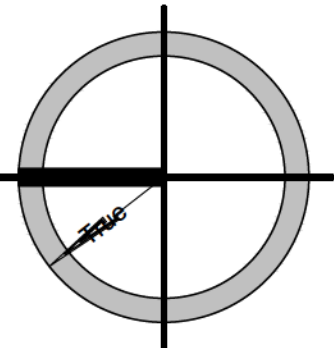


SCHEDULE 1: DESIGNER INFORMATION

I BRIAN MATTHEWS DECLARE THAT I REVIEW AND TAKE RESPONSIBILITY FOR THE DESIGN WORK AND AM QUALIFIED IN THE APPROPRIATE CATEGORY AS AN "OTHER DESIGNER" UNDER SUBSECTION 3.2.2.2. OF THE BUILDING CODE.
INDIVIDUAL BCIN: 41322
FIRM BCIN: 44237
SIGNATURE:

Drawings must NOT be scaled. Contractor must check and verify all dimensions, specifications and drawings on site and report any discrepancies to the designer prior to proceeding with any of the work.

TRUE & CONSTRUCTION NORTH



PRELIMINARY,
NOT FOR
CONSTRUCTION

1	05.29.17	ISSUED FOR HERITAGE REVIEW
NO.	DATE	ISSUED/REVISED
REF.	DATE	DESCRIPTION:
REVISIONS / ISSUANCE:		



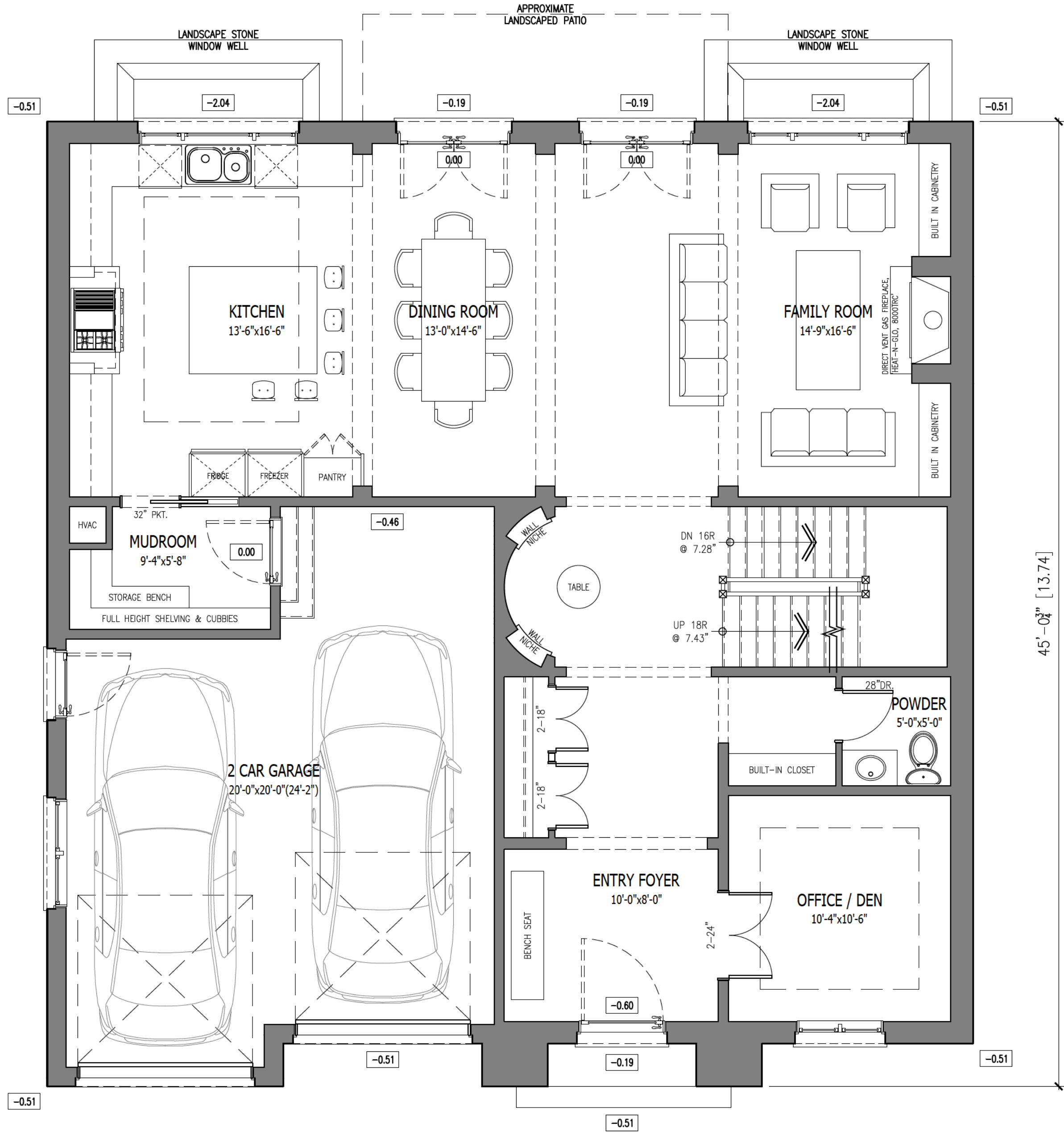
btM Drafting & Design
32 GUMBAR PLACE, HAMILTON, ONTARIO, L8M 2Y2
tel. 905.353.8005
cell. 905.912.2337
email. btm.drafting@gmail.com

CLIENT:
RIBSTON LOT 3

ADDRESS: RIBSTON ROAD,
CITY: MISSISSAUGA, ONTARIO

DRAWING TITLE:
BASEMENT FLOOR PLAN

DRAWN: B.T.M.		
DATE: 5.26.2017	SCALE: 1/4" = 1'-0"	
JOB NUMBER: 203-16	SHEET NUMBER: A4.1	

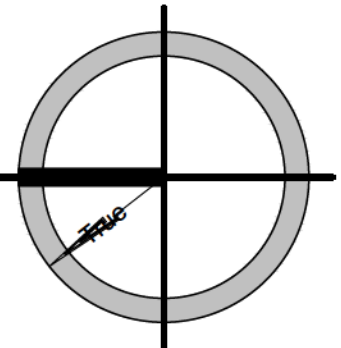


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INDIVIDUAL BCIN: 41322
FIRM BCIN: 44237
SIGNATURE:

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TRUE & CONSTRUCTION NORTH



PRELIMINARY,
NOT FOR
CONSTRUCTION

1	05.29.17	ISSUED FOR HERITAGE REVIEW
NO.	DATE	ISSUED/REVISED
REF.	DATE	DESCRIPTION:
REVISIONS / ISSUANCE:		



CLIENT:
RIBSTON LOT 3

ADDRESS: RIBSTON ROAD,
CITY: MISSISSAUGA, ONTARIO
DRAWING TITLE:
MAIN FLOOR PLAN

DRAWN: B.T.M.	
DATE: 5.26.2017	SCALE: 1/4" = 1'-0"
JOB NUMBER: 203-16	SHEET NUMBER: A4.2



Drawings must NOT be scaled. Contractor must check and verify all dimensions, specifications and drawings on site and report any discrepancies to the designer prior to proceeding with any of the work.

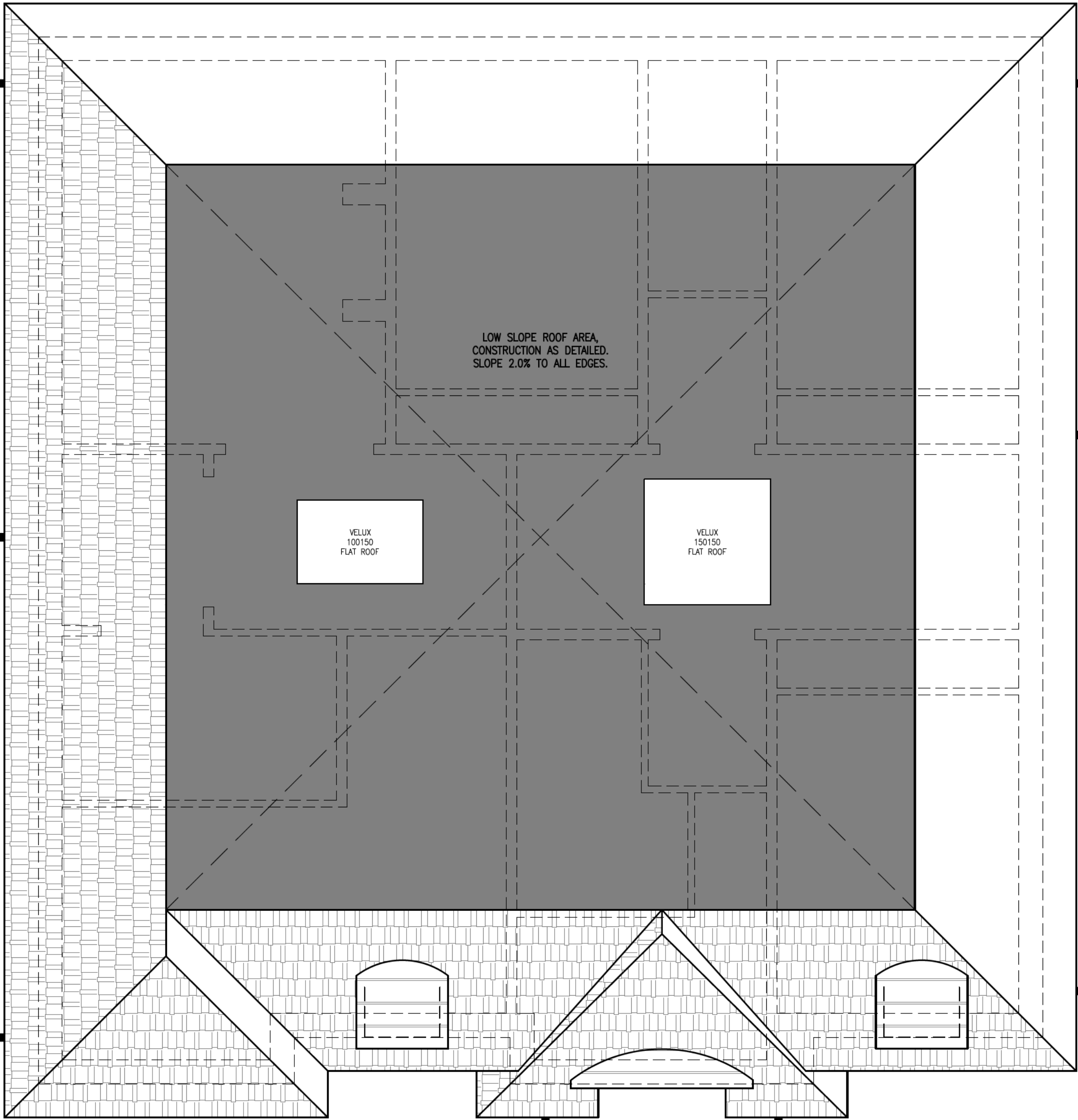
PRELIMINARY,
NOT FOR
CONSTRUCTION




CLIENT:
RIBSTON LOT 3

DRAWING TITLE:
SECOND FLOOR PLAN

A4.3



SCHEDULE 1: DESIGNER INFORMATION

I BRIAN MATTHEWS DECLARE THAT I REVIEW AND TAKE RESPONSIBILITY FOR THE DESIGN WORK AND AM QUALIFIED IN THE APPROPRIATE CATEGORY AS AN "OTHER DESIGNER" UNDER SUBSECTION 3.2.2.2. OF THE BUILDING CODE.
INDIVIDUAL BCIN: 41322
FIRM BCIN: 44237
SIGNATURE: 

Drawings must NOT be scaled. Contractor must check and verify all dimensions, specifications and drawings on site and report any discrepancies to the designer prior to proceeding with any of the work.

PRELIMINARY,
NOT FOR
CONSTRUCTION



1	05.29.17	ISSUED FOR HERITAGE REVIEW
NO.	DATE	ISSUED/REVISED
REF.	DATE	DESCRIPTION:
REVISIONS / ISSUANCE:		

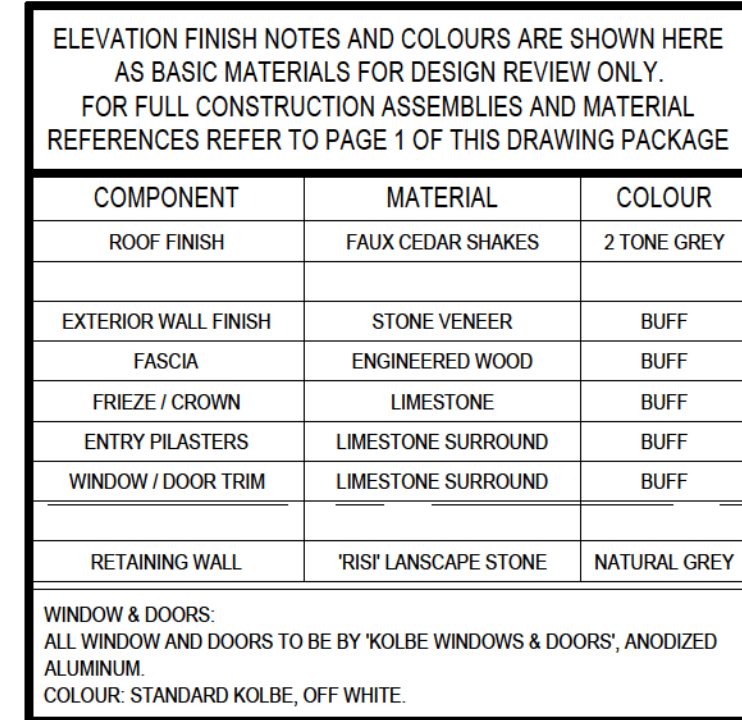


CLIENT:
RIBSTON LOT 3

ADDRESS: RIBSTON ROAD,
CITY: MISSISSAUGA, ONTARIO

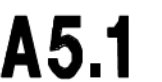
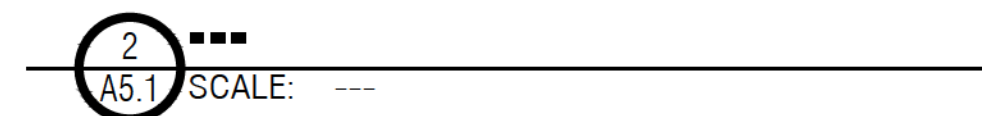
DRAWING TITLE:
ROOF FRAMING

DRAWN: B.T.M.		
DATE:	5.26.2017	SCALE: 1/4" = 1'-0"
JOB NUMBER:	203-16	SHEET NUMBER:
		S2.5

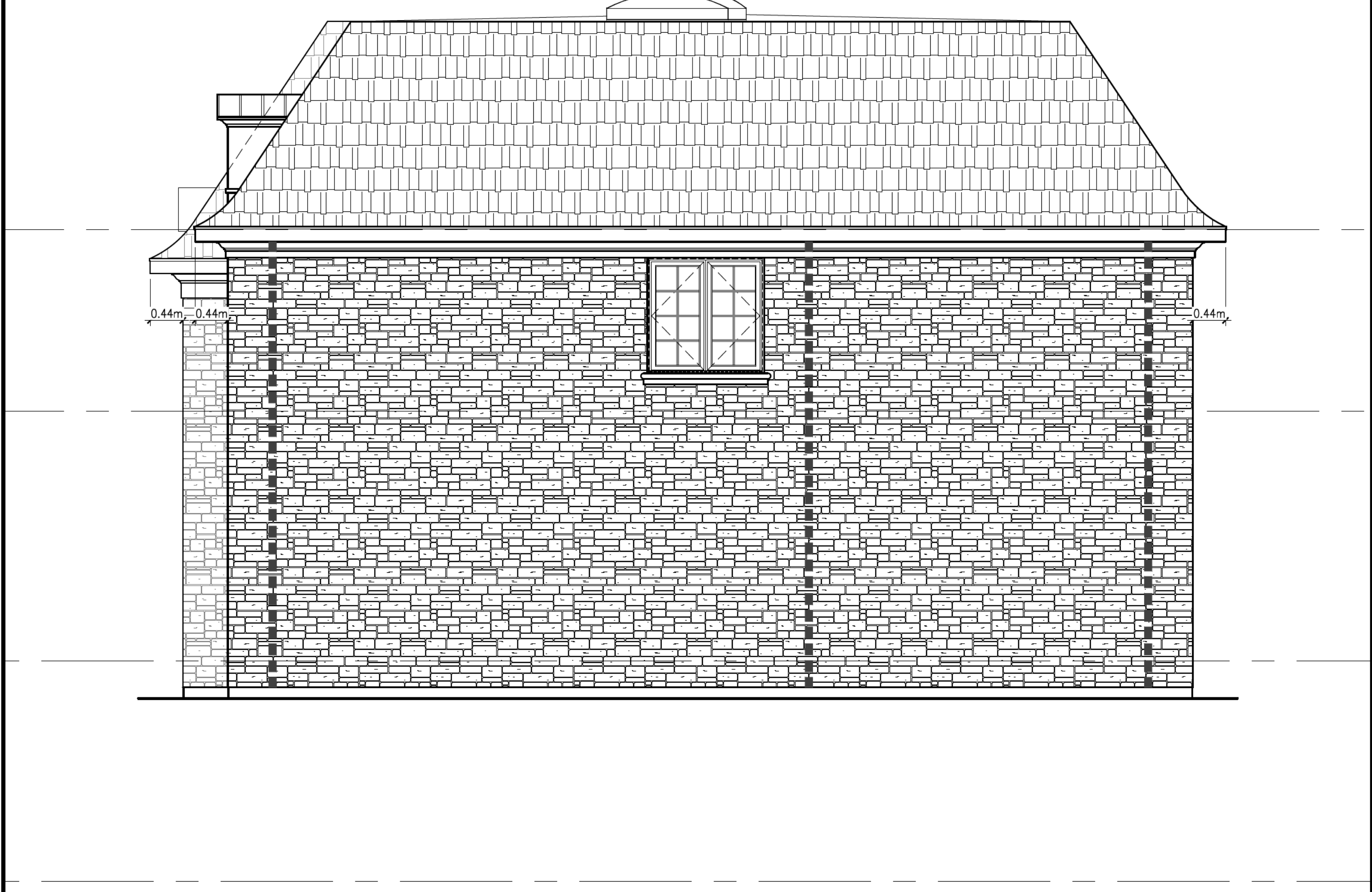
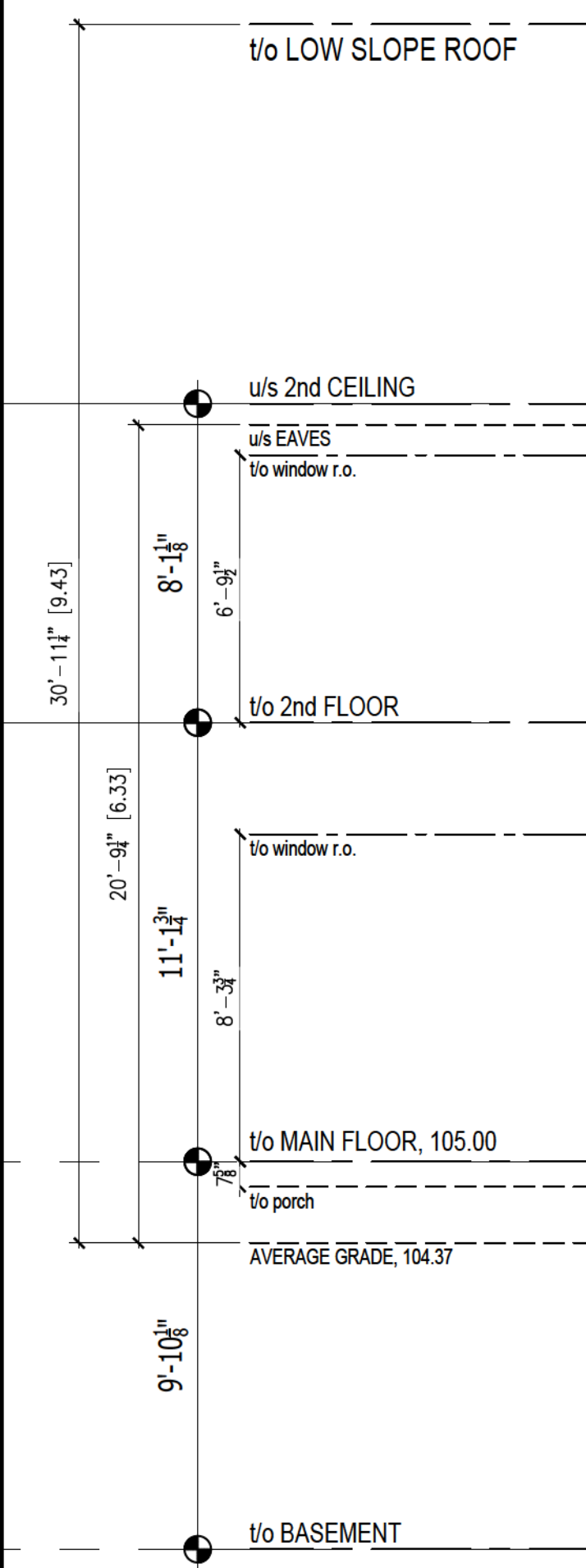


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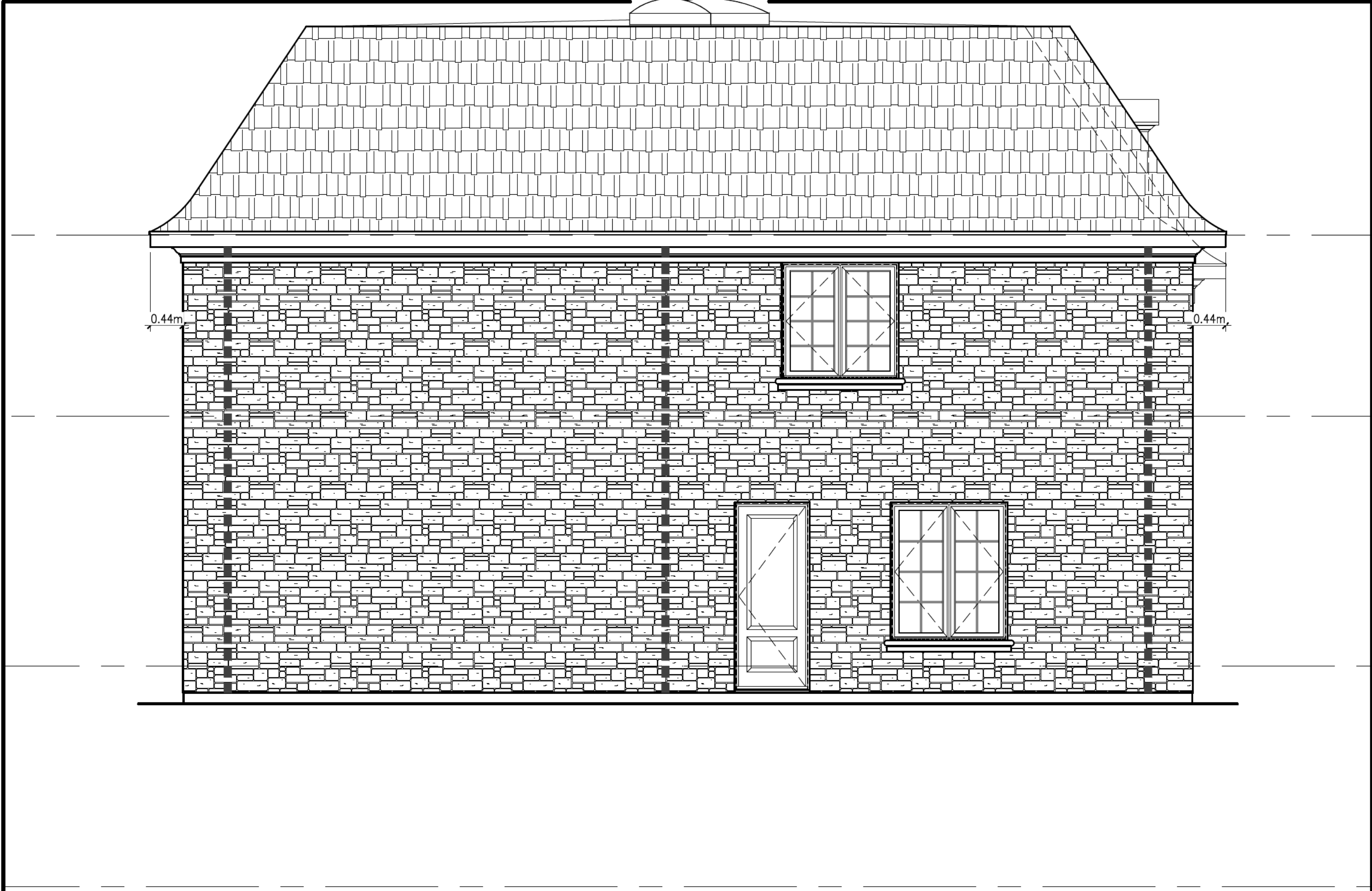
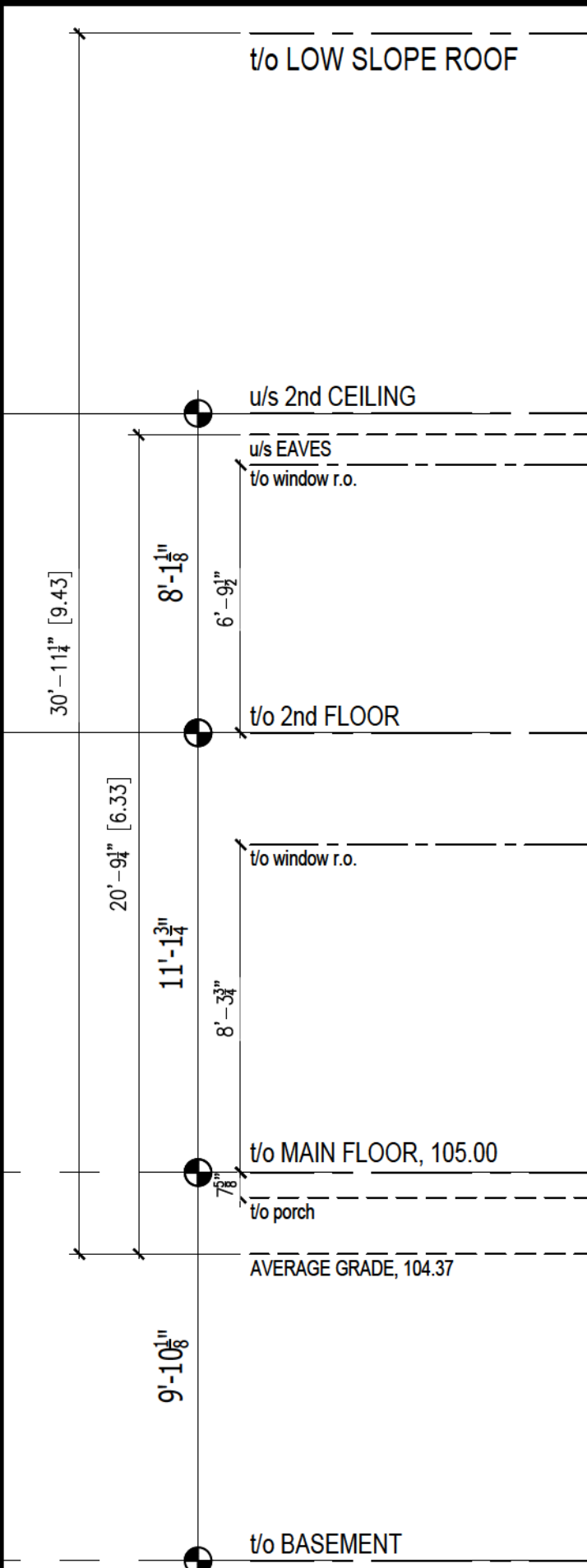
PRELIMINARY,
NOT FOR
CONSTRUCTION



ELEVATION FINISH NOTES AND COLOURS ARE SHOWN HERE AS BASIC MATERIALS FOR DESIGN REVIEW ONLY. FOR FULL CONSTRUCTION ASSEMBLIES AND MATERIAL REFERENCES REFER TO PAGE 1 OF THIS DRAWING PACKAGE		
COMPONENT	MATERIAL	COLOUR
ROOF FINISH	FAUX CEDAR SHAKES	2 TONE GREY
EXTERIOR WALL FINISH	STONE VENEER	BUFF
FASCIA	ENGINEERED WOOD	BUFF
FRIEZE / CROWN	LIMESTONE	BUFF
ENTRY PILASTERS	LIMESTONE SURROUND	BUFF
WINDOW / DOOR TRIM	LIMESTONE SURROUND	BUFF
RETAINING WALL	'RIS' LANDSCAPE STONE	NATURAL GREY
WINDOW & DOORS: ALL WINDOW AND DOORS TO BE BY 'KOLBE WINDOWS & DOORS', ANODIZED ALUMINUM. COLOUR: STANDARD KOLBE, OFF WHITE.		



2 SOUTH ELEVATION, RIGHT SIDE
SCALE: 1/4" = 1'-0"



4 NORTH ELEVATION, LEFT SIDE
SCALE: 1/4" = 1'-0"

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PRELIMINARY,
NOT FOR
CONSTRUCTION

1	05.29.17	ISSUED FOR HERITAGE REVIEW
NO	DATE	ISSUED/REVISED
REF.	DATE	DESCRIPTION:
REVISIONS / ISSUANCE:		



CLIENT:
RIBSTON LOT 3

ADDRESS: RIBSTON ROAD,
CITY: MISSISSAUGA, ONTARIO

DRAWING TITLE:
**NORTH & SOUTH
SIDE ELEVATIONS**

DRAWN: B.T.M.	SCALE: 1/4" = 1'-0"
DATE: 5.26.2017	JOB NUMBER: 203-16
JOB NUMBER: 203-16	SHEET NUMBER: A5.2

City of Mississauga
Corporate Report



Date: 2017/06/06

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
 Community Services

Originator's files:

Meeting date:
 2017/07/11

Subject

Request to Alter a Heritage Designated Property: 929 Old Derry Road West (Ward 11)

Recommendation

That the Corporate Report dated June 6, 2017, from the Commissioner of Community Services entitled *Request to Alter a Heritage Designated Property: 929 Old Derry Road West (Ward 11)*, to extend an existing paved walkway to provide a hard surface pedestrian connection to lead to the adjacent property at 7059 Second Line West as depicted in the Appendix 1 and 2, be approved.

Background

The subject property is designated under Part V of the Ontario Heritage Act as it forms part of the Meadowvale Village Heritage Conservation District (HCD). Changes to the property are subject to the Meadowvale Village Heritage Conservation District Plan, 2014 and substantive changes identified in said plan require a heritage permit. The subject proposal is for the extension of an existing paved walkway to provide a hard surface pedestrian connection to lead to the adjacent property at 7059 Second Line West.

Policy 13 of the Meadowvale Village HCD Plan notes that "changes to lot grade, excavation and the placement of fill which alter a property's landform" constitute a substantive alteration. The proposed walkway would require such changes to the property's landform. Substantive alterations will be evaluated as per Policy 14, including consideration of impact to individual property, HCD's form, scale, density and character, immediate streetscape, abutting properties, visibility from public realm, degree of change to existing lot grade and including but not limited to the following: compliance to section 4.2.1 of HCD Plan, retention, restoration and reuse of original materials and features.

The applicant has confirmed that neither a development application nor a building permit is required for the proposed walkway.

Comments

The proposed walkway will be the continuation of an existing walkway finished with unit pavers and it will use the same materials. The proposed walkway extension will not be visible from the public realm being internal to the subject property. The impact to the abutting property was anticipated by a recently approved proposal for the adjacent property at 7059 Second Line West, which included a walkway connection and a gate at the location where the walkway meets the joint property line. Both properties are under the same ownership. The walkway does not significantly alter the existing grade.

Financial Impact

There is no financial impact.

Conclusion

The proposal complies with the Heritage District Plan having no adverse impact to the Heritage District character therefore it should be approved.

Attachments

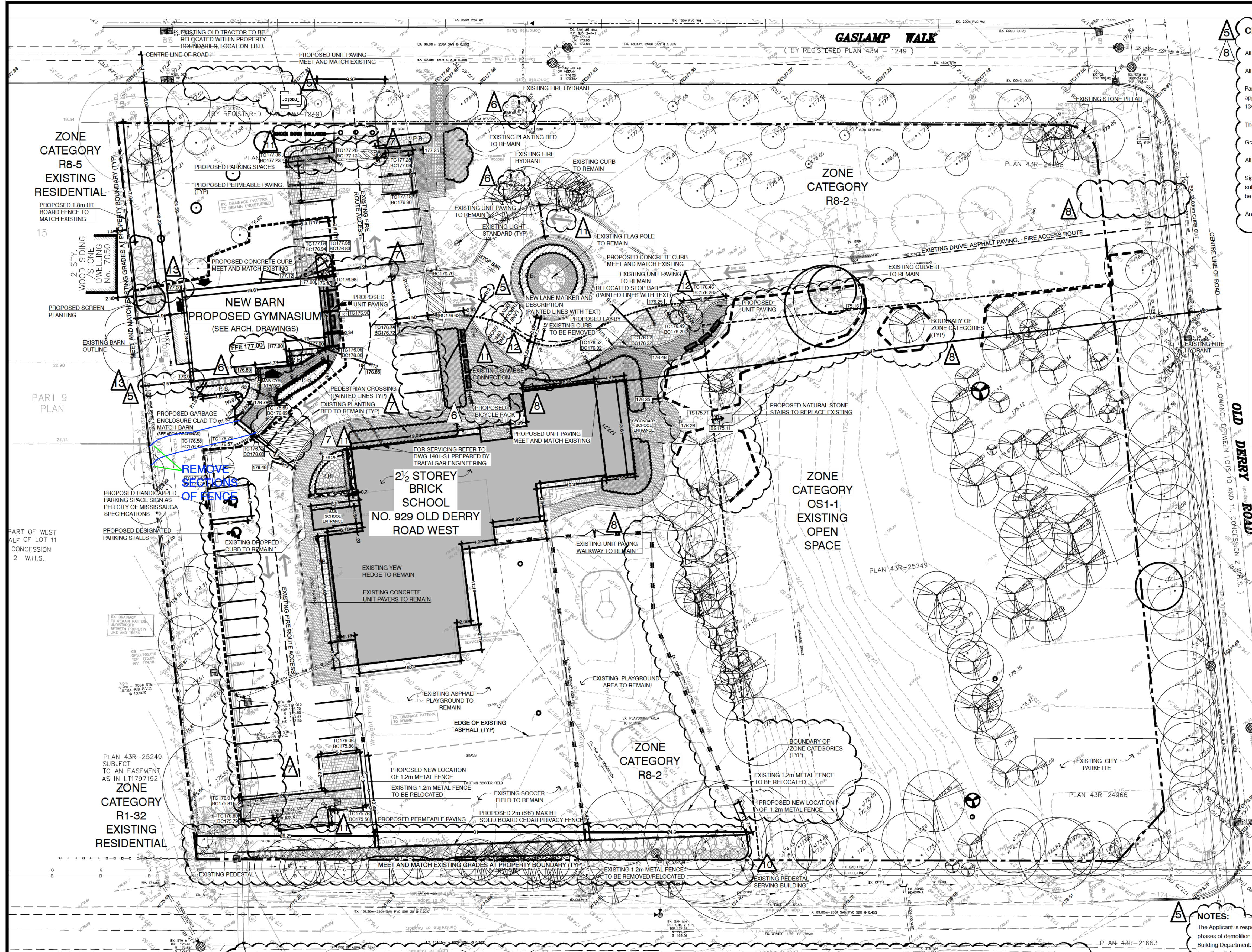
Appendix 1: Site Plan Drawing

Appendix 2: Proposal Description by Strickland Mateljan Design and Architecture



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: Cecilia Nin Hernandez, Heritage Coordinator



ZONING BY-LAW 225-2007	BY-LAW REQUIREMENT	PROVIDED ON SITE
PROPERTY ZONE (1)	R1 to R16, RM1 to RM9 and RA1 to RA5, C1 to C4, CC1 to CC4, OS1, OS2 and I zones.	EXCEPTION R8-2 and OS1-1
LOT TYPE		CORNER LOT
MINIMUM LOT AREA - CORNER LOT AS PER REGULATION 4.4.2.2	13,700 M ² (1.37 ha)	15349 M ² (1.53 ha) TOTAL LOT AREA.
MAXIMUM LOT COVERAGE (2)	20% FOR PRIVATE SCHOOLS 20% OF R8-2 zone = 2093 M ² MAX	EX. BUILDING COVERAGE = 923 M ² PRO. BUILDING COVERAGE = 240 M ² TOTAL LOT COVERAGE = 1163 M ²
MAXIMUM GROSS FLOOR AREA (3)	1500 M ²	1500 M ² ; 1260 M ² EXISTING and 240 M ² PROPOSED.
NUMBER OF PARKING SPACES (4)	1 SPACE PER 100 M ² (gross floor area) EX. BUILDING REQUIREMENTS = 15 PROP. BUILDING REQUIREMENTS = 3 18 INCLUDING 1 DESIGNATED SPACE & DAYCARE	41 INCLUDING 2 DESIGNATED SPACES
NOTES:		
(1) AS PER ZONING BY-LAW, EXCEPTION R8-2	(3) AS PER R8-2 PROVISIONS	
(2) 20% OF R8-2 ZONE AREA WHERE PROPOSED GYM IS LOCATED. OS1-1 DOES NOT APPLY	(4) AS PER PROVIDED SCHOOL PLANS	

ZONING BY-LAW 225-2007	BY-LAW REQUIREMENT	PROVIDED ON SITE
MINIMUM SETBACK OF A PARKING AREA TO A RESIDENTIAL ZONE	4.5M	4.57M
MINIMUM LOT FRONTAGE.	45M	108.35M
MINIMUM FRONT YARD.	7.5M	72.2M
MINIMUM SIDE YARD. (Both Sides)	10.8M	24.5M on east and 69M on the west.
MINIMUM REAR YARD.	7.5M	7.5M
MINIMUM BUILDING HEIGHT TO THE HIGHEST RIDGE.	9.5M	8.98M
MINIMUM OPEN SPACE.	40% of the LOT AREA in the R8-2 ZONE = 4186M ²	6936 M ²
MAXIMUM VEHICULAR PAVING.	N/A	2363 M ²
NOTES:		
MAXIMUM GROSS FLOOR AREA NOT APPLICABLE TO SPACES BELOW GRADE, SUCH AS BASEMENT.		
M ² STANDS FOR SQUARE METER		
SF STANDS FOR SQUARE FEET		
GFA STANDS FOR GROSS FLOOR AREA		

CITY OF MISSISSAUGA NOTES:

All exterior lighting will be directed onto the site and will not infringe upon the adjacent properties.

All roof top mechanical units shall be screened from view by the applicant.

Parking spaces reserved for people with disabilities must be identified by a sign, installed at the applicant's expense, in accordance with the design specifications of Figure 1 to Schedule 1 of By-law 134-83.

The applicant will be responsible for ensuring that all plans conform to Transport Canada's restrictions.

Grades will be met within a 33% maximum slope at the property lines and within the site.

All damaged areas are to be reinstated with topsoil and sod prior to the release of securities.

Signage shown on the site development plans is for information purposes only. All signs will be subject to the provisions of Sign By-law 0054-2002, as amended, and a separate sign application will be required through the Building Division.

Any fencing adjacent to municipal lands is to be located 15 cm (6.0 in.) inside the property line.

NOTES:

Fire access route will be designed to support a load of not less than 11,363 kg. per axle and have a change in gradient of not more than 1 in 12.5 over a minimum distance of 15m.

The fire route will be designated as per Bylaw 1036-81 as amended.

All surface drainage will be self contained, collected and discharged at a location to be approved prior to the issuance of a building permit.

The portions of the driveway within the municipal boulevard will be paved by the applicant.

All proposed curbing at the entrances to the site is to stop at the property line or at the municipal sidewalk.

All excess excavated material will be removed from the site.

The existing drainage pattern will be maintained except where noted.

NOTE:
EXISTING GRADES TO BE READ IN CONJUNCTION WITH PROPOSED GRADES SHOWN ON THE GRADING PLAN, SHEET L-4.

NOTE:
SEE SHEET TP-1 FOR HOARDING LOCATION AND TP-2 FOR HOARDING DETAILS.
SEE SHEET L-3 FOR CONSTRUCTION STORAGE AND TRADES PARKING.
SEE SHEET L-4 FOR PROPOSED GRADES.

NOTES:

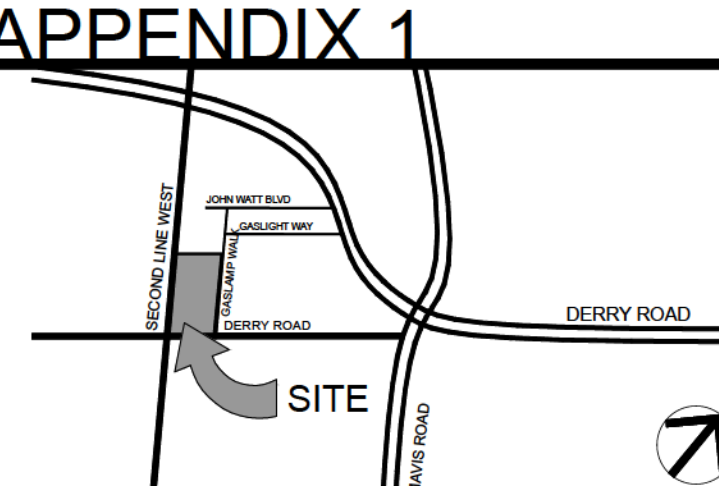
The Applicant is responsible for ensuring that tree protection hoarding is maintained throughout all phases of demolition and construction in the location and condition as approved by the Planning and Building Department. No materials (building materials, soil, etc.) may be stockpiled within the area of hoarding. Failure to maintain the hoarding as originally approved or the storage of materials within the hoarding will be cause for the Letter of Credit to be held for two (2) years following completion of all site works.

Signature of Owner/Applicant: _____

I hereby certify that this drawing conforms in all respects to the site development plans as approved by the City of Mississauga under file number SP 09/034/W11 and related files SP 00/097 and SP 03/100.

Signature of Architect/Engineer: _____

The City of Mississauga requires that all working drawings submitted to the Building Division as part of an application for the issuance of a building permit shall be certified by the architect or engineer as being in accordance with the provisions of the Building Act, R.S.O. 1990, Chapter B.12.



KEY PLAN

LEGEND:

- PROPERTY LINE
- ZONE BOUNDARY
- PAIGE WIRE TREE PROTECTION HOARDING (162 in m)
- SOLID BOARD TREE PROTECTION HOARDING (135 in m)
- EXISTING TREE TO REMAIN
- EXISTING TREE TO BE REMOVED
- EXISTING UNIT PAVING TO REMAIN
- PROPOSED UNIT PAVING
- PROPOSED PERMEABLE PAVING
- PROPOSED PLANTING BED
- WOOD FENCE ENTRY FEATURE
- PROPOSED BENCH
- EXISTING GRADES
- EXISTING COUNTOURS
- EXISTING FIRE ACCESS ROUTE
- EXISTING FIRE HYDRANT

BASE INFORMATION SUPPLIED IN PAPER FORM BY CLIENT.
DIGITAL FILES RECEIVED BY MICHAEL SPAZIANI ARCHITECT INC.
SURVEY INFORMATION RECEIVED JAN 15, 2009
FROM DAVID HORWOOD LIMITED
FILE NAME: 7067_TOPO-3.dwg

NO.	REVISIONS	DATE:
1	ISSUED FOR DISCUSSION	NOV 17, 2008
2	ISSUED FOR SPA	MAR 5, 2009
3	BUILDING RELOCATED AS PER COMMENTS	MAY 9, 2009
4	LAWN RE-GRADING REMOVED	MAY 11, 2009
5	REVISED AS PER CITY COMMENTS	JUNE 9, 2009
6	REVISED AS PER CITY COMMENTS	JULY 23, 2009
7	REVISED PAVING/PARKING AS PER COMMENTS	AUG 19, 2009
8	REVISED AS PER CITY COMMENTS	SEPT 18, 2009
9	REVISED FENCE/BUFFER AS PER COMMENTS	OCT 16, 2009
10	REVISED WOOD FENCE AS PER COMMENTS	OCT 28, 2009
11	REVISED AS PER TRAFFIC SAFETY COMMENTS	MAR 9, 2010
12	REVISED AS PER TRAFFIC SAFETY COMMENTS	APRIL 16, 2010
13	REVISED BUILDING LOCATION PER CLIENT	JULY 27, 2010

CLIENT: MARK LANIGAN (OWNER)
ROTHERGLEN SCHOOL
929 OLD DERRY ROAD
MISSISSAUGA, ONTARIO
P: 905-565-8707 F: 905-565-0485

PROJECT TITLE: ROTHERGLEN SCHOOL
FILE APPLICATION # - SP 09/034 W11
COMMITTEE OF ADJUSTMENT # - A/301/09
related files SP00/097 and SP03/100.

SCALE:	1:300m
DRAWN BY:	MP
DESIGN BY:	HS
CHECKED BY:	RE
DATE:	OCT. 14, 2008
PROJECT NO:	08-018

ICON
Architects Inc.
201-5000 Dufferin Street, Toronto, Ontario M3H 5T5 T&F: 416-840-9279

SHEET TITLE:

SITE PLAN

PART OF LOT 11, CONCESSION 2, WEST OF HURONTARIO STREET
(GEOGRAPHIC TOWNSHIP OF TORONTO, COUNTY OF PEE)
CITY OF MISSISSAUGA, REGIONAL MUNICIPALITY OF PEE

SHEET NO:

SP-1

STAMP:

REVISION NO:

13

NORTH

CONSTRUCTION NORTH

NOT FOR CONSTRUCTION
ISSUED FOR REVIEW & COMMENTS ONLY



May 8, 2017

Rotherglen School Walkway proposal – 929 Old Derry Rd., Mississauga ON

1. Introduction

-An executive summary of the scope of the project:

The proposal is to create a new permeable paver walkway approx. 15m long X 2.3m wide to connect an existing walkway on this property to a proposed new walkway on the neighbouring property at 7059 Old Derry Rd. W.

Note: 929 Old Derry Rd. is a 1.5 ha site containing several buildings, parking and driveway areas and open spaces. This document comments only that portion of the site affected by the proposed walkway.

-Background information to document the context of the proposal:

Site Plan application SP 16-149 W11 (7059 Old Derry Rd. W.)

- Identification of the property owner and stakeholders, current and proposed use

Rotherglen School, Meadowvale Elementary Campus. The buildings currently function as a private school providing instruction up to and including grade 8.

2. Project Description

Property Description:

- Identify the location, municipal address and provide an appropriate location map

*Part of Lot 11, Concession 2, West of Hurontario St.
929 Old Derry Rd. West, Mississauga.*



Key Plan

- Documentation of the existing conditions to include recent specialized photograph documentation, measured drawings, site plan, identification of site features such as topography, landscaping or other on-site features



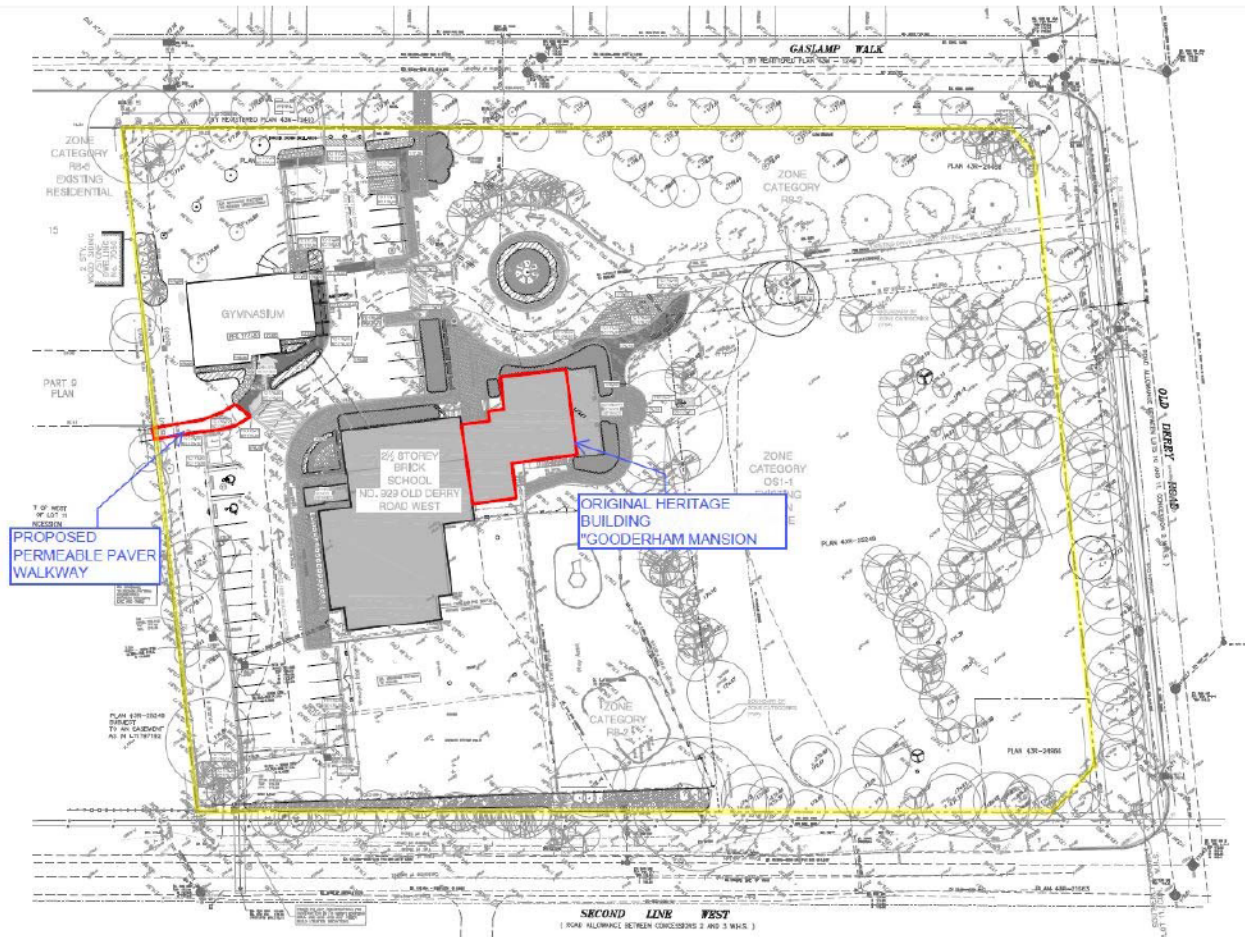
View looking south at proposed walkway



View looking north at proposed walkway

- Landscape inventory and documentation will include a site plan, views and vistas, water features, tree location and species, land forms, geological formations, fences, walls, berms, pathways, or any other landscape features

The proposed walkway is located on the northerly boundary of the property adjacent to the existing gymnasium and garbage storage area. There are no significant views or vistas into or out of the property associated with this location. There are no water features, significant land or geological formations associated with this area. There is a typical wood board fence across the northerly boundary of the property. This is not proposed to be altered except where a gated access will be created at the end of the proposed walkway.



Proposed Site Plan



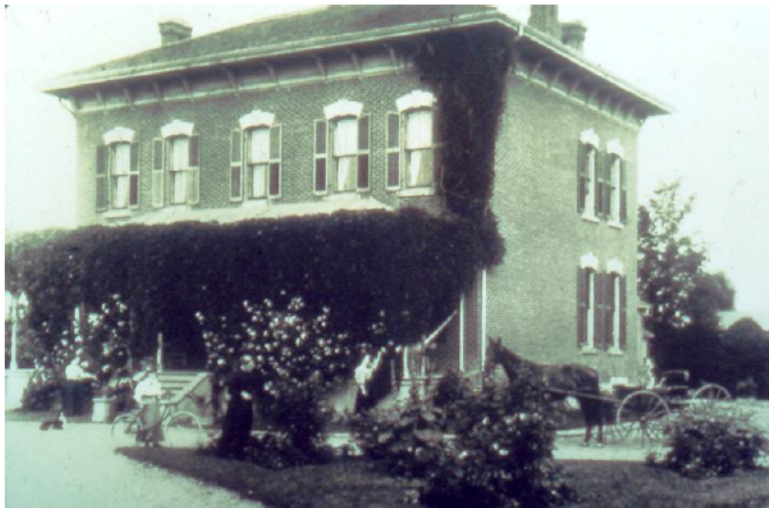
Approximate location of proposed path

- Identification of neighbouring properties, including any built form or features, required to illustrate the context of the subject property

To the north and east are single family dwellings in a faux-heritage subdivision built approximately 20 years ago. To the west is the Old Meadowvale Heritage Conservation District. To the north-west is 7059 Second Line W., a property recently acquired by the school which the proposed path is proposed to access.

- Summary of the history of the property outlining its development over time within a timeframe context

The main building on the site is known as the Gooderham mansion. This is an Italianate structure approximately 10,000 sq. ft. built as a single family home in 1870. It has subsequently been used for a number of other purposes and currently functions as the Meadowvale Campus of Rotherglen School. There are major additions to the north and west to effect this.



Gooderham Mansion c. 1900



Gooderham mansion - Rotherglen School today

- Documentation of land ownership from the original Crown Grant and subsequent records from the land registry office

Not applicable. The history of this building has been widely researched and reported

B) Significance:

- Statement of cultural heritage value or interest

City of Mississauga Statement of Cultural Heritage value:

This house was built by C.H. Gooderham circa 1870 and lived in the house for some years. It has had many occupants since, and was once called "Rose Villa" (circa 1900) and operated as a resort for wealthy Torontonians. Later it became a seminary and was also a residence for George Chavignaud (the artist), Walter Curry, an M.P. at the time and Major General Lessard, a veteran of the Boer War. This is a two storey, T-shaped brick structure which has a one storey addition to the rear. The roof is a low-pitch hip roof with a gable roof on the addition, while the main cornice is boxed and has ornate brackets and a paneled frieze. Two low shed dormers are on the east and west sides of the roof and there are three internally bracketed chimneys, two on either side of the main block and one in the tail. The house has a full basement, while the foundation is of coursed cut stone which support stretcher bond walls (this usually indicates a frame construction). There are five bays on the front south facade and five windows on the upper storey which are two over two paned, with double hung sash. The windows are segmental in shape and have curved lintels with vermiculated keystones and stone lugsills. There are also two of these windows on either side of the front door on the lower storey. There are four of these windows (two upper and two lower) on each side of the house and the same windows are on the tail. The addition has square headed one over one windows. The front door is segmental in shape and has a molded surround with engaged columns at the sides. The transom is glazed and the door has two raised panels. The facade of the house has been changed substantially by the extension of the roof line, with copies of the brackets and frieze to form the roof of a two storey verandah. The roof is supported by fluted Doric columns, which changes the house from an Ontario vernacular patterned brick house with a tent verandah to a "southern colonial mansion" type. This alteration has been extended by painting all the brickwork creamy white, much like the south. In 2004 a large red brick addition was put to the northwest of the original building. The property has been a school since circa 2000. The addition is "joined" to the original in a board and batten facade to act as a transition. The architectural elements of the addition were designed to compliment the original, yet remain distinct. The small barn behind the house was removed and a new structure similar in shape and size became the school's new gymnasium. The open space with views to and from the house to Old Derry Road are significant and relate to the history and use of the property over a long period of time. The views to the west on Second Line West are also important to the character of the Village. The large barn, some distance north, was demolished about 2000. The property was subdivided and sold to the land owners on Gaslamp Walk. Heritage Attributes: - The historical significance of the property under the ownership and development by the Gooderham family - The style, shape, form and materials of the original house structure. - The open green space and trees on the front (south) and west facades of the property that allow for views into and from the

property. - The retention of the topography on the property that reflects the 19th century creek and drainage on the lands Statement of Significance: The Gooderham Mansion, 929 Old Derry Road, is significant for its historic association with the Gooderham family, known for their controlling interest in the firm of Gooderham & Worts, and their land holdings and business within Meadowvale Village from circa 1860 to 1881. The structure has architectural significance for its size, shape, form and materials distinct within Meadowvale Village, and its context as an estate property and open green space and natural topographic features.

- Identification of the cultural heritage attributes and values of the property structures and landscape features

See above

- Identification of any recognized significance, such as a heritage designation by-law, historic plaque, etc.

The property is part of the Old Meadowvale Heritage Conservation District.

C) Planning and Policy Status:

- Provide details of the current land use and related Official Plan policies and Zoning

The property is zoned R8-2 and OS1-1 under the Mississauga Zoning By-law 0225-2007.

- Identify any regulatory requirements (e.g. heritage designation, flood plain requirements, etc.)

Heritage designation is noted above. There are no flood plain or Conservation issues. The proposed walkway is allowed under the zoning by-law.

3. Project Objectives

- Outline what is to be achieved by this project

The intention of the project is to a pathway to connect the campus of Rotherglen school with the property at 7059 Second Line W.

- Provide short term and long term goals and objectives

Short term and long terms goals are similar – to allow a pedestrian connection between these properties

- Proposed solutions for conservation of the property's heritage attributes

The proposed pathway is located at an insignificant edge of the property far from the character defining elements of the site. There are already many paths, parking lots and built landscape features on the property and this will mesh seamlessly with these. There will be no impact to the heritage attributes of the property because of this pathway.

- Provide the conservation policies to be used in this project (i.e. what conservation principles will be used to ensure long term conservation, maintenance, monitoring, and sustainable use of the property)

Not applicable. The long term use and conservation of the property is not affected by this proposal.

4. Statement of Heritage Intent

- An explanation is required that proposes the reasoning and considerations behind the choice of conservation treatments.

The proposed pathway is located at an inconspicuous part of the property and joins an already existing path. There is minimal impact to the site.

- Statement as to why one period of restoration over another was selected, rationale for new interventions, background resources used such as principles and conventions of heritage conservation.

As above, the intervention is minimal. There is no restoration or conservation.

- Statement as to the recording, inventory and disposition/retention of moveable cultural heritage resources (e.g. artifacts, archival material, salvaged material) and its incorporation into the conservation project.

Not applicable.

5. Condition Assessment of the Cultural Heritage Resource(s)

- Condition report of the cultural heritage resource(s) and specific attributes, identifying any deficiencies or concerns.

The condition of the area where the walkway is proposed is good. The adjacent structures are all modern elements to facilitate the school use.

- Detailed recommendations to mediate and prevent further deterioration. Direction as to use or change in use and how that relates to conserving the heritage attributes.

Not applicable.

- Outline opportunities and constraints with relation to all aspects of the project (i.e. budget, planning issues, public access, long term needs)

Not applicable.

- Recommendations for conservation treatments that reference the framework provided in Parks Canada Standards and Guidelines for the Conservation of Historic Places In Canada.

Not applicable.

6. Building System and Legal Considerations

- Statement to explain the building and site use from a practical, logistical and legal perspective

The site presently functions as a private school and has done so since about 2000. The use is allowed by the zoning by-law. There is no proposed change to the building or site use as a result of this pathway.

- Input from structural, mechanical, electrical, planning, geotechnical, trades, and all other required fields of expertise to ensure the project is viable and sustainable. Building and site system review may include:

- Site Work (e.g. landscaping, drainage, servicing)

Skira & Associates are the civil engineering consultants on this project. They have extensive experience in the Mississauga area.

- Trees, shrubs, other plantings,

Not applicable.

- Archaeological concerns and mitigation

Not applicable.

- Structural elements (e.g. foundation, load bearing)

Not applicable.

- Building Envelope (roof, wall cladding, window type), Ontario Building Code, Accessibility

SMDA and Belinda Jones Architect are the architectural consultants on the project

- Mechanical, Plumbing, Electrical

Not applicable

- Finishes and Hardware

The walkway is proposed to be permeable paver per the requirements of the Meadowvale HCD District Plan

- Fire Safety and Suppression

Not applicable

- Environmental Considerations, Lighting, Signage and Wayfinding, Security

No significant environmental considerations are expected. There is no requirement for lighting, signage, wayfinding or wayfinding as part of this proposal. There will be a locking gate installed where the path meets the 7059 Second Line W. property to prevent unauthorized use or entry.

- Legal Considerations (e.g. easements, encroachments, leasing, etc.)

The present owners are also the proposed operators of the building. There are no leasing arrangements. There are no encroachments. There is an easement for drainage in this area in favour of the City of Mississauga although the City has indicated that they have no issues.

7. Work Plan

- Timeline to describe, in chronological order, to meet the objectives and goals Statement as to specialized trades or skills that will be required to complete the work

The work will consist of:

- removal of existing sod and topsoil*
- preparation of sub-grade*
- installation of pavers*

The work will require qualified local trades but nothing particularly specialized. It is not expected that it will be difficult to find trades to execute the work.

- Proposed budget to meet and sustain the goals and timeline; long term and short term maintenance schedule

The budget has not been finalized

- Monitoring schedule, process and identify those responsible for monitoring

The professionals noted above will be responsible for review during the construction period.

8. Qualifications

- Heritage Conservation Management Plans will only be prepared by accredited, qualified professionals with demonstrated experience in the field of heritage conservation

A CV for Rick Mateljan is attached.

- Conservation Plans are usually a multidiscipline exercise whereby all consultants on the project must demonstrate accredited professionalism, experience and knowledge in their chosen field of expertise

All of the consultants are appropriately licensed in Ontario and have experience on similar projects.

9. Additional Information

- Bibliography of all documentation resources
- List of consultants and other professionals related to the project

The Consultant team is named above.

10. Additional Reports that may be required:

- Archaeological report, Arborist's report, Structural engineering report

Not applicable.

- Any other report that City staff may require to assess the project

11. Approval Authority

The City of Mississauga will be the approval authority for a Heritage Conservation Management Plan

Contact Information:

Inquiries regarding the submission and requirements of a Heritage Conservation Management Plan should be addressed to Heritage Planning, Culture Division, City of Mississauga

Email: culture.division@mississauga.ca

City of Mississauga

Corporate Report



Date: 2017/06/06

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
Community Services

Originator's files:

Meeting date:
2017/07/11

Subject

Request to Alter a Heritage Designated Property: 7080 Gaslamp Walk (Ward 11)

Recommendation

That the Corporate Report dated June 6, 2017, from the Commissioner of Community Services entitled *Request to Alter a Heritage Designated Property: 7080 Gaslamp Walk (Ward 11)*, to install an in-ground pool as depicted in the Appendix 1 and 2, be approved.

Background

The subject property is designated under Part V of the Ontario Heritage Act as it forms part of the Meadowvale Village Heritage Conservation District (HCD). Changes to the property are subject to the Meadowvale Village HCD Plan, 2014 and substantive changes identified in said plan require a heritage permit. The subject proposal is for an in-ground pool located in the rear yard of the property.

Policy 13 of the Meadowvale Village HCD Plan, states that "installation of pools and water features" constitute a substantive alteration. Substantive alterations will be evaluated as per Policy 14, including consideration of impact to individual property, HCD's form, scale, density and character, immediate streetscape, abutting properties, visibility from public realm, degree of change to existing lot grade and including but not limited to the following: compliance to section 4.2.1 of HCD Plan, retention, restoration and reuse of original materials and features.

Comments

The proposed in-ground pool extension will not be visible from the public realm being internal to the subject property. Fences already exist on the property and are proposed to remain. There is no visual impact to the adjacent properties given that the grade is not elevated or pronounced, the subject property borders the HCD and is only partially within the district boundary. The proposed pool location within the property actually straddles the HCD boundary, as it is estimated to be located approximately half within and half outside the district boundary as per the designation by-law schedule A (HCD Plan page 3).

Financial Impact

There is no financial impact.

Conclusion

The proposal complies with the Heritage District Plan having no adverse impact to the heritage district's character therefore it should be approved.

Attachments

Appendix 1: Site Plan Drawing

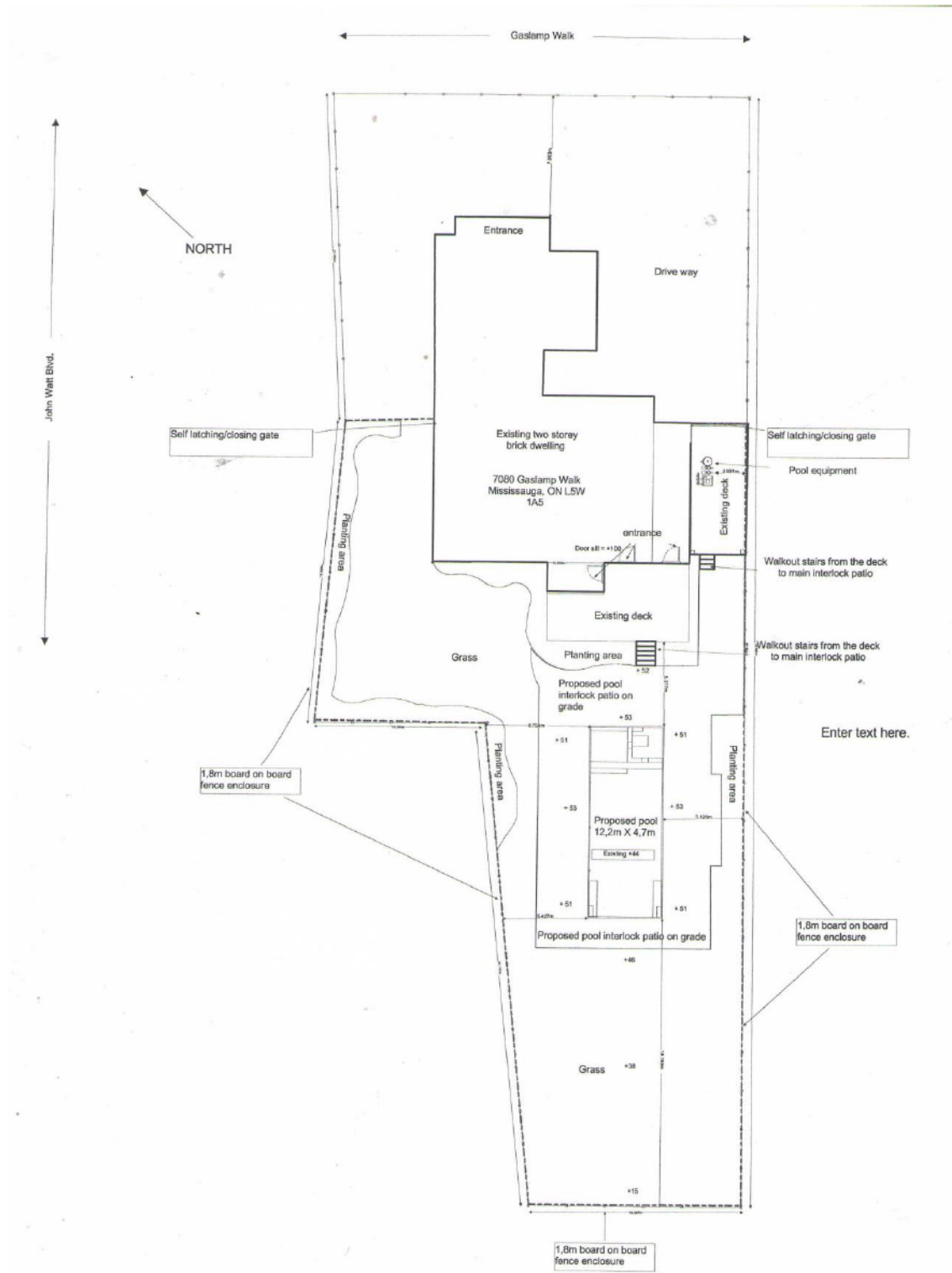
Appendix 2: Photos of the property



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: Cecilia Nin Hernandez, Heritage Coordinator

Appendix 1







City of Mississauga

Corporate Report



Date: 2017/06/06

Originator's files:

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
Community ServicesMeeting date:
2017/07/11

Subject

Request to Demolish a Heritage Listed Property: 3098 Merritt Avenue (Ward 5)

Recommendation

That the property at 3098 Merritt Avenue, which is listed on the City's Heritage Register, is not worthy of heritage designation, and consequently, that the owner's request to demolish proceed through the applicable process.

Background

Section 27.3 of the Ontario Heritage Act states that structures or buildings on property listed on the City's Heritage Register cannot be removed or demolished without at least 60 days' notice to Council. This legislation allows time for Council to review the property's cultural heritage value to determine if the property merits designation.

The owner of the subject property has submitted a heritage permit application to demolish and replace the existing detached dwelling. The subject property is listed on the City's Heritage Register as it forms part of the Malton Wartime Housing cultural landscape. This cultural landscape is noted for being a planned subdivision of the WWII and post-war era government efforts to provide mass produced housing to workers in industry related to the war effort and to veterans respectively within the City of Mississauga.

Comments

The owner of the subject property has requested permission to demolish the existing structure. The applicant has provided a Heritage Impact Assessment compiled by Megan Hobson, built heritage consultant. It is attached as Appendix 1. The consultant has concluded on page 19 of the submitted report that the structure at 3098 Merritt Avenue is not worthy of designation. Staff concurs with this finding.

Financial Impact

There is no financial impact.

Conclusion

The owner of 3098 Merritt Avenue has requested permission to demolish a structure on a property that is listed on the City's Heritage Register. The applicant has submitted a documentation report which provides information which does not support the building's merit for designation under the Ontario Heritage Act, as concluded on page 19 of the consultant's report. Staff concurs with this finding.

Attachments

Appendix 1: Heritage Impact Assessment



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: Cecilia Nin Hernandez, Heritage Coordinator

HERITAGE IMPACT STATEMENT



3098 MERRITT AVENUE, MISSISSAUGA
Malton War-time Housing Cultural Landscape

FINAL REPORT
02 JUNE 2017

MEGAN HOBSON
M.A. DIPL. HERITAGE CONSERVATION
Built Heritage Consultant
45 James Street, Dundas, ON L9H 2J5
905.975-7080
mhobson@bell.net

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APPENDIX A: LAND RECORDS	ATTACHED
APPENDIX B: SITE PHOTOS	ATTACHED
APPENDIX C: DRAWINGS (AS-FOUND DRAWINGS & PROPOSED DEVELOPMENT)	ATTACHED

1.0 BACKGROUND & METHODOLOGY

The subject property is located in the *Malton War-time Housing Cultural Landscape*. This report was prepared by heritage consultant Megan Hobson for the property owner of 3098 Merritt Avenue as a requirement for obtaining approval to demolish the existing one-storey dwelling and detached garage and construct a new two-storey dwelling with an attached garage. This report was prepared in accordance with the City of Mississauga's *Terms of Reference for Heritage Impact Statements for Cultural Landscapes*.

A site visit was undertaken by Megan Hobson on April 24th 2017 to assess and document the current condition of the property and its relationship to the surrounding built environment. Historical research was carried out, including a review of relevant primary and secondary sources, and a title search to determine past ownership of the property.

2.0 LOCATION AND SITE DESCRIPTION

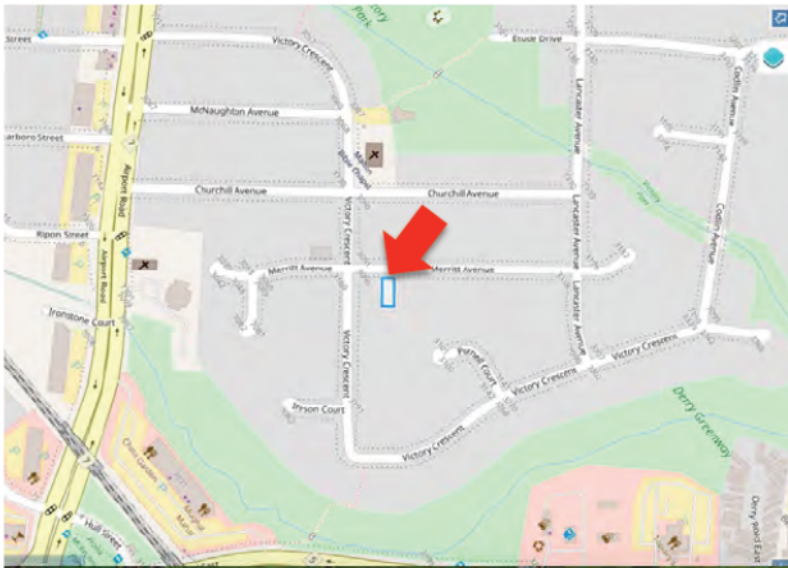
See Appendix A: Site Photos & Appendix C: Drawings

The *Malton Wartime Housing Cultural Landscape* is a planned subdivision in Malton, a residential neighbourhood in the north-east corner of Mississauga. It is just north of Pearson International Airport and is bounded by Derry Road on the south and Airport Road on the west.



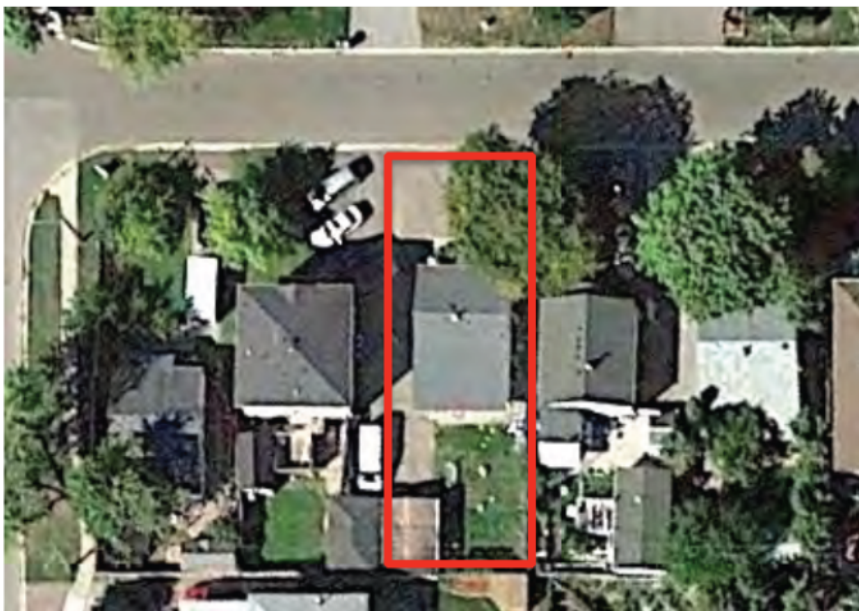
Location Map: Malton War Time Housing Subdivision

The subject property is located on the south side of Merritt Avenue and is the third lot east of Victory Crescent.



Location Map: 3098 Merritt Avenue

The lot is similar in size and configuration to lots throughout the subdivision. It is approximately 40 feet wide by 100 feet deep and the house is placed near the center of the lot and there is a side driveway and a small detached garage located in the back corner.



Aerial view of the subject property

In general, Merritt Avenue retains much of its original character and is primarily characterized by one and one-and-a-half storey bungalows with uniform setbacks. However, this pattern appears

to be changing, and there are newly constructed 2-storey houses on some of the lots in this block.



View looking down Merritt Avenue, the subject property is located on the right.

The house at 3098 Merritt Avenue is a very modest one-storey, three-bay, wood-frame structure with a rectangular plan and side-gable roof. It has a raised concrete block foundation. There is a concrete stoop with vinyl-clad railings on the front. The roof is asphalt shingle and the exterior cladding is vinyl siding with artificial stone on the lower portion of the front façade. The windows throughout are modern vinyl-clad windows.



Existing house & detached garage located in the rear yard.

The lot is level and there is a small front yard and a larger back yard. The front yard contains a lawn and foundation plantings near the house. There is one mature tree and a clump of cedars off to one side. There are no sidewalks on Merritt Avenue and the front lawn extends to the curb. There is a paved side driveway and a small wood-frame detached garage located at the back of the property. The back yard is grassed and enclosed on two sides by a wood fence.

The interior layout is simple and the front door opens directly into the living room. The ground floor contains a living room, kitchen, bathroom and two bedrooms. A small one-storey extension at the back contains an eating area, access to the back yard and stairs to the basement. The basement level contains a laundry room, a furnace room, a second bathroom and a recreation room with a kitchenette at one end. Interior finishes include drywall, wood paneling and laminate flooring.

Living room (left) and bedroom (right) on the ground floor.

Kitchen on the ground floor (left) and recreation room in the basement (right).

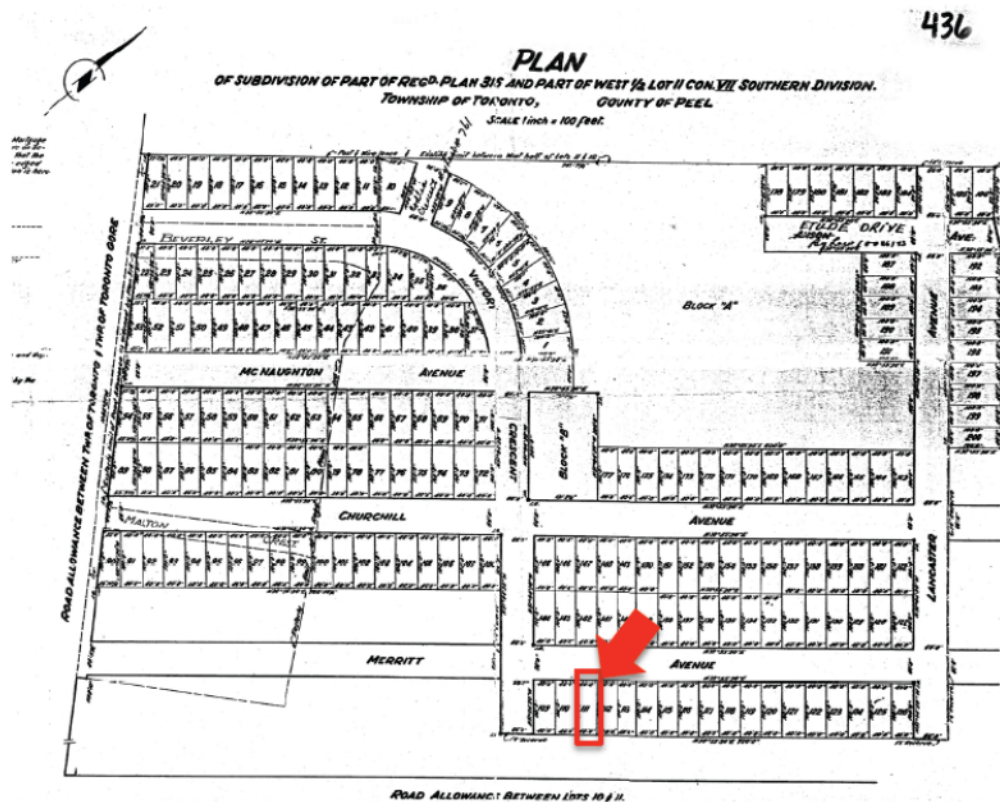
3.0 PLANNING CONTEXT

3.1 Malton War Time Housing Cultural Landscape (L-RES-5)

The subject property is located in the *Malton War-time Housing Cultural Landscape* and has been on the City's Heritage Register since 2005. As such, it is protected under Section 27 (1.2) of the *Ontario Heritage Act* and a *Heritage Impact Statement* prepared by a qualified heritage consultant is required for any significant alteration or enlargement of an existing dwelling or its total replacement.

The *Malton War-time Housing Cultural Landscape* consists of a small network of streets with approximately 200 building lots laid out by the Central Housing & Mortgage Corporation on which modest houses were built to standardized plans. Malton was a hub of aircraft building

and the subdivision provided homes and a family-oriented community for workers employed at Victory Aircraft and other aircraft related industries in Malton. The layout included land that was reserved for a school, a community hall and a park.



Location of the subject property on the original Plan of Sub-division registered by the Central Housing & Mortgage Co. in 1952.

At the time of Listing, the original layout and much of the original wartime building stock in the *Malton War-time Housing Cultural Landscape* was intact. The character of the subdivision is defined by wide streets lined with modest one and one-and-a-half storey frame houses. The subdivision has no sidewalks and the houses are set back from the road and typically have unfenced lawns in front.

Most of the individual houses within the *Malton War-time Housing Cultural Landscape* have been altered and/or enlarged in various ways. Until recently, these changes have been incremental and small in scale. New cladding and window replacements are typical, as are small additions such as porches and entry vestibules.

More recently there have been larger impacts to the area including demolition of individual houses to allow construction of larger two-storey residences that are noticeably different in style and materials. In response to this development pressure, Malton has been identified as a neighbourhood in Mississauga that is beginning to see a transition to newer housing and stronger policies have been developed to manage the changes that are occurring.

Examples of recent infill house in the Malton War Time Housing subdivision:



Churchill Avenue



McNaughton Avenue

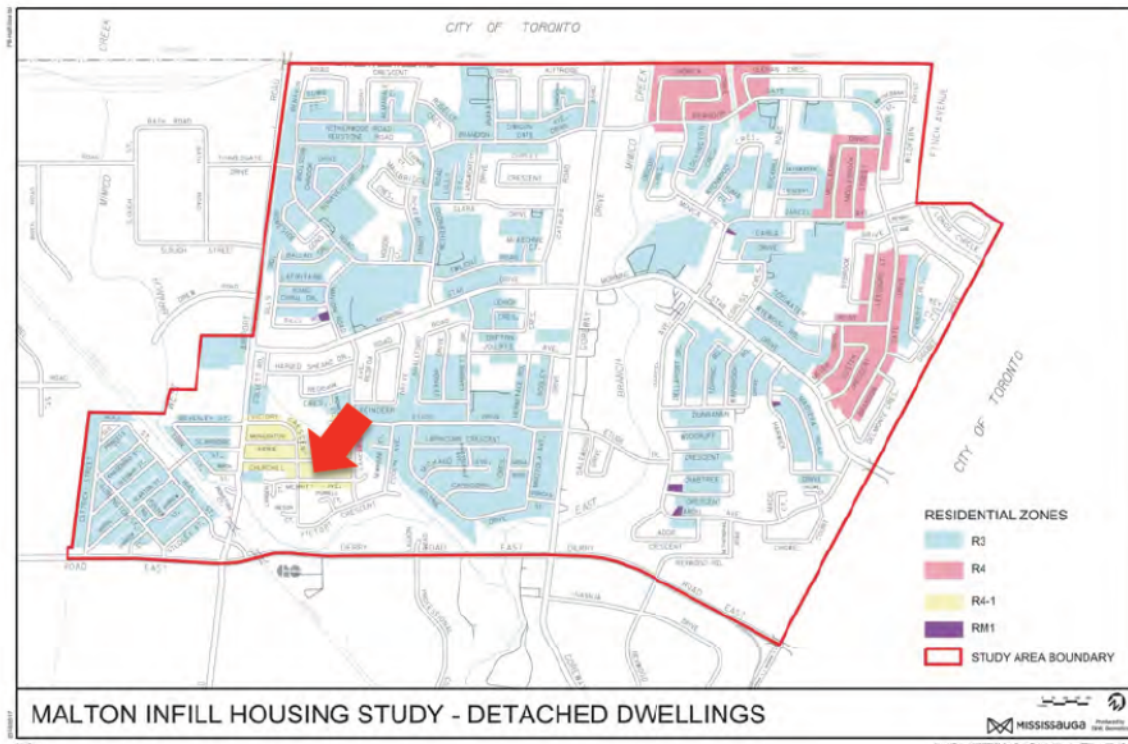


Victory Crescent



3.2 Malton Infill Housing Study

In 2016 the City released the *Malton Infill Housing Study* to ensure that new dwellings and additions are constructed in a manner that is sensitive to the surrounding context by introducing new zoning regulations to control massing. The area known as the *Malton War Time Housing Cultural Landscape* has been zoned R-4-1 for single-detached dwellings. Under the new zoning, maximum building height and Gross Floor Area has been slightly reduced. The maximum height permitted is 9.0 m to the ridge of the roof, reduced from 10.7 m to the mid-point of the roof previously, and the maximum lot coverage permitted is 30%, reduced from 35% previously.



The subject property is located in an area zoned R4-1 where new zoning regulations have recently been put in place to reduce building heights and lot coverage.

4.0 HISTORICAL CONTEXT

The subject property is one of approximately 200 standardized houses built by the Federal Government in 1952 to house wartime workers employed at the nearby Victory Aircraft manufacturing plant. The chart below provides a brief chronology of the transformation of this area from rural farmland in the 1850s to a planned subdivision in 1952. The aircraft manufacturing plant historically associated with Victory Village was demolished in 2005 but the aerospace industry continues to be a major employer in Mississauga and the Greater Toronto Area.¹

¹ City of Mississauga, Mississauga; Strength in Advanced Manufacturing. A Study in Automotive and Aerospace Clusters (2006).

² Adams and Sijpkens; pp. 17-18.

DATE	EVENT
c. 1820	Earliest settlement in Toronto Township
1854	Grand Trunk Railway line connects Malton to Toronto
1855	Subdivision of the Village of Malton, named after a place in Yorkshire County, England
1867	Malton chosen as the County seat
1868	Brampton replaces Malton as the County seat
1937	Toronto Harbour Commission purchases 13 farms (1,410 acres) to build an international airport and establish an aircraft manufacturing industry. The airport is named the Malton Airport.
1938	National Steel Car builds a manufacturing plant on the southwest corner of Airport and Derry Road
1939	World War II begins
1942	Federal Government expropriates National Steel Car and sets up a crown corporation called Victory Aircraft that produced Avro Lancaster bombers from 1942-45.
1942	The Canadian Government purchases 91.4 acres of farmland north of the Malton airport to build a housing subdivision for workers employed at the Victory Aircraft manufacturing plant. A sub-division is built by Wartime Housing Co. Ltd. that contains approximately 200 houses and is named ' Victory Village '. The street names have wartime references such as Victory, McNaughton, Churchill and Lancaster. Land is set aside for an elementary school (Victory Public School), a community hall (Victory Community Hall) and a public park (Victory Park).
1945	The Victory Aircraft manufacturing plant is bought by A.V. Roe Canada
1949	A.V. Roe begins working on the legendary Avro Arrow (CF-105), an advanced, supersonic, twin-engine, all-weather interceptor jet aircraft.
1951	Malton subdivision is ceded to Toronto Township
1952	Plan of Subdivision is registered so that individual lots can be sold (Plan 436).
1959	Manufacture of the Avro Arrow is cancelled by Prime Minister John Diefenbaker. About 15,000 employees at the Malton plant lose their jobs.
1962	A.V. Roe manufacturing plant bought by de Havilland Canada
1965	de Havilland manufacturing plant bought by Douglas Aircraft
196?	Victory Public School closes, students transferred to Malton Public School
1974	Malton become part of the City of Mississauga
1984	Malton Airport is renamed Lester B. Pearson International Airport
1997	McDonnell Douglas Canada manufacturing plant bought by Boeing Canada
2005	Boeing Canada manufacturing plant demolished

Table 1.0 Significant Dates

4.1 Macdonald/Codlin Farm

The northeast Toronto township of Malton was first settled in 1823. The building of the Grand Trunk Railway in the 1850's provided Malton with access to larger markets and the township prospered. The old Village of Malton was located west of Airport Road (the former town line between Toronto and Toronto Gore Townships) and north of Derry Road. The *1859 Tremaine Map* shows the original layout of the Village of Malton. The Malton Wartime Housing

subdivision is located on farmland located to the east of the village that was originally part of Toronto Gore. On the Tremaine map the owner of the 100-acre parcel comprised of the West half of Lot 11 in Concession VII is Alex McDonald. This would later be sold to the Codlin family.



Detail from Tremaine's Map of Peel County (1859) showing the 100-acre farm owned by Alex McDonald where the Malton war time housing subdivision is located.

4.2 Malton Airport

In 1937 the Toronto Harbour Commission purchased farmland near Malton to build an international airport. With federal and provincial government assistance, an aircraft manufacturing industry was established here. The airport was originally called the Malton Airport. Following construction of the Malton airport, Malton shifted from an agricultural to an industrial economy and became a world leader in aviation design and manufacturing. Other major industries, such as National Steel Car, also established manufacturing plants in Malton.



Aerial Photo showing the Malton Airport shortly after it was built in 1937. In 1984 it was renamed the Lester B. Pearson International Airport.

The Second World War boosted industrial development in Malton and the Malton Airport became a training facility for British Air Forces. The National Steel Car plant was expropriated by the Federal government in 1942 and a crown corporation called Victory Aircraft was set up. Wartime production required a large work force to built armaments and aircraft.



Wartime workers at the Government-owned Victory Aircraft Manufacturing Plant in Malton. Employees posing with a Lancaster Bomber produced at the plant.

4.3 Malton War Time Housing Subdivision

In order to attract a skilled and permanent workforce, the Federal government financed the design and construction of a residential subdivision on undeveloped farmland close to the Victory Aircraft manufacturing plant. This subdivision contained modest but well designed single detached homes suitable for young families who could lease them at very reasonable rates. Monthly rents ranged from \$22-30. There were four basic models; *Type H1* (a one-storey 24' x 24' dwelling with a living room, two bedrooms, kitchen and bath), *Type H22* (a one-storey 24 ½ x 28' version of the Type H1), and *Type H12* (a two-storey 24' x 28' unit containing additional bedrooms on the second floor).²

E.A. Bollinger NSARM accession no. 1975-3051941 no. 44-17 www.gov.ns.ca/nsarm/ c 2014



Examples of typical war time housing erected across Canada by the Federal Government. Built of pre-fabricated wooden components that were assembled on site.

² Adams and Sijpkens; pp. 17-18.

In typical wartime spirit, the Malton Wartime Housing subdivision was called *Victory Village* and the names of the streets contain wartime references such as Churchill, McNaughton and Lancaster. In a very short time, a healthy spacious neighbourhood was created with nearly identical houses on 40 x 100 ft. The large lots provided space for residents to establish Victory gardens to alleviate food shortages and improve the health of their families. A park, school and a community center were included in the layout and close communities developed as the residents worked and lived together.³

Staff architects employed by the Wartime Housing Corporation designed inexpensive homes of non-essential materials that could be erected almost overnight by mass production. Sections of wall, floor and roof were prefabricated and assembled on site by skilled crews that could erect a house in less than 36 hours. The exterior was clad in wood shingle, clapboard or weatherboard. Interiors had hardwood floors. Houses were heated by coal or wood burning stoves.

Although wartime housing was designed to be dismantled after the war, in many communities this never happened.⁴ After the war, the War-time Housing Corporation became the Canada Mortgage and Housing Corporation (CMHC), the federal crown corporation responsible for administering Canada's National Housing Act.⁵ The CMHC oversaw the sale of war-time houses across the country and oversaw construction of new housing for returning Veterans. After the war, many families living in the Victory Village stayed on and purchased their homes. Prices typically ranged from \$2,500 to \$4,500. The area saw an influx of Italian and Polish immigrants from the immediate post-war period through the 1960s.

4.4 South-Asian Immigration

Since the 1960s, the proximity to Pearson International Airport has attracted immigrants from India including a large number of Sikhs. The area also has a significant number of immigrants from Sri Lanka, Bangladesh, and Pakistan. These immigrants have transformed the area with specialty food and clothing stores, temples, mosques and gurdwaras serving the South-Asian community. There is a large Punjabi/Indian shopping plaza on Airport Road & Drew Road, opposite the Malton War Time Housing subdivision. This plaza contains the Sikh Heritage Museum and is adjacent to the Sri Guru Singh Sabha, a Sikh place of worship. In 2011 the Malton Majid mosque on Airport Road, adjacent to the Malton War Time Housing subdivision, opened as a place of worship and educational centre for the areas Muslim community.

³ National Film Board

⁴ Ibid.

⁵ Ann McAfee, 'Canada Mortgage and Housing Corporation', Canadian Encyclopedia (2006).

5.0 HERITAGE VALUE

Heritage values associated with the Malton War Time Cultural Landscape are identified in the City of Mississauga Cultural Landscape Inventory (L-RES-5). This area is valued for its historical associations with World War II and for the character of the built environment that “retains a number of post-war houses which represent some of the first mass produced housing in the GTA”

The subdivision is a physical reminder of Malton’s involvement in the Second World War and the aviation industry in the immediate post-war period.⁶ Much of this history is communicated by the names of the streets (i.e.; Churchill, McNaughton, Lancaster etc.) and the name of the public park (Victory Park) and the former Community Centre (Victory Hall). The subdivision was named Victory Village because of its wartime heritage and its proximity to the Victory Aircraft manufacturing plant. This nomenclature is important for preserving the area’s heritage value. Victory Park and Memorial Hall are also important for preserving the area’s historical associations with World War II.

The survival of much of the original wartime building stock gives the area a distinctive character. However, given that this was built as temporary housing, and given the increase in land prices and the development pressure in this area, it is reasonable to expect that many if not all of these houses will eventually be replaced by more substantial homes. This trend is already evident and there are numerous examples of new 2-storey brick and stone clad houses throughout the subdivision that have replaced the original housing stock.

The house at 3098 Merritt Avenue is typical of the original housing stock and similar to wartime houses built across Canada between 1942 and 1945. It is a very modest three-bay, single-storey, wood-frame structure with a rectangular plan and side-gable roof. It is a modified example of the standard H-1 Plan developed by the War-time Housing Corporation in communities across Canada.

It has a concrete block foundation and a basement level containing a laundry room, furnace room, bathroom and recreation room. Most of the original houses in the subdivision were not built with basements. It is possible that this house has a basement because it was relocated. It may be one of the houses that were moved here from Bramalea Road when the airport was expanded in 1950. Approximately one in four houses in the Malton War Time Housing subdivision were relocated here from Bramalea Road.⁷

More recent changes to the dwelling include recladding of the exterior and replacement of the original wood windows and doors. The house is clad with vinyl siding and there is artificial stone cladding on the lower portion of the main façade. There is a concrete stoop in front with vinyl-clad handrails. There are vinyl windows throughout, the vinyl-clad soffits and an asphalt shingle roof. Interior finishes have been updated and there is modern drywall and laminate flooring throughout. The original side gable roofline has not been altered but there is a one-storey rear extension with a shed roof.

⁶ Heritage Mississauga, *Malton; Founding a Village*.

⁷ City of Mississauga, *Cultural Heritage Landscape Inventory (2005); Appendix 2: Site Description for L-RES-5 War Time Housing (Malton)*.

5.1 Evaluation According to Ontario Regulation 9/06.

Compliance with Ontario Heritage Act, Ontario Regulation 9/06: Criteria for Determining Cultural Heritage Value or Interest

According to Subsection 1 (2) of Ontario Regulation 9/06, Criteria for Determining Cultural Heritage Value or Interest, a property may be designated under section 29 of the Act if it meets one or more of the following criteria:

1. The property has design value or physical value because it,
 - i. is a rare, unique, representative or early example of a style, type, expression, material or construction method,
 - ii. displays a high degree of craftsmanship or artistic merit, or
 - iii. demonstrates a high degree of technical or scientific achievement.

3098 Merritt Avenue is not rare or unique, because similar wartime houses were built in many communities across Canada. It is somewhat representative of the standard H-1 Plan developed by the War-time Housing Corporation but it has been subject to a number of later alterations. It does not display a high degree of craftsmanship or artistic merit because it was intended as a temporary structure to be dismantled after the war. It demonstrates a moderate degree of technical achievement in the standardization and mass assembly process used in its design, fabrication and construction.

2. The property has historical value or associative value because it,
 - i. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,
 - ii. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or
 - iii. demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.

3098 Merritt Avenue, as part of the larger Malton War Time Cultural Landscape, is historically associated with the Federal housing subdivision that was built to house workers associated with war time industries to provide temporary housing for war-time workers and their families. This association is significant to the history of Mississauga as a major center in Canada associated with the aviation industry and its significant contribution to the war effort. The historical associations are primarily reflected in the entire planned subdivision not by individual houses within the Malton Cultural Landscape. The physical fabric of the house does not yield information that contributes to an understanding of the community or its culture. As a mass-produced standard house type, it reflects the generic ideas of the Wartime Housing Corporation and is not associated with any particular architect, artist, builder, designer or theorist who is significant to the community.

3. The property has contextual value because it,
 - i. is important in defining, maintaining or supporting the character of an area,
 - ii. is physically, functionally, visually or historically linked to its surroundings, or
 - iii. is a landmark.

3098 Merritt Avenue has some contextual value as a component within the Malton War Time Housing Cultural Landscape. Individually it has some importance in defining the character of the area because it retains its original scale, but this importance has been somewhat eroded due to the installation of new exterior cladding, replacement doors and windows, and new interior finishes. It is not a landmark building.

The subject property does not meet provincial criteria for individual Designation under Part IV of the Heritage Act.

6.0 PROPOSED DEVELOPMENT

The applicant plans to demolish the existing one-storey house and detached garage and replace it with a two-storey house with a full basement and an integrated two-car garage. The driveway will be relocated to the other side of the lot and will be widened to accommodate two vehicles entering the garage. Relocation of the driveway will require removal of one mature deciduous tree and a clump of cedars in the front yard near the side property line.



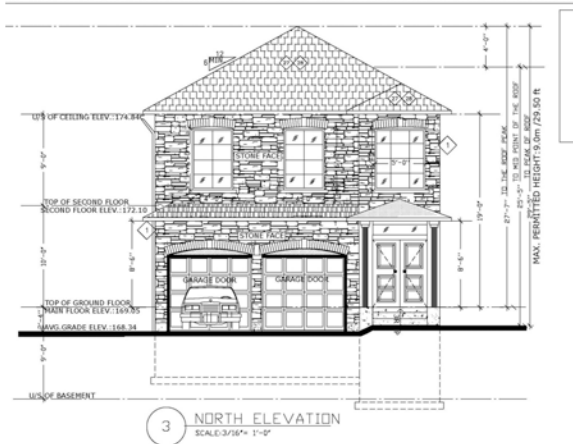
One tree and a clump of cedars will be removed so the driveway can be relocated.

The new house will not exceed the new zoning requirements in terms of the setbacks or overall building height. The proposed front and rear setbacks are 7.5m and the side setbacks are 1.81m, which are the minimum allowed. The height of the roof to the ridge is 9.0m, which is the maximum allowed. Minor variances will be required for the Lot Coverage and the Gross Floor Area (GFA). The Lot Coverage of the proposed residence will be 34.83%, which is consistent with the old zoning requirement that allowed up to 35%, but slightly over the 30% maximum lot coverage permitted under the new zoning. The total GFA of the proposed residence will be approximately 2,071sq. ft. which is slightly over the new GFA which has been capped at 1,862 sq. ft.



Streetscape; the proposed residence is comparable in scale and massing to adjacent infill housing and compatible with the existing war time housing stock.

The new house will have a hipped roof with asphalt shingle tile and the wall cladding proposed is a buff coloured stone veneer with lighter stone trim. The main façade facing Merritt Avenue is composed of three bays including the two-bay garage and the main entry located to one side. The three windows on the second floor are symmetrically arranged. The windows are rectangular with a shallow segmental arch above and simulated divided lights in a 4 over 4 configuration. The front entrance projects slightly from the front wall of the house and has a covered porch with a hipped roof. The main entrance is framed with columns and there is a double wood door with a glazed transom above. The garage doors are paneled and the openings are arched and trimmed to match the window openings.



A Neo-Traditional house design is proposed that is consistent with other infill housing in the area.

Stylistically the new design is typical of suburban house designs found throughout the GTA. The massing is box-like so that the floor area can be maximized based on the buildable area permitted. There are design elements on the main elevation (noted above) that provide some articulation of the massing from the street. The same architect designed a very similar house a few doors down at 3110 Merritt Avenue that was approved under the old zoning and is therefore slightly higher than the house being proposed for the subject property. In general, the architectural embellishments are modest and the design is fairly traditional in the use of a hipped roof, a low window to wall ratio, the use of rectangular windows (taller than they are wide) with segmentally arched tops, and the use of windows with divided lights in the manner of a traditional sash window.



3110 Merritt Avenue designed by the same architect in a similar manner.

7.0 POTENTIAL IMPACTS ON HERITAGE VALUE

The City of Mississauga has developed criteria for identifying the significant values associated with cultural landscapes. The *Cultural Landscape Inventory* provides a checklist of the specific attributes associated with the *Malton War Time Housing Cultural Landscape*.⁸ A *Heritage Impact Statement* must demonstrate how the proposed development will conserve these attributes.⁹ A list of these attributes and a conservation strategy is outlined below.

Built Environment

- **consistent scale of built features**

The proposed development includes demolition of a small one-storey war-time bungalow and construction of a new two-storey suburban house in its place. The increase in building height from one-storey to two-storeys is not significant and will not have a major impact on the cultural landscape. The original subdivision included one and one-and-a half-storey houses. A two-storey residence does not represent a significant increase in building height. Furthermore, the streetscape on Merritt Avenue already includes some newer two-storey homes.

Historical Associations

- **illustrates a style, trend or pattern**

The Malton War-time Housing cultural landscape is a relatively intact example a subdivision built by Wartime Housing Limited between 1941 and 1945. These developments were standardized across the country with only minor variations. Although these subdivisions were considered to be temporary housing, many of these houses are still in use. The proposed development involves demolition of one of a wartime house that may have been relocated here from Bramalea Road and has been subject to a number of alterations including replacement of original doors, windows, exterior cladding and interior finishes. This house is not rare or unique in the neighbourhood and there are several identical house plans that are better preserved on elsewhere in the subdivision.

- **direct association with important person or event**

The Malton War-time Housing subdivision is associated with Wartime Housing Limited, a crown corporation formed in 1941 to finance, design and construct housing for workers in areas where there was a shortage of suitable housing. After the war, Wartime Housing Limited became the Canadian Housing and Mortgage Corporation (CMHC). The development proposal will result in the loss of 1 of approximately 200 original houses that were constructed in the wartime housing subdivision in the Malton. The historical association is conveyed by the whole area and not by its component parts.

- **illustrates an important phase of social or physical development**

⁸ Cultural Landscape Inventory; War Time Housing (Malton) L-Res-5. Included as an Appendix to this report.

⁹ City of Mississauga, Terms of Reference for Cultural Landscape Heritage Impacts Statements, 2013. Included as an Appendix to this report.

The Malton War-time Housing subdivision illustrates the physical development of the small rural crossroads village of Malton following the construction of the Malton airport c. 1937. War-time conditions accelerated the growth of this area due to the rapid increase in industrial production and the federally funded construction of the Malton subdivision that provided 200 new homes on spacious paved streets with modern amenities such as water, sewage, hydro and telephone lines. The development proposal will result in the loss of one of the original war-time houses but it will be replaced by a new home that will support the ongoing use historically associated with this area since 1942 as a residential subdivision comprised of detached single-family homes. The renewal of the housing stock will change the individual built forms but the original lot divisions and street layout will be conserved.

Other

- **Historical or Archaeological Interest**

The Malton War-time Housing subdivision has historical interest because of its connection with Federal housing projects carried out during World War II that provided temporary housing and amenities for workers and their families close to major war-time production centers across Canada. The development proposal will not significantly impact the historical associations of this area.

8.0 MITIGATION MEASURES

The demolition of the existing residential building has been sufficiently mitigated through research and documentation undertaken as part of this Heritage Impact Assessment Report including:

- Title search to show past ownership back to the original Crown grant
- Site survey drawing indicating existing buildings and trees on the property
- Photograph-documentation of the house, yard and neighbourhood context
- Measured drawings of the interior layout of the house

No further mitigation is required.

9.0 CONCLUSIONS & RECOMMENDATIONS

The house at 3098 Merritt Avenue does not meet criteria for individual Designation under Part IV of the *Ontario Heritage Act*. As part of the Malton War Time Housing Cultural Landscape, historic research and documentation of the site prior to removal is required. This Heritage Impact Assessment fulfills those requirements and no further mitigation is recommended.

The proposed development is consistent with the new infill housing zoning regulations for this area and is similar to other developments that have been approved. A minor increase in soft landscaping in front of the house and planting of a new tree in the front yard to replace an existing tree that will be removed is also recommended.

10.0 QUALIFICATIONS OF THE AUTHOR

The author of this report is a member in good standing of the Canadian Association of Heritage Professionals. Formal education includes a Master of Arts in Architectural History from the University of Toronto and a diploma in Heritage Conservation from the Willowbank School of Restoration Arts. Professional experience includes an internship at the Ontario Heritage Trust, three years as Architectural Historian and Conservation Specialist at Taylor Hazell Architects in Toronto, and 5 years in private practice in Ontario as a heritage consultant. Other relevant experience includes teaching art history at the University of Toronto and McMaster University and teaching research methods and conservation planning at the Willowbank School for Restoration Arts in Queenston. In addition to numerous heritage reports, the author has published work in academic journals such as the *Journal of the Society of Architectural Historians* and the *Canadian Historical Review*.

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APPENDIX A: SITE PHOTOS

CONTEXT



Figure 1: View looking down Merritt Avenue, subject property is on the right.



Figure 2: 3098 Merritt Avenue.

EXTERIOR



Figure 3: Main elevation on Merritt Avenue.



Figure 4: Front entrance.



Figure 5: Side elevation and side yard (left side)



Figure 6: Side elevation and paved driveway (right side).



Figure 7: Rear elevation and back yard.



Figure 8: Back yard.



Figure 9: Back yard & detached garage.



Figure 10: Raised concrete block foundation, vinyl siding and artificial stone facing.

INTERIOR



Figure 11: Front entrance with modern stair railings, door and window.

Figure 12: Living Room looking towards front entrance.

Figure 13: Living Room with wood paneling, drywall and laminate flooring.

Figure 14: Living Room with faux-stone wall feature.

Figure 15; Bedroom 1

Figure 16: Bedroom !

Figure 17: Bedroom 2

Figure 18: Bedroom 2 closet.

Figure 19: Bathroom

Figure 20: Kitchen with modern drywall, wood paneling and laminate flooring.

Figure 21: Kitchen

Figure 22: Dining area in kitchen and rear entry from back yard.

Figure 23: Rear entrance and interior door to basement level.

Figure 24: Basement stairs

Figure 25: Basement Laundry Room.

Figure 26: Basement Furnace Room.

Figure 27: Basement Bathroom

Figure 28: Basement Bathroom

Figure 29: Basement Recreation Room

Figure 30: Basement Recreation Room

APPENDIX B: LAND RECORDS

ADDRESS: 3098 Merritt Avenue, Malton
 LEGAL DESCRIPTION: Lot 111, Plan 436, Mississauga

INST. NO.	DATE	TYPE	GRANTOR	GRANTEE	LANDS
	1828	Patent	Crown	King's College	200 acres (Lot 11, Concession VII, Twp. of Toronto Gore, Peel County)
22051	1842	B&S	King's College	Alexander McDONALD	100 acres (NW half of Lot 11)
30556	1842	Will	Alexander McDONALD	Mary McDONALD, wife	W ½ Lot 11
50805	1853	Indenture	Mary McDONALD, widow	Alex McDONALD, son	"
1808	1863	Will	Alex McDONALD	Eliza MCDONALD, relationship not specified	"
1218	1890	B&S	Executor of the Estate of Eliza McDONALD	Thomas CODLIN	W ½ Lot 11, N of the GTR
2512	1913	Will	Thomas CODLIN	Fred CODLIN	"
3431	1942	B&S	Fred CODLIN et ux	His Majesty the King in the Right of Canada – *see attached <i>Survey H-20-A</i> Dominion of Canada, Dept. of Munitions & Supply, Wartime Housing Ltd.	91.4 acres (Lot II Concession VII)
By-Law 1471 Township of Toronto	1951	Annexation	Twp. of Toronto Gore	Malton Police Village, Township of Toronto	"
Plan 436	1952	Plan of Subdivision	Central Mortgage & Housing Corporation – *see attached <i>Plan 436</i>		
99421	1956	Grant	Central Mortgage & Housing Corporation	Michael & Stella DOBREEN, as joint tenants	Lot 111
36928	1967	Grant	Michael & Stella DOBREEN	Antoni & Parania MELNYK	"
584964	1981	Grant	Antoni & Parania MELNYK	Stella DOBREEN	"
584965	1981	Grant	Stella DOBREEN and Michael DOBREEN, as party of the 3 rd Part	Danny Michael DOBREEN	"
852979	1988	Grant	Danny Michael DOBREEN	Robert Gerald & Linda KING, as joint tenants	"
RO1061233	1994	Transfer	Robert Gerald & Linda KING, as joint tenants	David Anthony & Sylvia NANLAL	"
LT2048671	2000	Transfer	David Anthony & Sylvia NANLAL	Gregory Marna McCOMB	"
PR411835	2003	Transfer	Gregory Marna McCOMB	CURRENT OWNER	"

NOTE: Title search performed by Chris Aplin, M.C.A. Paralegal Services

LOT No. 11

7 CONCESSION S.D.

14

Toronto
G.O.

No. of INSTRUMENT.	INSTRUMENT.	ITS DATE.	DATE OF REGISTRY.	GRANTOR.	GRANTEE.	QUANTITY OF LAND.	CONSIDERATION OR AMOUNT OF MORTGAGE.	REMARKS.
	patent	Jan 3. 1828		Crown	Kings College	200 a	£ 0	
21187	B & S	10 Nov 1841	22 June 1843	Kings College	Hugh Cook	100 a N.E. $\frac{1}{2}$	—	
22051	B & S	1 July 1842	25 July 1844	Kings College	Alexander Mc Donald	100 a N.W. $\frac{1}{2}$	—	
30556	Will	31 Aug 1842	7 Jan 1848	Alex. Mc Donald		W $\frac{1}{2}$		
50805	Ind	18 Aug 1853	2 Sep. 1853	Mary Mc Donald widow	Alexander Mc Donald ^{Son}	10 $\frac{1}{2}$	(error)	of a Mc Don
50805	B & L	18 Aug 1853	2 Sep 1853	Mary Mc Donald ^{widow} et al	L. Y. R. Compy.	part W $\frac{1}{2}$	14. 0	
9136	Will	15 June 1859	25 Apr 1861	Hugh Cook	Grand Trunk Railway Company	100 a N.E. $\frac{1}{2}$		

OMB order re: Annexation of pt
Toronto Gore
into Toronto
Township...

TOWNSHIP OF TORONTO (C.A. 1986)

第1章 绪论	1
第2章 电路的基本定律和定理	2
第3章 电阻电路的分析和计算	7
第4章 动态电路的分析和计算	15
第5章 正弦稳态电路的分析和计算	25
第6章 非正弦周期电流电路的分析和计算	35
第7章 线性电路的功率分析	45
第8章 互感和耦合电感	55
第9章 变压器和理想变压器	65
第10章 三相电路	75
第11章 交流电路的谐振	85
第12章 网络函数和频率响应	95
第13章 二端口网络	105
第14章 非线性电路	115
附录A 常用物理量的单位	125
附录B 常用数学公式	135
附录C 常用电路元件的符号	145
附录D 常用电路元件的参数	155
附录E 常用电路元件的型号	165
附录F 常用电路元件的符号	175
附录G 常用电路元件的参数	185
附录H 常用电路元件的型号	195
附录I 常用电路元件的符号	205
附录J 常用电路元件的参数	215
附录K 常用电路元件的型号	225
附录L 常用电路元件的符号	235
附录M 常用电路元件的参数	245
附录N 常用电路元件的型号	255
附录O 常用电路元件的符号	265
附录P 常用电路元件的参数	275
附录Q 常用电路元件的型号	285
附录R 常用电路元件的符号	295
附录S 常用电路元件的参数	305
附录T 常用电路元件的型号	315
附录U 常用电路元件的符号	325
附录V 常用电路元件的参数	335
附录W 常用电路元件的型号	345
附录X 常用电路元件的符号	355
附录Y 常用电路元件的参数	365
附录Z 常用电路元件的型号	375

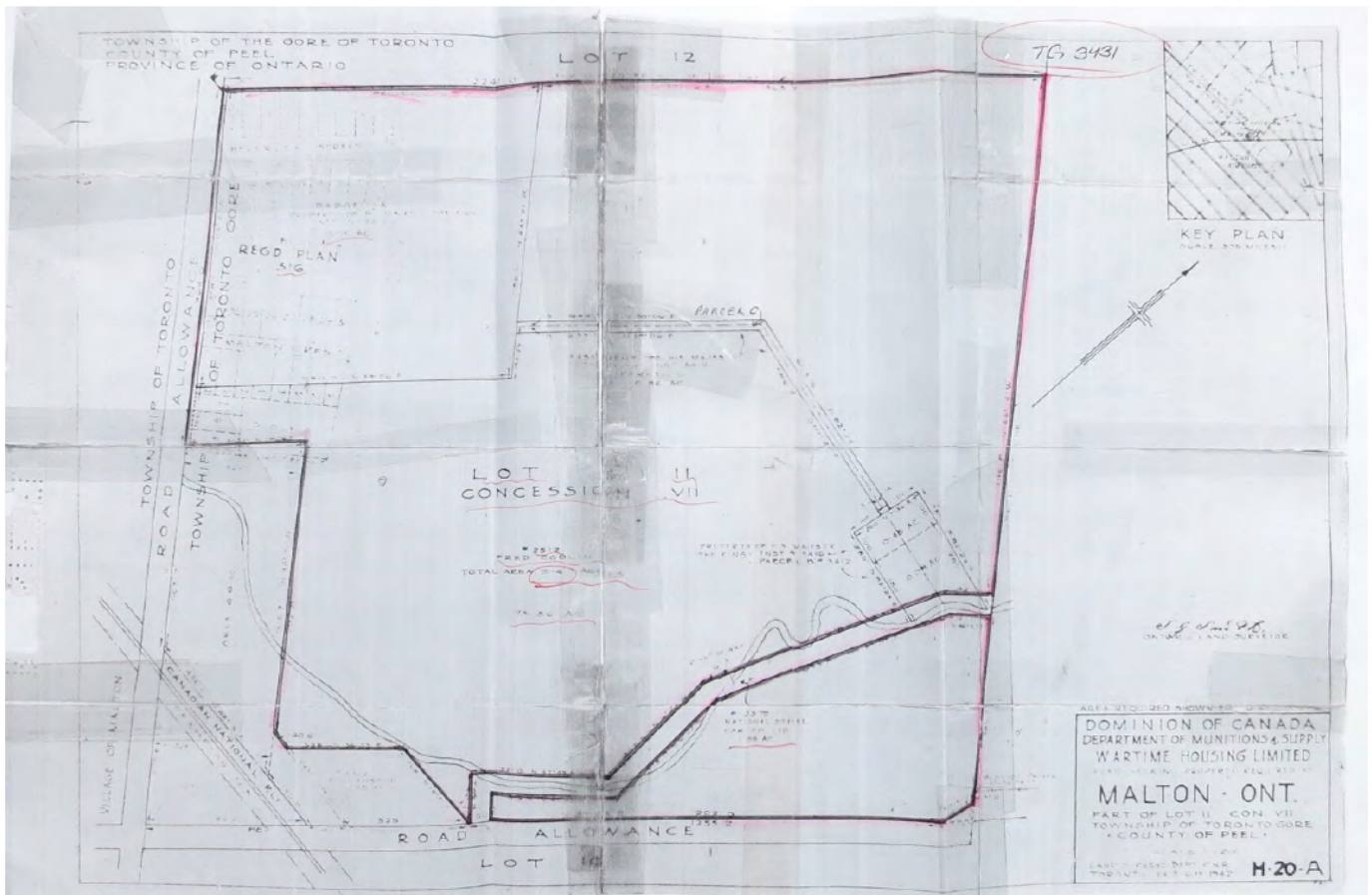
NUMBER	INSTRUMENT	DATE OF INSTRUMENT	DATE OF REGISTRATION	GRANTOR	GRANTEE	AREA	CONSIDERATION	REMARKS
687	A.G.	25Nov1900	17Nov1900	Ann Jane McDonald	Alex McDonald	100	\$100.00	
689	Chancery	23Dec1900	17Jan1901	Alex. McDonald	Quisting Little et al	100	\$100.00	
745	N.E.S.	18Mar1902	17Apr1902	Henry Cook	James Cook	50	\$100.00	
819	Chancery	27Mar1903	10Dec1903	Quisting Little et al	Alex. McDonald	100	\$100.00	
1206	Will	21Aug1903	30Oct1900	Alex. McDonald	Mize McDermid	100	\$100.00	
1218	B.A.S.	13Dec1900	13Dec1900	James Cook ex'r. of <i>Elsie McDonald</i>	Thomas Codlin	30	\$100.00	
1306	Lease	16Mar1901	18Dec1902	James Cook	Henry Cook et ux	50	\$100.00	
2444	Will	6Oct1908	7Apr1914	James Cook	James Cook	50	\$100.00	
2512	Will	8Sept1913	2Nov1918	Thomas Codlin	Fred Codlin	95	\$100.00	
2516	B.A.S.	20Nov1918	14Dec1918	James Codlin et ux <i>Jane Codlin</i>	Fred Codlin	95	\$100.00	
2633	Release	24Jun1921	15Jun1921	Henriette Hornell	James Cook	95	\$100.00	
2967	O.C.	30Oct1924	11Nov1924	William McCallister et ux Edna Whalley, Irene Galbreith, Annie Middlebrook heirs and heirs as at law of John McCallister <i>deceased</i>	John Galbreith	10	\$100.00	
<i>See Report No. 113. New York Suburban Lots Bond.</i>								
3061	Grant	30Jun1930	6May1931	John Galbreith et ux	Ira J. Rutherford	1 1/2	\$600.00	pt W.
3108	Grant	30May1931	26Aug1935	Ira J. Rutherford	Wm. H. Rutherford	1 1/2	\$1.00	as lying S of G.T.R.
3203	Grant	16May1936	27May1936	Tm. H. Rutherford et ux	Laurin D. Sheard	1 1/2	\$2.00	it is
3300	Grant	11May1939	10May1939	Fred Codlin et ux	Etzel Robinson	1 1/2	\$600.00	it is
3302	Grant	29May1939	21Mar1939	Fred Codlin et ux	Etzel Robinson	1 1/2	\$1.00	it is correcting 3300
3306	Easement	26Jun1939	4Jul1939	Fred Codlin et ux	National Steel Car Corp'n Ltd.		\$25.00	part W. Sketch attached. for sewer
3408	Agree't	3May1939	4Jul1939	Fred Codlin	Leslie Roy Limited	97		it is re: subdividing etc.
<i>See Report No. 700 (Codlin)</i>								
3416	Plan	21Jan1940	10Jul1939	Fred Codlin & Leslie Roy Lt.	a subdivision of part of			
3579	Grant	25Jun1941	26Jun1941	Fredrick Codlin et ux	National Steel Car Corp'n Ltd.		\$1.00	it is subject to r see report no. 700.
418 Ex'op.Plan		21Apr1942	9May1942	Wartime Housing Ltd.				it. for easement & sewer S.O.
418 Ex'op.Plan		6Jun1942	19Jun1942	Wartime Housing Ltd.				For sewer etc. Plan attached.
421	Grant	1Jul1942	23Oct1942	Martin Martin et ux	Wm. A. Lewis	100	\$500.00	
421	Grant	13Jul1942	20Oct1942	Wm. Lambie et ux Ex'rs. James Cook	Howard A. Martin & Marion Martin as joint tenants	100	\$900.00	As Grantees
421	G.T.	22Sep1942	7Oct1942	Henriette Wideman	Howard A. Martin & Marion Martin as joint tenants			

SHEET NO. 1
LOT NO. 11
CON. NO. 7
RANGE NO.

TOWNSHIP OF TORONTO

SHEET NO. 1
LOT NO. 11
CON. NO. 7
RANGE NO.

NUMBER	INSTRUMENT	DATE OF INSTRUMENT	DATE OF REGISTRATION	GRANTOR	GRANTEE	AREA	CONSIDERATION	REMARKS
3431	Grant	18Oct1942	29Oct1942	Fred Dodlin et ux	His Majesty the King <i>In the Right of Canada</i>	91.4	\$20,000.00	at pt. & C.L. of sketch attached.
3432	Exprop. Plan	28Nov1942	28Nov1942	Dept. of Munitions & Supply	(National Steel Car Corp'n)			pt. & C.L.
3436	Leasement	28Mar1939	28Aug 1943	Frederick Godlin	The Hydro Electric Power Comm.		\$6.00	3 poles.
3477	Exprop. Plan	31Oct1943	26Dec1943	Dept. of Munitions & Supply	National Steel Car Corp.			pt. & C.L. sketch attached.
3495	Grant	19Apr1944	28Apr1944	Carl Swartz	Ethel Johnston	1 1/2	\$200.00	as in No. 3496.
3496	Grant	19Apr1944	28Apr1944	Ethel Johnston	Carl Swartz	1 1/2	\$1,000.00	Comm in SE limit 100' E of C.L. of way. Thence S 22° E 100' parallel to Hwy. 200' A 1/2 X 1/2 parallel to Hwy 200' to E of
3519	Grant	27Sep1944	21Nov1944	Carl Swartz et ux	Elmer Guest	1 1/2	\$1,000.00	as in 3496.
3520	Grant	30Oct1944	21Nov1944	Elmer Guest et ux	Carl Swartz	1 1/2	\$420.00	as in 3496.
3536	Grant	31May1945	5Jul1945	Lechlen C. Sheard et ux	Lechlen C. Sheard & Bertha Sheard as joint tenants	1 1/2	\$1,000.00	W 1/2 lying S of C.T.R.
3590	Grant	16Apr1946	18Mar1946	Elmer Guest et ux	Core P. Maltby	1 1/2	\$200.00	as in 3496.
3591	D.M.	27Nov1946	18Mar1946	Carl Swartz	Elmer Guest			Sketch RE'g No. 3520.
3611	Grant	18Apr1946	7May1946	Lechlen C. Sheard et ux	Roy C. LeRose	1 1/2	\$4000.00	as in No. 3526.
3612	P.D.M.	7May1946	7May1946	Core P. Maltby (Trustee)	Elmer Guest	0.117	\$1.00	P.D. dis. 3590. as in 3510
3613	Grant	30Apr1946	7May1946	Elmer Guest et ux	Lawrence A. Knight	0.117	\$175.00	pt. Comm. 776.45' N 29° 30' E from S angle of half lot. Thence E 89° 27' N 128.1' Thence N 66° 21' E 169.6' Thence S 39° 33' W 111.2' to Comm.
3632	Grant	18Jun1946	29Jun1946	Roy C. LeRose	W. E. Harris & Josephine M. Harris as joint tenants		\$2,000.00	Part. Comm. at pt. 10' E & N of NW angle of said lot 11 Thence S 80° 15' E 75' X 513' to p. of b. together with subject to Rts. of way.
3633	Grant	20Jun1946	23Jul1946	Roy C. LeRose	John Lipiec & Rose Lipiec as joint tenants		\$2,000.00	Part. Comm. at pt. in S limit 64'4" E of NW angle Thence S 100'7" X N 66° 21' X W 73' to p. of c. Together with Rts. of way.
3634	Grant	24Jun1946	23Jul1946	John & Rose Lipiec	Roy C. LeRose		\$2000.00	pt. as in No. 3640.
3635	Grant	25Jun1946	23Jul1946	Elmer Guest et ux	John Brown, John E. Britton as joint tenants	0.141	\$1,000.00	at pt. Comm. at pt. located as follows: Comm. at S angle Thence N 39° 23' E 208' Thence S 88° 21' E 177' X N 89° 33' E 88.54' X 100' to p. of c. Thence Cont. S 66° 31' E 137.57' X S 1° 39' E 32' to p. of c. Together with Rts. of way.
3636	Grant	25Jun1946	23Jul1946	Core P. Maltby (Trustee)	Betty McMurphy (Trustee)		\$1,000.00	Assting No. 3630.
3637	Grant	25Jun1946	23Jul1946	Elmer Guest et ux	Marion & Howard A. Martin		\$1,000.00	Assting No. 3630.
3638	Grant	25Jun1946	23Jul1946	Elmer Guest et ux	Elmer Guest		\$1,000.00	Assting No. 3630.



Survey H-20-A; Dominion of Canada. Dept. of Munitions & Supply, Wartime Housing Ltd. (1942) [Peel Registry Office]

SHEET NO 4
LOT NO 11
CON NO 7
RANGE NO

7.4 - 44

TOWNSHIP OF TORONTO

SHEET NO 4
LOT NO 11
CON NO 7
RANGE NO

NUMBER	INSTRUMENT	DATE OF INSTRUMENT	DATE OF REGISTRATION	GRANTOR	GRANTEE	AREA	CO-OPERATION	REMARKS
4219	1881	29 Sep 1881	29 Sep 1881	Ivy Wolstenholme	William W. Williston & Noble W. Williston on joint account	1.25 acres		12/1/49
4235	D.M.	19 Oct 1881	18 Oct 1881	Charles H. Sawyer in Trust	Shirley W. McKendrick & Lorne McKendrick	1.25 acres		7/1/62
4236		20 Sep 1881	18 Oct 1881	Shirley W. McKendrick & Lorne McKendrick	Charles H. Sawyer in Trust	1.25 acres		7/1/62
4264	Grant	Nov 1881	8 Dec 1881	Paul S. Ironstone et ux & Daniel Lipman et ux	Wilfred J. Abel & William L. Harris as Abel's Trust Store, Melton	1.25 acres		12/1/62
4265	Grant	7 Dec 1881	8 Dec 1881	William L. Harris & Josephine H. Harris	Wilfred J. Abel & William L. Harris as Abel's Trust Store, Melton	1.25 acres		12/1/62
4280		7 Dec 1881	14 Dec 1881	Kiser Guest et ux	Nathaniel Thomas (Trustee)	1.25 acres		12/1/62
436	Plan	25 Jan 1882	5 Feb 1882	Central Mortgage & Investment Corp. Ltd.		1.25 acres		12/1/62
67503	Grant	14 Nov 1882	18 May 1882	Paul S. Ironstone et ux	Verne L. Etheridge	1.25 acres		12/1/62
67547	D.M.	5 May 1882	12 May 1882	W. Elgin Lewis	William Ball	1.25 acres		12/1/62
67561	D.M.	2 Jun 1882	20 Jun 1882	Paul S. Ironstone et ux	Veronica J. Barker & Paul S. Ironstone	1.25 acres		12/1/62
68262	Grant	27 May 1882	20 Jun 1882	Veronica J. Barker et ux	Paul S. Ironstone	1.25 acres		12/1/62
68377	Grant	15 Jun 1882	26 Jun 1882	Dugald Cameron et ux	Dugald Cameron Associated Ltd	1.25 acres		12/1/62
69186	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69187	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69188	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69189	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69190	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69191	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69192	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69193	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69194	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69195	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69196	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69197	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69198	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69199	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62
69200	Grant	1 Aug 1882	12 Aug 1882	Isaac A. Martin & Marion Martin	Lorne M. McKendrick & Shirley E. McKendrick as joint tenants	1.25 acres		12/1/62



Plan 436, Malton War-time Housing Subdivision, Central Housing Mortgage & Housing Co. (1952)

DATE PLAN REGISTERED: February 5, 1988.

OWNERS: Central Mortgage & Home L.R. Corp.

TRANSFER OF INTEREST

SHEET NO. 1

LOT NO. 111

STREET: R.F.L. Ave.

PLAN NO. 430

LOTS SUBDIVIDED: Plan 216 & Pt. 11, Con. 7 & 8

NUMBER	INSTRUMENT	DATE OF INSTRUMENT	DATE OF REGISTRATION	GRANTOR	GRANTEE	CONSIDERATION	REMARKS
74437	NOTICE	14 MAY 53	1 JUNE 53	RE: ZONING REG'S. MALTON AIRPORT			
91081	BY LAW	1 JUNE/54	9 JUNE/54	RE SUBDIV. CON.			
91081	NOTICE	20 SEP. 1955	20 SEP. 1955	TO AMEND NO. 74437			
19421	Grant	13 Sep. 50	9 Oct. 50	Michael Dobreen & Stella Dobreen, as part tenants	Michael Dobreen & Stella Dobreen, as part tenants	\$1,500.00 All.	
94-22	MORTGAGE	11 Sep. 50	9 Oct. 50	Michael Dobreen & Stella Dobreen	Stella Dobreen	\$1,500.00 All.	22/10/79
120053	NOTICE	3 JUNE 1959	15 JUNE 1959	RE-ZONING REGS MALTON AIRPORT			
144543	MORTGAGE	22 Sep. 74	26 Sep. 74	Oliver M. Hawthorn	Michael & Stella Dobreen	discharging No. 99422	
144544	MORTGAGE	15 Sep. 74	26 Sep. 74	Michael Dobreen & Stella Dobreen	Oliver M. Hawthorn	1,700.00 All.	22/10/79
144298	NOTICE	21 DEC 1961	13 MAR 1962	RE-ZONING REGS MALTON AIRPORT			
233651	D. M.	5 Oct. 1966	19 Oct. 1966	Oliver M. Hawthorn	Michael Dobreen and Stella Dobreen	discharging No. 144544	22/10/79
369281	Grant	11 Sep. 74	9 Oct. 74	Michael Dobreen & Stella Dobreen	Michael Dobreen & Stella Dobreen	\$1,500.00 All.	
448194	Notice	30 Nov 1972	12 Feb 1973	Amendment of Airport Zoning Regulations			FEB 19 1981
584964	Grant	14 07 81		MELNYK, Antoni	DOBREEN, Stella	\$2.00 All.	
584965	Grant	14 07 81		MELNYK, Laronia	DOBREEN, Stella	\$2.00 All.	
585283	MORT	16 07 81		DOBREEN, Michael as party of Third Part	DOBREEN, Danny M.	\$2.00 All.	
852979	Grant	05 07 88		DOBREEN, Danny Michael	KING, Robert Gerald	All	
					KING, Linda		
					as jr.		
852980	Mort	05 07 88		KING, Linda	GUARANTY Trust Co. of Canada	\$102,000.00 All	
Assigned by RD 1069520				KING, Robert Gerald			

Discharged by 40075901, King, Land Reg. 29/09/26

OWNERS Central Mortgage & Housing Corp.

7.4 - 47

LOTS SUBDIVIDED L. Plan 310 & Pt. 11, Con. 7 & 8.

TOWNSHIP OF WATTO

LOT NO 111

STREET ADDRESS AVE.

PLAN NO. 466

NUMBER	INSTRUMENT	DATE OF INSTRUMENT	DATE OF REGISTRATION	GRANTOR	GRANTEE	CONSIDERATION	REMARKS
85-081	Mort		05-07-88	KING, Linda KING, Robert Gerald	BENEFICIAL Realty Ltd.	\$16,200.00	011 DISCHARGED BY RO 1067420 ASST. CLERK 94-6-8
RO 1059520	Assignment		94-03-01	Central Guaranty Trust Company	Scotia Mortgage Corporation		Re: No. 852980
RO 1061233	Transfer		94-03-25	KING, Robert Gerald KING, Linda	NANLAL, David Anthony NANLAL, Sylvia as JT	125,000.00	ALL. Discharged by RO 1059520 Linda King Reg. 08/04/98
RO 1061234	Charge		94-03-25	NANLAL, David Anthony NANLAL, Sylvia	Canada Trustco Mortgage Company	107,100.00	ALL.

NOTICE
All Document/Instruments
subsequent to

JAN 21 1997

are recorded in the automated abstract
Index set out in subsection 21(5) of the
REGISTRY ACT

LAND
REGISTRY
OFFICE #43

PARCEL REGISTER (ABBREVIATED) FOR PROPERTY IDENTIFIER

13264-0210 (LT)

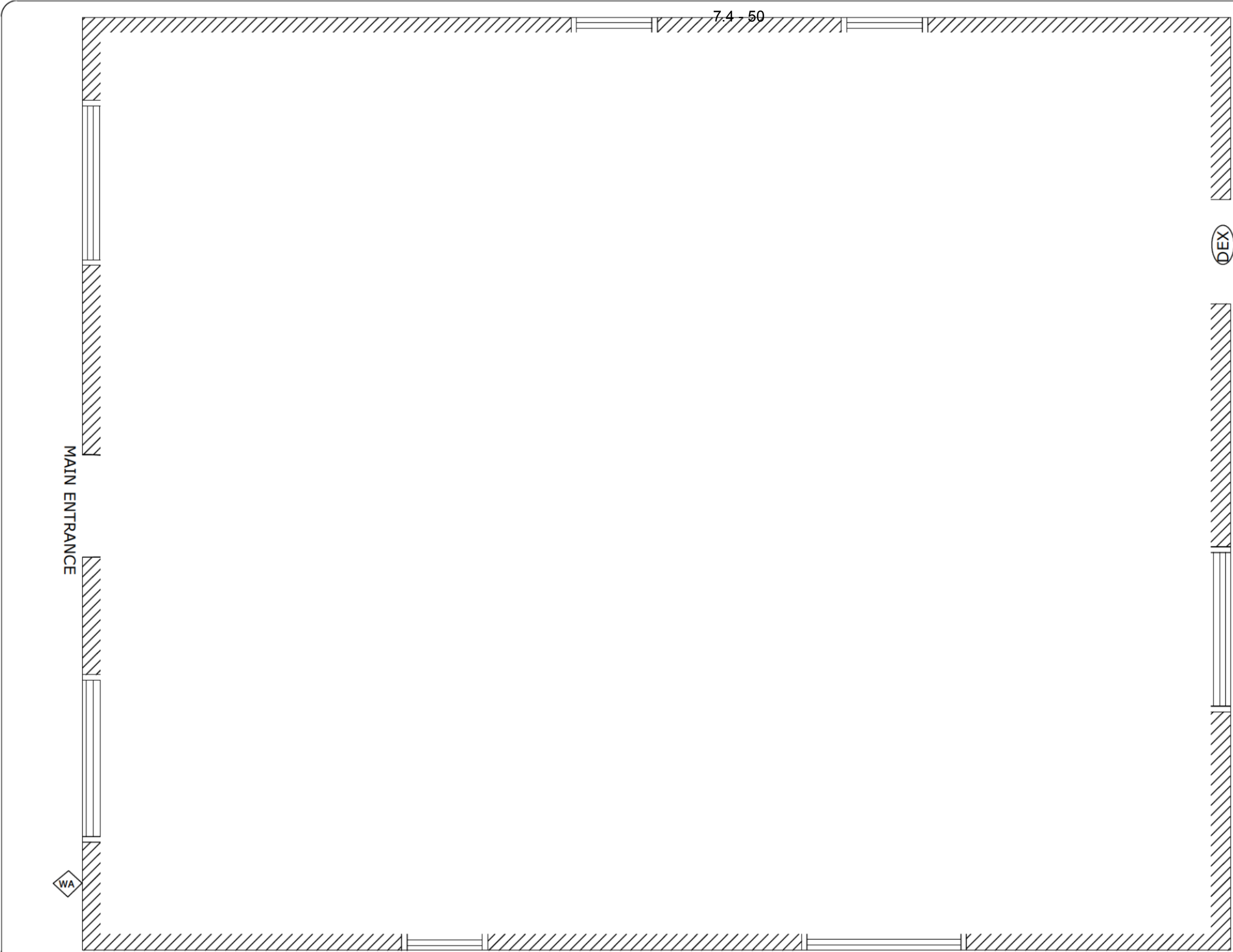
PAGE 2 OF 3
PREPARED FOR CA
ON 2017/05/15 AT 13:54:58

* CERTIFIED IN ACCORDANCE WITH THE LAND TITLES ACT * SUBJECT TO RESERVATIONS IN CROWN GRANT *

REG. NUM.	DATE	INSTRUMENT TYPE	AMOUNT	PARTIES FROM	PARTIES TO	CERT/ CHKD
LT2048671	2000/02/25	TRANSFER		*** COMPLETELY DELETED *** NANLAL, DAVID ANTHONY NANLAL, SYLVIA	MCCOMB, GREGORY MARNA	
LT2048672	2000/02/25	CHARGE		*** COMPLETELY DELETED *** MCCOMB, GREGORY MARNA		
LT2057426	2000/03/27	NOTICE		HER MAJESTY THE QUEEN IN RIGHT OF THE DEPARTMENT OF TRANSPORT CANADA	HSBC BANK CANADA	
REMARKS: PEARSON AIRPORT ZONING REGULATION						
LT2082962	2000/06/07	DISCH OF CHARGE		*** COMPLETELY DELETED *** CANADA TRUSTCO MORTGAGE COMPANY		C
REMARKS: RE: RG1061234						
PR411835	2003/03/28	TRANSFER	\$187,000	MCCOMB, GREGORY MARNA		
PR411836	2003/03/28	CHARGE		*** COMPLETELY DELETED *** JHA, RAJEEV RAMSHANKER JHA, SONAL RAJEEV		C
PR418487	2003/04/11	DISCH OF CHARGE		*** COMPLETELY DELETED *** HSBC BANK CANADA	SCOTIA MORTGAGE CORPORATION	
REMARKS: RE: LT2048672						
PR985889	2005/12/19	CHARGE		*** COMPLETELY DELETED *** JHA, RAJEEV RAMSHANKER JHA, SONAL RAJEEV	SCOTIA MORTGAGE CORPORATION	
PR985890	2005/12/19	DISCH OF CHARGE		*** COMPLETELY DELETED *** SCOTIA MORTGAGE CORPORATION		
REMARKS: RE: PR411836						
PR1873625	2010/08/10	CHARGE		*** COMPLETELY DELETED *** JHA, RAJEEV RAMSHANKER JHA, SONAL RAJEEV		
PR1893594	2010/09/20	DISCH OF CHARGE		*** COMPLETELY DELETED *** SCOTIA MORTGAGE CORPORATION	COMPUTERSHARE TRUST COMPANY OF CANADA	
REMARKS: PR985889.						

NOTE: ADJOINING PROPERTIES SHOULD BE INVESTIGATED TO ASCERTAIN DESCRIPTIVE INCONSISTENCIES, IF ANY, WITH DESCRIPTION REPRESENTED FOR THIS PROPERTY.
NOTE: ENSURE THAT YOUR PRINTOUT STATES THE TOTAL NUMBER OF PAGES AND THAT YOU HAVE PICKED THEM ALL UP.

**APPENDIX C: AS-FOUND DRAWINGS OF THE EXISTING HOUSE
AND DRAWINGS OF THE PROPOSED DEVELOPMENT**



DATE PLOTTED MAY/2017	ISSUED FOR HERITAGE REVIEW

No.	Date	Revisions



CONSULTANT :

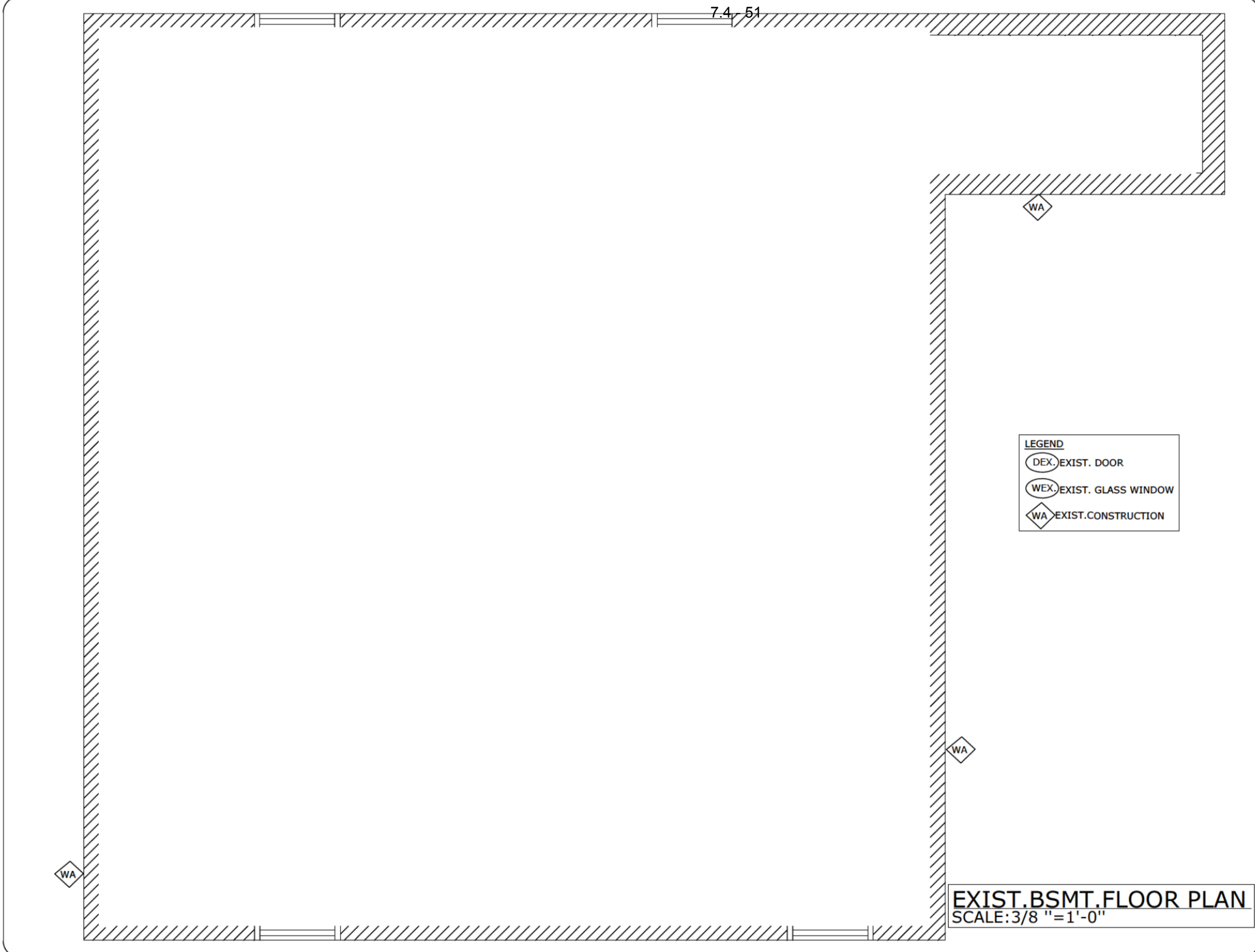
PROJECT :

3098 MERRITT DR.
MISSISSAUGA, ON.

DRAWING TITLE

EXIST.GROUND FLOOR PLAN

DRAWN BY:SK	
DATE :	MAY/2017
SCALE :	3/8"-1'-0"
PROJECT NO. 2017-292	DRAWING NO. A-2(E)



7.4.51

LEGEND

DEX. EXIST. DOOR

WEX. EXIST. GLASS WINDOW

WA. EXIST. CONSTRUCTION

EXIST.BSMT.FLOOR PLAN
SCALE: 3/8" = 1'-0"

DATE PLOTTED MAY/2017	ISSUED FOR HERITAGE REVIEW

No.	Date	Revisions



CONSULTANT :

PROJECT :

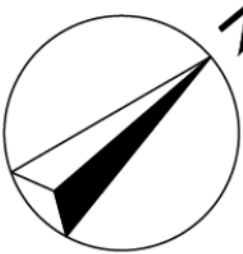
3098 MERRITT DR.
MISSISSAUGA, ON.

DRAWING TITLE	
EXIST.BSMT.FLOOR PLAN	
DRAWN BY: SK	
DATE :	MAY/2017
SCALE :	3/8"-1'-0"
PROJECT NO. 2017-292	DRAWING NO. A-1(E)

DATE PLOTTED MAY/2017	ISSUED FOR HERITAGE REVIEW

No	Date Plotted	Revisions

CHECKED BY : SK



PROJECT :

PROPOSED
TWO STOREY DWELLING
3098 MERRITT AVE.
MISSISSAUGA, ON.

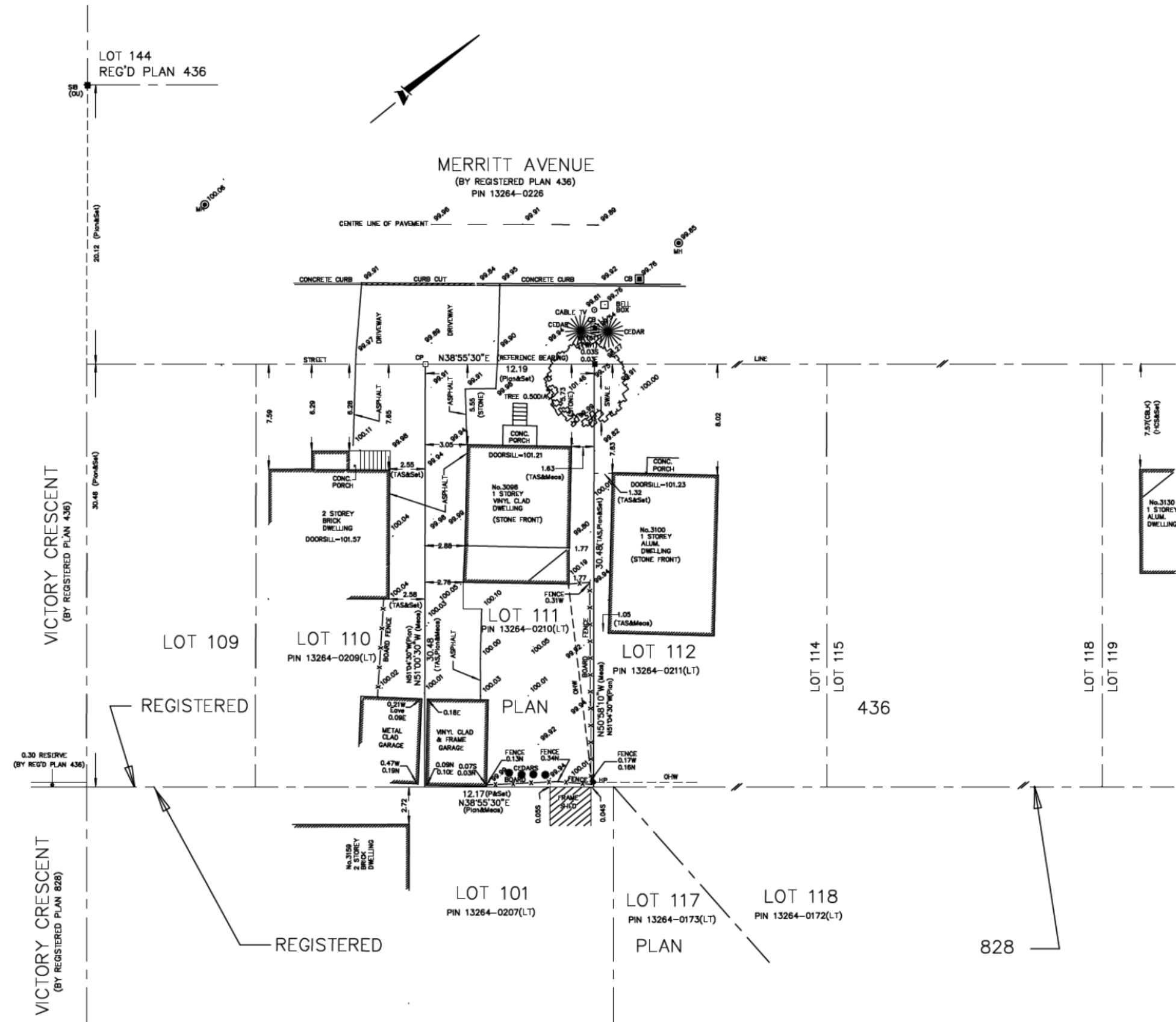
DRAWING TITLE

STREETSCAPE ELEVATIONS

DRAWN BY:	SK
DATE :	MAY/2017
SCALE:	N.T.S
PROJECT NO. 2017-292	DRAWING NO. A-5



MERRITT AVE. STREETSCAPE ELEVATIONS



SURVEYOR'S REAL PROPERTY REPORT
AND TOPOGRAPHIC DETAIL
PART 1) PLAN OF SURVEY OF
LOT 111
REGISTERED PLAN 436
CITY OF MISSISSAUGA
REGIONAL MUNICIPALITY OF PEEB
SCALE 1:200
5m 0m 5m 10m 15metres
TOM A. SENKUS, O.L.S.
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The reproduction, alteration, or use this Report, in whole or in part,
without express permission of Tom A.Senkus, O.L.S., is STRICTLY PROHIBITED.

METRIC
DISTANCES SHOWN ON THIS PLAN ARE IN METRES AND
CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048.

PART 2) PLAN REPORT

EASEMENTS AND/OR RIGHT-OF-WAYS
NO REGISTERED EASEMENTS. PLEASE NOTE OVERHEAD WIRES
ALONG REAR OF SUBJECT LANDS.
MONUMENTATION
TWO FRONT CORNERS ARE MARKED BY AN IRON BAR AND "PK" NAIL
SET IN ASPHALT.
REMARKS
PLEASE NOTE POSITION OF FENCES AS SHOWN ON PLAN.
BOUNDARIES
DISTANCES AS MEASURED ARE IN GENERAL AGREEMENT WITH
DIMENSIONS SHOWN ON REGISTERED PLAN.
COMPLIANCE WITH ZONING BY-LAWS
NO INVESTIGATION WAS MADE REGARDS MUNICIPAL
ZONING BY-LAWS FOR SETBACK REQUIREMENTS.

THIS REPORT WAS PREPARED FOR:
RAJEEV JHA
AND THE UNDERSIGNED ACCEPTS NO RESPONSIBILITY
FOR USE BY OTHER PARTIES.
THIS REPORT REFLECTS CONDITIONS OF TIME OF SURVEY. UPDATING MAY
BE REQUIRED TO ISSUE ADDITIONAL COPIES SUBSEQUENT TO DATE OF
SURVEYOR'S CERTIFICATE.

NOTES AND LEGEND

- DENOTES SURVEY MONUMENT PLANTED
- DENOTES SURVEY MONUMENT FOUND
- SB DENOTES STANDARD IRON BAR
- IB DENOTES IRON BAR
- WT DENOTES WITNESS
- OU DENOTES ORIGIN UNKNOWN
- OHW DENOTES OVERHEAD WIRE
- HP DENOTES HYDRO POLE
- DIA DENOTES DIAMETER
- Plan DENOTES REGISTERED PLAN 436
- P DENOTES REGISTERED PLAN 828
- HCS DENOTES H.C. SEWELL, O.L.S. (OCT. 12, 1951)
- CP DENOTES CONCRETE PIN
- CB DENOTES CATCH BASIN
- MH DENOTES MAN-HOLE
- CBK DENOTES CONCRETE BLOCK FOUNDATION
- 1611 & TAS DENOTES TOM A. SENKUS, O.L.S. (FEB. 2, 2006)

ELEVATION NOTE
ELEVATIONS ARE LOCAL AND REFERRED TO AN ASSUMED ELEVATION
OF 100.00 METRES.

BEARING NOTE
BEARINGS ARE ASTRONOMIC AND ARE REFERRED TO THE SOUTHERLY
LIMIT OF MERRITT AVENUE AS SHOWN ON REGISTERED PLAN BY 436.
HAVING A BEARING OF N38°55'30"E.

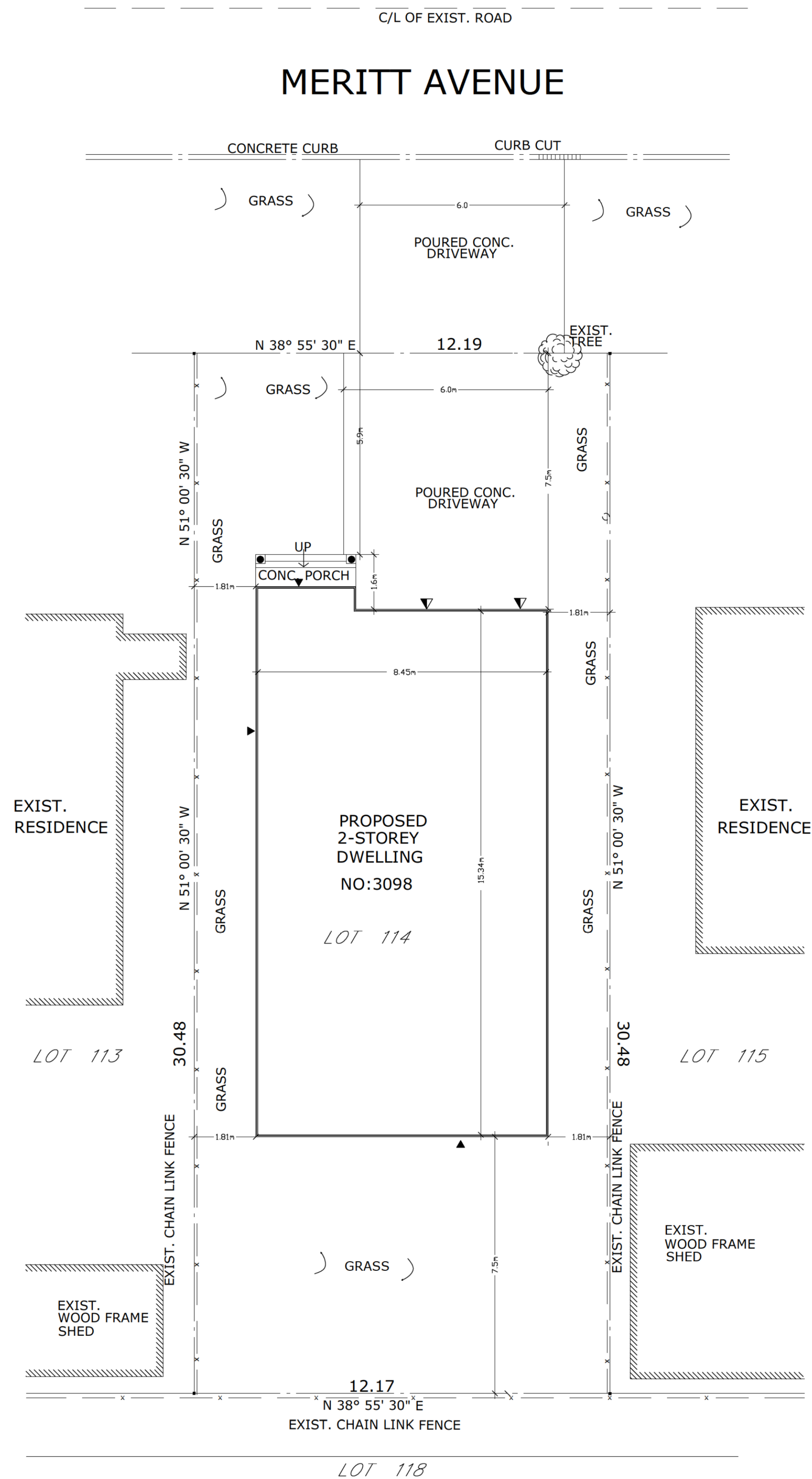
SURVEYOR'S CERTIFICATE

- I CERTIFY THAT:
- THIS SURVEY AND PLAN ARE CORRECT AND IN ACCORDANCE WITH
THE SURVEYS ACT, THE SURVEYORS ACT AND THE LAND TITLES ACT
AND THE REGULATIONS MADE UNDER THEM
 - THE SURVEY WAS COMPLETED ON THE 27th DAY OF APRIL, 2017

DATE: MAY 4, 2017
T. A. SENKUS
ONTARIO LAND SURVEYOR

TOM A. SENKUS
ONTARIO LAND SURVEYOR
40 BURROWS AVENUE
TORONTO (ISLINGTON), ONTARIO
M9B 4W7
PHONE: (416) 237-1893
E-MAIL: tomsenkus@rogers.com
mapcad@rogers.com

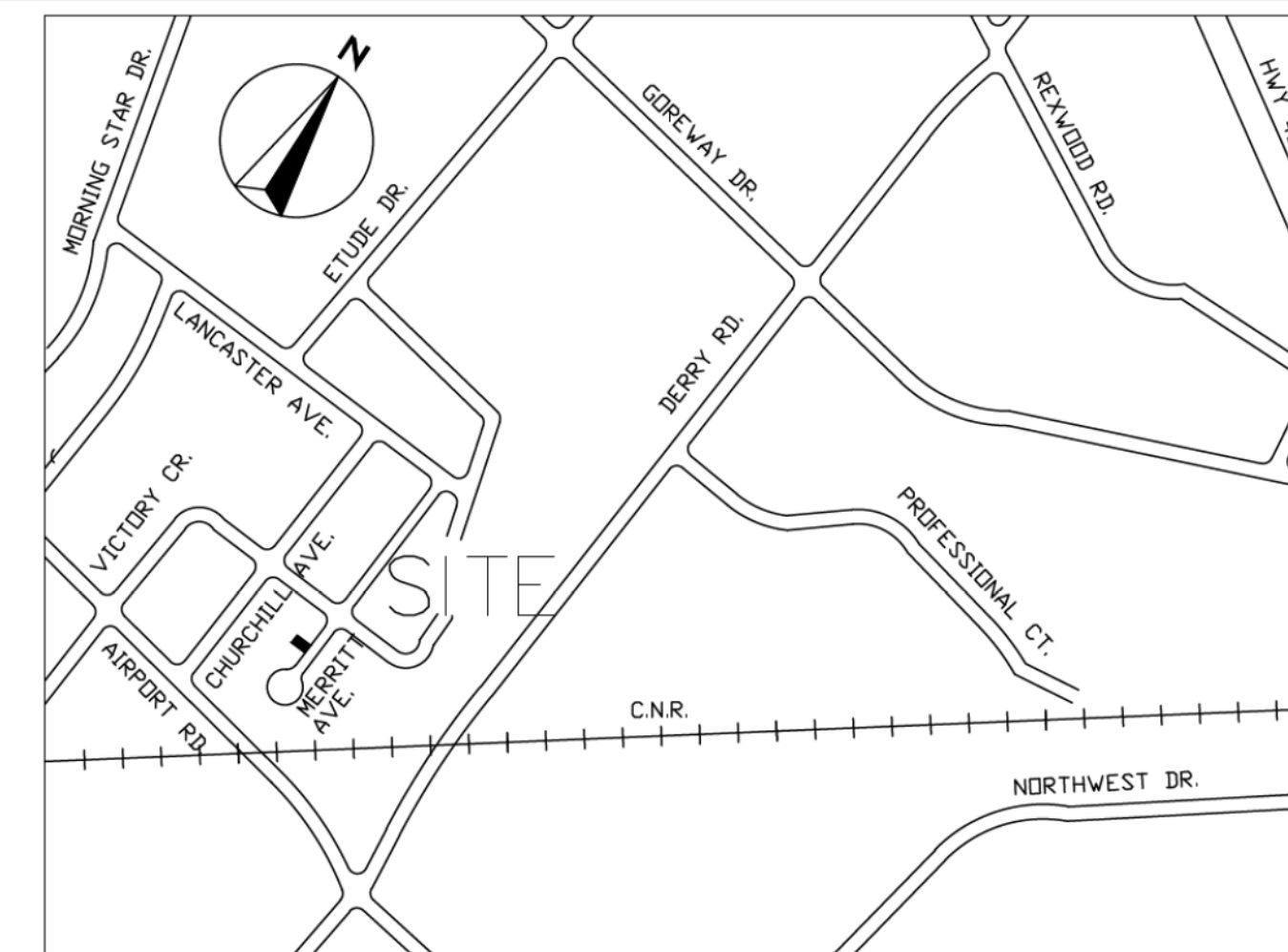
FILE: 06-01
CAD FILE: MERRITT-SRPR-TOPOT



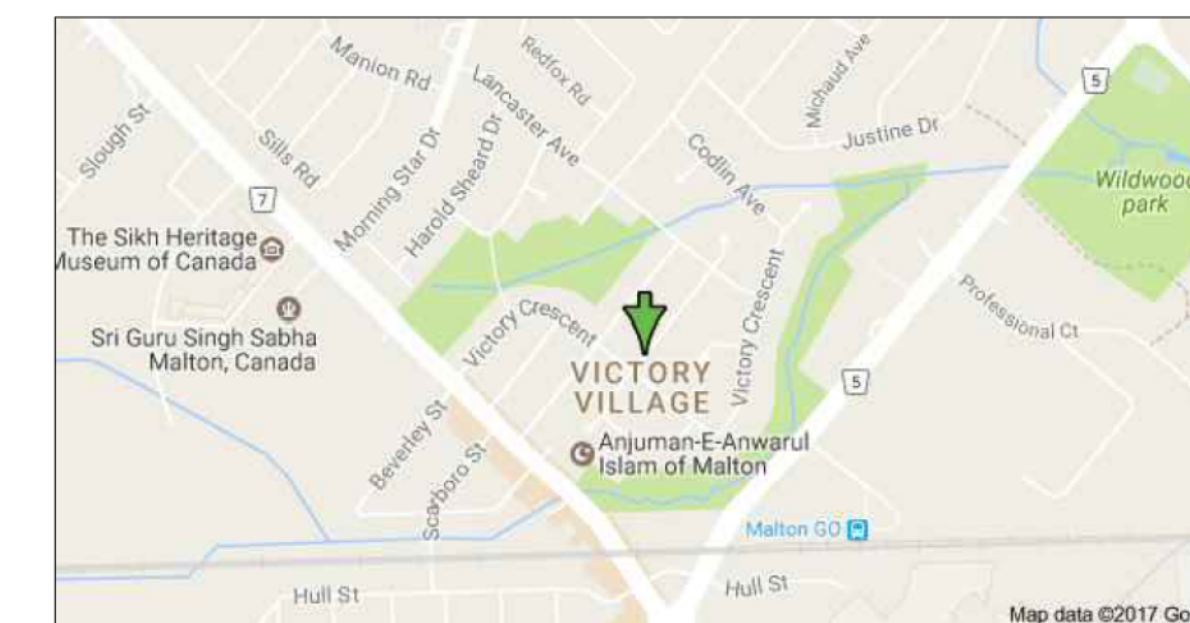
LEGAL DESCRIPTION

SURVEY INFORMATION SHOWN ON
THIS PLAN TAKEN FROM
PLAN OF BUILDING SURVEY OF
LOT 111, REGISTERED PLAN 436
CITY OF MISSISSAUGA
REGIONAL MUNICIPALITY OF PEEL

BENCH MARK
ELEVATIONS SHOWN HEREON ARE
CANADIAN GEODETIC DATUM, 1928
(NOT 1978 SOUTHERN ONTARIO READJUSTMENT)
AND ARE RELATED TO CITY OF MISSISSAUGA BENCH MARK #44
HAVING AN ELEVATION OF 169.343 METRES



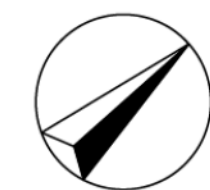
KEY PLAN



LOCATION PLAN(N.T.S)

SITE SATISITICS:		
LOT AREA	371.60m2	4,000.00ft2
GROUND FLOOR AREA	78.01 m2	839.72ft2
SECOND FLOOR AREA	114.39m2	1231.32 ft2
TOTAL GROSS FLOOR AREA	192.40m2	2,071.04ft2
ALLOWABLE LOT COVERAGE	30%	1,200.00 ft2/(111.50m2)
PROPOSED LOT COVERAGE	34.83%	
BASEMENT AREA	76.82m2	826.91 ft2

DATE PLOTTED MAY/2014	ISSUED FOR HERITAGE REVIEW



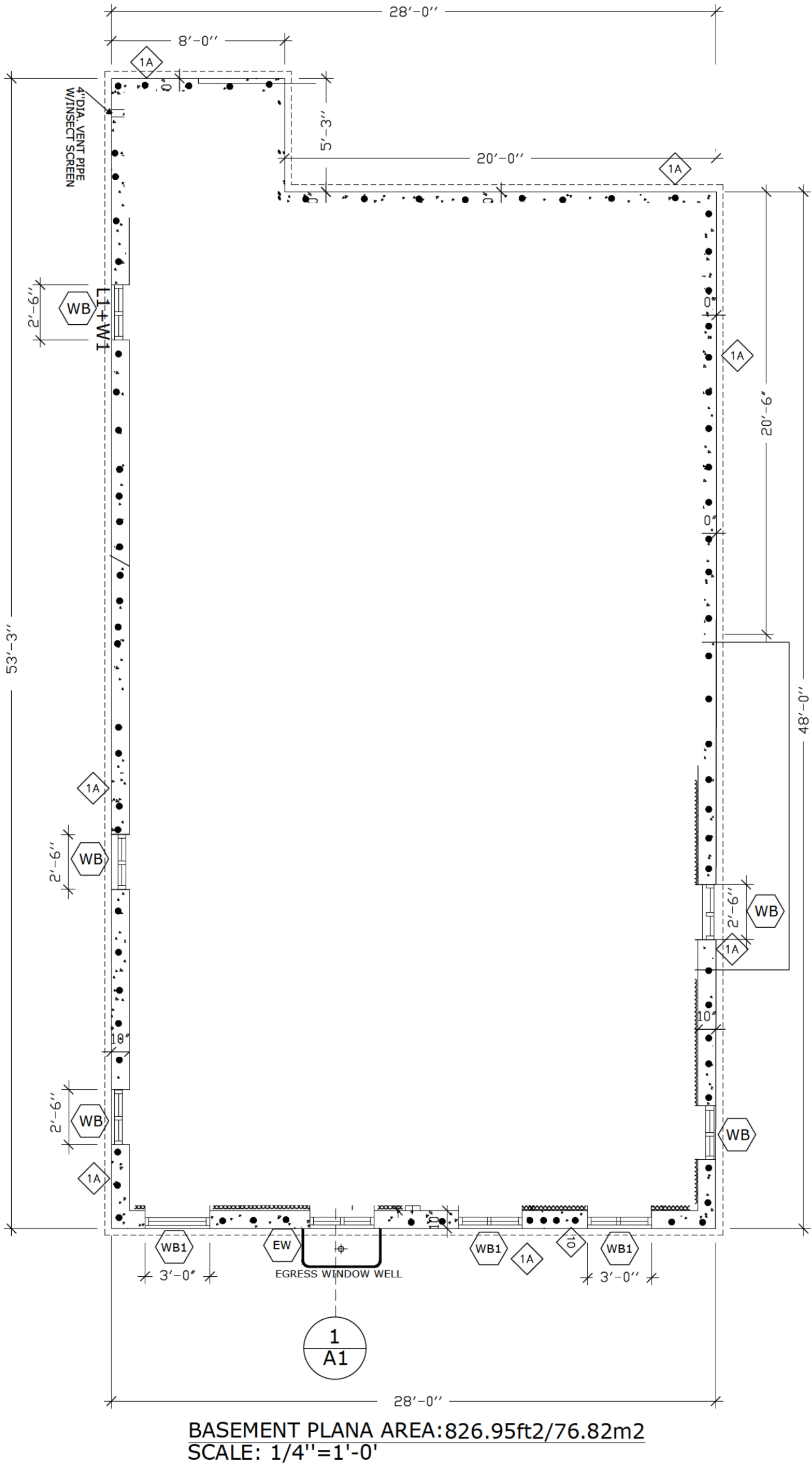
PROJECT :

PROPOSED
TWO STOREY DWELLING
3098 MERITT AVENUE
MISSISSAUGA, ON.

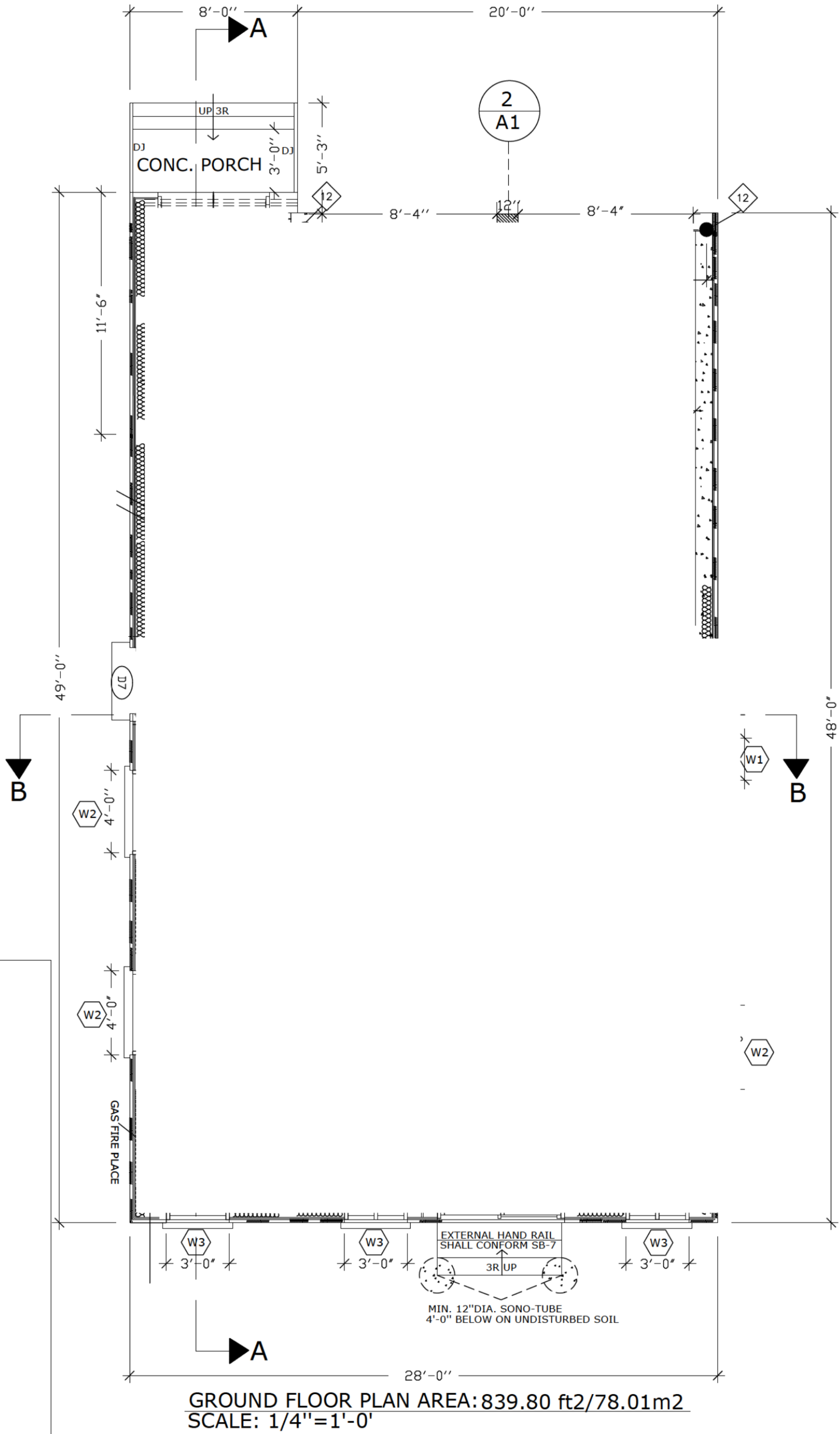
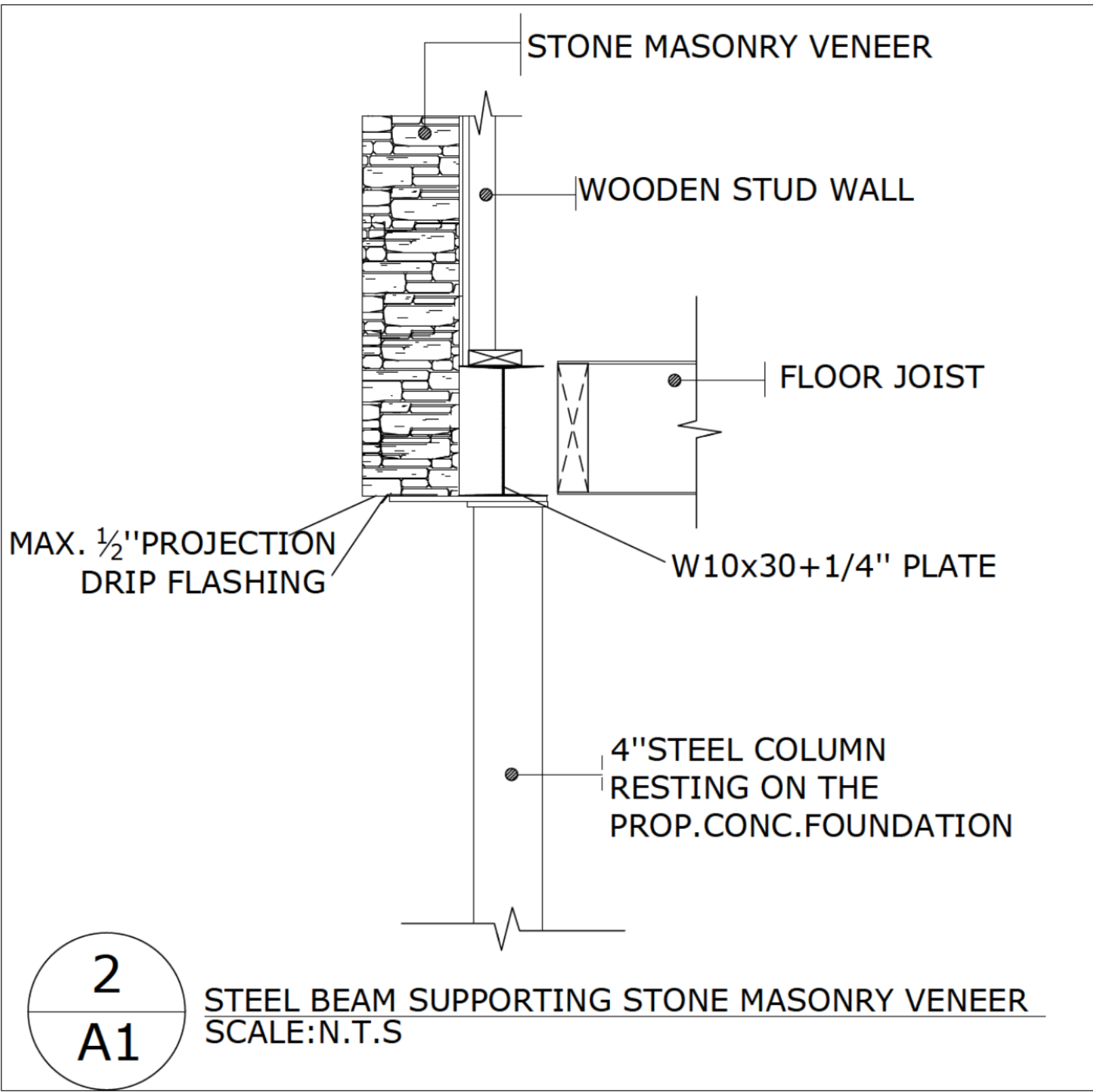
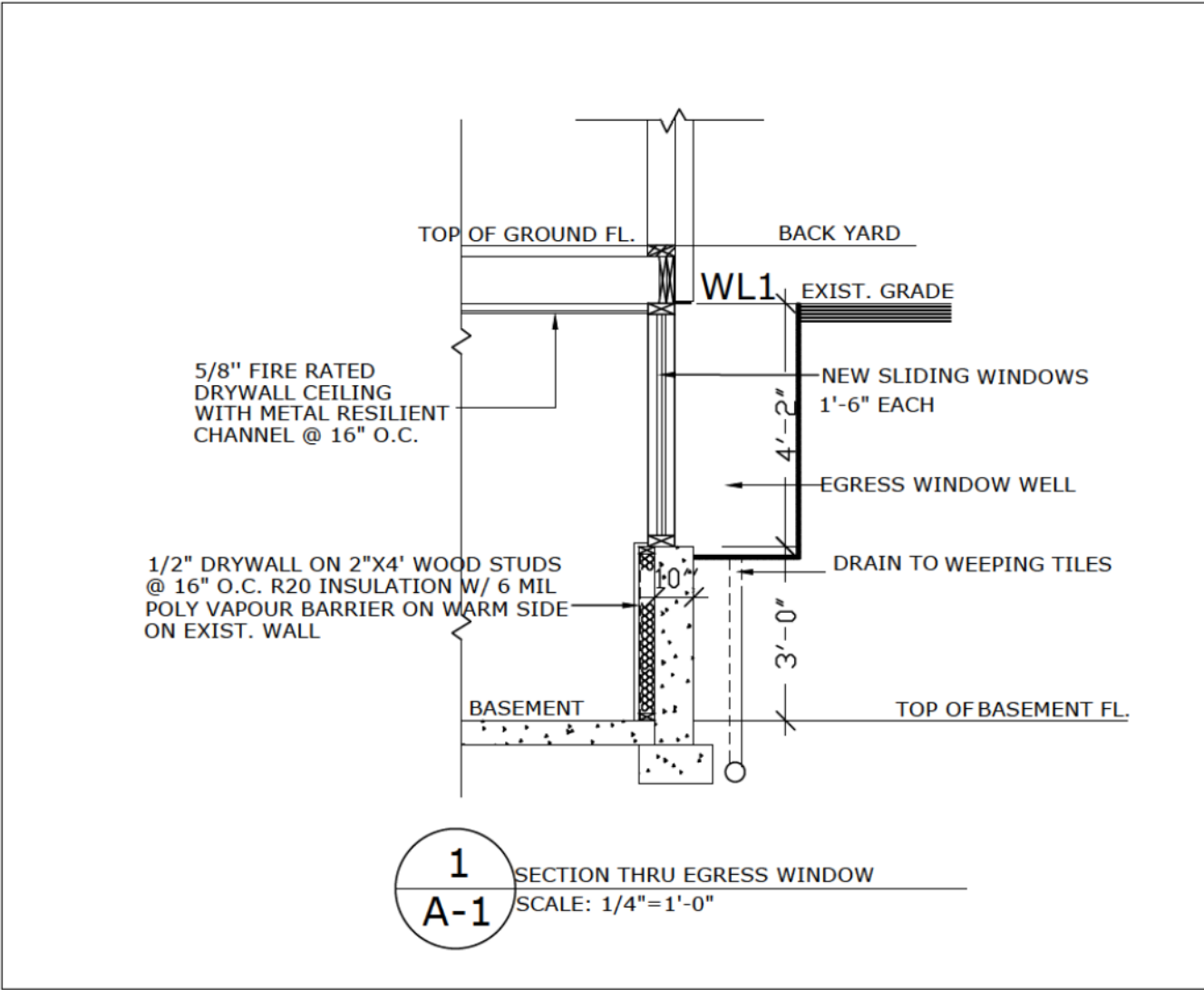
DRAWING TITLE

SITE PLAN

DRAWN BY :SK	APPROVED BY: SK
DATE:	MAY/2017
SCALE:	1:75
PROJECT NO. 2017-290	DRAWING NO. SP-1



- 1A TYP. 24"x8", 25MPa POURED CONCRETE FOOTING REINFORCED WITH 3-15M CONTINUOUS BAR ON UNDISTURBED SOIL w/MIN. 10M@300mm VERTICAL REINFORCEMENT
- 1B BASEMENT FLOOR MIN. COMPRESSIVE STRENGTH 20MPa
- 1C 5/8" FIRE RATED GYPSUM BOARD CONTINUOUS CEILING WITH METAL RESILIENT CHANNEL @ 16" O.C. SHALL CONFORM SB-3(F8c) 30MIN F.R.R.
- 1D 4'-0"x4'-0"x18" CONC. FOOTING MIN. COMPRESSIVE STRENGTH 15MPa W/5-15M BARS E-WAY (PROVIDE 3" CLEAR COVER)
- 1E TYP. 4"DIA. STEEL COLUMN 8"x8"x1/2" TOP PLATE 8"x8"x1/2" BOTTOM PLATE



- X1 STUD PARTITION: 1/2" DRYWALL ON BOTH SIDES OF 2"x4" WOOD STUDS @ 16" O.C. TO U/S OF EXIST. JOISTS W/ SOUND ATTENUATION INSULATION
- X2 INTERIOR STUD PARTITION: 1/2" DRYWALL ON BOTH SIDES OF 2"x4" WOOD STUDS @ 16" O.C.

NOTES:
PROVIDE THE SOIL REPORT VERIFICATION BY GEOTECHNICAL ENGINEER AT THE TIME OF EXCAVATION AND BEFORE CONSTRUCTION OF FOOTING & FOUNDATION.

DATE PLOTTED	ISSUED FOR
MAY/2017	HERITAGE REVIEW

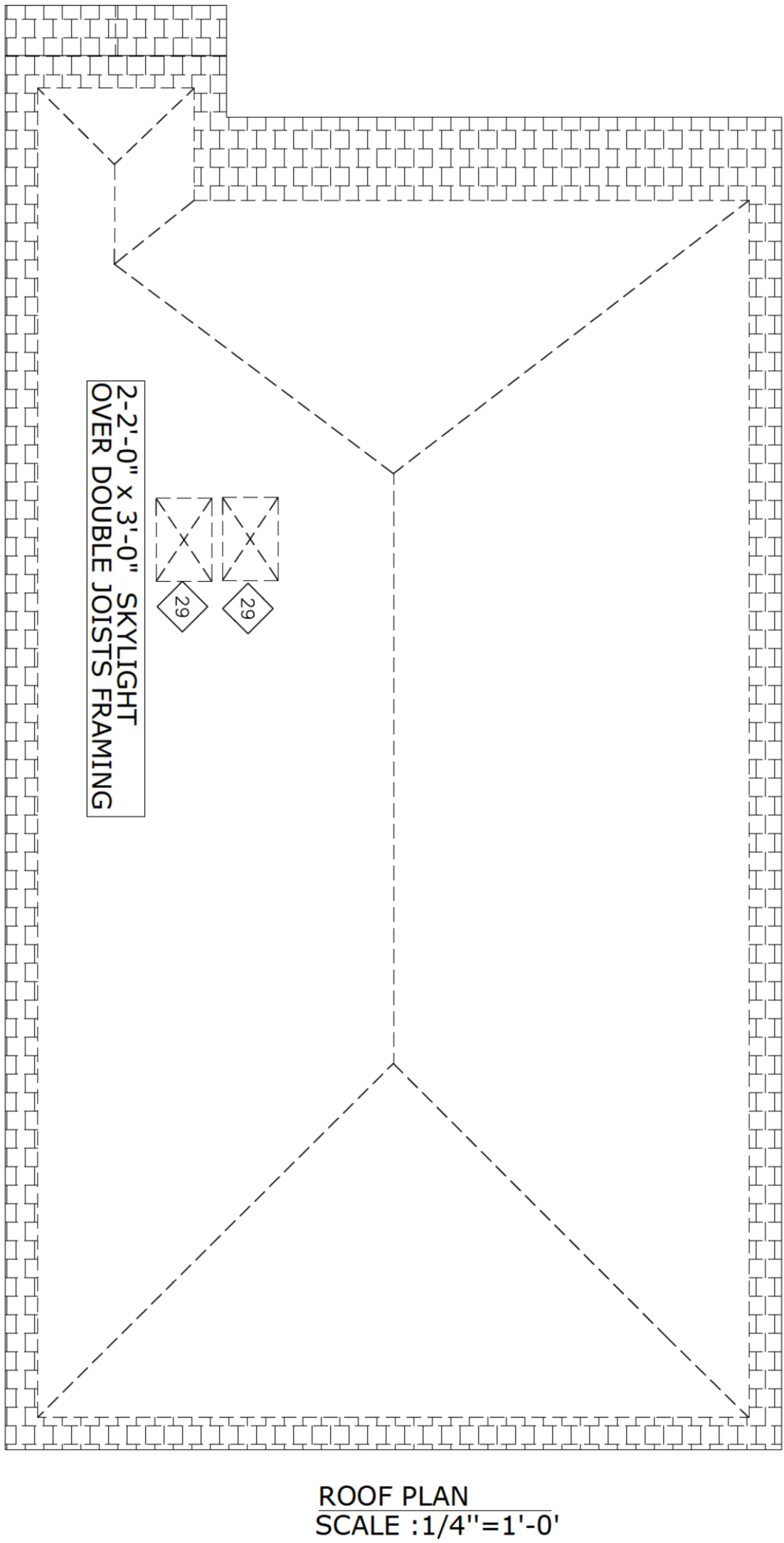
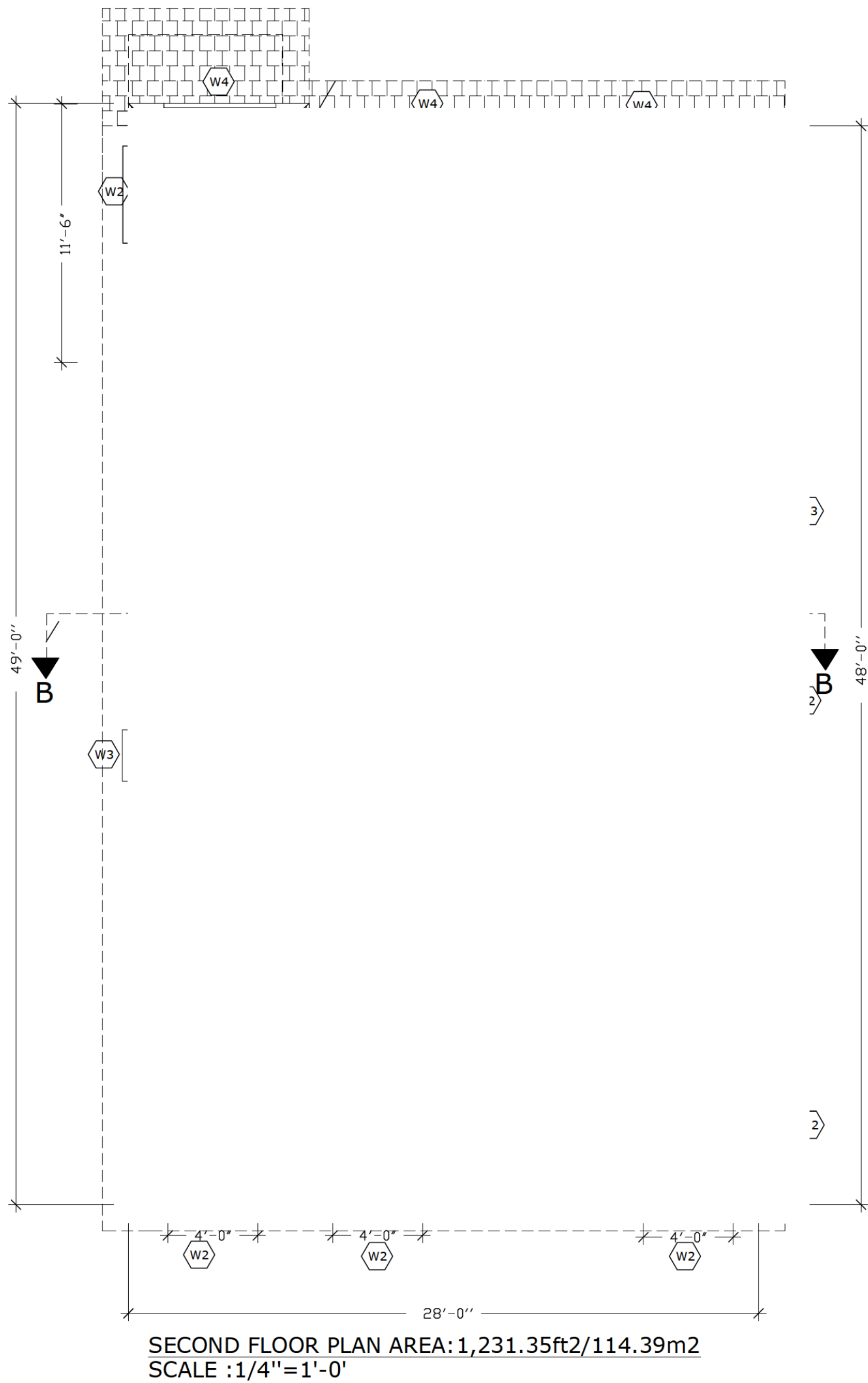
No.	Date of Revision	Revisions
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PROJECT :

PROPOSED TWO STOREY SINGLE FAMILY DWELLING
3098 MERITT AVE.
MISSISSAUGA, ON.

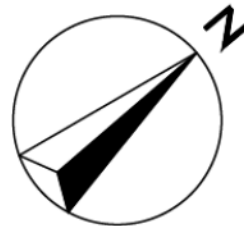
DRAWING TITLE
BASEMENT PLAN & GROUND FLOOR

DRAWN BY :	SK
DATE :	MAY/2017
SCALE :	1/4"=1'-0'
PROJECT NO. 2017-290	DRAWING NO. A-1



DATE PLOTTED	ISSUED FOR
MAY/2017	HERITAGE REVIEW

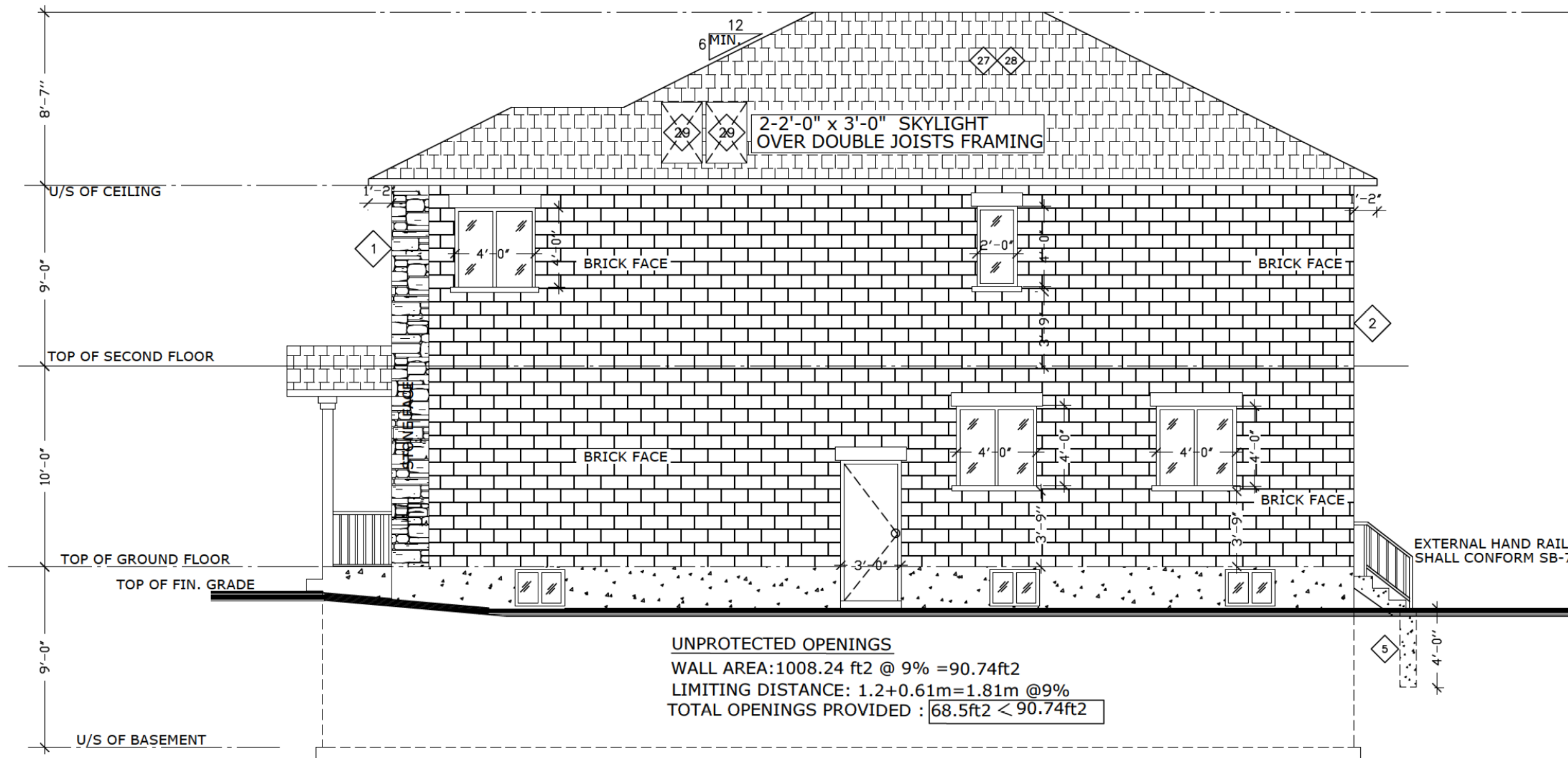
No.	Date of Revision	Revisions



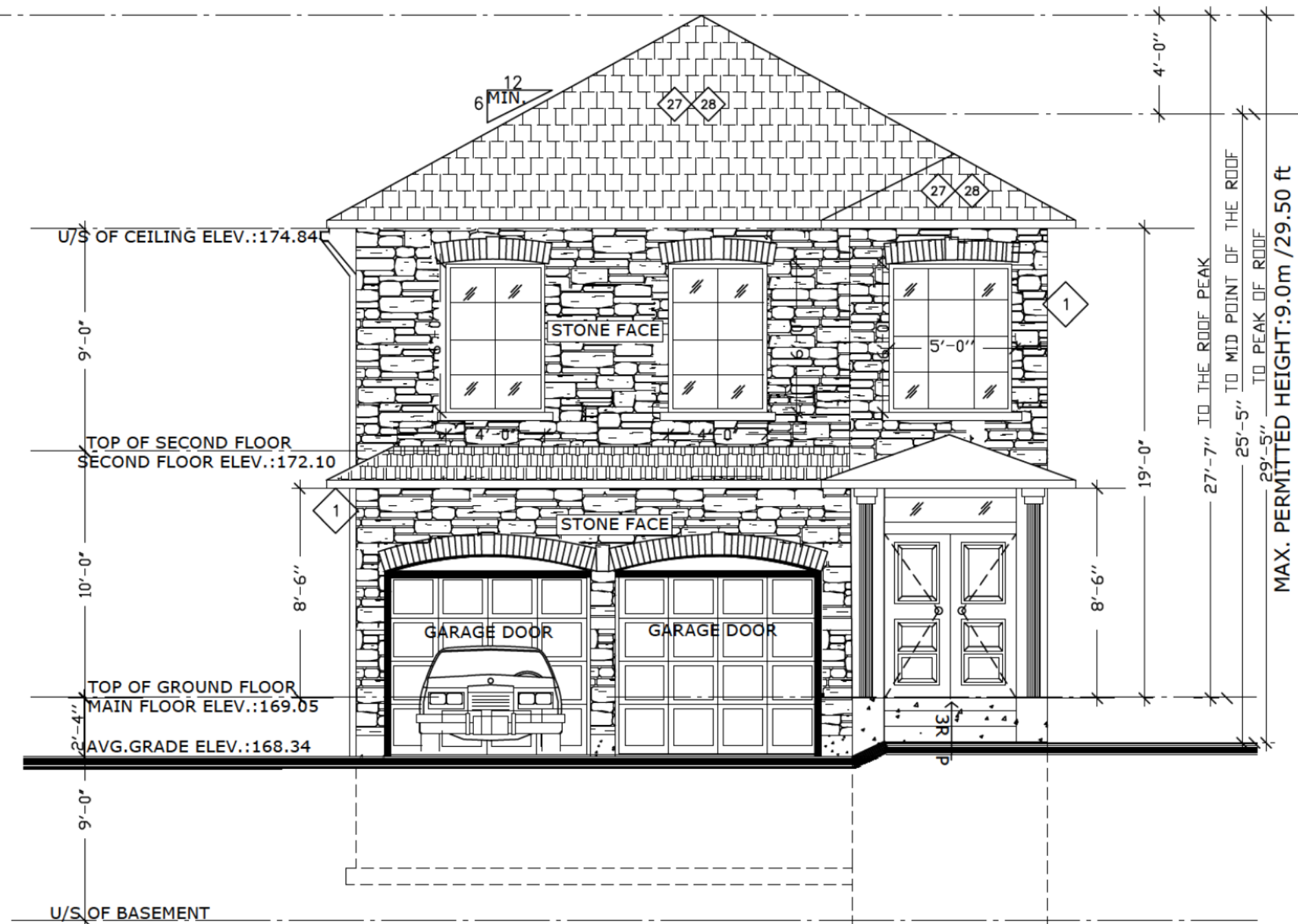
PROJECT :

PROPOSED
TWO STOREY
SINGLE FAMILY DWELLING
3098 MERITT AVE.
MISSISSAUGA, ON.

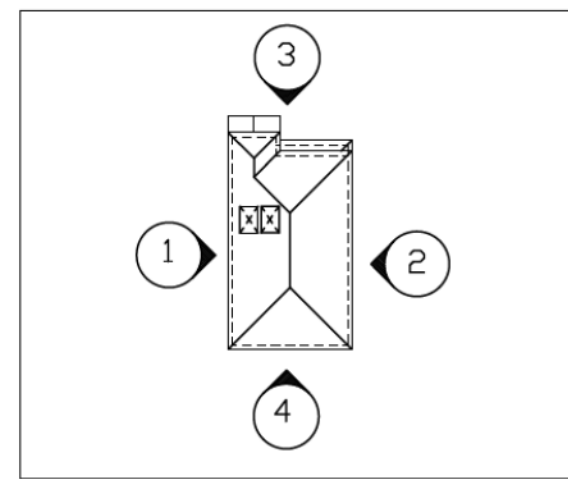
DRAWING TITLE	
ROOF PLAN SECOND FLOOR PLAN	
DRAWN BY :	SK
DATE :	MAY/2017
SCALE :	1/4"=1'-0'
PROJECT NO. 2017-290	DRAWING NO. A-2



1 WEST ELEVATION
SCALE: 3/16" = 1'-0"

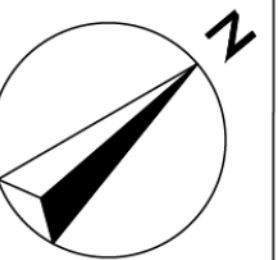


3 NORTH ELEVATION
SCALE: 3/16" = 1'-0"



DATE PLOTTED MAY/2017	ISSUED FOR HERITAGE REVIEW
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No.	Date of Revision	Revisions
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PROJECT :

PROPOSED
TWO STOREY
SINGLE FAMILY DWELLING
3098 MERITT AVE.
MISSISSAUGA, ON.

DRAWING TITLE

ELEVATIONS

DRAWN BY :	SK
DATE :	MAY/2017
SCALE :	3/16"-1'-0"
PROJECT NO. 2017-290	DRAWING NO. A-3

City of Mississauga

Corporate Report



Date: 2017/06/14

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
Community Services

Originator's files:

Meeting date:
2017/07/11

Subject

Request to Demolish a Structure on a Heritage Listed Property: 1695 Dundas Street West, Erindale Park (Ward 6)

Recommendation

That the temporary washroom at 1695 Dundas Street West, Erindale Park, which is listed on the City's Heritage Register, is not worthy of heritage designation, and consequently, that the owner's request to demolish proceed through the applicable process.

Background

Section 27.3 of the Ontario Heritage Act states that structures or buildings on property listed on the City's Heritage Register cannot be removed or demolished without at least 60 days' notice to Council. This legislation allows time for Council to review the property's cultural heritage value to determine if the property merits designation.

The owner of the subject property has submitted a heritage permit application to demolish and replace the washroom structure. The subject property is listed on the City's Heritage Register because it includes the ruins of the Erindale Dam and it forms part of the Erindale Park and Credit River Geological Formations cultural landscapes.

Comments

The owner of the subject property has requested permission to demolish the existing temporary washroom structure at Erindale Park. The applicant has provided a Heritage Impact Statement compiled by Hossack & Associates Architects. It is attached as Appendix 1. The consultant has concluded that the temporary washroom does not merit designation under the Ontario Heritage Act. Staff concurs with this finding.

Financial Impact

There is no financial impact.

Conclusion

The owner of 1695 Dundas Street West, Erindale Park, has requested permission to demolish a structure on a property that is listed on the City's Heritage Register. The applicant has submitted a documentation report which provides information which does not support the structure's merit for designation under the Ontario Heritage Act. Staff concurs with this finding.

Attachments

Appendix 1: Heritage Impact Statement



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: P. Wubbenhorst, Senior Heritage Coordinator

DEMOLITION HERITAGE IMPACT STATEMENT

for

ERINDALE PARK

WASHROOM BUILDING

at

1695 Dundas St W, Mississauga, Mississauga, ON



Prepared for:
The Corporation of the City of Mississauga



Prepared by
Hossack & Associates Architects
June 2017





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1. Site History

Erindale Park has a long history of uses; the first of which was a hydro-electric dam.

“The Erindale Dam is situated within Erindale Park, northeast of the intersection at Mississauga Road and Dundas Street West, in the former Township of Toronto, now the City of Mississauga. Within the park itself, the dam is located near the southwestern portion of the park where the Credit River begins its southern path towards Dundas Street West. The construction of the dam began in 1904 and was completed in 1910. The dam was operational from 1910 – 1923. The dam redirected part of the stream of the Credit River to a power plant where hydroelectric power was produced. After becoming uneconomical to repair and continue operations, the dam was closed in 1923. The portion of the dam within the Credit River was dynamited in 1941. Following use as an unsanctioned recreational area and a sanitary landfill site, the Erindale Park was opened in 1986.” – [Stantec Erindale Village Dam –Heritage Structural Condition and Recommendations Report, April 2016.](#)

“When the hydro plant at Erindale ceased operations in 1923, the property remained open to the public. After the plant closed, the area became a popular spot for recreational use, particularly for swimming and fishing at the Erindale Lake. However, due to many drownings, Toronto Township Council decided to prohibit access to the area in 1954 (Hicks: 2009:178).

The closure coincided with alteration of the river. The river bed was straightened, widened and relocated to its present location closer to the north-west bank valley. The adjustments to the area in 1954 created a large flat area which was first intended to be a park. In 1955, Toronto Council spent \$10,000 to build fences and hire park employees. However, the park was never opened due to legal issues. Council decided in 1959 that 133 acres of the 220 acre property was to be used as an operational sanitary landfill. The site was used as a landfill from 1961 to 1965 (Hicks 2009).

In 1973, David Culham became the councilor of Ward 6 within which Erindale Park was located. Culham wanted the area to become a park, as originally intended. It took more than 13 years for the vision to come to fruition. In January of 1986 the park designed by local designers Johnson, Sustronk and Weinstein, who also designed the Toronto Zoo, and Baker Salmona Hess was opened (The Landplan Collaborative Ltd., et al. 2005). Erindale Park is the largest park in the City of Mississauga, covering nearly 220 acres (Hicks 2009).” – [Stantec Erindale Village Dam –Heritage Structural Condition and Recommendations Report, April 2016.](#)

The current temporary washroom (in a trailer) was installed for use by Park patrons in 1999.

Demolition Heritage Impact Statement for Erindale Park Washroom
1695 Dundas St W, Mississauga, Mississauga, ON

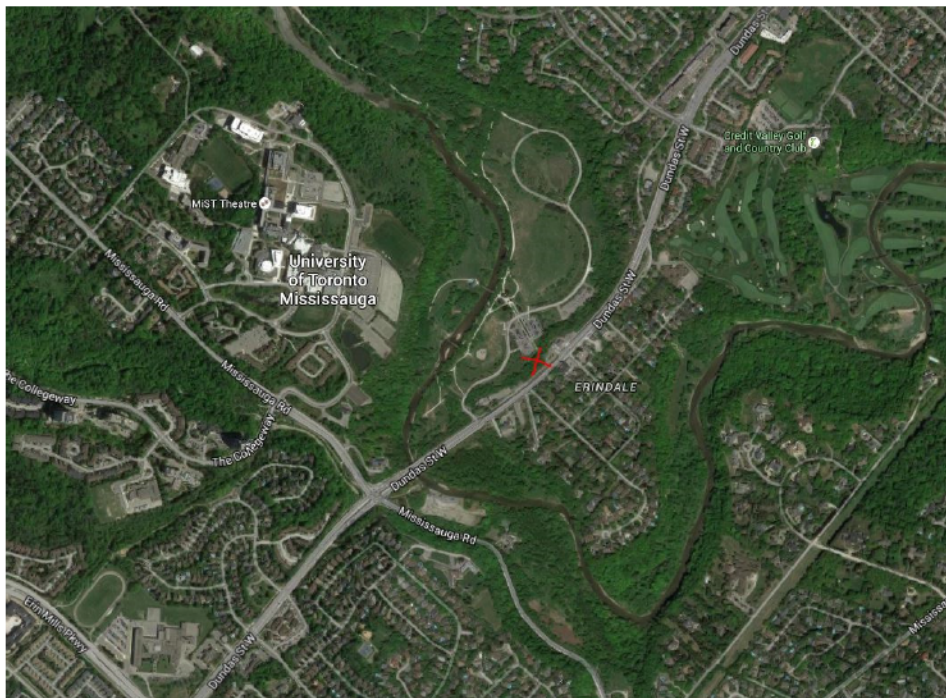
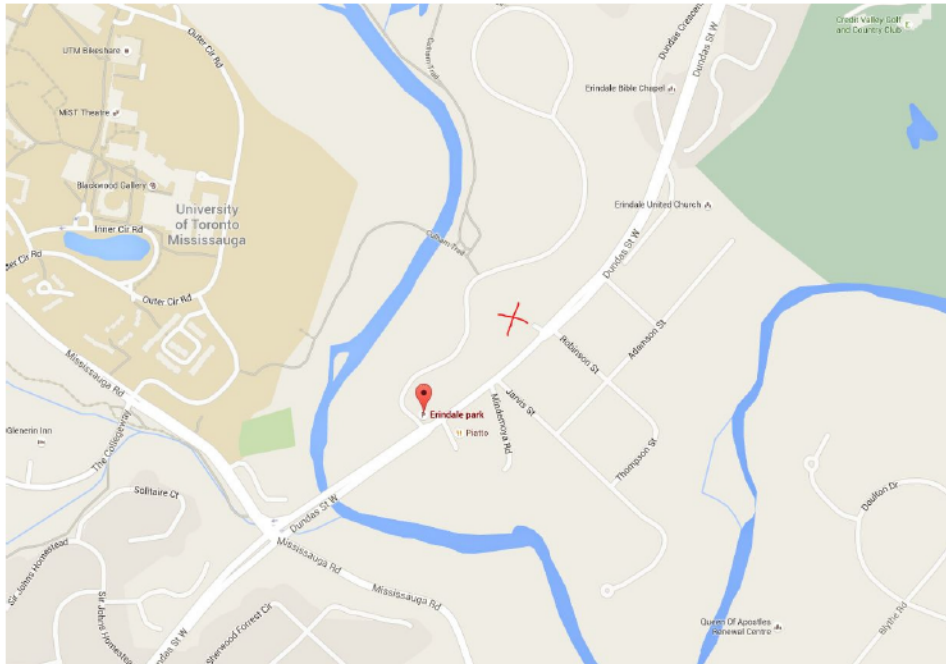
**HOSSACK
& ASSOCIATES
ARCHITECTS**



2. Location

Address: 1695 Dundas St W, Mississauga, Mississauga, ON

Legal Description: Part of Lots 4 & 5, Range 1 South of Dundas Street





3. Land Use

- The site is situated north of Dundas street W, east of Mississauga Rd.
- The proposed building site is adjacent (to the south) an existing parking area, which services the Erindale Park. The parking area provides parking for approx. 163 vehicles, including 3 accessible parking spaces.
- The lots is bounded to the south by a property zoned **C4-7 Commercial** permitted use. [Exception allowance C4-7 zone the permitted uses and applicable regulations shall be as specified for a C4 zone except that the following uses/regulations shall apply: Uses Not Permitted 6.2.5.7.1 \(1\) Restaurant \(2\) Take-out Restaurant.](#)
- The existing waterline is running through an easement referred to as Parts 1 and 2, Plan 43R-23314. The OLS has noted that there is no easement registered on title over Parts 1 and 2, Plan 43R-23314. There is no need for easement over Part 1 as it would be in favour of the CVC/City. However it should be noted that an easement should be registered over Part 2 on the adjacent property.
- The site grading is relatively flat in the area of the parking and existing washroom building to be demolished. The rises drastically in topography to the south (toward Dundas St. W) of the proposed building location, to a grade elevation 13.5m (approx. 44 feet) higher than the proposed building site.
- The site is heavily treed to the south. It should be considered for any proposed service connections for the new development, to be installed by horizontally boring for service connections, as to minimize the disruption to the parkland.
- The existing (and proposed) washroom site(s) are located adjacent the main visitor's parking lot, roughly 315m to the east of the main Park entrance.
- The site is currently Owned by the Credit Valley Conservation and is being leased to the City of Mississauga.



4. Zoning

Zone: **G1-14** Greenbelt Zone By-law: 0325-2008

10.1.1 Accessory Uses in G1 and G2 Zones

10.1.1.1 Trails are permitted accessory to natural heritage features and areas conservation.

10.1.1.2 A passive recreational use is permitted accessory to a stormwater management facility.

10.1.1.3 A fence is permitted accessory to a natural protection area and natural heritage features and areas conservation to prevent encroachment of abutting land uses and to establish property boundaries.

10.1.1.4 Parking areas are permitted accessory to natural heritage features and areas conservation and shall be constructed of a permeable type of material to minimize impacts on the natural environment.

Exception allowance for G1-14 per 10.2.2.14.1 (1) Temporary Tent and/or Stage

In summary, **no buildings** are permitted to be constructed on this site.

A Minor Variance is required to allow a park washroom building to be constructed.

A Minor Variance requesting a washroom building be considered in compliance with an:

OS1 Open Space Zone

9.1.1 Accessory Uses in **OS1** and OS2 Zones

9.1.1.1 Parking areas may be constructed of a permeable type of material.

9.1.1.2 Accessory uses in OS1 and OS2 zones shall include, but not be limited to, washroom/changeroom facility, picnic facility, maintenance/storage building or structure or an office for a permitted use, and shall be permitted accessory to an active or passive recreational use.

OS1 Setback requirements in meters (m)

Setback from property line: **4.5 m min.**

Building height: **No max requirement**

The minor variance for Zoning compliance was applied for and approved through the Committee of Adjustment on March 31, 2016 **A117-16 W6**.



5. Existing Building

The current temporary washroom (in a trailer) was delivered and installed in the Park in June 1999.

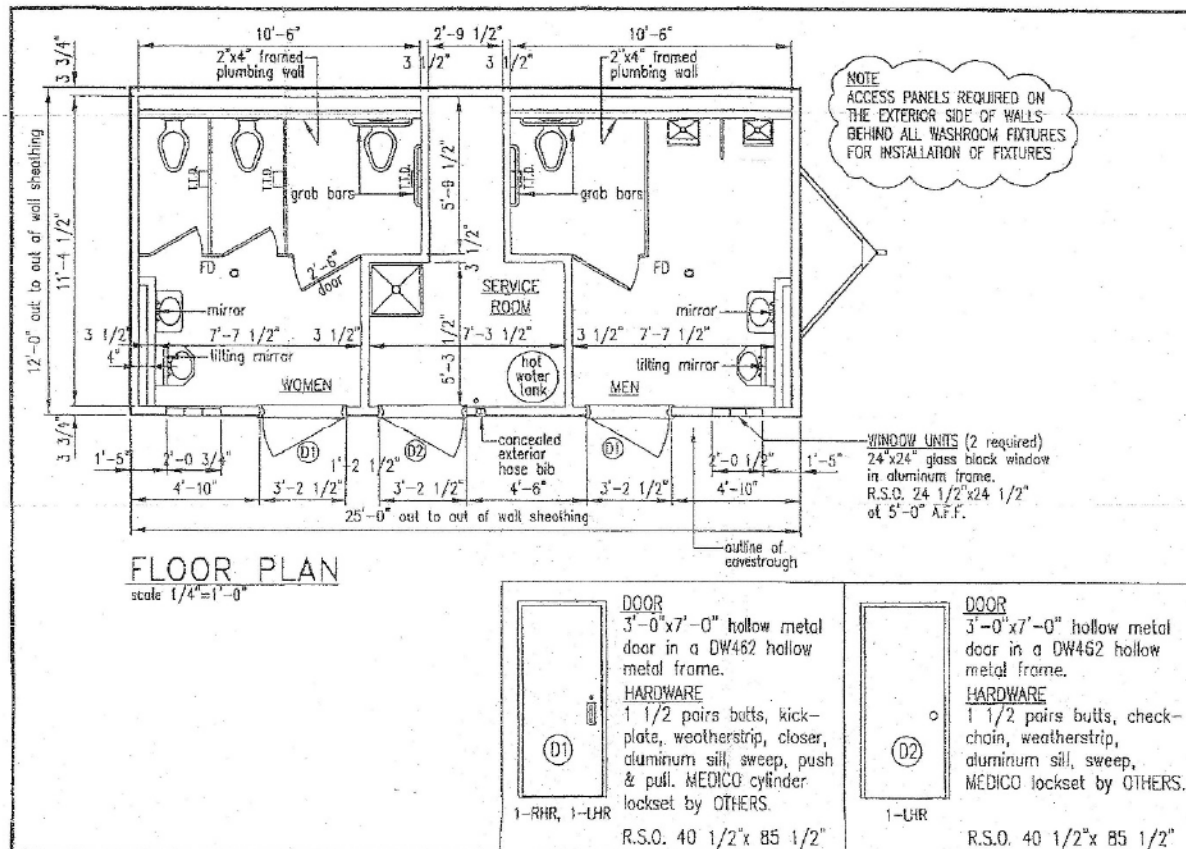
Building permit number **BP 3NEW 99-1619**.

Class 5 septic system permit number **PSS 99-1619**.

The trailer is a basic prefabricated frame with steel siding, wood stud and gypsum board walls and a wood framed and asphalt shingled roof. The trailer has three (3) male toilet fixtures and three (3) female fixtures. The trailer does not (and cannot) accommodate a universal toilet room or the accessible washroom requirements of today's standards. The trailer is **27m²** in area.

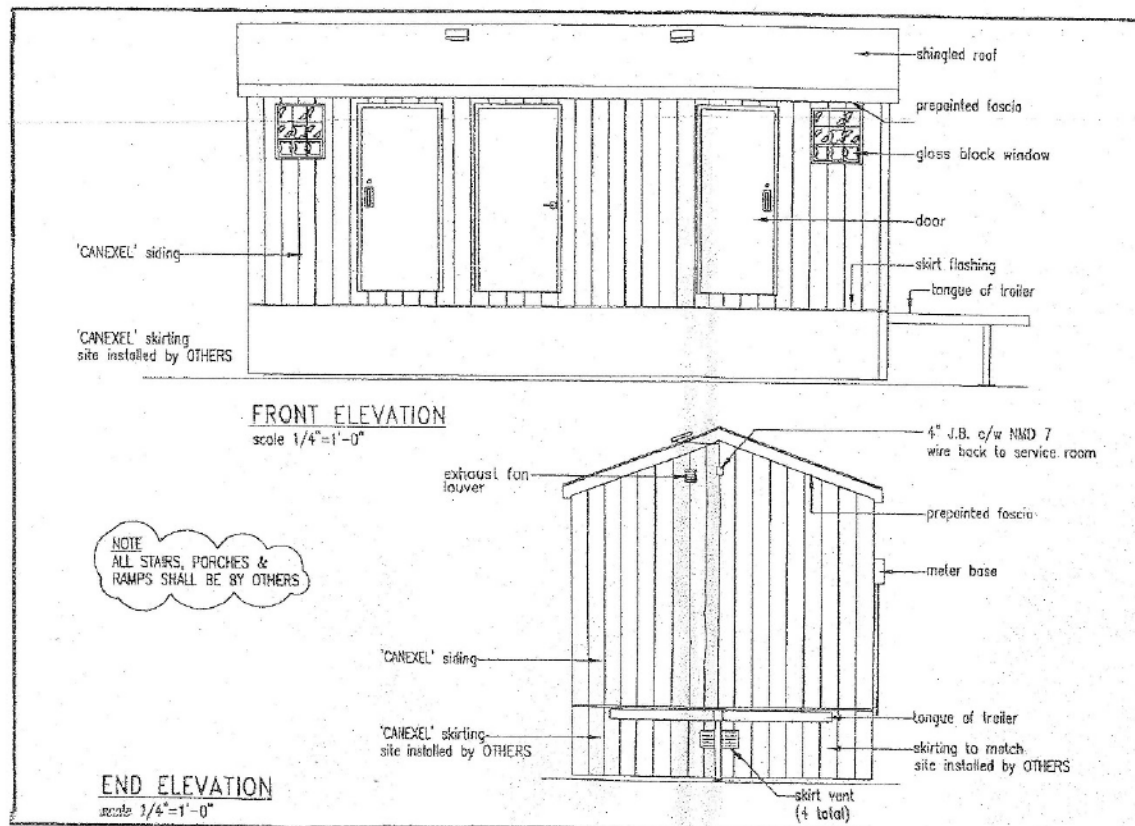
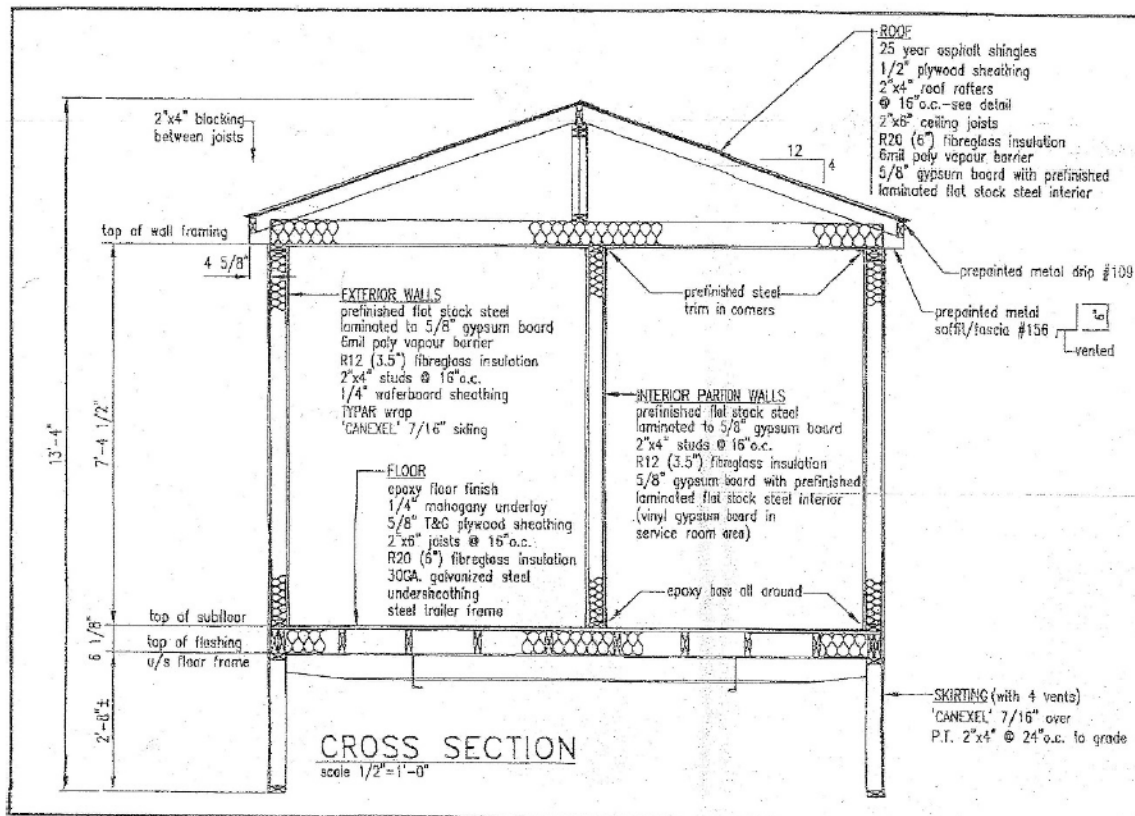
The current temporary washroom facility has exceeded its service life and is permanently closed for public health and safety concerns. The Parks department brought in temporary portable washroom facility until a new permanent structure for continued use for general public can be constructed.

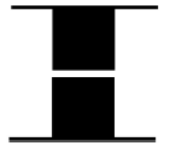
The washroom trailer was intended as a temporary measure until the funding and long-term planning of a permanent washroom structure could be realized.



Demolition Heritage Impact Statement for Erindale Park Washroom
1695 Dundas St W, Mississauga, Mississauga, ON

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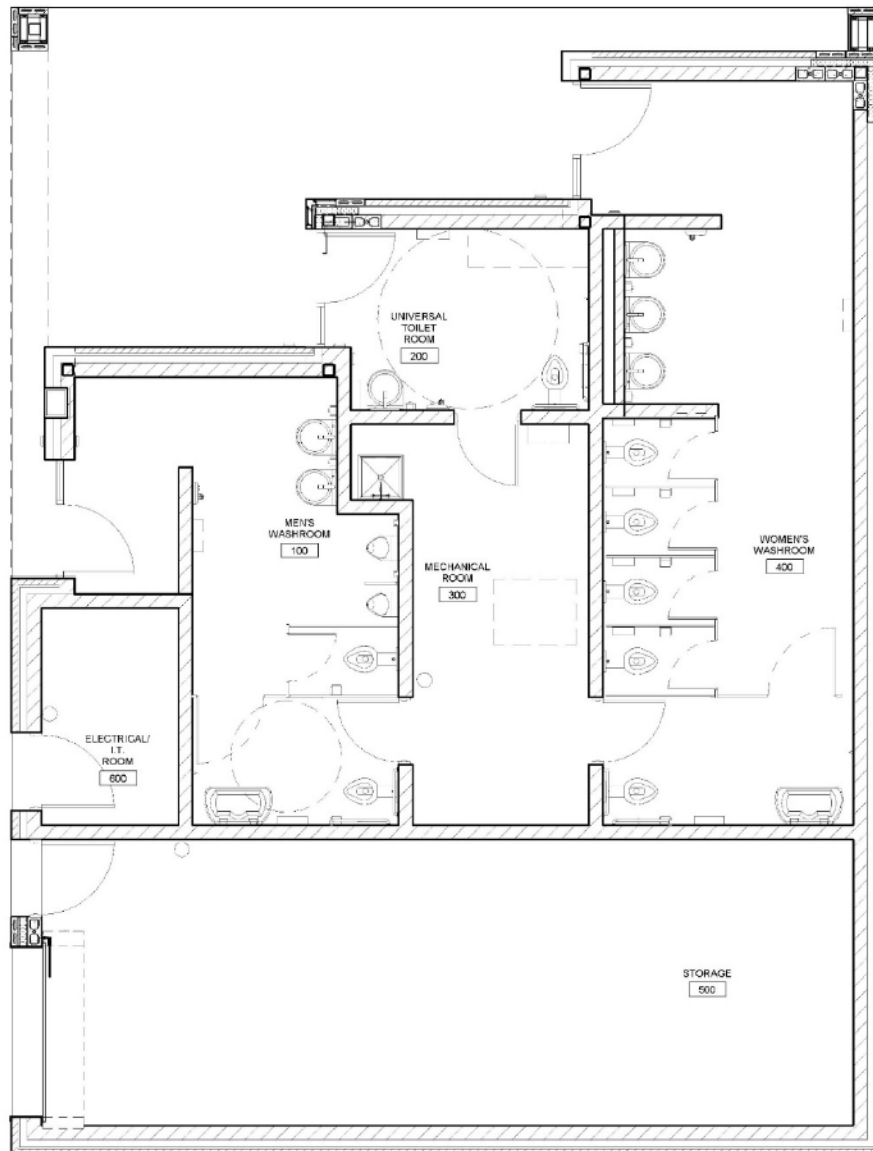


6. Proposed Building

The project was submit for building permit **BP 3NEW 17-6254** and is currently under review.

The proposed permanent washroom building is larger at 152m² and includes a separate universal toilet room, five (5) female toilet fixtures, four (4) male toilet fixtures and a connected 48m² Park's Storage area.

The proposed building is non-combustible with a concrete block and steel framed structure. The exterior materials are natural and traditional utilizing local made block veneer, steel 'wood look' siding and a gabled standing seam steel roof system.





7. Building Impact on Site & Character of Lands

Approved CVC permit number **17/035**.

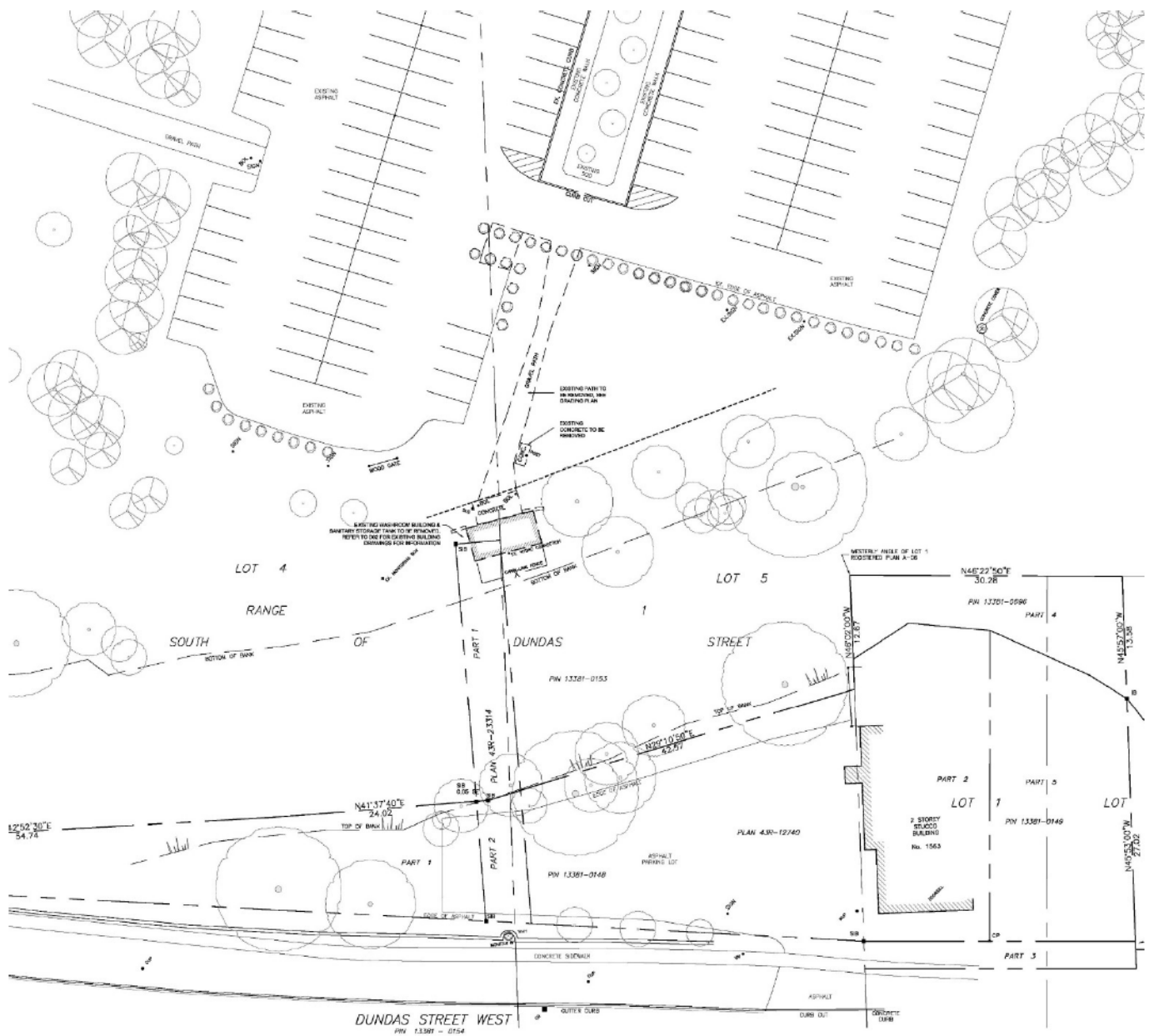
The building shall have little to no impact on the existing features and topography of the park with no existing trees being removed as a result of the new structure. A small asphalt apron to be added for safe access and a crushed limestone pathway for Parks to access the Storage area. The existing washroom site (after removals) would be graded and re-sodded to blend with the surrounding area. We have obtained CVC approval with no objections.

Storm water from the roof shall be drained to grade and a landscape swale behind the building shall direct water nearby to reintegrate with the natural overland flow of the site.





8. Existing Site Plan

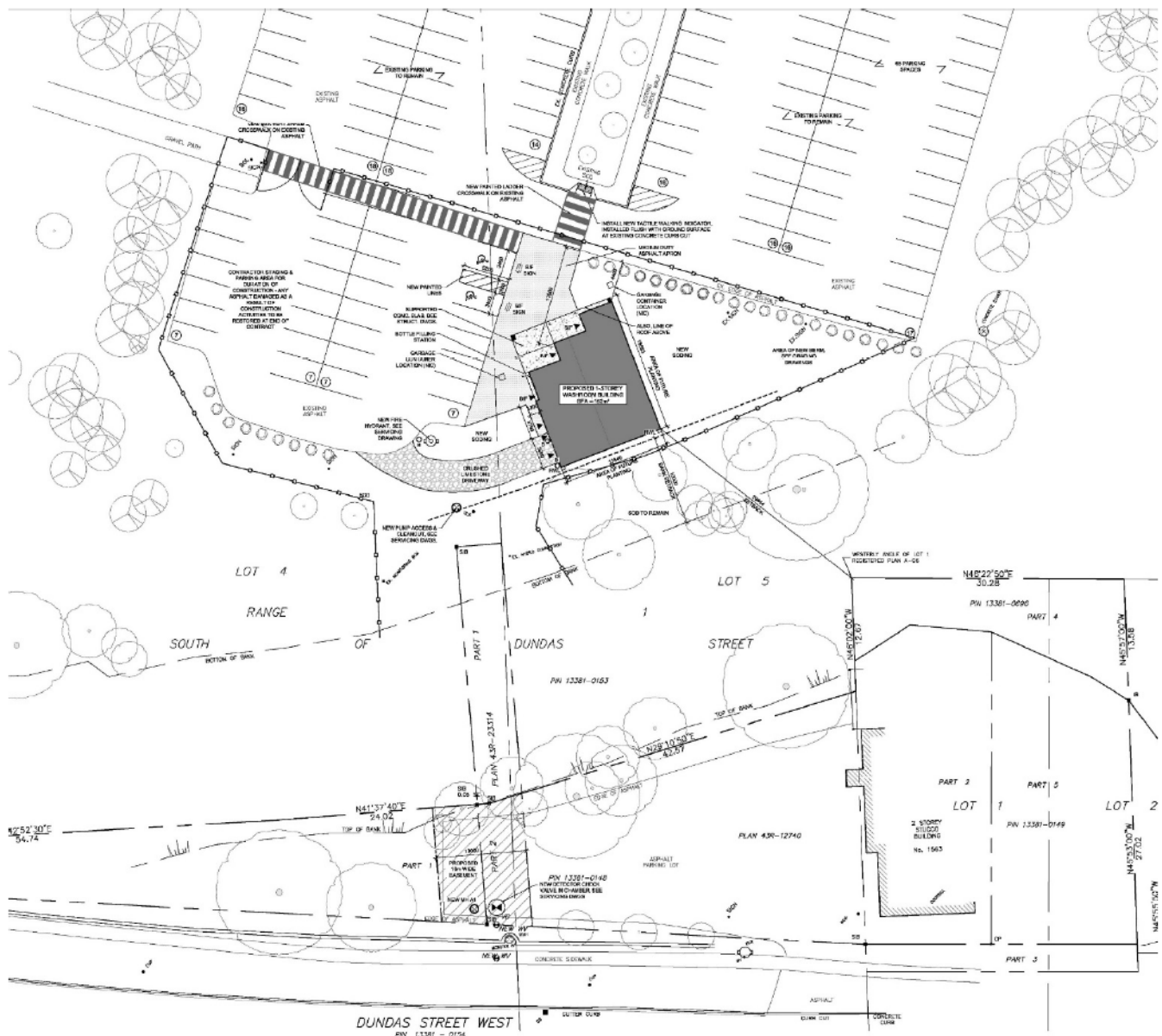




9. Proposed Site Plan

Approved Site Plan Express (SPAX) number **SPAX 17-13.**

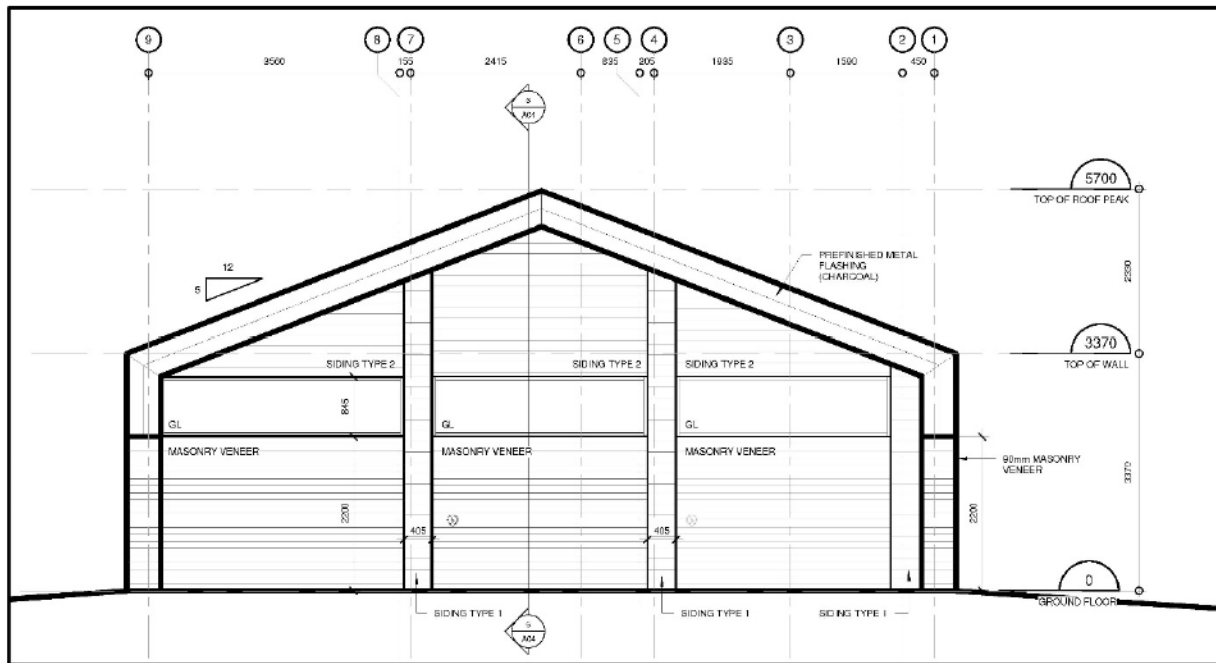
The proposed building is sited closer to the existing parking lot for accessibility and to respect the CVC's construction setback limits. The angled orientation addresses the building's presence and provides passive surveillance to the washroom doors from the main road into the Park.



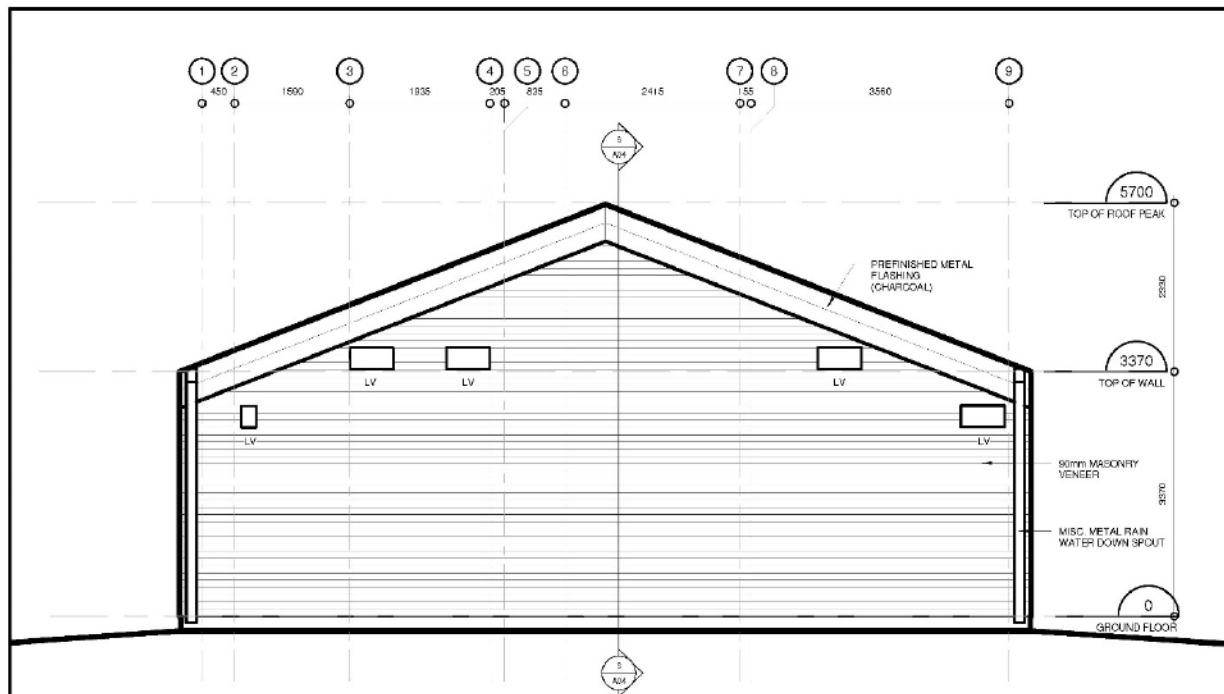


10. Exterior Elevations

North (Front) Elevation

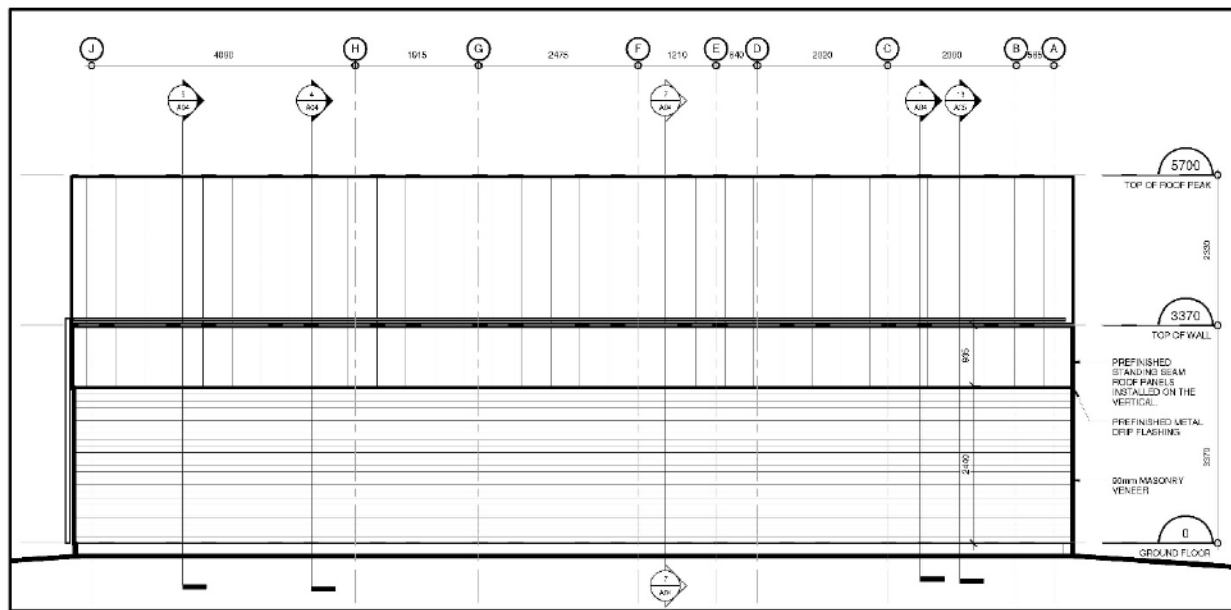


South (rear) Elevation

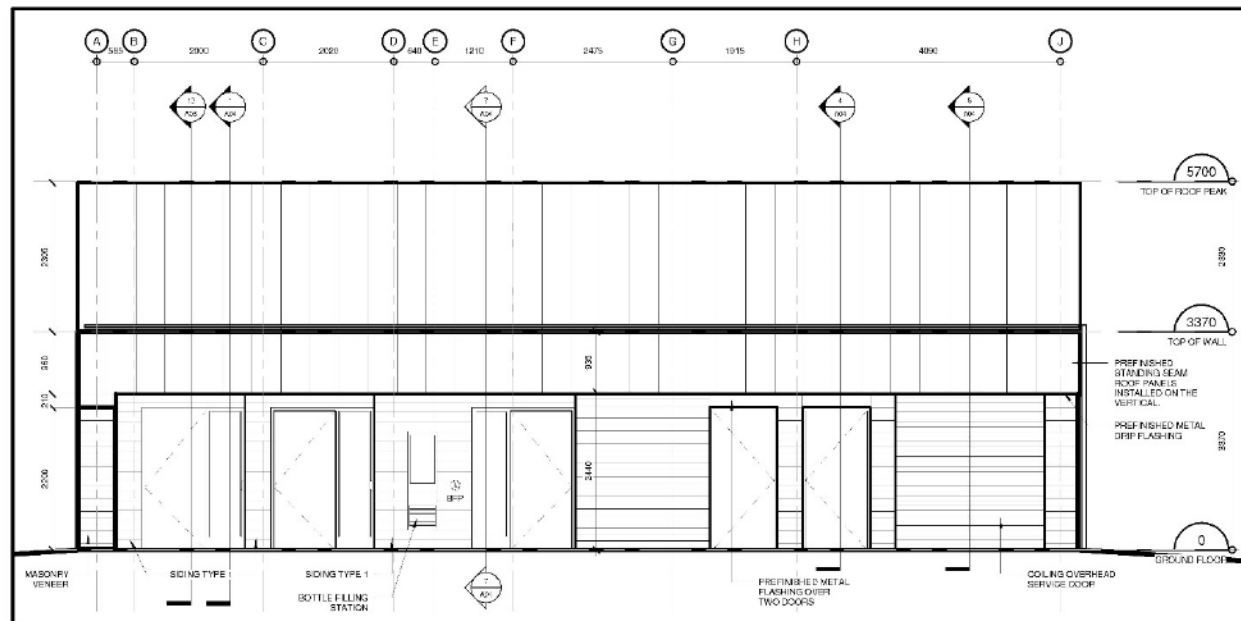




East (side) Elevation



West (side) Elevation





11. Heritage Value

The temporary washroom (in a trailer) installed in 1999 was intended to be 'temporary' with limited consideration given for contextual placement or aesthetic appeal in the Park. The trailer was a reaction to the need for washrooms on a limited budget with the long term plan that a permanent building would replace it in due course. The removal of the existing trailer would benefit the heritage and history of the Park and allow for a new permanent structure to provide an aesthetically respectful and long term accessible solution for the patrons and public. The current temporary washroom (in a trailer) has no heritage value.

This Demolition Heritage Impact Statement report has been prepared by Hossack & Associates Architects for the use of the City of Mississauga. Hossack & Associates Architects does not assume any liability for the use of this Demolition Heritage Impact Statement or the use of any information contained herein, or for damages resulting by the use of this Feasibility Study by other parties.

HOSSACK & ASSOCIATES Architects
June 2017.

City of Mississauga

Corporate Report



Date: 2017/06/15

Originator's files:

To: Chair and Members of Heritage Advisory Committee

From: Paul Mitcham, P. Eng, MBA, Commissioner of
Community ServicesMeeting date:
2017/07/11

Subject

An information report on the removal of the heritage properties located on Clarkson Road North and a review of the Heritage Permit process.

Recommendation

1. That the report from the Commissioner of Community Services dated June 15, 2017, on the removal of heritage properties located on Clarkson Road North and a review of the heritage permit process be received for information.

Report Highlights

- The property at 1109 Clarkson Road North was prematurely demolished on April 28, 2017
- The property at 1115 Clarkson Road North was found not to be of significant heritage value or interest and therefore not recommended for heritage protection
- The demolition has raised questions related to the heritage permit system and a necessary review to update related heritage planning policies
- Heritage Planning staff will return to the Heritage Advisory Committee later in 2017 with recommendations to improve the application process and Heritage By-law

Background

On April 28, 2017, a small commercial building located on the property of 1109 Clarkson Road North was demolished in advance of the 60 day heritage review period. The property, listed on the City's Heritage Register, is within the historic area known locally as Clarkson's Corners, a small collection of properties fronting onto Clarkson Road North directly north and south of the intersecting rail line. The subject property was one of a few properties, consolidated for development, on the south side of the rail line, being considered for redevelopment through an application to create a mix of residential and commercial uses.

As a result of the premature permitted demolition, members of the Heritage Advisory Committee asked for further information about the demolition and heritage permit process. The following recommendation was adopted at the Council meeting of May 24, 2017:

That staff be directed to prepare a report for the July Heritage Advisory Committee meeting providing:

- (a) A post evaluation of Clarkson Corners;
- (b) A review of the heritage permit process.

Comments

There are two adjacent heritage listed properties located at 1109 and 1115 Clarkson Road North. Both properties were included in a development application to repurpose the subject lands from a commercial and light industrial use to new residential and commercial. Each property has its own scenario and circumstances as it relates to heritage planning.

1109 Clarkson Road North

The following is a synopsis of what took place:

- Property was listed on the Heritage Register
- Property owner applied for demolition as part of a redevelopment application (commercial and residential mixed use)
- Heritage Planning requested a full Heritage Impact Assessment (HIA) in response to the request to demolish
- The proponent appealed the development application to the Ontario Municipal Board
- In early March, 2017, the proponent was informed by Heritage Planning that the HIA was incomplete
- On March 23, due to human error, the applicant was prematurely issued a Building Permit to demolish the structure. This action was unknown to Heritage Planning.
- The application to demolish was submitted on March 6, 2017.
- Heritage Planning acknowledged receipt of the revised application on April 19, 2017. Legal Services' advised that the 60 day notice should have begun immediately upon receipt of the revised application to demolish (approximately mid-March). Since there were no deficiencies noted in the period between receipt of the application and the date it was deemed complete.
- Heritage Planning accepted the complete HIA on April 19th and informed the applicant, thereby in Heritage Planning's opinion, beginning the 60 day notice period. However staff were unaware of the fact that the applicant had a permit to demolish

- The building was demolished on April 28th
- Legal Services determined the notice of intent to demolish on this file began on March 6th, when the applicant applied – thus the 60 day period ended May 5. The subject property was demolished prior to the 60 day period; however the property owner did have a permit to demolish.

1115 Clarkson Road North

The same process was followed as per 1109 Clarkson Road North with the exception that the property owner did not immediately seek to demolish the structure. The end of the 60 day notice period was May 5, 2017. It was therefore necessary for Council to consider the request to demolish prior to May 5th or it would have been deemed consent.

As a result of the demolition at 1109 Clarkson Road North and the realization of the notice timeframe, a report went directly to the May 3, 2017 meeting of General Committee. Based on the content of the report, it warranted presentation within Closed Session. Staff concluded the property failed to meet designation criteria and was therefore not worthy of heritage protection under the Ontario Heritage Act. Council approved the report recommendation.

The property demolition sparked a number of questions related to the heritage permit process. Directly associated with the heritage permit process is the need to clarify the commencement of the mandatory notice from a property owner to demolish a listed property, communications related to the notice, review of the City's Heritage By-law, review of the Heritage Permit Application Form, checks and balances in the City's MAX planning application system and consideration of a heritage designation rating system.

Issue # 1: Timing of a notice to demolish a listed or designated heritage property.

The Ontario Heritage Act states, "If property included in the register under subsection (1.2) has not been designated under section 29, the owner of the property shall not demolish or remove a building or structure on the property or permit the demolition or removal of the building or structure unless the owner gives the council of the municipality at least 60 days' notice in writing of the owner's intention to demolish or remove the building or structure or to permit the demolition or removal of the building or structure."

The commencement of the 60 day notice is initiated by the Heritage Planning staff receiving a complete application to demolish a listed property. In accordance with the City's Heritage By-law and provisions within the City's Official Plan, the property owner must provide a Heritage Permit Application Form, a Heritage Impact Statement (HIA) and any other documentation required by Council. It is possible that the application be found not complete whereby the applicant will be notified as to what is missing and be required to submit the necessary information or documentation.

From this point forward, in accordance with advice from Legal Services, once the revised application is accepted, Heritage Planning will notify the owner of receipt and the beginning and end dates of the 60 day notification period, plus inform the appropriate Planning and Building staff and any other internal notification which may be necessary.

If a property is designated under the Ontario Heritage Act, the property owner must apply to demolish a structure or any other alteration to the property. Council, upon receipt of an application, together with such information as it may require shall cause a notice of receipt to be served on the applicant. Should Council fail to make a decision within 90 days of sending the notice of receipt, it has deemed consent to the application.

Issue # 2: Communications related to a notice to demolish

It is a requirement under the Ontario Heritage Act that the municipality notify the owner of receipt of a request to demolish when the application has been accepted. The notice will clearly define the beginning and end of the 60 day notice period. A note will also be put in the MAX system (Mississauga Application Express) to alert anyone using the system of the heritage status. A copy of the receipt notice to the property owner will also be put in the Heritage Planning file for the subject property. Any other internal staff that may have an interest in the file will also be notified.

As a result of the premature demolition the Planning and Building Department have reviewed processes related to issuance of a building permit and have taken corrective action. All City staff related to planning matters use a system known as MAX, an internal electronic system to process a wide variety of planning and building permit applications. All listed and designated heritage properties on the City's Heritage Register are flagged in the MAX system. A new initiative in MAX has been created whereby a building permit application to demolish a structure on a heritage flagged property will not be issued to the applicant unless the owner can provide a clearance from Heritage Planning.

It has been clarified that properties listed on the City's Heritage Register will not be flagged for review by Heritage Planning if the applicant is seeking permission for an internal alteration, sign permit, or temporary tent structure.

Issue # 3: Review of the Heritage By-law

The Heritage By-law 109-16 provides definitions related to heritage planning, a delegation of authority to assist in the processing of applications to alter a heritage property, the heritage permit process, required documentation to be supplied by the applicant, and specific heritage permit process for property within a heritage conservation district.

A review of the Heritage By-law, in consultation with Legal Services and By-law Enforcement, will take into consideration the clarification and simplification of language used in the by-law in order to remove a level of interpretation, provide clarity to define a complete application, the review period of applications related to a 60 and 90 day notice to alter or demolish, clarify enforcement of the by-law, and clarify which properties do not require a heritage permit. The review will also consider the differences between applications as they apply to listed or designated properties, with a leaning toward less administration and review on listed properties. Heritage Planning staff will undertake the review in consultation with Legal Services and report back to the Heritage Advisory Committee later in 2017.

Other actions: Consideration of a heritage property designation rating system

Criteria to determine the cultural heritage value of a property is defined in Regulation 9/06 of the Ontario Heritage Act. There are three categories; design, historical, context. Within each category there are three more detailed criteria. The Ontario Heritage Act requires that a property has a minimum of one of the nine criteria in order to be considered for heritage designation.

A property owner can object to a heritage designation which leads to a referral to the Conservation Review Board (CRB). This tribunal provides a recommendation to Council who may take its advice or proceed on its own decision. Although the recommendation of the CRB is not binding, it does influence Council's decision. In reality most designations which go before the CRB have multiple criteria which meet those set out in Regulation 9/06. However, the argument of what meets the criteria is subjective and can be interpreted in different ways. To this end it is recommended that staff review and make considerations related to developing a rating system which would categorize the cultural heritage value of a property based on Regulation 9/06. This may provide a guide as to priority designations and the best use of limited resources required to process a heritage designation.

Financial Impact

There is no financial impact.

Conclusion

The application to demolish 1109 and 1115 Clarkson Road North resulted in a review of the application, a misinterpretation of the 60 notice to demolish and the premature building permit which allow the property owner to demolish. The series of events initiated a review of the heritage permit process and clarification on the acceptance of an application to demolish a listed heritage property.

As a result of the demolitions and review of the heritage permit process Heritage Planning staff have met with Planning and Building staff to clarify processes and will continue to meet with Legal Services and By-law Enforcement to review the Heritage By-law with recommendations to improve the process and by-law with a report back to the Heritage Advisory Committee later in 2017.



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: Mark Warrack, Manager, Heritage and Cultural Planning

Heritage Advisory Committee

2017/06/15
