

REVISED Council

Date: May 20, 2020
Time: 9:30 AM
Location: Online Video Conference

Members

Mayor Bonnie Crombie	
Councillor Stephen Dasko	Ward 1
Councillor Karen Ras	Ward 2
Councillor Chris Fonseca	Ward 3
Councillor John Kovac	Ward 4
Councillor Carolyn Parrish	Ward 5
Councillor Ron Starr	Ward 6
Councillor Dipika Damerla	Ward 7
Councillor Matt Mahoney	Ward 8
Councillor Pat Saito	Ward 9
Councillor Sue McFadden	Ward 10
Councillor George Carlson	Ward 11

Contact

Krystal Christopher, Legislative Coordinator, Legislative Services
905-615-3200 ext. 5411
Email krystal.christopher@mississauga.ca

Find it Online

<http://www.mississauga.ca/portal/cityhall/councilcommittees>

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1. **CALL TO ORDER**

2. **INDIGENOUS LAND STATEMENT**

"Welcome to the City of Mississauga Council meeting. We would like to acknowledge that we are gathering here today on the Treaty Lands and Territory of the Mississaugas of the Credit, and the traditional territories of the Anishinaabe, Haudenosaunee, Wyndot and Huron people. We also acknowledge the many First Nations, Inuit, Metis and other global Indigenous peoples who call Mississauga home. We welcome everyone."

3. **APPROVAL OF AGENDA**

4. **DECLARATION OF CONFLICT OF INTEREST**

5. **MINUTES OF PREVIOUS COUNCIL MEETING**

5.1 Council Minutes - May 6, 2020

6. **PRESENTATIONS**

7. **DEPUTATIONS**

8. **PUBLIC QUESTION PERIOD (15 Minutes)**

Written submissions can be submitted to city.clerk@mississauga.ca by Tuesday, May 19, 2020 before 12:00 PM.

9. **CONSENT AGENDA**

10. **MATTERS PERTAINING TO COVID-19**

10.1 Correspondence dated May 19, 2020 from the Medical Officer of Health, Region of Peel, regarding COVID-19 Update: Recommendations for Phase One reopening in Peel

10.2 Correspondence dated May 20, 2020 from Michelle DiEmanuele, President & CEO, Trillium Health Partners, regarding Trillium Health Partners current capacity related to COVID-19.

11. **INTRODUCTION AND CONSIDERATION OF CORPORATE REPORTS**

11.1 Calytera Software, Inc. (Amanda Web Services Module) Contract Amendment – File No. PRC000814

11.2 Delegated Authority for Facility Rental Contracts

11.3 Enersource Corporation – Shareholders Resolution in Lieu of Annual General Meeting and Approval of Alectra Resolutions

11.4 Request to Alter a Heritage Designated Property: 1620 Dundas Street West (Ward 7)

11.5 Request to Alter Heritage Designated Properties: 1234 Old River Road (Ward 1), 7076 Old Mill Lane and 62 Queen Street South (Ward 11)

- 11.6 Request to Demolish a Heritage Listed Property: 2740 Derry Road East (Ward 5)
- 11.7 Vendor of Record Designation for the Supply and Delivery of Replacement Parts and Repair Services for Vehicles and Equipment Managed by Fleet Services
- 11.8 Virtual Canada Day in Mississauga
- 11.9 Mississauga Urban Design Awards - Program Refresh
- 12. **PRESENTATION OF COMMITTEE REPORTS**
- 12.1 Audit Committee Report 1-2020 - dated May 11, 2020
- 13. **UNFINISHED BUSINESS**
- 14. **PETITIONS**
- 15. **CORRESPONDENCE**
- 16. **NOTICE OF MOTION**
- *16.1 To direct staff to investigate archived materials related to 6972 Airport Road; AVRO Arrow and/or AVRO Arrow buildings; agreements with Boeing Toronto Ltd.; to begin the Heritage Designation process of the Boeing lands at 6972 - 6982 Airport Road.
- 17. **MOTIONS**
- 17.1 To amend Resolution 0113-2020 by replacing Resolution number '0166-2019' with Resolution number '0167-2019'; and replacing 'June 24, 2019' in the first paragraph with 'July 3, 2019' (Housekeeping)
- 18. **INTRODUCTION AND CONSIDERATION OF BY-LAWS**
- 18.1 A by-law to appoint Screening Officers and to repeal By-law 0019-2020
- 18.2 A by-law to amend the Mississauga Official Plan regarding OZ17/010 W9, BCIMC Realty Corporation, Southwest corner of Battleford Road and Glen Drive

Item 17.1 / Resolution 0167-2019 / July 3, 2019
- 18.3 A by-law to amend the zoning by-law regarding OZ 17/010 W9, BCIMC Realty Corporation, southwest corner of Battleford Road and Glen Erin Drive

Item 17.1 / Resolution 0167-2019 / July 3, 2019
- 19. **MATTERS PERTAINING TO REGION OF PEEL COUNCIL**
- 20. **COUNCILLORS' ENQUIRIES**
- 21. **OTHER BUSINESS/ANNOUNCEMENTS**
- 22. **CLOSED SESSION**

23. CONFIRMATORY BILL

A by- law to confirm the proceedings of the Council of The Corporation of the City of Mississauga at its meeting held on May 20, 2020.

24. ADJOURNMENT

From: "Loh, Lawrence" <lawrence.loh@peelregion.ca>
Date: May 19, 2020 at 10:02:39 PM EDT
To: ZZT-Members of Regional Council <ZZT-MembersofRegionalCouncil@peelregion.ca>
Cc: ZZT-ELT Executive Leadership Team <ZZT-ELTExecutiveLeadershipTeam@peelregion.ca>, Alghabra <Omar.Alghabra@parl.gc.ca>, Anand <deepak.anand@pc.ola.org>, Bains <Navdeep.Bains@parl.gc.ca>, Cuzzetto <rudy.cuzzetto@pc.ola.org>, Fonseca <Peter.Fonseca@parl.gc.ca>, Jones <sylvia.jones@pc.ola.org>, Khalid <Iqra.Khalid@parl.gc.ca>, Khera <Kamal.Khera@parl.gc.ca>, Kusendova <natalia.kusendova@pc.ola.org>, Sabawy <sheref.sabawy@pc.ola.org>, Sahota <Ruby.Sahota@parl.gc.ca>, Sandhu <amarjot.sandhu@pc.ola.org>, Sangha <Ramesh.Sangha@parl.gc.ca>, Sarkaria <prabmeet.sarkaria@pc.ola.org>, Seeback <Kyle.Seeback@parl.gc.ca>, Sidhu <Sonia.Sidhu@parl.gc.ca>, Sikand <Gagan.Sikand@parl.gc.ca>, Singh <GSingh-QP@ndp.on.ca>, Spengemann <Sven.Spengemann@parl.gc.ca>, SSingh <SSingh-QP@ndp.on.ca>, Tangri <nina.tangri@pc.ola.org>, Yarde <KYarde-QP@ndp.on.ca>
Subject: COVID-19 Update: recommendations for Phase One reopening in Peel

Dear Members of Regional Council:

Today, the Ontario government announced details around Phase One of their provincial reopening plans. In a memorandum, they outlined specific [amendments](#) being made to the provincial emergency order Ontario Regulation 104/20 to permit the opening of:

- Outdoor sports facilities and multi-use fields (including baseball diamonds, soccer fields, frisbee golf locations, tennis, platform tennis, table tennis, pickleball courts, basketball courts, BMX parks, and skate parks)
- Off-leash dog areas; and
- Outdoor picnic sites, benches, and shelters in park and recreational areas

Today, the Chief Medical Officer of Health for Ontario made it clear that amenities like those listed above should not be made available for public use until it is safe to do so. I respectfully submit to you that it is not yet safe to re-open these facilities in Peel.

We have seen our new reported cases plateau in recent weeks, but we have not seen a sustained decrease in new cases, which is one of the key public health indicators identified in the Province's plan for reopening. Of great concern is that yesterday and the day before, we saw a notable increase in reported cases. In the past week, over 20 per cent of all new cases in Ontario were reported in Peel, a rate disproportionate to our population. We also continue to have a significant number of COVID-19 cases on wards and in intensive care units in both Mississauga and Brampton.

Since time is of the essence, I am providing the following recommendation here and not in a Council report. I strongly recommend the following course of action to Regional Council for Regional and/or municipal staff action:

- That Delay opening any outdoor facilities until at least Monday, May 25, 2020, with recognition that this recommended date may change with concerning trends in local epidemiology identified in the future;

- Develop a phased approach to opening such facilities in consultation with Peel Public Health, starting with lower risk, individual activities (e.g. off-leash dog parks or skateboarding). This phased approach should include plans to implement appropriate hygiene measures and a communication plan that reinforces the importance of maintaining physical distancing and avoiding crowding; and
- Develop an aligned enforcement plan that monitors facilities opened in each phase.

I recognize that this recommendation will delay opportunities for our residents to enjoy the recently announced provincial measures and recognize these continued sacrifices.

I am confident, however, that such actions are necessary to protect our the health of our community. Taking the time to consider how we reopen these facilities in a manner that respects our local epidemiology and context will ensure that we continue to stop the spread of COVID-19.

As always, I thank you for your continued support during these challenging times.

Sincerely yours,

Lawrence

Lawrence C. Loh, MD, MPH, FCFP, FRCPC, FACPM

Medical Officer of Health (A)

Public Health, Health Services

Region of Peel

7120 Hurontario Street RPO 667 Streetsville

Mississauga, ON L5M 2C2

tel +1 905 791-7800 x2856

e-mail lawrence.loh@peelregion.ca

May 20, 2020

Dr. Lawrence Loh
Interim Medical Officer of Health, Region of Peel
lawrence.loh@peelregion.ca

Dr. Loh,

Thank you for your continued commitment and partnership to managing COVID-19 in the Region of Peel. I am writing to you today to update you on Trillium Health Partners (THP) current capacity related to COVID-19.

THP continues to see a significant amount of COVID-19 related activity both inside our hospital, and at our Assessment Centres. Today, our hospital is sitting at over 85% capacity, with 101 COVID-19 patients in our care, we have also begun to hit over 90% capacity on some days. While the number of confirmed cases in the hospital has decreased slightly over the last several weeks, we continue to see more cases than most hospitals in Ontario.

Our region is one of the most impacted areas of the province, with 15% of Ontario cases living in the Region of Peel and over half of those in Mississauga. In the hospital, we continue to see day-over-day increases in the number of people who have acquired COVID-19. While the increases are marginally lower each day, we are still seeing an increase, which means the rate of infection is slowing, but has not changed to the point there is a decline.

As part of the provincial strategy to support Long-Term Care (LTC), our hospital has also been active in the community and we have now completed assessments in 19 LTC homes in our region. THP remains active in supporting three LTCs with redeployed hospital staff equipped with personal protective equipment (PPE).

While we have been able to maintain an adequate supply of PPE through strong conservation efforts and stewardship, like the rest of the province and country, any disruption in the supply chain or changes in our patient volumes would put our supply at risk. We continue to put mitigation strategies in place and are actively working with provincial partners to ensure we always have the supply needed to protect our staff and care for our community.

As we begin to look forward, THP has begun planning for the necessary reopening of some services, such as elective surgeries. All hospitals across Ontario were given criteria from Ontario Health for service resumption, this includes areas such as maintaining 85% capacity or having the ability to immediately create 15% capacity, as well as having a strong supply of PPE, such as a rolling 30-day stock on-hand.

While the hospital is actively planning how we can safely resume some services, we are still several weeks away from meeting the provincial standards. I look forward to providing you and our regional partners an update on our plans for resuming services, as well as keeping you regularly updated on our capacity. On behalf of the hospital, our health care workers and patients, thank you for your leadership in the Region of Peel. We stand alongside you in this challenging time.

Best,



Michelle DiEmanuele
President & CEO
Trillium Health Partners

CC: Mayor Bonnie Crombie, City of Mississauga | Dr. Naveed Mohammad, President & CEO, William Osler

City of Mississauga Corporate Report



<p>Date: April 16, 2020</p> <p>To: Mayor and Members of Council</p> <p>From: Gary Kent, CPA, CGA, ICD.D, Commissioner of Corporate Services and Chief Financial Officer</p>	<p>Originator's files:</p> <hr/> <p>Meeting date: May 20, 2020</p>
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Subject

Calytera Software, Inc. (Amanda Web Services Module) Contract Amendment – File No. PRC000814

Recommendation

1. That Council approve the contract amendment with Calytera Software, Inc. (formerly known as CSDC Systems Inc.) for purchasing an additional module for Amanda software as detailed in the corporate report entitled, "Calytera Software, Inc. (Amanda Web Services Module) Contract Amendment – File No. PRC000814" dated April 16th, 2020, by the Commissioner of Corporate Services and Chief Financial Officer.
2. That the Purchasing Agent or designate be authorized to execute the necessary amendments to increase the value of the contract between the City and Calytera Software, Inc. (formerly known as CSDC Systems Inc.) from the original amount of \$131,599 USD to an estimated amount of \$180,000 USD, for the supply of Amanda Web Services module and maintenance and support for such module for the remainder of the contract term for an estimated total amount of \$48,000 USD, subject to future budget approval.

Background

Amanda (CSDC Systems Inc.) is a software application that is used by City staff for issuing, tracking and managing business operating licenses that has been used by City Transportation & Works Compliance & Licensing and Mobile Licensing. The application has been used by staff since 1999 when the original contract was awarded to CSDC Systems.

In 2019 the City upgraded Amanda software to the latest version available. CSDC System became Calytera Software, Inc. in 2019 as a result of corporate name change.

The 2020 approved project to deliver Online Business and Mobile Licensing Services requires integration between the Online Business and Mobile Licensing Self-Serve solution and the existing backend licensing management system, Amanda. To provide integration between these two different systems a new module is required for Amanda. The new module is the Amanda Web Services module.

Comments

The City of Mississauga engaged the services of a vendor named eSolutions to provide a statement of work to create a citizen facing self-serve licencing system. The core of the solution involves pulling and pushing information to the existing Amanda business system, which is the City's back office system for all business and licencing requirements. eSolutions has recommended as part of its integration assessment that Amanda Web Services be purchased to provide seamless and secure integration and provide a direct link to the backend Amanda system.

After thorough discussions with affected business area leads in each of the business groups, City staff have confirmed that the Amanda Web Services module provides a mechanism for integrating the Online Services and Amanda applications. This new module is proprietary to Amanda and provides a secure integration point to 3rd party applications. It is recommended that the Calytera Software, Inc. contract be amended to include the Amanda Web Services module.

Purchasing By-law Authorization

The recommendation in this report is made in accordance with Schedule A of the Purchasing By-law #374-06, items 1(a) (iii), wherein it states that "the Goods and/or Services are only available from one supplier by reason of; the existence of exclusive rights such as patent, copyright or license"; and (b)(xi) which states that a single source procurement method may be applied when, "a need exists for compatibility with, or for the maintenance and support of a City Standard and there are no reasonable alternatives, substitutes, or accommodations".

Information Technology, Legal Services and Materiel Management staff are collaborating to establish the detailed requirements, negotiate the final arrangements and prepare the requisite forms including the agreement.

Financial Impact

Purchasing the Amanda Web Services will have a onetime cost impact of \$30,000 USD and an annual ongoing impact of \$6,000 USD for software maintenance per annum for a period of three (3) years. The onetime software acquisition cost plus current year (2020) software maintenance

fee will be paid out of PN20090. The ongoing operating impact for 2021 and 2022 software maintenance fees will be paid out of the Maintenance and Support cost element subject to future budget approvals. Appendix 1

Conclusion

It is recommended that the existing contract agreement with Calytera Software, Inc. be amended to increase the value of the contract between the City and Calytera Software, Inc. from the original amount of \$131,599 USD to an approximate amount of \$180,000 USD for the supply of Amanda Web Services module(s) and three (3) years of annual maintenance and support accordingly with an estimated amount of \$48,000 USD for the remaining of the contract term subject to budget approval.

Attachments

Appendix 1: Statement of Work Summary



Gary Kent, CPA, CGA, ICD.D, Commissioner of Corporate Services and Chief Financial Officer

Prepared by: Dan Pitu, Project Manager, IT – Project Portfolio & Development, Transportation & Works

Summary of Statement of Work

The following represents the key deliverables from Calytera for ongoing annual maintenance and support for Amanda Web Services:

- Amanda Web Service module

a)	Amanda Web Services Module	One-time fee unit price	\$30,000.00 USD
b)	Amanda Web Services Maintenance	TBD 11/30/2020	\$6,000.00 USD
c)	Amanda Web Services Maintenance	12/01/2020 11/30/2021	\$6,000.00 USD
d)	Amanda Web Services Maintenance	12/01/2021 11/30/2022	<u>\$6,000.00 USD</u>
		TOTAL:	\$48,000.00 USD

There are no maintenance cost increases through to November 30, 2022.

City of Mississauga

Corporate Report



<p>Date: April 30, 2020</p> <p>To: Mayor and Members of Council</p> <p>From: Paul Mitcham, P.Eng, MBA, Commissioner of Community Services</p>	<p>Originator's files:</p> <hr/> <p>Meeting date: May 20, 2020</p>
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Subject

Delegated Authority for Facility Rental Contracts

Recommendation

1. That the Commissioner of Community Services or designate, be authorized to negotiate and execute facility rental contracts and other related and ancillary agreements with facility rental users to permit temporary use of space not exceeding one year in bookable City properties as outlined in the corporate report entitled "Delegated Authority for Facility Rental Contracts" dated April 30, 2020 from the Commissioner of Community Services, in a form satisfactory to the City Solicitor.
2. That the necessary by-law be enacted.

Background

Section 23.1(1) of the Municipal Act, 2001 as amended, provides municipalities with the authority to delegate certain powers and duties that otherwise must be exercised by City Council. In many jurisdictions, it is common practice to delegate authority for such matters to staff in an effort to improve organizational efficiency.

During the Recreation CLASS Facility Rental Audit, it was recommended that a delegated authority by-law be initiated to designate the appropriate staff to sign facility rental contracts. Legal Services was consulted on this matter during the audit review and were in agreement with this recommendation.

Present Status

Currently, all outbound facility rental contracts are imprinted with an image of the Supervisor, CSC-Bookings & Registration signature. The audit review of the applicable by-law indicated that only the Commissioner, Community Services or their delegates have the authority to

administer the fees and charges By-law for their division. The authority to sign facility rental contracts has not been delegated.

Comments

In addition to the facility rental contracts that are prepared through the CLASS Recreation Management Software, the special event venues at Paramount Fine Foods Centre, Celebration Square, Living Arts Centre, Meadowvale Theatre, and Small Arms Building prepare agreements as part of their current business process. These agreements relate to terms outside of the standard facility rental contract as well as specific services and charges that are part of operating an event at these venues. The Commissioner of Community Services will review these types of agreements, delegate authority as deemed reasonable, and document accordingly, following approval of the by-law requested in this report.

Financial Impact

There are no financial impacts resulting from the Recommendations in this report.

Conclusion

A by-law delegating the authority to the Commissioner of Community Services or their designate to negotiate and execute facility rental contracts and other related and ancillary agreements with facility rental users will streamline contract execution and management and satisfy the audit recommendation.



Paul Mitcham, P.Eng, MBA, Commissioner of Community Services

Prepared by: Corey Groulx, Leader, Projects - Recreation

City of Mississauga

Corporate Report



Date: May 8, 2020

To: Mayor and Members of Council

From: Andra L. Maxwell B.A., LL.B., CIC.C, City Solicitor

Originator's files:

Meeting date:
May 20, 2020

Subject

Enersource Corporation – Shareholders Resolution in Lieu of Annual General Meeting and Approval of Alectra Resolutions

Recommendation

1. That City Council waive the requirement for an Annual General Meeting of Enersource Corporation on the basis that its financial statements for the financial year ended December 31, 2019 will to be presented when City Council resumes public deputations.
2. That City Council authorize the Mayor and City Clerk to execute the Alectra Resolutions attached as Appendices 1 and 2 to the report of the City Solicitor entitled "Enersource Corporation – Shareholders Resolution in Lieu of Annual General meeting and Approval of Alectra Resolutions," confirming the directors of Alectra, appointing its auditor and acknowledging receipt of the financial statements and annual report of Alectra for the financial year ended December 31, 2019.

Background

On January 31, 2017 Enersource, Horizon Utilities and PowerStream merged creating Alectra Inc. which subsequently acquired Brampton Hydro and most recently Guelph Hydro. The City of Mississauga continues to hold 90% of the shares in the repurposed Enersource Corporation. The sole business of Enersource Corporation is the management of the Alectra shareholding.

Comments

The shareholders' agreement in place between the City of Mississauga, BPC Energy Corporation and Enersource Corporation dated January 31, 2017 requires the audited annual financial statements to be delivered to the shareholders of Enersource on an annual basis. It is a common approach to waive the AGM when there are no substantive matters requiring shareholder action and this has been the practice of Enersource Corporation for several years. Other than waiving the AGM and approving the Alectra resolutions, there are no other Enersource matters for the City to consider at this time. The audited financial statements of

Enersource will be presented in a separate report from the Commissioner of Corporate Services, with a deputation by David Warner, Chair, Enersource Board of Directors, when Council resumes hearing public deputations.

The directors of Enersource Corporation have received notice of the Annual General Meeting of Alectra Inc. to be held on Friday, June 12, 2020. The shareholders of Alectra have been asked to approve a resolution confirming the board of directors of Alectra, appointing KPMG as auditors of Alectra, and acknowledging receipt of the financial statements and annual report for Alectra for the year ended December 31, 2019.

The Ontario *Business Corporations Act* authorizes shareholders of a corporation to pass a shareholders' resolution in lieu of holding an annual general meeting. The Board of Directors of Enersource is recommending that the City and Borealis exercise this option under the Shareholders' Agreement.

Financial Impact

N/A

Conclusion

This report recommends waiving the Annual General Meeting of Enersource Corporation and seeks authority to execute the required resolutions to confirm the directors of Alectra, appoint KPMG LLP as its auditor and acknowledge receipt of the financial statements and annual report of Alectra for the financial year ended December 31, 2019.

Attachments

1. Council of the City of Mississauga Resolution respecting Alectra Inc.
2. Resolution of the Shareholders of Enersource Corporation respecting Alectra Inc.
3. Resolution of the Directors of Enersource Corporation respecting Alectra Inc. – For Information only



Andra L. Maxwell B.A., LL.B., CIC.C, City Solicitor

Prepared by: Andra Maxwell, City Solicitor

Appendix 1

COUNCIL OF THE CITY OF MISSISSAUGA

WHEREAS the Corporation of the City of Mississauga (the “**City**”), Enersource Corporation (“**Enersource**”) and others are parties to the Unanimous Shareholders Agreement for Alectra Inc. (“**Alectra**”);

AND WHEREAS Enersource has received notice of the Annual General Meeting of Alectra Inc. to be held on Friday, June 12, 2020;

AND WHEREAS in connection with the AGM, it is proposed that the shareholders of Alectra approve a resolution (the “**Alectra Resolution**”):

- (a) Confirming the following persons as directors of Alectra:

Jane Armstrong
 Gerald Beasley
 Maurizio Bevilacqua
 Bonnie Crombie
 Giuseppina D’Agostino
 Sean Donnelly
 Fred Eisenberger
 Matthew Harris
 John Knowlton
 Jeff Lehman
 Norm Loberg
 Donald Lowry
 Teresa Moore
 Frank Scarpitti;

- (b) Appointing KPMG LLP as auditors of Alectra;
- (c) Acknowledging receipt of the financial statements of Alectra for the financial year ended December 31, 2019; and
- (d) Acknowledging receipt of the Annual Report/Sustainability Report of Alectra for the financial year ended December 31, 2019.

NOW THEREFORE IT IS RESOLVED THAT:

1. The execution and delivery by the City of a resolution of the shareholders of Enersource authorizing Enersource to approve the Alectra Resolution (the “**Enersource Resolution**”) is hereby authorized and approved.
2. The Mayor and the City Clerk are hereby authorized and directed to execute and deliver the Enersource Resolution.

- 2 -

3. The Mayor and the City Clerk are hereby authorized and directed to sign and/or dispatch and deliver all other resolutions, documents, notices, certificates to be signed and/or dispatched or delivered under or in connection with the foregoing matters or to take any action deemed necessary in respect of any of the foregoing.

Appendix 2

RESOLUTION OF THE SHAREHOLDERS OF ENERSOURCE CORPORATION

WHEREAS Enersource Corporation (the “**Corporation**”) has received notice of the Annual General Meeting (the “**AGM**”) of Alectra Inc. (“**Alectra**”) to be held on Friday, June 12, 2020.

AND WHEREAS in connection with the AGM, it is proposed that the shareholders of Alectra approve a resolution (the “**Alectra Resolution**”):

- (a) Confirming the following persons as directors of Alectra:

Jane Armstrong
Gerald Beasley
Maurizio Bevilacqua
Bonnie Crombie
Giuseppina D’Agostino
Sean Donnelly
Fred Eisenberger
Matthew Harris
John Knowlton
Jeff Lehman
Norm Loberg
Donald Lowry
Teresa Moore
Frank Scarpitti;

- (b) Appointing KPMG LLP as auditors of Alectra;
- (c) Acknowledging receipt of the financial statements of Alectra for the financial year ended December 31, 2019; and
- (d) Acknowledging receipt of the Annual Report/Sustainability Report of Alectra for the financial year ended December 31, 2019.

NOW THEREFORE IT IS RESOLVED THAT:

1. The Corporation in its capacity as a shareholder of Alectra be and it is hereby authorized to give its approval to the Alectra Resolution.

- 2 -

DATED the _____ day of _____, 2020.

**THE CORPORATION OF THE CITY OF
MISSISSAUGA**

Per: _____
Name:
Title:

Per: _____
Name:
Title:

BPC ENERGY CORPORATION

Per: _____
Name:
Title:

Per: _____
Name:
Title:

Appendix 3

RESOLUTION OF THE DIRECTORS OF ENERSOURCE CORPORATION

WHEREAS Enersource Corporation (the “**Corporation**”) has received notice of the Annual General Meeting (the “**AGM**”) of Alectra Inc. (“**Alectra**”) to be held on Friday, June 12, 2020;

AND WHEREAS in connection with the AGM, it is proposed that the shareholders of Alectra approve a resolution (the “**Alectra Resolution**”):

- (a) Confirming the following persons as directors of Alectra:

Jane Armstrong
 Gerald Beasley
 Maurizio Bevilacqua
 Bonnie Crombie
 Giuseppina D’Agostino
 Sean Donnelly
 Fred Eisenberger
 Matthew Harris
 John Knowlton
 Jeff Lehman
 Norm Loberg
 Donald Lowry
 Teresa Moore
 Frank Scarpitti;

- (b) Appointing KPMG LLP as auditors of Alectra;
- (c) Acknowledging receipt of the financial statements of Alectra for the financial year ended December 31, 2019; and
- (d) Acknowledging receipt of the Annual Report/Sustainability Report of Alectra for the financial year ended December 31, 2019.

NOW THEREFORE IT IS RESOLVED THAT:

1. Subject to and conditional upon the approval of the shareholders of the Corporation, the Corporation in its capacity as a shareholder of Alectra be and it is hereby authorized to give its approval to the Alectra Resolution.
2. The CEO and the Chair of the Corporation are hereby authorized and directed to execute and deliver the Alectra Resolution.
3. The CEO and the Chair of the Corporation are hereby authorized and directed to sign and/or dispatch and deliver all other resolutions, documents, notices, certificates to be signed and/or dispatched or delivered under or in connection with the foregoing matters or the AGM or to take any action deemed necessary in respect of any of the foregoing.

City of Mississauga Corporate Report



11.4

Date: April 24, 2020

To: Mayor and Members of Council

From: Paul Mitcham, P.Eng, MBA, Commissioner of
Community Services

Originator's files:

Meeting date:
May 20, 2020

Subject

Request to Alter a Heritage Designated Property: 1620 Dundas Street West (Ward 7)

Recommendation

That the proposed alteration of 1620 Dundas Street West, which is designated under Part IV of the Ontario Heritage Act, be approved as outlined in the Corporate Report dated April 24, 2020, from the Commissioner of Community.

Background

The subject property is designated under Part IV of the Ontario Heritage Act. Section 33 of the Act requires permission from Council in order to make alterations to property designated under Part IV of Act.

The subject property is owned by the City of Mississauga. The application calls for the installation of a wire chain link fence around a grassed area at the rear of the property around an area roughly 16 by 28 m (Appendix 1).

Comments

The owner of the subject property proposes to erect a wire chain link fence around the above area. The fence would be 1.2 m (4 feet) in height. The fence is required to provide a safe enclosure for children when participating in outdoor activities. The addition of the fence will add to the overall uses permitted for Erindale Hall, by allowing for early childhood education programs to be run out of the facility.

The proposed addition is located at the rear of the property and would have no impact on the Hall itself. It is a sufficient distance from the structure and it would not obstruct any sight lines or views. It would be obscured from Dundas Street by the parking lot.

Financial Impact

The cost is covered under the existing Huron Park facility operating budget.

Conclusion

The Recreation Department of the City of Mississauga has requested permission to alter the property at 1620 Old Dundas Street West, which is designated under the Ontario Heritage Act. Because the proposal has a minimal negative impact on the property's cultural heritage value, the alteration should be granted approval under the Ontario Heritage Act, pending all other required project approvals.

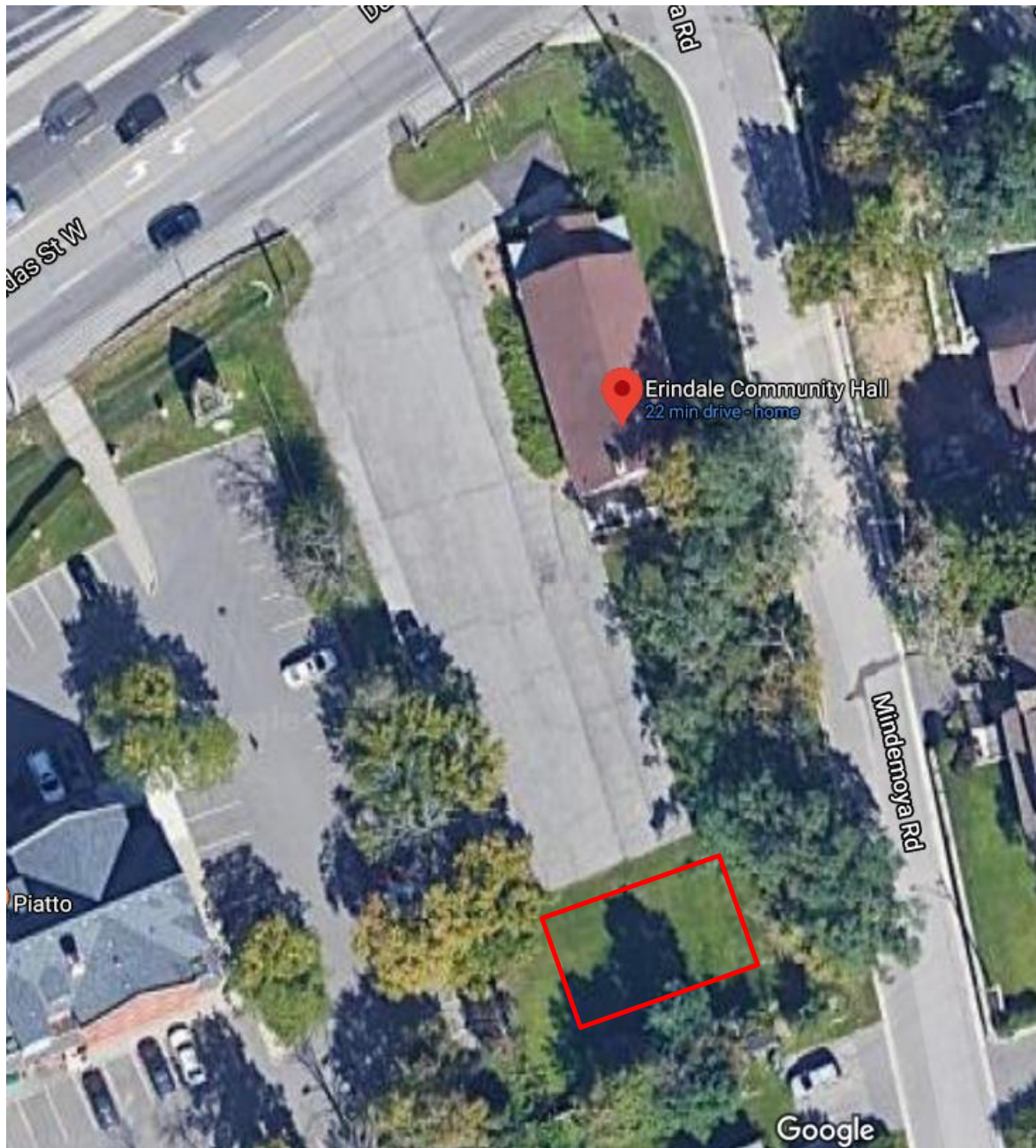
Attachments

Appendix 1: Drawing



Paul Mitcham, P.Eng, MBA, Commissioner of Community Services

Prepared by: John Dunlop, Supervisor, Heritage Planning



City of Mississauga Corporate Report



<p>Date: April 24, 2020</p> <p>To: Mayor and Members of Council</p> <p>From: Paul Mitcham, P.Eng, MBA, Commissioner of Community Services</p>	<p>Originator's files:</p> <hr/> <p>Meeting date: May 20, 2020</p>
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Subject

Request to Alter Heritage Designated Properties: 1234 Old River Road (Ward 1), 7076 Old Mill Lane and 62 Queen Street South (Ward 11)

Recommendation

That the request to alter the heritage designated properties at 1234 Old River Road, 62 Queen Street South and 7076 Old Mill Lane as per the Corporate Report from the Commissioner of Community Services dated April 24, 2020, be approved.

Background

The 1234 Old River Road and 62 Queen Street South are designated under Part IV of the Ontario Heritage Act. Section 33 of the Act requires permission from Council in order to make alterations to property designated under Part IV of Act. Permission is granted by the issuance of a permit allowing alteration to a designated property.

7076 Old Mill Lane is designated under Part V of the Ontario Heritage Act as it forms part of the Meadowvale Village Heritage Conservation District (HCD). Changes to the property are subject to the Meadowvale Village HCD Plan, 2014, and substantive changes identified in said plan require a heritage permit.

Comments

Alterations to the above properties and staff comment are as follows:

1234 Old River Road

- Removal of an 1858 rear 1.5 storey addition and replace it with a two storey sympathetic addition on the same footprint (Appendix 1).

The existing foundation for the 1.5 storey foundation would not support the proposed 2 storey addition. Therefore, the existing addition will have to be demolished and replaced with a new addition. This alteration would not impact the original heritage attributes of the

property, which comprised of an older log cabin house. The addition would be sympathetic to heritage attributes and character of the property. The new addition would not require any new openings to the balance of the house and the existing brick chimney would be incorporated into the alteration.

62 Queen Street South

- A ground sign at the front of the property to advertise the new business at this location (Appendix 2).

The sign is reversible and the brick podium helps coordinate it with the building. The overall size of the sign is large and as it is proposed to be in front of the heritage structure. Therefore, any further reduction in the size of the sign is encouraged to further mitigate the impact to the heritage structure.

7076 Old Mill Lane

- Removal of the current; front porch, rear extension, rear deck and aluminum siding (Appendix 3 and 4),
- Demolition of the existing frame garage and replacement with a barge board garage;
- Restoration of the original stucco finish, front bay window and pierced gingerbread barge board in the gable;
- Addition of a new, one and half storey extension, covered porch, large rear deck , Victorian dormer with gothic window
- Replacement of granular driveway with a permeable driveway.

These proposed changes are in keeping with best practice for heritage properties and comply with the Meadowvale Village HCD Plan. The Meadowvale Village Heritage Committee has reviewed and approved this application as of March 10, 2020.

Financial Impact

There is no financial impact resulting from the recommendation in this report.

Conclusion

The owners of the above properties seek permission to alter their designated properties. Because these proposed alterations minimize and mitigate the impacts to the cultural heritage value and interest these designated properties they should be approved.

Attachments

Appendix 1: Heritage Impact Assessment- 1234 Old River Road

Appendix 2: Sign Proposal- 62 Queen Street South

Appendix 3: Heritage Impact Assessment- 7076 Old Mill Lane

Appendix 4: Site Plan and Drawings- 7076 Old Mill Lane



Paul Mitcham, P.Eng, MBA, Commissioner of Community Services

Prepared by: John Dunlop, Supervisor, Heritage Planning

HERITAGE IMPACT ASSESSMENT



1234 OLD RIVER ROAD
MISSISSAUGA

06 March 2020

MEGAN HOBSON CAHP
M.A. DIPL. HERITAGE CONSERVATION
Built Heritage Consultant
mhobson@bell.net

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EXECUTIVE SUMMARY

The subject property contains a log cabin built by Robert Cotton in ... Originally the house had had a T-shaped frame addition attached at the rear that contained servants quarters. This addition was truncated in the mid-20th century. The remaining portion contains a family room on the ground floor and a bedroom with ensuite bathroom above. There are no original interior features. The only original features are the wood framing, wood cladding boards, and brick chimney. Due to the fact that this area was a service area, the upper floor is not full height and is very cramped. Structural investigation confirmed that the foundation and framing of the rear addition are not suitable for extending upward. It has been determined that the existing rear addition has limited heritage value and that the proposed addition is compatible with the historic log cabin that forms the main portion of the house. It is therefore recommended that the applicant be permitted to make the proposed changes. It is also recommended that the existing brick chimney be retained and incorporated into the new addition.

1.0 INTRODUCTION & METHODOLOGY

This report was prepared by heritage consultant Megan Hobson for the property owners of 1234 Old River Road as a requirement for obtaining a heritage permit to remove an existing 1.5-storey rear addition and construct a new 2-storey addition in the same location.

Site investigation and historical research were undertaken by the consultant. Heritage staff at the City of Mississauga provided property research undertaken by Heritage Mississauga. A structural investigation was carried out by structural engineer Michael Flynn and is included as an appendix to this report.

2.0 LOCATION & SITE DESCRIPTION

The subject property is located in a modern subdivision in Mississauga, on the east side of the Credit River, near the historic village of Port Credit. It fronts onto Old River Road between Inglewood Drive and Cotton Drive. The orientation of the house is slightly skewed and the lot is approximately three times the size of typical lots in the subdivision.



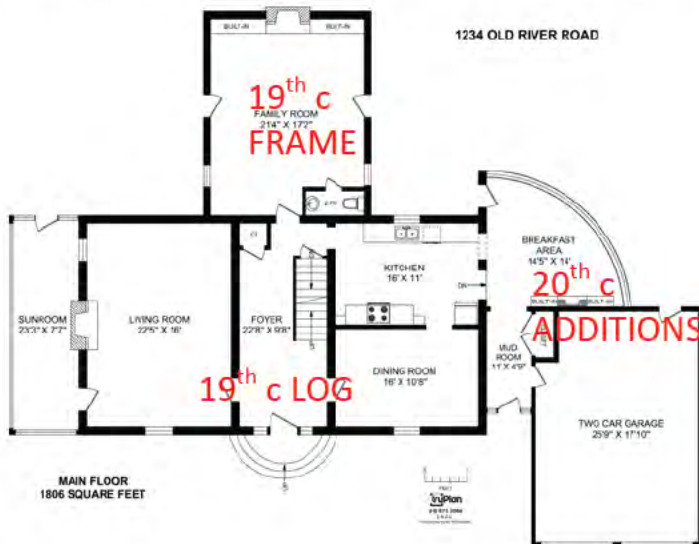
Location map & aerial view [Google]

The house is oriented towards Old River Road and has a T-shaped plan comprised of a two storey main section with a 1.5 storey tail. The front section is log construction. The rear section is frame construction. There are modern additions on the north side including a sunroom and a linked 2-car garage. The entire structure is clad with modern horizontal wood clapboard.

This property is Designated under Part IV of the *Ontario Heritage Act* and the house is known as the Cotton-Hawsworth House.



19th century log and frame construction with 20th century additions [Google]



2.0 CULTURAL HERITAGE VALUE

The Cotton-Hawsworth House is associated with Robert Cotton who emigrated from Ireland to Canada in 1837. Cotton was a well-known merchant and farmer in Toronto Township and held several offices in the community including Justice of the Peace in 1850. In 1856 Cotton purchased a large parcel of land that includes the subject property. Shortly after, he purchased and dismantled the old Mission House in the Credit Indian Village and brought it down river and had it rebuilt on his property. The house is one of the few remaining log buildings in Mississauga. The front of the property originally extended to the historic Centre Road (now Hurontario Street).

The building has undergone a number of later alterations and additions but still contains the rectangular log cabin and a portion of the rear frame addition built by Robert Cotton in the 1850s beneath modern wood siding.



1234 Old River Road – Main elevation (left) – Rear elevation (right)

The Cotton-Hawsworth House is a good representation of Georgian architecture. Typical of this style is the symmetry of the structure, the gable roof and gable end chimneys, the plain trim and the central front entrance. The three bay facade log house is sheathed in clapboard and a raised wooden or stringcourse divides the first and second storeys.

Character-defining elements that contribute to the heritage value of the Cotton-Hawsworth House include:

- two storey log and clapboard exterior
- stone foundation
- medium pitch gable roof
- symmetrical façade
- three brick chimneys
- central entrance with sidelights and transom
- coloured glass in transom
- six over six windows
- small paired quarter round windows in gable
- raised wooden string course between storeys

2.1 EVALUATION ACCORDING TO ONT. REG. 09/06

1234 OLD RIVER ROAD, MISSISSAUGA		
Criteria to Determine Cultural heritage value or interest	Assessment (Yes/No)	Rationale
1. Design or physical value:		
a) Is a rare, unique, representative or early example of a style, type, expression, material, or construction method	YES	<i>The property contains a rare example in Mississauga of a c. 1850 log cabin built from logs dismantled from an earlier structure that served as the Mission House on the Credit Indian Reserve.</i>
b) Displays a high degree of craftsmanship or artistic merit	NO	<i>It displays a moderate degree of craftsmanship as an example of log construction in the mid-19th century.</i>
c) Demonstrates a high degree of technical or scientific achievement	NO	<i>It demonstrates a moderate degree of technical achievement as an example of a log meeting house that was dismantled, relocated and rebuilt as a 2-storey residence in the mid-19th century.</i>
2. Historical or associative value:		
a) Has direct associations with a theme, event, belief, person, activity, organization, or institution that is significant to a community	YES	<i>It has direct associations with Robert Cotton and his descendants, a prominent family that has significance to the Port Credit community. This was the Cotton Homestead.</i>
b) Yields, or has potential to yield, information that contributes to an understanding of a community or culture	YES	<i>The property contributes to an understanding of the role of the Robert Cotton and his descendants in the early settlement and development of Port Credit. It has the potential to yield information about the Mission House formerly located on the Credit Indian Reserve.</i>
c) Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community	NO	<i>The property contains a vernacular building that was built by an unknown builder.</i>
3. Contextual value:		
a) Is important in defining, maintaining, or supporting the character of an area	NO	<i>It was originally part of a larger farmstead. It is associated with early settlement patterns and 19th century agricultural activities that have been largely erased by 20th century development.</i>
b) Is physically, functionally, visually, or historically linked to its surroundings	NO	<i>It is physically and historically linked to the Credit River and historic roads. Links to the Credit River have been severed by later development. The alignment of the house does not conform with the modern street pattern and is linked to the alignment of earlier roads.</i>
c) Is a landmark	YES	<i>The house is well known in the local community of Port Credit as the Cotton homestead.</i>

3.0 BUILDING EVOLUTION & CURRENT CONDITIONS

The Cotton-Hawsworth House has been subject to a number of alterations and additions. Historic photographs indicate that there was a porch across the front of the log cabin that wrapped around to the side elevation. The current sun porch on the side elevation is part of this earlier porch. The 'tent roof' has been retained and the sides have been enclosed.

Historic photos and fire insurance maps show a large rear addition and that gave the house an unusual H-shaped plan. This addition contained servants' quarters and the roofline is lower than the main house. In 1980 this addition was truncated and a large portion was demolished. The remnant that was retained is a 1.5 storey rectangular structure that contains one room on the lower level and one room on the upper level. Unlike the main house that is log construction with a full basement and stone foundation walls, the rear addition is frame construction and does not have a basement. The roof of the rear addition is independent of the roof over the main house. The addition is linked internally through a doorway at the back of the main hallway on the ground floor and through a doorway on the stair landing.

In the later 20th century the house was restored and large additions were built including the semi-circular solarium and garage. A non-historic entry porch on the main elevation was removed and new wood cladding and wood window shutters were installed at this time.



Historic photos, c. 1900 showing verandah and T-shaped rear addition



Current conditions – 2-bays and one brick chimney is all that remains of the T-shaped rear addition

4.0 PROPOSED ALTERATIONS

The owners need additional space for a master bedroom and ensuite in the addition. Currently the roof is low and sloped and the bathroom is very small and does not have any windows. A structural investigation determined that extending the addition upwards was not feasible because the existing rear addition does not have an adequate foundation. There is an un-insulated crawl space below the addition that is not structurally tied into the stone foundation of the main portion of the house. It was also determined that the exterior walls of the addition are constructed of wood framing that would have to be significantly reinforced to meet modern building code requirements. It was also noted that there was a significant amount of rot due to long term exposure to damp conditions.

Based on the findings of the structural investigation, the owners are proposing to tear down the old addition and build a new addition in the same location. The height of the roof will be increased so that the 2nd floor can be full height and more consistent with the height of the main portion of the house. The addition will be structurally independent of the main house and existing doorways into the addition will be maintained so that no new openings into the log walls of the main house will be required. The new addition will include a full basement. A new opening will be cut through the foundation of the main house to provide a connection into the new basement.



Left: ground floor of the existing rear addition – no original interior features or finishes
 Right: upper floor of the existing rear addition – cramped conditions due to low roofline

5.0 IMPACT ASSESSMENT

The existing 1.5 storey rear addition is a remnant of a larger addition that was built c. 1850. This was a service area and is constructed without a basement and does not have a full height second floor. The doors and windows are not original and the exterior has been re-clad with modern wood siding. There are no original interior features. The only original features are the wood framing, exterior wallboards and brick chimney. The addition is located at the rear and is not visible from the front of the property. Removal of the addition will not have a significant impact on heritage values or on the structural integrity of the main portion of the house.

The proposed addition will be located in the same location as an existing addition. The form and architectural character will be very similar to the existing rear addition and is compatible

with the main portion of the house. The roof will be slightly higher than the existing addition but will not be visible from the front of the property.

6.0 CONSERVATION STRATEGY

The conservation strategy for this property is to conserve the log cabin that forms the main portion of the house and to ensure ongoing use of this building as a well-maintained residence. The proposed alterations will have no negative impacts on the main portion of the house and will not be visible from the front of the property. The proposed removal of the historic addition at the rear has been mitigated through research and documentation contained in this report. Given the poor condition of the addition, the damp conditions in the crawlspace, and the lack of original features, retention of the addition is not recommended. The proposal also includes mitigation in the form of design measures so that the new addition will be compatible with the character and materials of the main portion of the house.



Left: wood framing and wood plank walls

Right: evidence of rot in floor framing

7.0 CONCLUSIONS & RECOMMENDATIONS

It is recommended that the proposed alterations, including removal of the existing 1.5-storey rear addition and construction of a new 2-storey rear addition be approved. It is also recommended that the owners do the following:

- Retain the existing brick chimney and incorporate it into the new addition
- Provide heritage staff with further details related to the salvage and re-building of the chimney

8.0 QUALIFICATIONS OF THE AUTHOR

The author of this report is a professional member of the Canadian Association of Heritage Professionals. Formal education includes a Master of Arts in Architectural History from the University of Toronto and a diploma in Heritage Conservation from the Willowbank School of Restoration Arts. Professional experience includes three years as Architectural Historian and

Conservation Specialist at *Taylor Hazell Architects* in Toronto, and 8 years in private practice in Ontario as a heritage consultant. Other relevant experience includes teaching art history at the *University of Toronto* and *McMaster University* and teaching Research Methods and Conservation Planning at the *Willowbank School for Restoration Arts* in Queenston. In addition to numerous heritage reports, the author has published work in academic journals such as the *Journal of the Society for the Study of Architecture in Canada* and the *Canadian Historical Review*.

9.0 SOURCES

Hicks, Kathleen A. *Port Credit; past to present*. (Mississauga, Mississauga Library, 2007)

Jones, Elizabeth. *Memoir of Elizabeth Jones: A little Indian girl who lived at the River-Credit Mission, Upper Canada*. (London; J. Mason, 1838) Accessed online 10 August 2017
https://archive.org/details/ldpd_11264233_000

Mississauga Local Architectural Conservation Advisory Committee, "Historic Structures Report; Cotton-Hawksworth House", February 14, 1984. (Heritage Planning files)

Parks Canada, *Standards & Guidelines for the Conservation of Historic Places in Canada*, 2nd ed. (2010) Accessed online 27 Feb 2018 <http://www.historicplaces.ca/media/18072/81468-parks-s+g-eng-web2.pdf>

Perkins Bull Collection M5-515, 'Cotton Family' (Heritage Planning Files)

APPENDIX A: DESIGNATION BY-LAW

11.5.



BY-LAW NUMBER 516-85

To designate the "Cotton-Hawksworth House" located at 1234 River Road, Mississauga, as being of architectural value and of historical interest.

WHEREAS The Ontario Heritage Act, R.S.O. 1980, Chapter 337, authorizes the Council of a municipality to enact by-laws to designate real property including all the buildings and structures thereon, to be of historic or architectural value or interest; and

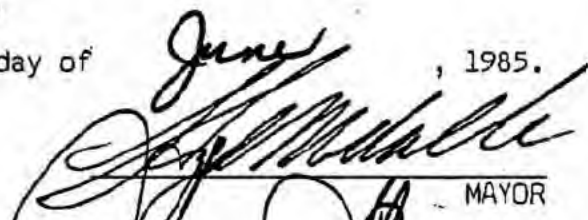
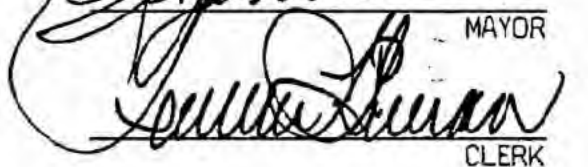
WHEREAS Notice of Intention to so designate the "Cotton-Hawksworth House" located at 1234 River Road, Mississauga, having been duly published and served and no notice of objection to such designation having been received by the Council of the Corporation of the City of Mississauga.

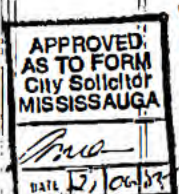
WHEREAS the reasons for the said designation are set out as Schedule 'A' hereto;

THEREFORE the Council of The Corporation of the City of Mississauga enacts as follows:

1. That the real property, more particularly described in Schedule 'B' hereto, known as the "Cotton-Hawksworth House" located at 1234 River Road, be designated as being of architectural value and historical interest.
2. That the City Clerk is hereby authorized to cause a copy of this by-law to be served upon the owner of the aforesaid property and upon the Ontario Heritage Foundation and to cause notice of this by-law to be published in a newspaper having general circulation in the City of Mississauga.
3. That the City Solicitor is hereby directed to register a copy of this by-law against the subject property.

ENACTED AND PASSED this 24th day of June, 1985.


MAYOR

CLERK



SCHEDULE 'A' TO BY-LAW NO. *516-85*SHORT STATEMENT OF THE REASONS FOR
THE PROPOSED DESIGNATION

It is recommended that the Cotton-Hawksworth House be listed on the Mississauga Heritage Inventory and considered for designation for its architectural and historical importance. Constructed in the 1850's of logs from the Credit Indian Village, the house is one of the few remaining log buildings in the City. Historically, the house was built by Robert Cotton, a well known 19th century merchant and farmer in Toronto Township. Now surrounded by a modern subdivision, the house remains a landmark within the Port Credit community.

SCHEDULE 'B' TO BY-LAW NO. 516.85

Description of Land: Part of Block 'A', Registered Plan 323

ALL AND SINGULAR that certain parcel or tract of land and premises situate, lying and being in the City of Mississauga, Regional Municipality of Peel (formerly in the Township of Toronto, County of Peel), Province of Ontario; and being composed of that portion of Block 'A', Registered Plan 323, designated as Part 2 on a Plan of Survey deposited in the Land Registry Office for the Registry Division of Peel as 43R-6925.

A handwritten signature in dark ink, appearing to read 'Ian D. Robinson', written over a horizontal line.

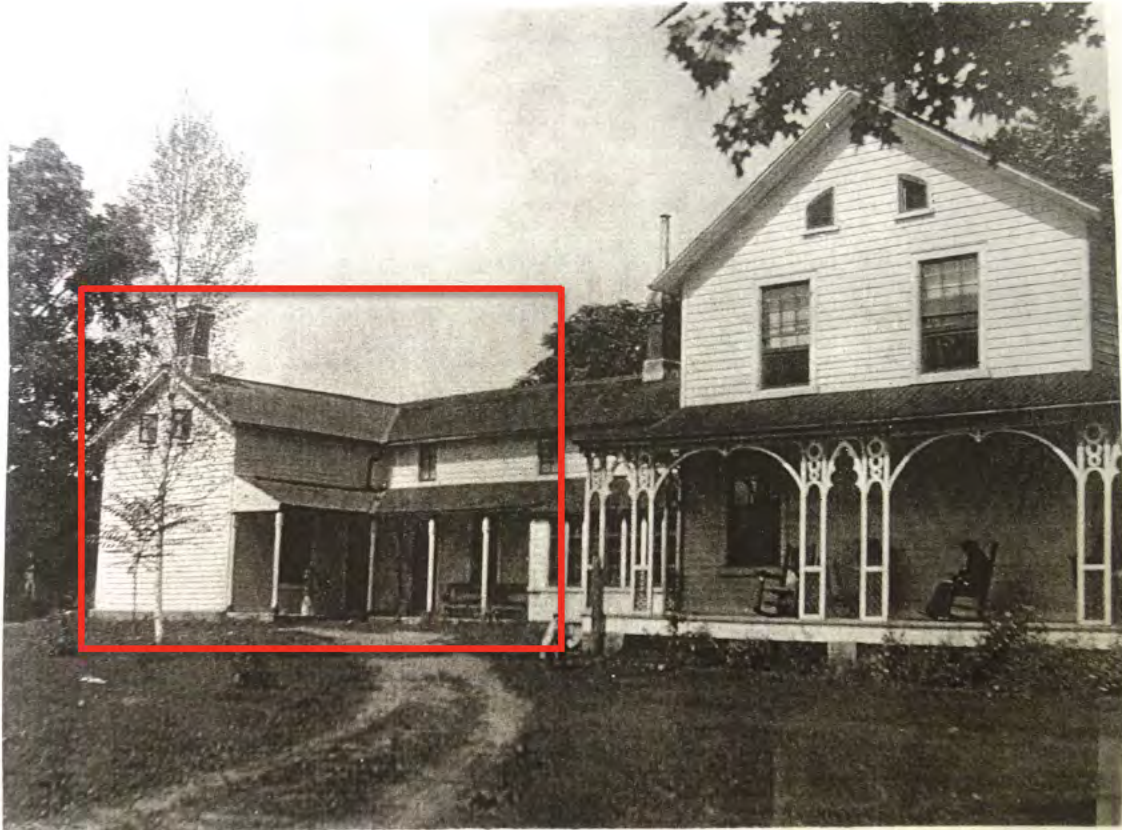
Ian D. Robinson,
Ontario Land Surveyor.



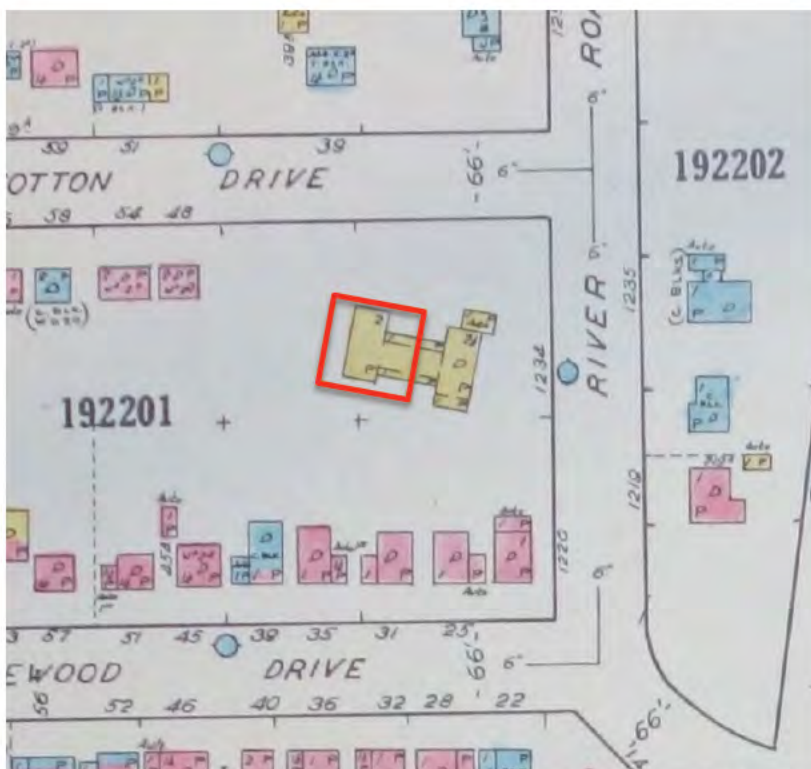
Sketch of log buildings at the Credit Indian Village [Briggs, 1883]



R.J. Cotton's house c. 1900 – rear addition that was removed in the 20th century outlined in red.
[Heritage Mississauga]



Rear addition that was removed in the 20th century outlined in red. [St. Peter's Church archives]



1952 Fire Insurance Plan – rear addition that was removed in the 20th century is outlined in red.



c. 1980s – the verandah along the front of the house has been removed – there is a small covered entry at the main door – the section of verandah along the side of the house has been enclosed – an attached garage has been added. [Heritage Mississauga]



c. 1990 – the entrance porch has been removed – new siding and window shutters have been installed [Heritage Mississauga]



1993 – the brick chimneys have been rebuilt – the earlier garage as been replaced by a breezeway and a larger garage in the same location [St. Peter's Church archives]

ABSTRACT OF TITLE

Lot 2, First Range
C. 1 R.

NUMBER OF INSTRUMENT	INSTRUMENT	DATE OF:		GRANTOR	GRANTED	QUANTITY OF LAND	CONSIDERATION	REMARKS
		DOCUMENT	REGISTRATION					
16026	B + S	27 Nov. 1867	5 Dec. 1867	Bank of Upper Canada	James W. Cotton		3870.00	all + O.L.
16027	Mortgage	27 Nov. 1867	5 Dec. 1867	James W. Cotton	Bank of Upper Canada		3096.00	"
379 G.R	Will	29 July 1884	24 Oct. 1885	James W. Cotton	Susan A. Cotton			all + O.L. 15
5840	B. + S.	1 Apr. 1856	4 Nov. 1886	The Crown	Robert Cotton		\$724,15.71	Part + O.L. 163 ac.

Cotton House
1234 River Rd.

ABSTRACT OF TITLE

Range 1, CLK, 1000

NUMBER OF INSTRUMENT	INSTRUMENT	DOCUMENT	DATE OF: REGISTRATION	GRANTOR	GRANTED	QUANTITY OF LAND	CONSIDERATION	REMARKS
532803	DM.	20 Oct. /79	25 Oct. /79	Ronald U. Toner Elaine Toner	William T. Hawksworth			Discharging 51557 US (p. 446)
461475	Grant	6 Jan /78	18 Jan /78	Sean A. Speakman	William T. Hawksworth		\$2.00 + c	Part as in no. 51556 US (p. 144)
453235	Grant	15 June /77	3 Nov. /77	Sean A. Speakman	Sean A. Speakman		\$2.00 + c	Part as in no. 51557 US
239978 US	Act of Mortgage	4971 25 Sept. 1972	1972 25 Oct. 1972	Victoria + Grey Trust Company	Ronald U. Toner + Elaine Toner as joint tenants		2.00	Discharging no. 51557 US, discharged by 532803
60511 US	DM	29 Sept. 1967	14 Dec. 1967	Victoria + Grey Trust Company	Ronald U. Toner + Elaine Toner			discharging no. 24 034 US
51557 US	Mortgage	11 Sept. 1967	15 Sept. 1967	Climbrose L. Speakman + Sean A. Speakman	Elaine Toner as joint tenants		20,000	Part as in no. 51556 US discharged by 532803
51556 US	Grant	29 June 1967	15 Sept. 1967	Ronald U. Toner + Elaine Toner	Climbrose L. Speakman + Sean A. Speakman, as joint tenants		2.00 + c	Part - com. at SEC, hence NW 105' x SW 150' x NE 200' for p. & c.
3218 US	DM	24 Jan. 1966	22 Feb. 1966	Victoria + Grey Trust Co.	Ronald U. Toner + Elaine Toner			discharging no. 133948

11.5.

ABSTRACT OF TITLE

NUMBER OF INSTRUMENT	INSTRUMENT	DOCUMENT	DATE OF: REGISTRATION	GRANTOR	GRANTED	QUANTITY OF LAND	CONSIDERATION	REMARKS
134051	D.M.	6 Dec. 1960	4 Jan 1961	Victoria + Grey Trust Co.	Ronald U. Torer et al.			discharging no. 92270
133998	7 Dec. 1960 Mortgage	9 Dec. 1960	28 Dec. 1960	Ronald U. Torer + Elaine Torer	Victoria + Grey Trust Co.		\$12,000	Part as in no. 92270
92270	Mortgage (not recorded in full)	15 Nov. 1955	21 Nov. 1955	Ronald U. Torer + Elaine Torer	Victoria + Grey Trust Co.		\$7,000	all except SW 1/4 50' of N 1/4 50'
78559	Grant	20 Nov. 1953	21 Dec. 1953	Elaine Torer + Ronald U. Torer	Paul Hedegard		\$1,000.00	Ely 150' of N 1/4 50'
73582	Grant	27 March 1953	21 April 1953	Elaine Torer + Ronald U. Torer	Herman Treasurer		\$1,000.00	Part - Subj 50'
45924	Grant	16 July 1945	1 Aug. 1945	Chas. A. Sandham	Elaine Torer + Ronald U. Torer, as joint tenants		\$7,500	all
4307 4307	Grant	24 June 1943	6 July 1943	F.J. Moore Construction Co. Ltd.	Chas. A. Sandham		\$4,000	all + O.L.
43007	Grant	31 March 1943	24 June 1943	Cyril E. Cotton et ux	F.J. Moore Construction Co. Ltd.		\$29,000	all + O.L.

ABSTRACT OF TITLE

NUMBER OF INSTRUMENT	INSTRUMENT	DATE OF:		GRANTOR	GRANTED	QUANTITY OF LAND	CONSIDERATION	REMARKS
		DOCUMENT	REGISTRATION					
43006	Grant	5 April 1943	24 June 1943	Dixie C. Cotton et ux.	Cyril E. Cotton	pt.	\$1.00 + C	ROL (see lot 2 for description)
323	Plan	3 May 1943	24 June 1943	Cyril E. Cotton et al.	a § subdivision			part + O.L.
43006	Grant	5 April 1943	24 June 1943	Dixie C. Cotton et ux.	Cyril E. Cotton	p.	\$1.00 + C	+ O.L.
14177	Grant	20 Oct Oct. 1910	26 Oct. 1910	Cyril E. Cotton beheher, Susan A. Cotton Widow	Dixie Cox Cotton	pt.	1	pt. + O.L.
5796 R.	Will	29 July 1884	24 Oct. 1885	Jas. W. Cotton	Susan A. Cotton			add + O.L.

ABSTRACT OF TITLE

NUMBER OF INSTRUMENT	INSTRUMENT	DOCUMENT	DATE OF: REGISTRATION	GRANTOR	GRANTED	QUANTITY OF LAND	CONSIDERATION	REMARKS
1119	D.M.	18 Dec. 1872	16 Jan 1873	C. Gamble	James W. Cotton			discharging no. 16027
16027	Mortgage	27 Nov. 1867	5 Dec. 1867	James W. Cotton	Bank of Upper Canada		\$ 3,096.00	all and O.L.
16026	B.T.S.	2 Nov. 1867	5 Dec. 1867	Bank of Upper Can.	James W. Cotton		\$ 3,870.00	all + O.L.
13861	D.P.	13 May 1865	17 Nov. 1865	Frederick W. Jarvis	Bank of Upper Canada			
30449	B.S.	13 Aug. 1856	14 Aug. 1856	Robert Cotton	Hamilton + Toronto N.Y.		\$ 929.10	
	Patent			The Crown				end



August 14, 2019

1234 Old River Road
Mississauga, ON
L5G 3G3

File No.: 16MF-585

Via email < >

Attention:

Re: Engineering Assessment – Evaluation of Attached Rear Addition

Client:

Loss Location: 1234 Old River Road, Port Credit, Ontario

Dear ,

Introduction:

Michael Flynn and Associates Ltd. (MFAL) was requested by to attend the above noted location and complete an independent evaluation of the feasibility of performing structural alterations to the roof of an attached rear addition. Our site investigation was completed by Stephen Côté, P.Eng., on June 19, 2019. At that time, the interior and exterior of the dwelling and attached rear addition were observed and documented to determine the achievability of increasing the ceiling height of the second floor master bedroom of the subject rear addition.

The report herein provides general background information of the subject dwelling and attached rear addition, and presents the findings of the initial site attendance. As discussed in this report, deficient foundation configurations (i.e. absent frost protection and inadequate support conditions) were discovered beneath the subject rear addition at the time of our site attendance. Absent ground cover and decay/degradation of exposed floor framing components were also observed within the crawlspace of the subject rear addition, suggesting the foundation and floor framing elements of the addition are inadequate to support the proposed structural alterations to the addition.

It is MFAL's opinion that reconstruction of only the roof system of the subject rear addition is not practical, given the existing construction configuration, deteriorated condition of framing members and inadequate foundation elements of the structure. MFAL recommends that the subject structure be systematically demolished and reconstructed in its entirety, to adequately support the replacement roof structure and meet relevant building codes.

It is relevant to note that in conjunction with the reconstruction of the subject addition, all architectural finishes (including roof surface, exterior finishes, interior floor finishes, etc.) can be

incorporated into the design of the replacement structure to ensure the replacement addition appears aesthetically consistent with that of appearances prior to reconstruction, including those of historic significance. It is noted that structural repair drawings will be required to be submitted to the local building department and the local historical building governing body for approval prior to any construction efforts. At this time, reconstruction plans for the addition have not been completed; however, MFAL would be pleased to prepare any necessary design and permit plans upon request.

Background Information:

The subject dwelling is a historic, two storey residential dwelling which, is located on Old River Road, in Port Credit, Ontario. The following is a brief history of the dwelling following limited research by MFAL.

The subject dwelling was reportedly originally constructed in 1856 and was designated by the city of Mississauga in 1985 for its heritage value, under part IV of the Ontario Heritage Act (by-law 516-85). The heritage house is a three bay facade, two storey building, with a medium pitch gable roof containing brick chimneys on each end of the gable roof. The historic portion of the dwelling measured approximately 36'-2" by 22'-8" and was observed to be log framed on a stone foundation, with clapboard exterior finishes. The subject two-storey, rear addition was observed to be balloon framed, supported by a foundation system that consisted of varying configurations. The subject rear addition measured approximately 21'-4" by 17'-2". A one storey wooden frame enclosed porch with a tent roof existed along the south elevation of the dwelling and measured approximately 23'-3" by 7'-7". A garage addition existed along the east elevation of the dwelling, measuring approximately 25'-9" by 17'-10" .

Scope of Investigation:

The investigation and evaluations completed by MFAL is limited to building structural systems and elements. Any review and recommendations relating to building plumbing, HVAC and electrical systems are to be coordinated and completed by others, if considered necessary. Landscaping features including site grading have also been omitted from the scope of this investigation.

In completing this investigation, MFAL completed the following:

- The general layout of the subject structure was observed and documented as necessary.
- Damaged elements were observed and documented to the extent possible in the absence of additional tear-out of interior finishes or removal of existing hoarding.
- Photographic documentation of the above was obtained.
- Limited Research into the history of the subject Heritage Home.
- Discussion with the Client were undertaken.

This report does not provide a comprehensive review of the structure in its entirety and does not address items that were concealed at the time of our investigation. This report has been prepared to satisfy the requirement of the client(s) for which it was prepared.

No third party should rely on this report without prior written consent of the undersigned.

Investigation Observations and Evaluations:

All photographs referred to within this report have been included as Appendices 'A' and 'B' of this report.

For reference purposes, Site Photographs (taken by MFAL) have been included as Appendix 'A' (Photographs No. 1 to No. 19) and Photographs provided by the owner (Photographs No. 20 to No. 23) are provided as Appendix 'B'.

Photograph No. 1 to No. 7: General View of the Subject Dwelling Exterior:

Photographs No. 1 to No. 7 provide general exterior views of the subject dwelling. For references purposes, the front elevation of dwelling shown in Photograph No. 1 faces south and will be herein referred to as the South Elevation, with remaining building elevations named relatively. The subject rear addition of the dwelling is shown in Photographs No. 3 to No. 7.

Photographs No. 8 to No. 14: General Interior View of Subject Rear Addition of Dwelling:

Photographs No. 8 to No. 14 provide general interior views of the subject rear addition of the dwelling. Specifically, Photographs No. 8 and No. 9 provide interior views of the main level of the addition, Photographs No. 10 to No. 13 provide interior views of the second level of the addition and Photograph No. 14 provides an interior view of the attic space of the subject addition.

Photograph No. 15 to No. 19: Deteriorated Building Materials along Base of Foundation Wall:

Photographs No. 15 to No. 19 provide views of the exposed floor framing and foundation elements of the subject rear addition. The floor framing along the exterior of the addition was observed to be deteriorated to varying degrees and in some areas was found to be no longer existent as a result of deterioration. Photographs No. 17 to No. 19 provide views of the degraded floor framing members observed to be in an extremely advanced state of decay along the exterior of the addition.

The observed magnitude of decay of wood members is a result of long term, exposure to elevated moisture conditions. In several areas, wood framing was observed to be directly in contact with the concrete foundation elements, absent a moisture barrier and the crawlspace was not equipped with a suitable ground sheet to prevent elevated moisture conditions. The observed conditions are prone to long-term decay and deterioration of wood framing members.

As stipulated in Article 9.23.2.3. of the current Ontario Building Code, wood framing members must extend at least six (6) inches (150mm) above grade or be separated from foundation elements by 0.05mm of polyethylene film, to prevent exposure of floor framing components to elevated moisture conditions within the crawlspace.

“9.23.2.3. Protection from Dampness

(1) Except as permitted in Sentence (2), wood framing members that are not pressure-treated with a wood preservative and that are supported on concrete in contact with the ground or *fill* shall be separated from the concrete by not less than 0.05 mm polyethylene film or Type S roll roofing.

(2) Dampproofing material referred to in Sentence (1) is not required where the wood member is at least 150 mm above the ground.

At a minimum, MFAL recommends that the full extent of deteriorated floor framing be removed and repaired/reconstructed; however, due to the existing construction configurations and inadequate foundation elements of the structure, systematic demolition and reconstruction of the rear addition is recommended.

Photographs No. 20 to No. 23: Views of Inadequate Foundation System:

Photographs No. 20 to No. 23 in Appendix ‘B’ provide views interior views of the crawlspace and the foundation elements of the subject rear addition. At the time of our site attendance, the foundation piers consisted of various combinations of concrete block and patio stones, absent of connection to the main level floor framing and absent frost protection. Photograph No. 21 provides a view of the connection between the foundation piers and the main level floor framing of the rear addition. The crawlspace of the addition was also noted to be unheated, exposing the piers to seasonal movement and movement as a result of frost heave.

At a minimum, MFAL recommends that the foundation elements are removed and replaced with adequate piers and footings that satisfy the requirements of the current 2012 Ontario Building Code. As per Subsection 9.12.2 and Article 9.15.3.2. of Division B of the 2012 Ontario Building Code, the replacement footings shall be constructed to extend beyond the depth of frost penetration or appropriately insulated and bear directly on undisturbed soil, bed rock or compacted fill:

“9.12.2. Depth

9.12.2.1. Excavation to Undisturbed Soil

(1) Excavations for foundations shall extend to undisturbed soil.

9.12.2.2. Minimum Depth of Foundations

(1) Except as provided in Sentences (4) and (5), the minimum depth of foundations below finished ground level shall conform to Table 9.12.2.2.”

Table 9.12.2.2.
Minimum Depths of Foundations
 Forming Part of Sentence 9.12.2.2.(1)

Item	Column 1	Column 2	Column 3	Column 4	Column 5
	Type of Soil	Minimum Depth of Foundation Containing Heated Basement or Crawl Space⁽¹⁾		Minimum Depth of Foundation Containing no Heated Space⁽²⁾	
		Good Soil Drainage	Poor Soil Drainage	Good Soil Drainage	Poor Soil Drainage
1.	Rock	No limit	No limit	No limit	No limit
2.	Coarse grained soils	No limit	No limit	No limit	Below the depth of frost penetration
3.	Silt	No limit	No limit	Below the depth of frost penetration ⁽³⁾	Below the depth of frost penetration
4.	Clay or soils not clearly defined	1.2 m ⁽³⁾	1.2 m	1.2 m but not less than the depth of frost penetration ⁽³⁾	1.2 m but not less than the depth of frost penetration

Notes to Table 9.12.2.2.:

⁽¹⁾ Foundation not insulated to reduce heat loss through the footings.

⁽²⁾ Including foundations containing heated space insulated to reduce heat loss through the footings.

⁽³⁾ Good soil drainage to not less than the depth of frost penetration.

And,

“9.15.3.2. Support of Footings

(1) Footings shall rest on undisturbed *soil*, *rock* or compacted granular *fill*.

(2) Granular *fill* shall not contain pyritic material in a concentration that would adversely affect its stability or the performance of assemblies separating dissimilar environments.”

As discussed herein, it is recommended the existing foundation elements are removed and replaced with a foundation system that satisfies the requirements of the current OBC and adequately supports a replacement structure; however, systematic demolition and reconstruction is considered necessary in order to facilitate the proposed revisions to the subject roof system.

Opinions and Recommendations:

As discussed herein, MFAL is of the opinion that reconstruction of only the roof system of the subject rear addition is not practical, given the existing deficient construction configurations of the addition and inadequate foundation elements of the structure. MFAL recommends that the subject structure be systematically demolished and reconstructed in its entirety, to adequately support the replacement structure and meet relevant building codes.

It is relevant to note that in conjunction with the reconstruction of the subject addition, all architectural finishes (including roof surface, exterior finishes, interior floor finishes, etc.) can be incorporated into the design to ensure the replacement addition appears aesthetically consistent with that of appearances prior to reconstruction. Where modern materials are necessary for use in reconstructing the subject addition, the intent is for them to be concealed as to not compromise the historic nature of the dwelling.

Additionally, it is noted that independent of the desires of the owner, collapse of the main level floor framing is expected to occur, absent of reconstruction of the floor framing and foundation elements. Reconstruction of the rear addition in its entirety is considered necessary to facilitate the reconstruction of the floor framing, foundation elements and intrinsically, the revision of the rear addition roof system.

Prior to the completion of any repair efforts detailed design must be completed by a qualified designer or Professional Engineer. It will be necessary that a building permit is obtained by the owner or the agent there of prior to any construction. MFAL would be pleased to prepare any necessary design and permit plans upon request.

Closing Remarks:

We trust you will find the above satisfactory for your purposes at this time. If you have any questions or require any additional assistance with this file, do not hesitate to contact the undersigned. As discussed, prior to any repair work, a building permit will be required. MFAL will await further direction before proceeding with any additional work on this file.

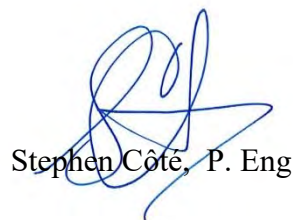
Regards,



Jacob D. Lemcke, EIT.

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Stephen Côté, P. Eng



Appendix ‘A’

Site Photographs
Date Taken: March 29, 2017



Photograph No. 1 – General View of Front (South) Elevation of Building



Photograph No. 2 – General View of Building Exterior (West Elevation)



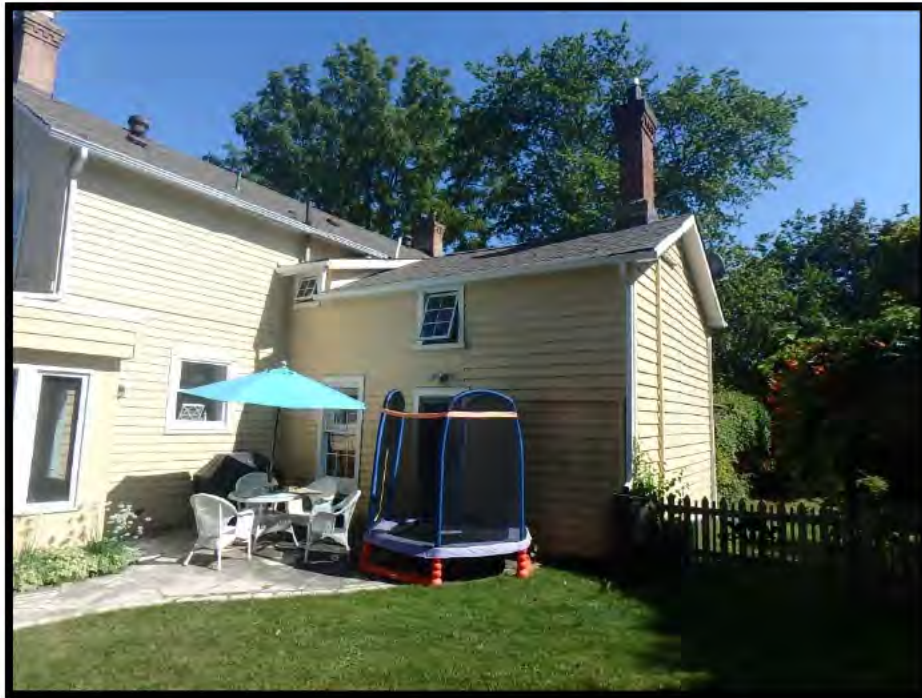


Photograph No. 3 – Partial View of Rear North Elevation at West Corner



Photograph No. 4 – General View of Rear North Elevation of Subject Rear Addition





Photograph No. 5 – General Exterior View of East Elevation of Rear Addition

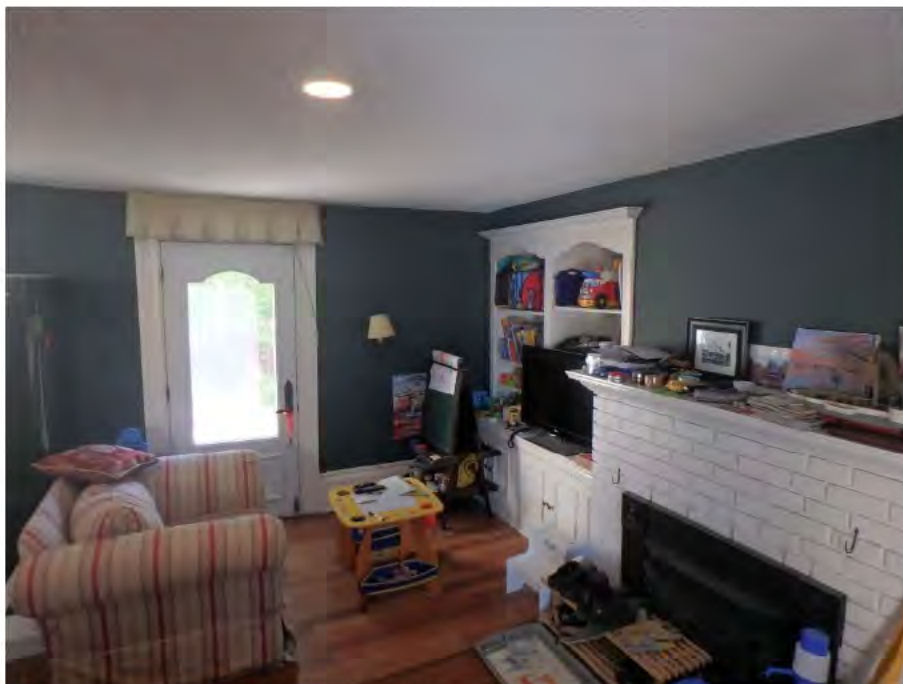


Photograph No. 6– Partial View of Rear Elevation of Dwelling and Attached East Additions



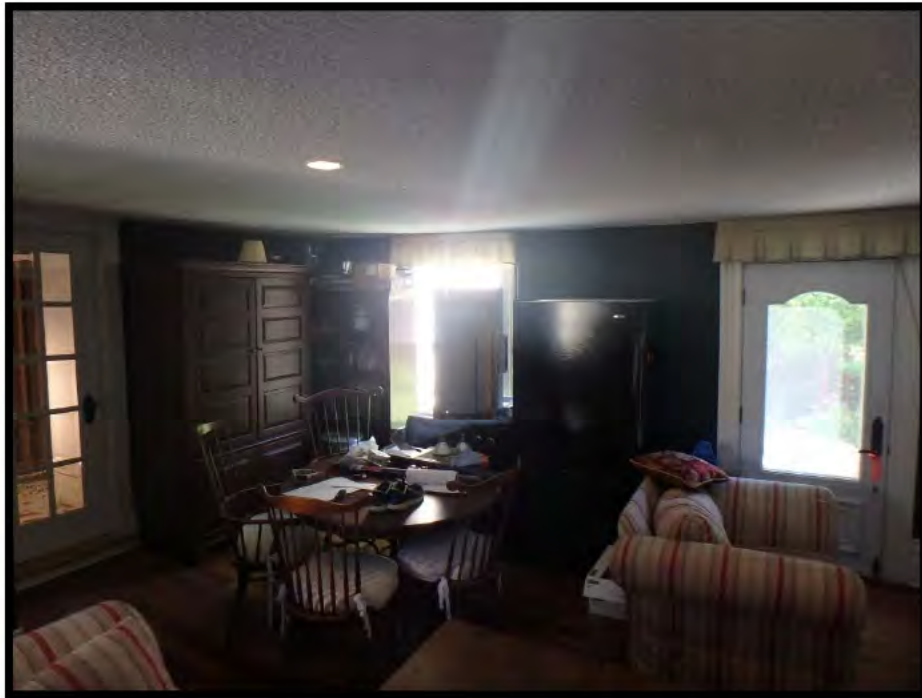


Photograph No. 7– General Exterior View of Subject Rear Addition Entrance

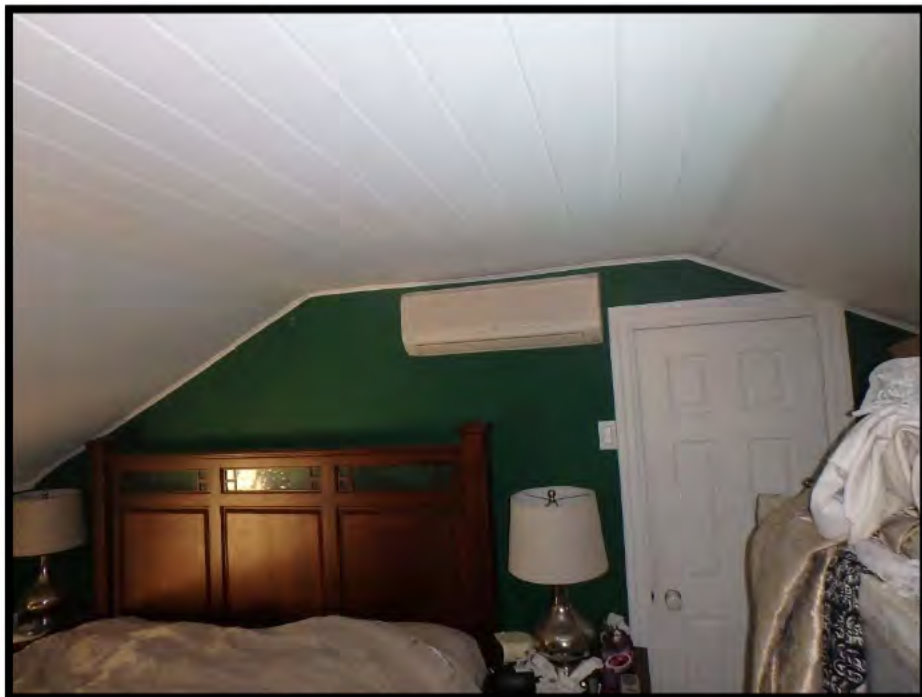


Photograph No. 8– General Interior View of Main Level of Subject Rear Addition



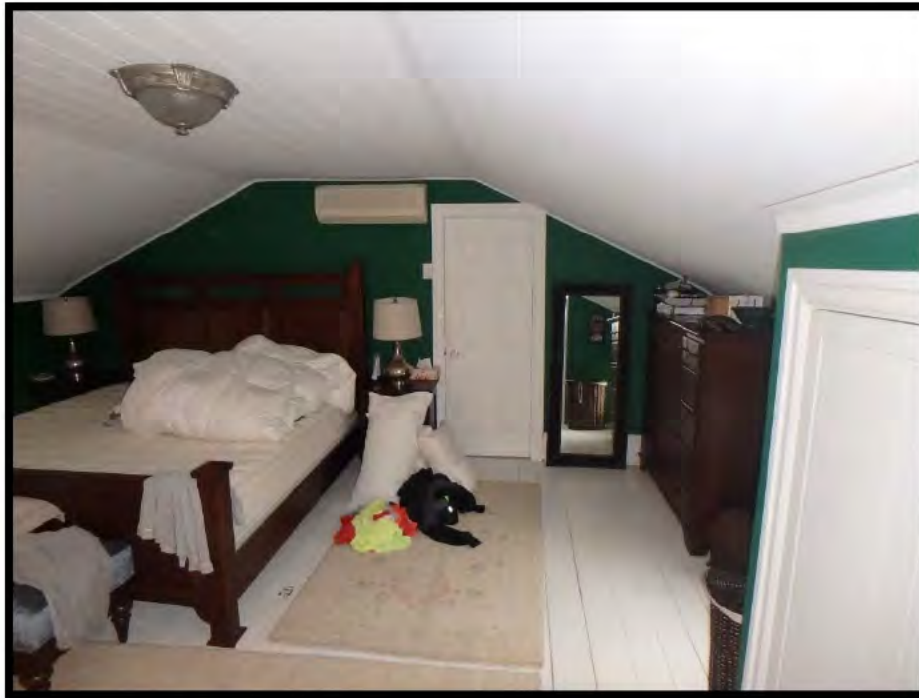


Photograph No. 9– General Interior View of Main Level of Subject Rear Addition



Photograph No. 10 – General Interior View of Second Level of Subject Rear Addition



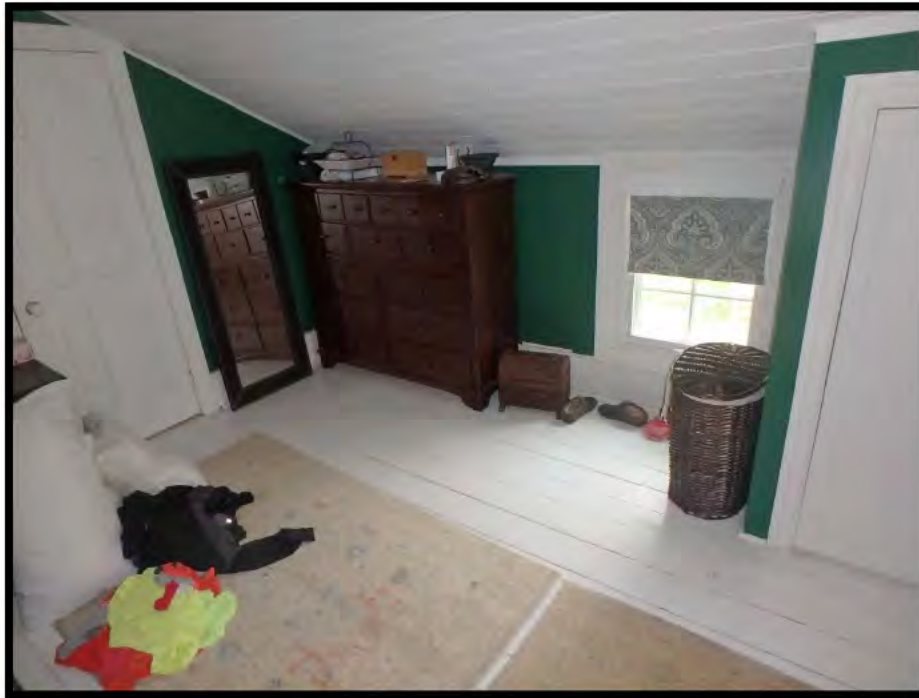


Photograph No. 11 – General Interior View of Second Level of Subject Rear Addition



Photograph No. 12 – General Interior View of Second Level of Subject Rear Addition





Photograph No. 13 – General Interior View of Second Level of Subject Rear Addition



Photograph No. 14 – General Interior View of Attic Space of Subject Rear Addition





Photograph No. 15 – View of 8” Concrete Block Foundation Elements of Rear Addition



Photograph No. 16 – View of 8” Concrete Block Foundation Elements of Rear Addition





Photograph No. 17 – View of Degradation of Floor Framing of Rear Addition



Photograph No. 18 – View of Degradation of Floor Framing of Rear Addition





Photograph No. 19 – View of Degradation of Floor Framing of Rear Addition



Appendix ‘B’

**Site Photographs
Provided By Owner**





Under North Door looking towards main house



Under North door looking West



Opening North side of rear extension



Underneath North door looking South across extension

GENERAL REQUIREMENTS

- THESE PLANS HAVE BEEN PREPARED FOR THE PURPOSE OF DETAILING ALTERATIONS / RENOVATIONS TO A DWELLING AT THE LOCATION ADDRESSED IN THE TITLE BLOCK BELOW.
- DIMENSIONS AND ALL OTHER INFORMATION SHOWN AND DESCRIBED SHALL BE VERIFIED ON-SITE BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF ANY WORK.
- DISCREPANCIES SHALL BE REPORTED TO MICHAEL FLYNN & ASSOCIATES LTD. (MFAL)
- DRAWINGS SHALL NOT BE SCALED UNLESS OTHERWISE NOTED.
- THE SCOPE OF THE WORK OUTLINED ON THESE DRAWINGS SATISFIES THE MINIMUM REQUIREMENTS OF THE ONTARIO BUILDING CODE (O.B.C.).
- MICHAEL FLYNN & ASSOCIATES LTD. MUST BE NOTIFIED OF ANY CHANGES PRIOR TO COMMENCING WITH THE WORK.
- ELECTRICAL, HVAC, PLUMBING AND SEPTIC SYSTEM DESIGNS BY OTHERS, COORDINATE WITH ALL SUB-TRADES (INCLUDING MECHANICAL AND ELECTRICAL) REGARDING PROVISIONS FOR ALL BUILT-INS.
- THE REQUIREMENTS OF DIVISION 'C', PART 1, ARTICLE 1.2.2.1 "GENERAL REVIEW BY ARCHITECT OR PROFESSIONAL ENGINEER" MAY APPLY TO THE PROPOSED WORK DETAILED ON THESE PLANS. CONTACT MFAL FOR ALL REQUIRED INSPECTIONS AS PER THE PRESCRIBED NOTICES IF A "COMMITMENT TO GENERAL REVIEW" FORM HAS BEEN ISSUED WITH THESE PLANS.

APPLICATION

- AS PER ARTICLE 1.1.2.4., DIVISION A, PART 1 OF THE 2012 ONTARIO BUILDING CODE (OBC); PART 9, DIVISION B (OBC) IS APPLICABLE TO THE BUILDING DETAILED HEREIN.

USE AND OCCUPANCY

- GROUP C, RESIDENTIAL
- BUILDING HEIGHT = +/- 28' 7"
- BUILDING AREA = +/- 2255 sq. ft.
- BUILDING IS GREATER THAN 5 YEARS OLD
- NO CHANGE OF USE IS PROPOSED

DESIGN STANDARDS

- PROPOSED CONSTRUCTION CONFORMS TO PART 9 OBC 2012.
- CONCRETE DESIGNS CONFORM TO CAN/CSA A23.3-14.
- STRUCTURAL STEEL DESIGN CONFORMS TO CAN/CSA S16-14.
- WHERE EXISTING ASSEMBLIES ARE TO BE REPAIRED: PROPOSED REPAIRS CONFORM TO ARTICLE 11.3.1.1., MATERIAL ALTERATION OR REPAIR OF A BUILDING SYSTEM.

DESIGN LOADS

- GROUND SNOW LOAD Ss 0.90 kPa (MISSISSAUGA, PORT CREDIT)
- GROUND RAIN LOAD Sr 0.40 kPa
- LIVE LOAD (DUE TO SNOW) LL 0.90 kPa

CONSTRUCTION NOTES

- THE CONTRACTOR SHALL COMPLY WITH THE 2012 ONTARIO BUILDING CODE, DIVISION B, PART 9. THE FOLLOWING NOTES ARE FOR QUICK REFERENCE ONLY WHERE NEW CONSTRUCTION IS REQUIRED.
- ALL REFERENCES TO SECTIONS, SUBSECTIONS, ARTICLES, AND SENTENCES REFER TO THE 2012 ONTARIO BUILDING CODE, DIVISION B.

SMOKE ALARMS

- SMOKE ALARMS SHALL CONFORM TO ARTICLE 9.10.19.
- SMOKE ALARMS CONFORMING TO CAN/ULC-S531, "SMOKE ALARMS", SHALL BE INSTALLED IN EACH FLOOR OF A DWELLING AND IN EACH BEDROOM.
- WITHIN DWELLINGS SUFFICIENT SMOKE ALARMS SHALL BE INSTALLED SO THAT THERE IS AT LEAST ONE SMOKE ALARM ON EACH FLOOR LEVEL, INCLUDING BASEMENTS, THAT IS 900 MM OR MORE ABOVE OR BELOW AN ADJACENT FLOOR LEVEL.
- SMOKE ALARMS ARE REQUIRED BY THE BUILDING CODE TO BE PROVIDED ON EVERY FLOOR AND IN EVERY SLEEPING ROOM OF RESIDENTIAL BUILDINGS, INCLUDING ALL HOUSES.
- IF A SLEEPING ROOM IS SERVED BY A HALLWAY, A SMOKE ALARM SHALL BE INSTALLED IN THE HALLWAY.
- ALL SMOKE ALARMS ARE REQUIRED TO INCLUDE A VISUAL COMPONENT CONFORMING TO NATIONAL FIRE PROTECTION ASSOCIATION STANDARDS.

CARBON MONOXIDE DETECTORS

- CARBON MONOXIDE DETECTORS SHALL CONFORM TO SUBSECTION 9.33.4.
- WHERE A FUEL-BURNING APPLIANCE IS INSTALLED IN A SUITE OF RESIDENTIAL OCCUPANCY, A CARBON MONOXIDE DETECTOR SHALL BE INSTALLED ADJACENT TO EACH SLEEPING AREA IN THE SUITE.
- WHERE A FUEL-BURNING APPLIANCE IS INSTALLED IN A SERVICE ROOM THAT IS NOT IN A SUITE OF RESIDENTIAL OCCUPANCY, A CARBON MONOXIDE DETECTOR SHALL BE INSTALLED, ADJACENT TO EACH SLEEPING AREA IN EVERY SUITE OF RESIDENTIAL OCCUPANCY THAT IS ADJACENT TO THE SERVICE ROOM, AND IN THE SERVICE ROOM.

CAULKING

- CAULKING SHALL CONFORM TO SUBSECTION 9.27.4.
- CAULKING SHALL BE PROVIDED WHERE REQUIRED TO PREVENT THE ENTRY OF WATER INTO THE STRUCTURE.
- CAULKING SHALL BE PROVIDED BETWEEN MASONRY, SIDING OR STUCCO AND THE ADJACENT DOOR AND WINDOW FRAMES OR TRIM, INCLUDING SILLS UNLESS SUCH LOCATIONS ARE COMPLETELY PROTECTED FROM THE ENTRY OF RAIN.
- CAULKING SHALL BE PROVIDED AT VERTICAL JOINTS BETWEEN DIFFERENT CLADDING MATERIALS UNLESS THE JOINT IS SUITABLY LAPPED OR FLASHED TO PREVENT THE ENTRY OF RAIN.

CONTRACTOR NOTES

- CONTRACTOR TO COMPLY WITH ALL PROVISIONS OF THE ONTARIO BUILDING CODE, LATEST EDITION, IN ALL AREAS OF CONSTRUCTION.
- ALL PRODUCTS SPECIFIED (OR APPROVED EQUIVALENTS) TO BE INSTALLED AS PER THE CERTIFIED MANUFACTURER'S INSTRUCTIONS AND ANY APPLICABLE CODE REQUIREMENTS.
- CONTRACTOR IS RESPONSIBLE FOR ALL TEMPORARY BRACING, SHORING AND WEATHER PROTECTION DURING ALL STAGES OF CONSTRUCTION NOT SPECIFICALLY DETAILED BY THESE PLANS.
- WHERE GENERAL REVIEW BY AN ENGINEER IS REQUESTED BY THE CHIEF BUILDING OFFICIAL OR OTHER PARTY, THE CONTRACTOR MUST REQUEST A "COMMITMENT TO GENERAL REVIEW" FORM FROM MFAL PRIOR TO THE COMMENCEMENT OF WORK.
- THE CONTRACTOR SHALL CONTACT MICHAEL FLYNN & ASSOCIATES LTD. FOR INSPECTION AT ALL STAGES PRESCRIBED IN O.B.C. 2012, DIV C, PART 1 ARTICLE 1.3.5.1. NO WORK IS TO CONTINUE UNTIL APPROVAL IS OBTAINED FROM MFAL AND LOCAL BUILDING AUTHORITY.
- ALL INQUIRIES WITH RESPECT TO CLARIFICATION OR EXPANSION OF THE ENCLOSED SPECIFICATIONS MUST BE FORWARDED TO MFAL PRIOR TO COMMENCING ANY WORK.
- ANY CONFLICT BETWEEN THE ENCLOSED SPECIFICATIONS AND APPLICABLE STATUTES OR REGULATIONS MUST BE REPORTED TO MFAL IMMEDIATELY UPON DISCOVERY. CLARIFICATION MUST BE RECEIVED BEFORE COMMENCING OR CONTINUING CONSTRUCTION.
- CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS PRIOR TO COMMENCEMENT OF THE WORK. A COPY OF ALL OBTAINED PERMITS TO BE FORWARDED TO MFAL BEFORE WORK IS COMMENCED.



SUBJECT DWELLING
SITE PHOTOGRAPH OBTAINED BY OWNER
DATED DECEMBER, 2016

DRIP FLASHING ABOVE WINDOWS AND DOORS AND BENEATH WINDOW SILLS

- DRIP FLASHING BENEATH WINDOW SILLS AND ABOVE DOORS AND WINDOWS SHALL CONFORM TO ARTICLE 9.20.13.12.
- EXCEPT FOR WALL OPENINGS LOCATED LESS THAN 150 MM ABOVE GROUND LEVEL, WHERE A CONCEALED FLASHING IS NOT INSTALLED BENEATH WINDOW AND DOOR SILLS, SUCH SILLS SHALL BE PROVIDED WITH AN OUTWARD SLOPE AND A DRIP LOCATED NOT LESS THAN 25 MM FROM THE WALL SURFACE.

EAVE PROTECTION FOR SHINGLES AND SHAKES

- EAVE PROTECTION FOR SHINGLES AND SHAKES SHALL CONFORM TO SUBSECTION 9.26.5.
- EAVE PROTECTION SHALL BE PROVIDED ON SHINGLE ROOFS, EXTENDING FROM THE EDGE OF THE ROOF A MINIMUM OF 6'-0" UP THE ROOF SLOPE.
- EAVE PROTECTION IS NOT REQUIRED OVER UNHEATED GARAGES, CARPORTS AND PORCHES.

VALLEY FLASHING

- VALLEY FLASHING SHALL CONFORM TO ARTICLE 9.26.4.3.
- CLOSED VALLEY FLASHING SHALL CONSIST OF SHEET METAL, SELF SEALING COMPOSITE MEMBRANES CONSISTING OF POLYETHYLENE AND BITUMINOUS MATERIAL OR ONE LAYER OF EITHER TYPE S SMOOTH SURFACE ROLL ROOFING OR TYPE M MINERAL SURFACE ROLL ROOFING (MINERAL SURFACE DOWN) NOT LESS THAN 600 MM WIDE, AND NAILS SHALL NOT PENETRATE THE FLASHING WITHIN 12" OF THE BOTTOM OF THE VALLEY CENTRELINE.
- OPEN VALLEYS SHALL BE FLASHED WITH AT LEAST ONE LAYER OF SHEET METAL NOT LESS THAN 600 MM WIDE, OR NO FEWER THAN 2 LAYERS OF ROLL ROOFING MEMBRANE.

VENTING

- VENTING SHALL CONFORM TO SUBSECTION 9.19.1.
- WHERE INSULATION IS INSTALLED BETWEEN A CEILING AND THE UNDERSIDE OF THE ROOF SHEATHING; AT EACH RAFTER SPACE INSTALL BETWEEN THE INSULATION AND THE SHEATHING VENT BAFFLES TO PERMIT THE MOVEMENT OF AIR FROM THE SPACE TO THE EXTERIOR.
- MAINTAIN MIN. 2" OF CONTINUOUS VENTILATION FROM EAVE SOFFIT TO ATTIC VENT(S). BAFFLE VENT TO TERMINATE ABOVE INSULATION. DO NOT FILL BAFFLE WITH LOOSE INSULATION.
- THE UNOBSTRUCTED VENT AREA SHALL BE NOT LESS THAN 1/300 OF THE INSULATED CEILING AREA UNLESS, WHERE THE ROOF SLOPE IS LESS THAN 1 IN 6 OR IN ROOFS THAT ARE CONSTRUCTED WITH ROOF JOISTS, THE UNOBSTRUCTED VENT AREA SHALL BE NOT LESS THAN 1/150 OF THE INSULATED CEILING AREA.

EXISTING BUILDING ASSEMBLIES (PRESUMED)

FOUNDATION WALL ASSEMBLY

- +/- 18" STONE FOUNDATION
- MOISTURE BARRIER
- 1" AIR SPACE
- 2x4 STUDS AT 16" O.C.
- R12 BATT. INSULATION
- CONTINUOUS 6 MIL POLYETHYLENE VAPOUR BARRIER
- 1/2" DRYWALL

EXTERIOR WALL ASSEMBLY

- HORIZONTAL VINYL SIDING SYSTEM
- 1x3 STRAPPING AT 16" O.C.
- WEATHER RESISTANT BARRIER
- 1/2" PLYWOOD SHEATHING
- 2x4 ROUGH CUT STUDS AT 16" O.C.
- R12 BATT. INSULATION
- CONTINUOUS 6 MIL POLYETHYLENE VAPOUR BARRIER
- 3/4" DRYWALL

INTERIOR WALL ASSEMBLY

- 3/4" LATH AND PLASTER
- 2x6 ROUGH CUT STUDS AT 16" O.C.
- 3/4" LATH AND PLASTER

ROOF ASSEMBLY

- ASPHALT SHINGLES
- ICE & WATER SHIELD / ROOF UNDERLAYMENT
- PLYWOOD SHEATHING
- 2x4 RAFTERS AT 16" O.C.
- R50 BATT. INSULATION
- 2x4 CEILING JOISTS AT 16" O.C.
- CONTINUOUS 6 MIL POLYETHYLENE VAPOUR BARRIER
- 1x3 STRAPPING AT 16" O.C.
- 1/2" DRYWALL

ROOFING MATERIALS

- ROOFING MATERIALS SHALL CONFORM TO SUBSECTION 9.26.2.
- ROOF SHEATHING SHALL CONFORM TO SUBSECTION 9.23.15.

SHEATHING MEMBRANE MATERIAL STANDARD

- SHEATHING MEMBRANE MATERIAL STANDARD SHALL CONFORM TO ARTICLE 9.27.3.2.
- SHEATHING MEMBRANES SHALL CONFORM TO THE PERFORMANCE REQUIREMENTS OF CAN/CBSB-51.32-M, "SHEATHING, MEMBRANE, BREATHER TYPE".
- INSTALLATION OF THE SHEATHING MEMBRANE SHALL CONFORM TO ARTICLE 9.27.3.3. AND THE MANUFACTURER'S INSTRUCTIONS.

FASTENING FOR SHEATHING OR SUBFLOORING

- FASTENING FOR SHEATHING OR SUBFLOORING SHALL CONFORM TO ARTICLE 9.23.3.5.
- FASTENING OF SHEATHING AND SUBFLOORING SHALL CONFORM TO TABLE 9.23.3.5.

NAILS

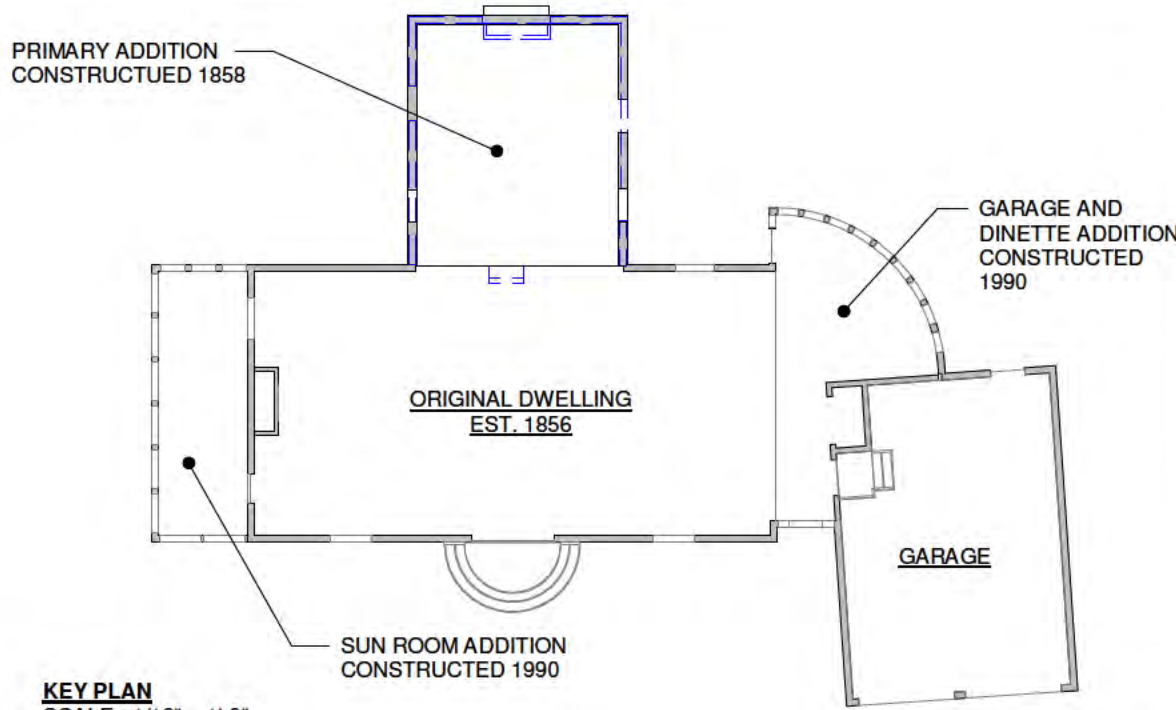
- NAILS SHALL CONFORM TO ARTICLE 9.26.2.2.
- NAILS SHALL CONFORM TO TABLE 9.23.3.4.
- NAILS USED FOR ROOFING SHALL BE CORROSION-RESISTANT ROOFING OR SHINGLE NAILS CONFORMING TO CSA B111, "WIRE NAILS, SPIKES AND STAPLES".
- ALL FASTENERS AND FRAMING CONNECTORS EXPOSED TO WEATHER TO BE HOT DIPPED GALVANIZED OR APPROVED EQUIVALENT.

ASPHALT SHINGLES

- ASPHALT SHINGLES SHALL CONFORM TO SUBSECTION 9.26.2. (ROOFING MATERIALS).
- ASPHALT SHINGLES SHALL CONFORM TO SUBSECTIONS 9.26.7. ASPHALT SHINGLES ON SLOPES OF 1 IN 3 OR GREATER AND SUBSECTION 9.26.8. ASPHALT SHINGLES ON SLOPES OF LESS THAN 1 IN 3.

DOWNSPOUTS

- DOWNSPOUTS SHALL CONFORM TO ARTICLE 9.26.18.2.
- WHERE DOWNSPOUTS ARE PROVIDED AND ARE NOT CONNECTED TO A SEWER, EXTENSIONS SHALL BE PROVIDED TO CARRY RAINWATER AWAY FROM THE BUILDING IN A MANNER THAT WILL PREVENT SOIL EROSION.



KEY PLAN
SCALE: 1/16" = 1'-0"

DRAWING LIST

- S-1 NOTES / KEY PLAN / PHOTO
- S-2 SITE LAYOUT
- S-3 MAIN FLOOR LEVEL - EXISTING LAYOUT
- S-4 SECOND FLOOR LEVEL - EXISTING LAYOUT
- S-5 LOWER LEVEL FLOOR PLAN
- S-6 MAIN FLOOR PLAN - PROPOSED
- S-7 SECOND FLOOR PLAN - PROPOSED
- S-8 ELEVATIONS
- S-9 ELEVATIONS

WINDOW SCHEDULE			
TYPE	ROUGH OPENING		COMMENTS
	WIDTH	HEIGHT	
W1	2' - 9"	1' - 8"	
W2	3' - 5"	5' - 10"	
W3	3' - 6"	3' - 8"	
W4	2' - 0"	3' - 8"	
W5	2' - 9"	5' - 9"	
W6	3' - 2"	3' - 8"	
W7	1' - 9"	5' - 3"	
W8	3' - 5"	5' - 5"	
W9	2' - 1"	2' - 8"	
W10	3' - 0"	3' - 0"	
W11	1' - 8"	1' - 8"	

LUMBER

- ALL LUMBER TO BE SPF NO.1/NO.2 UNLESS OTHERWISE INDICATED.
- ALL LUMBER EXPOSED TO WEATHER TO BE PRESERVATIVE TREATED OR RED CEDAR.
- APPLY TWO APPLICATIONS OF PRESERVATIVE TREATMENT TO ALL CUT OR DRILLED SURFACES WHERE P.T. LUMBER IS INDICATED.
- PREFABRICATED HANGERS AND FRAMING CONNECTORS TO BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.
- ALL FASTENERS AND FRAMING CONNECTORS EXPOSED TO WEATHER TO BE HOT DIPPED GALVANIZED OR APPROVED EQUIVALENT.
- FILL ALL CONNECTOR NAIL/SCREW HOLES AS SPECIFIED BY MANUFACTURER.

GYPSUM BOARD FINISH (TAPED JOINTS)

- GYPSUM BOARD FINISH (TAPED JOINTS) SHALL CONFORM TO SUBSECTION 9.29.5.

FLOORING

- FLOORING SHALL CONFORM TO SECTION 9.30.
- FINISHED FLOORING SHALL BE PROVIDED IN ALL RESIDENTIAL OCCUPANCIES.

VAPOUR BARRIERS

- VAPOUR BARRIERS TO CONFORM TO O.B.C. 9.25.4.
- VAPOUR BARRIER SHALL BE INSTALLED TO PROTECT THE WARM SIDE OF WALL, CEILING AND FLOOR ASSEMBLIES.
- WHERE POLYETHYLENE IS INSTALLED TO SERVE AS THE VAPOUR BARRIER, IT SHALL CONFORM TO CAN/CGSB-51.34-M, "VAPOUR BARRIER, POLYETHYLENE SHEET FOR USE IN BUILDING CONSTRUCTION".
- LAP ALL JOINTS A MINIMUM OF 4" AND SEAL WITH COMPATIBLE MATERIAL. (IE. TUCK TAPE)
- VAPOUR BARRIERS TO ACT CONTINUOUSLY AS PER O.B.C. 9.25.3.3.

NO.	DESCRIPTION	11.5.	DATE
1	FOR PRELIMINARY REVIEW		AUG. 5, 2019
2	PROPOSED ALTERATIONS		DEC. 20, 2019

NOT FOR CONSTRUCTION

DRAWINGS TO BE PRINTED ON ARCH C PAPER
(24" WIDE x 18" HIGH)



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PROJECT TITLE: **PROPOSED DWELLING ALTERATIONS**

DRAWING TITLE: **NOTES / KEY PLAN / PHOTO**

OWNER:

LOCATION: 1234 OLD RIVER ROAD,
PORT CREDIT, ONTARIO

DRAWN BY: A. MCKAY SCALE: As indicated

FILE NO.: **16MF-585** DRAWING NO. **S-1**

DRAWINGS REMAIN THE PROPERTY OF MFAL

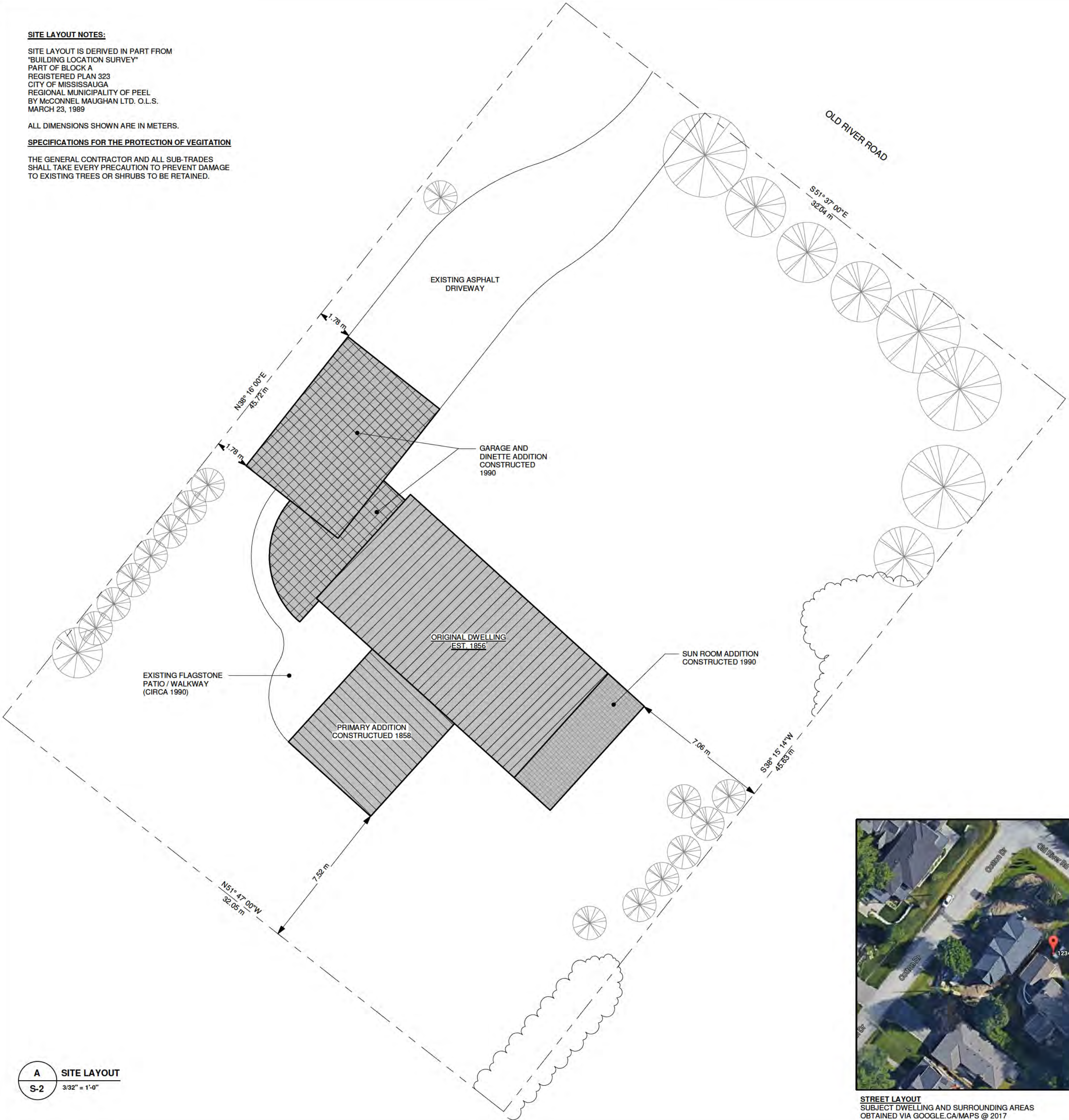
SITE LAYOUT NOTES:

SITE LAYOUT IS DERIVED IN PART FROM
"BUILDING LOCATION SURVEY"
PART OF BLOCK A
REGISTERED PLAN 323
CITY OF MISSISSAUGA
REGIONAL MUNICIPALITY OF PEEL
BY McCONNEL MAUGHAN LTD. O.L.S.
MARCH 23, 1989

ALL DIMENSIONS SHOWN ARE IN METERS.

SPECIFICATIONS FOR THE PROTECTION OF VEGETATION

THE GENERAL CONTRACTOR AND ALL SUB-TRADES
SHALL TAKE EVERY PRECAUTION TO PREVENT DAMAGE
TO EXISTING TREES OR SHRUBS TO BE RETAINED.



STREET LAYOUT
SUBJECT DWELLING AND SURROUNDING AREAS
OBTAINED VIA GOOGLE.CA/MAPS @ 2017

NO.	DESCRIPTION	11.5.	DATE
1	FOR PRELIMINARY REVIEW		AUG. 5, 2019
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PROJECT TITLE: **PROPOSED DWELLING
ALTERATIONS**

DRAWING TITLE: **SITE LAYOUT**

OWNER:

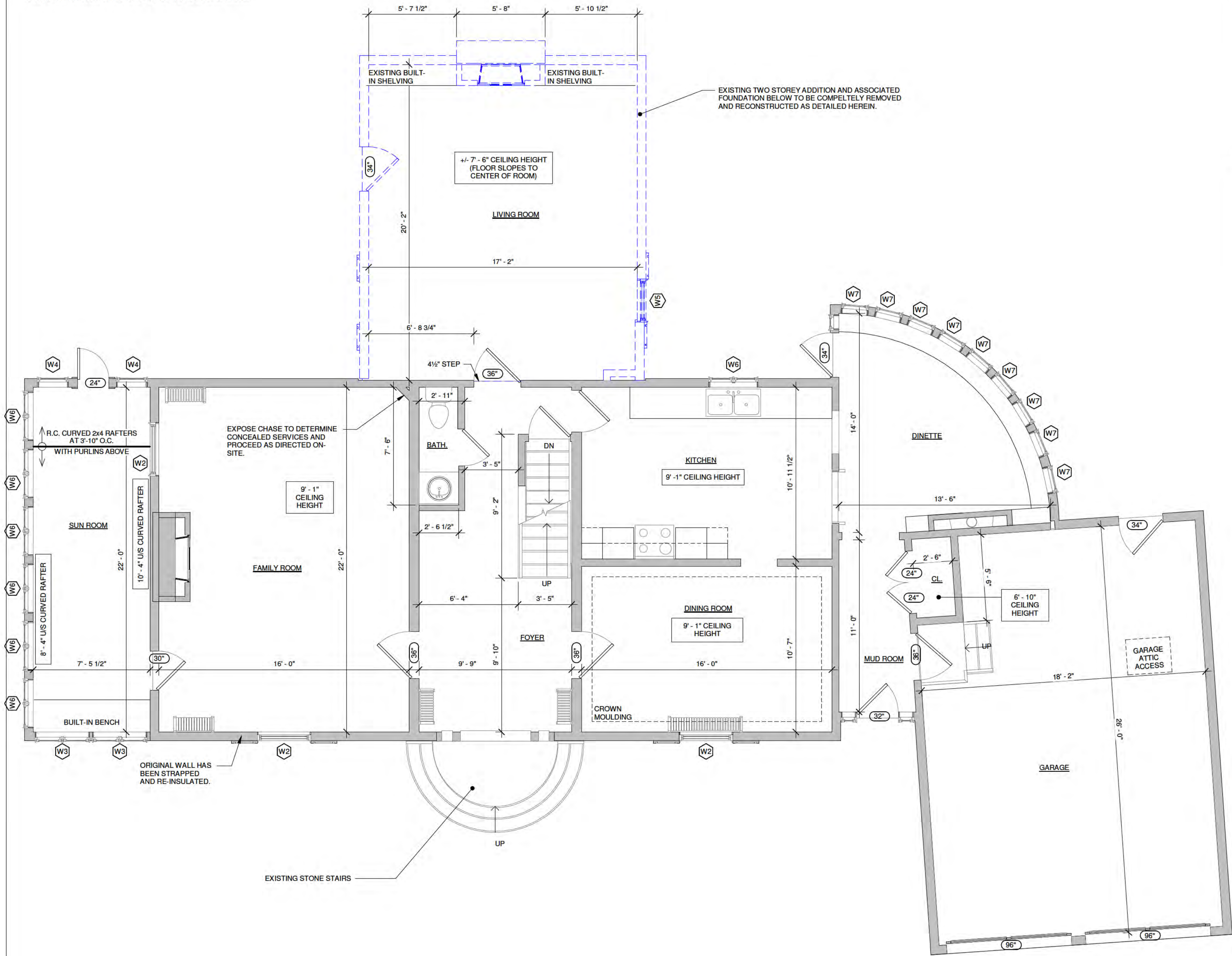
LOCATION: 1234 OLD RIVER ROAD,
PORT CREDIT, ONTARIO

DRAWN BY: A. MCKAY SCALE: 3/32" = 1'-0"

FILE NO.: **16MF-585** DRAWING NO. **S-2**

DRAWINGS REMAIN THE PROPERTY OF MFAL

NOTE:
CONTACT MFAL UPON REMOVAL OF FINISHES IF PRESUMED
INTERIOR PARTITIONS ARE DISCOVERED TO BE LOAD BEARING.



B FIRST FLOOR PLAN - DEMOLITION
S-3 1/4" = 1'-0"

NO.	DESCRIPTION	11.5.	DATE
1	FOR PRELIMINARY REVIEW		AUG. 5, 2019
2	PROPOSED ALTERATIONS		DEC. 20, 2019

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PROJECT TITLE: **PROPOSED DWELLING
ALTERATIONS**

DRAWING TITLE: **MAIN FLOOR LEVEL -
EXISTING LAYOUT**

OWNER:

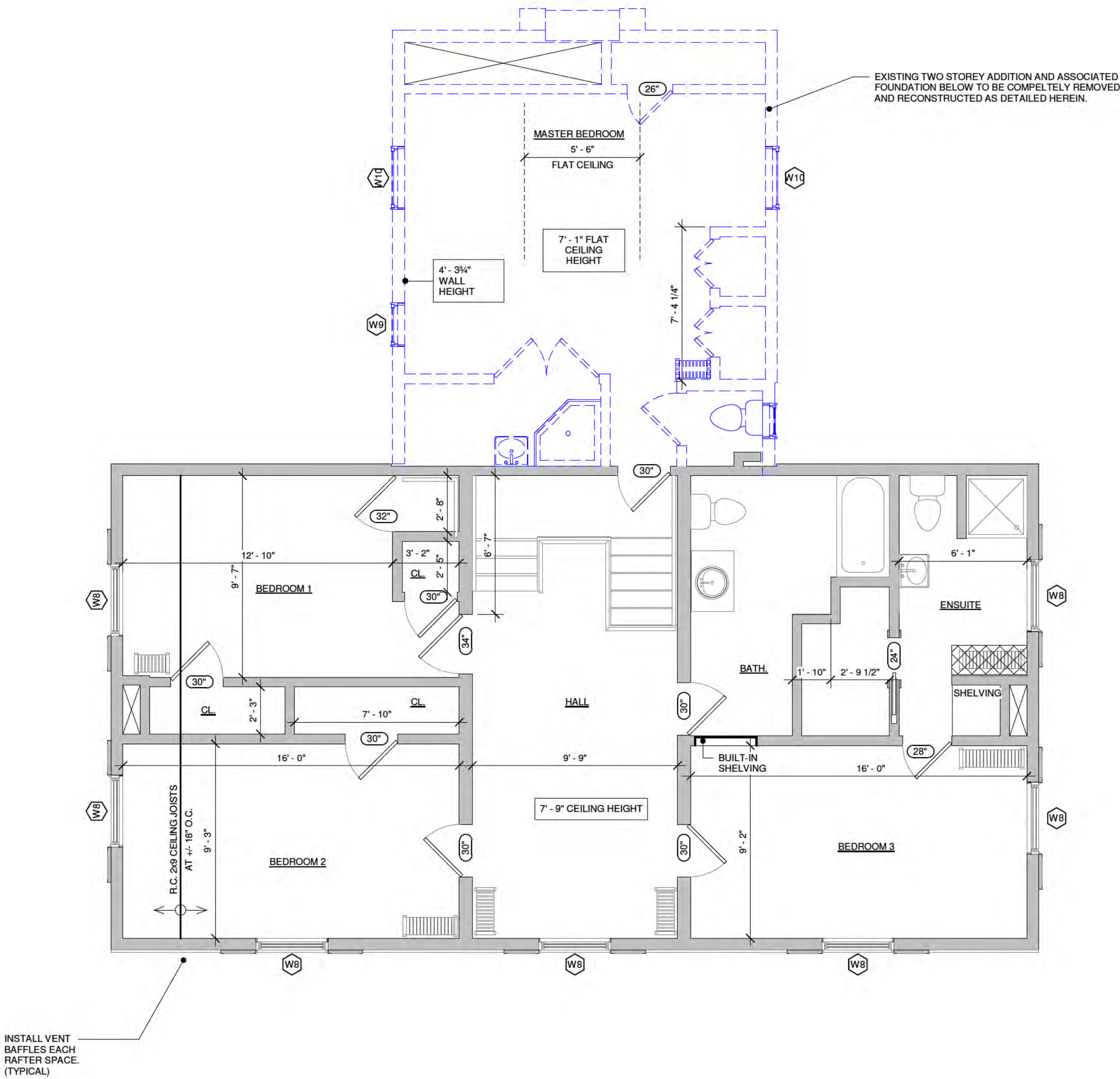
LOCATION: 1234 OLD RIVER ROAD,
PORT CREDIT, ONTARIO

DRAWN BY: A. MCKAY SCALE: 1/4" = 1'-0"

FILE NO.: **16MF-585** DRAWING NO. **S-3**

DRAWINGS REMAIN THE PROPERTY OF MFAL

NOTE:
CONTACT MFAL UPON REMOVAL OF FINISHES
IF PRESUMED INTERIOR PARTITIONS ARE
DISCOVERED TO BE LOAD BEARING.



C
S-4
1/4" = 1'-0"

SECOND FLOOR PLAN - DEMOLITION

NO.	DESCRIPTION	11.5.	DATE
1	FOR PRELIMINARY REVIEW	AUG. 5, 2019	
2	PROPOSED ALTERATIONS	DEC. 20, 2019	

NOT FOR CONSTRUCTION

DRAWINGS TO BE PRINTED ON ARCH C PAPER
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ENTIRETY OF THE DRAWING SET

PROJECT TITLE: **PROPOSED DWELLING
ALTERATIONS**

DRAWING TITLE: **SECOND FLOOR LEVEL
- EXISTING LAYOUT**

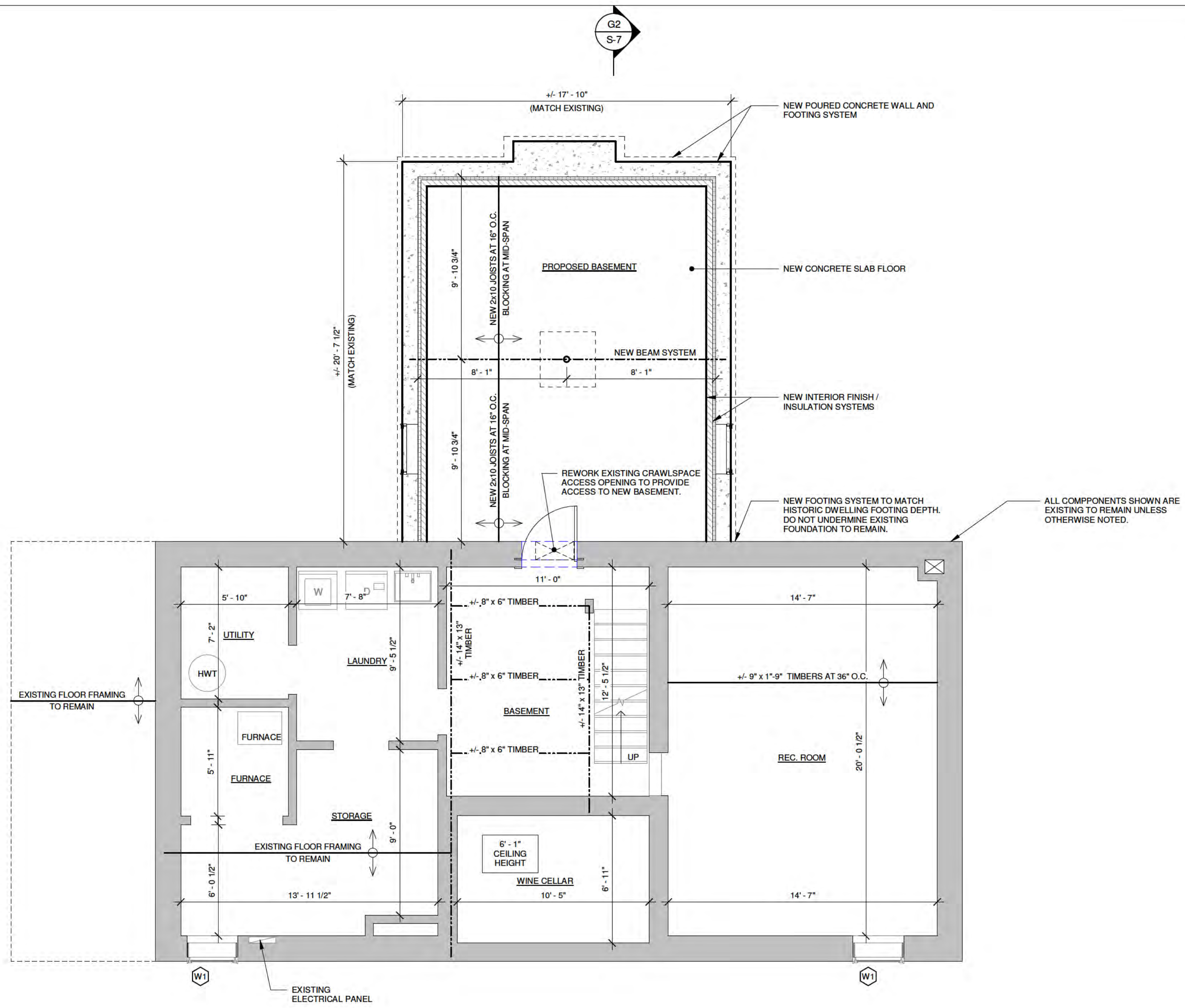
OWNER:

LOCATION: 1234 OLD RIVER ROAD,
PORT CREDIT, ONTARIO

DRAWN BY: A. MCKAY SCALE: 1/4" = 1'-0"

FILE NO.: **16MF-585** DRAWING NO. **S-4**

DRAWINGS REMAIN THE PROPERTY OF MFAL



NO.	DESCRIPTION	11.5.	DATE
1	FOR PRELIMINARY REVIEW		AUG. 5, 2019
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E
S-5
BASEMENT PLAN
1/4" = 1'-0"

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PROJECT
TITLE: **PROPOSED DWELLING
ALTERATIONS**

DRAWING
TITLE: **LOWER LEVEL FLOOR
PLAN**

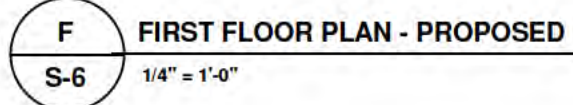
OWNER:

LOCATION: 1234 OLD RIVER ROAD,
PORT CREDIT, ONTARIO

DRAWN BY: AM / SA SCALE: 1/4" = 1'-0"

FILE
NO.: **16MF-585** DRAWING
NO. **S-5**

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PROJECT TITLE:	PROPOSED DWELLING ALTERATIONS
----------------	--------------------------------------

DRAWING TITLE:	MAIN FLOOR PLAN - PROPOSED
-------------------	---------------------------------------

OWNER:

LOCATION: 1234 OLD RIVER ROAD,
PORT CREDIT, ONTARIO

DRAWN BY: AM / SA	SCALE: 1/4" = 1'-0"
FILE NO.: 16MF-585	DRAWING NO. S-6

DRAWINGS REMAIN THE PROPERTY OF MFAL

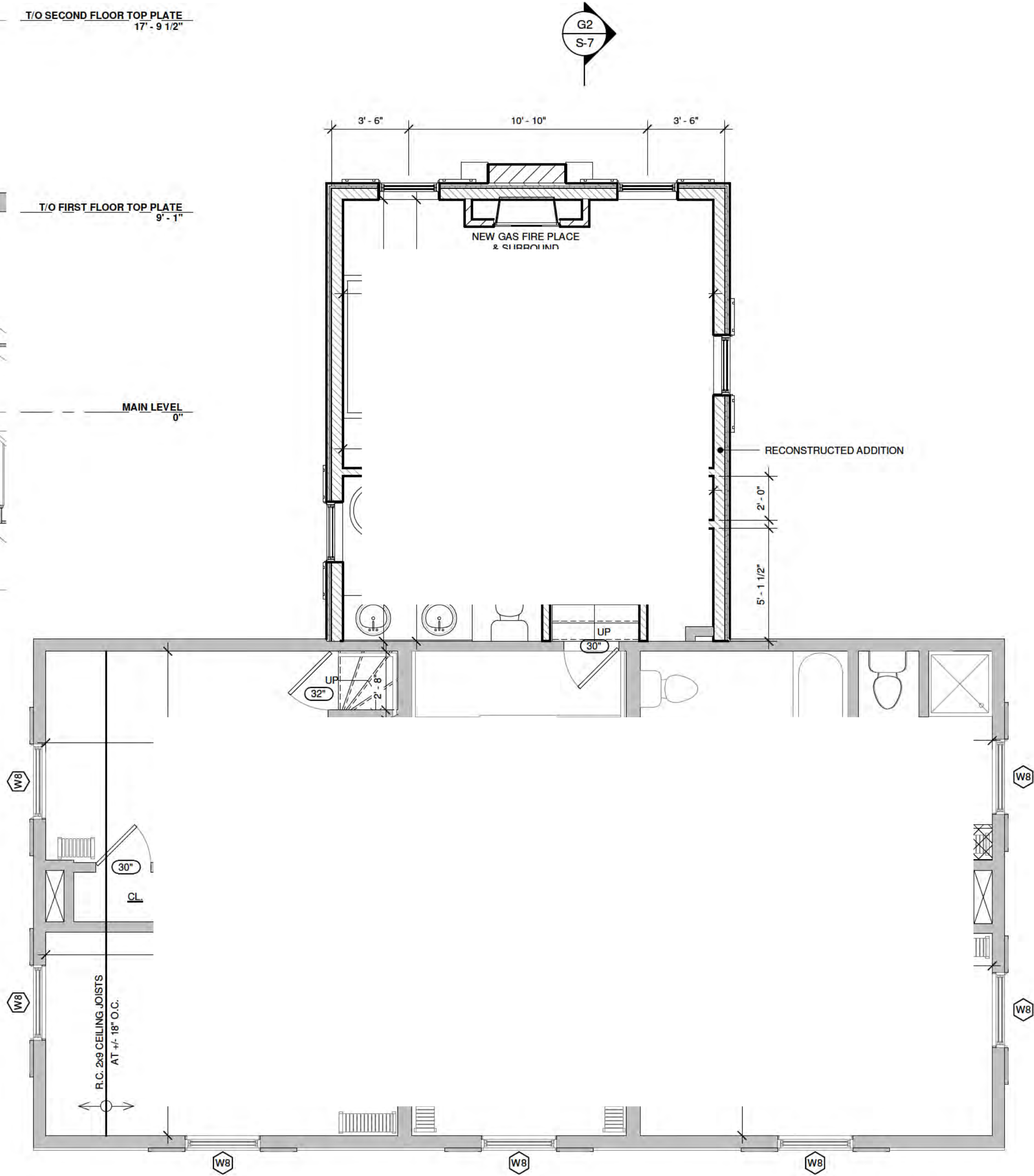
OVERFRAME EXISTING ROOF AS REQUIRED TO PROVIDE CONTINUOUS SLOPE TOWARDS ROOF EAVES:

OVERFRAMING ASSEMBLY

- 2x8 RIDGE SUPPORT
- 2x8 BEARING PLATES AT MAX. 4' - 0" O.C.
- 2x6 RAFTERS AT 16" O.C.
- POST RAFTERS TO BEARING PLATES AT MAX. 4' - 0" O.C.
- SECURE RAFTER / PLATE CONNECTIONS WITH 4x - 3½" SPIRAL NAILS.
- SECURE BEARING PLATES TO EACH ROOF RAFTER WITH 4x - 3½" SPIRAL NAILS.



G2 PARTIAL BUILDING SECTION
S-7 1/4" = 1'-0"



G SECOND FLOOR PLAN - PROPOSED
S-7 1/4" = 1'-0"

NO.	DESCRIPTION	11.5.	DATE
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2	PROPOSED ALTERATIONS		DEC. 20, 2019

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PROJECT TITLE: **PROPOSED DWELLING ALTERATIONS**

DRAWING TITLE: **SECOND FLOOR PLAN - PROPOSED**

OWNER:

LOCATION: 1234 OLD RIVER ROAD, PORT CREDIT, ONTARIO

DRAWN BY: AM / SA SCALE: 1/4" = 1'-0"

FILE NO.: **16MF-585** DRAWING NO. **S-7**

DRAWINGS REMAIN THE PROPERTY OF MFAL



H
S-8 SOUTH ELEVATION
3/16" = 1'-0"



I
S-8 WEST ELEVATION
3/16" = 1'-0"

NO.	DESCRIPTION	11.5.	DATE
1	FOR PRELIMINARY REVIEW		AUG. 5, 2019
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DRAWINGS TO BE PRINTED ON ARCH C PAPER
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PROJECT TITLE: **PROPOSED DWELLING
ALTERATIONS**

DRAWING TITLE: **ELEVATIONS**

OWNER:

LOCATION: 1234 OLD RIVER ROAD,
PORT CREDIT, ONTARIO

DRAWN BY: A. MCKAY SCALE: 3/16" = 1'-0"

FILE NO.: **16MF-585** DRAWING NO. **S-8**

DRAWINGS REMAIN THE PROPERTY OF MFAL



J EAST ELEVATION
S-9 3/16" = 1'-0"



K NORTH ELEVATION
S-9 3/16" = 1'-0"

NO.	DESCRIPTION	11.5.	DATE
1	FOR PRELIMINARY REVIEW		AUG. 5, 2019
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ENTIRETY OF THE DRAWING SET

PROJECT TITLE: **PROPOSED DWELLING
ALTERATIONS**

DRAWING TITLE: **ELEVATIONS**

OWNER:

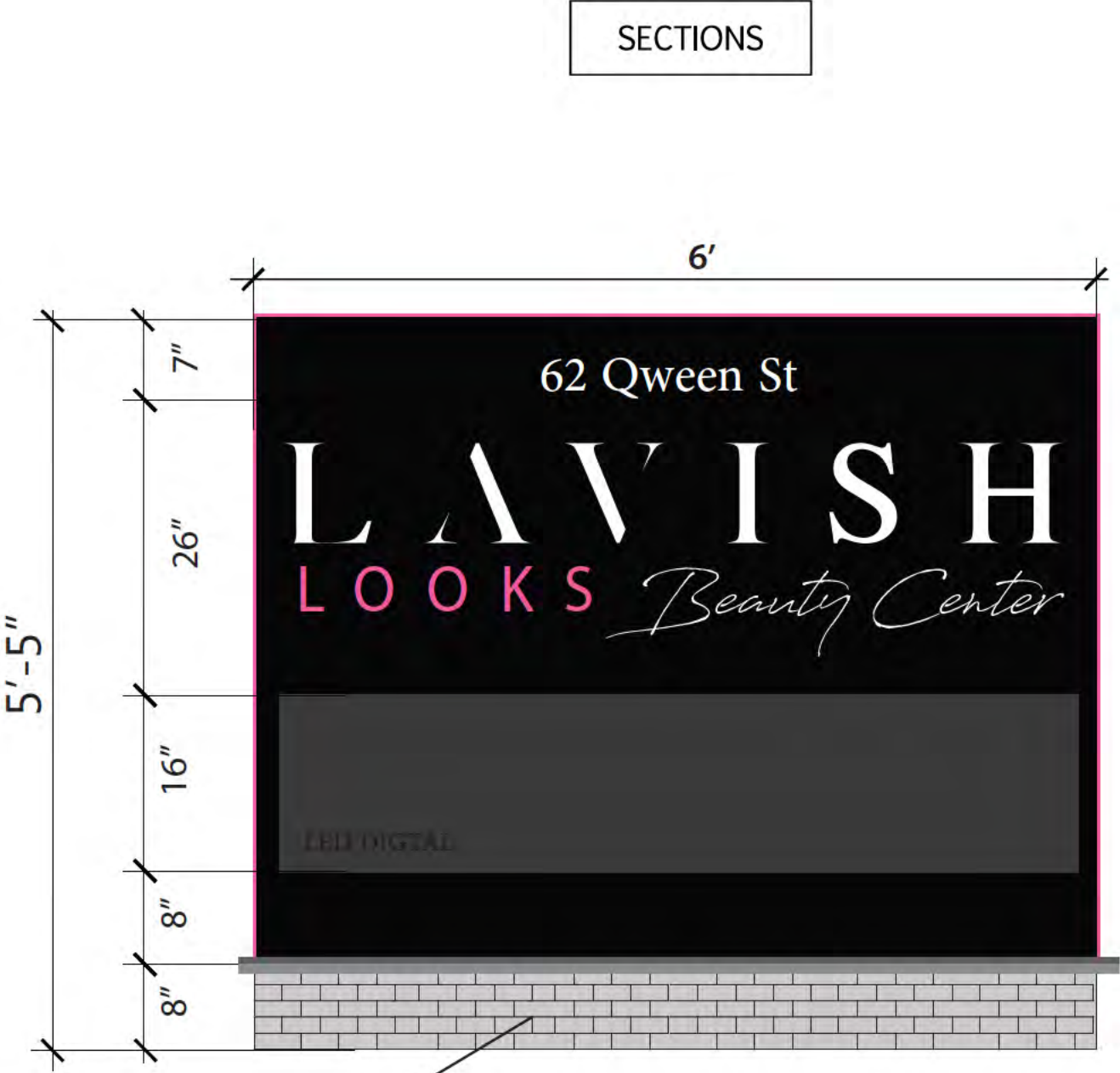
LOCATION: 1234 OLD RIVER ROAD,
PORT CREDIT, ONTARIO

DRAWN BY: AM / SA SCALE: 3/16" = 1'-0"

FILE NO.: **16MF-585** DRAWING NO. **S-9**

DRAWINGS REMAIN THE PROPERTY OF MFAL

Front View



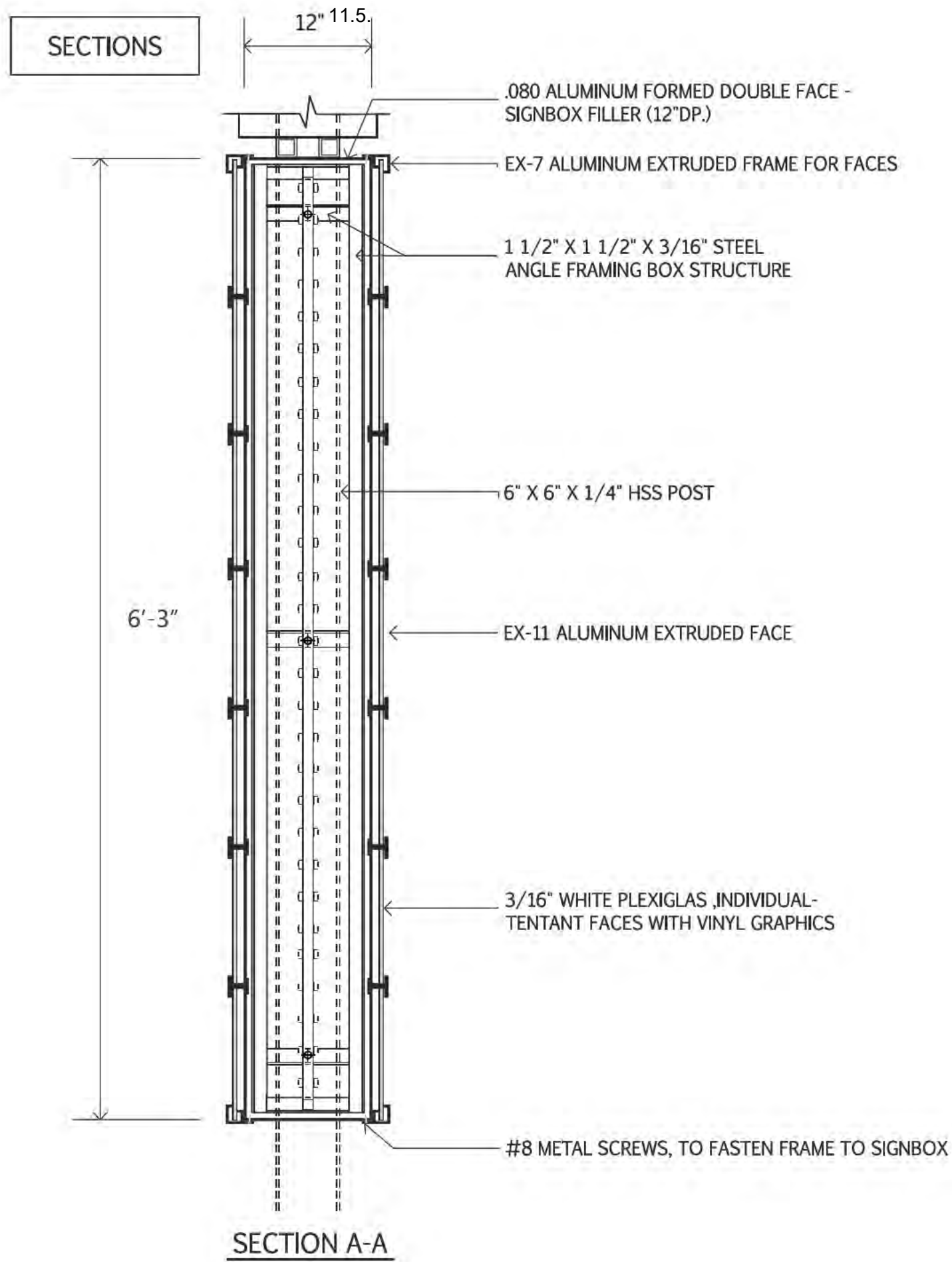
Wall Brick

CUSTOMER:	LAVISH	CONTACT:	Mohd	REVISION #	1-2
ORDER #		PHONE:		SITE ADDRESS:	
DATE:	08/02/2020	EMAIL:			62 Qween St. Mississauga, ON
		REP:	Mohd	CHECKED BY:	
		DESIGNER:			



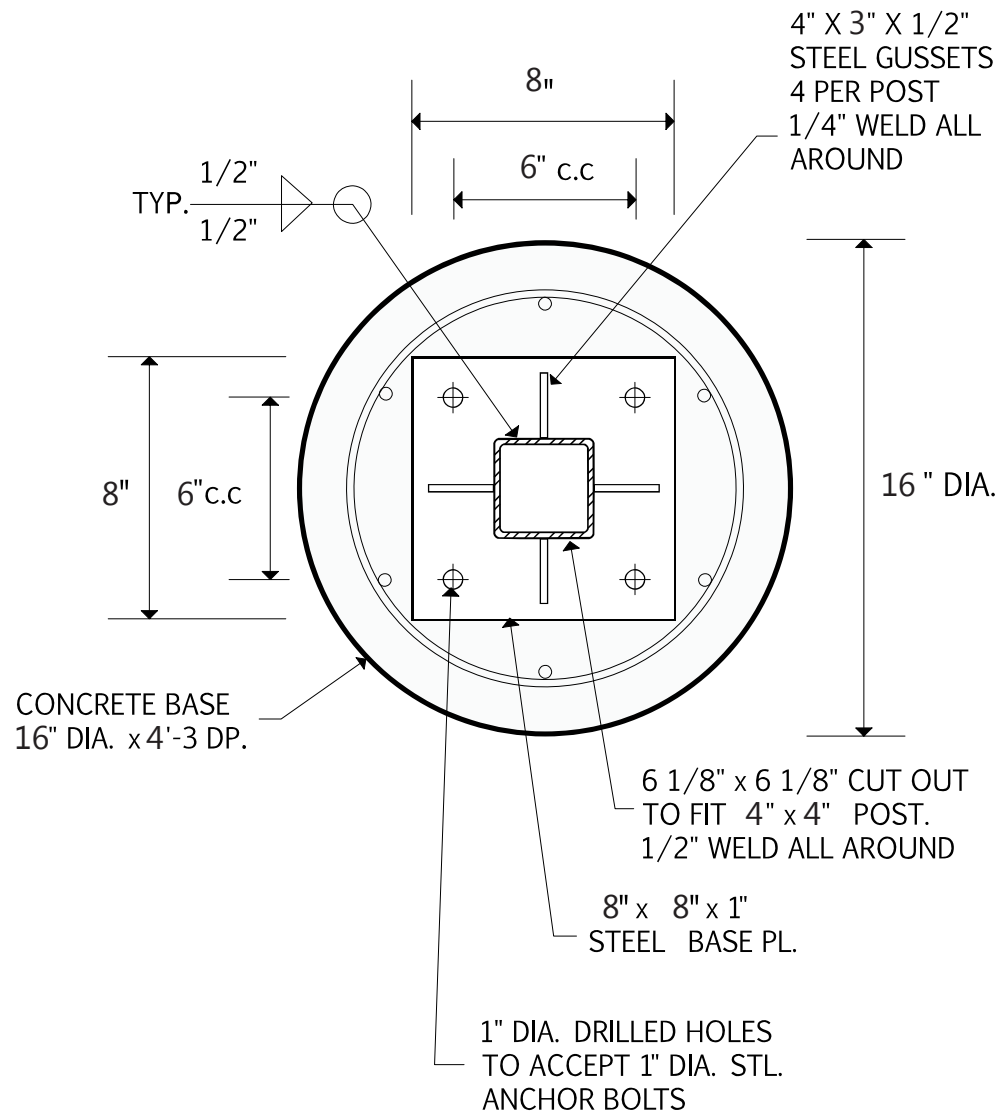
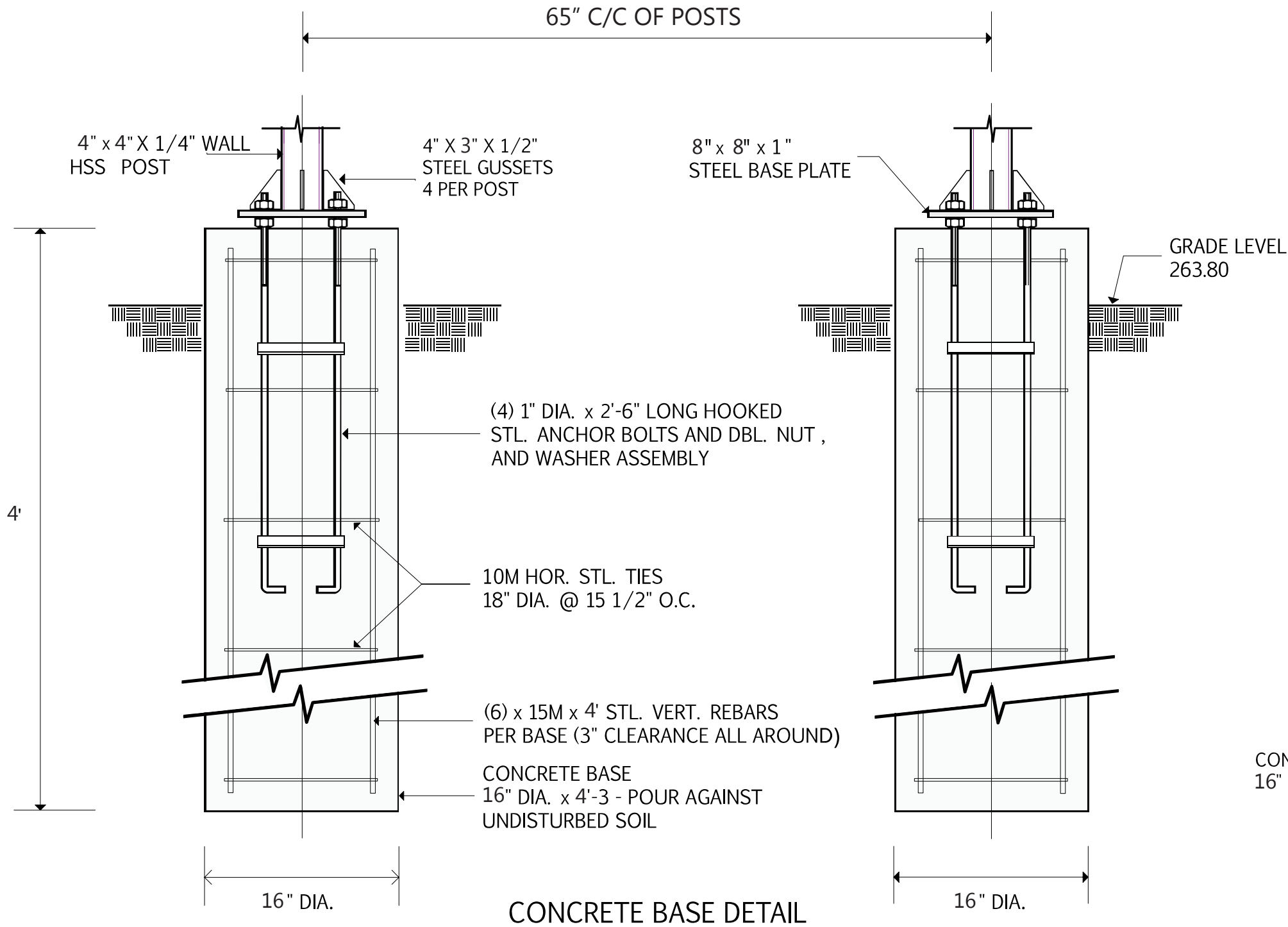
#6 - 5151 Everest Dr.
Mississauga, ON
L4W 2Z3

TYPICAL DETAILS & CONNECTIONS



CUSTOMER:	LAVISH	CONTACT:	Mohd	REVISION #	1-1
ORDER #		PHONE:		SITE ADDRESS:	
DATE:	03/03/2020	EMAIL:			62 Qween St. Mississauga, ON
		REP:	Mohd	CHECKED BY:	
		DESIGNER:			

BASE DETAILS



BASE PLATE DETAIL

CUSTOMER:	LAVISH	CONTACT:	Mohd	REVISION #	1-4
ORDER #		PHONE:		SITE ADDRESS:	
DATE:	03/03/2020	EMAIL:			62 Qween St. Mississauga, ON
		REP:	Mohd	CHECKED BY:	
		DESIGNER:			

Paula Wubbenhorst

From:
Sent: 2020/03/13 8:14 AM
To: Paula Wubbenhorst
Subject: Cover letter

To whom it may concern

The attached submission is a request to obtain a sign permit.

The sign is designed like our business logo (we will have it in our website, business cards, marketing such as flyers and online, and uniforms, etc), black background with the name Lavish Looks Beauty Centre, and under that is a digital 16" by 6' small size, and under that will be concrete post (documents attached) 16" diameter as requested with bricks around it to maintain the heritage look from ground to top 5'-5" long and 6' width.

Our first submission we had the size bigger 6'-3" long from ground x 6' width. We will be happy if you consider this size, take in consideration we cannot put a sign on the property (i mean big sign on top of the door) because we understand it's a heritage property, with that being said we need people to know that we have a business in this property because it looks like a house and with considering the codes.

Will attach more pictures of sign around Streetville right on to Queen Street with size varies from longer than 6' and lower. I will attach the first submission along side the new submission. With the bricks in the bottom.

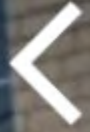
Thank you.

12:31 ↗

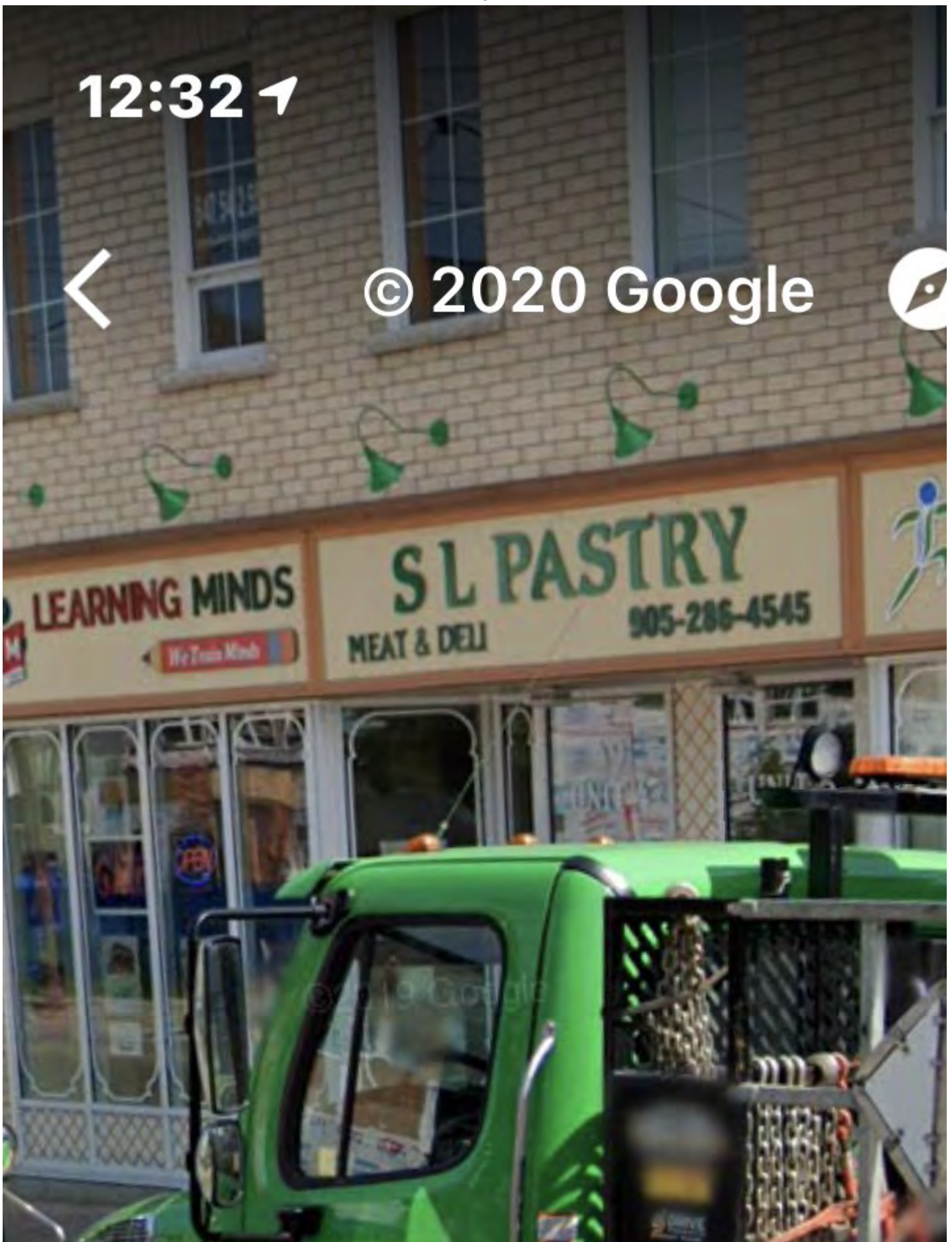




12:32



© 2020 Google



12:32 1



12:33 ↖



12:34 ↗



© 2020 Google



12:35 ↗



© 2020 Google



12:30 ↗



Heritage Impact Statement
7076 Old Mill Lane, Meadowvale Village
City of Mississauga



Existing House, view from the front. The garage is on the adjacent lot.

Paul Oberst, B. Arch., OAA, CAHP
Paul Oberst Architect
February 2020

Engagement:

I am an architect licensed in Ontario, and a professional member of the Canadian Association of Heritage Professionals (CAHP). I was engaged by the owners to produce a heritage impact statement regarding the property at 7076 Old Mill Lane, in Meadowvale Village in the City of Mississauga. I am also engaged to advise the project architect, Chris Wallace.

Contacts:

Heritage Consultant- Paul Oberst Architect 416-504-6497
oberst@bellnet.ca

Owner- Current Owner

Architect- Christopher Wallace Architect 905-753-1122
chris@cwallacearchitect.com

Table of Contents

1. The Proposal	3
2. Location	3
3. Historical Background	4
4. Current Development Controls	8
5. The Existing Buildings	10
6. Heritage Evaluation of the Existing Buildings	12
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8. Heritage Evaluation of the Proposal	19
9. Bibliography	21

Appendices

Consultant's CV

1. The Proposal

It is proposed to renovate the existing house at 7076 Old Mill Lane in the Meadowvale Village Heritage Conservation District and to construct additions that will be sympathetic to the architectural and landscape character of the District.

2. Location

The property is located on the west side of Old Mill Lane in Meadowvale Village in the City of Mississauga. The lot line is roughly opposite the intersection of Old Mill Lane and Pond Street. The property is described as Lot 43, Plan Toronto-5, and it bears the Roll Number of 21-05-040-098-20100-0000.



Figure 1. Aerial photo. Location of the house is indicated with the red dot.

3. Historical Background

3.1 Overview: Meadowvale Village is a historic settlement near the northern boundary of Mississauga, about 3 km west of Hurontario Street. It is located on the banks of the Credit River, at the intersection of two original concession roads, Second Line Road and the Sideroad, between Lots 10 & 11, now called Old Derry Road. Farming settlement began in 1819 with the arrival of John Beatty. As in many early Ontario settlements, the proximity of a road to a watercourse provided the opportunity for development of a water-powered milling industry. In 1838, John Simpson built the first successful sawmill in Meadowvale, a few years after James Crawford's attempted mill had faltered. Milling became the economic engine of the village, and milling activity continued for more than a century.

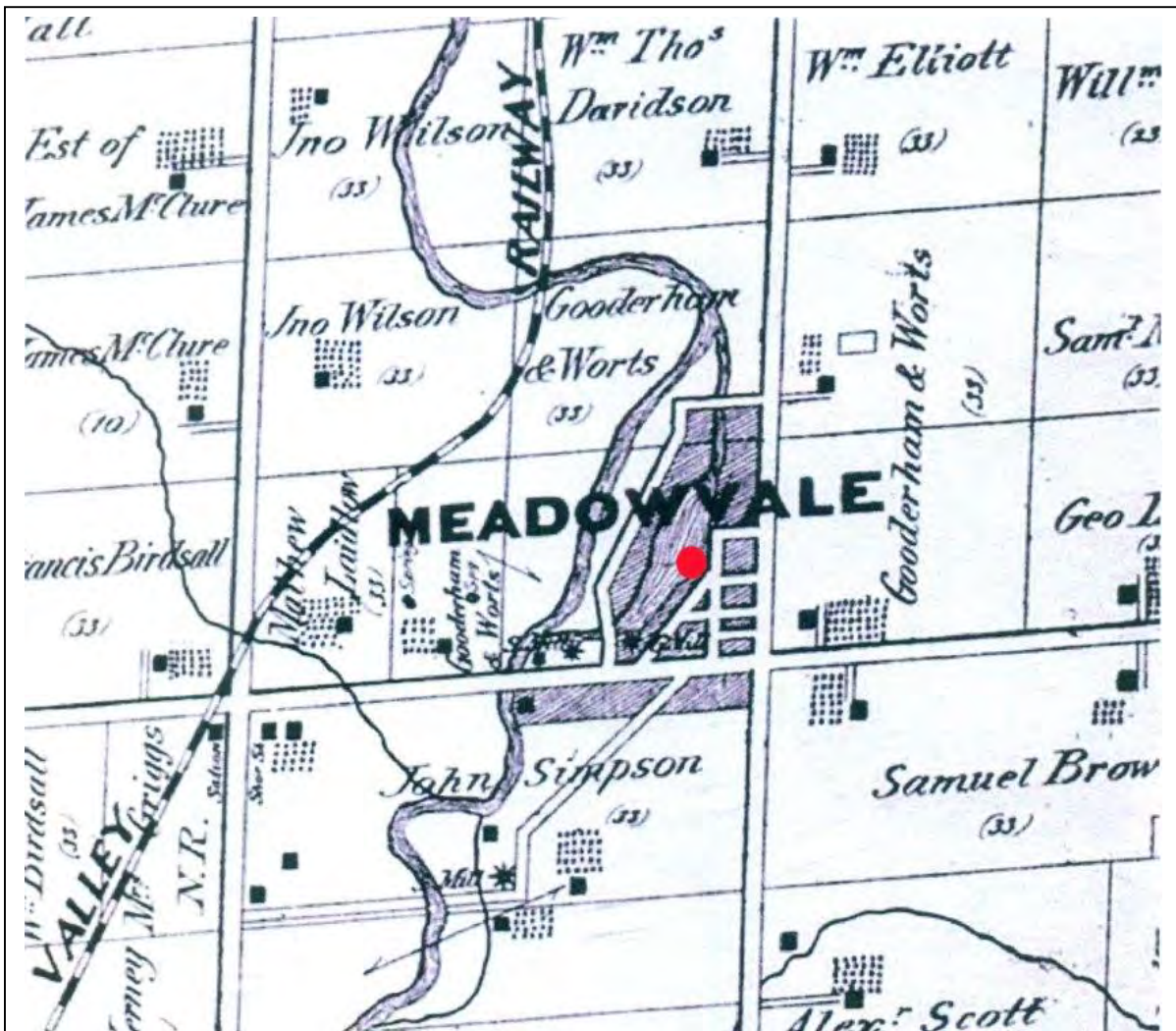


Figure 2. From 1877 Peel County Atlas. Houses on farms are shown individually, but within the village, the settled area is shaded, without showing individual buildings. Red dot is approximate site of the subject property. Two sawmills and a grist mill are shown on this map.

3.2 Lot 43: Francis Silverthorn came to Meadowvale around 1836, and became a significant figure in the history of the Village, constructing and operating a grist mill, a sawmill, and a stave factory, and acquiring numerous landholdings. In 1856 he commissioned a survey by Arthur Bristow, dividing his lands north of the sideroad (now Old Derry Road) and west of the concession road (now Second Line West). The plan was entitled “Plan of Building and Park Lots in Meadowvale”, registered on July 1, 1856 as Toronto Township Plan 5 (abbreviated today as PL TOR5). The current 7076 Old Mill Lane is on Lot 43, which is visible in Figure 3 just south of the intersection of Pond Street and Old Mill Lane.

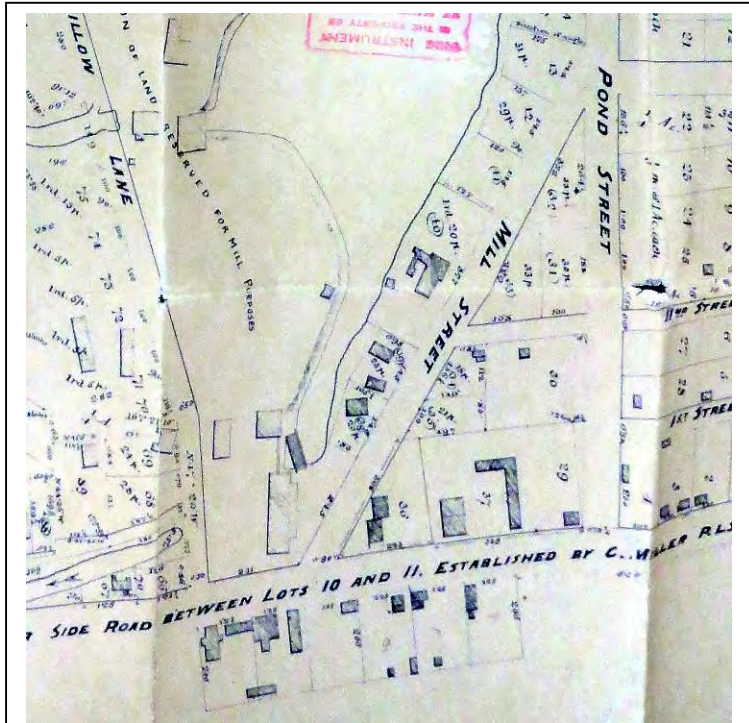


Figure 3. Detail from the Bristow Survey. The Silverthorn Mill is the large building west of Mill Street.

3.3 Historical Uses of the Lot:

According to the District inventory, the house is a “late nineteenth century structure that has changed little over the years”. It notes that it was the village post office between 1977 and 1981. It is believed to be the Robert Golden house, as sketched by Harry Spiers for his 1904 booklet *A Souvenir of the Village of Meadowvale on the Credit*. This attribution is supported by title research: In 1889 the property was purchased by Eleanor Pearson, widow, and Ellen Mary Golden, wife of Robert Golden. Ellen Mary Golden took full possession in 1907. The full chain of title appears below.

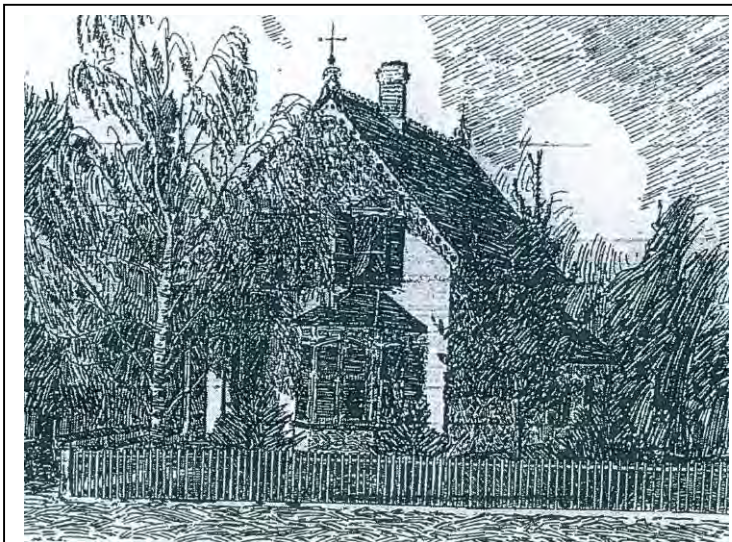
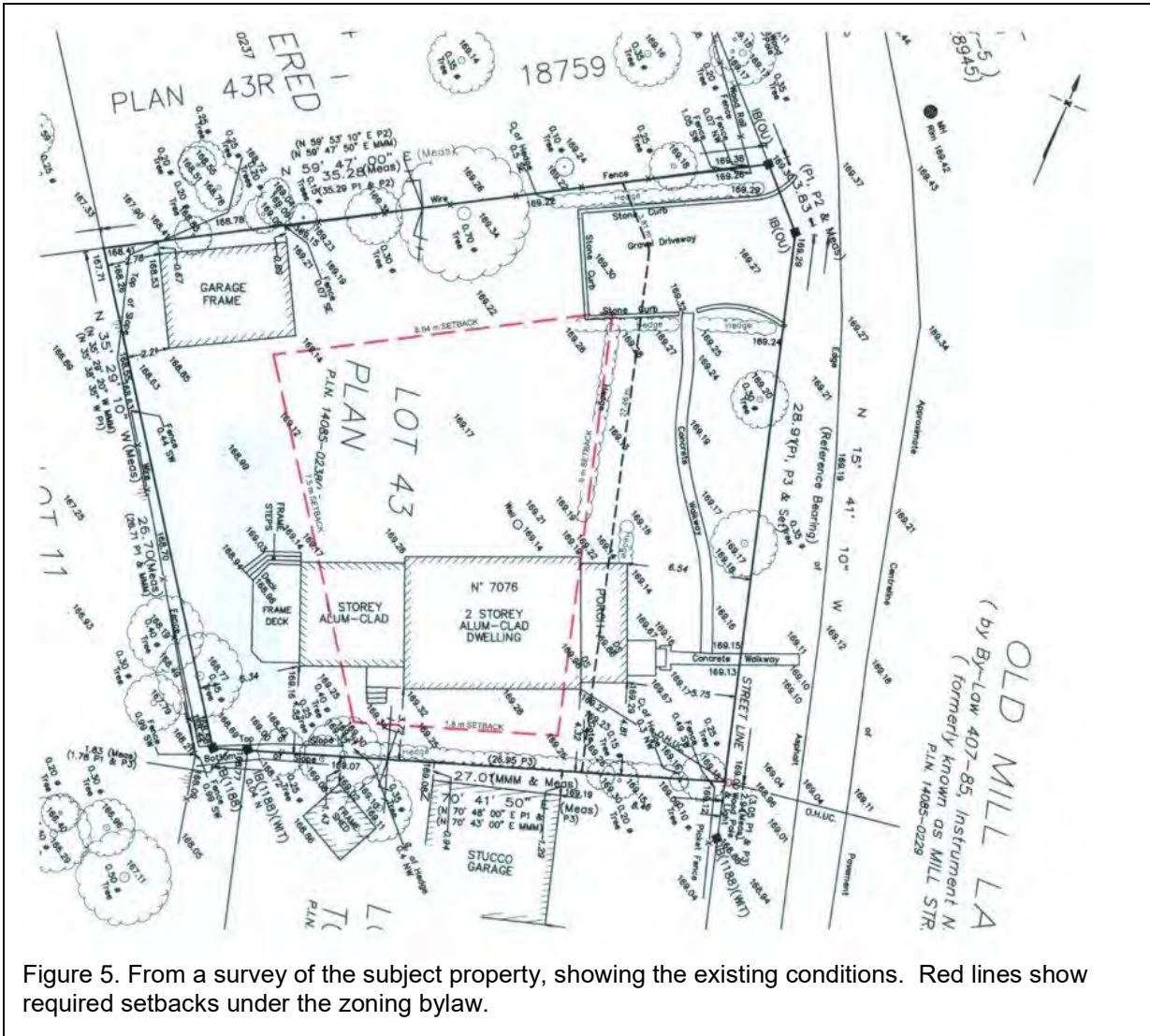


Figure 4. Residence of R. Golden by Harry Spiers, 1904. From *A Souvenir of the Village of Meadowvale on the Credit*.

CHAIN OF TITLE RE PIN 14085-0238 – 7076 OLD MILL LANE, MISSISSAUGA:

Patent - 23 July 1821? Crown to John Beatty;
 24570 Bargain & Sale - 7 May 1845 Beatty to James Crawford; (75 acres)
 42849 Grant- 12 Aug 1847 James Crawford to Aaron Silverthorn & Francis Silverthorn; (167 acres)
 43290 Mtg.- 18 Feb 1852 Frances James Silverthorn to John Wilmot;
 5018 Ass't of Mtg.- 15 Mar 1858 John Wilmot to James G. Worts;
 TOR-5- 7 July 1856 Plan; (This is the registration of the Bristow Survey)
 3402 B & S 7 - July 1881 ...Worts to Thomas Elliott; (This is first citation of Lot 43, TOR5.)
 6224 B & S - 3 Oct 1887 ...Elliott to Robert N. Irwin;
 6909 B & S - 13 Aug 1889 ...Irwin to Eleanor Pearson and Ellen Mary Golden, wife of Robert Golden;
 12659 B & S - 24 Apr 1907 ...Pearson to Ellen Mary Golden;
 14198 Grant - 14 Nov 1910 ...Golden to William D. Orr;
 26073 Grant - 28 Apr 1925 Estate of William D. Orr to Samuel J. McClure;
 41462 Grant - 16 Jan 1942 ...McClure to Margaret E. Copeland;
 50515 Grant - 5 Aug 1947 ...Copeland to Frances I. Copeland and Hugh A. Copeland;
 121660 Grant - 5 Aug 1959 ...Copelands to Joy Ogle and James P. Ogle; ...2
 PR968511 Transmission - 24 Nov 2005 ...Ogles to James Christopher Ogle;
 PR968782 Transfer - 24 Nov 2005 ...Ogle to Brian Leslie Hughes;
 PR1286334 Transfer - June 29 2007 ...Hughes to Tatiana Orlova;
 PR3498421 Transfer - 5 Jul 2019 ...Orlova to Current Owner.

3.4 Current Conditions on the Lot:



Lot 43 contains the existing dwelling near the southern lot line, and a frame shed (called a garage on the survey above) near the northwestern corner of the lot. There is a parking pad at the north eastern corner of the lot. There is a mature maple near the centre of the front lot line, and mature trees on the north and west (rear) lot line. Smaller trees and shrubs are mixed in along the boundaries. The hedges shown on the survey above are rather unruly, or currently missing.

4. Current Development Controls

4.1 Official Plan

a) The Mississauga Official Plan designates the land use of the subject property as Residential Low Density I. This designation allows Detached Dwellings, Semi-detached Dwellings, and Duplex Dwellings.

b) The Mississauga Official Plan designates the subject property as within the Meadowvale Village Neighbourhood. There are precincts within it, and the subject property is located in the Village Precinct, which includes all of the Heritage Conservation District and additional land to the south. The Neighbourhood plan specifies:

Under the heading of Site Development Standards

16.17.1.26. The development of properties within the Heritage Conservation District and the Village Precinct will be subject to site plan control. In the case of the Heritage Conservation District, development of properties will require the approval of the local Architectural Conservation Advisory Committee (LACAC) in accordance with the *Meadowvale Village Heritage Conservation District Plan*.

Under the heading of Buildings and Spaces

16.17.1.28 Buildings and Spaces Policies apply to all lands within the Meadowvale Village Character Area. In applying the following policies, the effect of buildings and spaces on the surrounding environment should be considered equally with the function and aesthetic appeal of the site itself:

- a. Sites within the Heritage Conservation District will be subject to the policies of the *Meadowvale Village Heritage Conservation District Plan* and *Conservation Principles and Design Guidelines for the Meadowvale Village Heritage Conservation District*.

4.2 Zoning Bylaw 0225-2007, enacting by-law BL 0308/11

The subject property is zoned R1-32. R1 designation allows only detached dwellings. Among the requirements of this designation, interior lots must have minimum frontage of 22.5m and a minimum area of 750 sq m. Interior lots have minimum setbacks of 9.0m front, 7.5m rear, side yards of 1.8m on one side and 4.2m on the other. Coverage is limited to 25%.

Additionally, in an R1-32 zone the following uses/regulations shall apply:

Regulations

4.2.2.32.1 The regulations of Sentence 3.1.1.7.1 of this By-law shall not Apply (this concerns surface treatment of driveways).

4.2.2.32.2 Minimum lot area=1050 sq m.

4.2.2.32.3 Minimum combined width of side yards for 2 storey building=27% of frontage.

4.2.2.32.4 A flat roof shall not be permitted.

4.2.2.32.5 Maximum height - highest ridge: sloped roof=7.0 m.

4.2.2.32.6 Maximum gross floor area=160 sq m plus 0.10 x lot area.

4.2.2.32.7 An attached garage shall not be permitted.

4.2.2.32.8 Maximum floor area of detached garage=50 sq m.

4.2.2.32.9 Maximum projection of the front garage face of a detached garage beyond any portion of the first floor front wall or exterior side wall=0.0m.

4.2.2.32.10 Maximum driveway width=6.0m

5. The Existing Buildings

The existing house is a modest two-storey front-gable frame building, with a one-storey rear extension. The basic form is as shown in the 1904 Harry Spiers drawing in Figure 4. Since then, an enclosed front verandah has been added, with the entry—with sidelights and a transom—on the left. Also added is a narrow rear deck. The house has been clad in metal siding—mostly vertical, with horizontal clapboard near the bottom. The foundation of the house is fieldstone, that of the verandah is moraine stone cobbles. Windows are 2/2 double-hung. The second storey window on the south elevation is a pointed arch in a steep gable—both typical of Gothic revival. Other windows have flat heads under segmental trim.

The garage is a front-gabled wood-frame structure.



Figure 6. Front (east) elevation.



Figure 7. Rear view from southwest, showing one-storey tail and rear deck..



Figure 8. North elevation.



Figure 9. View from southeast.



Figure 10. Front (east) elevation of shed/garage.

6. Heritage Evaluation of the Existing Buildings

The District Inventory evaluates the dwelling as follows:

Heritage Attributes:

- The size, shape, form, massing and materials of this original building which contributes to the nineteenth century character of the village HCD
- The location of the house on the property which allows for open space to the north of the house and clear views to the front and side facades

Statement of Significance:

The property at 7076 Old Mill Lane has significance in that it (is) believed to be one of the residences drawn by artist Henry (sic) Spiers who illustrated and wrote the 1904 “Souvenir of Meadowvale”. The structure has architectural significance as it is representative of the vernacular frame structures of the area in its size, shape, form and massing, although few in number within the Village. The property has contextual significance in that it is situated on Old Mill Lane and once would have backed onto Willow Lake. The house has setbacks that allow for a high visibility on its south, east and north facades.

In my professional opinion, this evaluation is correct. The inventory notes that it served as the Village Post Office from 1977 to 1981, but this is not significant in the 200-year history of the Village. The property is part of the development of the Bristow Plan for Meadowvale house lots, and the chain of title fairly well establishes that it was once the home of Robert Golden which was drawn and published by Harry Spiers. It is representative of contemporary homes in scale, form, and original materiality, and contributes to the 19th century character of the Village. This contribution is enhanced by the clear views provided, particularly on the front and north side prospects.



Figure 11. Stucco house at 7015 Pond Street. This little cottage has a large board-and-batten addition at the rear.



Figure 12. Stucco House at 1033 Barberry and board-and batten house at 1045 beyond.

All three of the houses shown above are within 150m of the subject property.

6.1 Worthiness for Part IV Designation

Ontario Regulation 9/06 sets out the criteria for designation, referenced in Section 29(1)(a) of the *Ontario Heritage Act* as a requirement for designation under Part IV of the Act.

The Regulation states that “A property may be designated under section 29 of the Act if it meets one or more of the following criteria for determining whether it is of cultural heritage value or interest:”

1. The property has design value or physical value because it,
 - i. is a rare, unique, representative or early example of a style, type, expression, material or construction method,
 - ii. displays a high degree of craftsmanship or artistic merit, or
 - iii. demonstrates a high degree of technical or scientific achievement.
2. The property has historical value or associative value because it,
 - i. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,
 - ii. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or
 - iii. demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.
3. The property has contextual value because it,
 - i. is important in defining, maintaining or supporting the character of an area,
 - ii. is physically, functionally, visually or historically linked to its surroundings, or
 - iii. is a landmark. O. Reg. 9/06, s. 1 (2).

My evaluation of the subject property, on the basis of these criteria follows:

1. i, The existing house is not a rare, unique, or early example of its style, type, expression, material or construction method. As with any building, it is representative of something, but this is not a criterion to be applied lightly.
1. ii, The craftsmanship or artistic merit of the house is not high.
1. iii, There is no demonstration of technical or scientific achievement in the building.
2. i, There is no direct association with a theme, event, belief, activity, organization or institution that is significant to the Meadowvale community. There is an association with the person of artist Harry Spiers, who is believed to have included a drawing of the house in his 1904 pamphlet (24 pages) *A Souvenir of the Village of Meadowvale on the Credit*.
2. ii, The house does not yield or have potential to yield significant information about the community or its culture.
2. iii, There is no identified architect, artist, builder, designer, or theorist.

3. i, The identified character of the area is based on its 19th century development. The existing building helps support that character. It has undergone fewer alterations of form than most of its nearby neighbours, although the exterior material is modern. The Inventory notes it as a “representative of the vernacular frame structures of the area in its size, shape, form and massing,” but also notes that there are few such buildings “within the Village”.

3. ii, The house is linked to its surroundings by virtue of having aged in place without much change. But it should be noted that the surroundings have changed: no mill, no pond.

3. iii, The building is not a landmark.

Ontario Regulation 9/06 states that a property “may be designated” if it meets any one of nine listed criteria. Not must be designated, or even should be designated. In other words, the criteria are necessary but not sufficient for designation without further consideration.

The Ministry’s *Heritage Property Evaluation* (page 21) notes that

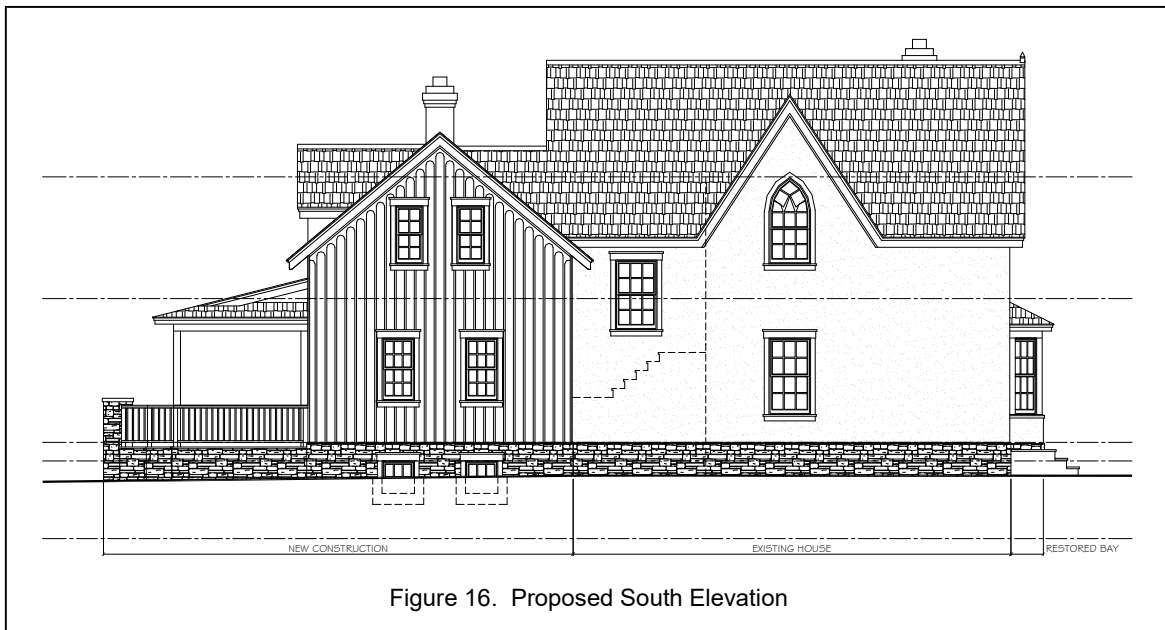
“This does not mean that the property is only evaluated within “one” category in order to allow for protection. When more categories are applied, more is learned about the property and its relative cultural heritage value or interest. As a result, a more valid decision regarding heritage conservation measures can be made.”

In my professional opinion, the property at 7076 Old Mill Lane in the Village of Meadowvale has cultural heritage interest, but does not meet the threshold for designation under Part IV of the Ontario Heritage Act. In my opinion, the protection afforded by its existing designation under Part V is appropriate.

7. The Proposal in Detail

The owners and their architect propose to alter the existing dwelling, by removing the modern metal siding, the later front porch addition, and the rear extension and deck. On the front elevation, the features shown in the Harry Spiers sketch will be restored: stucco finish, bay window, left side entry door, and pierced gingerbread barge board on the gable. On the north elevation a stilted Victorian dormer with a gothic window, matching the original on the south will be added to the roofline. At the rear of the house, a new side-gable addition will be constructed, projecting slightly to the south, and substantially to the north. The north projection will include a shed-roof porch sheltering the new main entrance on the north side of the original house. The addition will have a true board-and-batten exterior finish, distinguishing it from the original building. The main front wall of the addition is set back from the front of the original building by 9.6m (30'-6") giving prominence to the original form of the house. Interior arrangements are substantially altered, creating a modern layout.





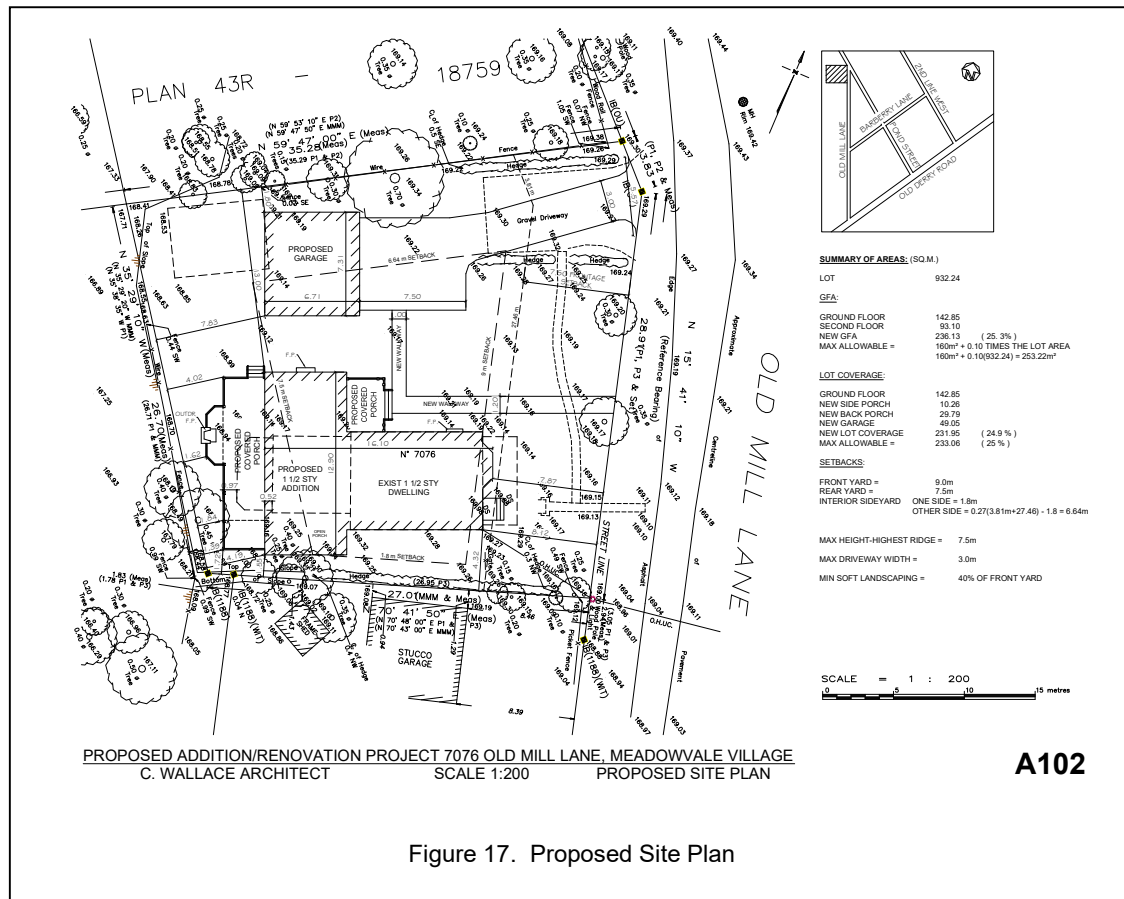


Figure 17. Proposed Site Plan

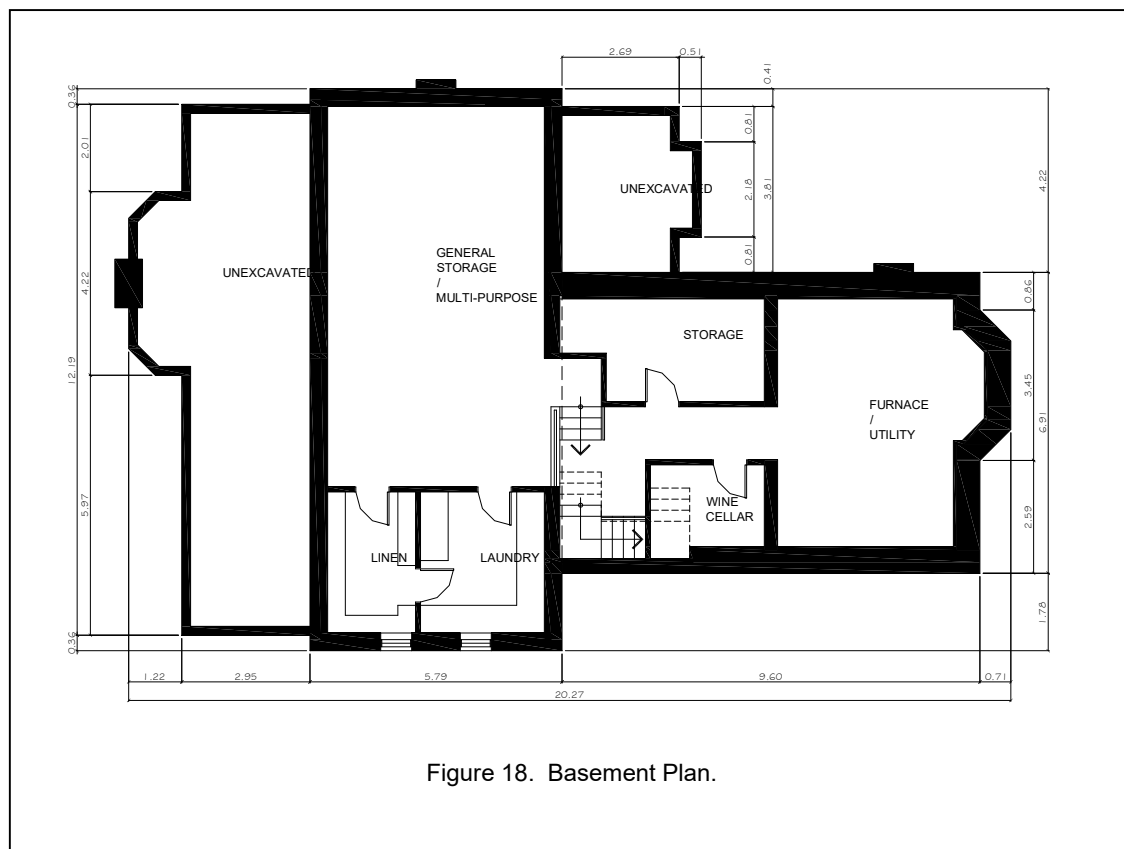
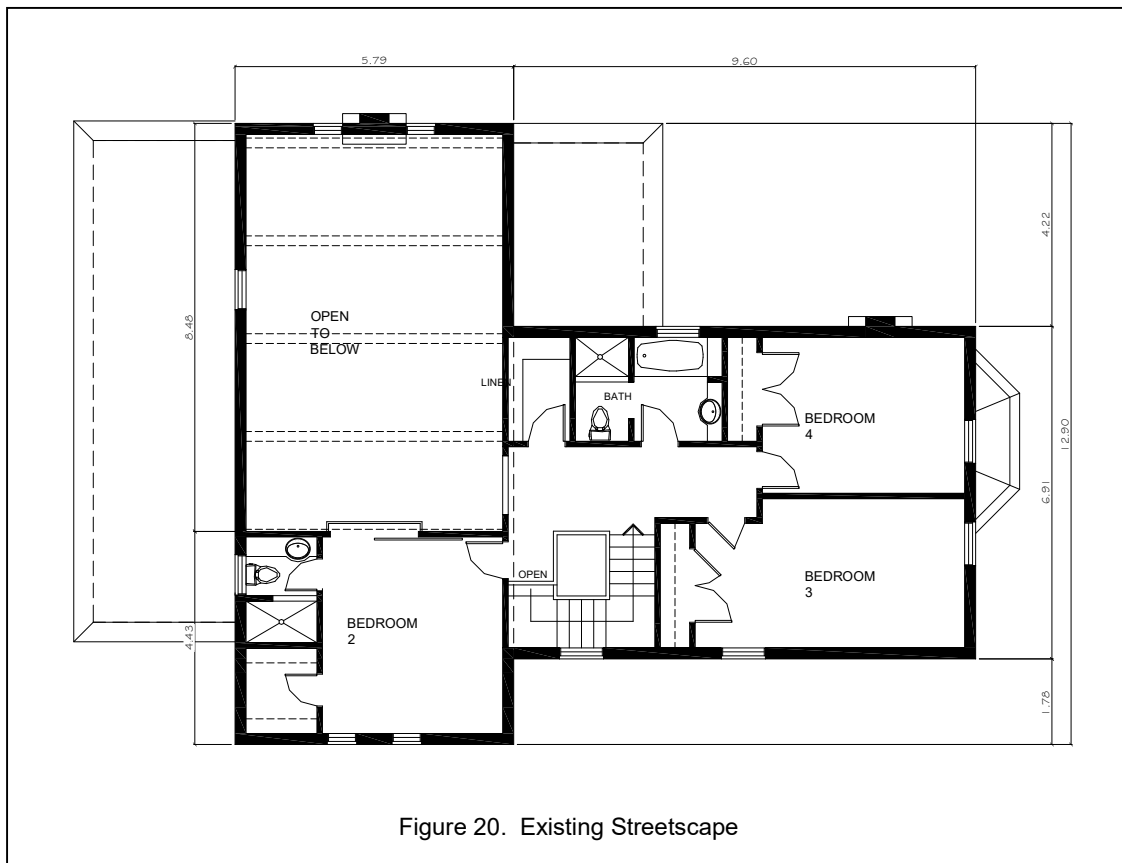
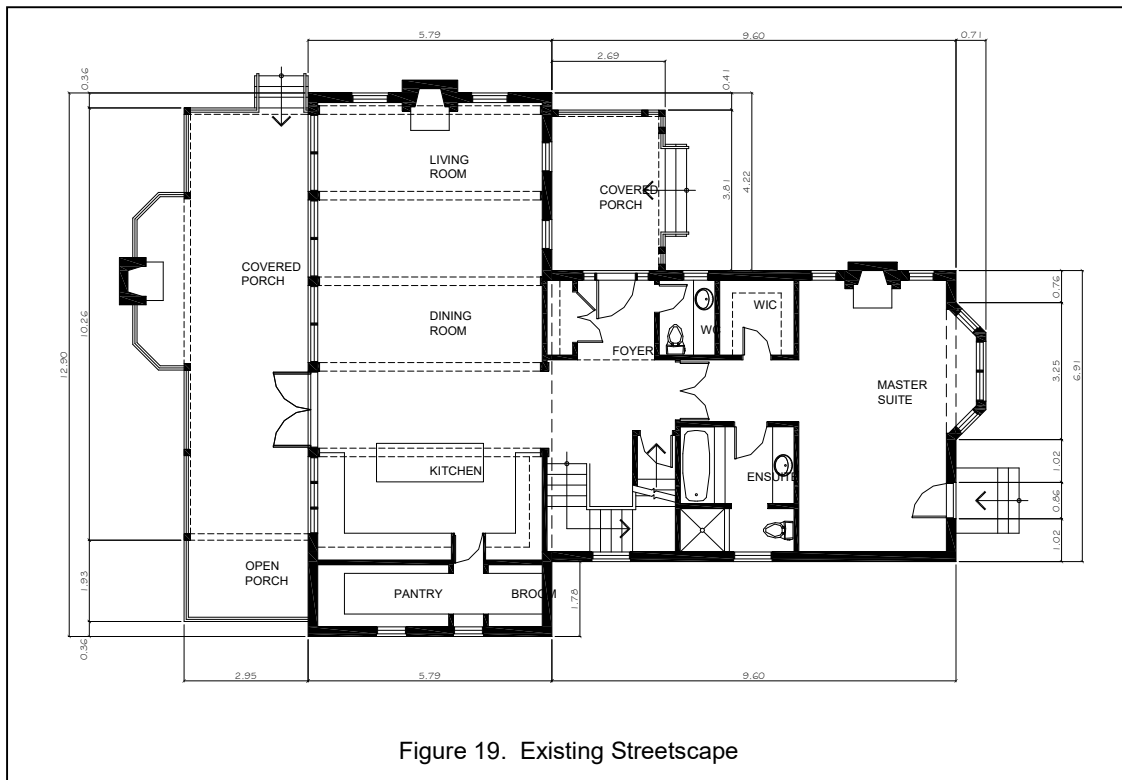


Figure 18. Basement Plan.



8. Heritage Evaluation of the Proposal

8.1. To Conserve the District Character

The Ministry of Tourism Culture and Sport describes the function of Heritage Conservation Districts in the first section of its Heritage Toolkit book *Heritage Conservation Districts: A Guide to Designation Under the Ontario Heritage Act*:

District designation enables the council of a municipality to manage and guide future change in the district, through the adoption of a district plan with policies and guidelines for conservation, protection and enhancement of the area's special character. (emphasis added.)

In other words, the Ministry recognizes that districts change. The aim is not to conserve every element in the area, but to conserve the area's overall character.

It has been common, therefore, for Heritage Conservation District Plans to make a statement about what that character is. Since the 2005 amendments to the Ontario Heritage Act, Plans have been required to contain "a statement explaining the cultural heritage value or interest of the heritage conservation district". The Toolkit describes this statement as follows:

The statement of cultural heritage value describes the heritage values that contribute to the special identity and character of the district that should be protected. A clear statement will help to promote understanding of the values and attributes and will assist decision-makers in ensuring that future changes and interventions contribute to, rather than detract from, the character of the area. Statements should be brief and succinct and should relate specifically to the identified values and attributes of the district.

The 2003 *Conservation Principles and Design Guidelines for the Meadowvale Village Heritage Conservation District*, describes the character of the Village:

The historic village of Meadowvale is situated on land first worked by Irish immigrants in 1819. Loggers were attracted to the stands of white pine, and by 1836, the village boasted a sawmill and 250 people. The town reached its greatest size just 30 years later when the Toronto-base distillery firm Gooderham and Worts purchased land, timber and mills to produce whisky barrels. The mill, millpond, and raceway gave shape to the town; though the mill is long gone, the historic road patterns and many period houses remain. Meadowvale derives its fundamental character from that era. (emphasis added.)

The *Meadowvale Village Heritage Conservation District Plan, 2014* expands on this with a **Description of Heritage Attributes**:

- a significant location, adjacent to the Credit River, in a cultural heritage landscape of integrated natural and cultural heritage elements within the river's low floodplain to the gentle sloping ridge;
- an ecological feature of the floodplain meadow on the Credit River which has existed for hundreds of years;
- a land pattern that retains the layout and plan of lots since the mid nineteenth century;
- a spatial organization of narrow soft landscaped streets with no shoulder, mature trees and a visual relationship that blends from public to private space among front and side yards void of privacy fencing;
- long term tradition of streetscapes with no formalized parking, sidewalks (except on Old Derry Road), basic signage and limited modest lighting;

- a consistency of building types, modest in architectural detail, vernacular style and size, reflecting the nineteenth century development of a milling village;
- later twentieth century residential styles that are compatible with the district character from a scale, materials and massing perspective;
- a common use of stacked plank construction with exterior stucco finish or wood siding, one-and-a-half storeys and limited use of brick;
- structures of compatible size, shape, form and style, although not necessarily of historic significance, contribute to the overall character of the village;
- visual identity of rural character roadway entry points to the village from the west on Old Derry Road and from the north along Second Line West, and the open green space of Old Ridge Park to the south;
- individual properties of particular character and significance are identified in “The Meadowvale Village Heritage Conservation District Plan Review List of Properties”.

Note: Items underlined above are characteristics supported by the proposal.

8.2 Consultant’s Assessment

In my professional opinion, the proposed alterations to the property at 7076 Old Mill Lane, in the Meadowvale Village Heritage Conservation District, merits approval. It conserves the heritage character of the District, in the context of its location on Old Mill Lane.

- The proposal improves the compatibility of the building with the heritage character of the District. The building, as it exists, has maintained its basic form, but has suffered incompatible alterations in the addition of the enclosed front porch, the installation of metal siding, and the removal of the gingerbread bargeboard and original front bay window.
- The spatial character is conserved, with the proposed addition being modest in size and set well back toward the rear of the building.
- The existing garage/shed, which is not served by the driveway, is being replaced by a new garage, of a traditional size and form, in a more useful location and configuration.
- The character of the landscaping is conserved.
- The architectural style and materiality of the proposal is compatible with historic styles found in the District, without mimicking them. The 1 ½-storey form is explicitly listed as one of the District’s heritage attributes in the Phase 1 Report.
- The exterior finish of the existing portion of the house is stucco, in keeping with the appearance shown in the Spiers drawing. The exterior finish of the new addition is board and batten, which has local precedent, and distinguishes the new work from the original, in keeping with the seventh of the Ministry’s *Eight Guiding Principles in the Conservation of Built Heritage Properties*.
- The proposal produces a sustainable use as a modern single-family dwelling. This is in keeping with Clause 9 of the ICOMOS *Charter for the Conservation of Historic Towns and Urban Areas* (commonly known as the *Washington Charter*): “The improvement of housing should be one of the basic objectives of conservation.” In my professional opinion the Washington Charter is applicable to Ontario’s Heritage Conservation Districts.

9. Bibliography

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City of Mississauga: *Meadowvale Village Heritage Conservation District Plan, 2014*

City of Mississauga: *Meadowvale Village Heritage Conservation District Plan, 2014 Property Inventory, Schedule B.1*

City of Mississauga: *Meadowvale Village Heritage Conservation District Plan Review, Draft Design Guidelines and Policies, 2013*

ICOMOS: *Charter for the Conservation of Historic Towns and Urban Areas*, 1987

Ontario: *Ontario Heritage Act*, R.S.O. 1990, Chapter O. 18, as amended.

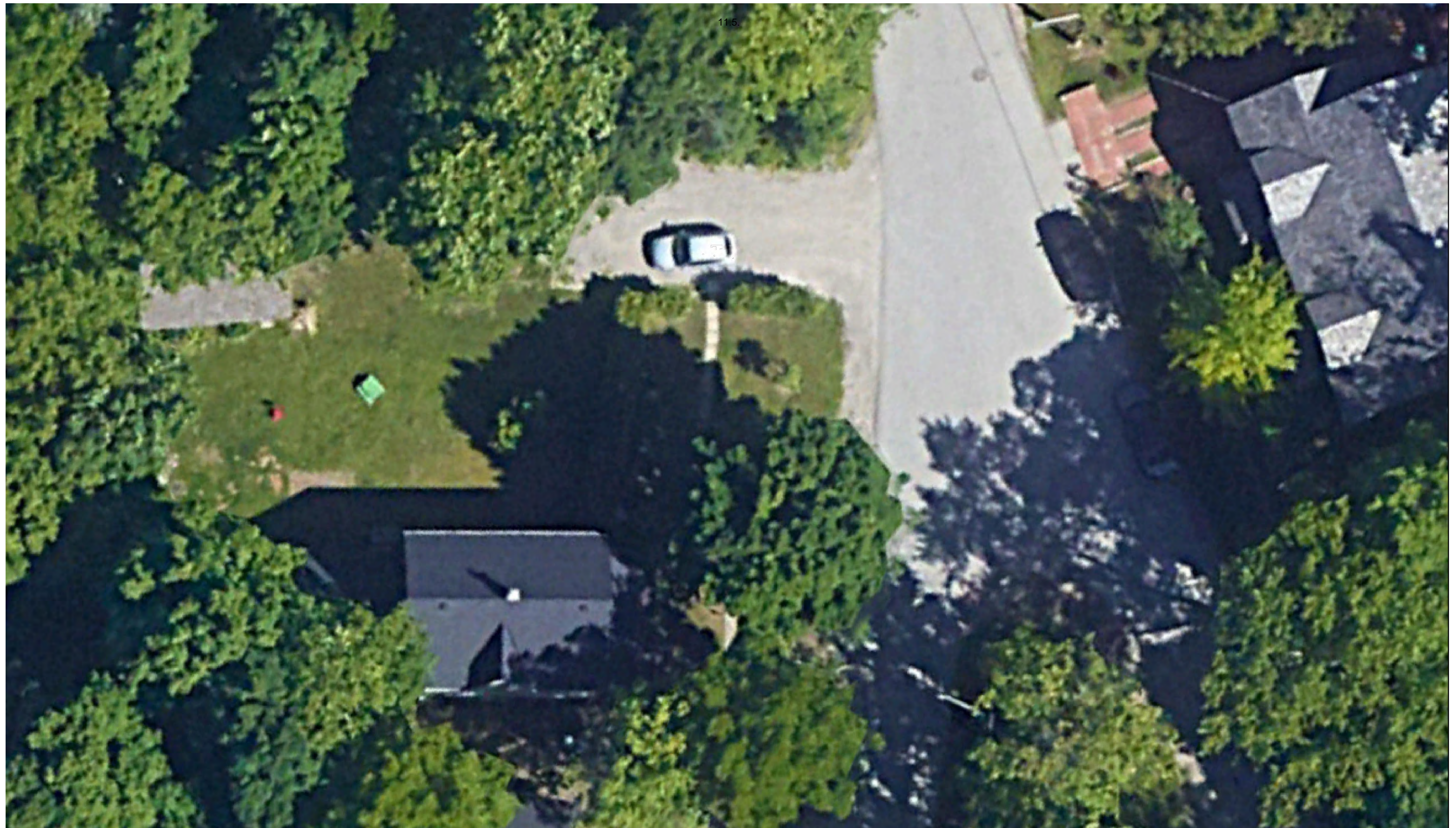
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A100	AIRPHOTO
A101	AS SKETCHED IN 1904
A102	EXISTING SURVEY/SITE PLAN
A103	PROPOSED SITE PLAN
A104	COLOURED SITE PLAN
A105	STREETSCAPE - BUILDINGS ONLY
A106	STREETSCAPE
A200	BASEMENT PLAN
A201	1ST FLOOR PLAN
A202	2ND FLOOR PLAN
A203	ROOF PLAN
A300	EAST ELEVATION (FRONT)
A301	NORTH ELEVATION
A302	WEST ELEVATION
A303	SOUTH ELEVATION
A400	SECTION A
A401	SECTION B
A402	SECTION C
A403	SECTION D
A500	GARAGE PLANS

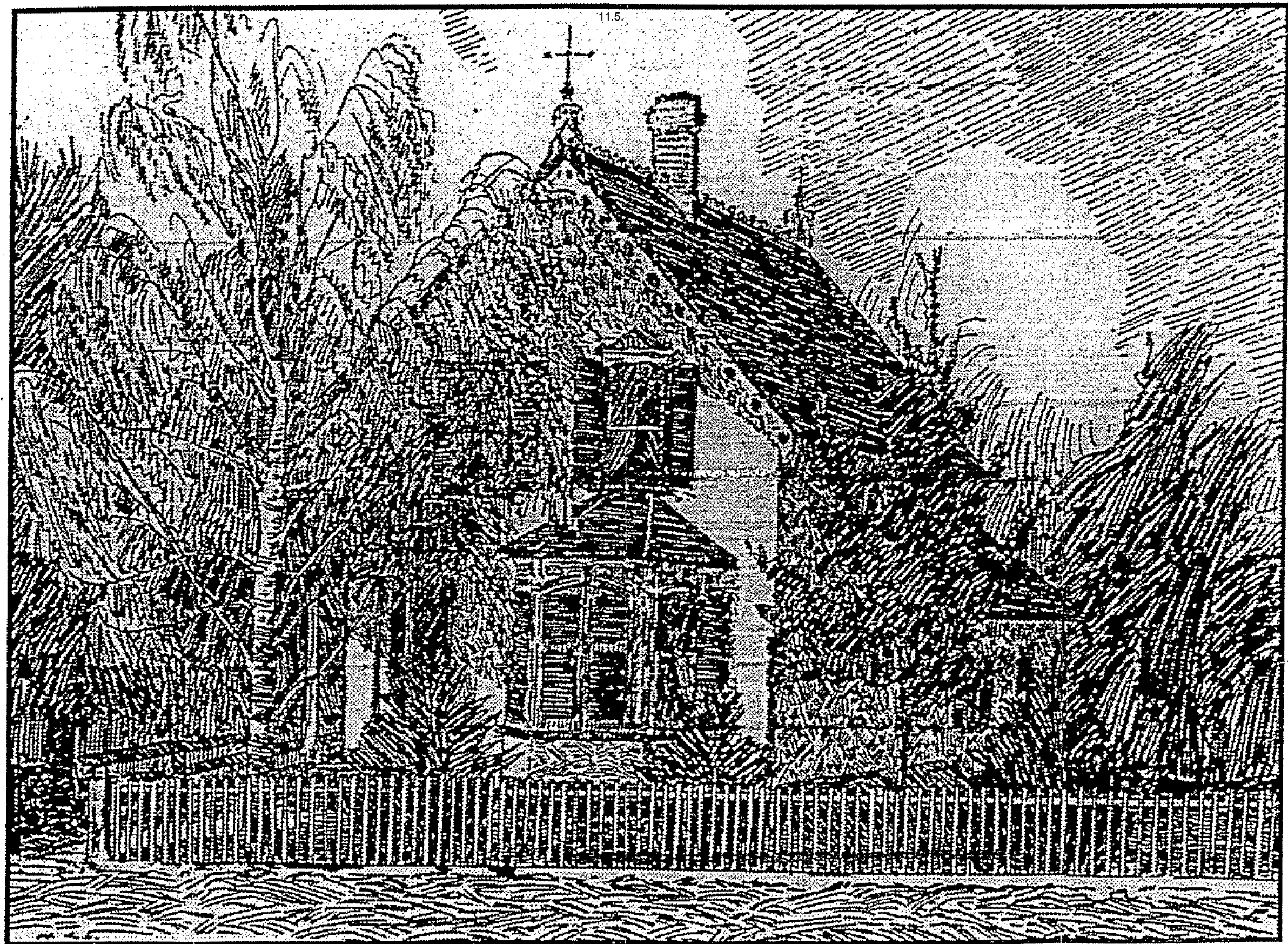


PROPOSED ADDITION/RENOVATION PROJECT
C. WALLACE ARCHITECT

SCALE 1/16" = 1'-0"

7076 OLD MILL LANE, MEADOWVALE VILLAGE
AIR PHOTO - EXISTING CONITIONS

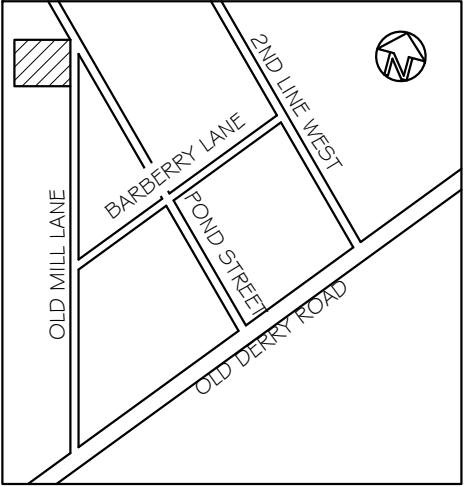
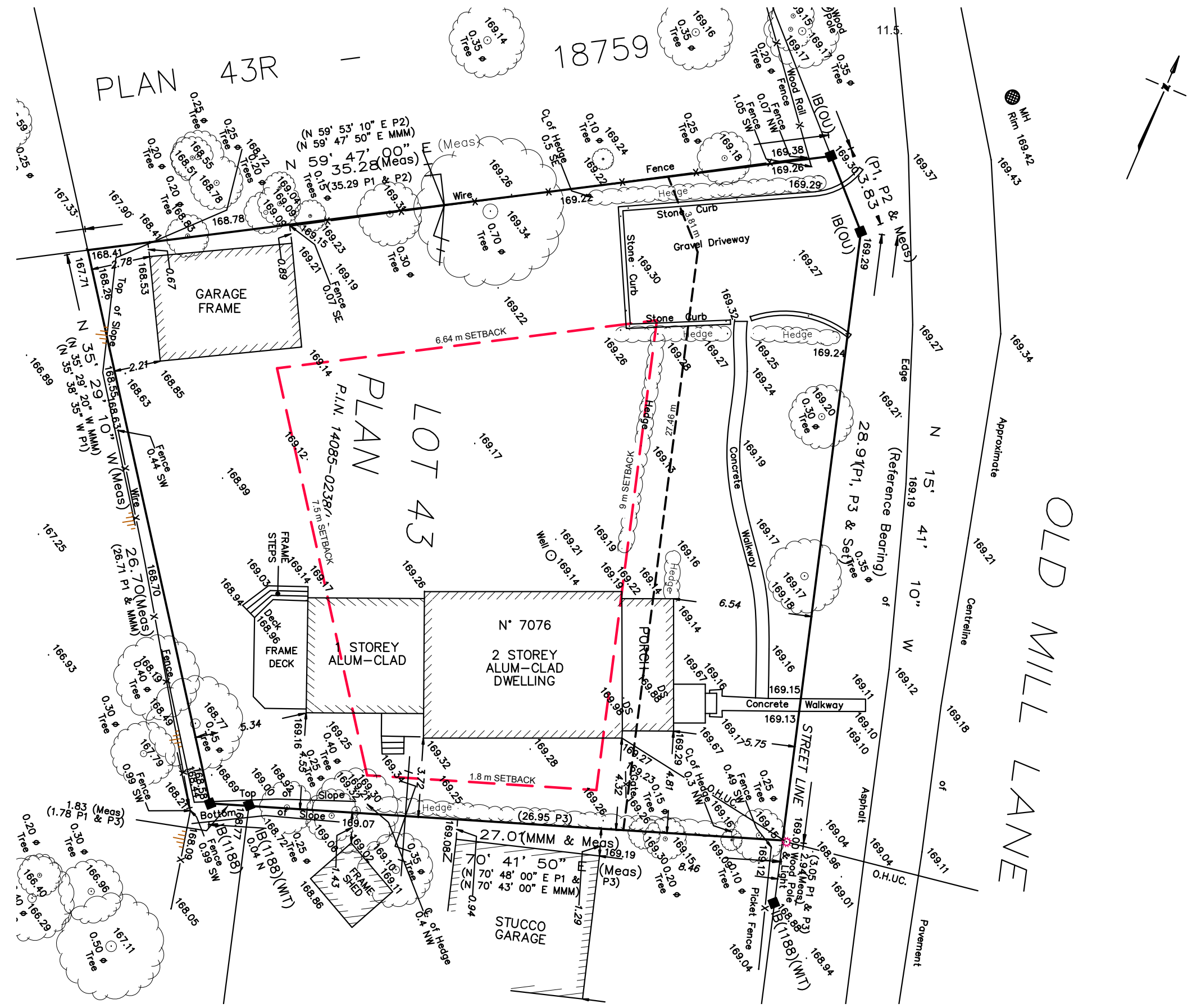
A100



PROPOSED ADDITION/RENOVATION PROJECT
C. WALLACE ARCHITECT

'076 OLD MILL LANE, MEADOWVALE VILLAGE
7076 OLD MILL LANE - AS SKETCHED IN 1905

A101



SUMMARY OF AREAS: (SQ.M.)

LOT	932.24
GFA:	
GROUND FLOOR	109.36
SECOND FLOOR	64.41
EXISTING GFA	173.77 18.6 %
MAX ALLOWABLE =	160m² + 0.10 TIMES THE LOT AREA
	160m² + 0.10(932.24) = 253.22m²

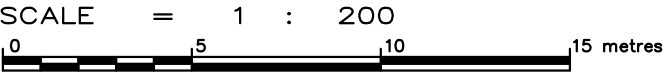
LOT COVERAGE:

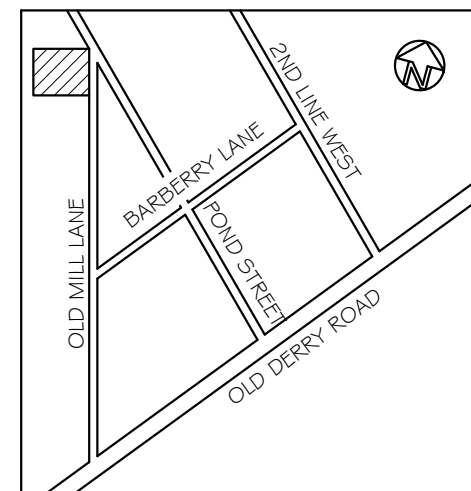
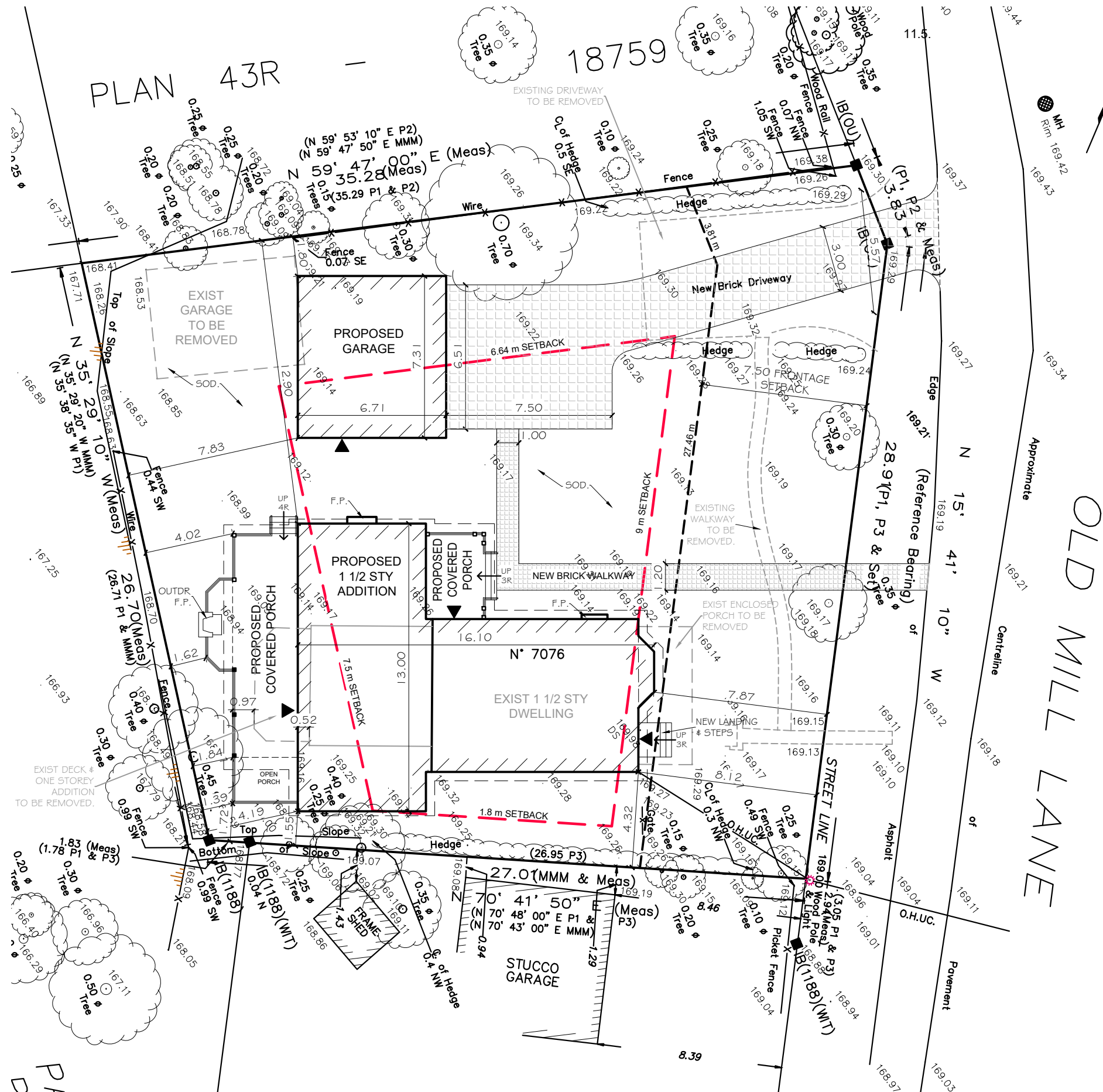
GROUND FLOOR	109.36
GARAGE	33.07
EXISTING LOT COVERAGE	142.43 15.3 %
MAX ALLOWABLE =	233.06 25 %

SETBACKS:

FRONT YARD =	9.0m
REAR YARD =	7.5m
INTERIOR SIDEYARD	ONE SIDE = 1.8m
	OTHER SIDE = .27(3.81m+27.46) - 1.8 = 6.64m

MAX HEIGHT-HIGHEST RIDGE =	7.5m
MAX DRIVEWAY WIDTH =	3.0m
MIN SOFT LANDSCAPING =	40% OF FRONT YARD





SUMMARY OF AREAS: (SQ.M.)

LOT	932.24
GFA:	
GROUND FLOOR	143.53
SECOND FLOOR	93.50
NEW GFA	237.03 (25.4%)
MAX ALLOWABLE =	160m² + 0.10 TIMES THE LOT AREA
	160m² + 0.10(932.24) = 253.22m²

LOT COVERAGE:

GROUND FLOOR	143.53
NEW SIDE PORCH	10.53
NEW BACK PORCH	41.00
NEW GARAGE	49.05
NEW LOT COVERAGE	244.11 (26.2%)
MAX ALLOWABLE =	233.06 (25%)

SETBACKS:

FRONT YARD =	9.0m
REAR YARD =	7.5m
INTERIOR SIDEYARD	ONE SIDE = 1.8m
	OTHER SIDE = 0.27(3.81m+27.46) - 1.8 = 6.64m

MAX HEIGHT-HIGHEST RIDGE = 7.5m

MAX DRIVEWAY WIDTH = 3.0m

MIN SOFT LANDSCAPING = 40% OF FRONT YARD

SCALE = 1 : 200

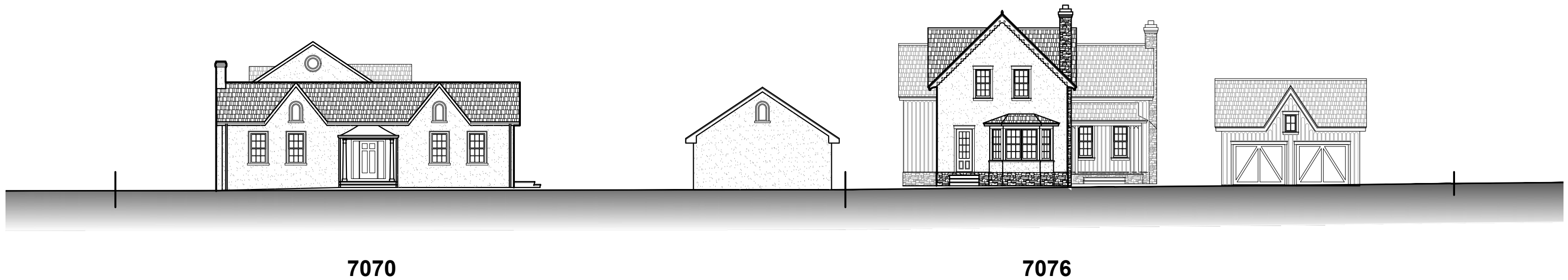
PROPOSED ADDITION/RENOVATION PROJECT, 7076 OLD MILL LANE, MEADOWVALE VILLAGE
C. WALLACE ARCHITECT SCALE 1:200 PROPOSED SITE PLAN

A103



PROPOSED ADDITION/RENOVATION PROJECT, 7076 OLD MILL LANE, MEADOWVALE VILLAGE
 C. WALLACE ARCHITECT SCALE 1/16"=1'-0" SITE PLAN

A104



PROPOSED ADDITION/RENOVATION PROJECT
C. WALLACE ARCHITECT

SCALE 1:200

7076 OLD MILL LANE, MEADOWVALE VILLAGE
STREETSCAPE - BUILDINGS ONLY

A105

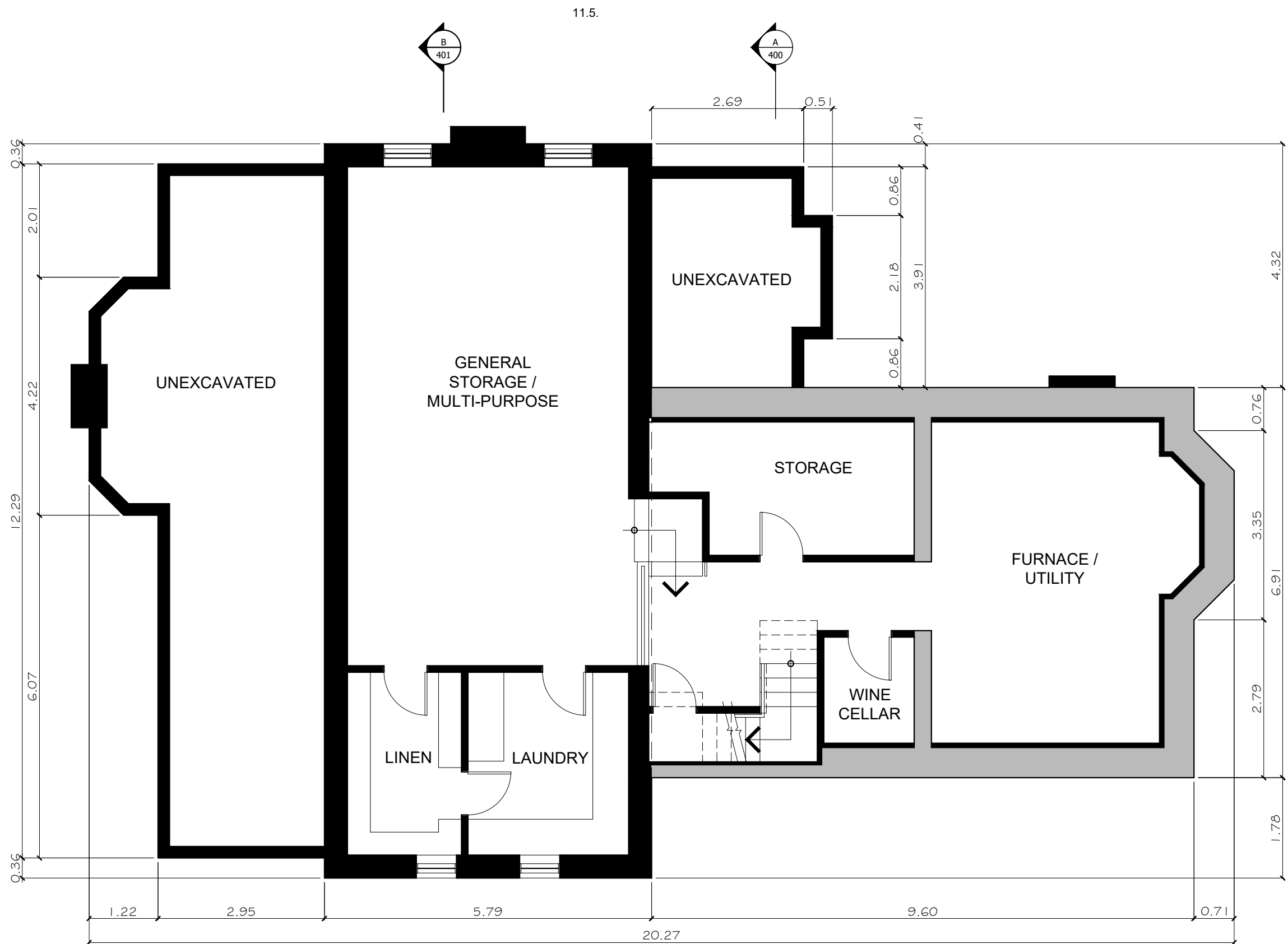


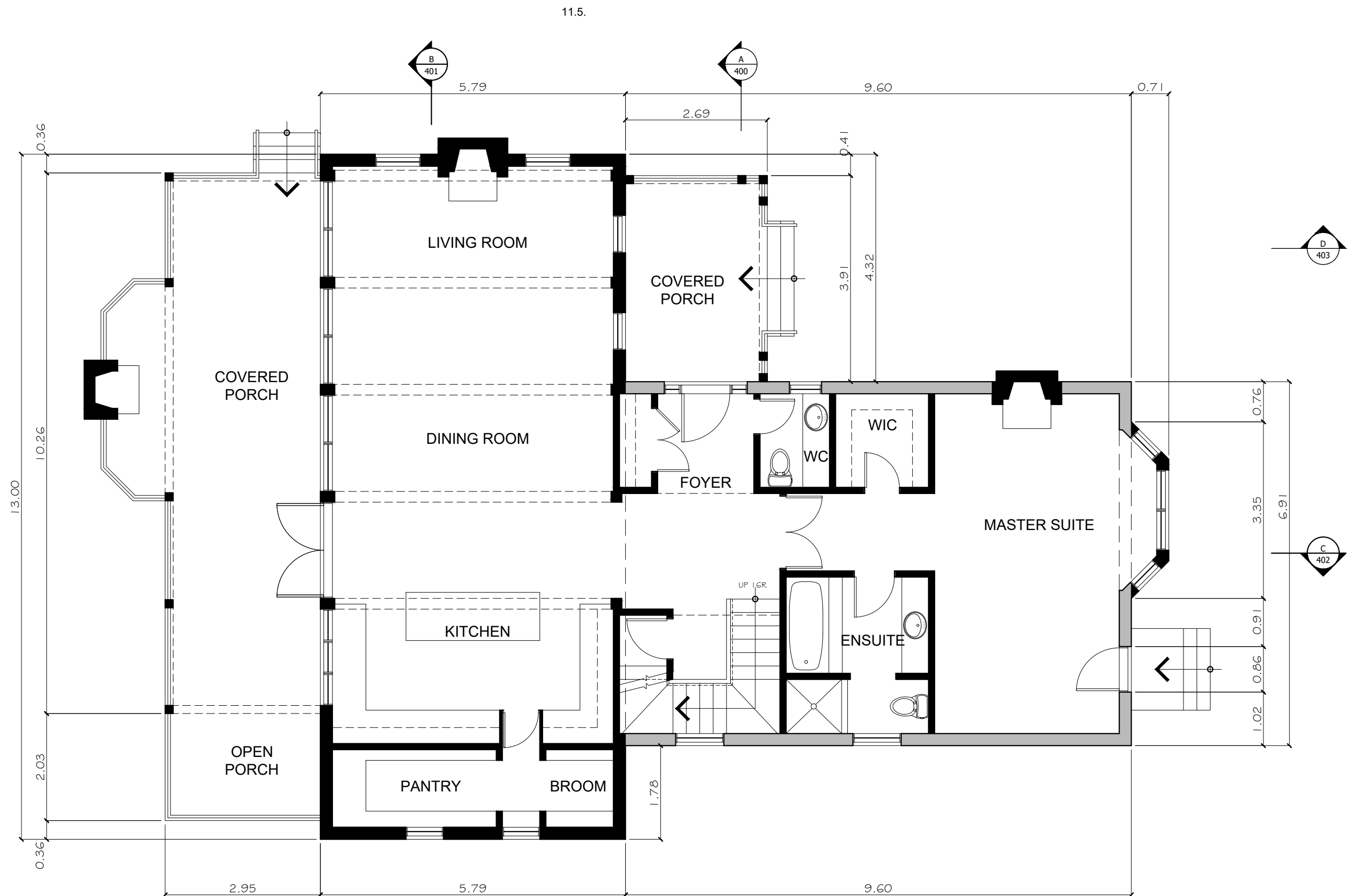
PROPOSED ADDITION/RENOVATION PROJECT
C. WALLACE ARCHITECT

SCALE 1:200

7076 OLD MILL LANE, MEADOWVALE VILLAGE
STREETSCAPE WITH TREES

A106





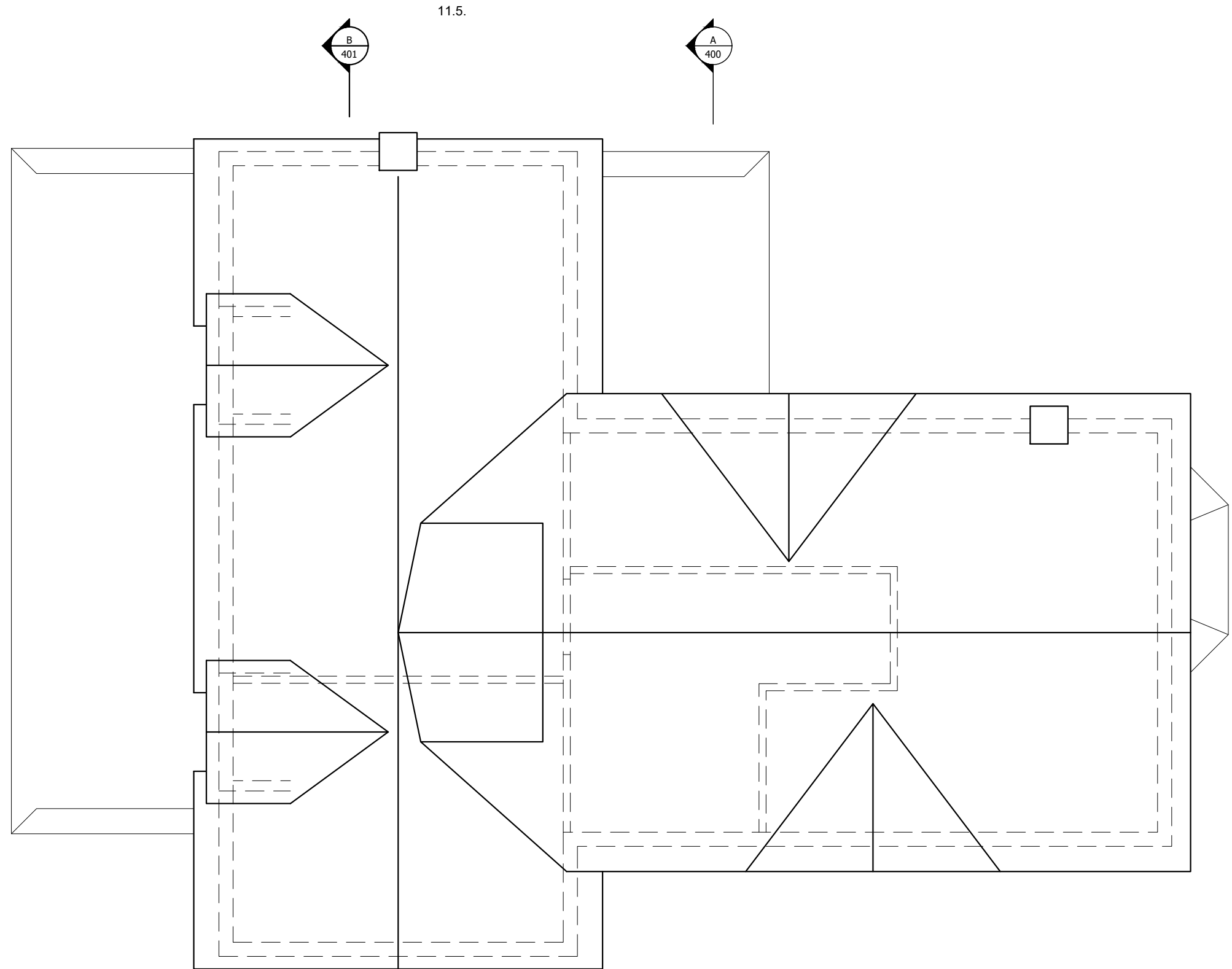
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C. WALLACE ARCHITECT

SCALE 1:75

7076 OLD MILL LANE, MEADOWVALE VILLAGE
GROUND FLOOR PLAN

A201





PROPOSED ADDITION/RENOVATION PROJECT
C. WALLACE ARCHITECT

SCALE 1:75

7076 OLD MILL LANE, MEADOWVALE VILLAGE
ROOF PLAN

A203





PROPOSED ADDITION/RENOVATION PROJECT
C. WALLACE ARCHITECT

SCALE 1:75

7076 OLD MILL LANE, MEADOWVALE VILLAGE
NORTH ELEVATION

A301



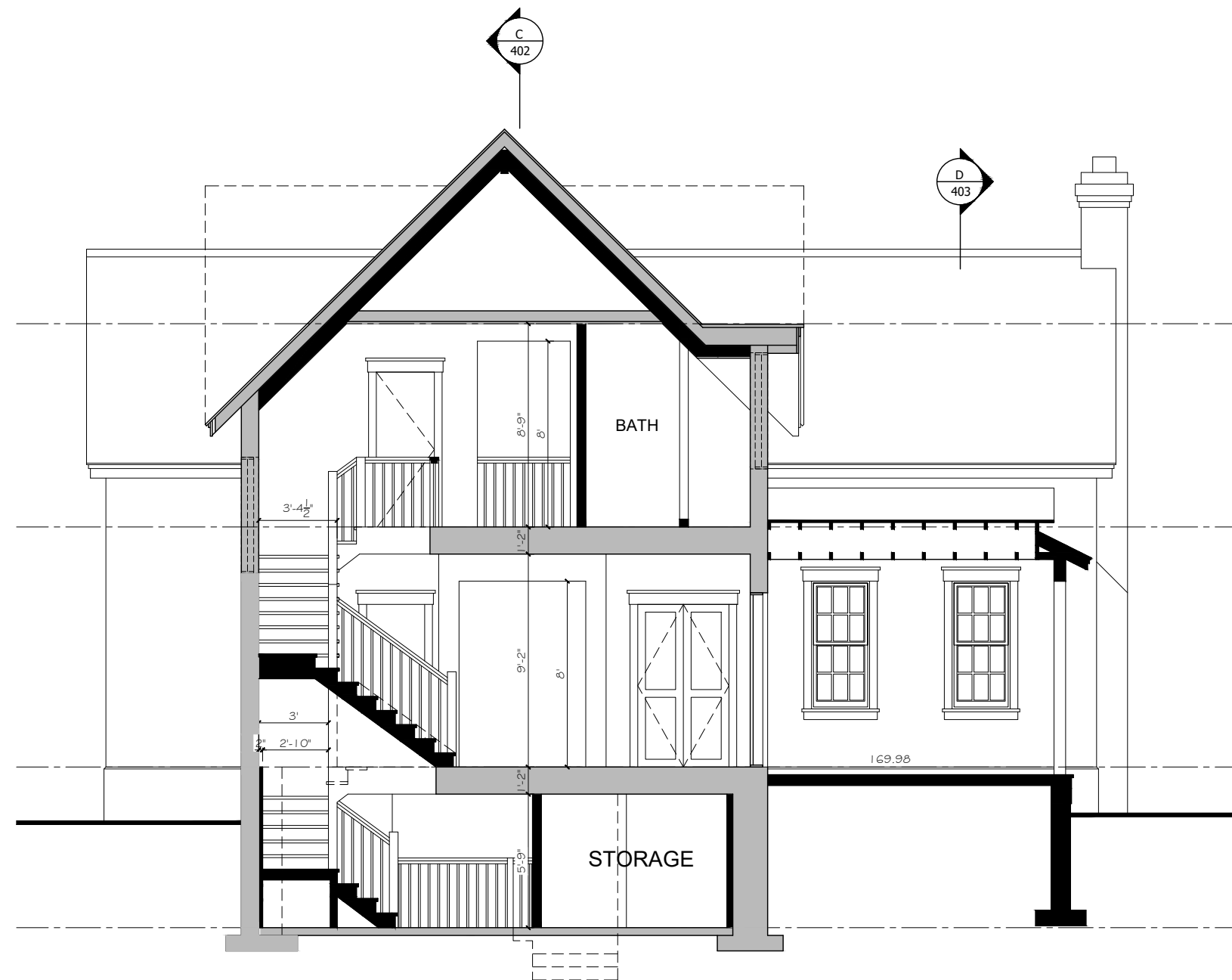
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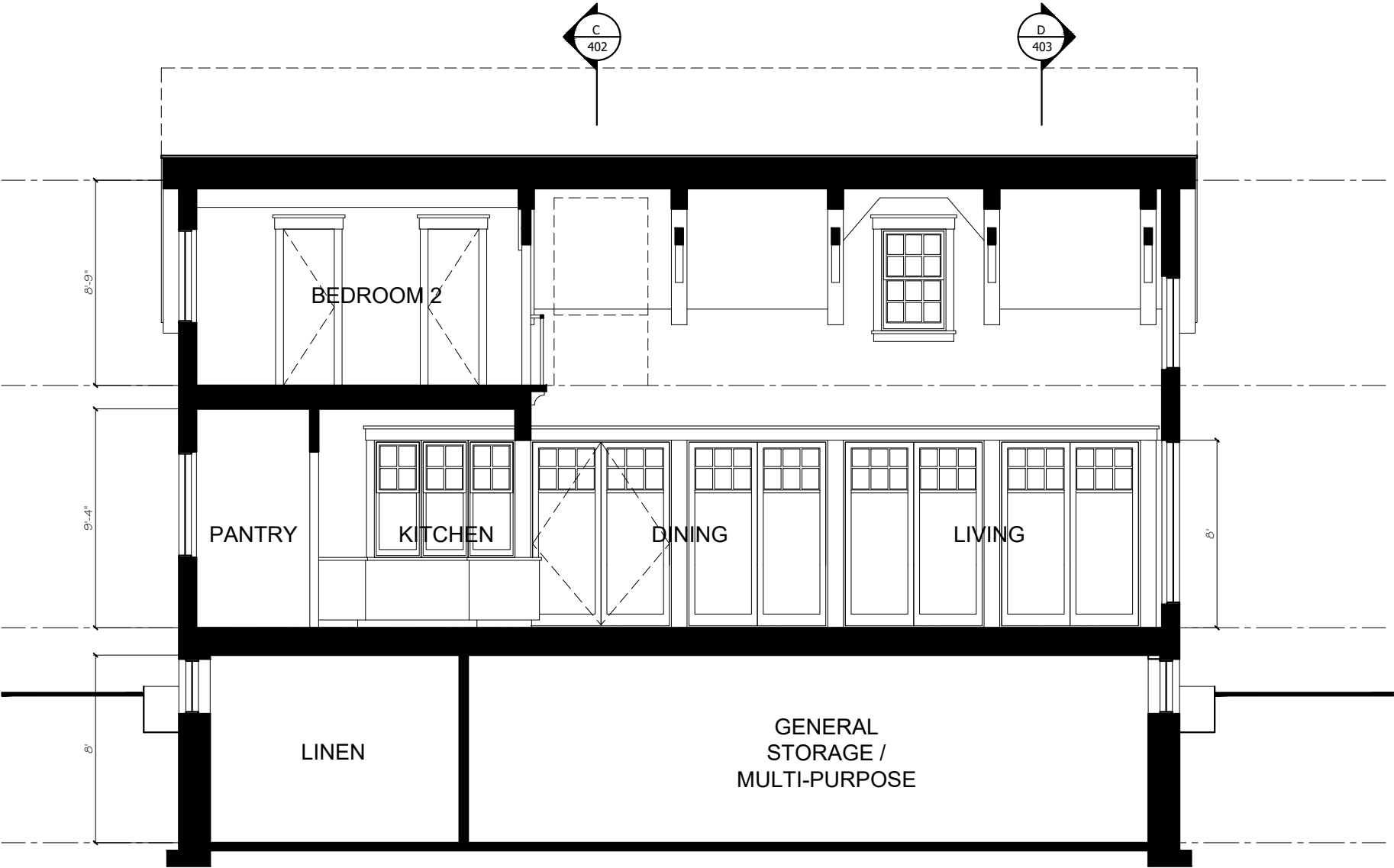
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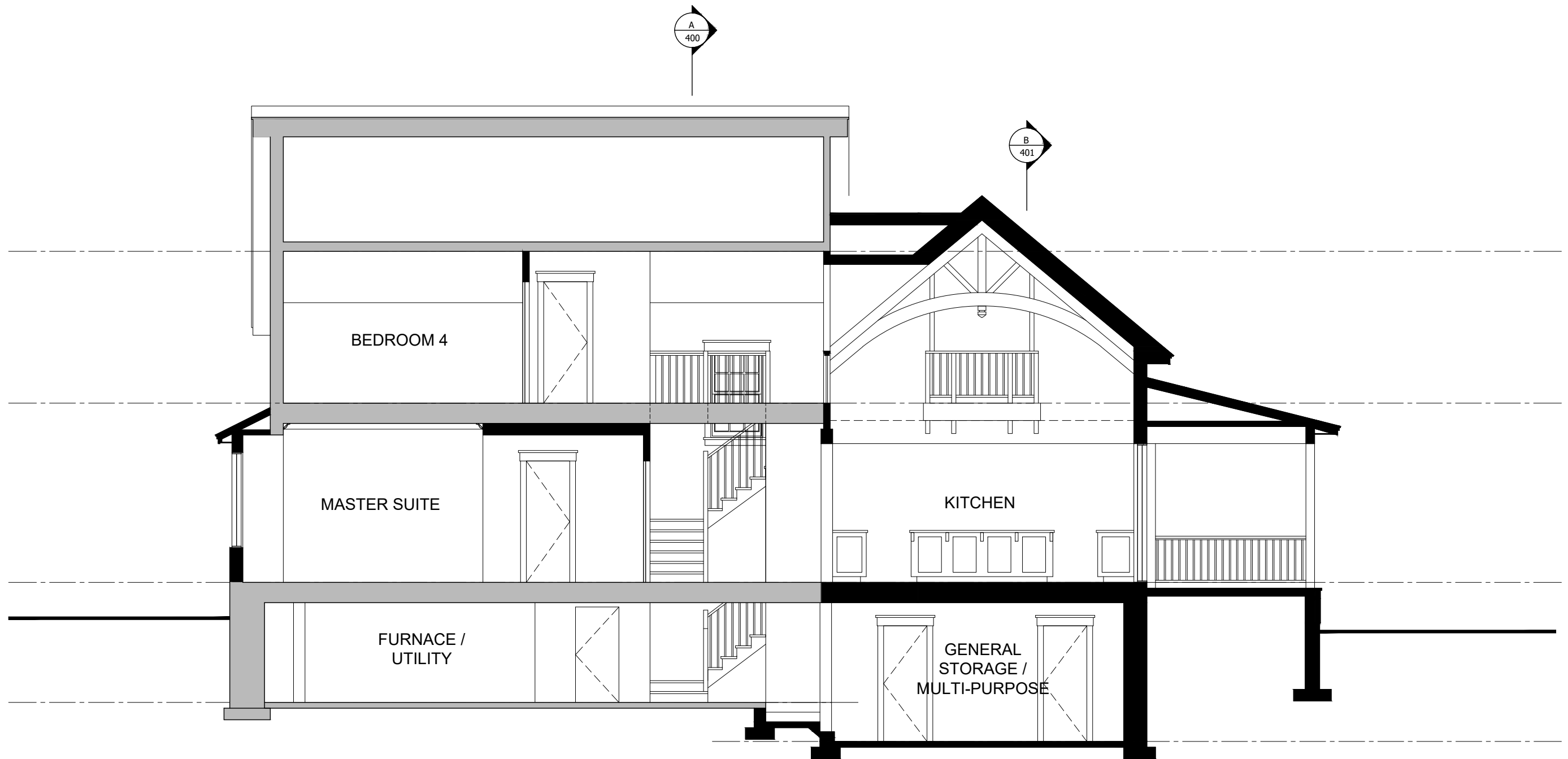
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WEST ELEVATION

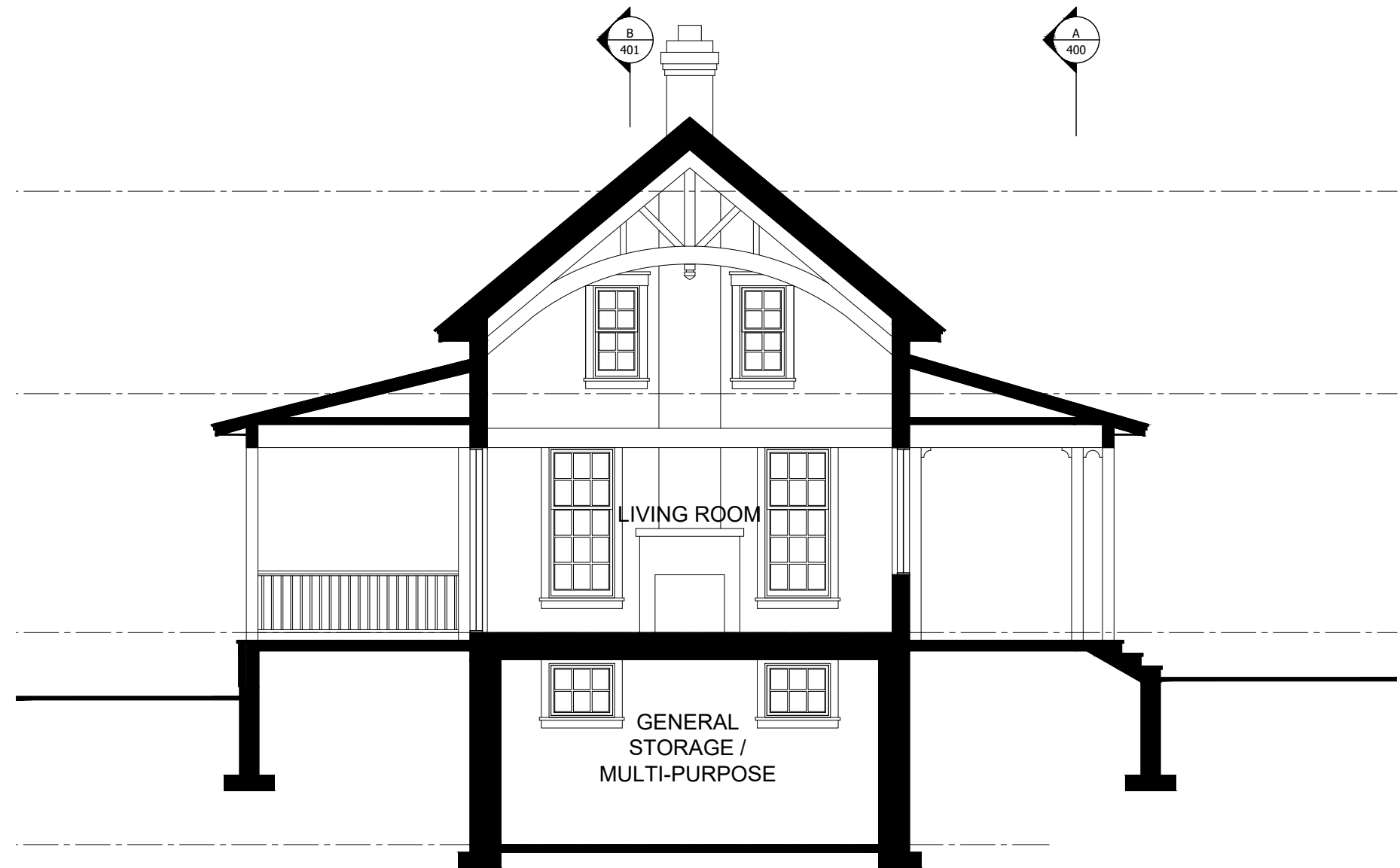
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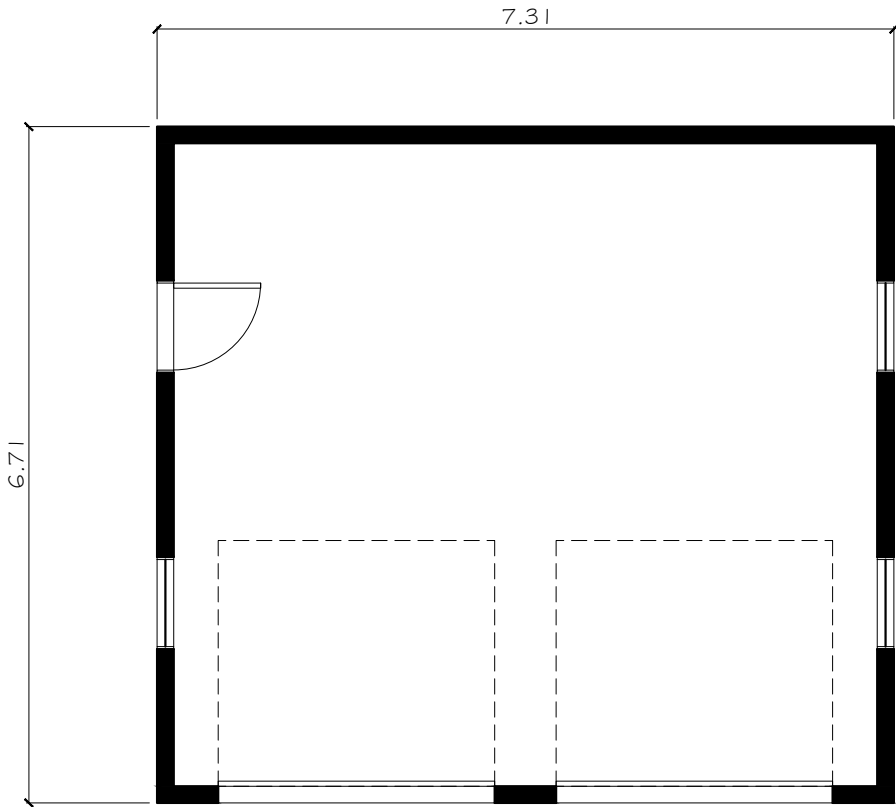




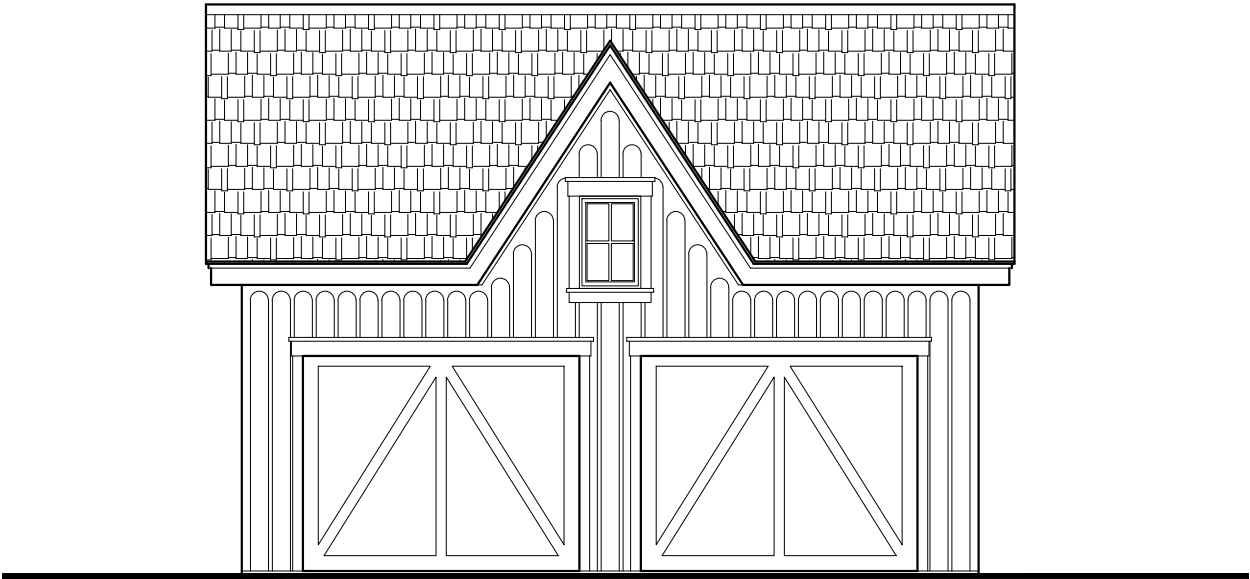




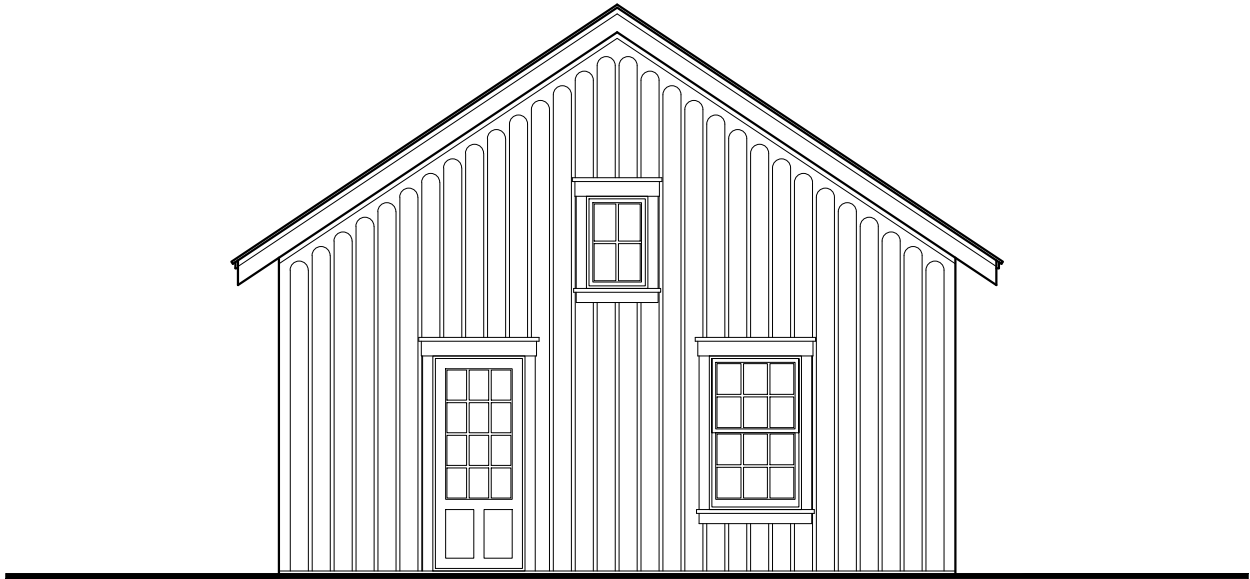




FLOOR PLAN



FRONT (EAST) ELEVATION



SOUTH ELEVATION

City of Mississauga

Corporate Report



<p>Date: April 24, 2020</p> <p>To: Mayor and Members of Council</p> <p>From: Paul Mitcham, P.Eng, MBA, Commissioner of Community Services</p>	<p>Originator's files:</p> <hr/> <p>Meeting date: May 20, 2020</p>
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Subject

Request to Demolish a Heritage Listed Property: 2740 Derry Road East (Ward 5)

Recommendation

That the structure at 2740 Derry Road East, which is listed on the City's Heritage Register, is not worthy of heritage designation, and consequently, that the owner's request to demolish, as per the Corporate Report dated April 24, 2020, from the Commissioner of Community Services be approved.

Background

Section 27.3 of the *Ontario Heritage Act* states that structures or buildings on property listed on the City's Heritage Register cannot be removed or demolished without at least 60 days' notice to Council. This legislation allows time for Council to review the property's cultural heritage value to determine if the property merits designation. The current state of emergency related to COVID-19 has resulted in the Province issuing emergency orders under Regulation 73/20 of the *Emergency Management and Civil Protection Act*, which suspends the legislated timeline in the *Ontario Heritage Act*.

This project is deemed as essential as it relates to the ongoing development of Greater Toronto Airport Authority lands and so permit approval is required to allow this demolition to continue at this time.

The structure located at the above address is a cogeneration plant which was built in 1992. It was operated by TransAlta and used to supply power to the adjacent Boeing facility. The cogeneration plant has now been decommissioned and the property is being remediated.

The owner of the subject property has submitted a heritage permit application to demolish the existing cogeneration plant. The subject property is listed on the City's Heritage Register as it was formally part of the overall Boeing facility and part of the former Victory Aircraft manufacturing facility which was responsible for the production of most of Canada's historic wartime aircraft.

Comments

The applicant has provided a Heritage Impact Assessment (Appendix 1). The consultant has concluded that the cogeneration plant at 2740 Derry Road East is not worthy of designation.

The HIA states that there will be no negative impacts to the historic character of the property through the demolition of the cogeneration plant.

Staff concurs that the cogeneration plant does not contribute to the overall heritage of the property. While the property itself has a storied history important to Malton and Mississauga's wartime efforts, the structure in is not representative of this part of the property's history. Staff finds that there is insufficient evidence to recommend designation for this structure.

Financial Impact

There is no financial impact resulting from the recommendation in this report.

Conclusion

The owner of 2740 Derry Road East has requested permission to demolish a structure on a property that is listed on the City's Heritage Register. The applicant has submitted a Heritage Impact Assessment that provides information which does not support the building's merit for designation under the *Ontario Heritage Act*.

Attachments

Appendix 1: Heritage Impact Assessment



Paul Mitcham, P.Eng, MBA, Commissioner of Community Services

Prepared by: John Dunlop, Supervisor, Heritage Planning

2740 DERRY ROAD EAST

HERITAGE IMPACT ASSESSMENT | MARCH 20, 2020



Project # 20-038-01
Prepared by AP/DE/PP/AB

ERA

PREPARED BY:

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EXECUTIVE SUMMARY

ERA Architects Inc. ("ERA") has prepared this Heritage Impact Assessment ("HIA") on behalf of TransAlta Corporation ("TransAlta") for the co-generation plant (the "site") at 2740 Derry Road East in the City of Mississauga. The property currently contains a decommissioned co-generation plant that was constructed in c. 1992 to supply power to the adjacent Boeing aircraft manufacturing facility, which was demolished in 2005. The property is currently leased by TransAlta Corporation from the owner, The Boeing Company ("Boeing").

The property is listed on the City of Mississauga Heritage Register, primarily for its association with the aviation history of Malton. The Reasons for Listing supplied by the City of Mississauga identifies features of the site that were removed in 2005.

TransAlta is seeking approval from the City to demolish the co-generation plant so that it can return the site to Boeing in a vacant condition. In the future, Boeing intends to transfer the property to the Greater Toronto Airport Authority.

ERA has conducted a site visit, background research and analysis, evaluated the property under Ontario Regulation 9/06 (Criteria for Determining Cultural Heritage Value or Interest), and considered the potential impact of the demolition of the co-generation plant.

While the property itself has historical associations with Canadian aerospace history, ERA has evaluated the site and determined that it does not represent a significant cultural heritage resource, nor does it merit designation under Part IV of the Ontario Heritage Act. The only physical fabric that remains on the property is the co-generation plant, which was a late addition to the site and is a utilitarian structure that is incidental to the aviation legacy of the broader lands.

It is our opinion that the co-generation plant does not have cultural heritage value and that its removal will not give rise to adverse heritage impacts.

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1 INTRODUCTION

1.1 SCOPE OF THE REPORT

ERA has been retained to prepare this HIA to consider the potential impacts of the demolition of the existing c. 1992 co-generation plant on the site's identified cultural heritage value. This HIA has been prepared for the associated demolition permit application #BPA-54682. According to the City of Mississauga Heritage Impact Assessment Terms of Reference (June, 2017):

A Heritage Impact Assessment is a study to determine the impacts to known and potential heritage resources within a defined area proposed for future development. The study would include an inventory of all heritage resources within the planning application area. The study results in a report which identifies all known heritage resources, an evaluation of the significance of the resources, and makes recommendations toward mitigation measures that would minimize negative impacts to those resources.

This HIA has considered the applicable provincial and municipal heritage policy framework. Multiple sources of data have been collected, sorted and analyzed for this assessment. Both primary and secondary sources have been drawn from, including: historical maps, atlases, aerial photographs, archival photographs, the City of Mississauga website, and from observations made during a site visit.

1.2 PRESENT CLIENT CONTACT

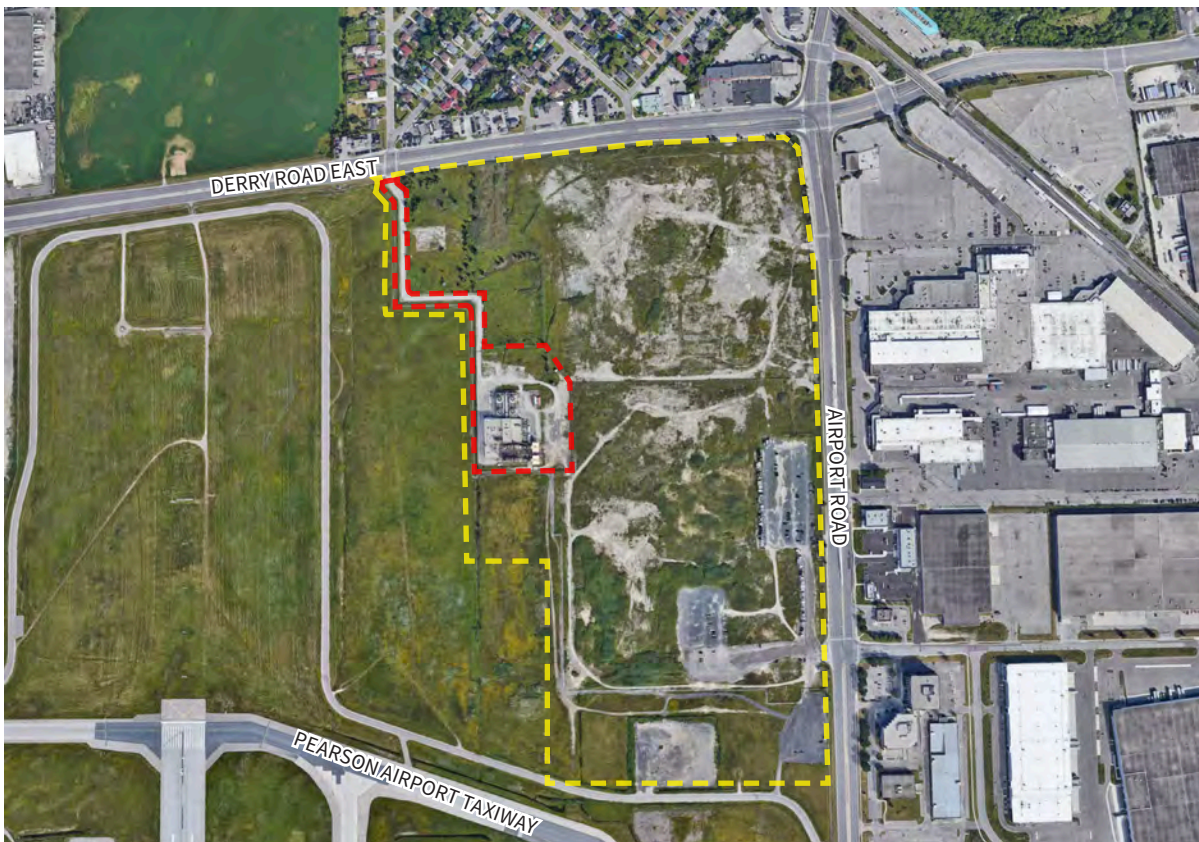
c/o Daniel Morais, TransAlta Corporation
405 Smythe Road
Ottawa, Ontario K1H 5A4
T: 613-916-6184

1.3 SITE LOCATION AND DESCRIPTION

The site is located on the south side of Derry Road, west of Airport Road, and adjacent to the historic Village of Malton within the City of Mississauga. As indicated on the map below, the co-generation plant area is situated within a larger parcel that formerly contained an airplane manufacturing complex dating back to 1938, and more-recently occupied by The Boeing Company.

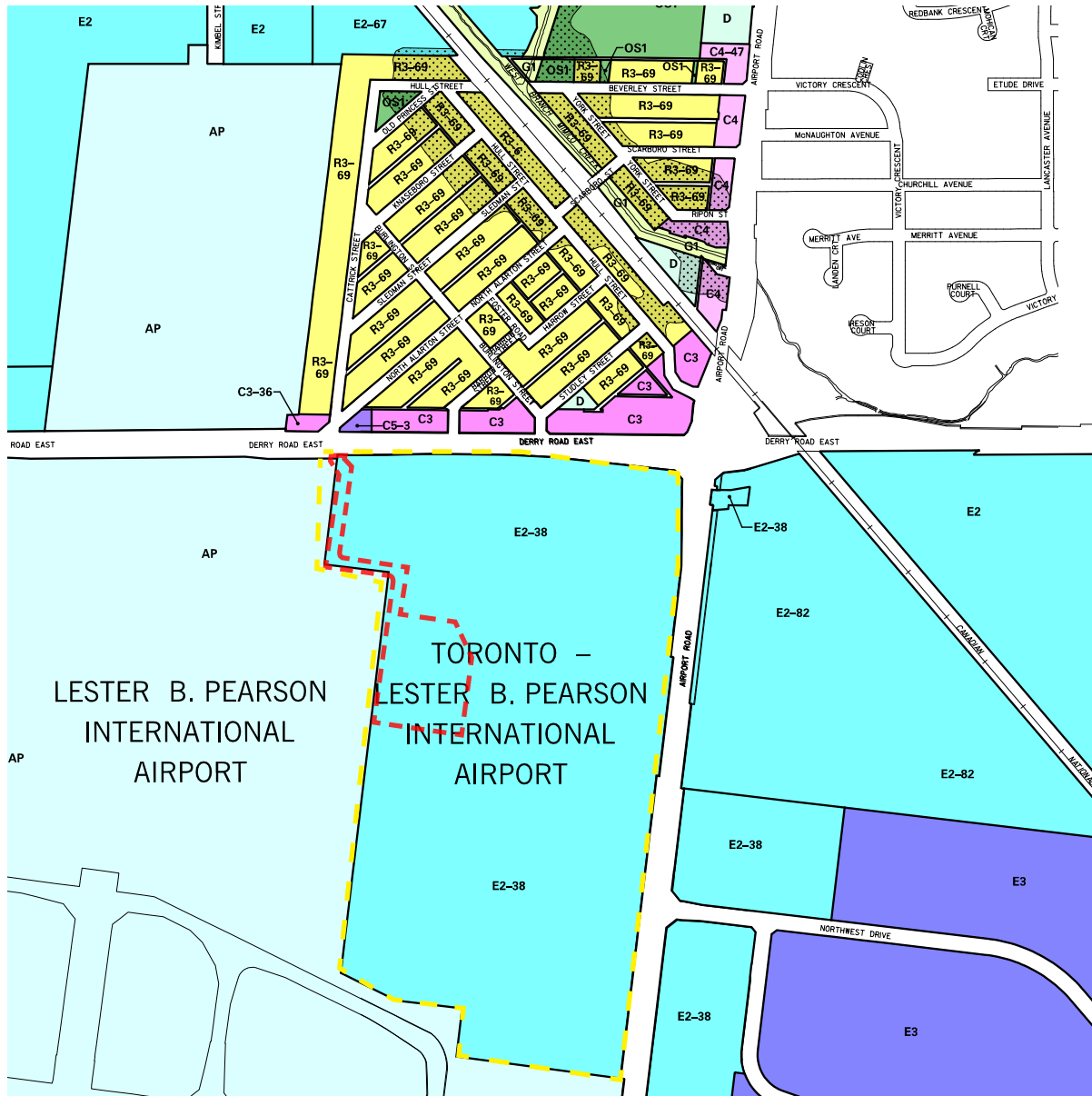
Following demolition of the Boeing complex in 2005, the site has been primarily a vacant lot, with the exception of the extant co-generation plant. The cogeneration plant, commissioned in 1992 operated as an electricity and steam energy facility until it was decommissioned in January 2018.

The site is adjacent to a mixture of land uses; the residential neighbourhood of Old Malton Village to the north, industrial properties to the east and the Toronto Pearson Airport to the south and west.



Aerial photograph - dashed red line indicates approximate extent of co-generation plant site area; dashed yellow line indicates approximate extent of former Boeing plant lands (Google Earth, 2020; annotations by ERA).

City of Mississauga Zoning Map (2007)



As depicted on the excerpt above, taken from the Mississauga Colour Zoning Map (by-law 0225-2007) the subject site is zoned "E2-38" (Business Employment). Lands to the west and south, occupied by Pearson International Airport, are zoned "AP" (Lester B. Pearson International Airport). Lands to the east are also zoned Business Employment. Lands to the north along the north side of Derry Road East are zoned "C3" (General Commercial) as well as "C5" (Motor Vehicle Commercial) and the residential lands further north "R3" (Detached Dwellings). The legend to the Colour Zoning Map is located on the following page.

ZONING BY-LAW 0225-2007

ZONING CATEGORIES	ZONES	ZONING DESCRIPTIONS
Residential	R1 – R18	Detached Dwellings
	RM1, RM2, RM3	Semi-Detached Dwellings
	RM7	Detached, Semi-Detached, Duplex, Triplex Dwellings
	RM4, RM5, RM6	Townhouse Dwellings
	RM8, RM9	Horizontal Multiple Dwellings
	RA1, RA2, RA3, RA4, RA5	Apartment, Long Term Care, Retirement Dwellings
Office	O	Office
Commercial	C1	Convenience Commercial
	C2	Neighbourhood Commercial
	C3	General Commercial
	C4	Mainstreet Commercial
	C5	Motor Vehicle Commercial
City Centre	CC1	Retail/Car Commercial
	CC2	Mixed Use
	CC3	Mixed Use – Transition Area
	CC4	Mixed Use /Mixed Use – Transition Area
	CCOS	City Centre Open Space
Employment	E1	Business Employment in Nodes
	E2	Business Employment
	E3	Industrial
Open Space	OS1	Community Park
	OS2	City Park
	OS3	Cemetery
Greenlands	G1	Recreation Hazard
	G2	Natural Features
		Greenlands Overlay
Parkway Belt	PB1, PB2	Parkway Belt
Utility	U	Utility
Institutional	I	Hospital and University /College
Development	D	Existing Use
Buffer	B	Buffer/Berm /Fence
Airport	AP	Lester B. Pearson International Airport

Legend for colour zoning map (City of Mississauga, 2007).

1.4 CONTEXT PHOTOS



Looking south at entrance to the property from Derry Road (ERA, 2020).



North elevation of co-generation plant (ERA, 2020).



Co-generation plant, east elevation (ERA, 2020).



Co-generation plant, west elevation (ERA, 2020).



Co-generation plant, east and north elevations (ERA, 2020).



View northeast over former Boeing plant lands (ERA, 2020).

1.5 HERITAGE CONTEXT

The subject site is listed on the Mississauga Heritage Register. The reasons for listing are reproduced in Section 4 of this HIA (Statement of Significance). The site does not contain any heritage resources that are designated under Part IV or Part V of the Ontario Heritage Act.

The site is also located proximate to the Pearson International Airport Cultural Landscape (F-TC-1), which is listed on the Heritage Register and described as follows:

Pearson International Airport combines both the current airport and the associated light industrial lands which surround it. The core site, originally Malton Airport, was developed after World War II as a result of the expansion of the air travel industry and was located next to the A. V. Roe aircraft manufacturing plant. The surrounding areas are characterized by low-rise industrial and service complexes which are generally businesses requiring access to air transport. The 1960's Terminal, designed by Parkin Associates architects, set a precedent for airport terminal design internationally. The massive new terminal currently under construction ensures its position as the busiest air hub in Canada and has been the site of arrival of many important personages to Canada as well as tens of thousands of new immigrants. The airport is also a significant cultural landscape because of its relationship to the now fabled AVRO ARROW constructed and test flown on this site in the 1950's. Other famous aircraft developed here included the CF-100 and the first passenger Jetliner in the world the AVRO XC-102.

(http://www5.mississauga.ca/pdfs/Cultural_Landscape_Inventory_Jan05.pdf).

1.6 HERITAGE POLICY CONTEXT

At the Provincial level, cultural heritage is addressed in the Growth Plan (2019) and the Provincial Policy Statement (2014).

Growth Plan (2019)

The Growth Plan, 2019 came into effect on May 16, 2019. The Growth Plan is the Province of Ontario's initiative to plan for growth and development in a way that supports economic prosperity, protects the environment, and helps communities achieve a high quality of life.

With the objective of "protecting what is valuable", Section 4.2.7 of the Growth Plan, 2019 states:

1. Cultural heritage resources will be conserved in order to foster a sense of place and benefit communities, particularly in strategic growth areas.

Provincial Policy Statement (2014)

The Provincial Policy Statement, 2014 (“PPS”) sets out the Ontario government’s land use vision for how we settle in the landscape, create built environment, and manage land and resources over the long term to achieve livable and resilient communities.

Section 2.6 of the PPS contains policies addressing Cultural Heritage, the most relevant of which include:

2.6.1 Significant built heritage resources and significant cultural heritage landscapes shall be conserved.

2.6.3 Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.

Peel Region Official Plan

Chapter 3.6 of the Peel Region Official Plan (consolidated December, 2018) sets out Regional policies on cultural heritage. The preamble to Chapter 3.6 states that:

The Region of Peel encourages and supports heritage preservation, and recognizes the significant role of heritage in developing the overall quality of life of residents and visitors to Peel. The region supports identification, preservation and interpretation of the cultural heritage features, structures, archaeological resources, and cultural heritage landscapes in Peel (including properties owned by the Region), according to criteria and guidelines established by the Province.

Mississauga Official Plan

Chapter 7 (Complete Communities) of the City of Mississauga Official Plan (consolidated November 22, 2019) contains policies pertaining to cultural heritage properties (subsection 7.4.1 - Cultural Heritage Resources and subsection 7.4.2 - Cultural Heritage Properties).

Policy 7.4.1.1 states:

The heritage policies are based on two principles:
(a) heritage planning will be an integral part of the planning process; and
(b) cultural heritage resources of significant value will be identified, protected and preserved.

Policy 7.4.1.7 states:

Mississauga will maintain a Heritage Register of property, including structures and cultural landscapes that should be preserved as cultural heritage resources. The cultural heritage resources in the Heritage Register will be assessed based on their design or physical value, historical or associative value, contextual value and archaeological significance including the aggregation of both natural and cultural heritage resources.

Pearson International Airport Cultural Landscape

In 2005, The Landplan Collaborative Ltd. produced a Cultural Landscape Inventory for the City of Mississauga that identified cultural landscapes within the municipality, including the Pearson International Airport Cultural Landscape. A series of qualities are assigned to each landscape, including:

- Landscape environment
- Built environment
- Historical associations
- Other

The qualities attributed to the Pearson International Airport Cultural Landscape are indicated below.

Pearson International Airport

F-TC-1

Location	Occupies a large site in the northeast corner of the City bounded by Airport Road and Derry Road
Heritage or Other Designation	None
Landscape Type	Transportation (Airport)

LANDSCAPE ENVIRONMENT

- ☐ Scenic and Visual Quality
- ☐ Natural Environment
- ☐ Horticultural Interest
- ☒ Landscape Design, Type and Technological Interest

HISTORICAL ASSOCIATION

- ☒ Illustrates Style, Trend or Pattern
- ☒ Direct Association with Important Person or Event
- ☒ Illustrates Important Phase in Mississauga's Social or Physical Development
- ☐ Illustrates Work of Important Designer

BUILT ENVIRONMENT

- ☒ Aesthetic/ Visual Quality
- ☐ Consistent Early Environs (pre-World War II)
- ☐ Consistent Scale of Built Features
- ☒ Unique Architectural Features/ Buildings
- ☐ Designated Structures

OTHER

- ☒ Historical or Archaeological Interest
- ☒ Outstanding Features/ Interest
- ☐ Significant Ecological Interest
- ☒ Landmark Value



2 BACKGROUND RESEARCH & ANALYSIS

2.1 PRE-COLONIZATION

2740 Derry Road is located the traditional territory of the Huron-Wendat and Petun First Nations, the Seneca, and most recently, the Mississaugas of the Credit River. Archaeological evidence suggests that the Huron-Wendat lived in and cultivated portions of the land currently known as the City of Mississauga at least as early as the 15th century. European contact, the fur trade, and disease initiated the displacement of the Wendat in the 17th century, whereupon the Haudenosaunee Confederacy occupied the territory.

The territory was the subject of the Sewatokwa'tshera't (Dish with One Spoon) wampum belt covenant, an inter-nation treaty ratified in 1794 between the Haudenosaunee Confederacy of the Anishnaabeg and allied nations to peacefully share and care for the resources around the Great Lakes.

During the American Revolution, the British Crown began purchasing large tracts of land for incoming Loyalists. On September 12, 1806, Treaty 14, or the Head of the Lake Purchase was signed by representatives of the Crown and people from the Mississauga of New Credit First Nation. This treaty expanded upon Treaty 13-A which was signed the previous year and included 74,000 acres that extended west from the Etobicoke Creek, to Burlington Bay and north six miles to modern day Eglinton Avenue. This area was surveyed in 1806 and expanded upon in 1819 into 100 and 200-acre parcels that would become Toronto Township within the County of Peel. Small towns and villages began to establish themselves along the surveyed Concessions and town lines as settlers moved to the area.



Sewatokwa'tshera't wampum belt covenant (Six Nations Polytechnique, n.d.).

2.2 THE VILLAGE OF MALTON

The town of Malton emerged around the corners of modern-day Derry and Airport Road, with its first settler recorded in 1823. Another early settler, Richard Halliday immigrated from Malton in Yorkshire, England which the Ontario town name derives from. By 1850, the growing village contained a general store, cobbler, hotels, churches and a blacksmith shop. The arrival of the Grand Trunk Railway in 1854 encouraged the growth and development of the village, providing direct access to the Toronto markets, and became a major grain handling and export centre.



Village of Malton street map c. 1877 (Historical Atlas of Peel County).

OPPOSITE: view of Malton and site, looking south-west (1948).

The influence of the railway is reflected in the street layout within the Old Malton neighbourhood, with streets aligned parallel to the direction of the railroad tracks.

Over time, Malton transitioned from a primarily agricultural community to an industrial community, spurred by the development of the Malton Airport and the adjacent aircraft manufacturing uses.

2.3 AIRPLANE MANUFACTURING

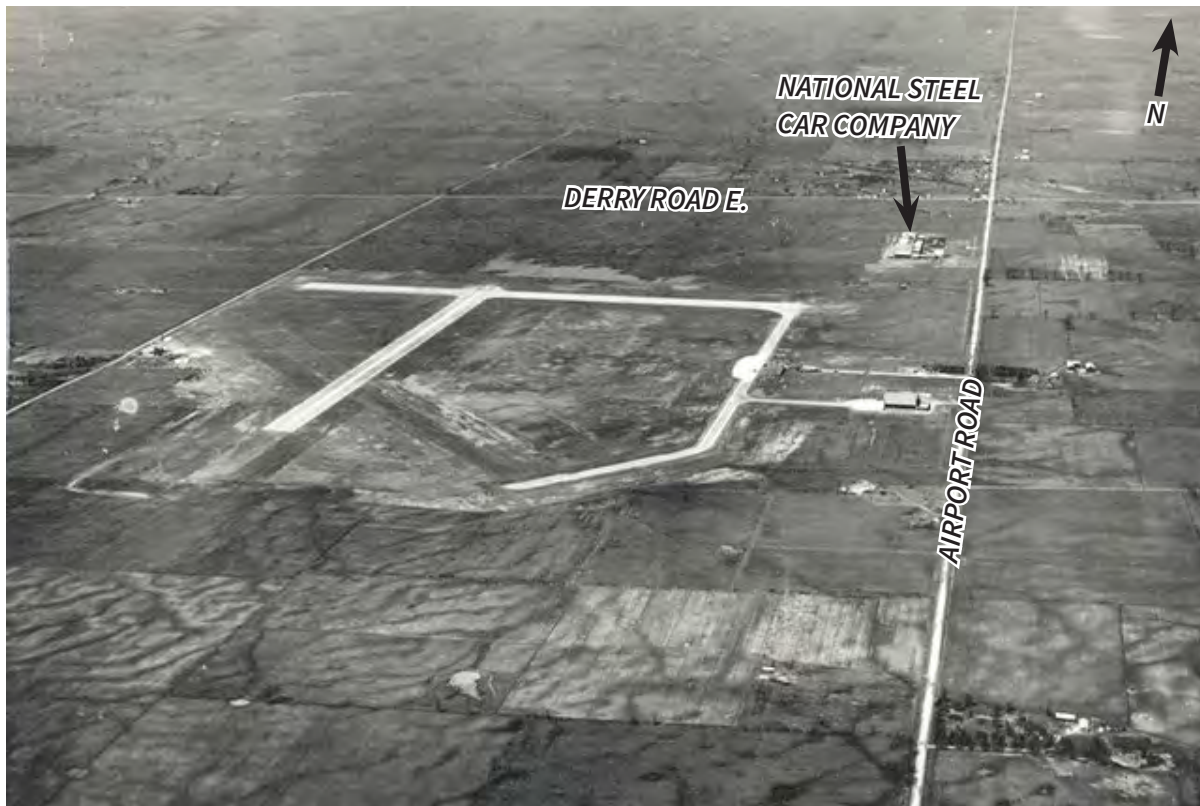
In 1935, the federal government announced that an international airport would be constructed in Toronto. In 1937, properties on Toronto Island and in Malton were acquired for the immediate construction of two airports. Toronto Island airport would be the primary airport, with Malton providing extended service.

In 1938, the National Steel Car Company acquired the farmlands north of the Malton airport and established an aircraft manufacturing plant as part of the Canadian Associated Aircraft consortium, which pooled expertise from across the airline industry to produce British military aircraft in Canada. In 1942, as part of the war effort, the Canadian government took ownership and management of the plant and renamed it Victory Aircraft Limited. During the Second World War the Malton plant was Canada's largest aircraft manufacturer, producing thousands of aircrafts including Lancaster bombers, Anson trainers, Lincoln bombers and Lancasterians.

Following the war, the plant was purchased by the UK based aircraft producer Hawker Siddeley Group, creating A.V. Roe Canada Limited. They designed and built the famed Avro Arrow, Canada's first supersonic aircraft. In 1959, Prime Minister John Diefenbaker terminated the Avro Arrow project and the five completed planes were dismantled. After the cancellation, A.V. Roe downsized, and the plant was subsequently occupied by several successive companies including de Havilland (1962), Douglas Aircraft (1965), McDonnell Douglas (1981) and Boeing Canada (1997). Ultimately, in 2005, the plant was demolished.

2.4 MISSISSAUGA CO-GENERATION PLANT

In 1992, the Mississauga Co-Generation Plant was commissioned and operated by TransAlta Corporation. The plant simultaneously produced steam for industrial functions and electricity for the provincial power grid. In January, 2018, the plant was decommissioned.



Malton Airport c. 1937 (Aerospace Heritage Foundation of Canada; annotations by ERA).



The 100th Lancaster bomber manufactured in Malton on the site (Aerospace Heritage Foundation of Canada, n.d.).

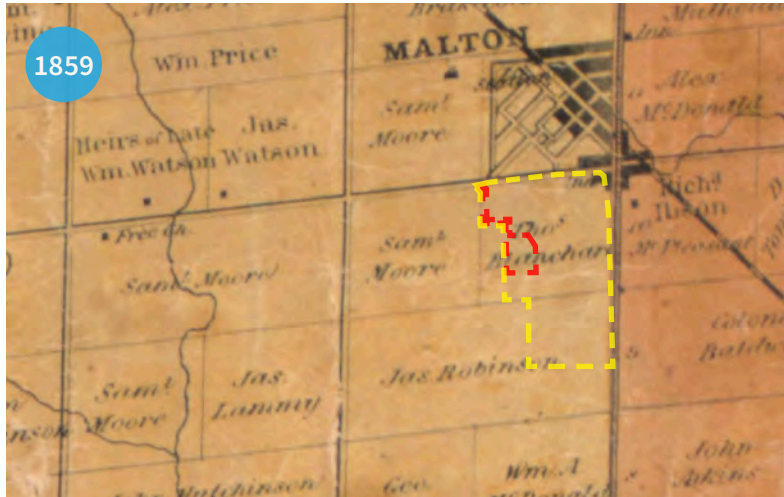


Avro Arrow in flight, Malton Airport and the site in background. (Aerospace Foundation of Canada, n.d.).

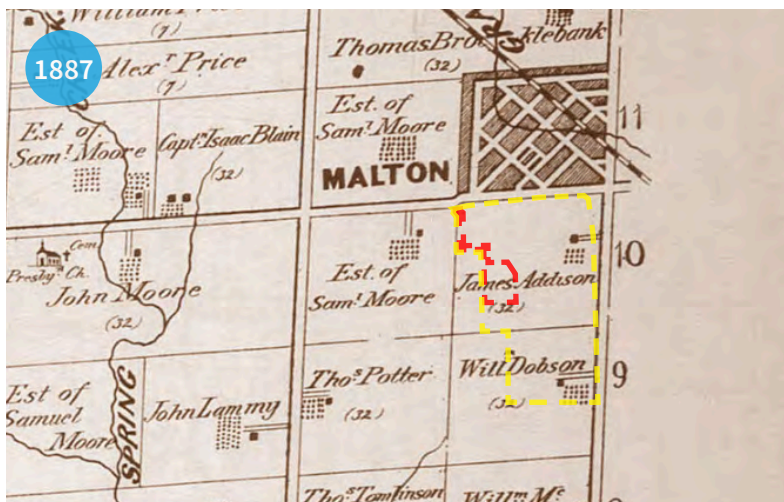


Site, during McDonnell Douglas Canada tenancy (Aerospace Foundation of Canada, n.d.).

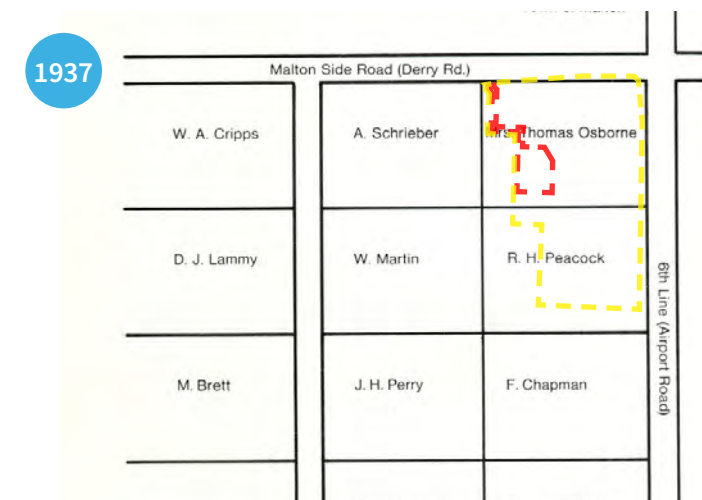
ARCHIVAL MAPS



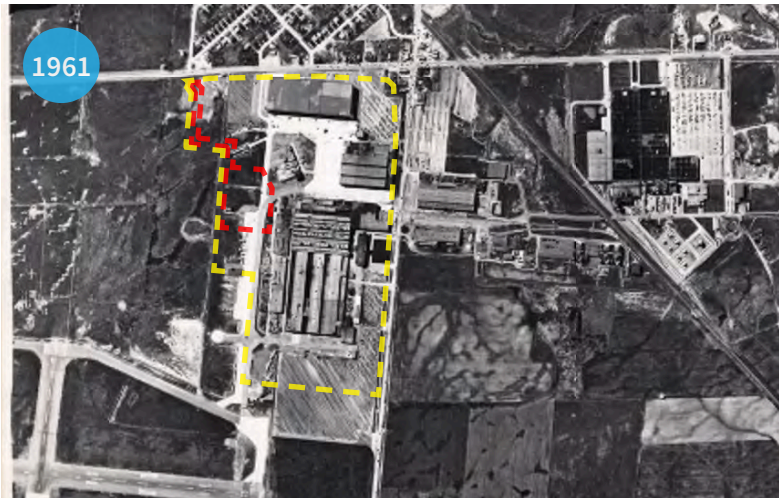
1. 1859 Tremaine's York County Map showing the approximate location of the site and former aircraft manufacturing lands (Source: McGill Digital Library; annotated by ERA Architects).



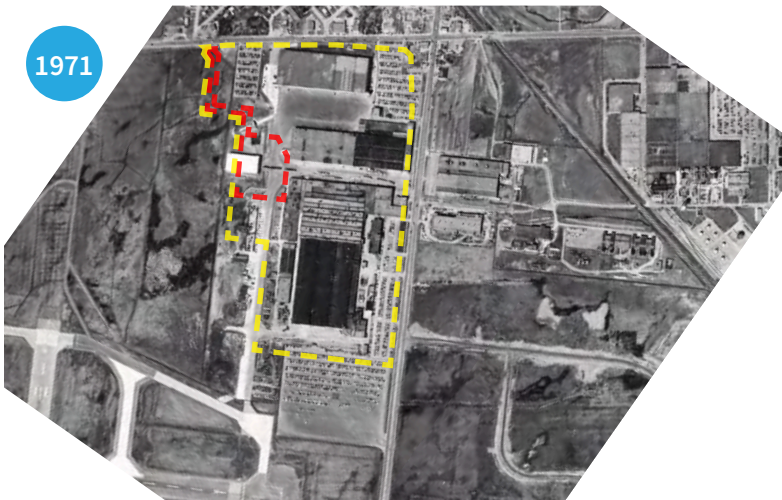
2. Map showing the approximate location of the site and former aircraft manufacturing lands. (Source: McGill Digital Library; annotated by ERA Architects).



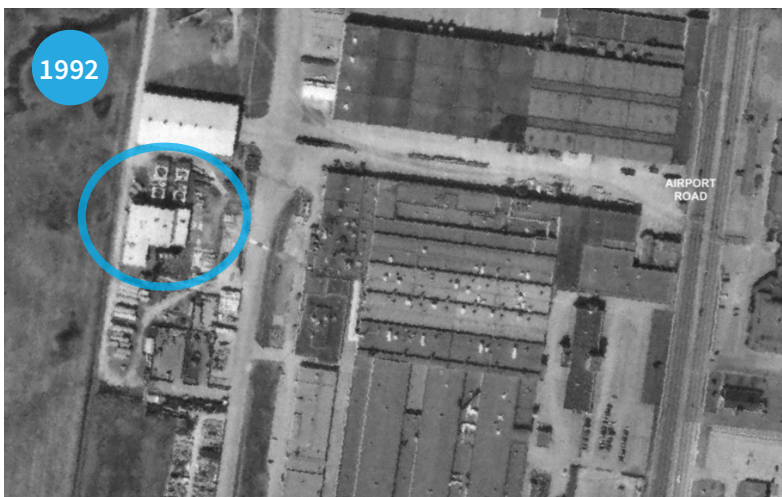
3. Map showing the approximate location of the former site and aircraft manufacturing lands. (Source: Malton Memories, annotated by ERA Architects).



4. Aerial photograph showing the approximate location of the site and former aircraft manufacturing lands. (Source: City of Mississauga Aerial Photos, annotated by ERA Architects).



5. Aerial photograph showing the approximate location of the site and former aircraft manufacturing lands. (Source: City of Mississauga Aerial Photos, annotated by ERA Architects).



6. Aerial photograph showing the newly-added co-generation plant c. 1992 (Source: City of Mississauga Aerial Photos, annotated by ERA Architects).

3 ASSESSMENT OF EXISTING CONDITION

The subject site contains a decommissioned co-generation plant, built in 1992. The plant is utilitarian and comprises a series of interconnected components such as smokestacks, pipes, and cooling towers. ERA conducted a site visit on March 6, 2020 to observe the exterior of the plant - the interior was not accessible at the time of the site visit.

The plant is accessed by a private driveway extending south from Derry Road East at the intersection of Cattrick Street. There are several outbuildings surrounding the plant, and the site is surrounded by a chain-link security fence.

The adjacent former aircraft manufacturing lands are vacant, save for some gravel parking lots accessed from Airport Road.

4 STATEMENT OF SIGNIFICANCE

4.1 MUNICIPALLY PREPARED REASONS FOR LISTING

The subject site is listed on the City of Mississauga Heritage Register. The reasons for listing provided by Mississauga Heritage Planning staff are as follows:

The property represents a long-time history of the aviation industry which is identified with Malton. The complete history of the site has not been written, but it is noted that the site was owned by National Steel Car in 1938. The company produced freight cars, buses and automobiles. The site was chosen for manufacturing due to its proximity to the airport.

By the early 1940s National Steel Car had built about 4,000 square feet of aviation assembly space and produced just under 1000 aircraft. In 1942, Victory Aircraft was created to boost aircraft production for World War II. The Lancaster bomber was produced at this location whereby at the height of the war, there was one Lancaster per day being produced.

In 1945 Victory Aircraft was replaced by A.V. Roe, which went on to produce the CF 100 and the CF 105 Arrow. In 1962 Avro Aircraft Division closed, but the site has retained an aircraft manufacturing presence since that time. The site has a combination of large hangers, office structures and physical plants which date from the early 1940s to the present.

The reasons for listing pre-date the demolition of the aircraft manufacturing complex in 2005 and have not been updated to reflect this demolition. The listing description continues to identify elements of the site that no longer exist and thus no longer reflects the existing condition of the site.

4.2 ONTARIO REGULATION 9/06 EVALUATION

In accordance with the requirements of the City of Mississauga's Heritage Impact Assessment Terms of Reference, ERA has evaluated the co-generation plant using the criteria prescribed under Ontario Regulation 9/06 (Criteria for Determining Cultural Heritage Value or Interest).

The evaluation indicates that the property does not satisfy any of the O. Reg. 9/06 criteria, and is not a candidate for designation under the Ontario Heritage Act. The co-generation plant was built in 1992 to generate power for the aircraft plant and the local electricity grid, and was incidental to the primary use - demolished in 2005 - that is described in the now out-of-date reasons for listing included on the City's Heritage Register.

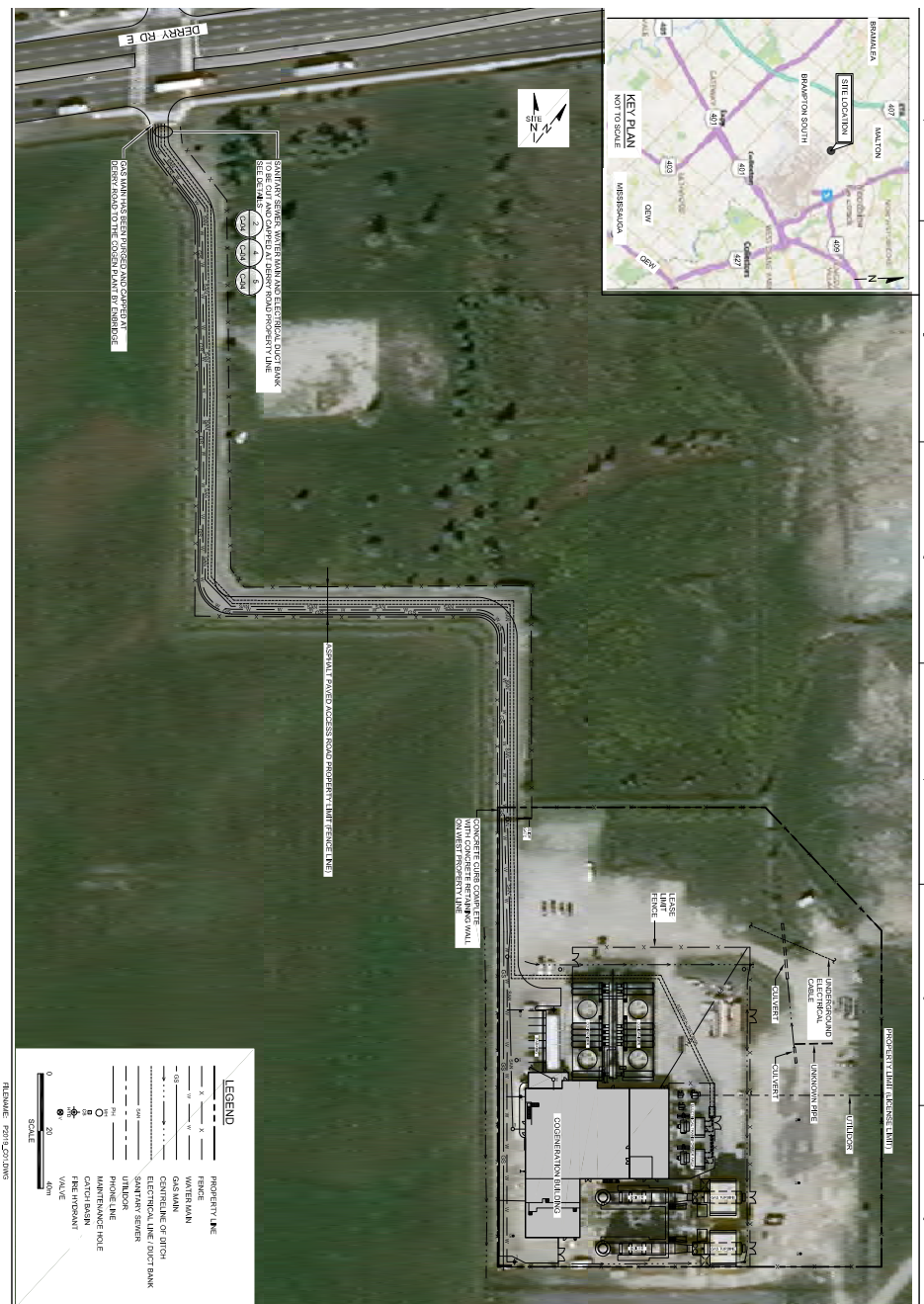
The 9/06 evaluation of the co-generation plant is presented on the following page.

CRITERION	Y/N	COMMENTS
(1) The property has design value or physical value because it:		
i) is a rare, unique, representative or early example of a style, type, expression, material or construction method.	No.	The co-generation plant is a utilitarian structure built in 1992 solely for the purpose of producing steam and electricity. It is not a rare, unique, representative or early example of a style, type, expression, material or construction method.
ii) displays a high degree of craftsmanship or artistic merit.	No.	The co-generation plant is a power plant that does not display any degree of craftsmanship or artistic merit.
iii) demonstrates a high degree of scientific or technical achievement.	No.	The co-generation plant does not display or present a high degree of technical or scientific achievement.
(2) The property has historical value or associative value because it:		
i) has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community.	No.	While the property was once the site of a significant aerospace manufacturing hub, the use ceased when the complex was demolished in 2005. The remaining co-generation plant is only incidental to the former use and does not serve as a meaningful link to the former use.
ii) yields, or has the potential to yield, information that contributes to an understanding of a community or culture.	No.	The co-generation plant does not have the potential to yield information that would contribute to an understanding of a community or culture.
iii) demonstrates, or reflects the work or ideas of an architect, builder, designer or theorist who is significant to a community.	No.	The co-generation plant is a utilitarian structure that has no known association to an architect, builder, designer or theorist.
(3) The property has contextual value because it:		
i) is important in defining, maintaining or supporting the character of an area.	No.	The co-generation plant does not define, maintain or support the character of the area.
ii) is physically, functionally, visually or historically linked to its surroundings.	No.	The co-generation plant is located in the centre of a large, inaccessible field and has no discernible relationship to its surrounding context.
iii) is a landmark.	No.	The building is not prominent in its context and does not represent a landmark.

Per the evaluation above, the co-generation plant does not meet any of the prescribed 9/06 criteria, and it is not a candidate for designation under the Ontario Heritage Act.

5 DESCRIPTION OF PROPOSED SITE ALTERATION

The contemplated site alteration consists of the demolition of the co-generation plant, and any subsequent site remediation as may be required. After demolition, the site will be left in a vacant state.



Site plan showing the extent of the demolition area (Source: Jacobs).

6 IMPACT OF PROPOSED DEMOLITION

ERA has evaluated the co-generation plant using the criteria prescribed through Ontario Regulation 9/06 (Criteria for Determining Cultural Heritage Value or Interest) and has determined that the plant is not a cultural heritage resource and does not merit protection under the Ontario Heritage Act. Consequently, the demolition of the plant does not represent a negative heritage impact.

The Ontario Heritage Toolkit contemplates several types of potential heritage impacts that should be considered in the context of a proposed development. The table below summarizes these possible impacts and any relevant considerations in light of the proposed development.

POSSIBLE IMPACTS	COMMENTS
(1) Destruction of any, or part of any, significant heritage attributes or features;	N/A - No impact.
(2) Alteration that is not sympathetic, or is incompatible, with the historic fabric and appearance;	N/A - There are no heritage resources being altered.
(3) Shadows created that alter the appearance of a heritage attribute or change the viability of a natural feature or plantings, such as a garden;	N/A .
(4) Isolation of a heritage attribute from its surrounding environment, context or a significant relationship;	N/A - There are no heritage attributes that will be isolated as a result of the proposed demolition.
(5) Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features;	N/A.
(6) A change in land use such as rezoning a battlefield from open space to residential use, allowing new development or site alteration to fill in the formerly open spaces; and	N/A. There is no change in use or replacement construction proposed at this time.
(7) Land disturbances such as a change in grade that alters soils, and drainage patterns that adversely affect an archaeological resource.	N/A.

ERA has considered the potential impact of the proximate Pearson International Airport Cultural Landscape. Given the isolation of the co-generation plant from the core context of the airport, there is no anticipated adverse impact on the identified Cultural Landscape.

7 CONCLUSION

This HIA finds that there are no heritage resources on the site, and the proposed demolition of the co-generation plant does not yield heritage impacts on the site or to the proximate Pearson International Airport Cultural Landscape. As such, a conservation strategy and mitigation measures are not provided or recommended.

8 PROJECT PERSONNEL

ANDREW PRUSS

Andrew Pruss is a Principal with ERA. He has been involved in all aspects of architectural projects ranging from single-family residences and condominiums to institutional, commercial and hotel projects.

DAN EYLON

Dan Eylon is an Associate and Planner with ERA Architects. He is a member of the Canadian Association of Heritage Professionals. He received his Master of Arts in Planning from the University of Waterloo after completing a Bachelor of Fine Art at the Ontario College of Art & Design.

PETER PANTALONE

Peter Pantalone is a Planner and Project Manager with ERA Architects. He has a Master of Environmental Studies from the York University Planning Program and is a candidate member of the Ontario Professional Planners Institute.

ALY BOUSFIELD

Aly is an intern in the cultural planning team at ERA Architects and provides site research and heritage assessments for projects at ERA. She is currently a student at the Willowbank School of Restoration Arts working towards a diploma in heritage conservation, building upon a bachelor's degree in Sociology from the University of Guelph and a post-graduate certificate in Urban Design from Simon Fraser University. Aly is passionate about understanding places through a cultural landscape perspective that explores the interconnectedness between natural, built and intangible heritage.

9 SOURCES

- Aerospace Heritage Foundation of Canada. <http://www.ahfc.org/index.php>
- City of Mississauga Aerial Photographs: 1954, 1961, 1971. <https://www.mississauga.ca/portal/services/maps>
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- Hicks, Kathleen. "Malton: Farms to Flying", Friends of the Mississauga Library System, 2006. http://www.mississauga.ca/file/COM/9634_MaltonBook.pdf
- Library and Archives Canada, Digital Archive. "National Film Board of Canada Fonds: Malton" & "A.V. Roe Fonds". <https://www.bac-lac.gc.ca/eng/collectionsearch/Pages/collectionsearch.aspx>
- McGill University. "The Canadian County Atlas Digital Project". <http://digital.library.mcgill.ca/countyatlas/default.htm>.
- Ontario Historical County Maps Project. "County of Peel, Township of Toronto North". <http://maps.library.utoronto.ca/hgis/countymaps/simcoe/index.html>

10 APPENDICES

APPENDIX I Heritage Property Application for 2720 Derry Road East, dated
February 4, 2020

Heritage Property Application

11.6.

Community Services
Culture Division
201 City Centre Drive, Suite 202
Mississauga, ON L5B 2T4
FAX: 905-615-3828
www.mississauga.ca/heritageplanning



Personal information collected on this form and other required documents is collected under the authority of the *Ontario Heritage Act*, s.33(1)(2) and s.42(1,2,1,2.2) and City of Mississauga Heritage By-law 0078-2018 as amended. The information will be used to process the application. Questions about the collection of this personal information should be directed to the Supervisor, Heritage Planning, 201 City Centre Drive, Suite 202, Mississauga ON L5B 2T4, Telephone 905-615-3200 ext. 5366.

LOCATION DETAILS

(Please Print Clearly)

For Office Use Only:

Heritage Property Application Number: _____

Will the Heritage Advisory Committee review be required? ☐ Yes ☐ No

Municipal Address 2740 Derry Road East, Mississauga

Legal Address Part of Lot 10, Concession 6, East of Hurontario Street, Designated as Part 9, 17 and 22 on Reference Plan 43R-27815, City of Mississauga, Region of Peel

Property Owner TransAlta - Daniel Morais Contact Address 405 Smyth Road, Ottawa Ontario

Postal Code K1H 5A4 Phone 613-916-6184 Email Address Daniel_Morais@transalta.com

HERITAGE DESIGNATION BY-LAW NUMBER (if applicable): _____

What type of Application is Required?

Alteration or addition ☐ Yes ☐ No

Demolition ☒ Yes ☐ No

New Construction ☐ Yes ☐ No

Repeal of Designation By-law ☐ Yes ☐ No

Is there a corresponding application, such as:

a) Building permit number _____

b) Site Plan application number _____

c) Rezoning application number _____

d) Other Demo App; Temp Project # BPA-54682

Description of Work to be Completed:

Please attach reports, drawings, site plans, and photographs to better illustrate the project. These may be required depending on the scale of the project.

Above grade demolition and below grade demolitions of 2740 Derry Road East, Mississauga.

Further information is attached.

Name Daniel Morais

Please Print

Date 2020/02/04

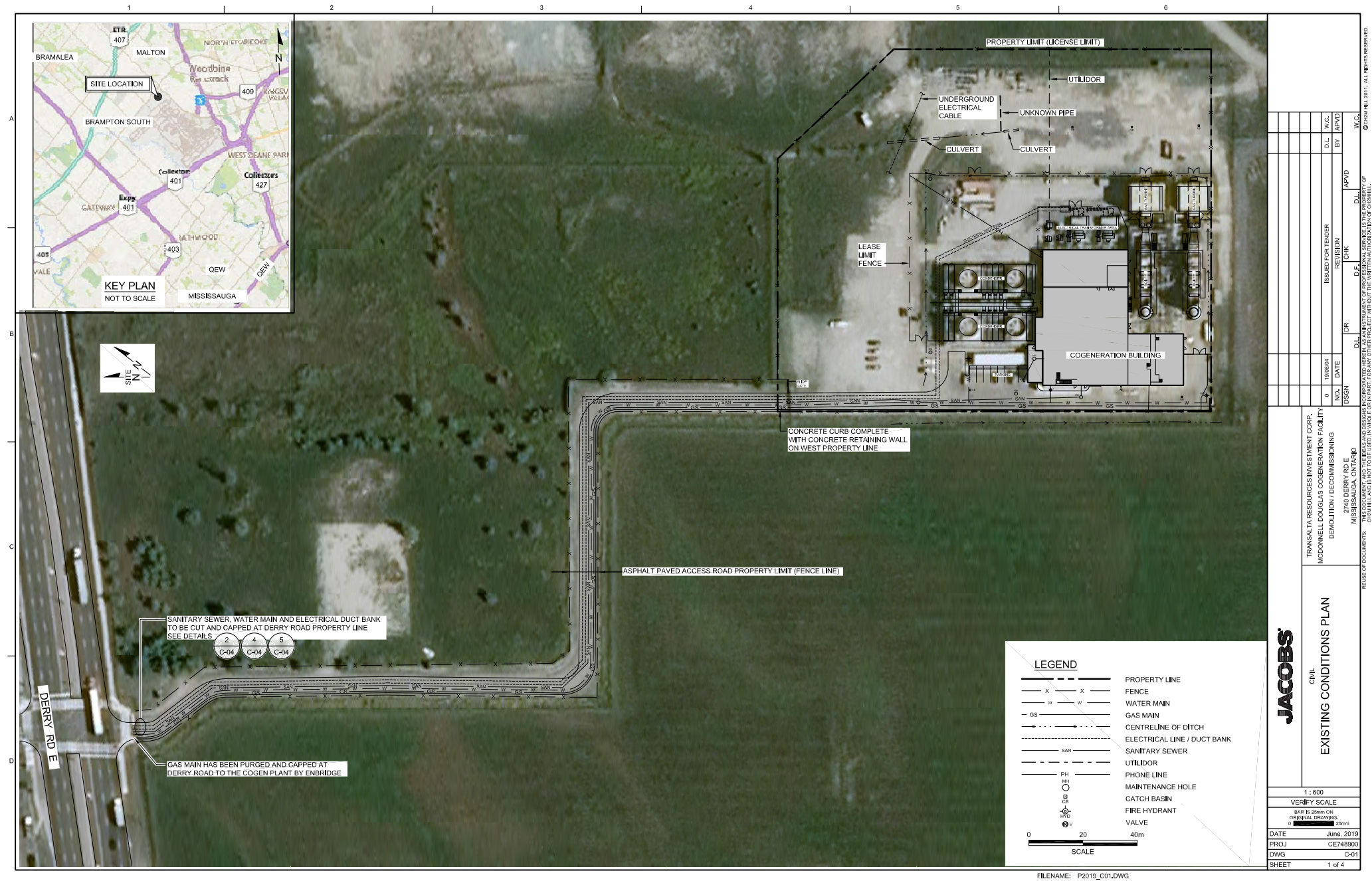
YYYY / MM / DD

By signing below, I acknowledge or agree that, as per section 38 of the *Ontario Heritage Act*, any person authorized by Council as per City of Mississauga Heritage By-law 0078-2018, upon producing proper identification, inspect at any reasonable time property designated or proposed to be designated under Part IV of the *Ontario Heritage Act* where a notice of intention has been served and published under subsection 29 (3).

Signature (of Property Owner) Daniel Morais

Digital versions of heritage permit applications must be submitted to heritage.planning@mississauga.ca.

For information or assistance, please contact Heritage Planning staff at 905-615-3200, ext. 5385 or 5366.



City of Mississauga Corporate Report



<p>Date: May 6, 2020</p> <p>To: Mayor and Members of Council</p> <p>From: Geoff Wright, P.Eng, MBA, Commissioner of Transportation and Works</p>	<p>Originator's files:</p> <hr/> <p>Meeting date: May 20, 2020</p>
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Subject

Vendor of Record Designation for the Supply and Delivery of Replacement Parts and Repair Services for Vehicles and Equipment Managed by Fleet Services

Recommendation

1. That the vendors included in Appendix 1, attached be designated as vendors of record for the supply and delivery of replacement parts and repair services for vehicles and equipment managed by Fleet Services, as outlined in the report from the Commissioner of Transportation and Works, dated May 6, 2020 and entitled "*Vendor of Record Designation for the Supply and Delivery of Replacement Parts and Repair Services for Vehicles and Equipment Managed by Fleet Services*".
2. That the Purchasing Agent be authorized to execute annual contracts with each of the designated vendors of record, in the estimated amounts shown in Appendix 1, for the supply and delivery of replacement parts and repair services for vehicles and equipment managed by Fleet Services, as outlined in the report from the Commissioner of Transportation and Works, dated May 6, 2020 and entitled "*Vendor of Record Designation for the Supply and Delivery of Replacement Parts and Repair Services for Vehicles and Equipment Managed by Fleet Services*".

Background

Fleet Services (Works Operations and Maintenance Division) manages the entire life cycle of the City of Mississauga's Corporate Fleet, which comprises approximately 1,600 vehicles and equipment. Efficient maintenance and repair of the Fleet requires the timely availability of quality replacement parts and dealer level service. The Fleet consists of many different types of specialized equipment where Original Equipment Manufacturer (OEM) parts are required to preserve warranty, to ensure proper fitment and reliable operation. Aftermarket parts are used whenever possible. Specialized repairs are often sublet to OEM vendors when it is more cost effective to do so, or when specialized equipment and/or dealer resources are required.

Parts and service availability are key requirements to maintaining the City's Fleet. Staff have identified OEM parts and service vendors that are authorized by the manufacturers to provide the services for the City of Mississauga. These vendors are located in proximity to the City of Mississauga. The vendors identified in Appendix 1, attached, are considered to be uniquely qualified and authorized by their respective manufacturers to provide OEM parts and service for the products they represent.

OEM Vendor

OEM parts and repair services are only available from the manufacturer, or from the manufacturer's authorized dealer, and will continue to be required as long as the equipment remains active in the Fleet.

The OEM vendors are sole sources as defined in the Purchasing By-Law #374-2006 which states under Schedule A, Section 1, (a) the goods and/or services are only available from one supplier by reason of: (iii) the existence of exclusive rights such as patent, copyright or licence

Comments

The vendors of record should be established until such a time as the parts and/or service they supply are no longer required or if the vendor fails to perform or ceases to carry on business.

By-law 374-2006 further requires Council authority to award sole source contracts having a value of \$100,000 or more.

Financial Impact

There are no additional financial impacts resulting from the recommendations in this report. The annual estimated cost for parts and sublet services for the vendors in Appendix 1 is \$775,000 excluding taxes. Funds for this purpose are available in the existing operating budget within Fleet Services.

Conclusion

Efficient maintenance and repair of the Fleet requires the timely availability of quality replacement parts and dealer service. The Fleet consists of many different types of specialized equipment where OEM parts are required to preserve warranty, to ensure proper fitment and reliable operation. Aftermarket parts are used whenever possible. Specialized repairs are often sublet to OEM vendors when it is more cost effective to do so, or when specialized equipment and/or dealer resources are required.

Staff recommends that the vendors identified in Appendix 1 be designated as vendors of record.

Attachments

Appendix 1: Vendor of Record list



Geoff Wright, P.Eng, MBA, Commissioner of Transportation and Works

Prepared by: John Crozier, Manager, Fleet Services, Works Operations and Maintenance Division

APPENDIX 1

Vendor of Record for Fleet Services

Vendor	Method	Description	Estimated Annual Contract Value
G.C Duke	Sole Source	Authorized OEM for Jacobsen, Cushman, Monster Vac	\$200,000
Kooy Brothers	Sole Source	Authorized OEM for parts and service for Kubota equipment	\$175,000
Zamboni	Sole Source	Original Equipment Manufacturer of Zamboni Ice Resurfacers	\$100,000
Viking Cives Limited	Sole Source	Original Equipment Manufacturer of Viking Snow Removal Equipment	\$100,000
Strongco Equipment	Sole Source	Authorized OEM for parts and service for Volvo Construction Equipment	\$100,000
Turf Care	Sole Source	Authorized OEM for parts and service for Toro equipment	\$100,000

City of Mississauga

Corporate Report



<p>Date: May 11, 2020</p> <p>To: Mayor and Members of Council</p> <p>From: Paul Mitcham, P.Eng, MBA, Commissioner of Community Services</p>	<p>Originator's files:</p> <hr/> <p>Meeting date: May 20, 2020</p>
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Subject

Virtual Canada Day in Mississauga

Recommendation

That Council authorize Culture Division staff, with support from Strategic Communications, to plan and coordinate a Virtual Canada Day celebration as outlined in the Corporate Report dated May 11, 2020, from the Commissioner of Community Services entitled "Virtual Canada Day in Mississauga".

Background

With the unfortunate cancellation of Canada Day events across the country and the City's cancellation of all events up to July 3, due to the COVID-19 Pandemic, the Department of Canadian Heritage have modified their grant rules to allow for the funding of "Virtual Canada Day Celebrations". In light of this funding opportunity, City staff is seeking Council's approval to produce a Virtual Canada Day Celebration for Mississauga using the approved Heritage Canada funding.

Present Status

To date, staff has been in discussions with Global TV, who have indicated their interest in partnering with the City to broadcast segments of Mississauga's Virtual Canada Day. In addition, staff has held preliminary discussions with potential sponsors, prominent Mississauga personalities, and performers, to determine their interest with participating in a virtual celebration on July 1, 2020.

Comments

The proposed Virtual Canada Day celebration would be developed around the theme of "with glowing hearts, we see thee rise". It would consist of a pre-recorded video produced by Culture staff (Celebration Square) with support from Strategic Communications staff, as lead organizers of the Canada Day official ceremony component. The 30 to 45 minute video will be released on

Canada Day, at a time to be confirmed, with shorter video segments featured on social media throughout the day.

Mississauga's Virtual Canada Day video will feature a mix of programming including:

- a modified Official Ceremony with the Mayor and Members of Council;
- a celebration of the 40th anniversary of O Canada becoming the official anthem;
- recognition of the many Canada Day events that take place across the city;
- an initiative that encourages Mississauga residents to participate in a simultaneous tribute to Canada, developed in collaboration with the Peel Indigenous Network, sponsors and the City's other Canada Day organizers;
- interviews and short performances with some of Mississauga's most prominent personalities;
- a social media contest in the lead-up to July 1 for Mississauga residents to win a virtual "meet and greet" with prominent personalities; and
- a tribute to Canada utilizing the lighting available at the Civic Centre including Celebration Square and the clock tower. The light show will be webcam streamed to residents on the City's website.

Financial Impact

The cost of Mississauga's Virtual Canada Day event would be funded by The Department of Canadian Heritage grant and previously secured sponsorships for Mississauga's Canada Day event.

Conclusion

In creating this virtual celebration video, the City of Mississauga will foster a sense of community and national pride for residents and an opportunity for us to connect virtually, in celebration of Canada's 153rd birthday, during this difficult time. If approved, more specific details on the event will be finalized and communicated in the coming weeks.



Paul Mitcham, P.Eng, MBA, Commissioner of Community Services

Prepared by: Melissa Agius, Manager, Celebration Square, Meadowvale Theatre and Culture Technical Services

City of Mississauga Corporate Report



<p>Date: April 27, 2020</p> <p>To: Mayor and Members of Council</p> <p>From: Andrew Whittemore, M.U.R.P., Commissioner of Planning & Building</p>	<p>Originator's files: MUD Awards</p> <hr/> <p>Meeting date: May 20, 2020</p>
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Subject

PUBLIC MEETING INFORMATION REPORT

Mississauga Urban Design Awards - Program Refresh

Recommendation

That the report dated April 27, 2020, from the Commissioner of Planning and Building regarding the report entitled "Mississauga Urban Design Awards – Program Refresh," be received for information.

Report Highlights

- This report is to provide an update of the program's refresh process for the Mississauga Urban Design Awards (MUD Awards) that will ensure competitiveness and continued success in the future.
- Top new changes in the MUD Awards program include new branding, a new timeline, improved promotions, new categories, and new ceremony format.
- In 2020, a special online voting 'Retrospective People's Choice Award' will take place to honor all previous Award of Excellence winners from 1981 to the present.

Background

The City of Mississauga initiated an annual Urban Design Awards (MUD Awards) program in 1981. It is the longest-running program of its kind in Ontario. In 2014, the program became a biennial event. In 2018, the City celebrated the 35th anniversary of the MUD Awards program.

The awards program offers the opportunity to receive recognition for urban design excellence from a jury panel of urban visionaries based on pre-set judging criteria. The program promotes excellence in urban design, publicizes the City's desire for design leadership, and assists in raising the image and visual quality of the City. It encourages new development to fit not only into the existing context but also into the future city vision as set out in the Strategic and Official Plans.

Council	2020/04/27	2
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MUD Awards

Although the award program was successful in 2018, a couple of matters came to staff's attention that, in turn, led to a review of the program elements. Firstly, over the last ten years, there has been a decline in the number of private-sector submissions. Secondly, it was noted that the awards event timeline conflicted with both the municipal elections and the National Urban Design Awards timelines.

Comments

Staff have undertaken a review of the MUD Awards program, including benchmarking with other municipalities, to refresh the format of the existing program. These include:

- Awards Timeline
- Marketing and Communications
- Awards Categories
- Submissions
- Partnerships and Sponsorships
- Awards Ceremony and Rewards

The following highlights the changes in each theme:

Awards Timeline

The awards ceremony will be shifted one year ahead from 2020 to 2021 to avoid conflicts with the municipal elections and to align with national programs.

Given the municipal election cycle, a shift to 2021 will ensure no future conflict. Moreover, this shift will better align with the National Urban Design Awards (NUD Awards) submission deadline. The NUD Awards is offering to waive the submission fees to those that have won an award of excellence recently at the municipal level.

As part of the transition, in 2020, an online 'People's Choice Awards' version will be featured to recognize the public's favourite projects selected through an online voting process from the previous award of excellence winners starting from 1981 until 2018 MUD Awards, "The best of 35 award programs".

Marketing and Communications

A new marketing and communications plan is also proposed. Key deliverables of the plan will include new branding and update of the visual identity; digital presence improvement, including a new online voting platform; webpage and online submission portal; community partnerships; as well as improved promotions and outreach to the public and industry.

The plan will raise the profile of urban design in Mississauga as a key driver of city building, design excellence, and quality of life. More specifically, it will encourage online public participation in the 2020 People's Choice Awards and will foster increased private industry submissions to the 2021 Urban Design Awards.

Awards Categories

New award categories will be introduced to ensure that all the project types are represented and the submissions from the private-sector are highlighted and treated separately. The award categories are as follows:

- **Urban Elements:** A standalone object, public art, small-scale building component or landscape element which contributes significantly to the quality of public realm and/or neighbourhood including, but not limited to, infill housing, additions, public art in public or private lands, street furniture, light fixtures, canopies, signage, walkways, stairways, fences, etc.
- **Private Project(s) in Context:** A building or group of buildings in all types and scales that achieve urban design excellence and is precedent-setting for a project of its type. Submissions should address how the project contributes to successful city-building through its contextual relationship, design quality, and measures of sustainable and healthy design. Submissions may include, but not only limited to: residential, employment, commercial, mixed-use, heritage restoration, and adaptive re-use buildings, private open spaces, and plazas, etc.
- **Public Project(s) in Context:** A building or group of buildings that serve the public and accessible to the public. All building types and scales are eligible. Submissions may include, but not only limited to: education, healthcare, recreation, cultural, community, and civic buildings, bridges, heritage restoration, adaptive re-use buildings, parks, streetscape, public or private open spaces, plazas, landscaped areas, stormwater facilities, etc.

Submissions

The MUD Awards submissions are open to architects, urban designers, landscape architects, planners, developers, owners, builders, construction companies, consultants, and the general public. In the past, the MUD Awards submissions have been in hard copy printed format. Participants will now also be able to submit their projects online in digital format to make it easier and more convenient.

Partnerships and Sponsorships

- **Long-term partnerships:** The long-term partnership with the Region of Peel, Public Health Unit, established in 2014, will remain and continue in the future. The Region of Peel has provided technical and financial support to the MUD Awards by introducing a Healthy by Design award category and adding healthy by design measures to the judging criteria. In addition, a new partnership is proposed with the City's Culture Division.

Short-term partnerships: In 2018, the MUD Awards established a short-term partnership with Sheridan College, School of Architectural Technology at Hazel McCallion campus who undertook a poster competition; hosted the awards ceremony event; provided catering and resources. Similar short-term partnerships with universities, colleges, schools, and other community groups, will be proposed based on the award theme selected.

- **Sponsorships:** Sponsorships from affiliations such as Ontario Association of Architects (OAA), Ontario Professional Planners Institute (OPPI), Ontario Association of Landscape Architects (OALA), and from professional organizations such as Urban Land Institute (ULI), The Council for Canadian Urbanism (CanU), Canadian of New Urbanism (CNU), Toronto Society of Architects (TSA), Canadian Society of Landscape Architects (CSLA), Heritage Mississauga, are proposed to increase the awareness of the MUD Awards program within the professional industry.

Awards Ceremony and Rewards

Winning projects are usually announced during the awards ceremony, followed by a reception attended by approximately 100 guests each year included prominent members of the development sector, awards participants, and staff.

To increase the MUD awards program profile, it is proposed to transform the awards ceremony to be a less formal and more celebratory event to encourage winners and members of the community to participate. A keynote speaker(s) related to the awards theme will be added to the ceremony program. Exposure will be increased for the awards winners on various media platforms, open galleries in the Mississauga Civic Centre, libraries, and community centers.

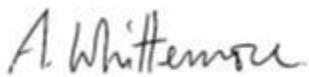
Financial Impact

The recommendation contained herein does not have any additional financial impact to the City of Mississauga.

Conclusion

A refresh allows updating all aspects of the MUD Awards program, including award timelines, branding, categories and criteria, online submissions, partnerships and sponsorship opportunities, refinement of the jury panel, and enhancement of the award ceremony and rewards. It is anticipated that this will lead to an increase in the number of submissions, increase recognition, encourage additional private-sector involvement in the MUD Awards program, and ultimately help promote excellence in urban design.

Attachments



Andrew Whitemore, M.U.R.P., Commissioner of Planning & Building

Prepared by: Amr Merdan, Urban Designer

REPORT 1 - 2020

To: CHAIR AND MEMBERS OF AUDIT COMMITTEE

The Audit Committee presents its first report for 2020 and recommends:

AC-0001-2020

That the deputation by Wesley Anderson, Manager, Finance and Treasury Services regarding the 2019 Financial Statements be received.

(AC-0001-2020)

AC-0002-2020

That the 2019 Audited Financial Statements for City of Mississauga (consolidated), City of Mississauga Public Library Board, City of Mississauga Trust Funds, Clarkson Business Improvement Area, Port Credit Business Improvement Area, Streetsville Business Improvement Area, Malton Business Improvement Area, and Enersource Corporation be received for information.

(AC-0002-2020)

AC-0003-2020

That the 2019 External Audit Findings Report, dated April 23rd, 2020 from the Commissioner of Corporate Services and Chief Financial Officer, which includes the Audit Findings Report from KPMG for the fiscal year 2019 for the City of Mississauga (City), be received for information.

(AC-0003-2020)

AC-0004-2020

1. That the option to extend the existing contract with KPMG LLP for Statutory Audit Services, File Ref: FA.49.480-15, for the audit years 2020-2024 in accordance with the City's Purchasing By-law 374-06, as amended, per the report entitled "Contract Amendment for Statutory Audit Services Procurement FA.49.480-15" dated April 13, 2020 from the Commissioner of Corporate Services and Chief Financial Officer be exercised.
2. That the Purchasing Agent or designate be authorized to execute an amendment to the existing contract with KPMG LLP to increase the total contract value to \$1,552,500 and extend the contract end date to November 1, 2025, in accordance with section 18(2)(d)(i) of the City's Purchasing By-law 374-06, as amended.
3. That KPMG LLP be appointed the City's external auditors for the audit years 2020 to 2024, in accordance with the Audit Committee Terms of Reference.
4. That the amendment referred to in recommendation 2 include audits of accommodation providers as are required to enforce the Municipal Accommodation Tax (MAT) By-law.

(AC-0004-2020)

AC-0005-2020

That the report dated February 7, 2020 from the City Manager & Chief Administrative Officer regarding the status of outstanding audit recommendations as of December 31, 2019 be received for information.

(AC-0005-2020)

AC-0006-2020

That the report dated April 20, 2020 from the Director, Internal Audit with respect to the 2019-2020 Internal Audit Work Plan be approved.

(AC-0006-2020)

MOTION

Moved by: C.Parrish
Seconded by: G.Carlson

May 20, 2020

Whereas the Boeing Lands on the west side of Airport Road, south of Derry Road West, are part of property subject to a request for a demolition permit to remove the Transalta plant at 2740 Derry Road;

Whereas the property in question has long been of interest to the Heritage Advisory Committee and the City of Mississauga;

Whereas the property was once owned by A.V. Roe with its manufacturing facilities, office building and Arrow hangars;

Whereas the A.V. Roe facilities were demolished some time before December 2003;

Whereas a letter was received by Council dated February 5, 2003 from Mr. S. Fisher, President Boeing Toronto Ltd., with respect to the property at 6972 Airport Road;

Whereas Council was copied a letter for receipt dated March 13, 2003 from Councillor K. Mahoney, Chair of the Heritage Advisory Committee, to the CEO of the GTAA requesting "a parcel of land on the Boeing site at 6972 Airport Road as a park dedication to the celebration of the aviation history" on those lands;

as well as

A letter to Boeing Toronto Ltd. requesting "consideration of retaining the facade of the AVRO Arrow hangar on the Boeing site at 6972 Airport Road";

Whereas December 12, 2003, Council recommended the matter of collecting artifacts related to the AVRO Arrow and/or the Arrow hangar be referred to the Collections and Artifact Storage Subcommittee of the Museums of Mississauga Advisory Committee;

Whereas the Boeing Lands, restricted to the west side of Airport Road, are all Listed on the Municipal Heritage Register, but not Designated under the Ontario Heritage Act;

Therefore Be It Resolved:

That Council direct staff to retrieve the letter dated February 5, 2003 from Mr. S. Fisher, President Boeing Toronto Ltd, with respect to the property at 6972 Airport Road from archived materials; and further

That Council direct staff to seek an inventory of any artifacts related to the AVRO Arrow and/or the AVRO Arrow buildings demolished on the Boeing Lands, that may be in the possession of the City or Region or any other institution; and further

That Council direct staff to investigate any and all agreements between Boeing Toronto Ltd. and the City of Mississauga with the intent of taking all necessary steps to ensure agreements are upheld; and further

That staff begin the process of designation, under Part IV of the Ontario Heritage Act, of the Boeing Lands at 6972-6982 Airport Road in order to ensure that Mississauga's aerospace history is properly commemorated on site; and further

Council approve the demolition of the Transalta plant at 2740 Derry Road East as per the staff recommendation in item 11.6 of the Council Agenda of May 20, 2020.



Boeing Toronto, Ltd.
P.O. Box 6013
Toronto, Ontario L5P 1B7
Canada

November 9th, 2005
Your ref: D.02.10

Mayor Hazel McCallion,
300 City Centre Drive,
MISSISSAUGA,
Ontario,
L5B 3C1.



Dear Mayor McCallion,

In 2003 I made a commitment to the City to establish a commemorative site on the present Boeing property in Malton. This commemoration was to honour the achievement of the men and women that have worked at the factory here in Malton over the years. Conceptual plans and ideas were formulated at that time to include an artifacts room and a plaque or plinth within public eyeshot.

As we progressed through the balance of the year and 2004 I consulted with several interested heritage groups and kept in regular contact with Mr. Mark Warrack, Heritage Co-coordinator, Community Services for the City of Mississauga, on our progress. My intention was to install this site on completion of our consolidation move from the old site.

In January 2005 Boeing decided to terminate the production of the Boeing 717 single aisle commercial jetliner. Despite work that myself and my management team had done in 2004 to protect us from such a decision Boeing ultimately decided to close the facility.

On September 7th, 2005 I met with members of your team to discuss how we could move forward and ensure the commitment to preserving the memory of this site could best be accomplished. The enclosed donation of \$25,000 CDN from The Boeing Company is made on behalf of all the employees who have been employed at the site and contributed to the achievements that have helped put Malton and Mississauga on the map. It is our desire that this contribution be used by the City, either in concert with the GTAA or for a stand alone project at the will of the City to commemorate the achievements of the men and women at the site. I will keep in touch with Mr. Warrack and would gladly be available should he need help and guidance as to the appropriateness of the commemoration.

On behalf of the Boeing Company I thank you and members of the council for recognizing the value of the aeronautical accomplishments of all the past employees at this site. I wish you good fortune for the future.

Yours sincerely,

Stephen J. Fisher
President,
Boeing Toronto Ltd

Cc: Councillor Katie Mahoney, Ward 8
Ms Janice Baker City Manager
Mr. Mark Warrack, Heritage Co-coordinator, Community Services

MLW

MemorandumCommunity Services Department
Planning and Heritage Section

TO: Eve Adams, Councillor, Ward 5

FROM: Paul Mitcham, Commissioner, Community Services

DATE: November 28, 2007

FILE: GTAA

SUBJECT: Commemorative Feature on the former Boeing Lands, GTAA

In 2005 Boeing Toronto Limited ceased operation and began to remove all structures from their lands west of Airport Road, south of Derry Road East. Boeing in partnership with the GTAA saved all of the limestone facing from the Administrative Building and donated \$25,000 toward a future commemorative structure honouring the area's aeronautical heritage. The GTAA has recently confirmed that they have the stone stored at an indoor location and will be pursuing a commemorative structure in the future.

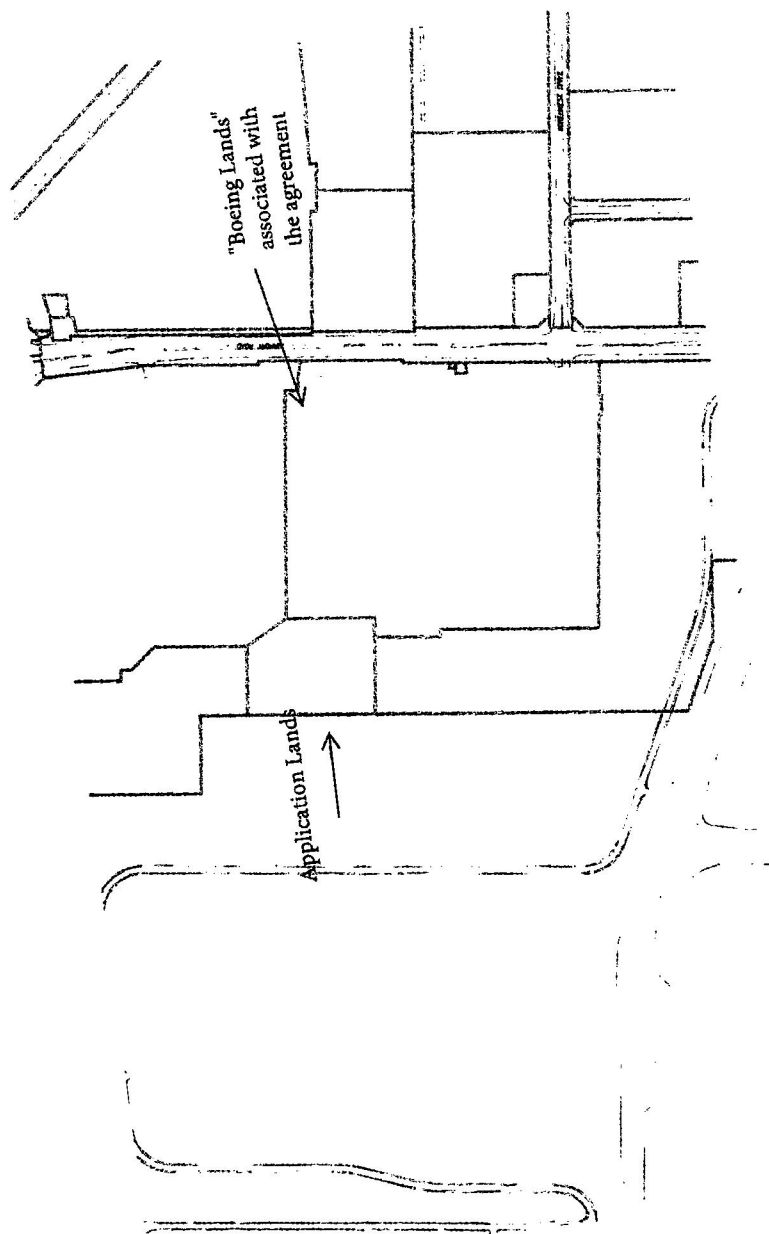
At the time Boeing was dismantling their buildings and divesting of all property, they contacted a number of local organizations who may have had an interest in acquiring their aeronautical artifacts and archives. I believe much of their collection went to the Toronto Aerospace Museum, the Region of Peel Archives and the remainder into private collections. The City did not have the means to acquire or care for any museum related material.

To date the GTAA has not decided upon the type of commemorative feature. It was discussed in 2005 that a small park, perhaps three acres in size, would be appropriate to house a commemorative structure and the necessary parking and landscaping to complete the site. Should this concept develop then it would be appropriate for the Community Services Park Planning and Development Section to take the lead in such a project. Consultation with other staff and stakeholders, such as MOMAC, would be encouraged as an important part of the process.

The GTAA has indicated that it may be a few years before they will acquire all of the former Boeing lands. Planning and Heritage staff will be the main contact with the GTAA. Ms Irene Hawrylyshyn, GTAA, is their representative who will keep us informed of the timing for this project.

If you have any further questions or concerns about the management of this potential project please do not hesitate to contact me.

Paul A. Mitcham, P.Eng
Commissioner, Community Services



WHEREAS on April 29, 2020, the Council of the City of Mississauga adopted Resolution 0113-2020, to amend Resolution 166-2019 to approve the official plan amendment and rezoning applications, submitted by BCIMC Realty Corporation, under file OZ 17/010 W9 to permit a 12 storey residential apartment building with 174 apartment dwelling units, located on the southwest corner of Glen Erin Drive and Battleford Road;

NOW THEREFORE LET IT BE RESOLVED

That the Resolution 0113-2020 be amended by replacing Resolution number '0166-2019' with Resolution number '0167-2019'; and replacing 'June 24, 2019' in the first paragraph with 'July 3, 2019'.