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# Mississauga Official Plan

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MOP Consolidation Version 6 - July 30, 2014



# Mississauga Official Plan

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# 1 Introduction

---

## 1.1 Background

Mississauga is at a decisive moment in its history – most of its greenfield lands have been developed and much of its infrastructure is in place. New growth will take place primarily through infilling and redevelopment in appropriate areas, which can benefit from growth and change, such as the Hurontario Street Corridor. Many areas, such as existing stable residential Neighbourhoods, will experience little change in the future.

Mississauga Official Plan provides a new policy framework to protect, enhance, restore and expand the Natural Areas System, to direct growth to where it will benefit the urban form, support a strong public transportation system, and address the long term sustainability of the city. Mississauga Official Plan will be an important instrument in city building. All change within the urban environment will be considered for its capacity to create successful places where people, businesses and the natural environment will collectively thrive.



Figure 1-1: Formed in 1974, Mississauga is recognized as Canada's sixth largest city and Ontario's third largest city, with a population of over 730,000 residents representing cultures from around the world. Mississauga has many attractions and events that run at various times throughout the year. The Downtown is a central spot for activities, including the Farmers' Market.

The City is required to review the Official Plan on a regular basis, as directed by the *Planning Act*. The Official Plan Review process was initiated to establish policies that will guide Mississauga through its next stage of growth and to align with the legislative and policy initiatives set out by the Province of Ontario and the Region of Peel.

In recent years, there has been a shift particularly at the Provincial level towards the promotion of development that is sustainable, supportive of public transit and oriented towards pedestrian based communities. The vision and planning direction that the City of Mississauga supports is consistent with recent Provincial policy initiatives.

The City will fulfill the Federal government requirements of an Integrated Community Sustainability Plan through its different municipal plans and policies, including Mississauga Official Plan.

### 1.1.1 Mississauga Strategic Plan

On April 22, 2009, following the comprehensive public engagement process “Our Future Mississauga – Be part of the conversation”, which connected with over 100,000 people, City Council approved a new Strategic Plan comprised of a Vision Statement and five Strategic Pillars for Change. Within the Strategic Plan, each Strategic Pillar for Change is connected with specific action items that will propel the Strategic Plan forward. These key action items are outlined in the Strategic Plan’s Action Plan – a complementary document that includes key indicators, targets, actions and funding options for each Strategic Pillar for Change.

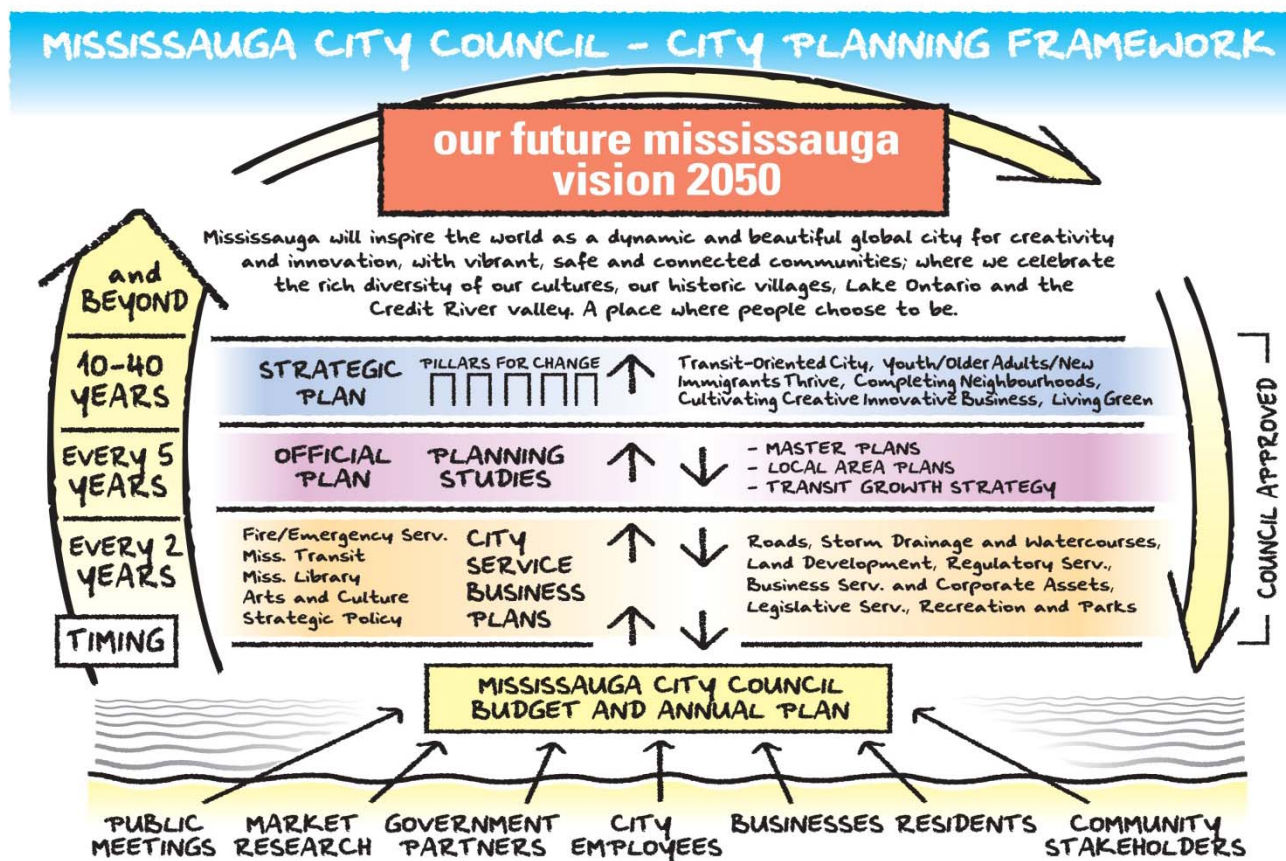


Figure 1-2: Mississauga’s planning framework is based on collaboration between the public, City Council and professional staff. Together, documents which express Mississauga’s vision and guide growth and development are created, implemented, reviewed and amended when necessary.



The Strategic Plan provides a long term vision to guide Mississauga as it embarks upon the next stage of its development. The City's first Strategic Plan was written to provide direction during a period of rapid greenfield development. Mississauga's history to date has been shaped by a pattern of suburban growth, strong service delivery, and prudent fiscal management. The city has grown from a collection of cherished villages to Ontario's third largest city, and the sixth largest in Canada. Mississauga is now entering a new stage in its evolution – one of intensification and urbanization. New challenges have arisen as Mississauga has grown and the Strategic Plan was updated to guide the city through the next phase of growth.

The Vision Statement articulated in the Strategic Plan depicts what the city will resemble in 40 years: "Mississauga will inspire the world as a dynamic and beautiful global city for creativity and innovation, with vibrant, safe, and connected communities; where we celebrate the rich diversity of our cultures, our historic villages, Lake Ontario, and the Credit River Valley. A place where people choose to be."

The Strategic Plan has identified five Strategic Pillars for Change, intended to provide guidance towards the creation of a city for the 21st century. Each Strategic Pillar for Change has its own unique direction statement and principle, along with specific strategic goals to ensure that the vision is achieved. They are described below.



### **Developing a Transit Oriented City**

Direction: Our Future Mississauga is a city where people can get around without a vehicle, and where transit will directly influence and shape the form of the city. Transit will be a desirable choice that connects people to destinations, and will underpin an environmentally responsible, inclusive, vibrant and successful city.

Principle: Mississauga is a city that values clean air and healthy lifestyles through the promotion of

transit as a preferred, affordable, and accessible choice.

#### **Strategic Goals:**

- Develop Environmental Responsibility
- Connect our City
- Build a Reliable and Convenient System
- Increase Transportation Capacity
- Direct Growth



### **Ensuring Youth, Older Adults and New Immigrants Thrive**

Direction: Our Future Mississauga is a city where people can spend their entire lives – where teenagers want to be, where young professionals choose to locate, where immigrants are welcomed and their cultures become a visible part of the city's fabric; and where people can age-in-place gracefully.

Principle: Mississauga is a city that thrives on its social and cultural diversity.

#### **Strategic Goals:**

- Ensure Affordability and Accessibility
- Support Aging in Place
- Attract and Retain Youth
- Attract Post-Secondary Institutions
- Nurture Diverse Cultures
- Integrate Places of Religious Assembly



### Completing Our Neighbourhoods

Direction: Our Future Mississauga is a beautiful, sustainable city with safe neighbourhoods that support a strong, connected and vibrant community – a place where all can live, work and prosper. People can play as a child, walk to meet a friend, fall in love, raise a family and grow old.

Principle: Mississauga is a city that nurtures a unique quality of life within each neighbourhood, where residents value the beauty and variety of the natural environment, engage in **active transportation**, and support a rich, healthy, and prosperous social and cultural mosaic through all stages of the life cycle.

#### Strategic Goals:

- Develop Walkable, Connected Neighbourhoods
- Build Vibrant Communities
- Create Great Public Spaces
- Celebrate our Community
- Provide Mobility Choices
- Build and Maintain Infrastructure
- Nurture “Villages”
- Maintain a Safe City
- Create a Vibrant Downtown



### Cultivating Creative and Innovative Businesses

Direction: Our Future Mississauga is a global hub of creative and innovative activity where talent and business thrive.

Principle: Mississauga is a city that values a strong global business future, fostering a prosperous and sustainable economy that attracts and grows talent.

#### Strategic Goals:

- Develop Talent
- Attract Innovative Business
- Meet Employment Needs
- Strengthen Arts and Culture
- Create Partnerships for Innovation



### Living Green

Direction: Our Future Mississauga is a city that coexists in harmony with its ecosystems, where natural areas are enhanced, forests and valleys are protected, the waterfront connects people to Lake Ontario, and communities are nurtured so that future generations enjoy a clean, healthy lifestyle.

Principle: Mississauga is a city that values its shared responsibility to leave a legacy of a clean and healthy natural environment.

#### Strategic Goals:

- Lead and Encourage Environmentally Responsible Approaches
- Conserve, Enhance and Connect Natural Environments
- Promote a Green Culture

The City of Mississauga will use the Strategic Plan to define the city’s priorities, processes and short and long term plans as well as prioritize budget and resource allocations.

Mississauga Official Plan is the document for implementing the actions identified in the Strategic Plan with respect to land development.

## 1.1.2 Mississauga Official Plan: Purpose

Mississauga Official Plan provides direction for the next stage of the city's growth and articulates a vision for a future Mississauga through specific guidance for both the public and private sectors in the ongoing evolution of the city. Mississauga Official Plan provides planning policies to guide the city's development to the year 2031, as required by Provincial legislation.

Mississauga Official Plan provides policies to manage and direct the physical change of the city and the effects of such change on the social, economic, cultural and natural environment. It also forms the basis for detailed land use designations and urban design policies, and sets the context for the review and approval of development applications.

## 1.1.3 Mississauga Official Plan: Organization and Structure

Mississauga Official Plan is organized as follows:

### Part 1 – Introduction, Policy Context and Promote Collaboration

This section identifies relevant background policies, primarily through a review of the Provincial, Regional and City policy context that have provided general direction for the Mississauga Official Plan. It also includes policies to promote collaboration in the planning process.

### Part 2 – City Wide Policies

The city wide policy framework for Mississauga Official Plan is contained in this section. A vision for the future of the city is articulated and key policy directions are established.

### Part 3 – Land Use Designations

The directions in Part 2 are implemented in this section. It presents the city structure and identifies Character Areas. Where applicable, Special Site Policies for each Character Area describe the unique condition and identity of different areas within the

city. Special Site Policies also describe exceptions in the policy framework and the policies that apply to these sites.

## Part 4 – Implementation and Glossary

This section sets out implementation related policies, strategies, regulations, and monitoring mechanisms to achieve the vision. The Glossary defines terms used throughout Mississauga Official Plan.

### Schedules

Schedules form part of this Plan and must be read in conjunction with Parts 2 and 3 of this Plan.

### Appendices

Appendices are provided for information and are not part of this Plan.

### Local Area Plans

Local Area Plans are also part of Mississauga Official Plan. Local area plans address unique circumstances particular to a specific area and must be read in conjunction with Parts 1 to 4 and the schedules of this document.

## 1.1.4 How to Read Mississauga Official Plan

To understand the planning rationale and policy objectives of Mississauga Official Plan, also referred to as "Official Plan", "the Plan" or "this Plan", it should be read in its entirety and all relevant text, tables, and schedules are to be applied to each situation.

The vision articulated in Mississauga Official Plan is contingent on an understanding of the city as a complex, urban place where sometimes conflicting considerations must be effectively balanced so that the city can move forward with planning approaches that align with larger city building objectives.

Mississauga Official Plan replaces the 2003 Mississauga Plan. The following provides guidance for the interpretation of this document:

- a. Mississauga Official Plan document includes text, tables, schedules, figures and associated captions, and appendices. The text, tables, maps and schedules are the policies of Mississauga Official Plan and must be read together. The figures and associated captions and the appendices have been included for information and illustration purposes only and are not policy. The text, tables, maps and schedules will be amended through an official plan amendment. Figures and associated captions and appendices will be adopted and amended by by-law.
- b. The location of boundaries and symbols, including land use designations, are approximate and they are not intended to define the exact locations except where they coincide with major roads, railways, transmission lines, major **watercourses**, or other bodies of water, and other clearly recognizable physical features. Future roads and public transit systems are shown in approximate locations only. Numbers are approximations, except designated rights-of-way widths.
- c. Mississauga may undertake or require a number of studies to address planning matters including the following:
  - a municipal comprehensive review is an official plan review or an official plan amendment, initiated by the City, that has city wide policy implications. This includes, among other matters, changes to the urban structure or conversion of employment lands;
  - a local area review of a Character Area, **Corridor** or **Major Transit Station Area** is typically undertaken by or on behalf of the City and will be incorporated into this Plan by amendment;
  - a development master plan is prepared by a development proponent at the direction of the City and to the City's satisfaction to assist with the evaluation of development applications until such time as a local area review has been completed; and
  - planning studies may address a variety of Official Plan policies including matters relating to land use, transportation, environment, or urban design that are limited in scope or geography. These reviews are typically undertaken by or on behalf of the City and may or may not result in an amendment to this Plan.
- d. Interpretation of the intent of Mississauga Official Plan, or any part thereof, will be made by City Council on the advice of staff. Where terms such as "acceptable" or "appropriate" are used, the determination of compliance will be made by City Council on the advice of staff.
- e. Where there is a conflict between the policies relating to the natural and cultural heritage and the rest of this Plan, the direction that provides more protection to the natural and cultural heritage will prevail.
- f. There are sites within Character Areas that merit special attention. While special sites are areas with unique circumstances, they complement and support the long term vision of the Plan. As such, they are to be read in conjunction with the general land use policies and all other applicable policies of the Plan. Unless otherwise stated, the lands may be developed in accordance with their land use designation and/or the uses permitted by the special site. It is intended that special sites will be reviewed during the preparation of local area plans or other planning studies.
- g. There are sites in Mississauga identified in Part 3 as "exempt". Exempt sites reflect unique circumstances that are not representative of the vision, direction and planning policies of the Plan, but nonetheless are recognized because they contain established land uses. Mississauga Official

- Plan will not be interpreted to prevent the use of any land or building on these sites for a purpose that is contrary to the Plan, if such uses or buildings lawfully existed on the date of the approval of the Plan. Generally such uses will be encouraged to relocate to lands appropriately designated. "Exempt sites" will be reviewed during the preparation of local area plans or other planning studies. It is intended that these lands will eventually be redeveloped in accordance with the underlying designation. In the interim, lands zoned to permit such uses or buildings are deemed to be in conformity with the provisions of the Plan. The lands may be developed in accordance with their land use designation and/or the uses permitted by the individual exempt site.
- h. Provided that the purpose, effect, intent, meaning, and substance are in no way affected, the following technical revisions to the Plan are permitted without an official plan amendment:
    - changing the numbering, cross referencing, and arrangement of the text, tables, maps and schedules, figures and associated captions, or appendices;
    - revising base map information; altering punctuation or language for consistency; and
    - correcting grammatical, dimensional and boundary, mathematical or typographical errors.
  - i. The indication of any proposed services or infrastructure, roads, or parks in text or on maps or schedules is not a commitment by City Council to provide such services within a given time period unless otherwise stated in the Plan and is subject to budgetary and other applicable approvals.
  - j. Singular terms include the plural and plural terms include the singular.
  - k. "Airport" means the Toronto – Lester B. Pearson International Airport.
  - l. "character" means the aggregate of the features including the attributes of the physical, natural and social dimensions of a particular area or neighbourhood.
  - m. "City", when capitalized, means The Corporation of the City of Mississauga.
  - n. "city", when not capitalized, means the geographic area of the City of Mississauga.
  - o. "community infrastructure" means lands, buildings, and structures that support the quality of life for people and communities by providing public schools, private schools, emergency services, private clubs, **community facilities**, daycare/day program and places of religious assembly. Private club means a social, cultural, athletic or recreational club or fraternal organization that is not operated for profit.
  - p. "compatible" means development, which may not necessarily be the same as, or similar to, the existing or desired development, but nonetheless enhances an established community and coexists with existing development without unacceptable adverse impact on the surrounding area.
  - q. "conform to" means to comply with or be in agreement with a policy or requirement of the Plan.
  - r. "conserve" means the identification, protection, use and/or management of cultural, heritage and archaeological resources in such a way that their heritage values, attributes, and integrity are retained. This may be addressed through a conservation plan or heritage impact assessment.
  - s. "consistent with" means to be in agreement or not in conflict with a policy or requirement of the Plan.



- t. “current” means most recently approved practices and standards or policies that are in effect.
- u. “discourage” means not permitted unless it can be demonstrated that compliance with the policy:
- is not possible; or
  - would not result in good planning; or
  - does not meet the overall intent of this Plan.
- v. “density” means the intensity of use permitted on a property or the concentration of jobs or people within a defined area. When referring to development, density means the floor space of a building(s) or number of units in relation to a given area of land.
- w. “Downtown” corresponds to the “Urban Growth Centre” as defined in the Growth Plan for the Greater Golden Horseshoe 2006 and refers to the city structure element.
- x. “encourage” means to carefully consider or take into account.
- y. “enhance” means to complement and assist in furthering the aesthetic and intrinsic value of a neighbourhood, site, or structure. As applied to the environmental policies of the Plan, enhance means intensifying components of a natural area through management measures to increase stability, **biodiversity**, and long term viability.
- z. “existing” includes built and approved development at the time this Plan is adopted by City Council.
- aa. “may” means a discretionary, but not a mandatory policy or requirement of the Plan.
- bb. “Neighbourhood”, when capitalized, refers to the city structure element.
- cc. “neighbourhood”, when not capitalized, refers to an undefined geographic area containing primarily residential dwellings.
- dd. “pedestrian” means a person who travels by foot or with a mobility assisted device, e.g. a wheelchair, and matters pertaining to pedestrian movement including universal accessibility.
- ee. “preserve” when referring to Natural Areas, means maintaining a natural area by protecting the stability, **biodiversity** and long term viability of its components. When referring to heritage resources, preservation includes both short term and interim measures to protect or stabilize the area or feature, as well as long term actions to retard deterioration or prevent damage so that the area or feature can be kept serviceable through routine maintenance and minimal repair, rather than extensive replacement and new construction. Change.
- ff. “Region”, when capitalized, means the Region of Peel.
- gg. “region”, when not capitalized, refers to the geographic area of the Region of Peel and surrounding area.
- hh. “restore” means developing components of a natural area through the re-creation or reinstatement of conditions previously associated with stability, **biodiversity**, and long term viability.
- ii. “should” means to carefully consider or take into account.
- jj. “sustainable” means development that meets the needs of the present without compromising the ability of future generations to meet their own needs.
- kk. “will” denotes a mandatory requirement of the Plan.
- ll. Italicized terms throughout the text do not appear in the Glossary. These correspond to

specific documents and policies when identified by their title (e.g., *Planning Act*).

mm. Italicized and bolded terms throughout the text are defined in the Glossary. The definition of the following terms are as defined in the Provincial Policy Statement or the Growth Plan for the Greater Golden Horseshoe:

- Affordable
- Areas of natural and scientific interest (ANSI)
- Built up area
- Built boundary
- Complete communities
- Cultural heritage landscape
- Designated greenfield area
- Development
- Dynamic beach hazard
- Ecological function
- Endangered species
- Erosion hazard
- Fish habitat
- Flood fringe
- Flooding hazard
- Flood plain
- Flood proofing standard
- Floodway
- Ground water feature
- Intensification
- Major office
- Modal share
- Multi-modal
- One hundred year flood
- Redevelopment
- Regional market area

- Sensitive land uses
- Significant areas of natural and scientific interest
- Significant coastal wetlands
- Significant habitat of endangered species and threatened species
- Significant valleylands
- Significant wetland
- Significant wildlife habitat
- Significant woodlands
- Site alteration
- Special needs
- Special policy area
- Threatened species
- Transportation corridor
- Transportation Demand Management
- Valleylands
- Watershed
- Wetlands
- Wildlife habitat
- Woodlands

For convenience, Appendix A provides the definition of the terms listed above. If there is a discrepancy between the definition in Appendix A and the Provincial Policy Statement or the Growth Plan for the Greater Golden Horseshoe, the definition in the Provincial Policy Statement or the Growth Plan for the Greater Golden Horseshoe will apply.

The meaning of a term not defined in the Glossary or included in the above list of terms defined in the Provincial Policy Statement and Growth Plan for the Greater Golden Horseshoe, will be as defined in the Canadian Oxford Dictionary, 2<sup>nd</sup> edition.

nn. Metric measurements are used in this Plan.

## Steps to determine the designation and use of a property:

Step 1	Locate the property on Schedule 1, Urban System, to determine the applicable components of the Urban System. Reference should be made to the relevant sections regarding the components in Chapter 5, Direct Growth.
Step 2	If the property is located in the Green System, locate the property on Schedule 3, Natural System, to determine if the property is affected by the Natural Areas System or Natural Hazard Lands. If the property is located in, or adjacent to these systems, reference should be made to the relevant sections in Chapter 6, Value the Environment.
Step 3	Locate the property on Schedule 10: Land Use Designations and identify the designation. Refer to the applicable General Land Use Policies in Chapter 11.
Step 4	Locate the property on Schedule 9, Character Areas to determine which element of the City Structure (i.e. Downtown, Major Node, Community Node, Corporate Centre, Neighbourhood, Employment Area or Special Purpose Area) the property is located within. Reference should be made to the relevant policies for the element in which the property is located in Chapters 12 to 18.
Step 5	Determine from Schedule 9, Character Areas, the individual Character Area (e.g. Downtown Core or Applewood Neighbourhood) where the property is located. Refer to the policies for that Character Area in Chapters 12 to 18.
Step 6	Part 2: City Wide Policies contains policies that will affect how a property may be used in accordance with its land use designation. Reference should be made to all relevant policies in Part 2.

Figure 1-3: Steps to determine the designations and use of property.

## 2 Policy Context

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Mississauga Official Plan must conform with a hierarchy of policy and legislation at the federal, provincial, regional and municipal levels. In particular, provincial policy initiatives provide a strong direction for the growth management and development strategies found herein. This chapter provides information regarding the policies and responsibilities of other levels of government.

### 2.1 Provincial Documents

#### 2.1.1 *Planning Act*

The *Planning Act*, R.S.O. 1990, c.P.13, as amended, is the provincial legislation that defines the policy framework for land use planning in Ontario. The *Planning Act* enables and directs municipalities to prepare Official Plans, in order to establish planning goals and policies that will coordinate growth, guide future land use, and manage and direct physical change.

The *Planning Act* has been established to:

- promote sustainable economic development in a healthy natural environment within a provincial policy framework;
- provide for a land use planning system led by provincial policy;
- integrate matters of provincial interest into municipal planning decisions;
- provide for planning processes that are fair by making them open, accessible, timely and efficient;
- encourage cooperation and coordination among various interests; and
- recognize the decision making authority and accountability of municipal councils in planning.

Mississauga Official Plan implements the *Planning Act* at the local level. It provides a framework for comprehensive, integrated, and long term planning that reflects the principles and requirements of the *Planning Act*.

#### 2.1.2 Provincial Policy Statement

The current Provincial Policy Statement (PPS) came into effect on March 1, 2005. The PPS was issued under Section 3 of the *Planning Act* and it provides direction on matters of provincial interest related to land use planning and development.

The PPS promotes Ontario's long term prosperity and social well-being by:

- wisely managing change and promoting efficient land use and development patterns;
- protecting natural heritage, water, agriculture, mineral and cultural heritage and archaeological resources for their economic, environmental and social benefits; and
- reducing the potential for public cost or risk to Ontario residents from natural or human hazards.

#### 2.1.3 Provincial Growth Plan

Prepared under the *Places to Grow Act 2005*, the Growth Plan for the Greater Golden Horseshoe 2006 is the centrepiece of a regional growth management strategy. The Growth Plan took effect on June 16, 2006.

The Growth Plan vision is grounded in the following principles that provide the basis for guiding decisions on how land is developed, resources are managed and public dollars are invested:

- build compact, vibrant and complete communities;

- plan and manage growth to support a strong and competitive economy;
- protect, conserve, enhance and wisely use the valuable natural resources of land, air and water for current and future generations;
- optimize the use of existing and new infrastructure to support growth in a compact, efficient form;
- provide for different approaches to manage growth that recognizes the diversity of communities in the Greater Golden Horseshoe; and
- promote collaboration among all sectors – government, private and non-profit – and residents, to achieve the vision.

#### 2.1.4 Parkway Belt West Plan

The Parkway Belt West Plan, prepared under the *Parkway Belt Planning and Development Act 1973* was approved on July 19, 1978. The Parkway Belt West Plan was deemed to be a development plan under the *Ontario Planning and Development Act, 1994*. The Parkway Belt West Plan was originally introduced as part of the development framework for the Toronto-Centred Region. The Parkway Belt West Plan defines a contiguous multipurpose corridor extending from Markham across to Hamilton.

The purposes of the Parkway Belt West Plan are:

- to separate and define the boundaries of urban areas in the western Greater Toronto Area;
- to link urban areas by providing space for the movement of people, goods, energy and information;
- to provide a land reserve for future linear facilities (including major transportation, communication, and utility facilities) and for unanticipated activities; and

- to provide a linked system of open space and recreational facilities.

Some lands originally within the Parkway Belt West have since been removed through amendment.

#### 2.1.5 Greenbelt Plan

The Greenbelt Plan was established under the *Greenbelt Act, 2005*. Ontario's Greenbelt is a broad band of permanently protected land that extends from the Niagara River to the eastern end of the Oak Ridges Moraine. The Greenbelt Plan protects environmentally sensitive land and farmland in Ontario's Greater Golden Horseshoe area from urban development. The Greenbelt Plan identifies where major urban growth cannot take place.

Mississauga is not located within the Greenbelt area and, as such, the *Greenbelt Act, 2005* does not apply in Mississauga. However, the Greenbelt Plan recognizes that the natural heritage systems contained in the Greenbelt are connected to systems beyond the Greenbelt. Two of the external connections identified from the Greenbelt to Lake Ontario are the Credit River and Etobicoke Creek, which flow through Mississauga. The Greenbelt Plan provides direction to municipalities for the long term protection and enhancement of these external connections.

#### 2.1.6 Regional Transportation Plan

The mandate to create a Regional Transportation Plan (RTP) is embodied in the *Greater Toronto Transportation Authority Act, 2006*, which established Metrolinx and directed it to create a long term strategic plan for an integrated, multi-modal, regional transportation system. As defined by the Act, this is to be a transportation plan that:

- takes into account all modes of transportation;

- makes use of intelligent transportation systems;
- promotes the integration of local transit systems with each other and with the GO Transit system;
- works toward easing congestion and commute times, and reducing transportation related emissions of smog precursors and greenhouse gases; and
- promotes transit supportive development and the viability and optimization of transit infrastructure.

This Plan titled *The Big Move: Transforming Transportation in the Greater Toronto and Hamilton Area* was adopted by the Metrolinx Board of Directors on November 28, 2008. Metrolinx will use this Plan to provide input to major Regional and local planning exercises including significant development applications that may have an effect on the Regional Transportation System.

## 2.2 Region of Peel Official Plan

The Region of Peel Official Plan (ROP) was adopted by Regional Council on July 11, 1996 and approved by the Minister of Municipal Affairs and Housing on October 22, 1996. Subsequent amendments have revised ROP, including a series of amendments to bring it into conformity with the Growth Plan for the Greater Golden Horseshoe 2006.

The ROP is a long term plan to manage the Region's growth and development. The *Planning Act* requires that the Official Plans of lower-tier municipalities conform to the Official Plans of their respective upper-tier municipalities.

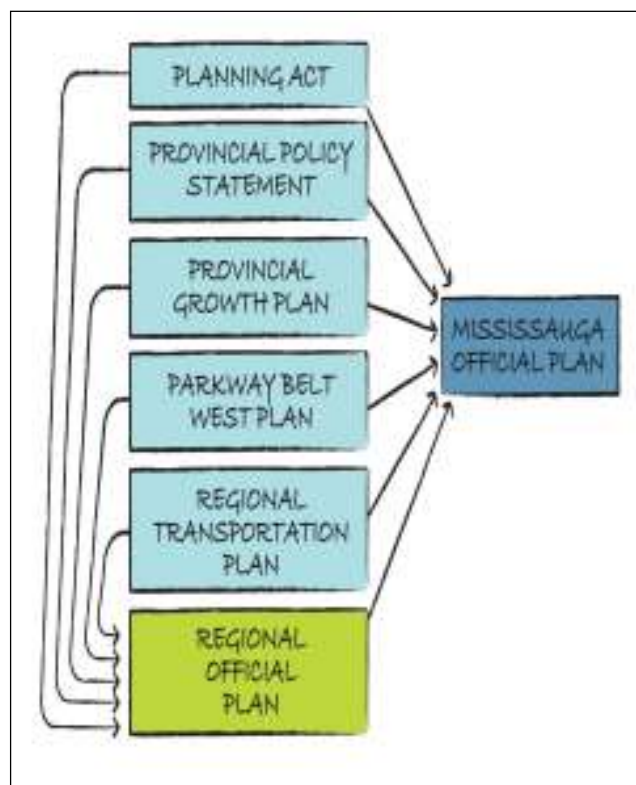


Figure 2-1: Many documents influence Mississauga Official Plan, providing direction and guidance. Mississauga Official Plan is in conformity with these documents.

The Province has delegated approval authority of Mississauga Official Plan and official plan amendments to the Region of Peel.



# 3 Promote Collaboration

## 3.1 Introduction

Mississauga promotes participation, collaboration, education and partnerships with the public, businesses, institutions, cultural industries and all levels of government in the planning process.

3.1.1 Mississauga encourages all individuals and groups to take an active interest in the planning of the city.

3.1.2 Mississauga will ensure ongoing consultation with the public and stakeholders on the implementation of this Plan.

3.1.3 Mississauga will take into consideration people with disabilities in the consultation with the public.

3.1.4 Mississauga will develop a public participation program as part of any Official Plan review. Public participation programs will be designed to increase public awareness, encourage

active public involvement in the planning process, and, where appropriate, facilitate consultation and joint planning.

3.1.5 Planning policies and reports will be made available to encourage continuing public awareness and input into the process and to assist in preparing briefs, submissions and responses to planning activities.

3.1.6 In the consideration of proposed development applications to amend the official plan, the zoning by-law and plans of subdivision, adequate information will be made available to the public, signs outlining the proposed land use will be erected on-site by development applicants and at least one public meeting will be held.

3.1.7 Statutory notice of the public meeting will be given by advertisement in a local newspaper. This method may be augmented by individual notice forwarded to owners and occupants of land within 120 m of the lands subject to a development application as shown on the last revised assessment



Figure 3-1: An example of public engagement for the Hurontario/Main Street Corridor Master Plan, which explores the introduction of rapid transit on Hurontario Street from Port Credit to downtown Brampton.

roll or tax roll, and to those people who have requested notice. Public notice must be given 14 days prior to the holding of a public meeting by Planning and Development Committee and 30 days prior to the meeting of City Council. At the discretion of the Ward Councillor, mobile signs may also be used to provide additional notification.

3.1.8 If an open house is required by the *Planning Act*, notice will be given by advertisement in a local newspaper.

3.1.9 The public meeting will be held by Planning and Development Committee, which is a standing Committee of City Council, or by City Council.

3.1.10 If the meeting is held by the Planning and Development Committee, the recommendation will be considered by City Council at a meeting open to the public. Any person may arrange to appear before Planning and Development Committee or City Council by contacting the Office of the City Clerk. Consideration of Planning and Development Committee recommendations by City Council for matters requiring a public meeting under the *Planning Act*, will not occur until 30 days from the date when notice of public meeting was given. In unusual circumstances - for example, when an Ontario Municipal Board hearing has been scheduled or when no City Council meeting is scheduled because of a holiday period - Planning and Development Committee recommendations may be brought forward for the consideration of City Council prior to the expiry of the 30 day period.

3.1.11 Should a proposed plan of subdivision, official plan amendment and/or a proposed amendment to the zoning by-law change significantly subsequent to the public meeting, City Council may hold a second public meeting. If the change is not significant and still reflects the intent of the recommendation of City Council, a further public meeting may not be required prior to approval of the application by City Council.

## 3.2 Community Planning

Mississauga is committed to an ongoing process to develop local area plans. An integral part of this process is the engagement of the local community to ensure that their needs, desires and aspirations are addressed.

3.2.1 The public and other stakeholders will be engaged throughout the preparation of local area plans. This may include but not be limited to, the development of a vision for the study area, the preparation of a directions report to form the basis of the local area plan, the formulation of policies and revisions based on comments received from stakeholders.

3.2.2 The City may consider establishing a Local Advisory Panel as input to the local area plan review.



Figure 3-2: Mississauga is committed to engaging the community in the planning process. As planning studies are prepared the City may use a variety of techniques to involve the community, such as holding community meetings, hosting open houses, conducting workshops or creating working groups.



# 4 Vision

## 4.1 Introduction

The vision for Mississauga is that it will be a beautiful sustainable city that protects its natural and cultural heritage resources, particularly the Lake Ontario waterfront, Credit River and other valley corridors, and its established, stable neighbourhoods. The City will plan for a strong,

diversified economy supported by a range of mobility options and a variety of housing and community infrastructure to create distinct, complete communities. To achieve this vision the City will revitalize its infrastructure, conserve the environment and promote community participation and collaboration in its planning process.



Figure 4-1: Mississauga is situated near the centre of the Greater Golden Horseshoe, one of the fastest growing regions in North America.

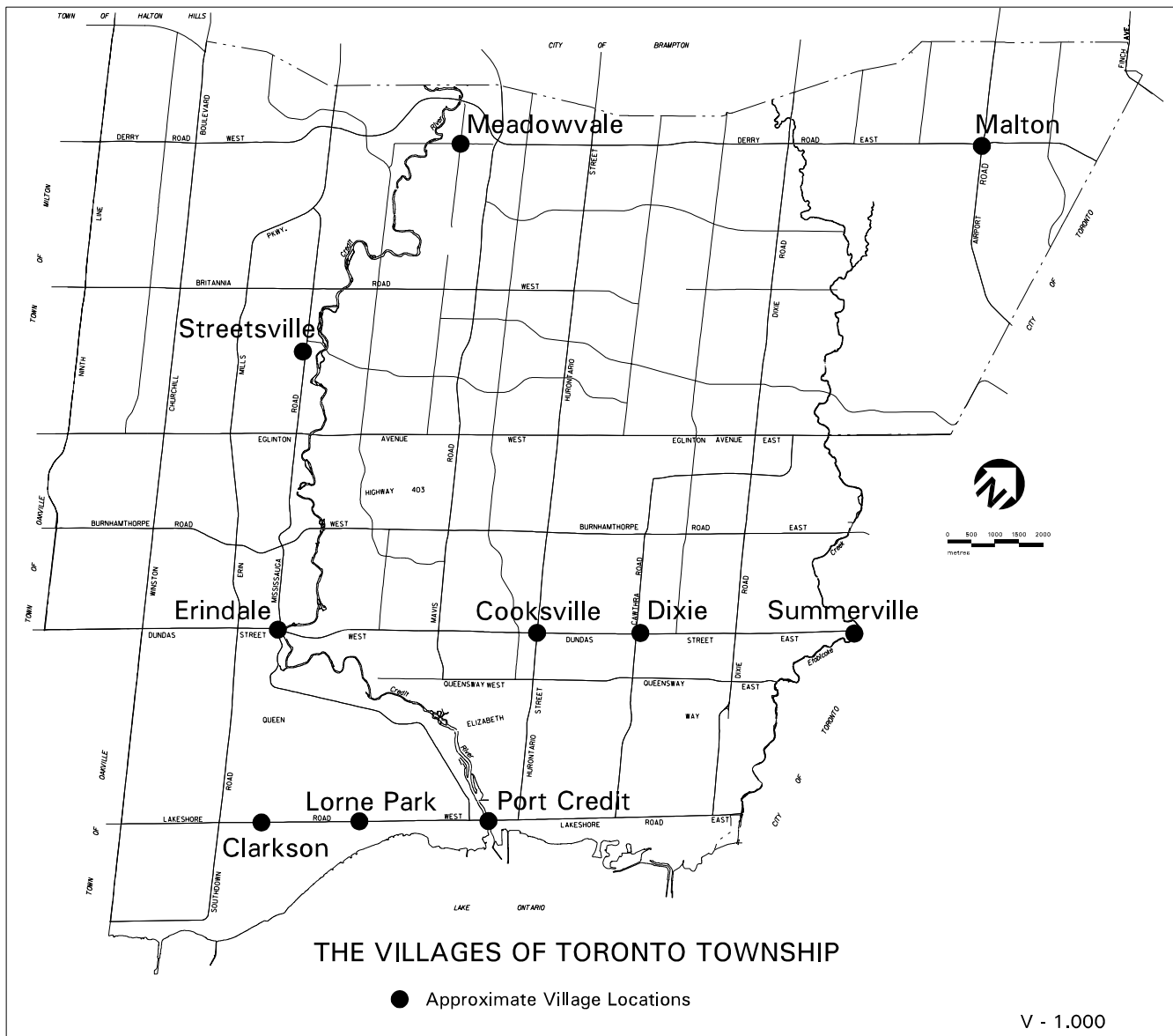


Figure 4-2: At the beginning of the 19th century, many hamlets and villages were established in the Toronto Township area. As fortunes declined in the late 1800's, some villages like Summerville disappeared. Later in the 20th century, other new settlements were formed and villages such as Erindale and Malton experienced a resurgence.

Mississauga is strategically located at the heart of the Greater Golden Horseshoe - one of the largest and fastest growing urbanized regions in North America. The region's economic output accounts for over half of the provincial gross domestic product and represents two thirds of Ontario's population, with most residing within a 100 km radius of Mississauga's Downtown Core.

Mississauga is located on the shores of Lake Ontario and as a waterfront community, the lake is a significant element of the city's identity. In addition to the Lake Ontario shoreline, Mississauga has a Natural Areas System that is characterized by many river valley systems and natural areas. Protection, enhancement and restoration of these features are essential to the City's vision for the future.

Mississauga is known for its safe, high quality neighbourhoods, excellent schools and valued community amenities. It is a location of choice for employers and citizens. During the Our Future Mississauga community visioning exercise, thousands agreed that Mississauga is a great place to live, learn, work and play. The objective of this Plan is to build upon our successes and guide future development so that Mississauga will continue to be a great place.

## 4.2 Historical Context

Incorporated in 1974, Mississauga is a comparatively young city, but its long and rich history provides a context for better understanding the city's past and future.

The name "Mississauga", believed to mean 'river of the north of many mouths', is reminiscent of the Aboriginal peoples who once occupied the land. In the 1600s, European traders who encountered the Ojibwa tribe in the area around the Credit River Valley, named them 'Mississaugas' after the Mississagi River on the northwestern shore of Lake Huron. The Credit River continued to provide important functions for trading, transportation and fisheries.

Government officials from York, now known as Toronto, bought 34 000 hectares of the Mississauga Tract in 1805, and on August 2, 1805 this region was formed into Toronto Township. Some of the settled communities include Clarkson, Cooksville, Dixie, Erindale (called Springfield until 1890), Lorne Park, Malton, Meadowvale, Port Credit, Streetsville and Summerville. Many of the smaller communities were located near natural resources, waterways for industry and fishing, and routes leading into York. Most early villages served the farming community or local industries that processed natural resources including lumber, grain and farm products.



Figure 4-3: The City Council Chambers, the site of City Council meetings, has an extraordinary ceiling created by artist Sharon McCann illustrating the Ojibwa legend of the Great Bear and the Seven Hunters.

In 1820, the Crown made a second purchase and additional settlements were established. This led to the eventual displacement of the Mississaugas, who were relocated in 1847 to a reserve in the Grand River Valley near present day Hagersville.

In light of the continued growth seen in this area, the Toronto Township Council was formed in 1873 to oversee the affairs of the various villages, which were unincorporated at that time.

Throughout much of the 19th and first half of the 20th century, most of present day Mississauga, except for small villages, was agricultural land that included fruit growing orchards. By the 1880's, cottages were being constructed along Lake Ontario as weekend getaway houses for city dwellers. After World War I, these became year-round residences from which to commute.

The majority of growth in Mississauga occurred in post World War II following the establishment of an extensive regional highway network, including the Queen Elizabeth Way and Highways 401, 427 and 403. These major transportation projects significantly improved access to different settlements and markets within the region. GO Train service was added along the Lakeshore Road railway line in 1967, but most of the communities created during this period were car oriented.

Port Credit and Streetsville became towns in 1961 and 1962, respectively. In 1968, the township settlements of Lakeview, Cooksville, Lorne Park, Clarkson, Erindale, Sheridan, Dixie, Meadowvale Village and Malton were amalgamated to form the Town of Mississauga.

Growth and development generally moved north and west from southern Mississauga with large scale developments such as in Meadowvale and Erin Mills being built in the 1960's and 1970's.

In 1974, the Town of Mississauga amalgamated with Port Credit, Streetsville and portions of the Townships of Toronto and Trafalgar to form the City of Mississauga.



Figure 4-4: The development of the Toronto - Lester B. Pearson International Airport effectively prohibits new residential uses in the city's northeast due to requirements that sensitive land uses be distanced from higher airport noise levels. Although the airport has implications on land use, it is a major transportation hub that is vital to Mississauga's economy.

As the population grew from 33 000 in 1951, infrastructure improvements, residential expansion, and industrial and commercial development ensued. Lands were no longer developed into small town scaled parcels but instead large tracts of land were planned for residential and industrial subdivisions. In general, residential and industrial/employment uses were separated in the city.



Figure 4-5: Grid roads gave way to circuitous road patterns and cul-de-sacs to discourage traffic from cutting through neighbourhoods; tall noise walls were erected along major streets to shield neighbourhoods from traffic noise.

In the postwar period, as cars became the dominant mode of transportation, the economic and social function of main streets as the hub of commercial and social interaction was diminished. The primary function of major roads became the rapid movement of cars and trucks, resulting in higher traffic volumes and faster driving speeds. Many primary corridors became unfriendly environments for pedestrians and cyclists. Gradually, community designs focused on separating cars and noise from people. The resulting network of curvilinear local roads would present challenges for transit planners.

Over the past two centuries, Mississauga evolved from an agrarian society to one of the most multiculturally diverse and economically successful cities. It quickly became a popular and affordable place to live and invest.



The physical legacies of Mississauga's rapid expansion period are evident today and pose planning challenges in becoming the sustainable urban community envisioned by this Plan.

## 4.3 Current Context

As Canada's sixth largest city, Mississauga has been one of the fastest growing and most economically successful cities in the country. In 1976, the city had a population of approximately 250 000 and supported more than 130 000 jobs. In 2009 these figures increased to 730 000 people and 453 000 jobs.



Figure 4-6: Mississauga welcomes the more than 50% of its population born outside of Canada, in many cultural festivals throughout the year.

In terms of population and housing, Mississauga is ranked as the third most densely developed municipality in the region after Toronto and Hamilton. The Provincial Growth Plan for the Greater Golden Horseshoe projects that an additional 3.7 million people will live in the region by 2031, accompanied by the creation of 1.8 million new jobs. During this time Mississauga's population and employment is expected to increase to approximately 805 000 people and 510 000 jobs.

People of diverse backgrounds, ages and abilities are choosing to live, work and invest in Mississauga.

### Housing Breakdown | 2009



Figure 4-7: In Mississauga, housing choices are available for a range of household sizes and types, including working families, singles and seniors.

They not only want to raise their families in the community but they also want to spend their senior years in communities that offer appealing amenities and healthy, urban lifestyle options.

At a city level, there is a wide range of housing choice in Mississauga. Given that the city is at the end of its greenfield growth phase, new residential development is expected to be accommodated in already developed areas in compact forms such as townhouses and apartments.

With a thriving and diverse economy, Mississauga boasts more than 60 "Fortune 500" companies representing a variety of employment sectors. Employment continues to remain strong, and Mississauga is expected to maintain its current role



Figure 4-8: Mississauga must continue to maintain a supply of traditional employment lands to maintain current and future needs.

as a net importer of labour in the Greater Golden Horseshoe. After Toronto, Mississauga is the leading player in the Greater Golden Horseshoe office market. While office growth is burgeoning and expected to maintain pace for years to come, it is necessary to maintain a stable inventory of industrial lands to preserve and attract a wide range of employment.

Mississauga's sustained population and employment growth will continue to present both challenges and opportunities that need to be addressed through an appropriate growth management strategy. In accordance with the Provincial Policy Statement, Provincial Growth Plan, and Region of Peel Official Plan, Mississauga will embrace sustainability in its future growth and land use decisions.

## 4.4 Guiding Principles



Figure 4-9: Mississauga is home to residents of all ages who thrive in their communities.

Mississauga Official Plan has been prepared to assist in the implementation of the long term vision formulated through a comprehensive public engagement process known as, "Our Future Mississauga – Be Part of the Conversation" that resulted in a new Strategic Plan. The goals,

objectives and policies of Mississauga Official Plan align with the City's Strategic Plan and focus specifically on the long term land use, growth and development plans for Mississauga.

Mississauga Official Plan will guide the physical evolution of the city through the management of growth and land development to 2031 and beyond. The Official Plan adopts a new approach to land use planning in Mississauga, with a focus on the strategic management of growth and change through the integration of land use, transportation and design objectives. It includes promoting growth in locations where it is financially sustainable and where it can be developed in compact efficient forms, supported by existing and planned infrastructure. This approach also includes the protection and enhancement of stable areas including the city's natural and cultural heritage resources and residential neighbourhoods.

In order to protect what is valuable and shape change responsibly, Mississauga Official Plan subscribes to the following key guiding principles for land use:

1. Mississauga will promote development decisions that support the sustainability of our natural heritage system and enhance the quality of life for our present and future generations;
2. Mississauga will protect, enhance and where possible restore distinct **natural features, areas and linkages, including their ecological functions**, particularly those associated with the Lake Ontario waterfront and the city's river and valley corridor system;
3. Mississauga will preserve the character, cultural heritage and livability of our communities;
4. Mississauga will maintain and promote a strong and sustainable, diversified economy that provides a range of employment opportunities for residents and attracts lasting investment to secure financial stability;
5. Mississauga will provide a range of mobility options (e.g., walking, cycling, transit, vehicular)

for people of all ages and abilities by connecting people with places through coordinated land use, urban design and transportation planning efforts;

6. Mississauga will plan for a wide range of housing, jobs and community infrastructure resources so that they are available to meet the daily needs of the community through all stages of life;
7. Mississauga will support the creation of distinct, vibrant and complete communities by building beautifully designed and inspiring environments that contribute to a sense of community identity, cultural expression and inclusiveness; and
8. Mississauga will be a city that promotes the participation and collaboration of all sectors including residents, employees, entrepreneurs, government, business, education and non-profit, to achieve this vision.

## 4.5 Achieving the Guiding Principles

Mississauga Official Plan will implement the guiding principles through the following strategic actions:

### Direct Growth



Figure 4-10: Growth will be directed to areas identified for intensification such as the Downtown Core. The above model illustrates actual and potential development within the Downtown Core, helping to visualize how new growth will relate to existing structures.

As Mississauga continues to evolve, growth will be strategically managed by determining the appropriate arrangement and balance of land uses, including population and employment densities. Growth will be directed to key locations to support existing and planned transit and other infrastructure investments. Growth will not be directed to areas of the city that need to be preserved and protected (e.g. stable residential areas, Natural Areas System and cultural heritage resources).

Mississauga will direct growth by:

- focusing on locations that will be supported by planned and **higher order transit**, higher density, pedestrian oriented development and community infrastructure, services and facilities;
- protecting stable areas and natural and cultural heritage; and
- achieving balanced population and employment growth.

### Value the Environment

Mississauga has natural areas of exceptional beauty and quality. Mississauga will serve as a steward of



Figure 4-11: Located on the scenic east bank of the Credit River, over two thirds of Riverwood Park's 60+ ha will be preserved, to provide and protect the habitat for over 359 species of native plants and 46 species of birds and animals.



the environment by protecting, enhancing, restoring and expanding its Natural Areas System, making use of sustainable green infrastructure, and preserving and protecting trees.

Mississauga will value the environment by:

- promoting public and private community stewardship of the environment for present and future generations;
- protecting, enhancing and restoring Mississauga's Natural Areas System and its functions;
- protection of life and property; and
- promoting a culture of conservation and sustainable healthy lifestyles that minimize the impact on the environment through pollution prevention, public health and safety.

## Complete Communities

Mississauga will contain healthy, vibrant communities that provide residents with a range and diversity of housing types and mobility choices; the ability to engage in healthy, safe and active lifestyles; access to daily needs within close proximity to where they live, work, study, shop, play and congregate; and a sense of belonging and community pride.

Mississauga will complete communities by:

- promoting an urban form and development that supports public health and active living;
- ensuring that communities include or provide easy access to a range of uses and services required to meet all or most of the daily needs of residents through all stages of their lives; e.g., housing, transportation, employment, recreation, social interaction and education;
- promoting expressions of art, culture and design to foster a distinct sense of community identity; and
- preserving the Natural Areas System and cultural resources to retain a connection to the past.



Figure 4-12: Complete communities preserve historic and cultural resources and support artistic expression and individual community designs that promote healthy lifestyles.



## Create a Multi-Modal City

Mississauga will integrate land use and transportation planning and sustainable design so that new development is directed to locations that support existing and planned transit and **active transportation** facilities. The needs of all road users will be considered in the design and management of transportation infrastructure. To ensure that Mississauga is well served by efficient and higher order regional transit services, Mississauga will capitalize on provincial transit service initiatives to minimize dependency on cars for commuting.

Mississauga will create a multi-modal city by:

- developing and promoting an efficient, safe and accessible transportation system for all users;
- promoting a transportation network that connects nodes with a range of transportation modes, to reduce dependency on cars for local trips;
- promoting transit as a priority for moving people;
- implementing a viable and safe **active transportation** network for cyclists and pedestrians of all abilities;
- encouraging transportation demand management (TDM) practices;

- developing priority routes for the efficient movement of goods;
- exploring and promoting opportunities to improve multi-modal connections between the city's transportation network and the Airport to facilitate movement of goods to key markets and border crossings; and
- developing an integrated and seamless network of mobility transportation hubs and **higher order transit** stations.



Figure 4-13: Transit and **active transportation** is a priority for Mississauga's urban vision. (Houston, Texas)

## Build a Desirable Urban Form

Mississauga will promote a strong civic identity and city of experiences that celebrate the attractive and vibrant waterfront, the Downtown and communities, the rich history and cultural diversity, high architectural standards and quality public art. Mississauga will promote green development building standards and technologies.

Mississauga will build a desirable urban form by:

- ensuring that the urban form of the city (e.g., buildings, streets, **streetscapes**, landscapes, public spaces such as parks and squares, infrastructure) contributes positively to everyday living in Mississauga;
- promoting an urban form that will address the live, learn, work and play needs of present and future generations;
- creating vibrant mixed use communities; and
- using placemaking initiatives to support active living and improved public health, comfort and social interaction in the city.

### What is Placemaking?

Placemaking is not just the act of building or fixing up a space, but a whole process that fosters the creation of vital public destinations: the kind of places where people feel a strong stake in their communities and a commitment to making things better. Simply put, Placemaking capitalizes on a local community's assets, inspiration, and potential, ultimately creating good public spaces that promote people's health, happiness, and well-being.



Figure 4-14: Streetsville is a vibrant mixed use community that has a rich history and a strong civic identity.

## Foster a Strong Economy

Mississauga has a progressive and diversified economy. Maintaining its current strength, while further diversifying its base by affording the opportunity for people of all ages and backgrounds to thrive, will be important for its future success. The City will foster innovative and creative businesses by capitalizing on a dynamic downtown, attractive corporate centres and hi-tech infrastructure, and by enabling the efficient movement of goods.



Figure 4-15: Mississauga's educational opportunities are important resources, providing talent to meet the needs of existing and future employers.

Mississauga will foster a strong economy by:

- continuing to support existing businesses and attract a diverse range of new employment opportunities including green businesses, particularly to accommodate those living in Mississauga;
- continuing to be a net importer of talent;
- supporting existing and future office, industrial, institutional and commercial businesses;
- maintaining an adequate supply of employment lands to meet future needs;
- ensuring there is adequate infrastructure to support development;
- promoting new office development in strategic locations; and
- attracting post-secondary educational facilities to stimulate investment and the development of talent to meet the needs of future employment opportunities.



## Promote Collaboration

Mississauga will facilitate participation and collaboration among all sectors - residents, employees, government, business, education and non-profit - by providing opportunities to participate in shaping the city and neighbourhoods. Mississauga is committed to a multidisciplinary decision making process to achieve desirable, meaningful outcomes.

Mississauga will promote collaboration by:

- ensuring that all stakeholders are afforded opportunities to participate in the land use planning process; and
- encouraging participation, collaboration, education and partnerships with stakeholders in the implementation of this Official Plan.

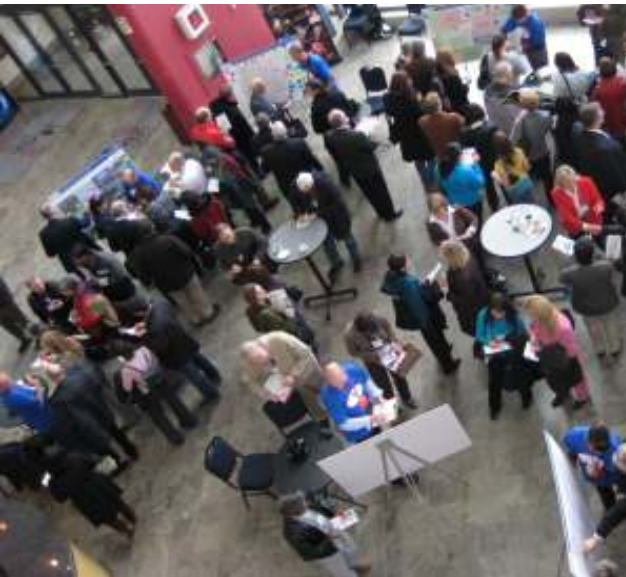


Figure 4-16: As part of the Strategic Plan public engagement process, connections were made with over 100 000 people. The Mississauga Official Plan implements the land use components of the Strategic Plan.

## Conclusion



Figure 4-17: Through its various images, the Mississauga Coat of Arms conveys the past, present and future of a City proud of its growth and confident of its future.

Although there may be some variation to the sequence and approach to the implementation of the Strategic Plan as expressed through Mississauga Official Plan, the city vision and key guiding principles, upon which the Plan is based, will continue to remain intact. Through the sustainable management of growth and land, Mississauga Official Plan will guide the physical evolution of Mississauga where present and future generations will continue to thrive.

# 5 Direct Growth

## 5.1 Introduction

Mississauga's population and employment growth prospects are expected to remain strong over the next 25 years. Mississauga has sufficient land to accommodate projected growth to 2031 and beyond. As Mississauga is now at the end of its greenfield growth phase, new growth will be accommodated through redevelopment and intensification within developed areas. This chapter describes the Urban System that will be used as the framework for determining where population and employment growth will be encouraged and, conversely, those areas of the city that are expected to remain relatively stable. As such, this chapter also indicates where major infrastructure investments will be directed.

The ability to manage change wisely and direct growth to key strategic locations is critical for Mississauga's continued success and prosperity. Mississauga's population and employment growth will be encouraged in areas with existing and proposed service and infrastructure capacity, particularly transit and community infrastructure. Housing and job growth will be balanced and phased

to ensure that required services and amenities keep pace with development. Existing stable neighbourhoods, valuable cultural heritage resources and the Natural Areas System will be protected.

Over time, the city will evolve to include a vibrant Downtown, a number of mixed use Major Nodes and Community Nodes, several prestigious Corporate Centres, stable residential Neighbourhoods and diverse Employment Areas. The Airport and the University of Toronto Mississauga are unique destinations. All these areas will coexist with a healthy system of green spaces and be connected by a network of **Corridors** that support high levels of transit use and mobility options.

Encouraging compact, mixed use development in appropriate locations will provide greater opportunities to live and work in Mississauga and reduce the need for extensive travel to fulfill the needs of day-to-day living. Directing growth to locations with existing or planned higher order or express transit service and enhancing opportunities for walking and cycling will allow for competitive alternatives to vehicular travel, which will minimize impacts on our environment and promote public health.



Figure 5-1: The future plan for the Downtown will help to nurture a vibrant, walkable and compact area, offering a variety of choices and experiences for people. This includes great streets that provide pleasant walking and shopping experiences; restaurants and outdoor cafes; places to congregate such as public squares, urban parks and outdoor markets; entertainment districts and cultural places.

Mississauga will promote future development patterns that are sustainable – those that “meet the needs of the present without compromising the ability of future generations to meet their own needs”. (Brundtland Report, 1987) To achieve this, Mississauga will integrate environmental, land use, urban design and transportation planning objectives.

Where the review of a Character Area, **Corridor** or **Major Transit Station Area** has not been completed within five years of a development application being submitted, or where such a review is underway, a development proponent may be required to prepare a development master plan in support of a development application. A development master plan will be prepared at the discretion of the City and to the City’s satisfaction. A terms of reference that delineates the area of study and identifies matters to be addressed, will be provided by the City. The development master plan will be used to guide all development proposals until such time as the policies for the Character Area, **Corridor** or **Major Transit Station Area** have been reviewed and updated by a local area review.

Mississauga’s Urban System is comprised of the following distinct, yet, interconnected components which collectively serve the needs of those dependent upon them:

- Green System;
- City Structure; and
- **Corridors**.

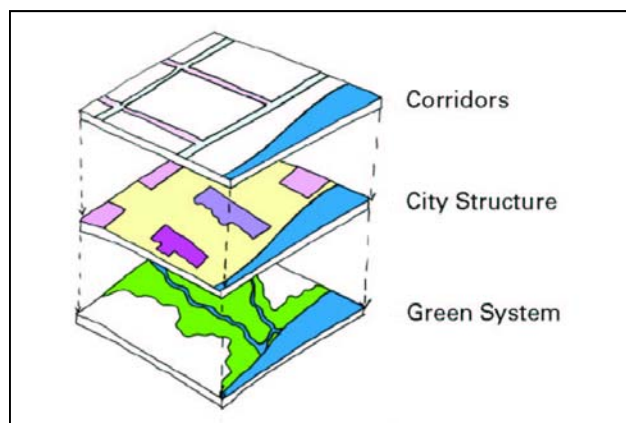


Figure 5-2: Three interrelated layers - Green System, City Structure and Corridors - form the Urban System.

These are shown comprehensively on Schedule 1: Urban System and individually on Schedule 1a: Green System, Schedule 1b: City Structure, and Schedule 1c: Corridors.

5.1.1 The population and employment forecasts for Mississauga are shown in Table 5-1.

**Table 5-1: Population and Employment Forecasts**

Year	Population	Employment
2009	730 000	453 000
2011	738 000	455 000
2021	768 000	500 000
2031	805 000	510 000

5.1.2 Mississauga will ensure that there is adequate land capacity to accommodate population and employment growth to 2031.

5.1.3 Forecast growth will be directed to appropriate locations to ensure that resources and assets are managed in a sustainable manner to:

- a. protect ecological functions, public health and safety;
- b. utilize existing and proposed services and infrastructure such as transit and community infrastructure;
- c. minimize environmental and social impacts;
- d. meet long term needs;
- e. build strong, livable, universally accessible communities; and
- f. promote economic prosperity.

5.1.4 Most of Mississauga’s future growth will be directed to Intensification Areas.

5.1.5 Mississauga will ensure that the City’s natural, environmental, and cultural resources are maintained for present and future generations.

5.1.6 Mississauga encourages compact, mixed use development that is transit supportive, in



appropriate locations, to provide a range of local live/work opportunities.

5.1.7 Mississauga will protect and conserve the character of stable residential Neighbourhoods.

5.1.8 Mississauga will protect employment lands to allow for a diversity of employment uses.

5.1.9 New development will not exceed the capacity of existing and planned engineering services, transit services and community infrastructure. Development proposals may be refused if existing or planned servicing and/or infrastructure are inadequate to support the additional population and employment growth that would be generated or be phased to coordinate with the provision of services and infrastructure.

5.1.10 The population and employment forecasts are premised on the adequacy of services and infrastructure to support growth in the appropriate locations. This includes the Mississauga Bus Rapid Transit corridor and **higher order transit** along Hurontario Street and Dundas Street. If satisfactory arrangements for the implementation of **higher order transit** currently being planned are not made, the population and employment forecasts may be reduced in accordance with the capacity of the transportation system.

## 5.2 Green System

The natural environment sustains all life and is vital to the ecological, economic, social and spiritual well-being of the city. In an urban setting such as Mississauga, the built environment must be integrated with the natural environment in a manner that protects and enhances natural systems and provides the city's inhabitants a multitude of opportunities to connect with nature.

The Green System as shown on Schedule 1a, is composed of:

Shoreline are among the many natural features associated with the city's past and contribute to its

- the Natural System, which includes lands within the Natural Areas System;
- lands subject to Natural Hazards; and
- Parks and Open Spaces. These include lands designated Public and Private Open Space, Greenbelt, Parkway Belt West and open space associated with educational facilities (e.g., school yards) and utilities as shown on Schedule 4: Parks and Open Spaces.

The Green System provides many important functions and services and provides the fundamental necessities of life – clean air, land and water. It provides habitat for trees, flora, fauna and aquatic life; recharges ground water; cleans the air and water and limits the damage that may result from flooding and erosion. The Green System provides opportunities for passive and active recreation, entertainment and social interaction, as well as for respite and appreciation of nature. It plays a role in preserving and enhancing the city's cultural, archaeological and natural heritage for residents, employees and tourists. The waterfront, the Credit River and the former Lake Iroquois



Figure 5-3: The Sixteen Mile Creek is an element within the Green System that is not only naturalized, but provides for recreation including cycling, tennis and a variety of sports activities. The Lisgar Middle School is also located within the Sixteen Mile Creek area.

unique identity. The Green System is essential in creating a beautiful and comfortable city that supports the physical and social well-being of its inhabitants.

The Green System is the first layer of the Urban System. It is essential to building a strong community and a competitive economy and must be considered in all land use and planning decisions. A robust Green System ensures the health of the natural ecosystem and is an essential contributor to quality of life.

5.2.1 Mississauga will establish strategies that protect, enhance and expand the Green System and will include a target for the lands within the city that will be included in the Green System. The City's strategy for protecting, enhancing and restoring the Green System consists of initiatives in the following areas:

- a. establishing an appropriate planning framework in strategic planning documents;
- b. information management and monitoring;
- c. regulation and compliance;
- d. land securement;
- e. stewardship;
- f. promotion and education;
- g. naturalization/restoration; and
- h. management of natural areas.

5.2.2 Mississauga will promote and encourage the restoration of natural forms, functions and linkages.

5.2.3 Mississauga will seek to enhance opportunities for the appreciation and enjoyment of the Green System.

## 5.3 City Structure

The City Structure recognizes that various areas of the city perform different functions. For example, the Downtown contains a mix of uses whereas, in other areas, residential or employment uses predominate. The City Structure organizes the city into functional areas to establish the framework for planning policies that will guide development. Schedule 1b: Urban System - City Structure, identifies the following elements of the City Structure:

- Downtown;
- Major Nodes;
- Community Nodes;
- Corporate Centres;
- Neighbourhoods;
- Employment Areas; and
- Special Purpose Areas.

The various elements of the City Structure will play a unique role in accommodating development. Some will be areas where growth is focused and

### City Structure | Urban Hierarchy

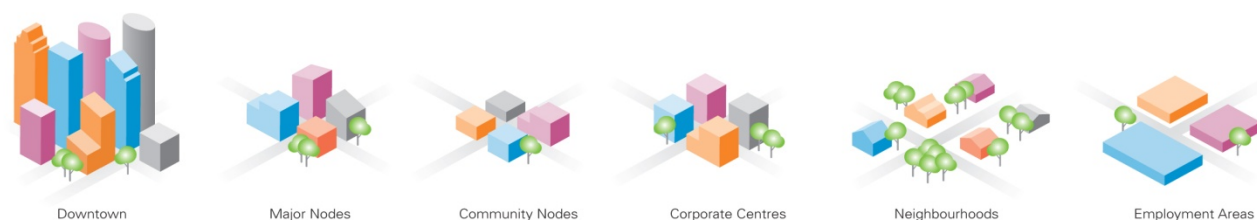


Figure 5-4: To establish a framework for planning policies, the City Structure organizes Mississauga into functional areas. The City Structure is the basis of the urban hierarchy, which provides guidance regarding density, height, uses and appropriate growth.



directed, while others will accommodate some development, but will not be the primary location for future growth. For example, in the Downtown, major new development is expected in conjunction with transit investment while Neighbourhoods will experience little change.

The City Structure is the basis of the following urban hierarchy:

- The Downtown will contain the highest densities, tallest buildings and greatest mix of uses;
- Major Nodes will provide for a mix of population and employment uses at densities and heights less than the Downtown, but greater than elsewhere in the city;
- Community Nodes will provide for a similar mix of uses as in Major Nodes, but with lower densities and heights;

- Corporate Centres will provide for employment uses at densities and heights similar to Major Nodes or Community Nodes;
- Neighbourhoods and Employment Areas will accommodate the lowest densities and building heights. Neighbourhoods will focus on residential uses and associated services and facilities. Employment Areas will accommodate a diverse mix of employment uses, but will not permit residential uses; and
- Special Purpose Areas are unique areas of the city. Densities, building heights and mix of uses will relate to the unique role these areas play within the city.

Any proposed changes to the urban hierarchy will not be permitted unless considered through a municipal comprehensive review.

Location	Height*		Density Range (residents and jobs combined per gross hectare)	Population to Employment Ratio
	Minimum	Maximum		
Downtown	3	Not specified	200 by 2031; strive for 300 to 400	1:1
Major Nodes	2	25	200 to 300	2:1 to 1:2
Community Nodes	2	4	100 to 200	2:1 to 1:2
Corporate Centres	2 along Corridors and in Major Transit Station Areas	Not specified	—	—
Neighbourhoods		4	—	—
Employment Areas	2 in Major Transit Station Areas and Intensification Corridors	Not specified	—	—
Intensification Corridors	2	As per City Structure Element	—	—
Corridors	2 except in Employment Areas	As per City Structure Element	—	—
Major Transit Station Areas	2	As per City Structure Element	—	—
Designated Greenfield Area	—	—	Minimum 50	—

\*Figure 5-5: Height, Density and Population to Employment Ratio Requirements.

### 5.3.1 Downtown



Figure 5-6: The Downtown will be the focus for the city as a whole with the highest density, tallest buildings and greatest mix of uses. The Downtown will be the centre of civic, cultural and entertainment facilities for the city.

The Downtown represents a unique area within the City Structure. Much of the city's new population and employment growth will locate in the Downtown. It is the civic and cultural centre of the city and a destination within the Greater Toronto and Hamilton Area. The Downtown will be connected to key regional and city destinations by an efficient local network of transportation and transit corridors and regional **higher order transit** services.

The Downtown will be a vibrant city and regional centre where residents are able to live, work and play. It is also where employees, shoppers, tourists, theatergoers and students gather in a mixed use environment, where development is pedestrian friendly, the public realm is inviting and the street network is supportive of walking and cycling as viable and convenient forms of transportation. Opportunities to enjoy nature in a variety of urban open spaces that include trees and other natural elements will be provided. Major infrastructure investments including **higher order transit** and community and cultural facilities will be encouraged in the Downtown.

The Downtown is Mississauga's Urban Growth Centre as identified in the Provincial Growth Plan.

5.3.1.1 The Downtown is comprised of the lands along Hurontario Street between Highway 403 and the Queen Elizabeth Way, as identified on Schedule 1b: Urban System – City Structure and Schedule 9: Character Areas.

5.3.1.2 The Downtown will be divided into four Character Areas:

- a. Downtown Core;
- b. Downtown Fairview;
- c. Downtown Cooksville; and
- d. Downtown Hospital.

5.3.1.3 The Downtown is an Intensification Area.

5.3.1.4 The Downtown will achieve a minimum gross density of 200 residents and jobs combined per hectare by 2031. The City will strive to achieve a gross density of between 300 to 400 residents and jobs combined per hectare in the Downtown.

5.3.1.5 Development applications within the Downtown proposing a change to the designated land use, which results in a significant reduction in the number of residents or jobs that could be accommodated on the site, will not be permitted unless considered through a municipal comprehensive review.

5.3.1.6 The Downtown will achieve an average population to employment ratio of 1:1, measured as an average across the entire Downtown.

5.3.1.7 Local area plans will determine how the density and population to employment targets will be achieved within the Downtown.

5.3.1.8 The Downtown will support opportunities for residents to work in Mississauga.

5.3.1.9 The Downtown will develop as a major regional centre and the primary location for mixed use development. The Downtown will contain the greatest concentration of activities and variety of uses.

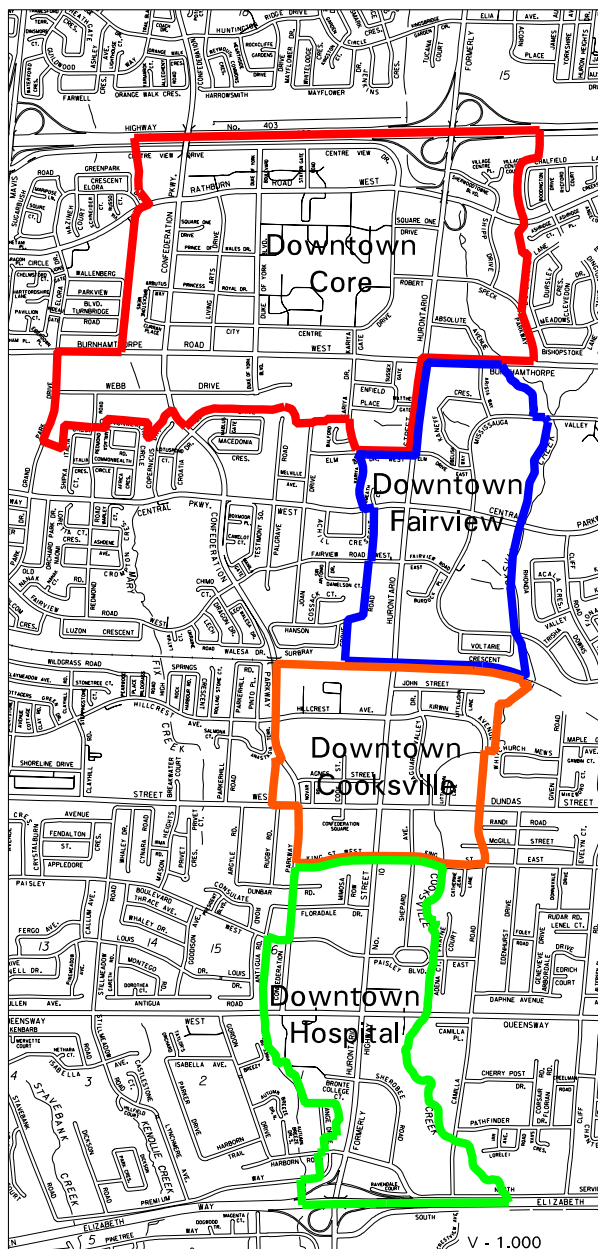


Figure 5-7: The Downtown is made up of four Character Areas: Downtown Core, Downtown Fairview, Downtown Cooksville and Downtown Hospital.

5.3.1.10 The Downtown will be planned as a focal area for investment in community infrastructure, as well as institutional, commercial, recreational, educational, cultural and entertainment uses.

5.3.1.11 Development in the Downtown will be in a form and density that achieves a high quality urban environment.

5.3.1.12 The Downtown will be served by frequent transit services, including **higher order transit** facilities, which provide connections to all parts of the city and to neighbouring municipalities.

5.3.1.13 The Downtown will be developed to support and encourage **active transportation** as a mode of transportation.

## 5.3.2 Major Nodes

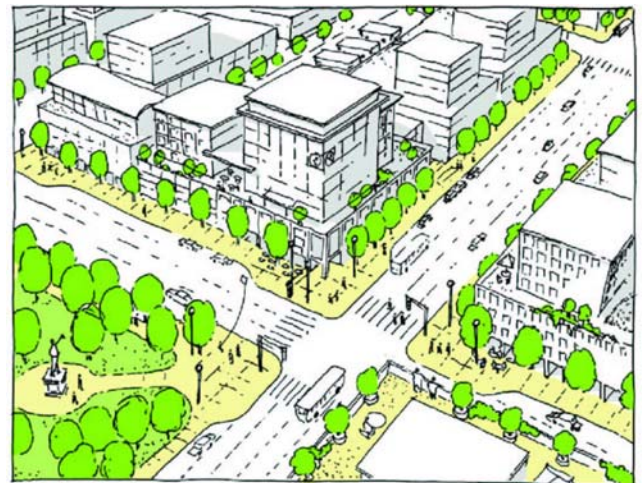


Figure 5-8: Major Nodes will develop as prominent centres with a regional and city focus, and be served by **higher order transit**. Major Nodes will provide a mix of uses including employment, commercial, residential, educational and open space.

Major Nodes will be planned as prominent centres of mixed use activity with a variety of employment opportunities, such as office and institutional jobs and regional shopping services that draw people beyond the adjacent neighbourhoods. It is also anticipated that Major Nodes will provide a variety of higher density housing for people throughout the different phases of their lifecycle and for a variety of income groups.

Major Nodes have considerable existing or planned capacity to accommodate both residential and employment uses and as such could have a positive effect on travel demand. They are located close to planned **higher order transit** – the Uptown Node is located directly on the proposed **higher order transit** facility on the Hurontario Street Corridor and



the Central Erin Mills node is located in proximity to the Bus Rapid Transit corridor.

5.3.2.1 There are two Major Nodes in Mississauga:

- a. Central Erin Mills, and
- b. Uptown.

5.3.2.2 Local area plans will confirm or determine detailed boundaries for Major Nodes.

5.3.2.3 Major Nodes are Intensification Areas.

5.3.2.4 The Major Nodes will achieve a gross density of between 200 and 300 residents and jobs combined per hectare.

5.3.2.5 Development applications within a Major Node proposing a change to the designated land use which results in a significant reduction in the number of residents or jobs that could be accommodated on the site, will not be permitted unless considered through a municipal comprehensive review.

5.3.2.6 Major Nodes will achieve an average population to employment ratio of between 2:1 to 1:2, measured as an average across the entire area of each node.

5.3.2.7 Local area plans will determine how the density and population to employment targets will be achieved within Major Nodes.

5.3.2.8 Major Nodes will develop as city and regional centres and be a primary location for mixed use development.

5.3.2.9 Investments in community infrastructure, as well as commercial, recreational, educational, cultural and entertainment uses, will be encouraged in Major Nodes.

5.3.2.10 Major Nodes are intended to serve as older adult clusters where community infrastructure, services, and programs to serve the needs of older adults will be directed.

5.3.2.11 Development in Major Nodes will be in a form and density that achieves a high quality urban environment.

5.3.2.12 Major Nodes will be served by frequent transit services, including **higher order transit** facilities, which provide connections to destinations within the city and to neighbouring municipalities.

5.3.2.13 Major Nodes will be developed to support and encourage **active transportation** as a mode of transportation.

### 5.3.3 Community Nodes

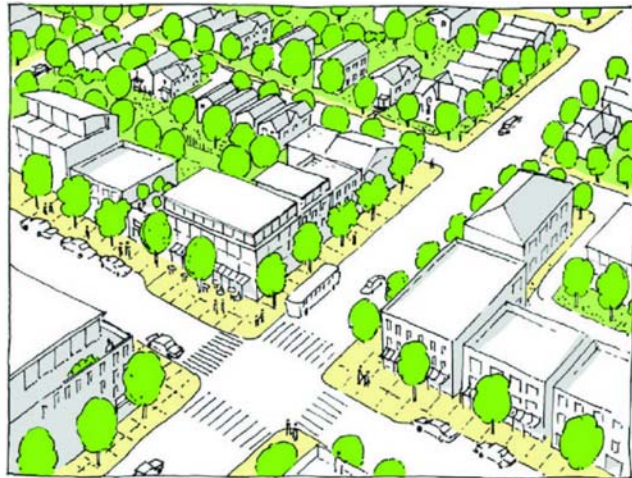


Figure 5-9: Community Nodes will be the focus of a mix of uses including commercial, residential, educational and open spaces.

Community Nodes provide access to a multitude of uses that are required for daily living – local shops and restaurants, **community facilities**, cultural, heritage and entertainment uses, schools, parks, open space as well as a diverse housing stock that meets housing needs of the adjacent population as they move through their lifecycle.

Community Nodes such as Port Credit and Streetsville already exhibit many of the desirable characteristics of an established Community Node – compact, mixed use development, pleasant, walkable streets and a strong sense of place and community identity.

Although developed around indoor shopping malls, other Community Nodes contain a variety of community infrastructure such as, recreational facilities, libraries, police stations and places of religious assembly. These Nodes provide valued

community benefits, but often in a form that is characterized by large blocks, surface parking, and single storey buildings with an internal focus. As these Community Nodes redevelop they will be expected to take on a more urban, pedestrian friendly form.

The Dixie-Dundas Node and Lakeview Node are emerging Nodes that have a commercial base but require new community infrastructure and a more pedestrian friendly approach to development.

5.3.3.1 There are 10 Community Nodes in Mississauga:

- a. Clarkson Village;
- b. Dixie-Dundas;
- c. Lakeview;
- d. Malton;
- e. Meadowvale;
- f. Port Credit;
- g. Rathwood/Applewood;
- h. Sheridan;
- i. South Common; and
- j. Streetsville.

5.3.3.2 Local area plans will confirm or determine detailed boundaries for Community Nodes.

5.3.3.3 Community Nodes are Intensification Areas.

5.3.3.4 Community Nodes will achieve a gross density of between 100 and 200 residents and jobs combined per hectare.

5.3.3.5 Development applications within a Community Node proposing a change to the designated land which results in a significant

reduction in the number of residents or jobs that could be accommodated on the site, will not be permitted unless considered through a municipal comprehensive review.

5.3.3.6 Community Nodes will achieve an average population to employment ratio between 2:1 to 1:2, measured as an average across the entire area of each node.

5.3.3.7 Local area plans will determine how the density and population to employment targets will be achieved within Community Nodes.

5.3.3.8 Community Nodes will develop as centres for surrounding Neighbourhoods and be a location for mixed use development.

5.3.3.9 Investments in community infrastructure, as well as commercial, recreational, educational, cultural and entertainment uses, will be encouraged in Community Nodes.

5.3.3.10 Community Nodes are intended to serve as older adult clusters where community infrastructure, services, and programs to serve the needs of older adults will be directed.

5.3.3.11 Development in Community Nodes will be in a form and density that complements the existing character of historical Nodes or that achieves a high quality urban environment within more recently developed Nodes.

5.3.3.12 Community Nodes will be served by frequent transit services that provide city wide connections. Some Community Nodes will also be served by **higher order transit** facilities, which provide connections to neighbouring municipalities.

5.3.3.13 Community Nodes will be developed to support and encourage **active transportation** as a mode of transportation.

### 5.3.4 Corporate Centres

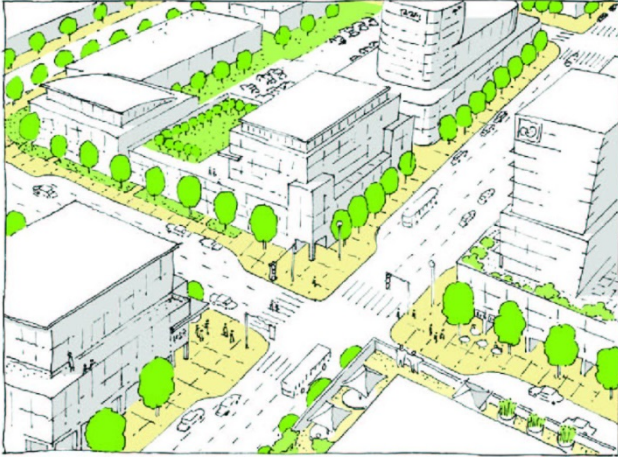


Figure 5-10: Corporate Centres will be the focus for major office uses with an emphasis on high quality architecture and urban design.

Corporate Centres represent major employment concentrations outside of the Downtown. Corporate Centres contain a mix of high density employment uses with a focus on major office development. Corporate Centres are also where many prestigious research and manufacturing businesses are found, often in facilities exhibiting high architectural and urban design standards. Interior locations of Corporate Centres may include a broader, lower density mix of employment uses.

5.3.4.1 There are four Corporate Centres in Mississauga:

- a. Airport Corporate;
- b. Gateway Corporate;
- c. Meadowvale Business Park; and
- d. Sheridan Park.

5.3.4.2 Local area plans will confirm or determine detailed boundaries for Corporate Centres.

5.3.4.3 Corporate Centres are Intensification Areas.

5.3.4.4 Corporate Centres will include a mix of higher density employment uses. Residential uses and new **major retail** developments will not be permitted in Corporate Centres.

5.3.4.5 Corporate Centres will support opportunities for the resident labour force to work in Mississauga.

5.3.4.6 Conversion of lands within Corporate Centres to non-employment uses will only be permitted through a municipal comprehensive review.

5.3.4.7 Local area plans will address the mix of business uses and density requirements within each Corporate Centre. These Plans may result in the establishment of minimum employment and building densities, building heights, urban design standards or transportation policies, among other matters.

5.3.4.8 Corporate Centres will be planned to achieve compact transit supportive development at greater employment densities, particularly near **higher order transit** stations.

5.3.4.9 Land uses permitted by this Plan that support commuter needs and support the use of nearby **higher order transit** facilities in off-peak travel times will be encouraged.

5.3.4.10 Development will be required to create an attractive public realm and provision of community infrastructure, transportation infrastructure, and other services required to support employees.



### 5.3.5 Neighbourhoods



Figure 5-11: Stable Neighbourhoods will remain intact, while intensification will generally be directed to **Corridors**.

Mississauga has a variety of Neighbourhoods reflecting the various stages of its development. Some of these Neighbourhoods surround historic centres that date back to the 1800s, however, most of Mississauga's Neighbourhoods were built in the latter decades of the 20th century. As such, most of Mississauga's housing stock is quite new and in a good state of repair.

Neighbourhoods are characterized as physically stable areas with a character that is to be protected. Therefore, Mississauga's Neighbourhoods are not appropriate areas for significant intensification. This does not mean that they will remain static or that new development must imitate previous development patterns, but rather that when development does occur it should be sensitive to the Neighbourhood's existing and planned character.

Some community infrastructure will be provided within Neighbourhoods, however, most services for day-to-day living will be provided in Major Nodes or Community Nodes in close proximity to Neighbourhoods.

5.3.5.1 Neighbourhoods will not be the focus for intensification and should be regarded as stable residential areas where the existing character is to be preserved.

5.3.5.2 Residential intensification within Neighbourhoods will generally occur through infilling and the development of existing commercial sites as mixed use areas.

5.3.5.3 Where higher density uses are proposed, they should be located on sites identified by a local area plan, along **Corridors** or in conjunction with existing apartment sites or commercial centres.

5.3.5.4 Intensification of commercial sites that results in a significant loss of commercial floor space will be discouraged.

5.3.5.5 Intensification within Neighbourhoods may be considered where the proposed development is compatible in built form and scale to surrounding development, enhances the existing or planned development and is consistent with the policies of this Plan.

5.3.5.6 Development will be sensitive to the existing and planned context and will include appropriate transitions in use, built form, density and scale.

5.3.5.7 Transportation planning within Neighbourhoods will give priority to **active transportation** modes.

### 5.3.6 Employment Areas



Figure 5-12: Employment Areas are characterized by businesses that are land extensive and/or have low employment densities.

Employment Areas are stable areas containing diverse industrial and business employment operations. Some uses require extensive land resources and have low employment densities. Examples include logistics, warehousing and storage yards. Employment Areas also include small and mid-size office developments either in stand-alone buildings or in conjunction with other uses. In key locations, such as those with excellent transportation services, large office developments will be found. Retail, service or infrastructure uses that support the employment base are also located in Employment Areas.

In order to protect lands for employment uses, uses intended to serve the city's residential population will be discouraged from locating in Employment Areas. Exceptions may be made for residential serving uses that would create land use conflicts with residential uses or are uses identified as beneficial to the City that cannot be accommodated in residential areas. Some examples are large scale sporting venues, places of religious assembly, or colleges and universities.

Many businesses within Employment Areas rely on the delivery of goods and services by truck. As such, goods movement infrastructure within Employment

Areas is necessary in order for businesses to remain competitive within the regional economy.

5.3.6.1 Mississauga will maintain an adequate supply of lands for a variety of employment uses to accommodate existing and future employment needs.

5.3.6.2 Mississauga will maintain a sustainable, diversified employment base by providing opportunities for a range of economic activities.

5.3.6.3 Employment uses that support opportunities for residents to work in Mississauga will be encouraged.

5.3.6.4 Mississauga will provide the necessary infrastructure to support existing and planned employment uses.

5.3.6.5 Conversion of lands within Employment Areas will only be permitted through a municipal comprehensive review.

5.3.6.6 Infrastructure in Employment Areas will be planned to support land uses with a goods movement focus.

5.3.6.7 Where feasible and appropriate, development will be encouraged to be transit supportive and minimize surface parking.

5.3.6.8 Land uses serving the residential population of the city will be discouraged and only permitted where a use is beneficial to the city and cannot locate in a residential area due to land use conflicts or the unavailability of a suitable site. Acceptable locations for these uses will be identified through a municipal comprehensive review and local area plans.

5.3.6.9 Higher density employment uses, such as office, will be required within **Major Transit Station Areas**.

5.3.6.10 Additional development within Employment Areas will be permitted where the proposed development is compatible in built form and scale to surrounding development, enhances the existing or

planned development and is consistent with the policies of this Plan.”

### 5.3.7 Special Purpose Areas

Special purpose areas are unique destinations of city and regional significance. They have some or all of the following characteristics:

- specialized land uses and planning needs;
- serve a large number of users;
- substantial transportation demands;
- play a significant role in the economy;
- impact surrounding land uses and land use patterns;
- generate significant employment;
- specialized infrastructure needs; and
- are land extensive.

To date, two special purpose areas have been identified. The City may identify additional special purpose areas as they emerge.

#### 5.3.7.1 Special Purpose Areas are:

- a. the University of Toronto Mississauga (UTM); and
- b. the Airport.

5.3.7.2 The City may, through amendment of this Plan, add Special Purpose Areas.

5.3.7.3 Planning studies will be prepared for Special Purpose Areas as needed.

5.3.7.4 Infrastructure to meet the needs of Special Purpose Areas will be a priority.

5.3.7.5 The City will collaborate with UTM, the Airport and proponents of new Special Purpose Areas, as appropriate.



Figure 5-13: The Airport moves large volumes of people and freight on a daily basis.



Figure 5-14: UTM is a valued post-secondary educational institution located in Mississauga.



## 5.4 Corridors

Mississauga was planned with a grid of arterials, which have historically served as the chief conduits for moving cars and freight. This grid now forms the basis of a system of **Corridors**.

**Corridors** connect various elements of the city to each other. Over time, many of these **Corridors** will evolve and accommodate multi-modal transportation and become attractive public places in their own right with complementary land uses. **Corridors** are important elements of the public realm, as they link communities and are locations where people experience the city on a day-to-day basis.

Some **Corridors** have been identified as appropriate locations for intensification. Additional policies have been developed for **Intensification Corridors** to recognize their development potential.



Figure 5-15: **Corridors** connect the city and link communities. They are where people experience the city on a day-to-day basis and over time will accommodate multi-modal transportation facilities. Dundas Street and Hurontario Street have been identified as areas where growth will be directed.

5.4.1 A **Corridor** is generally comprised of the road right-of-way as well as the lands on either side of the road. The **Corridors** are shown conceptually on Schedule 1c: Urban System - Corridors.

5.4.2 Where **Corridors** run through or when one side abuts the Downtown, Major Nodes, Community Nodes and Corporate Centres, development in those segments will also be subject to the policies of the City Structure element in which they are located. Where there is a conflict, the policies of the Downtown, Major Nodes, Community Nodes and Corporate Centres will take precedence.

5.4.3 **Corridors** that run through or abut the Downtown, Major Nodes, Community Nodes and Corporate Centres are encouraged to develop with mixed uses oriented towards the **Corridor**.

5.4.4 Development on **Corridors** should be compact, mixed use and transit friendly and appropriate to the context of the surrounding Neighbourhood and Employment Area.

5.4.5 Where higher density uses within Neighbourhoods are directed to **Corridors**, development will be required to have regard for the character of the Neighbourhoods and provide appropriate transitions in height, built form and density to the surrounding lands.

5.4.6 Local area plans will review land use and design policies for **Corridors** and may delineate the boundaries of **Corridors**.

5.4.7 Land uses and building entrances will be oriented to the **Corridor** where possible and surrounding land use development patterns permit.

5.4.8 **Corridors** will be subject to a minimum building height of two storeys and the maximum building height specified in the City Structure element in which it is located, unless Character Area policies specify alternative building height requirements or until such time as alternative building heights are determined through planning studies. Except along **Intensification Corridors** and within **Major Transit Station Areas**, the minimum

building height requirement will not apply to Employment Areas.

5.4.9 Transit services infrastructure will utilize **Corridors** to connect Intensification Areas.

5.4.10 Local area plans will consider the appropriateness of transit supportive uses at the intersection of two **Corridors**. Local area plans may permit additional heights and densities at these locations provided that the development reduces the dependency on cars and supports the policies of this Plan.

5.4.11 Hurontario Street and Dundas Street have been identified as **Intensification Corridors**. These are Intensification Areas. Additional **Intensification Corridors** may be identified in the future.

5.4.12 Not all segments of **Intensification Corridors** are appropriate for intensification. Planning studies for **Intensification Corridors** will identify appropriate locations for intensification and the appropriate densities, land uses and building heights.

5.4.13 Low density residential development will be discouraged from locating within **Intensification Corridors**.

## 5.5 Intensification Areas

Future growth will primarily be directed to Intensification Areas. Other areas of the city, such as Neighbourhoods, will receive modest additional growth in keeping with established land use patterns and their existing or planned character. Employment Areas are expected to continue to build out with a similar mix and density of uses as currently exists.

Intensification Areas will be attractive mixed use areas, developed at densities that are sufficiently high to support frequent transit service and a variety of services and amenities. It is expected that more efficient use of land within Intensification Areas will occur as single storey buildings and surface parking lots are replaced with multistorey developments and structured parking facilities.

More than three-quarters of the city's growth in population and employment to 2031 will be accommodated in Intensification Areas. Considerable development capacity exists within Intensification Areas to accommodate growth to 2031 and beyond.

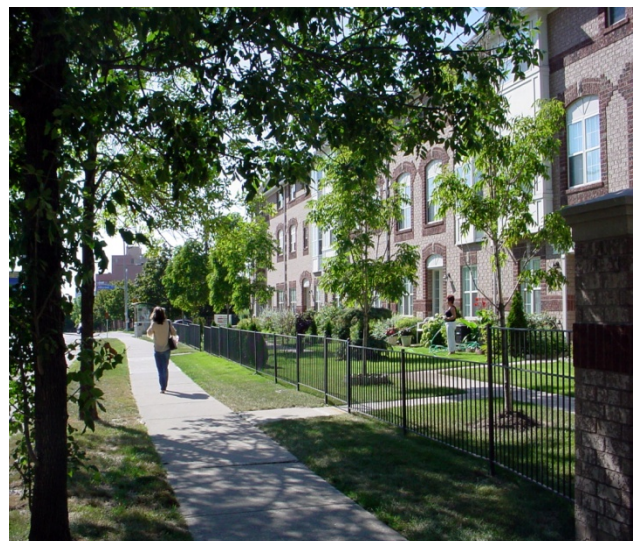


Figure 5-16: There are many forms of intensification. Sometimes intensification can occur in the form of high rise buildings, most commonly found in the Downtown, however, in other areas of the city, the most appropriate form of intensification may occur in lower density forms, to be consistent with the character of surrounding areas.



5.5.1 The focus for intensification will be Intensification Areas, which are the Downtown, Major Nodes, Community Nodes, Corporate Centres, **Intensification Corridors** and **Major Transit Station Areas**, as shown on Schedule 2: Intensification Areas.

5.5.2 Local area plans for the Downtown, Major Nodes, Community Nodes and Corporate Centres will determine appropriate locations for intensification within these areas.

5.5.3 Planning studies will delineate the boundaries of **Intensification Corridors** and **Major Transit Station Areas** and identify appropriate densities, land uses and building heights.

5.5.4 Intensification Areas will be planned to reflect their role in the City Structure hierarchy.

5.5.5 Development will promote the qualities of complete communities.



Figure 5-17: In the future, nodes will be a focal point for intensification. These areas will not only serve those living within the nodes, but also those living in surrounding Neighbourhoods and will provide a pedestrian friendly environment with compact, mixed use developments and mainstreets. (Streetsville Community Node)

5.5.6 Development applications within Intensification Areas proposing a change to the designated land use, which results in a significant reduction in the number of residents or jobs that could be accommodated on the site, will not be permitted unless considered through a municipal comprehensive review.

5.5.7 A mix of medium and high density housing, community infrastructure, employment, and commercial uses, including mixed use residential/commercial buildings and offices will be encouraged. However, not all of these uses will be permitted in all areas.

5.5.8 Residential and employment density should be sufficiently high to support transit usage. Low density development will be discouraged.

5.5.9 Intensification Areas will be planned to maximize the use of existing and planned infrastructure.

5.5.10 Major office development will be encouraged to locate within the Downtown, Major Nodes, Corporate Centres, **Intensification Corridors** and **Major Transit Station Areas**. **Secondary office** development will be encouraged within Community Nodes.

5.5.11 Where there is a conflict between the Intensification Area policies and policies regarding the Natural Areas System and heritage resources, the policies of the Natural Areas System and heritage resources will take precedence.

5.5.12 Development will be phased in accordance with the provision of community infrastructure and other infrastructure.

5.5.13 **Major Transit Station Areas** will be subject to a minimum building height of two storeys and a maximum building height specified in the City Structure element in which it is located, unless Character Area policies specify alternative building height requirements or until such time as alternative building heights are determined through planning studies.



Figure 5-18: Underutilized sites with surface parking areas and single storey buildings have considerable development capacity. As these sites redevelop, the opportunity exists to create a finer grained road network and introduce sustainable design elements, as well as a broader mix of uses. The intensification of these sites may be a gradual process that takes place over a number of years.

5.5.14 Pedestrian movement and access from major transit routes will be a priority in Intensification Areas

5.5.15 Intensification Areas will be served by transportation Corridors containing transit and active transportation and may contain higher order transit facilities.

5.5.16 **Major Transit Station Areas** will be planned and designed to provide access from various transportation modes to the transit facility, including consideration of pedestrians, bicycle parking and commuter pick-up/drop-off areas.

## 5.6 Designated Greenfield Area

There are lands in the Churchill Meadows Neighbourhood Character Area that are identified as a designated greenfield area pursuant to the Growth Plan for the Greater Golden Horseshoe.

5.6.1 Character Area policies may specify alternative density requirements, provided the total designated greenfield area will achieve a minimum density of 50 residents and jobs combined per hectare, excluding permitted environmental take outs.



# 6 Value the Environment

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## 6.1 Introduction

Mississauga is located on the shore of Lake Ontario, part of the largest system of freshwater lakes in the world. Mississauga contains watersheds of the Credit River, Etobicoke Creek and other **watercourses** that form part of the Great Lakes drainage basin. Mississauga is partially within the Carolinian Forest Ecoregion, Canada's most biologically diverse ecological region. This region contains Canada's most rare and endangered plants and animals, and is the most threatened ecological region in Ontario.

The City's Strategic Plan identifies "Living Green" as one of the five pillars of the strategic vision for

the city. Living Green involves implementing measures that are sensitive to, and complement, the natural environment. As the city continues to grow, it is imperative that growth does not compromise the natural environment, including the climate. The health of the natural environment is critical to human and economic vitality and the overall well-being of society. It provides the fundamental necessities of life – clean air, land and water – and is an essential component of the fabric and character of communities. Further, climate change affects land use policies and transportation choices that can contribute to improving the quality of the environment and lead to developing a sustainable city. These policies are the subject of this chapter.



Figure 6-1: As an environmentally responsible community, Mississauga is committed to environmental protection, conducting its corporate operations in an environmentally responsible manner and promoting awareness of environmental policies, issues and initiatives. Residents and businesses have a large role to play to help protect and enhance the land, air, water and energy resources that are enjoyed by all in the city. (Credit River Valley)



Promoting transit as a form of transportation supported by transit supportive uses, which employ compact design principles, will assist in addressing the issues that are negatively impacting the environment. Other chapters of this Plan address these matters and support the Living Green pillar of the Strategic Plan.

There are many opportunities for all lands within the city to contribute to the health of the natural environment. The Green System in Mississauga, consisting of the Natural Areas System, **Natural Hazard Lands** and Parks and Open Spaces contribute to a valuable natural environment in the city. These areas provide habitats for flora and fauna to thrive and locations for residents, employees and visitors to recreate and enjoy nature. The Urban Forest, comprising trees on public and private properties in the city, also contributes to a healthy and sustainable city, and should be protected and enhanced where possible.



Figure 6-2: Mississauga's Natural Areas and their ecological functions will be preserved and enhanced, and natural resources managed wisely, so that current and future generations enjoy a healthy and safe environment.

Water, air and land are essential elements of the environment affected by human activity. Issues such as stormwater, air quality, **contaminated sites**, noise and **waste** generation have a significant impact on the environment and require mitigation and management to reduce their impacts. Sustainably managing land means directing growth to protect and enhance the natural environment, maximize public benefit and contribute to the economy. It means that development is integrated into the community, while negative impacts to the Green System, the Urban Forest, ecological processes and biological diversity are avoided. It also means protecting, enhancing and, where possible, restoring the Natural Areas System.

The rehabilitation and development of brownfield sites presents an opportunity to remediate existing contamination and provide opportunities for community improvement. The generation of **waste** and how it is managed is another critical factor in creating a healthy environment. Noise is a common occurrence in an urban environment. Traffic and aircraft noise as well as noise generated by various land use activities needs to be managed and mitigated in order to create a comfortable living and working environment.

6.1.1 Mississauga will:

- a. protect, enhance and restore the Natural Areas System;
- b. protect life and property from natural and human made hazards;
- c. promote pollution prevention, reduction of natural resource consumption and increased use of **renewable energy**; and
- d. ensure land use compatibility.

6.1.2 Mississauga will promote an **ecosystem approach** to planning.

6.1.3 Mississauga will protect the quality and integrity of its air, land, water and biota for current and future generations.

6.1.4 Mississauga will promote pollution prevention in order to help protect the quality of the air, land and water.

6.1.5 Mississauga will promote education, awareness, community involvement and commitment to community stewardship for the protection and enhancement of the environment.

6.1.6 Mississauga will work with other jurisdictions and levels of government and encourage and support partnerships among the City, industries, businesses and the community to improve air quality, protect and enhance the natural environment, reduce energy use and manage **waste**.

6.1.7 Mississauga will work with other jurisdictions and levels of government, industries, businesses and the community to address climate change mitigation and adaptation.

6.1.8 Sensitive land uses will not be permitted adjacent to existing major facilities such as the airport, transportation corridors, wastewater treatment plants, **waste** sites and industrial and aggregate activities, if adverse effects from these facilities cannot be mitigated.

6.1.9 Sensitive land uses may be considered in proximity to major facilities such as the Airport, transportation corridors, wastewater treatment plants, **waste** sites, industries and aggregate activities only where effective control is provided through appropriate site and building design, buffers and/or separation distances to prevent adverse effects from these facilities.

6.1.10 In accordance with the Provincial Government guidelines, the development proponent will be required to undertake a feasibility study in those cases where:

- a. a sensitive land use is proposed within the area of influence of a facility that generates contaminant discharges; or
- b. facility generates contaminated discharges or a proposed facility is likely to generate

contaminated discharges. The study will evaluate the impacts, both before and after any proposed mitigation measures are applied and identify options for mitigation both at the source or elsewhere to the satisfaction of the City and other appropriate approval authorities.

## 6.2 Living Green

To create a sustainable environment, everyone should aspire to “live green”. The integration of green development techniques contribute to the environment in a variety of ways. For example, landscaped areas can be naturalized, trees can be planted, stormwater can be managed on-site and green roofs can be constructed.

Climate change is a daunting issue that requires the collective actions of many. While no individual development or municipality can solve the issue of climate change, it is necessary to consider the environmental impacts of every development proposal and planning decision, and mitigation measures to avoid environmental harm and adapt to changing environmental conditions.

Other chapters of this Plan address creating an urban structure that directs growth to Intensification Areas where compact, mixed use areas will be supported by transit and where walking and cycling will be viable modes of transportation. This is essential to creating an environmentally sustainable city.



Figure 6-3: Naturalized landscaping with native, non-invasive plants species in the city's employment areas benefits the environment in many ways, such as improving air quality, reducing water consumption and pesticide use, and providing habitat for birds and insects.



This Plan also contains policies regarding the Natural Areas System. In addition to preserving and enhancing Natural Areas, **stormwater best management practices** for new development can also be employed. Use of green development standards such as Leadership in Energy & Environmental Design (LEED), Green Globes or other customized standards can do much to ensure that new development or existing development is environmentally sustainable.

Individual sites and portions of the public realm can contribute to the health of the environment by incorporating measures such as:

- orienting buildings to be “solar ready” to take advantage of passive heating and cooling;
- connecting to district energy systems;
- using **renewable energy** sources such as solar or geothermal energy;
- managing stormwater runoff using **stormwater best management practices**;
- naturalizing landscapes with native, non-invasive species;
- planting trees;
- installing green roofs or white roofs;
- supporting urban agriculture;
- preventing and reducing pollution; and
- considering the impact of development on sensitive land uses.

6.2.1 Mississauga will strive to be a leader in sustainable development to mitigate, manage and adapt to the impacts of climate change.

6.2.2 Mississauga will build communities that are environmentally sustainable and encourage sustainable ways of living.

6.2.3 Mississauga will develop a green development strategy to enhance environmental sustainability.

6.2.4 Mississauga may develop incentive programs to encourage green development.

6.2.5 Mississauga encourages the retrofitting of existing buildings and developed sites to be more environmentally sustainable.

6.2.6 Mississauga will encourage naturalized landscaped areas adjacent to Natural Areas using native, non-invasive species.

6.2.7 Mississauga will require development proposals to address the management of stormwater using **stormwater best management practices**.

6.2.8 Mississauga will encourage the use of green technologies and design to assist in minimizing the impacts of development on the health of the environment.

6.2.9 Pollution concerns may affect land, water and air quality. Mississauga will support other levels of government in their efforts to monitor land, water and air quality and where feasible, to establish programs to screen proposals for their impacts in this regard.

6.2.10 Mississauga will support and encourage initiatives and pollution prevention programs to prevent and reduce the causes and impacts of pollution.

6.2.11 A Pollution Prevention Plan must be undertaken for development, which has the potential to generate pollutant discharges to a storm sewer system or to a water body prior to approval. The plan must consider the use of processes, practices, materials or technology that avoids or minimizes the creation of pollutant discharges to a storm sewer system or to a water body. The implementation of the recommended measures will be conditions of approval.

6.2.12 Mississauga will encourage tree planting on public and private lands allowing for an increase in the Urban Forest canopy.

## 6.3 Green System

The Green System makes up almost 23 per cent of total land cover in Mississauga and is composed of:

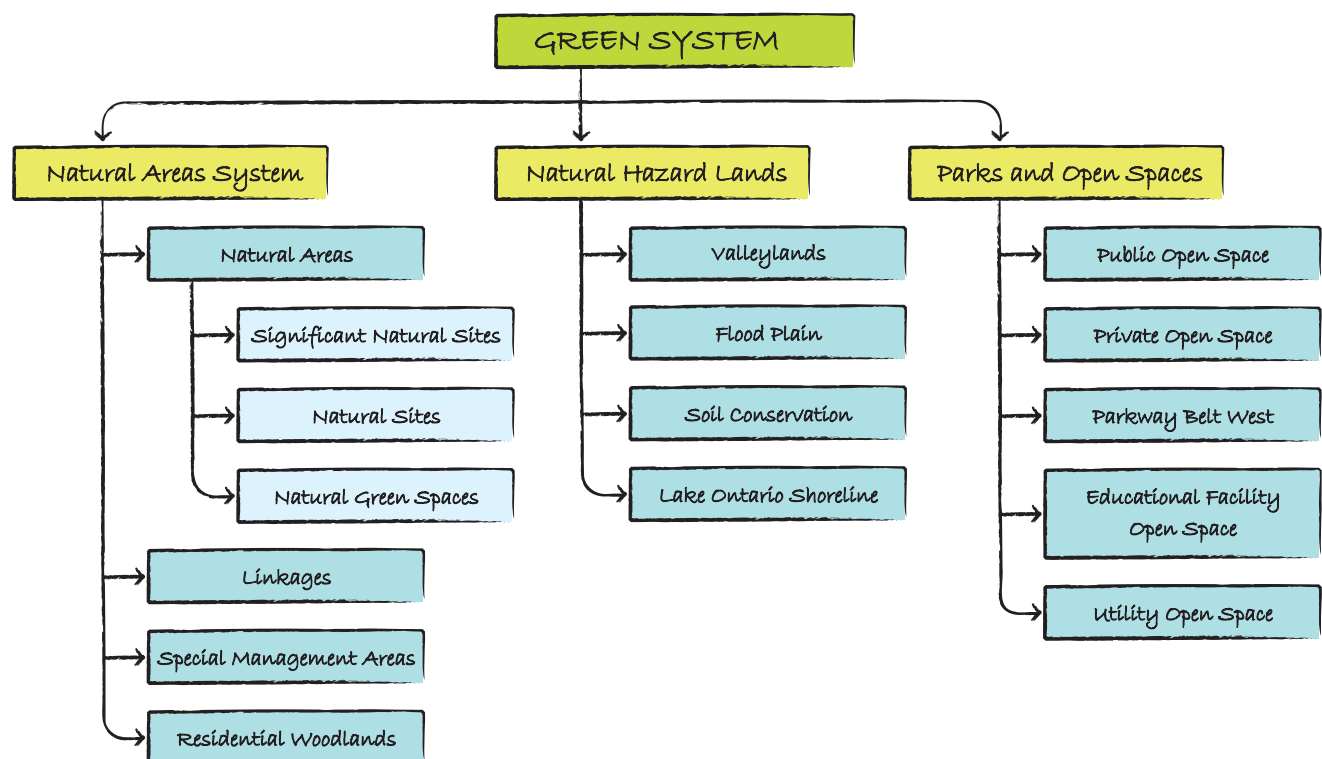
- Natural Areas System:
- **Natural Hazard Lands;** and
- Parks and Open Spaces.

Lands within the Natural Areas System perform an essential ecological function. They sustain **biodiversity** by providing habitat for plants and animals and they clean the air and water. The connectivity of the Natural Areas System is important for maintaining native vegetation communities and providing corridors for urban wildlife. Preserving and enhancing these lands in their natural state is essential to the overall health and functioning of the natural environment. As such, Mississauga will promote and be proactive in the management of its Natural Areas System.

**Natural Hazard Lands** are generally associated with **valley and watercourse corridors** and the Lake Ontario shoreline. These areas are generally unsafe for development due to naturally occurring processes such as flooding and erosion and are shown in Schedule 3: Natural System.

**Watercourse** corridors and the Lake Ontario shoreline, including the physical hazards associated with these areas, are critical to the Natural Areas System due to the ecological functions they provide. Of particular concern within **valley and watercourse corridors** is the preservation and enhancement of fish habitat as an indicator of a healthy environment and for leisure activity and tourism.

**Natural hazard lands, Natural Areas and buffers** are generally designated Greenbelt to protect life and property and to provide for the protection and enhancement of Natural Areas and features and their ecological functions.



NOTE: While illustrated as separate elements, many components of the Green System fall within all three categories, i.e. the Credit River, which is a significant natural site, subject to valleyland and flood plain policies, and can be either public or private open space.

Figure 6-4: The Green System is composed of the Natural Areas System, **Natural Hazard Lands** and Parks and Open Spaces.

Buffers are vegetated protection areas that provide a physical separation of development from the limits of **Natural Hazard Lands** and Natural Areas. Benefits and functions of buffers can include the following:

- maintenance of slope stability and reduction of erosion on valley slopes;
- attenuation of stormwater runoff;
- reduction of human intrusion into Natural Areas and allowance for predation habits of pets, such as cats and dogs;
- protection of tree root zones to ensure survival of vegetation;
- provision of a safety zone for tree fall next to woodlands;
- enhancement of woodland interior and edge areas through native species plantings; and
- enhanced wildlife habitat and corridors for wildlife movement.

**Natural Hazard Lands**, Natural Areas and buffers may provide opportunities for passive recreational activities, in appropriate locations.

Parks and Open Spaces within the Green System, as shown on Schedule 4: Parks and Open Spaces,



Figure 6-5: Mississauga's parks, green spaces, recreation areas and Natural Areas make up the majority of the city's Green System. In addition to its recreational use, the BraeBen Golf Course, built on the former Britannia Landfill site, provides natural habitat through the design of landscaping and water features.

have primary uses such as recreational, educational, cultural and utility services. These lands contain a significant amount of open space such as landscaped areas, lawns, sports fields, etc. These areas have the potential to be managed in a manner that supports and enhances the Natural Areas System.

Mississauga will give priority to actions that protect, enhance, restore and expand the Green System and the natural environment for the benefit of existing and future generations.

### **6.3.1 Natural Areas System**

Mississauga's natural heritage system is known as the Natural Areas System.

The Natural Areas System consists of the following:

- Natural Areas;
- Linkages;
- Special Management Areas; and
- Residential Woodlands.

The location and extent of the Natural Areas System is conceptually illustrated on Schedule 3: Natural System. Detailed information regarding the Natural Areas System can be found in the Natural Areas Survey and supporting fact sheets.

Although some Natural Areas are of higher quality than others, a fundamental premise is that all Natural Areas and their ecological functions are part of the Natural Areas System, and the total or partial loss of any portion of the system diminishes the entire system. As such, all Natural Areas will be protected, enhanced and restored. In addition, Linkages and Special Management Areas should be restored to Natural Areas or managed to support the Natural Areas System. Residential Woodlands should be protected and enhanced.

Natural Areas include features such as **valley and watercourse corridors**, meadows, woodlands, and



wetlands that represent the presettlement landscape and include remnant parcels of native vegetation or areas that have been restored to a natural state through naturalization or successional growth.

#### 6.3.1.1 Natural Areas include Significant Natural Sites, Natural Sites and Natural Green Spaces.

a. Significant Natural Sites are areas that meet one or more of the following criteria:

- all areas of natural and scientific interest (ANSI), **environmentally sensitive or significant areas** and other areas designated for outstanding ecological features;
- all areas with a **Floristic Quality Index (FQI)** of greater than or equal to 40;
- all areas with a mean **floristic co-efficient** greater than or equal to 4.5;
- all woodlands greater than or equal to ten ha;
- all areas that support Provincially significant species or **species at risk** listed as **special concern**, threatened species or endangered

species;

- all woodlands with the potential to provide interior conditions;
- all woodlands that support old growth trees (greater than or equal to 100 years old);
- all significant wetlands, significant coastal wetlands and **other wetlands** greater than two ha; and
- the Credit River and Etobicoke Creek valleys; and

b. Natural Sites are areas that meet one or more of the following criteria:

- all woodlands greater than or equal to two ha but less than ten ha (woodland being defined as forests, which support appropriate understorey as well as woody canopy species);
- all woodlands composed of uncommon (in the context of the city) canopy species;



Figure 6-6: Historically, agricultural practices and land development have resulted in displacement and fragmentation of much of the natural environment. The Credit River Valley Corridor is a major component of Mississauga's Natural Areas System, containing the majority of the city's Natural Areas.



- all areas that represent uncommon vegetation associations in the city;
- all areas that support regionally significant plant or animal species;
- all areas with a Floristic Quality Index (FQI) of 25 to 39.99;
- all areas with a mean **floristic co-efficient** of 3.5 to 4.49; and
- all areas that include natural (i.e. not engineered) landscape features including but not limited to valleylands, **watercourses** and unusual landform features; and

c. Natural Green Spaces are areas that meet one or more of the following criteria:

- all **watercourses** that have some riparian vegetation other than mowed grass, even if they are predominantly engineered;
- all wooded areas that are less than 2 ha and do not fulfill any of the criteria for Significant Natural Sites or Natural Sites; and
- Lake Aquitaine and Lake Wabukayne.

6.3.1.2 Linkages are areas that serve to link two or more of the components of the Natural Areas System within the city, or to natural areas outside of the city boundaries. Linkages include, but are not

limited to the following:

- stormwater management facilities including ponds and **watercourses**;
- designated public open space;
- rights-of-way; and
- green space along major arterial roads providing there is an adequate barrier between the linkage and the roadway.

Linkages are lands that are necessary to connect Natural Areas to maintain **biodiversity** and support ecological functions. Where lands within Linkages have been restored or enhanced to a natural state they will be identified as Natural Areas and where lands have not been restored they will remain as Linkages within the Natural Areas System.

6.3.1.3 Special Management Areas are lands adjacent to existing Natural Areas with the potential for restoration or which should be planned or managed specially due to their proximity to the existing Natural Area. While the primary use of these lands may be for parks, stormwater management or other purposes, they provide opportunities for ecological benefits to the Natural Areas System. When lands within Special Management Areas have been enhanced or restored to a natural state they will be identified as a Natural Area. Where lands have not been restored, they will continue to be identified as a Special Management Area within the Natural Areas System.

6.3.1.4 Residential Woodlands are areas within Neighbourhoods, generally in older residential areas with large lots that have mature trees forming a fairly continuous canopy. Some areas have minimal native understorey due to maintenance of lawns and landscaping. Residential Woodlands provide a number of ecological benefits such as habitat for tolerant canopy birds (both in migration and for breeding) and other urban wildlife and facilitating **ground water recharge** due to the high proportion of permeable ground cover. Development proposals in Residential Woodlands will seek to protect, enhance, restore and expand the existing tree



Figure 6-7: Mississauga promotes and is proactive in the management of its Natural Areas and the protection of its ecological functions.

canopy, understorey, ecosystem functions and wildlife habitat.

6.3.1.5 The Natural Areas System will be protected, enhanced, restored and expanded through the following measures:

- a. placing those areas identified for protection, enhancement, restoration and acquisition through development applications in the appropriate land use designation and zoning category to ensure their long term protection;
- b. placing those areas identified for protection, enhancement and restoration in public ownership, where feasible;
- c. discouraging fragmentation of ownership of Natural Areas and buffers;
- d. using native plant materials and non-invasive species, and reducing and/or eliminating existing invasive, non-native plant species to improve ecological value and the sustainability of indigenous vegetation, where appropriate;
- e. retaining areas in a natural condition and/or allowing them to regenerate to assume a natural state;
- f. controlling activities that may be incompatible with the retention of **natural features, areas and Linkages, including their ecological functions**;
- g. the promotion of stewardship within privately and publicly owned Natural Areas; and
- h. regulation of encroachment into Natural Areas and other public open spaces.

6.3.1.6 Mississauga will establish a program of protection alternatives for the Natural Areas System. This may include, but will not be limited to: information/education programs, stewardship or



Figure 6-8: Natural Areas provide habitat for many plants, birds, insects and animals which are important for maintaining biological diversity.

management agreements, Parks Watch, and land securement.

6.3.1.7 The expansion and connection of the Natural Areas System will be encouraged. Where appropriate, Natural Areas and buffers will be incorporated with public parkland and will be managed in accordance with Natural Areas System policies.

6.3.1.8 In Natural Areas, recreation potential will be restricted to protect their ecological function and sustainability. Formalized passive recreational uses may be permitted to mitigate the impacts of uncontrolled public access.

6.3.1.9 Where lands defined as part of the Natural Areas System are privately owned, it is not intended that they be free and open to the general public. Consideration will be given, however, to public acquisition of these areas through the development approval process or through the City's land securement program.

6.3.1.10 Mississauga may require ecologically based woodland management plans of a landowner prior to municipal acquisition.

6.3.1.11 Mississauga will have regard for the maintenance of the long term ecological integrity of the Natural Areas System in all decisions regarding development and site alteration.

6.3.1.12 The exact limit of Natural Areas will be determined through studies, as well as the processing of development applications.

6.3.1.13 Development and site alteration will not be permitted within or adjacent to Natural Areas, Linkages and Special Management Areas unless it has been demonstrated that there will be no negative impacts to the features and ecological functions of the Natural Areas System. An Environmental Impact Study (EIS) will be required and the Terms of Reference will be provided by the City. The EIS will be approved by the City, in consultation with the relevant conservation authority, at the early stages of a proposal's consideration. The EIS will delineate the area to be analysed, describe existing physical conditions, identify environmental opportunities and constraints, and evaluate the ecological sensitivity of the area in relation to a proposal. It will also outline measures to protect, enhance, and restore the **natural features, area and Linkages including their ecological functions.**

6.3.1.14 Public sector undertakings such as roadways and linear utility corridors will not be permitted within the Natural Areas System unless it has been demonstrated through an Environmental Assessment or other appropriate study that there will be no negative impacts to the features and ecological functions of the Natural Areas System. Essential services, such as water and wastewater systems or road crossings, will only be permitted if other alternatives are studied and are determined to be not feasible. In addition, any negative impacts on the Natural Areas System will be mitigated and compensation provided to the satisfaction of the City.

6.3.1.15 When public works, not subject to the *Environmental Assessment Act*, are planned to traverse, coincide with, or otherwise affect the

Natural Areas System, an Environmental Impact Study will be required.

6.3.1.16 Development, site alteration and new utilities will not be permitted in Provincially significant wetlands, significant coastal wetlands and significant habitat of endangered species and threatened species.

6.3.1.17 Conservation, education, trails and nature appreciation activities may be allowed in significant wetlands and significant coastal wetlands subject to review and approval by the City and appropriate conservation authority.

6.3.1.18 Development and site alteration on lands adjacent to a significant wetland, coastal wetland and significant habitat of endangered species and threatened species will require an Environmental Impact Study, demonstrating no negative impact to the natural features or on their ecological function, to the satisfaction of the City and appropriate conservation authority.

6.3.1.19 Development and site alteration in and adjacent to **other wetlands** will require the completion of an Environmental Impact Study to the satisfaction of the City and appropriate conservation authority demonstrating no negative impacts to the natural features or on their ecological function.

6.3.1.20 Development and site alteration will not be permitted in areas of fish habitat and potential fish habitat, except in accordance with Provincial and Federal requirements. Setbacks and buffers adjacent to fish habitat areas will be determined by an Environmental Impact Study, which shall conform to approved fisheries management plans.

## Areas of Fish Habitat and Potential Fish Habitat

- |  |  |
|--|--|
| <ul style="list-style-type: none"> <li>• Lake Ontario Shoreline</li> <li>• Credit River and its tributaries:             <ul style="list-style-type: none"> <li>◦ Aquitaine Creek,</li> <li>◦ Carolyn Creek,</li> <li>◦ Fletcher's Creek,</li> <li>◦ Levi Creek,</li> <li>◦ Loyalist Creek,</li> <li>◦ Mullet Creek,</li> <li>◦ Sawmill Creek,</li> <li>◦ Wabukayne Creek</li> </ul> </li> <li>• Etobicoke Creek and its tributaries</li> <li>• Sheridan Creek and its tributaries</li> <li>• Turtle Creek and its tributaries</li> <li>• Applewood Creek</li> </ul> | <ul style="list-style-type: none"> <li>• Avonhead Creek</li> <li>• Birchwood Creek</li> <li>• Cawthra Creek</li> <li>• Cooksville Creek</li> <li>• Joshua Creek</li> <li>• Kennollie Creek</li> <li>• Lornewood Creek</li> <li>• Mary Fix Creek</li> <li>• Mimico Creek</li> <li>• Serson Creek</li> <li>• Sixteen Mile Creek</li> <li>• Stavebank Creek</li> <li>• Tecumseh Creek</li> <li>• Wolfedale Creek</li> </ul> |
|--|--|

Figure 6-9: Areas of Fish Habitat and Potential Fish Habitat are identified by the Provincial Government.

6.3.1.21 Development and site alteration will not be permitted in significant woodlands, significant valleylands, significant wildlife habitat and significant areas of natural and scientific interest (ANSI) unless it has been demonstrated, through an Environmental Impact Study, that there will be no negative impacts on the Natural Areas System or natural features including their ecological function.

6.3.1.22 Development and site alteration will not be permitted in the Core Areas of the Greenlands System, as defined in the Region of Peel Official Plan, except for those uses permitted in the Greenbelt designation of this Plan.

6.3.1.23 Mississauga, in consultation with the appropriate conservation authority, will continue to enhance and restore the **watercourses** and Lake Ontario shoreline, including the use of native non-invasive plant materials, establishment of buffer areas and shoreline restoration and protection, where applicable, to improve ecological functions.



### 6.3.2 Natural Hazard Lands

The health of the natural environment is intricately tied with conserving the stability and quality of land, soil and water. A priority for development and site alteration is to protect life and property and restore the health and stability of soil and land where it is compromised.

**Natural Hazard Lands** are generally unsafe and development and site alteration will generally not be permitted due to the naturally occurring processes of erosion and flooding associated with river and stream corridors and the Lake Ontario shoreline. **Natural Hazard Lands**, shown on Schedule 3: Natural System, will be designated Greenbelt.

Proper management of the Lake Ontario shoreline, the **watercourses** and their riparian corridors is crucial to ecosystem health and diversity, sustainable living and the protection of human health and safety.

#### 6.3.2.1 Valleylands

Valleylands are shaped and reshaped by natural processes such as flooding and erosion. In general, erosion hazards associated with valleylands include consideration for slope stability and **watercourse** erosion, which are also interrelated with the flood hazard. The degree and frequency with which the physical change occurs in these systems depends on many factors such as extent and type of vegetation present, soil/bedrock type, and the characteristics of the erosion and flood hazards present.

Development adjacent to valleylands and **watercourse** features must incorporate measures to ensure public health and safety; protection of life and property; as well as enhancements and restoration of the Natural Areas System.



Figure 6-10: **Valley and watercourse corridors** such as the Credit River corridor are subject to naturally occurring physical and ecological processes such as flooding and erosion. This can result in conditions that are hazardous to life and property, making these lands unsuitable for development.

6.3.2.1.1 Development and site alteration will not be permitted within erosion hazards associated with valleyland and **watercourse** features. In addition, development and site alteration must provide an appropriate buffer to erosion hazards, as established to the satisfaction of the City and appropriate conservation authority.

6.3.2.1.2 Development adjacent to valleyland and **watercourse** features may be required to be supported by detailed slope stability and stream erosion studies, where appropriate.

6.3.2.1.3 Development on lands containing a **watercourse** system will be subject to the recommendations of the applicable erosion rehabilitation study where one has been established for that watershed. Where no such recommendations or study are in place, it shall be demonstrated by the proponents of development that the **watercourse** is stable, either with or without the installation of erosion protection works, to the satisfaction of the City.

### 6.3.2.2 Flood Plain

Lands subject to flooding are a danger to life and property and, as such, development is generally prohibited. However, it is recognized that some historic development has occurred within flood plains and may be subject to special flood plain policy consideration.

6.3.2.2.1 Development in flood plains will be subject to the **one-zone concept**, except where a special policy area or **two-zone floodplain management concept** has been approved.

6.3.2.2.2 Development and site alteration is generally prohibited on lands subject to flooding.

6.3.2.2.3 Where historic development has occurred in the flood plain, minor works may be permitted subject to detailed studies to the satisfaction of the City and appropriate conservation authority.

6.3.2.2.4 The construction of buildings or structures permitted in or adjacent to the flood plain will be

protected to the elevation of the Regulatory Flood and will not impact upstream or downstream properties. Additional flood protection measures to be implemented relative to individual development applications will be determined by the City and the appropriate conservation authority.

6.3.2.2.5 Access for development adjacent to or within the flood plain will be subject to appropriate conservation authority policies and the policies of the City.

6.3.2.2.6 In recognition of municipal trans-boundary **watercourses** such as the Credit River and Etobicoke Creek, which are identified in the Provincial Greenbelt Plan as external connections, as well as other **watercourses**, emphasis will be placed on partnerships among municipalities and conservation authorities to improve the ecosystem health of the **watercourse** corridors. Stewardship of these systems should improve their ability to function as a greenway given their scale and relationship to Lake Ontario.

### 6.3.2.3 Soil Conservation

Soil is vulnerable to erosion by wind and water, particularly during the construction process. Erosion affects water resources by reducing water quality and the condition of aquatic habitat through siltation. Reduced water quality in rivers, creeks and Lake Ontario also affects recreational opportunities such as fishing. Erosion can also damage vegetation by exposing roots, which assist in stabilizing soils. Loss of vegetation compromises the Natural Areas System and Urban Forest. Eroded soils compromise the functionality of key infrastructure such as sewers and ditches, thereby increasing the frequency and severity of flooding. In addition, soil erosion, due to wind, causes dust and particulate matter, which affects human health.

Appropriate measures associated with development must be taken to safeguard public safety, protect property, enhance recreational opportunities and prevent damage to the environment due to erosion.



Figure 6-11: Erosion can result in serious danger to property, people, water resources, vegetation and infrastructure. Adherence to development standards and policies reduces these dangers and protects life and property.

6.3.2.3.1 Proponents of development and site alteration will ensure there are no risks to life, safety, health, property and ecosystem health associated with soil erosion.

6.3.2.3.2 Proposals for development and site alteration will incorporate appropriate buffers adjacent to **watercourses**, Natural Areas and parks to protect against soil erosion and sediment impacts.

6.3.2.3.3 Topsoil will be protected by regulating and controlling construction, design and maintenance activities or any activity resulting in site alteration.

6.3.2.3.4 Maintaining vegetation to protect against erosion and degradation of topsoil will be required unless authorized by the City.

6.3.2.3.5 Development and site alteration must comply with the City's Erosion and Sediment Control By-law to the satisfaction of the City and appropriate conservation authority, where applicable.

6.3.2.3.6 An Erosion and Sediment Control Study may be required for development and site alteration, where appropriate.

#### 6.3.2.4 Lake Ontario Shoreline

The Lake Ontario shoreline is an integral component of the Green System and is a key Provincial linkage due to the unique ecological functions and habitats it provides. In addition, it has an important role in leisure activity and tourism.

To sustain the health of shoreline and watershed ecosystems, the local physical and ecological functions should be retained in an undisturbed state to the greatest extent possible and, where deemed appropriate, enhanced and restored. Effective natural hazards management and ecological conservation can only occur on a comprehensive shoreline or watershed/**sub-watershed** basis.

6.3.2.4.1 Where modifications to the existing Lake Ontario shoreline occur they should contribute to its restoration, the healthy functioning of coastal processes, and include opportunities for the creation and enhancement of aquatic and other wildlife habitat, where appropriate.

6.3.2.4.2 Development and site alterations along the Lake Ontario shoreline will be evaluated in the context of their potential impact on the overall physical and ecological functions occurring within the defined shoreline or watershed management area.

6.3.2.4.3 Mississauga will encourage that the health and integrity of the Lake Ontario shoreline be protected, enhanced and, where possible, restored through development. Any mitigative measures to address natural hazards associated with the Lake Ontario shoreline will protect and enhance ecological functions.





Figure 6-12: Jack Darling Park is a public waterfront park located midway between Southdown Road and Mississauga Road. This scenic park provides paths and waterfront trails for pedestrians and cyclists. The park is designed with a number of amenities including picnic areas, comfort stations, a splash pad, tennis courts, open space area, a toboggan hill, playgrounds, beaches, trails, and a footpath that leads to Rattray Marsh, a Provincially significant coastal wetland.

6.3.2.4.4 Development and site alteration will not be permitted within Hazardous Lands adjacent to the Lake Ontario shoreline, which are impacted by flooding hazards, erosion hazards and/or dynamic beach hazards, unless it meets the requirements of the appropriate conservation authority and the policies of the City.

6.3.2.4.5 Development proposals may be required to incorporate and/or restore **natural features, areas and Linkages, including their ecological functions**, along the Lake Ontario shoreline.

6.3.2.4.6 As a condition of development approval, lands adjacent to the Lake Ontario shoreline may be placed in public ownership for their long term protection. Prior to placing lands in public ownership, the applicant will be required to determine what shoreline protection works are required, if any, and will be required to install such works to the satisfaction of the City, the appropriate conservation authority and other public agencies

that have jurisdiction over the Lake Ontario shoreline.

### **6.3.3 Parks and Open Spaces**

Parks and Open Spaces within the Green System include:

#### Public Open Space

- City parks and trails;
- public golf courses;
- public cemeteries;
- stormwater management facilities;
- conservation;
- recreation; and
- urban agriculture.





Figure 6-13: Cemeteries are permitted within Public Open Space and Private Open Space. Cemeteries are serene places for remembrance. Some cemeteries also include passive amenities such as sitting areas and trails. (Streetsville Public Cemetery)

#### Private Open Space

- private parks;
- private golf courses;
- private cemeteries;
- conservation; and
- urban agriculture.

#### Parkway Belt West lands

#### Educational Facilities

- open space associated with educational facilities e.g. school yards.

#### Utilities

- major utility and service corridors; and
- other open space lands in public ownership e.g. water reservoirs, pumping stations.

Public parkland is a vital component in the life of residents and contributes to the environmental, social and economic health of the city. City parks contribute to environmental sustainability and strengthen communities by making them more attractive places to live, work and play.

Access to parks allows for regular physical activity, which improves health, reduces the risk of a wide range of diseases and is important to social and mental health. In addition to the benefits of exercise, contact with the natural environment and opportunities for social interaction improves psychological health.

Parks also have a role in creating a complete community and strong economy. The availability of a park system is a factor for residents and businesses concerned about quality of life.

The city has an extensive system of public parks and trails, comprised of over 2 700 hectares of public parkland. Many of these parks are in Neighbourhoods and were acquired as residential areas were developed. In the next phase of the city's development, it will be important to develop parks that are more urban in nature within the Downtown and Uptown Major Node.

Public parkland expresses two distinct parkland functions that occur at various scales and levels of accessibility: destination parks and community parks. The different park classifications are further described and their locations identified in the Future Directions for Library, Recreation, Parks and Natural Areas Master Plan.

Open space includes such uses as golf courses, cemeteries, private open space and lands associated with community centres, public schools and utility corridors.

These lands have a variety of functions. Some such as golf courses and private open space, provide a recreational function, while others provide social, educational or utility services.

While it is understood that the open space portions of these lands must be maintained in a manner that

accommodates their primary function, these lands can make a significant contribution to a healthy environment by employing environmentally sensitive management techniques and practices. Allowing portions of their lands to naturalize, limiting the use of fertilizers and pesticides by utilizing integrated best management practices, planting native trees and shrubs, and maximizing land area with pervious surfaces, are some methods that provide environmental benefits.

6.3.3.1 Mississauga will value and wisely manage its parkland and open spaces.

6.3.3.2 Mississauga will own, lease, operate, maintain and administer public parkland and facilities to meet the recreational, cultural, educational and social needs of residents.

6.3.3.3 Public parkland will be designed to allow access for a variety of complementary activities through interconnections of pathways, a multi-use recreational trail and the public parkland network; and to provide a safe and accessible environment through development of clear sightlines, openness and visible entrances that can be achieved by maximizing street frontages where possible.

6.3.3.4 Public parkland will contain unstructured or landscaped areas for sedentary uses, where possible.

6.3.3.5 Parks should generally be accessible for residents within 800 m of their dwelling and be located as centrally as possible within a residential area.

6.3.3.6 The minimum city wide parkland provision is 1.2 ha per 1 000 population.

6.3.3.7 A park containing major recreational and sport facilities serving an area greater than Mississauga, may be established.

6.3.3.8 The provision of recreational facilities within destination parks and community parks will be responsive to identifiable needs and in general conformity with the guidelines contained in the

Future Directions for Library, Recreation, Parks and Natural Areas Master Plan.

6.3.3.9 Where possible, destination type parks should provide a higher level of accessibility to persons with disabilities.

6.3.3.10 In addition to the parkland identified on Schedules 4: Parks and Open Spaces and 10: Land Use Designations, additional public parkland may be acquired through the processing of development applications or through purchase.

6.3.3.11 Land conveyed to Mississauga for use as public parkland and/or Greenbelt will be in a condition that is acceptable to the city.

6.3.3.12 Mississauga will negotiate with the appropriate authorities for the use of rights-of-way to accommodate public open space uses.

6.3.3.13 Public parkland may incorporate components of the Natural Areas System to provide opportunities for enjoyment, appreciation and protection of nature.

6.3.3.14 Natural Areas acquired by Mississauga will be designated in accordance with the policies of this Plan. Recreational activities will be restricted to protect the ecological viability of these areas.



Figure 6-14: Mississauga has more than 480 parks and woodlands, these include parks for active recreational uses, while others include naturalized areas that are to be preserved and enhanced. Open spaces are fundamental to the Green System as they provide not only a recreational use but also social, educational and utility services. (Lake Aquitaine)

6.3.3.15 Where Public Open Space contains or abuts the Natural Areas System, the policies for the Natural Areas System will apply.

6.3.3.16 The potential for Public Open Space areas to expand or connect the Natural Areas System will be encouraged to ensure that sensitive areas, particularly woodlands, are maintained and enhanced.

6.3.3.17 Stormwater retention and stormwater quality ponds are generally not appropriate uses for public parkland. In some instances however, overland flow and stormwater facilities may be accommodated in public parkland.

6.3.3.18 Wherever possible, significant treed areas throughout Mississauga will be incorporated into the Public Open Space network. Where appropriate, these areas will be retained in a natural condition or be permitted to regenerate to assume a natural state. Active recreation will be restricted to lands that have been specifically acquired and developed for such purposes.

6.3.3.19 Mississauga recognizes the Lake Ontario waterfront as a vibrant area of lake dependent and lake enhanced activities, with natural habitat areas protected, enhanced and restored and heritage resources incorporated. Through land acquisition, capital works and the review of proposals, Mississauga will endeavour to ensure this vision is realized.

6.3.3.20 Mississauga will encourage open space landowners to employ stormwater management best management practices and planting of native non-invasive species.

6.3.3.21 Cemeteries will be permitted in Public Open Space and Private Open Space designations and will be subject to the following:

- a. as cemeteries constitute an open space use, consideration will be given to using public cemeteries for passive open space purposes. However, cemeteries that are privately owned are not intended to be open to the public;

- b. cemeteries and related facilities will be located to minimize conflict with existing and future land use and transportation; and
- c. cemeteries will recognize, reflect and integrate all natural and cultural heritage resources within and/or adjacent to cemetery property.

6.3.3.22 Where lands are designated Private Open Space, it is not intended that they be free and open to the general public nor that they will be necessarily acquired by the City or any other public agency. Consideration will be given however, to public acquisition of these lands through the development approval process or through the City's land securement program.

6.3.3.23 The development of private parks may be permitted subject to the following conditions being met:

- a. adequate access;
- b. compatibility with adjacent uses;
- c. protection, enhancement and restoration of the Natural Areas System; and
- d. an approved site plan, where applicable.

## **6.4 Urban Forest**

The Urban Forest within Mississauga consists of 2.7 million trees on both private and public property and exists on lands within and outside of the Green System.

Trees are a fundamental component of a healthy city and sustainable community. As such, trees are a valuable asset to the city and contribute to community pride and cultural heritage.

The city's Natural Areas System specifically recognizes the Residential Woodlands in the Mineola, Gordon Woods and Clarkson-Lorne Park neighbourhoods.





Figure 6-15: All trees and woodlands make up Mississauga's Urban Forest. Trees and woodlands play an important role in climate moderation, air and water quality, erosion control, providing wildlife habitat and have a significant role in reducing air temperature in the city.

Trees in the urban setting provide environmental, social and economic benefits such as:

- reducing air pollution by removing carbon, dust and airborne particles;
- improving overall air quality;
- reducing urban heat island effect;
- reducing energy needs for cooling and heating;
- assisting in mitigating climate change effects;
- intercepting rainfall to reduce runoff, increase **groundwater recharge** and prevent soil erosion;
- reducing noise pollution;

- creating wildlife habitat and flora and fauna diversity;
- assisting in improving public health; and
- contributing to the quality and character of the urban environment.

6.4.1 Mississauga will protect and enhance the Urban Forest.

6.4.2 The preservation of trees and woodlots on public and private property that serve to connect and enhance the overall vegetative system and improve wildlife habitat will be encouraged.

6.4.3 Development and site alteration will demonstrate that there will be no negative impacts to trees. An arborist report that demonstrates tree preservation and protection both pre and post construction, must be prepared to the satisfaction of the City in compliance with the City's tree permit by-law.

6.4.4 The Urban Forest will be protected and enhanced by:

- a. developing and implementing a strategic planting program, specific to distinct geographic areas within the city;
- b. developing and implementing a strategic proactive maintenance program pertaining to trees on public land;
- c. providing sustainable growing environments for trees by allocating adequate soil volumes and landscaped areas during the design of new development and infrastructure projects;
- d. ensuring development and site alteration will not have negative impacts on the Urban Forest;
- e. increasing tree canopy coverage and diversity, by planting trees appropriate to the location;
- f. regulating the injury and destruction of trees on public and private property;
- g. promoting the management and enhancement of the Urban Forest on public and private lands;



- h. providing public education and stewardship;
- i. providing strategic partnerships with regulatory agencies to address invasive alien species and diseases; and
- j. compliance with by-laws pertaining to tree preservation and protection.



Figure 6-16: Mississauga is fortunate to be located on the shore of Lake Ontario, part of the largest system of freshwater lakes in the world. The Great Lakes and their watersheds make up one of Canada's richest and most biologically diverse regions, home to a huge variety of fish, wildlife and plant species.

## 6.5 Water

Many Ontarians, including the residents and businesses in Mississauga, depend on Lake Ontario for a safe and reliable source of drinking water. In addition, the Lake Ontario waterfront provides recreational opportunities for both residents of Mississauga and visitors.

Only one percent of the water in the Great Lakes Basin is renewed annually through rainfall and snowmelt. Therefore, it is imperative to conserve water use and to protect the quantity and quality of surface and groundwater resources.

### 6.5.1 Water Conservation

Water is a valuable resource. Water conservation measures will ensure present and future generations have access to a safe and abundant water supply, which will sustain life and ensure economic prosperity.

6.5.1.1 Mississauga will work with the Region of Peel and the conservation authorities to promote conservation of water use through education and promotion initiatives, and through the development of policies, where appropriate.

6.5.1.2 Water conservation measures will be implemented in development.

6.5.1.3 A water conservation plan will be required for development proposing a large use of municipally treated and supplied water. The Plan will consider alternatives to the use of water and evaluate mitigation measures to reduce the use of water, where technically feasible.

### 6.5.2 Stormwater and Drainage

Stormwater management continues to evolve from a philosophy of providing drainage and protection from flooding, to recognizing and attempting to mitigate the impacts of urbanization on water quality and **watercourse** erosion, to a more current recognition of stormwater as a resource and the importance of implementing preventative approaches to stormwater management by minimizing runoff through **stormwater best management practices**.

The effective management of stormwater is vital in protecting life, property, infrastructure and the natural environment. The safe conveyance of storm flows, minimization of flood risks, enhancement of water quality, reduction of erosion and improvement of natural features and aquatic life and habitat will be a priority. Efforts will be made to preserve the natural hydrologic cycle using **stormwater best management practices**. Stormwater management facilities may be part of the Green System.

6.5.2.1 Mississauga will use a water balance approach in the management of stormwater by encouraging and supporting measures and activities that reduce stormwater runoff, improve water quality, promote evapotranspiration and infiltration, and reduce erosion using **stormwater best management practices**.



Figure 6-17: The drainage for the parking area at Riverwood Park has been designed to mimic natural ecological functions such as water infiltration and purification. The runoff from this bio-swale outlets to a small wetland feature on the park site.

6.5.2.2 Mississauga will require that development applications be supported by **stormwater best management practices** in accordance with relevant plans, studies, development standards and policies. Additional measures may be specified by the City based on known concerns related to storm sewer capacity, pollution prevention, flood risk and erosion, and protection of the city's Natural Areas System, including its ecological function. **Stormwater best management practices** must be approved by the city, appropriate conservation authority and Provincial Government, where applicable.

6.5.2.3. The location and design of surface drainage and stormwater management facilities will respect the Natural Areas System and will include

naturalization to the satisfaction of the City and the appropriate conservation authority.

6.5.2.4 Surface drainage and stormwater management facilities will be installed for the safety of residents and to protect infrastructure and property.

6.5.2.5 The design of storm drainage and stormwater management facilities will consider interim and ultimate development conditions.

6.5.2.6 The design of stormwater management facilities and **surface drainage facilities** must conform to City standards, policies and guidelines. A buffer may be required as determined by the City.

6.5.2.7 Protective measures should be developed and implemented, in consultation with the appropriate conservation authority and Provincial Government, for significant **ground water recharge** and discharge areas, where appropriate.

6.5.2.8 The design of storm drainage and stormwater management facilities will enhance the natural and visual landscape and ecological functions and provide recreational opportunities, if appropriate.

## 6.6 Air Quality

Clean air is critical to human and environmental health. The most effective strategies to ensure air quality are to encourage and achieve complete communities with a compact urban form, including alternative modes of transportation such as walking, cycling and transit and ensuring the compatibility of land uses. It is equally important to protect, enhance and restore the Natural Areas System and Urban Forest, which all assist in capturing carbon emissions, reducing the heat island effect and providing overall air quality benefits.

6.6.1 To improve air quality, Mississauga will:

- a. promote the use of alternative modes of transportation such as transit, cycling and walking;

- b. give preference to compact, mixed use and transit oriented development that reduces car dependency;
- c. direct growth to Intensification Areas;
- d. encourage a balance of housing and jobs that provide opportunities for shorter commutes and **active transportation** modes; and
- e. protect, enhance, restore and expand the Natural Areas System.

6.6.2 Mississauga recommends that the Ministry of Environment establish higher regulatory standards than are currently used by the Ministry, including establishing standards for emissions not currently regulated, and take into account the cumulative effects of emissions, and background pollutant concentrations prior to approving applications for Certificates of Approval.

6.6.3 Mississauga will promote building and site design that minimizes vehicular idling, energy consumption and maximizes the use of **renewable energy** and vegetative cover.

6.6.4 Appropriate techniques to mitigate odour and dust will be encouraged to be incorporated in development.

6.6.5 When determining land use compatibility, regard will be given to odours, air particulates, noise and other contaminants, which may impact adjacent or nearby land uses and natural areas. Incompatible land uses such as sensitive land uses and those uses that are sources of noise, odour and dust will be separated and/or the nuisances will be mitigated, so they do not interfere with each other.

6.6.6 Development proposals for a crematorium may be considered subject to the following:

- air, noise, dust, odour and other fugitive emissions will be appropriately mitigated at the source in accordance with Provincial requirements;
- permitted land uses which may be subject to adverse effects arising from the proposed

crematorium use are not located within the area of influence of the proposed crematorium as determined by appropriate studies acceptable to the City;

- there is no impact to the permitted land uses, planned function or viability of the surrounding lands;
- the proposed crematorium is not located within a multi-unit building;
- the scattering or interment of human remains is not permitted, except in cemeteries;
- the development is appropriately screened from adjacent uses and the public realm; and
- the site is appropriately buffered and landscaped.

## 6.7 Brownfield Sites

The rehabilitation of brownfield sites supports the economic prosperity of the city, reduces the environmental risk posed by these properties and enhances the community in which they are located.

6.7.1 To ensure that **contaminated sites** are identified and appropriately addressed by the proponent of development, the following will be required:

- a. the owners of lands proposed for development will submit information as required by the City to identify the potential for contamination;
- b. landowners will consider all potential sources of contamination such as disposal of **waste** materials, raw material storage, residue left in containers, maintenance activities and spills and may also include contamination from adjacent commercial properties, such as, gas bars, motor vehicle service stations, motor vehicle repair garages and dry-cleaning facilities;





Figure 6-18: As Mississauga matures and builds out the last of its greenfields, brownfields will become a major component of future development. An example of a successful brownfield development is the former St. Lawrence Starch plant (originally established in 1889) located in Port Credit.

- c. the development approval or approval of amendments to this Plan for known or potentially **contaminated sites** will be deferred until the proponent of development undertakes a study assessing the potential for contamination in accordance with the Provincial Government regulations and standards and City policies; and
- d. if the study indicates potential for soil or ground water contamination, an assessment of the soil and groundwater conditions will be required. If contamination is confirmed, a remedial action plan in accordance with Provincial Government regulations and standards appropriately addressing **contaminated sites** will be required. Recommendations contained within the plan will be implemented by way of conditions to development approval.

6.7.2 If a **contaminated site** cannot be remediated to the land use designation shown on Schedule 10: Land Use Designations, the land use designation will be reviewed based on the remediation plan and an alternative appropriate land use designation may be considered.

6.7.3 Policies regarding **contaminated sites** should not be construed as a commitment by the City to identify all **contaminated sites**; rather they should be regarded as an effort by the City to responsibly obtain and utilize available information as part of the planning process.

6.7.4 Mississauga will actively promote the redevelopment and clean up, if necessary, of brownfield sites.

6.7.5 Mississauga will encourage the Provincial and Federal Governments to provide legislation and financial incentives that will facilitate the redevelopment of brownfield sites.

## 6.8 Waste Management

**Waste** management is the responsibility of everyone - government, industries, businesses, communities, and individuals at both the regional and city level. Effectively managing, collecting and disposing of **waste** facilitates human and environmental health.



The efficient use of materials and resources and minimizing **waste** generation through reduction, reuse and recycling is critical to the success of an integrated solid **waste** management system. In addition, the provision of **waste** disposal and



treatment facilities is part of **waste** management.

The Region is responsible for the collection, processing, transfer and safe disposal of **waste**

Figure 6-19: Recycling has become a part of our daily living. Promoting the reduction, reuse and recycling of **waste** is a priority in Mississauga.

generated by residential uses. The collection and transportation of non-residential **waste** to **waste** disposal facilities operated by the Region is the responsibility of non-residential users.

6.8.1 Mississauga will manage **waste** in a sustainable way.

6.8.2 Mississauga will support and promote reduction, reuse and recycling of **waste** in private and public sector operations.

6.8.3 Mississauga will establish site design standards that allow adequate flexibility in **waste** handling for development proposals. Standards will address a range of **waste** management options including on-site material separation, multiple **waste** streams and composting.

6.8.4 Mississauga will promote the reduction of **waste** generated through construction.

6.8.5 **Waste transfer stations, waste processing stations** and composting facilities are permitted in Business Employment and Industrial areas subject to meeting the following criteria:

- a. The location and operation of **waste transfer stations, waste processing stations** and composting facilities must comply with all Municipal, Regional and Provincial Government requirements including, where applicable, certification under the *Environmental Protection Act*;
- b. The sites for such facilities will be located, planned, designed, operated and maintained in such a way as to:
  - ensure compatibility with adjacent, existing and future land uses;
  - reduce environmental impact, within applicable standards; and
  - mitigate dust, odour, health and safety concerns, noise, and visual impacts, within applicable standards.

6.8.6 Restrictions are placed on the development of closed **landfill** sites and the adjacent lands of closed sites. Closed **landfill** sites have limited capability to support certain land uses and development will be restricted where such activity would constitute a hazard to human or ecosystem health. The size and extent of a **landfill's** influence area is dependent on many factors and is determined by site specific and detailed studies prepared by the applicant of development proposed within the potential influence area. These studies will be prepared to the specifications of the Provincial Government. Where no information is available on the influence area of the site, Provincial Government standards identify a 500 metre radius surrounding the **waste** cell for assessing potential impacts from the **waste** site.

6.8.7 Within a period of 25 years or less, development on lands formerly used for the disposal of **waste**, requires approval of the Provincial Government.

## 6.9 Noise

Although ambient noise levels are part of living in an urban environment, excessive noise levels can adversely impact quality of life and, in extreme circumstances, public health. The most common source of noise complaints in Mississauga is from aircraft and motorized vehicles on highways and local roadways. Rail and industrial activities are also a source of noise in the city.

Sound barriers should be avoided wherever possible and feasible. Where sound cannot be mitigated at its source, noise abatement measures such as appropriate site planning, spatial separation and building design techniques are preferred, wherever possible.

As the city continues to develop and intensify, particularly with mixed uses, noise will continue to be of concern. Special attention must be given to land use compatibility and the incorporation of noise attenuation methods.

### 6.9.1 Stationary Noise

Natural gas pumping stations, roof top cooling units and a wide variety of industrial processes are all examples of stationary noise sources. Due to the unique nature of this type of noise, it can be difficult

to mitigate through the use of sound barriers. Instead, consideration must be given to appropriate land use planning and building design techniques when locating sensitive land uses in the vicinity of stationary noise sources. Conversely, existing and proposed noise sources near residential and other sensitive uses should incorporate mitigation measures at the source.

6.9.1.1 In order to discourage the encroachment of sensitive land uses on existing industrial noise sources, a Noise Impact Study will be submitted prior to approval of development in proximity to an existing industrial noise source. This will identify options for mitigation at the source and at the proposed development site.

6.9.1.2 Industrial, commercial or utility development will not be permitted where the noise transmitted to existing or proposed residential areas, or other noise sensitive use, exceeds the mitigated outdoor and plane of window noise criteria established by the Provincial Government.

6.9.1.3 The sound levels anticipated on the site of a proposed development will be established on the basis of a predictable “worst case” scenario using only methods acceptable to the Provincial Government.

Outdoor and Plane of Window Sound Level Criteria – Stationary Noise	
Type of Space	Hourly (Leq)*, dBA**
Outdoor Point of Reception (Time Period 07:00 h – 23:00 h)	50***
Plane of Window (Time Period 07:00 h – 23:00 h)	50***
Plane of Bedroom Window (Time Period 23:00 h – 7:00 h)	45***
* Leq - The equivalent energy level.	
** dBA - The A-weighted sound pressure level. A measure of sound weighted such that it resembles human perception and response to sound.	
*** Or Hourly Leq of the ambient road traffic, whichever is greater.	

Figure 6-20: Outdoor and Plane of Window Sound Level Criteria – Stationary Noise (Ministry of Environment)

6.9.1.4 Development that includes outdoor passive recreation areas will generally not be permitted in locations where the mitigated outdoor noise levels are forecast to exceed the limits specified by the applicable Ministry of the Environment noise guideline.

6.9.1.5 Development with a residential component such as dwellings, or any development that includes bedrooms, sleeping quarters or reading rooms and other noise sensitive uses that will be subject to high levels of noise from a stationary noise source, will only be permitted if noise mitigation measures are implemented at the source of the noise or if the development contains mitigative measures which will result in noise levels that comply with the limits specified by the applicable Ministry of the Environment noise guideline.

## 6.9.2 Aircraft Noise

There are areas of Mississauga that are subject to high levels of aircraft noise. As a result, policies are required that set out the restrictions on development within the areas subject to high levels of aircraft noise. The policies of this Plan are based on a six runway configuration of the Airport.

6.9.2.1 New residential development and redevelopment and infilling, which increases the number of dwelling units beyond that permitted by the existing zoning, will not be permitted within the Airport Operating Area outlined on Schedule 10: Land Use Designations and Appendix B: Toronto - Lester B. Pearson International Airport: Airport Influence Area and Map 6-1 Airport Operating Area.

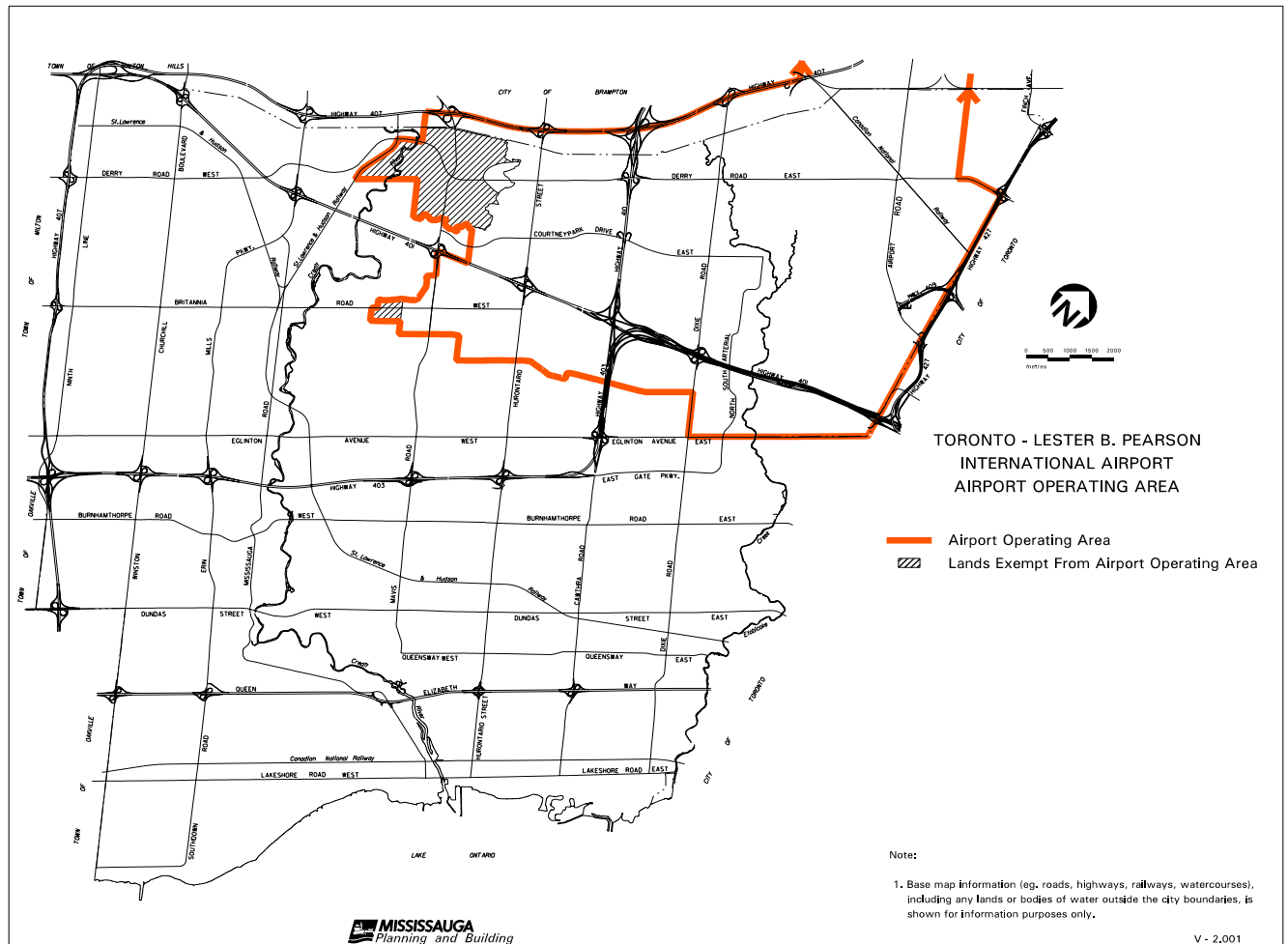
6.9.2.2 Notwithstanding Policy 6.9.2.1, development, redevelopment or infilling applications for residential development for lands within the



Figure 6-21: Although the Airport contributes to the city's strong economy, some communities are directly affected by the sound levels emitted by the airplanes.

Airport Operating Area identified as "Exempt Area", as shown on Map 6-1, may be processed for approval provided that all of the following are satisfied:

- a. appropriate conditions relating to airport noise are included in the approval;
- b. the application was filed prior to February 1, 1997 or the lands were designated Residential prior to February 1, 1997;
- c. the site is located below the 35 ***noise exposure projection (NEP)/noise exposure forecast (NEF) composite noise contour***, and
- d. the redevelopment or infilling has a density not greater than the highest density of immediately adjacent existing residential development located within the Airport Operating Area.



Map 6-1: Airport Operating Area

6.9.2.3 Notwithstanding Policy 6.9.2.1, redevelopment or infilling, which does not significantly increase the number of dwelling units within the Malton Character Area may be permitted, provided the site is below the 35 **noise exposure projection (NEP)/noise exposure forecast (NEF) composite noise contour** and has a density not greater than the highest density of immediately adjacent existing residential development located within the Airport Operating Area.

6.9.2.4 New development and redevelopment or infilling for hospitals, nursing homes, daycare facilities and public and private schools within the

Airport Operating Area will not be permitted as a principal or accessory use.

6.9.2.5 Notwithstanding Policy 6.9.2.4, redevelopment or infilling for hospitals, nursing homes, daycare facilities and public and private schools within the Malton, Meadowvale Village and East Credit Character Areas may be permitted inside the Airport Operating Area on an individual basis below the 35 **noise exposure projection (NEP)/noise exposure forecast (NEF) composite noise contour**. Redevelopment or infilling for daycare facilities may be permitted accessory to an



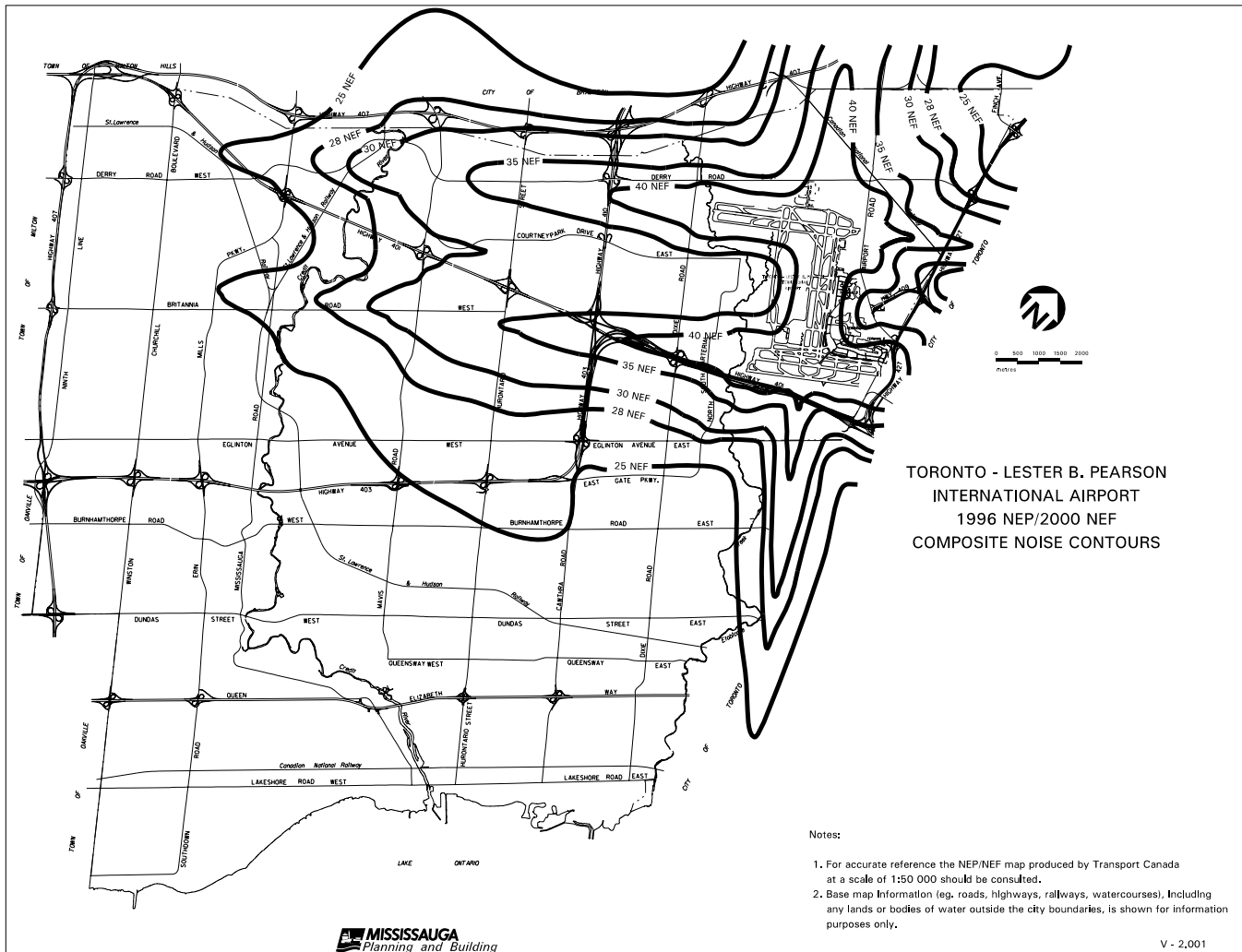


Figure 6-22: 1996 NEP/2000 NEF Composite Noise Contours

employment use in the Gateway and Airport Corporate Character Areas below the 35 **noise exposure projection (NEP)/noise exposure forecast (NEF) composite noise contour**.

6.9.2.6 For the purposes of this section, redevelopment means an application for approval under the *Planning Act* for:

- a. the creation of one or more new lots;
- b. the creation of one or more new dwelling units;
- c. a change in land use;
- d. the construction of buildings or structures; and
- e. where the subject lands have or previously had one or more buildings erected thereon.

6.9.2.7 For the purposes of this section, infilling means an application for approval under the *Planning Act* for:

- a. the creation of one or more new lots;
- b. the creation of one or more new dwelling units;
- c. a change in land use;
- d. the construction of buildings or structures; and
- e. where the subject lands comprise less than two ha and the lands have no buildings erected thereon and are located in an area having existing uses of the same or similar character as the use proposed.

6.9.2.8 Mississauga will require tenants and purchasers to be notified when the proposed development or redevelopment is located between the **noise exposure projection (NEP)/noise exposure forecast (NEF) composite noise contours** of 25 and under 30, as determined by Transport Canada. Notice is also to be provided regarding the requirement for the provision for central air conditioning.

6.9.2.9 Mississauga will require tenants and purchasers to be notified when the proposed development or redevelopment is located at the **noise exposure projection (NEP)/noise exposure forecast (NEF) composite noise contours** of 30 and above, as determined by Transport Canada. Notice is also to be provided regarding the requirement for the installation of central air conditioning.

6.9.2.10 Uses listed in Table 6-1 that are located at or above the corresponding 1996 **noise exposure projection (NEP)/2000 noise exposure forecast (NEF) contour** as determined by Transport Canada, will require a noise study as a condition of development. The noise study is to be undertaken by a qualified acoustical consultant in accordance with Provincial Government policy to the satisfaction of the City prior to development approval to determine appropriate acoustic design criteria.

**Table: 6-1 Aircraft Noise Studies**

<b>LAND USE</b>	<b>Noise Exposure Projection (NEP)/Noise Exposure Forecast (NEF) Contour</b>
Residential Passive use parks Public and private schools Day care facilities Libraries Place of religious assembly Theatres Auditoria Hospitals Nursing Homes	25 or Greater
Hotels Motels Retail or service commercial Office Athletic fields Playgrounds Outdoor swimming pools	30 or Greater
Industrial Warehousing Arena	35 or Greater

### 6.9.3 Road Noise

As intensification occurs in the Downtown, Major Nodes, Community Nodes and along **Corridors**, road noise will increasingly be of concern. Careful attention must be paid to site planning and building design techniques to mitigate noise levels consistent with an urban environment.

6.9.3.1 Where residential and other land uses sensitive to noise are proposed in close proximity to Provincial Highways, it may be necessary to mitigate noise impact, in part, by way of subdivision design. A Noise Impact Feasibility Study will be submitted prior to approval in principle of such land uses located within 50 m of arterial and major collector rights-of-way and within 100 m of a Provincial Highway right-of-way, or as required by the City or Region.

6.9.3.2 Residential development or development that includes outdoor living areas will not be permitted in locations where the mitigated outdoor noise levels are forecast to exceed limits specified

by the applicable Ministry of the Environment noise guideline by five **dBA** or more. A detailed noise study will be required to demonstrate that every effort has been made to achieve the sound level criteria specified by the applicable Ministry of the Environment noise guideline, for an outdoor living area and the noise study shall prove to the satisfaction of the City that the noise level in the outdoor living area, after applying attenuation measures, is the lowest level aesthetically, technically, administratively and economically practical. The attenuated outdoor noise criteria may be exceeded by five **dBA** if a warning (consistent with Provincial Government Guidelines) regarding noise levels, is provided to prospective residents of the development area.

6.9.3.3 Development with a residential component such as dwellings, or any development which includes bedrooms, sleeping quarters, living rooms or reading rooms which will be subject to high levels of traffic noise, will only be permitted if it includes structural features which result in interior noise levels that comply with the indoor standards specified by the applicable Ministry of the Environment noise guideline.

Outdoor and Indoor Sound Level Limits – Road and Rail		
Type of Space	Equivalent Sound Level	
	Level ( <i>Leq</i> )*, <i>dBA</i> **	
	Road	Rail
Sleeping quarters of residential units, hospitals, nursing/retirement homes, etc. (Time period 23:00 h – 07:00 h)	40	35
Sleeping quarters of hotels/motels (Time period 23:00 h – 07:00 h)	45	40
Living/dining rooms of residential dwelling units, theatres; places of religious assembly; schools, etc. (Time period 7:00 h – 23:00 h)	45	40
General offices, reception areas retail shops and stores, etc. (Time period 7:00 h – 23:00 h)	45	40
General offices, reception areas retail shops and stores, etc. (Time period 7:00 h – 23:00 h)	50	45
Outdoor living area (Time period 7:00 h – 23:00 h)	50	55
<p>* <b>Leq</b> – The equivalent energy level.</p> <p>** <b>dBA</b> – The A-weighted sound pressure level. A measure of sound weighted such that it resemble human perception and response to the sound</p>		

Figure 6-23: Outdoor and Indoor Sound Level Limits – Road and Rail. (Ministry of Environment)

6.9.3.4 Where residential and other land uses sensitive to noise are proposed within 500 m of a freeway, 250 m of a provincial highway or 100 m from other roads, development proponents will be required to submit detailed noise studies delineating mitigative noise measures required to meet Provincial Government criteria and Region of Peel noise guidelines. The recommendations of the approved reports are to be implemented as conditions of development.

6.9.3.5 Where the acoustical analysis indicates that anticipated sound levels in the outdoor living area would exceed the outdoor sound level limits stipulated by the applicable Ministry of the Environment noise guideline by up to five **dBA**, Mississauga will require tenants and purchasers to be notified of such. Notice will also be required when road noise necessitates central air conditioning or the provision for central air conditioning to achieve the indoor noise levels limits stipulated by the Ministry of the Environment noise guideline.

6.9.3.6 Detailed noise reports prepared to analyze the impacts of road noise on a development are to incorporate the ultimate Annual Average Daily Traffic (AADT) for the road.

6.9.3.7 As a condition of approval of development applications, notice will be given by the developer to the purchasers and tenants of existing and potential impacts of the right-of-way and the maintenance of the required abatement measures.

## 6.9.4 Rail Noise, Safety and Vibration

Railways in urban areas require particular consideration not only because of the high levels of noise they generate, but also because of ground borne vibration. Safety is also a concern as intensification occurs in the vicinity of railway tracks. In addition, the encouragement of active modes of transportation will require consideration of cyclist and pedestrian safety in conjunction with railway operations.

6.9.4.1 Where residential and other land uses sensitive to noise are proposed in close proximity to rail lines, it may be necessary to mitigate noise impacts, in part by way of the subdivision design. A Noise Impact Study will be submitted prior to approval in principle of such lands located within 100 m of a Principal Main Rail Line right-of-way or within 50 m of a Secondary Main Rail Line. Residential development or any development that includes outdoor, passive and recreational areas will generally not be permitted in locations where the mitigated outdoor noise levels are forecast to exceed the limits specified by the applicable Ministry of the Environment noise guideline by five **dBA** or more. A detailed noise study will be required to demonstrate that every effort has been made to achieve the outdoor sound level criteria specified by the applicable Ministry of the Environment noise guideline and the noise study shall prove to the satisfaction of the City that the noise level in the outdoor living area, after applying attenuation measures, is the lowest level aesthetically, technically, administratively and economically practical.



Figure 6-24: Railways, while a vital part of transportation system and economy, can pose noise, vibration and safety concerns.



6.9.4.2 Development with a residential component such as dwellings, or any development which includes bedrooms, sleeping quarters, living rooms, reading rooms and other noise sensitive uses which will be subject to high levels of railroad noise, will only be permitted if it includes structural features that result in interior noise levels that comply with the indoor standards specified by the applicable Ministry of the Environment noise guideline.

6.9.4.3 Mississauga will require that the owner/developer engage a qualified noise consultant to undertake an analysis of noise and vibration and recommend abatement measures as necessary to meet Provincial and Region of Peel Guidelines and the requirements of the applicable rail company, to the satisfaction of the City, where sensitive land uses and other noise or vibration sensitive development that includes sleeping quarters, reading rooms and offices, are proposed within:

- 500 m of a Principal Main Rail Line for noise;
- 250 m of a Secondary Main Line for noise;
- 100 m of other railway lines for noise; and
- 75 m of all rail lines for vibration.

6.9.4.4 Mississauga will require tenants and purchasers to be notified where the analysis

indicates that anticipated sound levels in the outdoor living area would exceed the outdoor sound level limits stipulated by the applicable Ministry of the Environment noise guideline by up to five **dBA**. Notice will also be required when rail noise necessitates central air conditioning or the provision for central air conditioning to achieve the indoor noise level limits stipulated by the applicable Ministry of the Environment noise guideline.

6.9.4.5 As a condition of approval of development applications, notice will be given by the developer to purchasers and tenants of existing and potential impacts of rail use and operations and the maintenance of the required abatement measures.

6.9.4.6 Development applications for dwellings, significant additions thereto and places of public assembly, will incorporate an appropriate safety setback as determined by the City in consultation with the appropriate railway company, which takes into account berms, topography, intervening structures and the surrounding pattern of development.

6.9.4.7 Through development applications, the incorporation of security fencing to prevent trespassing on the railway right-of-way may be required.

# 7 Complete Communities

## 7.1 Introduction

One of the Strategic Plan pillars is to complete our neighbourhoods. Over the last thirty years many people moved to Mississauga because of its high quality and affordable housing. While important, housing is not enough. Communities need the infrastructure that supports physical and emotional well-being - the ability to live, learn and play. Other important ingredients in nurturing quality of life are cherishing the city's history, cultivating art and culture and valuing the distinctive characteristics of

all areas within Mississauga. This chapter addresses these aspects of community building and, taken together with the other policies in this Plan, will create communities that enable people to not only live and work in Mississauga, but also thrive.

Complete communities meet the day-to-day needs of people throughout all stages of their life. At a city wide level, Mississauga is a complete community. It has a wide assortment of housing choices, employment opportunities and numerous commercial, social and institutional venues allowing its inhabitants to experience the benefits of city living. The Downtown, Major Nodes and



Figure 7-1: Creating complete communities is an important goal of this Plan. This means that the services and facilities needed for daily living are readily available, and there are opportunities for people to fully engage in community life.



Community Nodes are also planned to be complete communities. They will offer a range of residential and employment opportunities and a wide variety of services.

Some areas within the city will not meet all the needs for daily living. Residents living in one of the city's many Neighbourhoods may need to travel some distance to work in another part of the city such as a Corporate Centre or Employment Area. However, other services such as schools, shopping facilities, recreation centres or libraries should be available either within the Neighbourhood or in a nearby Major Node or Community Node, preferably accessible by a short walk or cycling trip.

The policies contained in this chapter, address elements of urban living particularly important to completing communities. This chapter includes policies regarding:

- meeting the housing needs of people of all ages, abilities and income groups;

- providing opportunities for the education of children, continuing learning experiences of adults, opportunities for physical fitness, leisure and social interaction, facilities for worship and spiritual contemplation;
- protecting and enjoying the city's rich cultural heritage;
- providing the inclusion of art and culture in the daily experience of those that live in and visit the city; and
- creating areas with distinct identities that foster community identity and pride.

The goal of these policies is to create inclusive communities where people are connected, supported and allowed to flourish.

7.1.1 Mississauga will encourage the provision of services, facilities and housing that support the population living and working in Mississauga.



Figure 7-2: Mississauga is home to residents of all ages. Mississauga Official Plan's policies are intended to foster vibrant and complete communities that will enable all residents to thrive.

7.1.2 The creation of complete communities and the implications for public health will be considered by Mississauga when making planning decisions.

7.1.3 In order to create a complete community and develop a built environment supportive of public health, the City will:

- a. encourage compact, mixed use development that reduces travel needs by integrating residential, commercial, employment, community, and recreational land uses;
- b. design streets that facilitate alternative modes of transportation such as public transit, cycling, and walking;
- c. encourage environments that foster incidental and recreational activity; and
- d. encourage land use planning practices conducive to good public health.

7.1.4 Mississauga will raise awareness of the link between the built environment and public health.

7.1.5 Mississauga may require a Health Impact Statement associated with development proposals.

7.1.6 Mississauga will ensure that the housing mix can accommodate people with diverse housing preferences and socioeconomic characteristics and needs.

7.1.7 In cooperation with the appropriate public and private agencies and other levels of government, Mississauga will provide community infrastructure, supportive of complete communities, to meet the civic, cultural, educational, recreational, religious, social and emergency service needs of residents, employees and visitors.

7.1.8 Mississauga will recognize the significance of and act responsibly in the identification, protection, and enhancement of structures, sites, cultural heritage landscapes, environments, artifacts, traditions, and **streetscapes** of historical, architectural or archaeological significance.

7.1.9 Public art and culture will be encouraged as a means of enhancing the identity and unique character of the city and its various communities. Incentive programs may be developed to encourage the creation of public art.

7.1.10 When making planning decisions, Mississauga will identify, maintain and enhance the distinct identities of local communities by having regard for the built environment, natural or heritage features, and culture of the area.

## 7.2 Housing

The provision of suitable housing is important to ensure that youth, older adults and immigrants thrive, which is one of the Strategic Plan pillars. Mississauga is fortunate to have a diverse mix of housing that is largely in good condition.

Population by Age Group | 2009 and 2031

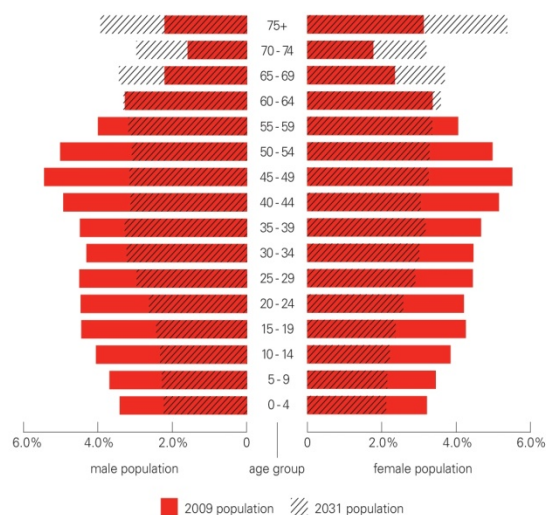


Figure 7-3: As the city's overall population continues to grow, so does the aging population. The age structure is affected by the natural increase and aging of the existing population base, and less affected by the age of new residents. A growing trend in Mississauga is to age-in-place, which means that when children move out to establish their own households, parents remain in the family home. Many will want to remain in the same community.



Mississauga is a desirable place to locate for those who work within the city or surrounding municipalities. The city provides affordable housing options in a variety of housing forms and locations across the city. The Region is responsible for providing social housing. While the city has some social housing, the inventory of social housing is not adequate to meet the need that exists.

Much of Mississauga's housing was built in the last three to four decades and geared to the needs of families with children. As these households mature, the dwelling units and Neighbourhoods that they occupy may no longer meet their needs. Opportunities for aging-in-place or alternative housing within the community will assist households as they move through the lifecycle. This may include introducing alternative forms of housing within Neighbourhoods such as supportive housing for seniors and secondary suites.

Attracting and retaining young adults and families to Mississauga is important to ensure that employers have a strong local labour force on which to draw and that the demographic mix of the city remains diverse. It is also important to accommodate new immigrants choosing to locate in the city.

It is expected that most future additions to the housing stock will be higher density forms, particularly apartments. The Plan encourages the creation of new housing in the Downtown, Major Nodes and Community Nodes that meets the needs of a diverse population. While housing in these areas is expected to be attractive to young and older adults, the needs of families must also be considered. As housing in Neighbourhoods is vacated by older adults, opportunities will exist for new families coming to the city. However, opportunities for families to live in the Downtown, Major Nodes and Community Nodes will also be encouraged.

7.2.1 Mississauga will ensure that housing is provided in a manner that maximizes the use of community infrastructure and engineering services, while meeting the housing needs and preferences of Mississauga residents.

7.2.2 Mississauga will provide opportunities for:

- a. the development of a range of housing choices in terms of type, tenure and price;
- b. the production of a variety of affordable dwelling types for both the ownership and rental markets; and
- c. the production of housing for those with special needs, such as housing for the elderly and shelters.

7.2.3 When making planning decisions, Mississauga will ensure that housing is provided in a manner that fully implements the intent of the Provincial and Regional housing policies.

7.2.4 Mississauga will ensure that the quality and quantity of the existing housing stock is maintained.

7.2.5 The onus will be placed on the applicant/developer to address Provincial and Regional housing requirements.

7.2.6 Mississauga will consider the contribution that can be made to current housing needs by housing programs of other levels of government and will seek to maximize the use of those programs that meet the City's housing objectives.

7.2.7 Mississauga will directly assist all levels of government in the provision of rental housing by:

- a. supporting the efforts of the Region and other local not for profit housing organizations in providing low and moderate income rental housing and accommodation for those with special needs;
- b. assisting the development of new rental units through the promotion of, and participation in, programs aimed at producing rental housing; and
- c. supporting the preservation of the rental housing stock.

7.2.8 Design solutions that support housing affordability while maintaining appropriate functional and aesthetic quality will be encouraged.

7.2.9 The provision of housing that meets the needs of young adults, older adults and families will be encouraged in the Downtown, Major Nodes and Community Nodes.

7.2.10 Mississauga will encourage the Region to provide social housing in appropriate locations to meet the needs of the local population.

7.2.11 Mississauga will work with the Region to develop a housing strategy that will establish and implement affordable housing targets.

7.2.12 Conversion of residential rental properties to a purpose other than the purpose of a residential rental property, or the demolition of residential rental properties exceeding six dwelling units will not be permitted if it adversely affects the supply of affordable rental housing as determined by affordable housing targets and rental vacancy rates.

## 7.3 Community Infrastructure

Community infrastructure is a vital part of complete communities, contributing to the quality of life and well-being of residents. It is essential in meeting social, cultural, education, recreational, and spiritual needs for a growing and multicultural resident population. In addition to the services provided by

the City, community infrastructure is also provided by other agencies, levels of government and the private sector.

Access and transportation connections to community infrastructure are important. Generally, new community infrastructure is encouraged to locate in Intensification Areas and **Corridors** to minimize traffic impacts on local roads.

7.3.1 Community infrastructure will support the creation of complete communities.

7.3.2 The preferred location for community infrastructure will be within the Downtown, Major Nodes, Community Nodes and **Corridors**. Where appropriate, community infrastructure may also be located within Neighbourhoods and Corporate Centres. Community infrastructure will generally not be located within Employment Areas. Where permitted within Employment Areas, these uses will be located along the periphery of Employment Areas.

7.3.3 Community infrastructure located within Neighbourhoods may include schools, emergency services, private clubs, daycare/day programs and places of religious assembly. Where community infrastructure is located in Neighbourhoods it will generally serve the local or nearby Neighbourhoods.



Figure 7-4: Community infrastructure provides valuable services to all members of the community including public schools, private schools, emergency services, private clubs, **community facilities**, daycare/day programs and places of religious assembly. The City provides high quality services at state of the art facilities.

City wide or region wide community infrastructure may not be permitted in Neighbourhoods.

7.3.4 Community infrastructure that generates large amounts of traffic will be located to minimize impacts on the transportation system.

7.3.5 Community infrastructure will generally be:

- a. in proximity to transit facilities;
- b. on **Corridors**, major and minor collector roads, preferably at intersections;
- c. connected to trails, cycling facilities, where possible;
- d. in proximity to other community infrastructure and places of gathering, where possible; and
- e. accessible to persons with disabilities.

7.3.6 Mississauga will cooperate and assist other levels of government and public and private agencies in providing community infrastructure that are not within the jurisdiction of the City.

7.3.7 The type of community infrastructure as well as its scale, design, layout and configuration permitted at any location, may be limited to ensure visual and functional compatibility with surrounding development.

7.3.8 Where possible, community infrastructure will be encouraged to develop shared parking

facilities.

7.3.9 School sites will be determined during the processing of development applications and will have regard for the site policies established by the School Boards.

7.3.10 School sites will be used for schools under the jurisdiction of the Peel District School Board, the Dufferin-Peel Catholic District School Board, the Conseil Scolaire de District Centre-Sud-Ouest, or the Conseil Scolaire de District Catholique Centre-Sud, or private schools.

7.3.11 The preferred locations for places of religious assembly will be the Downtown, Major Nodes and Community Nodes. Otherwise, places of religious assembly will be encouraged to locate in **Corridors**, preferably at their intersections.

7.3.12 The availability and location of existing and planned community infrastructure will be taken into account so that new community infrastructure can be provided efficiently and effectively and tailored to meet the needs of the population in each community.

7.3.13 The proponent of an intensification project may be required to provide a Community Infrastructure Impact Study. A Community Infrastructure Impact Study will, among other things, assess the proximity to and adequacy of existing community infrastructure, human services and emergency services to meet increased demand



Figure 7-5: Mississauga's Community Services Department has a variety of programs for every age and interest, offered at various **community facilities**.



caused by proposed intensification. A Community Infrastructure Impact Study will identify necessary community infrastructure and the need for staging to ensure that development does not precede necessary community infrastructure improvements. A Community Infrastructure Impact Study will require the approval of the City and other appropriate approval agencies.

## 7.4 Heritage Planning

Heritage planning is the responsibility of the Provincial Government and the City. The Heritage Advisory Committee has been established to advise City Council on matters pertaining to cultural heritage value or interest.

### 7.4.1 Cultural Heritage Resources

Mississauga's cultural heritage resources reflect the social, cultural and ethnic heritage of the city and, as such, are imperative to conserve and protect. Cultural heritage resources are structures, sites, environments, artifacts and traditions that are of cultural, historical, architectural, or archaeological value, significance or interest. These include, but are not limited to:

- structures such as buildings, groups of buildings, monuments, bridges, fences and gates;
- sites associated with an historic event;
- environments such as landscapes, **streetscapes**, flora and fauna within a defined area, parks, heritage trails and historic corridors;
- artifacts and assemblages from an archaeological site or a museum; and
- traditions reflecting the social, cultural, or ethnic heritage of the community.

To celebrate the past and create a sense of place and identity, Mississauga will designate cultural heritage resources in accordance with the *Ontario Heritage Act*.

7.4.1.1 The heritage policies are based on two principles:

- a. heritage planning will be an integral part of the planning process; and
- b. cultural heritage resources of significant value will be identified, protected, and preserved.

7.4.1.2 Mississauga will discourage the demolition, destruction or inappropriate alteration or reuse of cultural heritage resources.

7.4.1.3 Mississauga will require development to maintain locations and settings for cultural heritage resources that are compatible with and enhance the character of the cultural heritage resource.

7.4.1.4 Mississauga will encourage other levels of government to enact legislation and develop programs that promote the preservation and rehabilitation of cultural heritage resources.

7.4.1.5 Mississauga will encourage private and public support and the allocation of financial resources for the preservation and rehabilitation of cultural heritage resources.

7.4.1.6 Mississauga will foster public awareness of and commitment to, the protection and enhancement of cultural heritage resources.

7.4.1.7 Mississauga will maintain a Heritage Register of property, including structures and cultural landscapes that should be preserved as cultural heritage resources. The cultural heritage resources in the Heritage Register will be assessed based on their design or physical value, historical or associative value, contextual value and archaeological significance including the aggregation of both natural and cultural heritage resources.

7.4.1.8 The Heritage Register will contain a legal description of the property, the name and address of the owner, a statement explaining the cultural



heritage value or interest of the property and a description of the heritage attributes of the property.

7.4.1.9 Local area plans may suggest ways to protect cultural heritage resources of major significance by prohibiting uses or development that would have a deleterious effect on the cultural heritage resource, and encouraging uses and development that preserve, maintain and enhance the cultural heritage resource.

7.4.1.10 Applications for development involving cultural heritage resources will be required to include a **Heritage Impact Statement** prepared to the satisfaction of the City and other appropriate authorities having jurisdiction.

7.4.1.11 Cultural heritage resources designated under the *Ontario Heritage Act*, will be required to preserve the heritage attributes and not detract or destroy any of the heritage attributes in keeping with the *Ontario Heritage Tool Kit*, the Ontario Ministry of Culture, and the *Standards and Guidelines for the Conservation of Historic Places in Canada*, Parks Canada.

7.4.1.12 The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a **Heritage Impact Statement**, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction.



Figure 7-6: The Bradley Museum is a small saltbox style farmhouse that was constructed in 1830 by Lewis and Elizabeth Bradley. After 20 years in a rugged cabin, this United Empire Loyalist couple and their seven children called this modest house home. Bradley House opened to the public in 1967. It was originally restored by the Mississauga Heritage Foundation to reveal the everyday life of early settlers in Ontario.



Figure 7-7: Heritage Conservation Districts enable the City to manage and guide change through the adoption of a plan and guideline for the conservation, protection and enhancement of each area's special character. Meadowvale Village, at Old Derry and Second Line roads, is a historic community within the modern City of Mississauga. The City recognized the significance of Meadowvale Village in 1980 by designating it a Heritage Conservation District pursuant to the *Ontario Heritage Act*, the first designated in Ontario. More recently, Port Credit Village was designated a Heritage Conservation District in 2004.

7.4.1.13 Cultural heritage resources must be maintained in situ and in a manner that prevents deterioration and protects the heritage qualities of the resource.

7.4.1.14 Cultural heritage resources will be integrated with development proposals.

7.4.1.15 Mississauga will regulate use and other matters, as appropriate, for heritage preservation through zoning by-laws.

7.4.1.16 Mississauga will acquire **heritage easements**, apply restrictive covenants, and enter into development agreements, as appropriate, for the preservation of cultural heritage resources.

7.4.1.17 Public works will be undertaken in a way that minimizes detrimental impacts on cultural heritage resources.

7.4.1.18 Mississauga recognizes the Credit River and Etobicoke Creek valleys as heritage corridors with both prehistoric and historical significance.

historical/associative and/or physical/design value that is to be preserved. Properties of cultural heritage value are designated under the *Ontario Heritage Act*, on the City's Heritage Register, and include listed properties that have not been designated under the Act, but that City Council believes to be of cultural heritage value or interest.

7.4.2.1 Programs for the selective acquisition of cultural heritage properties by the City will be considered. Assistance from the Federal and Provincial Governments will be requested in realizing these programs.

7.4.2.2 Prior to the demolition or alteration of a cultural heritage resource, documentation will be required of the property to the satisfaction of the City, and any appropriate advisory committee. This documentation may be in the form of a **Heritage Impact Statement**.

7.4.2.3 Development adjacent to a cultural heritage property will be encouraged to be compatible with the cultural heritage property.

## 7.4.2 Cultural Heritage Properties

Cultural heritage properties are those properties or defined areas that are determined to be of cultural, historical, archaeological or natural significance and/or value. A heritage designation is applied to properties that have contextual, archaeological,

## 7.4.3 Heritage Conservation Districts

In some locations, the cultural heritage value or interest is more than an individual structure or property. It may be a collection of cultural heritage attributes that form the character and appearance of

an area, including buildings, structures, cultural landscapes, open spaces, roads, fences, and other property features. Although such locations may include individual cultural heritage properties, they may also be an area where no building or element may be significant on its own but collectively they contribute to the special cultural heritage value character of the area that is worthy of preservation and identification as a Heritage Conservation District.

Heritage Conservation Districts are areas defined by the City as being of unique character to be conserved through a designation by-law pursuant to the *Ontario Heritage Act*. Mississauga has two Heritage Conservation Districts. Meadowvale Village, designated in 1980 and Ontario's first Heritage Conservation District, and Old Port Credit Village, designated in 2004. Additional Heritage Conservation Districts may be considered.

7.4.3.1 Heritage Conservation Districts will be designated by the City in accordance with the *Ontario Heritage Act* and the following criteria:

- a. most of the structures or heritage elements, in a grouping, that have a unique character and reflect some aspect of the heritage of the community or are of historic, architectural, natural, or cultural significance; or
- b. an environment that should be preserved because of its cultural heritage, cultural landscape, or scenic significance.

7.4.3.2 Heritage Conservation District Plans will contain the following:

- a. a statement of the objectives to be achieved in designating the area as a heritage conservation district;
- b. a statement explaining the cultural heritage value or interest of the heritage conservation district;
- c. a description of the heritage attributes of the heritage conservation district and of properties in the district;
- d. policy statements, guidelines and procedures for achieving the stated objectives and managing change in the heritage conservation district; and
- e. a description of the alterations or classes of alterations that are minor in nature and that the owner of a property in the heritage conservation district may carry out or permit to be carried out on any part of the property, other than the interior of any structure or building on the property, without obtaining a Heritage Permit.

7.4.3.3 Applications for development within a Heritage Conservation District will be required to include a ***Heritage Impact Statement*** and Heritage Permit, prepared to the satisfaction of the City and the appropriate authorities having jurisdiction.



## 7.4.4 Archaeological Resources



Figure 7-8: An excavation was conducted in 1972 at the original location of the Cherry Hill house at the northwest corner of Cawthra Road and Pinkney Drive. After a subsequent excavation of the site, a collection of 30,000 artifacts including glass, ceramics, pottery, metal, bone and miscellaneous material was discovered and in 1986 the collection was turned over to the Bradley Museum. One of the findings includes the china plate pictured above called Spero; the pattern is dated between 1886 and 1891.

The city's human history spans thousands of years and is reflected through physical remains that have been left behind by individuals or groups of people. These physical remains are archaeological resources and can be found lying on top of the ground, buried in the earth or under water.

Archaeological resources help us understand the people who previously inhabited the area and are important in preserving the city's history and identity.

7.4.4.1 Mississauga will cooperate with the Provincial Government to designate archaeological sites in accordance with the *Ontario Heritage Act*.

7.4.4.2 Removal of artifacts from an archaeological site will be prohibited except in accordance with the

requirements of the *Ontario Heritage Act*, the Provincial Government and the City.

7.4.4.3 Any archaeological or other artifacts of heritage significance discovered on properties during the development of privately owned land will be deeded gratuitously to the appropriate public authority.

## 7.4.5 Archaeological Protection Areas

Archaeological protection areas are sites of archaeological value and interest of major significance to the history and identity of the city, worthy of preservation and possible acquisition by the City.

An archaeological site is a defined location noted through the scientific study of material remains of past human life and activities. Archaeological sites are an irreplaceable part of heritage and although the history of Mississauga as a city is short, it is rich, varied and unique in 10 000 years of archaeological resources. What is discovered from archaeological sites contributes to a better understanding of the past. The removal of archaeological material is a destructive process. Archaeological protection areas retain these resources for future generations.

7.4.5.1 As a condition of development, the City in consultation with the Provincial Government, may require that an archaeological assessment be undertaken. Should any significant archaeological remains be discovered, an appropriate mitigation strategy will be developed. Any salvage excavation of archaeological remains will be conducted to the satisfaction of the City and the Provincial Government.

7.4.5.2 Programs for the survey and excavation of archaeological protection areas will be considered. Assistance from the Federal and Provincial Governments will be requested in realizing these programs.



7.4.5.3 Any report on surveys or excavations in Mississauga must be filed with the City at the time the report is filed with the Provincial Government.

7.4.5.4 Mississauga reserves the right to refuse a completed archaeological report if it is not satisfactory to the City.

7.4.5.5 To protect against the inappropriate removal of artifacts, Mississauga may keep the existence and location of archaeological protection sites confidential.

7.4.5.6 Mississauga will investigate establishing and maintaining an inventory of archaeological sites and reporting to the Provincial Government all known sites.

7.4.5.7 Mississauga will consider the use of public open space for conserving archaeological protection sites where appropriate.

7.4.5.8 Sites of archaeological value and interest of major significance to the history and identity of the City, worthy of preservation and possible City acquisition, are categorized as archaeological protection areas.

7.4.5.9 Archaeological protection areas should be designated archaeological sites by the Provincial Government in accordance with the *Ontario Heritage Act*.

7.4.5.10 Mississauga, in cooperation with the Provincial Government, will endeavor to preserve archaeological sites of major significance by prohibiting uses or development on archaeological protection areas that would have a deleterious effect on the archaeological site.

7.4.5.11 Sites of lesser archaeological value and interest but still representing an important contribution to the history and identity of the City, worthy of documentation and study, may be subject to an archaeological assessment and mitigation.

7.4.5.12 Mississauga will endeavour to protect the archaeological heritage of the City by identifying archaeological sites and encouraging documentation.

## 7.5 Cultural Infrastructure

Culture contributes to creating engaging, lively and richly textured places where people want to live and visit. It plays a significant role in creating vibrant and liveable communities, contributes to the economy and should reflect and celebrate the culture, histories and traditions of the community. Incorporating culture creates a social environment that supports community building.

Cultural infrastructure refers to those properties that are used primarily or occasionally for creation, presentation or delivery of cultural products or events or which are themselves cultural artifacts. These properties may have been constructed as purpose built cultural buildings or they may have had, or continue to have, another primary purpose. These properties may be owned by governments, institutional and not for profit corporations, or by private interests. Some properties will conform to the description of conventional cultural facility type (e.g., museum) while others may not. The public and physical places where digital culture is facilitated (e.g., internet cafes, wireless hotspots) are included.

The development of cultural infrastructure should be community driven and neighbourhood focused. A broadly distributed range of opportunities for citizens of all ages to participate in a wide variety of cultural activities, builds the foundation for strong cultural institutions and an authentic identity in the future.

7.5.1 Cultural infrastructure should:

- a. be directed to Intensification Areas;
- b. be a part of creating complete communities; and
- c. recognize and strengthen distinct identities.

7.5.2 Community Improvement Plans should be used to offer incentives to guide the development of cultural infrastructure clusters. They may provide incentive grants and loans to:



Figure 7-9: The Living Arts Centre (LAC) opened its doors on October 7, 1997, adding an exciting cultural dimension to Mississauga's Downtown. Serving as an important resource for the arts, education and business, LAC features over 225,000 square feet of multiple performance venues, studio spaces and exhibition display areas. The LAC is a valued cultural resource that benefits the community.

- a. preserve and adaptively reuse heritage buildings;
- b. initiate façade improvement programs for heritage buildings in commercial areas with a focus on buildings in Intensification Areas;
- c. encourage the conversion of spaces for cultural uses; and
- d. encourage public art.

7.5.3 Mississauga will support cultural development by considering the needs of the cultural community when:

- a. acquiring or selling municipal land; and
- b. building and rehabilitating municipal facilities.

7.5.4 At the discretion of the City, municipal parking facilities may be used to meet or reduce the parking requirements for cultural facilities where it does not impair the functioning of other uses. The

City will develop corporate policies regarding which cultural facilities are eligible and appropriate implementation criteria and conditions.



Figure 7-10: Public art is created for specific sites, responding to a series of conditions, including built forms and elements, landscaping, historical events and cultural and community identities, and interpreting contemporary life. Public art contributes appreciably to the experience of urban space, making it a rich and engaging environment. Many areas around the city include pieces of public art, including the Credit Valley Hospital.



## 7.6 Distinct Identities

Mississauga was created in 1974 through the amalgamation of the towns of Port Credit and Streetsville and the Town of Mississauga. These towns and settlement areas predate 1900 and continue to retain historic features.

More recent residential development occurred in phases generally from the south end of the city proceeding northward. These areas are distinguished by their common development characteristics, age, development pattern, architectural style and intensity of use.

The character of Mississauga's communities is derived in part, from physical elements such as topographical and natural features, distinct buildings, streets and places, all of which provide a sense of individuality. The waterfront, the major valley

features associated with the Credit and Etobicoke rivers and the former Lake Iroquois Shoreline, are highly visible and important physical elements that create a distinct identity for the city as a whole and the specific communities in which they are located.

### 7.6.1 Diverse Character

As development of existing communities proceeds, Mississauga will ensure the distinct character of existing areas, including their natural heritage features, cultural heritage, built heritage and archaeological resources are preserved and enhanced for present and future generations.

As new areas develop, particularly the Major Nodes and Community Nodes, a distinct identity should be created based on the history, needs and characteristics of residents.



Figure 7-11: My Mississauga events have become a major attraction in the Downtown. A number of free events including cultural shows and dances, concerts and community festivals are held each summer. These events help promote arts and culture in the city and reach a broad audience, from youth to older adults of various backgrounds.

Corporate Centres are also encouraged to develop unique identities. For example, Meadowvale Business Park, renowned for its bio-medical businesses, and Sheridan Park, a well established research park, both developed as prestige employment areas set in a suburban landscape. While they are encouraged to intensify they should have a different character than other Corporate Centres such as Gateway Corporate Centre which is currently developing and is located in an **Intensification Corridor** to be served by **higher order transit**. As such, Gateway Corporate Centre is expected to have a strong urban built form that will contribute to creating a distinct identity.

7.6.1.1 Mississauga will strive to protect and enhance the desirable character of areas with distinct identities and encourage the development of distinct identities for other areas.

7.6.1.2 Built form within Intensification Areas should provide for the creation of a sense of place through, among other matters, distinctive architecture, high quality public art, streetscaping (including street trees), and cultural heritage recognition.

7.6.1.3 A distinct identity will be maintained for each Character Area by encouraging common design themes and compatibility in scale and character of the built environment.

7.6.1.4 The historic character and sense of community will be maintained through the preservation and protection of existing residential Neighbourhoods.

7.6.1.5 New development will be compatible with the physical, social and environmental attributes of the existing community.

7.6.1.6 Mississauga will strive to conserve cultural heritage resources by incorporating them into community design.

## 7.6.2 Lake Ontario Waterfront

The Lake Ontario waterfront shoreline within Mississauga, which measures approximately 22 km, is a part of the Natural Areas System and is a major public destination. The waterfront includes a diversity of uses that range from industrial, commercial, recreational and tourism to residential uses while providing important ecological features and functions.



Figure 7-12: The Mississauga waterfront communities include all or a portion of Southdown, Clarkson-Lorne Park, Lakeview and Port Credit.





Figure 7-13: Mississauga is located on the Lake Ontario shoreline. The waterfront contains a number of natural areas and public spaces that are major destinations within the city.

The Mississauga waterfront communities encompass all or portions of the Southdown, Clarkson-Lorne Park, Port Credit and Lakeview Character Areas. These communities have a strong orientation to the waterfront and their identity is associated with their historic relationship to Lake Ontario. Maintaining and strengthening these relationships will be a factor in planning decisions affecting these communities.

Access to the water, parks and open space, as well as its recreational facilities, enhance the quality of life for residents and visitors. Mississauga has 22 waterfront parks that vary in size, use and features. Future development in waterfront communities should have regard for the Mississauga Waterfront Parks Strategy, a comprehensive long term plan to

manage the future development of the City's waterfront parks. The waterfront will continue to serve as a regional destination for public uses and mixed use development, without compromising ecological features and functions, access to the shoreline and water, water views, and its unique historic and natural character. Among the waterfront's unique features are Rattray Marsh, a shingle beach, several harbours, marinas and historic villages. In addition, areas of the waterfront support ongoing industrial operations that contribute to the physical diversity and visual interest of the city.

Lake Ontario was a key factor in the settlement of Mississauga and influenced the settlement pattern

of the city, as well as, the city's cultural and natural heritage.

7.6.2.1 To ensure that the waterfront continues to remain a viable natural asset for present and future generations Mississauga will:

- a. continue to pursue the acquisition of physical and visual public access to the waterfront having regard for the function of the Natural Areas System and the risks posed by potential natural hazards; and
- b. permit and promote a range of uses that recognize the waterfront as a focus for recreation, tourism and economic development activities while having regard for the natural hazards and natural environment associated with these areas.

7.6.2.2 Port Credit Harbour will be the focus for tourism and economic development on the waterfront. The function and image of Port Credit as a centre for commercial activity and tourism will be enhanced and promoted. In addition, planning studies will consider the entire waterfront and identify other tourism and economic development opportunities.

7.6.2.3 All publicly owned land should be retained by the City or other levels of government, and contribute to public use and enjoyment of the Lake Ontario waterfront.

7.6.2.4 The review of applications for development along the Lake Ontario waterfront and the mouth of the Credit River, will have regard for the following:

- a. provision of public views of the lake from within and throughout the property;
- b. maintain existing or create new view corridors to the lake and along the shoreline;
- c. recognition, reflection, and integration of cultural heritage resources;
- d. provision of public access to and along the water's edge, in particular the waterfront trail

system (e.g., through the acquisition of parkland);

- e. potential to provide linkages for other trail systems, public access nodes, and ***natural features, areas and linkages including their ecological functions;***
- f. design of shoreline forms that do not pose physical barriers to the water;
- g. natural hazards;
- h. restoration and shoreline improvements;
- i. natural heritage features and their functions;
- j. opportunities for nature appreciation;
- k. compatibility among land uses;
- l. the privacy and security of private property;
- m. mix of appropriate uses;
- n. form and scale appropriate to the waterfront location;
- o. ensure that public open space adjacent to the shoreline is clearly seen to be open to the public;
- p. dedication of patent water lots to the City or appropriate public agency;
- q. provision of a variety of appropriate uses and activities which are lake dependent and lake enhanced; and



Figure 7-14: Lakefront Promenade is located on the Lake Ontario shoreline and is a major destination park.



- r. development of public shoreline parkland and the provision of associated recreational facilities.

7.6.2.5 Public Open Space and development adjacent to the Lake Ontario Waterfront Trail should be designed to enhance the trail user's experience of Lake Ontario by maximizing views of Lake Ontario and by creating a varied, visually stimulating, comfortable and human scaled edge to the waterfront trail.

7.6.2.6 The implementation of development proposals should enhance and promote the image and identity of Mississauga as a waterfront city with a unique waterfront advantage for development that will consider, among other uses, recreation, retail, cultural and tourism activities.

## 7.7 Urban Agriculture

Mississauga, like other cities around the world, is dependent on the importation of food to sustain its population. While this dependency is expected to continue, Mississauga supports a variety of urban agricultural practices that allow its residents access to freshly grown local produce, supports the surrounding agricultural community, enhances local food security, reduces the energy costs associated with transporting food great distances and achieves a more sustainable community.

7.7.1 Mississauga supports the following urban agricultural practices:

- a. urban gardening;
- b. community gardening; and
- c. the creation of rooftop gardens.

7.7.2 Farmers' markets will be encouraged particularly in Intensification Areas.



Figure 7-15: The Garden of the Valley, located in the Mississauga Valleys Character Area, is a community garden. It includes plots that can be rented and community plots that are gardened by volunteers. Each plot is approximately 3.0 m x 1.2 m and is gardened organically without the use of any pesticides, herbicides or chemical fertilizers. The gardens provide residents, with not only the opportunity to grow plants and vegetables, but also to socialize and network.

# 8 Create a Multi-Modal City

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## 8.1 Introduction

The City will create a multi-modal transportation network for the movement of people and goods that supports more sustainable communities. The multi-modal transportation system is composed of the following modes of travel:

- transit;
- vehicular (e.g., cars and trucks);
- **active transportation** (e.g., walking and cycling);
- rail (passenger and freight); and
- air travel (passenger and freight).

While vehicle trips will continue to account for a significant share of the total trips, the length of these trips should shorten in response to the creation of mixed use nodes that support the daily needs of surrounding residential and business communities, and the share of auto trips will be reduced as opportunities to travel by transit, cycling and walking improve.

Mississauga is evolving from a city that has a suburban, vehicle oriented built form to a more urban municipality. The transformation of the transportation system to meet the needs of the future is not without significant challenge. Mississauga's transportation infrastructure, which is largely built and relatively new, was designed around a grid of widely spaced major roads designed to move large volumes of vehicles efficiently. Within



Figure 8-1: Mississauga promotes a range of transportation modes. In addition to providing for the car, facilities for transit, cycling, and walking are a priority. Promoting a range of transportation choices will be particularly important in areas where intensification is encouraged, such as in the Downtown.



the grid are a series of collector roads and local streets where vehicles move at slower speeds and pedestrians and cyclists can safely share the roadway. Rail corridors, Provincial highways and the Airport link Mississauga to surrounding communities and beyond.

Much of the transportation system serving Mississauga is under the jurisdiction of other levels of government and agencies. This includes the Federal Government, the Province, Metrolinx, the Region, the Greater Toronto Airports Authority (GTAA) and private agencies, such as Canadian National Railway and the St. Lawrence and Hudson Railway. As such, Mississauga must coordinate and partner with others to create a multi-modal transportation system.

It is vital to preserve the capacity of the road system to meet the needs of Mississauga's population and employment growth as well the growth in surrounding communities that will utilize Mississauga's transportation system. At the same time it will be essential to provide more opportunities for car-pooling, transit and **active transportation** choices.

Although many of Mississauga's roads have generous right-of-way widths, it will not be possible to accommodate the needs of all modes of travel on all roads, nor will it be necessary to do so to create a multi-modal network. Strategic decisions will be made regarding which roads will be prioritized for different modes of travel.

While arterial roads will continue to move large volumes of traffic, the design of these thoroughfares must be sensitive to surrounding land uses. Arterial roads in employment areas will continue to prioritize goods movement, to support the vital role the transportation system plays in the economic health of the city. This will contrast with transportation priorities in Intensification Areas, where the needs of transit, pedestrians and cyclists will be in the forefront. In Intensification Areas, transportation decisions will support the creation of a fine grain street pattern, low traffic speeds, a mix of travel modes and attention to the design of the public realm.



Figure 8-2: **Higher order transit** is proposed along Hurontario Street and will complement intensification. The illustration shows the City's vision for **higher order transit** along Hurontario Street.

Improving connections from surrounding areas to Intensification Areas will also be a priority. These connections will focus on increasing opportunities for walking and cycling, which may result in consolidating vehicular entrances to support the creation of continuous building frontages with entranceways facing public streets and oriented to pedestrians.

Creating a multi-modal transportation system that supports transit and **active transportation** options goes hand-in-hand with creating compact, complete communities, and providing future generations with the opportunity to lead healthier, longer, more active lives. Transportation planning will complement environmental planning, land use planning and urban design.

8.1.1 Through the creation of a multi-modal transportation system, Mississauga will provide transportation choices that encourage a shift in lifestyle toward more sustainable transportation modes, such as transit and **active transportation**.

8.1.2 Mississauga will plan and manage the transportation system to provide for the safety of all users.

8.1.3 The City will strive to incorporate **stormwater best management practices** in the planning, design and construction of municipal road and off street parking facility projects. Decisions regarding the specific implementation of **stormwater best management practices** will be made on a project by project basis in accordance with relevant drainage plans and studies, and development standards and policies.

8.1.4 Mississauga will strive to create a transportation system that reduces dependence on non-renewable resources.

8.1.5 Mississauga will work in partnership with other levels of government and other agencies to support the reduction of transportation related greenhouse gas emissions.

8.1.6 Mississauga will ensure that the transportation system will provide connectivity among

transportation modes for the efficient movement of people and goods.

8.1.7 Mississauga will create a well connected multi-modal transportation system that prioritizes services and infrastructure for Intensification Areas.

8.1.8 To better utilize existing infrastructure, Mississauga will encourage the application of transportation demand management (TDM) techniques, such as car-pooling, alternative work arrangements and shared parking.

8.1.9 Mississauga will ensure that transportation corridors are identified and protected to meet current and projected needs for various travel modes.

8.1.10 Mississauga will separate transportation modes within transportation corridors, where appropriate.

8.1.11 Transit will be a priority for transportation infrastructure planning and major transportation initiatives.

8.1.12 Mississauga supports opportunities for multi-modal uses where feasible, in particular prioritizing transit and goods movement over those of single occupant vehicles.

8.1.13 Mississauga will coordinate transportation investments to implement the policies of this Plan.

8.1.14 Mississauga will work with other municipalities, levels of government and agencies to create a well connected, efficient, accessible, multi-modal transportation system.

8.1.15 The policies of Mississauga Official Plan apply to all transportation infrastructure under the jurisdiction of the City. Other levels of government and agencies should support the policies of this Plan through their transportation infrastructure investment decisions.

8.1.16 In reviewing development applications, Mississauga will require area wide or site specific transportation studies to identify the necessary transportation improvements to minimize conflicts between transportation and land use, and to ensure that development does not precede

necessary road, transit, cycling and pedestrian improvements. Transportation studies will consider all modes of transportation including auto traffic, truck traffic, transit, walking and cycling.

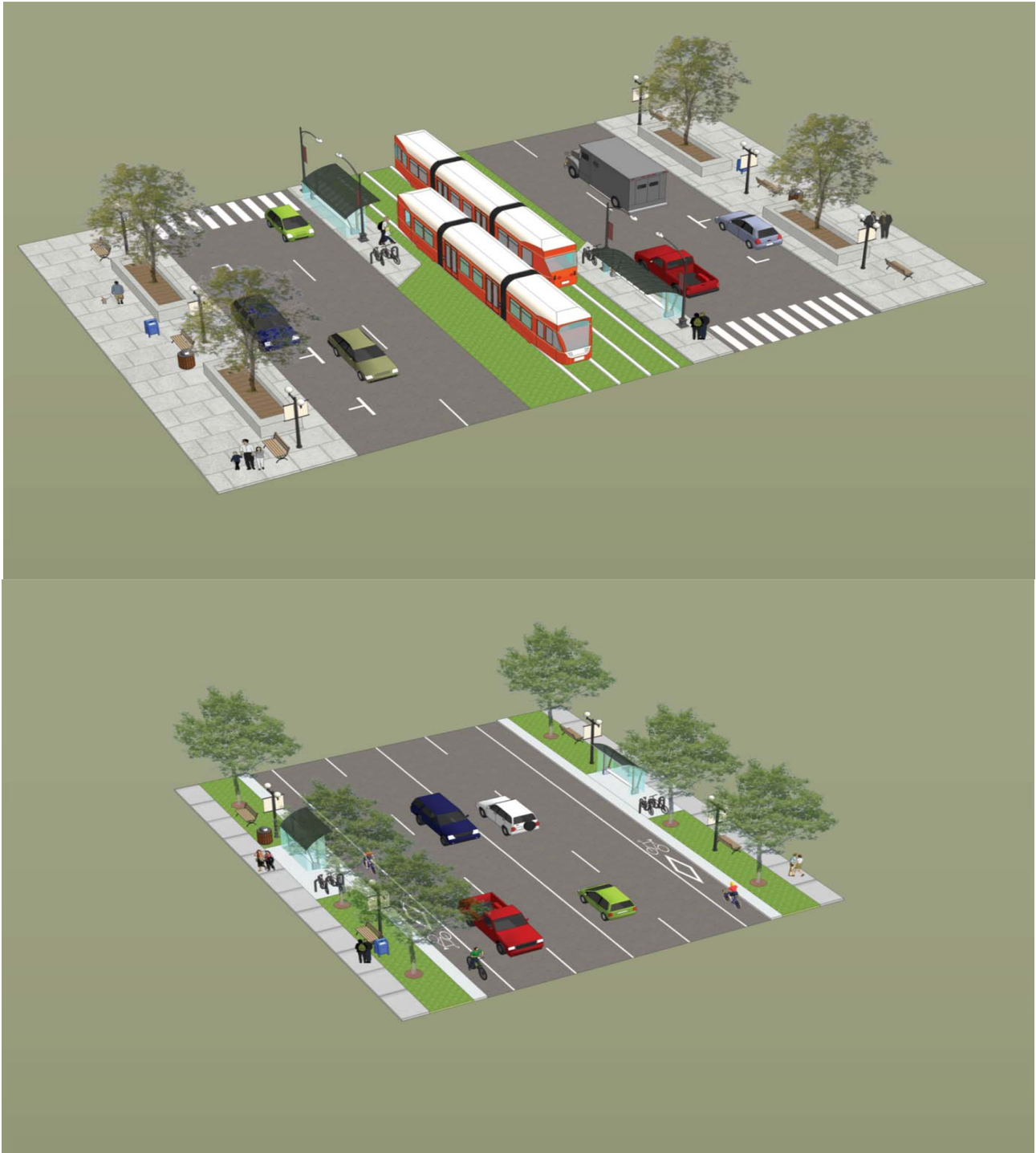


Figure 8-3: The ability to create multi-modal roadways will be influenced by right-of-way widths. Wider rights-of-way will allow for dedicated space for different transportation modes, however, where rights-of-way are narrower transportation modes will need to share space.



## 8.2 Multi-Modal Network

The creation of a multi-modal transportation system is important to Mississauga to enable the efficient movement of people and goods. The regional transportation system will primarily be accommodated on Provincial highways, rail and **higher order transit** corridors and at the Airport. The local transportation system will primarily be accommodated on City roads and multi-use trails. This system will focus on the day-to-day travel needs of those who live, work or play in Mississauga and will increasingly emphasize opportunities for transit and **active transportation**.

Schedules 5, 6 and 7 show the long term road, transit and cycling networks that will form the basis of the transportation system. Tables 8-1 to 8-4 provide the designated right-of-way widths based on the road classification system for City roads. Schedule 8 shows the designated right-of-way widths for arterial and major collector roads necessary to achieve the long term multi-modal transportation system under the City and Region's jurisdiction.

### 8.2.1 Corridor Protection

While this Plan focuses on promoting transit as a viable choice for the movement of people, there will still be a need for ongoing improvements to the road network to enhance mobility and accessibility for all users. The creation of new roads to support more compact development and increased traffic volumes associated with future growth, will also be required in some areas.

To support growth and to ensure the safe, efficient and environmentally responsible movement of people and goods, the City will protect for new roads and rights-of-way. Rights-of-ways may contain road surfaces, sidewalks, utilities, transit facilities, cycling routes, multi-use trails, **streetscape** works

and other uses such as public art and signage. Detailed design studies will determine which functions are accommodated within a particular right-of-way and the dimensions of those facilities within the right-of-way. The City may require land for the rights-of-way (including easements) or the widening of rights-of-way through conditions of approval for development applications.

8.2.1.1 The City's multi-modal transportation network will be maintained and developed to support the policies of this Plan by:

- a. protecting and developing the network rights-of-way by acquiring the additional property needed to achieve designated widths;
- b. designated right-of-way widths are considered the basic required rights-of-way along roadway sections. At intersections, grade separations or major physical topographical constraints, wider rights-of-way may be required to accommodate necessary features such as embankments, auxiliary lanes, additional pavement or sidewalk widths, transit facilities, cycling facilities or to provide for necessary improvements for safety in certain locations;
- c. providing an appropriate transition where there are different road classifications or right-of-way widths at municipal boundaries, in consultation with the respective municipalities;
- d. protecting land for future rail grade separations to support a safer and more efficient transportation system;
- e. requiring the conveyance of lands of abutting properties for widening as a condition of subdivision, severance, minor variance, condominium or site plan approvals, for nominal consideration; and
- f. working closely with partner transportation agencies, including the GTAA, to facilitate the protection or acquisition of future corridors or properties where potential land needs are identified.

8.2.1.2 The location and alignment of transportation facilities are conceptually shown on Schedules 5: Long Term Road Network, 6: Long Term Transit Network and 7: Long Term Cycling Routes. Location of future facilities will be determined through the appropriate studies.

8.2.1.3 Mississauga's multi-modal network includes road, transit, cycling and pedestrian facilities. In some locations transportation modes may share the same facility, however, to ensure the efficiency and safety of the transportation network and its users, transportation modes may, in other locations, use separate facilities.

8.2.1.4 Right-of-way widths are intended to accommodate the following:

- a. transit, including **higher order transit** corridors, transit stations and facilities along **higher order transit** corridors;
- b. vehicles (e.g., cars and trucks); and
- c. **active transportation** facilities.

8.2.1.5 Mississauga may acquire lands for a public transit right-of-way along **higher order transit** corridors, where the creation of a public transit right-of-way separate from, adjacent to, or in addition to, a road right-of-way is deemed appropriate.

8.2.1.6 Mississauga will promote the integration of transportation facilities to maximize opportunities for multi-modal travel.

## 8.2.2 Road Network

Mississauga's road network will strive to balance the needs of all users – transit, cyclists, pedestrians, goods movement and motorists. Roads will be classified on the basis of their primary role within the transportation network and to support the evolution of the city structure.

8.2.2.1 Mississauga's road network will consist of the following road classification:

- a. arterials will be designed as principal transportation corridors for high volumes of people and goods. Creation of new additional direct vehicle access to an arterial will be discouraged. The City may through negotiations seek to consolidate or eliminate direct vehicle access to arterials in order to improve traffic safety and the functioning of transit and pedestrian/cycling routes and to achieve operational objectives;
- b. major collectors in Neighbourhoods will be designed to accommodate moderate volumes of traffic and will be the focus of **active transportation** facilities. Vehicular access will be designed to minimize conflicts with **active transportation** modes. In Employment Areas, major collectors will be designed to serve a moderate volume of business and goods movement traffic. Vehicular access will be designed to support the efficient flow of goods movement traffic. Where possible, consolidation of access will be encouraged in neighbourhoods and employment areas. Local area plans may provide further guidance on vehicular access;
- c. minor collectors and local roads will be designed to accommodate low levels of traffic and to provide property access. To ensure safety, the efficient function of the thoroughfare and other matters, the access locations to private property will be controlled; and
- d. minor adjustments to the basic right-of-way widths and alignments for roads may be made without further amendment to this Plan subject to the City being satisfied that the role and function of such roads are maintained. Major adjustments to the basic right-of-way widths and alignments for roads will require an amendment to this Plan.

8.2.2.2 Mississauga will create a multi-modal road network through:

- a. a transportation system that provides mobility and accessibility to all users;
- b. opportunities for transit priorities;

- c. pedestrian and cycling access and routes; and
- d. priority truck routes for the efficient movement of goods.

8.2.2.3 Mississauga will strive to create a fine-grained system of roads that seeks to increase the number of road intersections and overall connectivity throughout the city.

8.2.2.4 The creation of a finer grain road pattern will be a priority in Intensification Areas.

8.2.2.5 Additional roads may be identified during the review of development applications and the preparation of local area plans. The City may require the completion of road connections and where appropriate, the creation of a denser road pattern through the construction of new roads.

8.2.2.6 The subdivision of lands will not be permitted if the City requires public ownership of the lands for pedestrian, cycling or vehicular access to create local road connections to existing developed or undeveloped lands.

8.2.2.7 Future additions to the road network should be public roads. Public easements may be required where private roads are permitted.

8.2.2.8 Permanent below or at grade encroachments into the road system will not be permitted, however above grade amenities such as canopies/awnings may be considered.

## 8.2.3 Transit Network

Mississauga's transit network forms part of the interregional transportation system and is intended to both shape and support future growth in the city. To achieve this, the transit network will be centred on a system of linked regional and local mobility hubs, mixed use nodes and key destinations where major trip generating uses will be encouraged to locate.

The City will work with other transit providers and agencies such as Metrolinx to promote transit as the preferred choice for moving people, particularly during peak travel times in the city and region.

Mississauga Official Plan promotes **active transportation** and the development of Community Nodes to reduce the need to travel great distances by car in fulfilling one's daily needs.

The transit network will be supported by compact, pedestrian oriented, mixed land use development in nodes and where appropriate, in mobility hubs and along **Corridors**.

Implementation measures such as transit priority and alternative on demand service providers will be considered to promote transit as a preferred transportation option that is accessible to people of all abilities.



Figure 8-4: **Higher order transit** such as the Highway 403/Eglinton Bus Rapid Transit will provide competitive alternatives to the automobile.





Figure 8-5: Various transportation forms exist within the city. The transit network is extensive and serves the large resident population and employment base, as well as those passing through the city.

8.2.3.1 Mississauga will seek to develop and maintain a system of transit services aimed at providing a competitive alternative to the automobile, for access throughout the city and neighbouring municipalities.

8.2.3.2 Mississauga will operate a network of local grid services on major roadways and local feeder routes, which are connected at key transit terminals and commuter rail stations.

8.2.3.3 Mississauga Transit will connect to commuter rail services operated by GO Transit that provide access to downtown Toronto and other destinations within the region.

8.2.3.4 The City will initiate express transit on **Intensification Corridors** and will continue to employ express services as part of the implementation of the Mississauga Bus Rapid Transit.

8.2.3.5 The City will construct the Bus Rapid Transit along the Highway 403/Eglinton Avenue corridor as the east-west spine within Mississauga, to form part of a regional transit system in accordance with the Metrolinx Regional Transportation Plan.

8.2.3.6 To create a city wide transit grid network, Mississauga will decentralize existing transit services away from the Downtown Core and connect bus rapid transit stations to other Intensification Areas.

8.2.3.7 The Downtown will be served by local and **higher order transit** facilities, which provide connections to neighbouring municipalities. The City

will work with surrounding municipalities, the Region, the Greater Toronto Airports Authority and the Province to create an interconnected **higher order transit** system that links Intensification Areas, surrounding municipalities, the regional transit system and the Airport.

8.2.3.8 Decisions on transit planning and investment will be made according to the following criteria:

- a. using transit infrastructure to shape growth, and planning for high residential and employment densities that ensure the efficiency and viability of existing and planned transit service levels;
- b. placing priority on increasing the capacity of existing transit systems to support Intensification Areas;
- c. expanding transit service to areas that have achieved, or will be planned to achieve, transit supportive residential and employment densities, together with a mix of residential, office, institutional and commercial development, wherever possible;
- d. providing priority access to the Downtown, other Intensification Areas and the Airport; and
- e. increasing the modal share of transit.

8.2.3.9 Access to transit will be provided within walking distance of the places where people live and work, and of major destinations such as the Lake Ontario waterfront.

8.2.3.10 Accessible transit facilities and passenger amenities, such as bus bays, bus loops, bus stop platforms and shelters, will be acquired through the processing of development applications, where appropriate.

## 8.2.4 Active Transportation

The City will continue to develop an integrated cycling network, to make cycling a more viable choice for commuting. Decisions regarding the detailed characteristics and development of primary and secondary cycling routes will be guided by the Cycling Master Plan.

**Active transportation** facilities will address the needs of individuals with disabilities, including those who require mobility assisted devices such as, walkers, wheelchairs and scooters.

To encourage **active transportation** and support the development of healthy communities, the City will promote pedestrian activity as an integral part of the multi-modal transportation network.

The purpose of Schedule 7: Long Term Cycling Routes is to connect key city destinations and locations, such as Major Transit Stations, with cycling routes and provide cycling linkages to adjacent municipalities. The cycling facilities shown on Schedule 7 consist of Primary Off-Road Routes, Primary On-Road / Boulevard Routes, Primary On-

Road / Boulevard Routes (Regional), Crossings and Connections to Adjacent Municipalities in accordance with the Cycling Master Plan.

8.2.4.1 Bicycle racks and bicycle storage facilities will be provided at transit terminals.

8.2.4.2 Mississauga will protect and may acquire the lands required for the cycling facilities shown on Schedule 7: Long Term Cycling Routes, through the development approval process and capital works program.

8.2.4.3 Proponents of development applications, will be required to demonstrate how pedestrian and cycling needs have been addressed.

8.2.4.4 Mississauga will require that access, and parking facilities and other destination amenities, such as shower facilities and clothing lockers for cyclists, are incorporated into the design of all buildings and **Major Transit Station Areas**, as appropriate.

8.2.4.5 Sidewalks or multi-use trails will be provided on all new roads.

8.2.4.6 Sidewalks or multi-use trails in the vicinity of all transit stops will be provided.

8.2.4.7 Sidewalks or multi-use trails and pedestrian amenities will be a priority in Intensification Areas.

8.2.4.8 Mississauga will provide pedestrian connections to Intensification Areas.



Figure 8-6: People often use multiple modes of transportation in their daily commute. Supplying bike racks on buses is one example of how Mississauga supports cycling.

## 8.3 Transportation Infrastructure Design

Mississauga will implement a range of transportation infrastructure design and management measures to optimize the operational safety and efficiency of the multi-modal transportation system.

### 8.3.1 Road Design

The City will ensure that the design of roads promotes safety and comfort for all users. Roads will also be designed to complement and minimize impacts to adjacent land uses and communities.

8.3.1.1 The City will design its roads in a manner that:

- a. has regard for the safe movement of all road users, including transit, cyclists, pedestrians and motorists;
- b. is context sensitive having regard for existing and planned land uses, urban design, community needs and funding availability;
- c. minimizes the disruption to the Natural Areas System and preserves, where appropriate, existing tree canopies; and
- d. is sensitive to local cultural heritage resources.

8.3.1.2 Within Intensification Areas and Neighbourhoods, the design of roads and **streetscapes** will create a safe, comfortable and attractive environment for pedestrians, cyclists and motorists by:

- a. reducing lane width, where appropriate;
- b. providing streetscaping to reduce the apparent width of the right-of-ways;
- c. locating sidewalks and cycling facilities where conflicts with motorized traffic are minimized; and
- d. creating safe road crossings for pedestrians and cyclists.

8.3.1.3 Where feasible and appropriate, the widths of lanes dedicated to vehicular traffic may be reduced to accommodate transit facilities and pedestrians, enhance **streetscapes** and pedestrian and cycling facilities.

8.3.1.4 Mississauga will ensure that any maintenance or physical modification of **scenic routes** reinforces or enhances the “scenic route

qualities” of roadways classified as **scenic routes**. If major modifications are expected to have an adverse impact on these qualities, an amendment to this Plan will be required. Standard road improvements or general road maintenance that are necessary to support traffic safety will be permitted without amendment to this Plan. **Scenic routes** are shown on Schedule 5: Long Term Road Network.

8.3.1.5 Roads may be widened to accommodate transit, cycling and pedestrian facilities and to provide additional through lanes in Employment Areas if deemed essential to goods movement. Elsewhere, additional through lanes on existing roads will be considered on an exceptional basis only and will be subject to special study.

### 8.3.2 Transit Design

The design and management of transit facilities will employ a variety of techniques, which consider the convenience and comfort of transit users, to promote transit as a primary mover of people.

8.3.2.1 Mississauga will employ transit priority measures on priority corridors shown on Schedule 6: Long Term Transit Network, such as queue jump lanes and transit signal priority, along with express services, new intelligent transportation systems (ITS), fare integration, and service coordination with GO Transit and neighbouring transit systems.

8.3.2.2 **Major Transit Station Areas** will be planned and designed to provide access from various transportation modes to the transit facility, including consideration of pedestrian, bicycle parking and commuter pick-up/drop-off areas.

### 8.3.3 Cycling and Pedestrian Design

The design of cycling and pedestrian transportation facilities will focus on enhancing safety, particularly when located within the road right-of-way and will provide greater integration with transit.

8.3.3.1 The incorporation of cycling facilities will be considered in the construction of new roads and the rehabilitation and reconstruction of existing roadways, through the following measures:

- a. re-striping roadways for bicycle lanes;
- b. introducing multi-use trails or bicycle paths on boulevards;
- c. using wider shared curb lanes for bicycles; and
- d. widening roadways to accommodate bicycle lanes.

8.3.3.2 Mississauga will seek to optimize the efficiency of the network with measures such as intersection improvements, operational improvements and traffic signal optimization.

8.3.3.3 Pedestrian movement and access from major transit routes will be a priority in Intensification Areas.

8.3.3.4 Pedestrian convenience and safety will be a priority in determining location and design of transit facilities within Intensification Areas.

## 8.4 Parking

Parking can shape land use patterns, support good urban design, promote economic development, and influence travel behaviours and choice of transportation modes.

As Mississauga continues to grow and develop, less land will be devoted to parking, particularly within Intensification Areas. The parking that is provided should increasingly be in structured - preferably underground - parking facilities and on-street where it can be shared amongst multiple users.

In other parts of the city, while some changes to parking provisions may occur, sufficient parking should be provided to ensure that the established residential character of Neighbourhoods and the economic function of employment uses is not adversely affected.

8.4.1 Off-street parking facilities for vehicles and other modes of travel, such as bicycles, will be provided in conjunction with new development and will:

- a. provide safe and efficient access from the road network so that ingress and egress movements minimize conflicts with road traffic and pedestrian movements;
- b. provide for the needs of people with disabilities; and
- c. support transportation demand management (TDM) initiatives.

8.4.2 Mississauga will encourage the shared use of parking and allow off-site parking, where appropriate.

8.4.3 Consideration will be given to reducing off-street parking requirements for development to reflect levels of vehicle ownership and usage, and as a means of encouraging the greater use of transit, cycling and walking, subject to, among other matters:

- a. access to transit;
- b. level of transit service;
- c. traffic generation; and
- d. impact on the surrounding area.



Figure 8-7: Parking garages are a better use of space than surface parking and provide an opportunity to incorporate a mix of uses, as this parking garage in Burlington illustrates.



8.4.4 Mississauga may require or consider receiving a cash payment-in-lieu of all, or part, of the zoning by-law requirements for parking, having regard for:

- a. the objectives of municipal parking strategies;
- b. the advancement of environmental, design, transportation or economic development objectives and policies of this plan;
- c. the presence of site constraints that prevent the provision of the required number of on-site parking spaces;
- d. property use that is not considered overdevelopment; and
- e. areas where municipal parking facilities are available and the existing parking supply within proximity of the subject site can accommodate the on-site parking deficiency.

8.4.5 In situations where a significant number of required parking spaces are being provided through payment-in-lieu, in an area where limited or no municipal parking facilities are available, Mississauga will have regard for:

- a. an identified municipal interest in providing public parking facilities in the area;
- b. the timing for the delivery of the municipal parking facilities;
- c. the adequacy of alternatives to on-site parking until municipal parking facilities are delivered;
- d. the effect the on-site parking deficiency would have on the viability of the site and the impact on the surrounding area; and
- e. the number of spaces proposed to be considered for payment-in-lieu as it relates to the magnitude of municipal interest.

8.4.6 Street designs will consider opportunities to maximize on-street parking. The provision of on-street parking will be balanced with the needs of other modes of transportation sharing the right-of-way.

8.4.7 Within Intensification Areas, Mississauga will give consideration to:

- a. reducing minimum parking requirements to reflect transit service levels;
- b. establishing maximum parking standards to support transit investments, particularly **higher order transit** investments;
- c. limiting surface parking by requiring a portion be provided within structured parking facilities;
- d. requiring structured parking facilities to be underground, where viable;
- e. proactively maximizing on-street public parking in appropriate locations;
- f. coordinating parking initiatives with transportation demand management (TDM) programs in order to effectively link transit planning, parking and other related issues in a comprehensive manner; and
- g. requiring parking phasing and implementation plans that, among other matters, will include a surface parking reduction strategy that will ensure the layout of the parking lot and buildings will allow for future development.

8.4.8 Mississauga may develop municipal parking facilities to support transit, provide shared parking and encourage development.

8.4.9 In appropriate locations, Mississauga will take an active role in providing off-street parking. The City may partner with private developers to deliver municipal parking facilities that will be used as a shared public resource, through the use of payment-in-lieu of off-street parking and/or site specific joint ventures. Investment in public parking facilities should be directed to projects that achieve the following objectives:

- a. provide strategically located public parking structures that can serve a variety of uses;
- b. serve development within a proposed **higher order transit** corridor;

- c. provide an appropriately sized structure considering economies of scale, efficiency of structure, character of the area and financial aspects;
- d. allow for the consolidation of pre-existing surface lots to encourage intensification;
- e. make efficient use of publicly owned land;
- f. integrate commercial uses into the ground level façade for above grade structures;
- g. allow for integration of community infrastructure;
- h. provide for convenient pedestrian linkages to, from and through the parking structure to connect with surrounding development; and
- i. consider temporary surface parking lots to secure strategic locations for future public parking structures.

8.4.10 In some circumstances, the City may consider allowing the use of municipal parking facilities to meet or reduce the parking requirements for cultural facilities where it does not impair the functioning of other uses or the economic viability of the area.

8.4.11 Development within and adjacent to Neighbourhoods will mitigate parking impacts on the residential use.

8.4.12 Mississauga will discourage parking in neighbourhoods on local streets for non-residential purposes.

## 8.5 Transportation Demand Management

Transportation demand management (TDM) measures encourage people to take fewer and shorter vehicle trips to support transit and **active transportation** choices, enhance public health and reduce harmful environmental impacts. TDM is



Figure 8-8: High Occupancy Vehicle (HOV) lanes such as those on Highway 403, encourage people to carpool or take transit.

most effective when supported by complementary land use planning, good urban design and transit improvements.

8.5.1 Mississauga will encourage TDM strategies that promote transit use and **active transportation**, and reduce vehicle dependency, single occupant vehicle travel, trip distance and time and peak period congestion.

8.5.2 Mississauga will work with other levels of government, agencies and the private sector to encourage TDM measures.

8.5.3 Mississauga will encourage employers to implement TDM programs.

8.5.4 Mississauga will manage parking in Intensification Areas to encourage the use of alternative modes of transportation and the reduction of vehicular congestion.

8.5.5 Mississauga will encourage land uses permitted by this Plan that make efficient use of the transportation system and parking facilities during off-peak hours.

8.5.6 In appropriate areas, Mississauga will encourage a fee for parking and the separation of parking costs from other costs, such as transit fares, building occupancy and residential unit prices.

8.5.7 Prior to approval of development applications, particularly those that will generate significant employment opportunities, a TDM plan may be required that demonstrates, among other things, the following:

- a. building orientation that supports transit service;
- b. minimize distance between main building entrances and transit stations/stops;
- c. development that is integrated into the surrounding pedestrian and cycling network;
- d. parking facilities designed to provide safe and efficient access for pedestrians and cyclists emanating from the surrounding transit and **active transportation** network;
- e. secure, conveniently located, weather protected, on-site bicycle storage facilities, and associated amenities such as showers, change rooms and clothing lockers;
- f. reserved, priority car-pool parking spaces and, where applicable, car-share spaces and taxi stands;

- g. parking spaces for scooters, motorcycles and other similar motorized vehicles;
- h. techniques to manage the supply of on-site parking; and
- i. measures that:
  - increase the proportion of employee trips made by transit, walking and cycling;
  - increase the average car occupancy rate;
  - reduce the demand for vehicular travel; and
  - shift travel times from peak to off-peak periods.

8.5.8 Car-pooling will be encouraged through the provision of High Occupancy Vehicle Lanes, priority parking, and other measures as appropriate.

8.5.9 Further TDM policies may be identified through a Transportation Master Plan.



Figure 8-9: The Downtown Core Mobility Hub is an example of where people can live, work, shop and recreate in a mixed use environment supported by transit.

## 8.6 Mobility Hubs

Mobility hubs have employment, housing, shopping and recreational uses concentrated around a **Major Transit Station Area** and are connected by a variety of modes of transportation such as walking, cycling, and regional and local transit.

Mobility hubs include both **gateway** and **anchor hubs** as shown on Schedule 6: Long Term Transit Network.

8.6.1 Mississauga will promote the development of land use and transportation facilities around **anchor hubs** and **gateway hubs** in a manner that supports the Metrolinx Regional Transportation Plan.

8.6.2 Mobility hubs will be planned and designed to provide access from various transportation modes to the transit station, including consideration of pedestrians, bicycle parking and commuter pick-up/drop-off areas.

8.6.3 Mobility hubs may be required to provide amenities such as secure storage facilities for bicycles, car-share drop-off areas, heated waiting areas, traveller information centres, cafes and restaurants, as well as services such as daycares,

grocery stores or post offices.

8.6.4 Access to mobility hubs and **Major Transit Station Areas** will be promoted through the provision of pedestrian and cycling linkages, transit and adequate commuter parking facilities, and the potential for development of structured parking.

## 8.7 Goods Movement

Efficiently moving goods is critical to the economic health of the city. Mississauga will develop a transportation network to support its significant role as a goods movement hub.

In some locations, particularly in certain Employment Areas surrounding the Airport, goods movement will be the priority of the transportation system.

8.7.1 Mississauga will integrate land use and transportation system planning to promote and better integrate multi-modal goods movement.

8.7.2 Activities generating substantial truck traffic will be encouraged to locate near or adjacent to provincial highways and arterial roads.



Figure 8-10: Several 400 series highways and major roads traverse Mississauga and support the many businesses reliant on efficient goods movement.



8.7.3 Mississauga will encourage strategic linkages to inter-modal facilities and 400 series highways to facilitate the efficient movement of goods.

8.7.4 A denser grid of roads will be established where required in Employment Areas to support the efficient movement of goods.

8.7.5 In the Northeast Employment Area, priority will be for road improvements that support goods movement. Planning studies may identify other areas where goods movement will be a priority for road improvements.

8.7.6 Mississauga will support priority truck routes through road design.

8.7.7 Arterials and major collectors will serve as truck routes. Minor collectors in Employment Areas may serve as truck routes.

8.7.8 Mississauga will work with the Province and Region to coordinate and optimize systems of moving goods.

8.7.9 To support the 400 series highways as part of the provincial goods movement network, Mississauga will work with the Province to pursue opportunities to provide additional connections at interchanges, and necessary highway improvements, at key locations including:

- a. Hurontario Street and Provincial Highway 401;
- b. Hurontario Street and Provincial Highway 407;
- c. Mavis Road and Provincial Highway 401;
- d. Centreview Drive and Provincial Highway 403;
- e. Provincial Highway 401 E/B off-ramp (west of Etobicoke Creek);
- f. widening of Provincial Highway 401 from its interchange with Provincial Highway 410 to the western limit of Mississauga;
- g. widening of Provincial Highway 410 from its interchange with Provincial Highway 401 to the northern limit of Mississauga;
- h. completion of a partial interchange at Courtney Park Drive and Provincial Highway 410, to provide access to and from the north;
- i. construction of a partial interchange at Provincial Highway 401 in the vicinity of the Etobicoke Creek, to service the area to the north; and
- j. improvements to the Dixie Road and Queen Elizabeth Way interchange.



Figure 8-11: The rail corridors in Mississauga are shared by both freight and passenger trains, such as the GO train depicted above. The City recognizes these corridors as assets in the transportation system.

## 8.8 Rail Corridors

Passenger and freight rail services are an important element of the transportation system for Mississauga and the surrounding region.

8.8.1 In planning for new or existing transportation corridors, Mississauga will consider increased opportunities for moving people and goods by rail, where appropriate.

8.8.2 Mississauga will cooperate with other levels of government and the railway companies in locating, planning, and designing new freight and passenger terminals, to ensure that such facilities are compatible with the transportation network and land use.

8.8.3 Mississauga will cooperate with the appropriate authorities to provide adequate provision for safety in the planning, design and operation of rail facilities.

8.8.4 The City will continue to construct road/rail grade separations to support a safe and efficient transportation system, and to maintain an adequate level of service on the road network.

The following have been identified as priority needs:

- a. Torbram Road and Canadian National Railway

(CNR) (north);

- b. Torbram Road and CNR (south);
- c. Goreway Drive and CNR;
- d. Drew Road Extension and CNR;
- e. Erindale Station Road and St. Lawrence and Hudson Railway;
- f. Wolfedale Road and St. Lawrence and Hudson Railway;
- g. Ninth Line and St. Lawrence and Hudson Railway; and
- h. Tenth Line and St. Lawrence and Hudson Railway.

8.8.5 Mississauga will continue to seek financial assistance from other levels of government for the provision of road/rail grade separations.

## 8.9 Airport

Canada's largest airport is a major transportation facility and destination within Mississauga, serving an important regional, national and international role.



Figure 8-12: The Airport supports the local and regional economy and is a significant component in the city's transportation network.

8.9.1 Mississauga will work with the GTAA and other stakeholders to ensure improved transit connections to the Airport and surrounding employment lands.

8.9.2 Mississauga will support goods movement access to the Airport to promote the Airport as a key goods movement hub.

## Table 8-1: Road Classification – Arterials

Street	From	To	Jurisdiction	R-O-W*
Airport Rd.	North City boundary	Highway 427	Peel	45 m
Britannia Rd. W.	Ninth Line	Erin Mills Pkwy.	Peel	36 m
Britannia Rd. W.	Erin Mills Pkwy.	Approximately 200 m west of Mississauga Rd.	Peel	40 m
Britannia Rd. W.	Approximately 200 m west of Mississauga Rd.	Credit River	Peel	36 m
Britannia Rd. W.	Credit River	Mavis Rd.	Peel	45 m
Britannia Rd. W.	Mavis Rd.	Hurontario St.	Peel	43.5 m
Burnhamthorpe Rd. W.	Ninth Line	Erin Mills Pkwy.	Mississauga	35 m
Burnhamthorpe Rd. W.	Erin Mills Pkwy.	Confederation Pkwy.	Mississauga	50 m
Burnhamthorpe Rd. W.	Confederation Pkwy.	Hurontario St.	Mississauga	60 m
Burnhamthorpe Rd. E.	Hurontario St.	Arista Way	Mississauga	60 m
Burnhamthorpe Rd. E.	Arista Way	Etobicoke Creek	Mississauga	50 m
Cawthra Rd.	Eastgate Pkwy.	Burnhamthorpe Rd. E.	Peel	45 m
Cawthra Rd.	Burnhamthorpe Rd. E.	Silver Creek Blvd.	Peel	36 m
Cawthra Rd.	Silver Creek Blvd.	Queensway E.	Peel	40 m
Cawthra Rd.	Queensway E.	Lakeshore Rd. E.	Peel	36 m
Courtneypark Dr. W.	Mavis Rd.	Hurontario St.	Mississauga	35 m
Courtneypark Dr. E.	Hurontario St.	Netherhart Rd.	Mississauga	35 m
Future Arterial / Creekbank Rd.	Highway 401	Eglinton Ave. E.	Mississauga	30 m
Derry Rd. W.	Ninth Line	Argentia Rd.	Peel	36 m
Derry Rd. W.	Argentia Rd.	Hurontario St.	Peel	45 m
Derry Rd. E.	Hurontario St.	Highway 427	Peel	45 m
Dixie Rd.	North City boundary	Rometown Dr.	Peel	45 m
Dundas St. W.	Ninth Line	Highway 403	Mississauga	42 m
Dundas St. W.	Highway 403	Mindemoya Rd.	Mississauga	35 m
Dundas St. W.	Mindemoya Rd.	Proudfoot St.	Mississauga	30 m
Dundas St. W.	Proudfoot St.	Hurontario St.	Mississauga	35 m
Dundas St. E.	Hurontario St.	Etobicoke Creek	Mississauga	35 m
Eastgate Pkwy.	Cawthra Rd.	Dixie Rd.	Mississauga	67 m
Eastgate Pkwy.	Dixie Rd.	Fieldgate Dr.	Mississauga	50 m



<b>Street</b>	<b>From</b>	<b>To</b>	<b>Jurisdiction</b>	<b>R-O-W*</b>
Eastgate Pkwy.	Fieldgate Dr.	Eglinton Ave. E.	Mississauga	65 m
Eglinton Ave. W.	Ninth Line	Winston Churchill Blvd.	Mississauga	30 m
Eglinton Ave. W.	Winston Churchill Blvd.	Erin Mill Pkwy.	Mississauga	40 m
Eglinton Ave. W.	Erin Mills Pkwy.	Hurontario St.	Mississauga	45 m
Eglinton Ave. E.	Hurontario St.	Eastgate Pkwy.	Mississauga	45 m
Eglinton Ave. E.	Eastgate Pkwy.	Etobicoke Creek	Mississauga	65 m
Eglinton Ave. W.	Etobicoke Creek	East City boundary	Toronto	50 m
Erin Mills Pkwy.	Turner Valley Rd. / Mississauga Rd.	Queen Elizabeth Way	Peel	45 m
Finch Ave.	C.N.R. tracks	Highway 427	Peel	36 m
Hurontario St.	North City boundary	Highway 403	Mississauga	45 m
Hurontario St.	Highway 403	Elm Dr.	Mississauga	50 m
Hurontario St.	Elm Dr.	St. Lawrence & Hudson Railway tracks	Mississauga	45 m
Hurontario St.	St. Lawrence & Hudson Railway tracks	Queen Elizabeth Way	Mississauga	35 m
Hurontario St.	Queen Elizabeth Way	Lakeshore Rd.	Mississauga	30 m
Lakeshore Rd. W.	Winston Churchill Blvd.	Southdown Rd.	Mississauga	35 m
Lakeshore Rd. W.	Southdown Rd.	Approximately 25 m east of Crozier Crt.	Mississauga	35 m
Lakeshore Rd. W.	Approximately 25 m east of Crozier Crt.	Hurontario St.	Mississauga	26 m
Lakeshore Rd. E.	Hurontario St.	Seneca Ave.	Mississauga	26 m
Lakeshore Rd. E.	Seneca Ave	Greaves Ave.	Mississauga	30 m
Lakeshore Rd. E.	Greaves Ave.	Etobicoke Creek	Mississauga	35 m
Mavis Rd.	North City boundary	Highway 401	Mississauga	35 m
Mavis Rd.	Highway 401	Highway 403	Mississauga	40 m
Mavis Rd.	Highway 403	Queensway W.	Mississauga	35 m
Mississauga Rd.	North City boundary	Turner Valley Rd.	Peel	45 m
Netherhart Rd. / Future Arterial	Courtneypark Dr. E.	Highway 401	Mississauga	35 m
Ninth Line	Highway 401	Highway 403	Mississauga	35 m
Ninth Line	Highway 403	Dundas St. W.	Halton	35 m
Queensway W.	Mavis Rd.	Hurontario St.	Peel	36 m
Queensway E.	Hurontario St.	Etobicoke Creek	Peel	45 m
Royal Windsor Dr.	Winston Churchill Blvd.	Southdown Rd.	Mississauga	35 m
Southdown Rd.	Queen Elizabeth Way	Lakeshore Rd. W.	Mississauga	35 m

<b>Street</b>	<b>From</b>	<b>To</b>	<b>Jurisdiction</b>	<b>R-O-W*</b>
Winston Churchill Blvd.	North City boundary	Dundas St. W.	Mississauga	35 m
Winston Churchill Blvd.	Dundas St. W.	North Sheridan Way	Peel	45 m
Winston Churchill Blvd.	North Sheridan Way	Bromsgrove Rd.	Peel	36 m
Winston Churchill Blvd.	Bromsgrove Rd.	Royal Windsor Dr.	Peel	45 m
Winston Churchill Blvd.	Royal Windsor Dr.	Lakeshore Rd. W.	Peel	36 m

\* These are considered basic rights-of-way. At intersections, grade separations or major physical topographical constraints, wider rights-of-way may be required to accommodate necessary features such as embankments, auxiliary lanes, additional pavement or sidewalk widths, transit facilities, cycling facilities, or to provide for necessary improvements for safety in certain locations.

## Table 8-2: Road Classification – Major Collectors

Street	From	To	Jurisdiction	R-O-W*
Aquitaine Ave.	Tenth Line W.	Millcreek Dr.	Mississauga	26 m
Argentia Rd.	Ninth Line	Creditview Rd.	Mississauga	26 m
Atwater Ave.	Mineola Gdns.	Ogden Ave.	Mississauga	20 m
Avebury Rd.	Britannia Rd. W.	Matheson Blvd.	Mississauga	30 m
Battleford Rd.	Tenth Line	Erin Mills Pkwy.	Mississauga	26 m
Belgrave Rd.	Highway 401 at Mavis Rd. interchange R-O-W	Cantay Rd.	Mississauga	30 m
Bloor St.	Central Pkwy. E.	Dixie Rd.	Mississauga	26 m
Bloor St.	Dixie Rd.	Etobicoke Creek	Mississauga	30 m
Bramalea Rd.	North City boundary	Derry Rd. E.	Mississauga	30 m
Bristol Rd. W.	Credit River	Approximately 55 m east of Albert St.	Mississauga	20 m
Bristol Rd. W.	Approximately 55 m east of Albert St.	Creditview Rd.	Mississauga	26 m
Bristol Rd. W.	Creditview Rd.	Hurontario St.	Mississauga	30 m
Bristol Rd. E.	Hurontario St.	Kennedy Rd.	Mississauga	30 m
Britannia Rd. E.	Hurontario St.	Kennedy Rd.	Mississauga	26 m
Abilene Dr. / Britannia Rd. E. (Future Major Collector-conceptual)	Kennedy Rd.	Highway 410	Mississauga	26 m
Britannia Rd. E.	Highway 410	Tomken Rd.	Mississauga	26 m
Britannia Rd. E.	Tomken Rd.	Netherhart Rd. / Future Arterial	Mississauga	26 m
Camilla Rd.	Dundas St. E.	King St. E.	Mississauga	26 m
Cantay Rd.	Mavis Rd.	Britannia Rd. W.	Mississauga	30 m
Capston Dr.	Kateson Rd.	Hurontario St.	Mississauga	26 m
Central Pkwy. W.	Burnhamthorpe Rd. W.	Mavis Rd.	Mississauga	26 m
Central Pkwy. W.	Mavis Rd.	Hurontario St.	Mississauga	30 m
Central Pkwy. E.	Hurontario St.	Rathburn Rd. E.	Mississauga	35 m
Central Pkwy. E.	Rathburn Rd. E.	Highway 403	Mississauga	30 m
Central Pkwy. E.	Highway 403	Eglinton Ave. E.	Mississauga	26 m
Centre View Dr.	Mavis Rd.	Approximately 600 m east of Mavis Rd.	Mississauga	30 m
Centre View Dr.	Approximately 600 m east of Mavis Rd.	Station Gate Rd.	Mississauga	50 m

<b>Street</b>	<b>From</b>	<b>To</b>	<b>Jurisdiction</b>	<b>R-O-W*</b>
Centre View Dr.	Station Gate Rd.	Rathburn Rd. W.	Mississauga	30 m – 50 m
Clarkson Rd. N.	South Sheridan Way	Lakeshore Rd. W.	Mississauga	22 m
Confederation Pkwy.	Eglinton Ave. W.	Highway 403	Mississauga	30 m
Confederation Pkwy.	Highway 403	Webb Dr.	Mississauga	40 m
Confederation Pkwy.	Webb Dr.	King St. W.	Mississauga	30 m
Confederation Pkwy.	King St. W.	Queensway W.	Mississauga	26 m
Creditview Rd.	Derry Rd. W.	Eglinton Ave. W.	Mississauga	30 m
Creditview Rd.	Eglinton Ave. W.	Burnhamthorpe Rd. W.	Mississauga	26 m
Proposed east-west road opposite Top Flight Dr.	Derrycrest Dr.	Hurontario St.	Mississauga	30 m
Derrycrest Dr.	Proposed east-west road opposite Top Flight Dr.	Derry Rd. W.	Mississauga	30 m
Dixie Rd. (Scenic Route)	Rometown Dr.	Lakeshore Rd. E.	Peel	20 m
Drew Rd.	Tomken Rd.	Airport Rd.	Mississauga	26 m
Duke of York Blvd.	North 403 Major Collector Rd.	Webb Dr.	Mississauga	27.5 m
Edwards Blvd.	North City boundary	World Dr.	Mississauga	26 m
Erin Centre Blvd.	Tenth Line	Winston Churchill Blvd.	Mississauga	26 m
Erin Centre Blvd.	Winston Churchill Blvd.	Erin Mills Pkwy.	Mississauga	30 m
Erin Centre Blvd.	Erin Mills Pkwy.	Mississauga Rd.	Mississauga	26 m
Erindale Station Rd.	Central Pkwy. W.	Dundas St. W.	Mississauga	26 m
Financial Dr.	North City boundary	Derry Rd. W.	Mississauga	30 m
Fowler Dr.	Lincoln Green Way	North Sheridan Way	Mississauga	20 m
Fowler Dr.	North Sheridan Way	Erin Mill Pkwy.	Mississauga	26 m
Glen Erin Dr.	Derry Rd. W.	Britannia Rd. W.	Mississauga	26 m
Glen Erin Dr.	Britannia Rd. W.	Eglinton Ave. W.	Mississauga	30 m
Glen Erin Dr.	Eglinton Ave. W.	Burnhamthorpe Rd. W.	Mississauga	26 m
Glen Erin Dr.	Burnhamthorpe Rd. W.	Dundas St. W.	Mississauga	30 m
Goreway Dr.	North City boundary	Derry Rd. E.	Mississauga	35 m
Goreway Dr.	Derry Rd. E.	Highway 427	Mississauga	26 m
Hillcrest Ave.	Confederation Pkwy.	Hurontario St.	Mississauga	26 m
Indian Rd.	Lorne Park Rd.	Mississauga Rd.	Mississauga	20 m
Kateson Dr.	Courtneypark Dr. W.	Capston Dr.	Mississauga	30 m
Kennedy Rd.	North City boundary	Matheson Blvd. E.	Mississauga	30 m



<b>Street</b>	<b>From</b>	<b>To</b>	<b>Jurisdiction</b>	<b>R-O-W*</b>
Kennedy Rd.	Matheson Blvd. E.	Eglinton Ave. E.	Mississauga	30 m
King St. W.	Confederation Pkwy.	Hurontario St.	Mississauga	26 m
King St. E.	Hurontario St.	Camilla Rd.	Mississauga	26 m
Kirwin Ave.	Hurontario St.	Dundas St. E.	Mississauga	26 m
Leanne Blvd.	Erin Mills Pkwy.	North Sheridan Way	Mississauga	26 m
Lincoln Green Way	Erin Mills Pkwy.	Fowler Dr.	Mississauga	35 m
Lorne Park Rd.	Indian Rd.	Truscott Dr.	Mississauga	20 m
Madill Blvd. extension	Kateson Dr.	Hurontario St.	Mississauga	23 m – 26 m
Main St.	Queen St. S.	Approximately 90 m east of Wyndham St.	Mississauga	30 m
Main St.	Approximately 90 m east of Wyndham St.	Credit River	Mississauga	20 m
Maritz Dr.	Derry Rd. W.	Courtneypark Dr. W.	Mississauga	30 m
Matheson Blvd. W.	Terry Fox Way	Hurontario St.	Mississauga	30 m
Matheson Blvd. E.	Hurontario St.	Highway 403	Mississauga	30 m
Matheson Blvd. E.	Highway 403	Future Arterial / Creekbank Rd.	Mississauga	26 m
Matheson Blvd. E.	Future Arterial / Creekbank Rd. Creekbank Rd.	East City boundary	Mississauga	30 m
McLaughlin Rd.	North City boundary	Matheson Blvd. W.	Mississauga	30 m
McLaughlin Rd. (Scenic Route)	Matheson Blvd. W.	Bristol Rd. W.	Mississauga	26 m
McLaughlin Rd.	Bristol Rd. W.	Eglinton Ave. W.	Mississauga	26 m
Meadowpine Blvd.	North City boundary	Meadowvale Blvd.	Mississauga	30 m
Meadowvale Blvd.	North City boundary	Derry Rd. W.	Mississauga	30 m
Millcreek Dr.	Derry Rd. W.	Erin Mills Pkwy.	Mississauga	26 m
Mineola Gdns.	Mineola Rd. E.	Atwater Ave.	Mississauga	20 m
Mineola Rd. E.	Hurontario St.	Mineola Gdns.	Mississauga	20 m
Mississauga Rd.	Erin Mills Pkwy.	St. Lawrence & Hudson Railway tracks	Mississauga	26 m
Mississauga Rd. (Scenic Route)	St. Lawrence & Hudson Railway tracks	Indian Rd.	Mississauga	26 m
Mississauga Rd. (Scenic Route)	Indian Rd.	Canadian National Railway tracks	Mississauga	23-26 m
Mississauga Rd. N. (Scenic Route)	Canadian National Railway tracks	Lakeshore Rd. E.	Mississauga	26 m
Morning Star Dr.	Airport Rd.	Highway 427	Mississauga	26 m

<b>Street</b>	<b>From</b>	<b>To</b>	<b>Jurisdiction</b>	<b>R-O-W*</b>
North 403 Major Collector Rd.	Mavis Rd.	Hurontario St.	Mississauga	30 m
North Service Rd.	Hurontario St.	Cawthra Rd.	Mississauga	22 m
North Service Rd.	Cawthra Rd.	Brentano Blvd.	Mississauga	20 m
North Sheridan Way	Winston Churchill Blvd.	Erin Mills Pkwy.	Mississauga	20 m
North Sheridan Way	Fowler Dr.	East-West section of North Sheridan Way	Mississauga	26 m
North Sheridan Way	East-West section of North Sheridan Way	Mississauga Rd.	Mississauga	20 m
Ogden Ave.	South Service Rd.	Lakeshore Rd. E.	Mississauga	20 m
Ponytrail Dr.	Rathburn Rd. E.	Burnhamthorpe Rd. E.	Mississauga	30 m
Queen St. N.	St. Lawrence & Hudson Railway tracks	Britannia Rd. W.	Mississauga	26 m
Queen St. S. (Scenic Route)	Britannia Rd. W.	St. Lawrence and Hudson Railway tracks	Mississauga	20 m
Rathburn Rd. W.	Creditview Rd.	Mavis Rd.	Mississauga	26 m
Rathburn Rd. W.	Mavis Rd.	Approximately 50 m east of Elora Dr.	Mississauga	30 m
Rathburn Rd. W.	Approximately 50 m east of Elora Dr.	Station Gate Rd.	Mississauga	40 m
Rathburn Rd. W.	Station Gate Rd.	Centre View Dr.	Mississauga	55 m
Rathburn Rd. W.	Centre View Rd.	Hurontario St.	Mississauga	40 m
Rathburn Rd. E.	Hurontario St.	Approximately 150 m east of Shipp Dr.	Mississauga	40 m
Rathburn Rd. E.	Approximately 150 m east of Shipp Dr.	Ponytrail Dr.	Mississauga	30 m
Rathburn Rd. E.	Ponytrail Dr.	Etobicoke Creek	Mississauga	35 m
Ridgeway Dr.	Eglinton Ave. W.	Dundas St. W.	Mississauga	26 m
Sheridan Park Dr.	Winston Churchill Blvd.	Erin Mills Pkwy.	Mississauga	35 m
South Service Rd.	Hurontario St.	Park Royale Blvd.	Mississauga	20 m
South Sheridan Way	Winston Churchill Blvd.	Mississauga Rd.	Mississauga	20 m
Tenth Line W.	Argentia Rd.	Britannia Rd. W.	Mississauga	30 m
Tenth Line W.	Britannia Rd. W.	McDowell Dr.	Mississauga	26 m
Tenth Line W.	McDowell Dr.	Tacc Dr.	Mississauga	30 m
Tenth Line W.	Tacc Dr.	Erin Centre Blvd.	Mississauga	26 m
Tenth Line W.	Erin Centre Blvd.	Eglinton Ave. W.	Mississauga	30 m
Terry Fox Way	Britannia Rd. W.	Eglinton Ave. W.	Mississauga	30 m

<b>Street</b>	<b>From</b>	<b>To</b>	<b>Jurisdiction</b>	<b>R-O-W*</b>
The College Way	Ridgeway Dr.	Mississauga Rd.	Mississauga	26 m
Thomas St.	Ninth Line	Tenth Line W.	Mississauga	26 m
Thomas St.	Tenth Line	McFarren Blvd. / Gafney Dr.	Mississauga	30 m
Thomas St.	McFarren Blvd. / Gafney Dr.	Queen St. S.	Mississauga	20-26 m
Tomken Rd.	North City boundary	Highway 401	Mississauga	30 m
Tomken Rd.	Highway 401	Eastgate Pkwy.	Mississauga	35 m
Tomken Rd.	Eastgate Pkwy.	Dundas St. E.	Mississauga	26 m
Topflight Dr.	Hurontario St.	Edwards Blvd.	Mississauga	26 m
Torbram Rd.	North City boundary	Derry Rd. E.	Mississauga	30 m
Truscott Dr.	Winston Churchill Blvd.	Sandgate Cres.	Mississauga	20 m
Truscott Dr.	Sandgate Cres.	Lorne Park Rd.	Mississauga	26 m
Wainscot Dr.	Eglinton Ave. W.	White Clover Way	Mississauga	26 m
Whittle Rd.	Highway 401 at Hurontario St. interchange R-O-W	Matheson Blvd. E.	Mississauga	26 m
World Dr.	Hurontario St.	Edwards Blvd.	Mississauga	26 m

\* These are considered basic rights-of-way. At intersections, grade separations or major physical topographical constraints, wider rights-of-way may be required to accommodate necessary features such as embankments, auxiliary lanes, additional pavement or sidewalk widths, transit facilities, cycling facilities, or to provide for necessary improvements for safety in certain locations.

## Table 8-3: Road Classification - Minor Collectors

The road right-of-way (R-O-W) for minor collectors will be 20 m – 26 m, with the following exceptions:

Character Areas*	R-O-W Range**
Airport Corporate Centre	26 m – 30 m
Airport Special Purpose Area	n/a
Churchill Meadows Neighbourhood	22 m – 24 m
Gateway Corporate Centre	24 m – 27 m
Gateway Employment Area	24 m – 27 m
Mavis-Erindale Employment Area	26 m
Mineola Neighbourhood	20 m
Northeast Employment Area	24 m – 26 m
Southdown Employment Area	24 m – 26 m
University of Toronto Mississauga Special Purpose Area	n/a
Western Business Park Employment Area	24 m – 30 m

\* Refers to all streets in the character area except for the street sections specified on next page.

\*\* These are considered basic rights-of-way. At intersections, grade separations or major physical topographical constraints, wider rights-of-way may be required to accommodate necessary features such as embankments, auxiliary lanes, additional pavement or sidewalk widths, transit facilities, cycling facilities, or to provide for necessary improvements for safety in certain locations.



<b>Character Area</b>	<b>Street</b>	<b>From</b>	<b>To</b>	<b>Jurisdiction</b>	<b>R-O-W Range*</b>
Churchill Meadows Neighbourhood	Erin Centre Blvd.	Ninth Line	Tenth Line	Mississauga	26 m
Dixie Employment Area	Hensall Circle	North of Dundas St. E.	South of Dundas St. E.	Mississauga	15 m
Downtown Core	Kariya Dr.	Burnhamthorpe Rd. W.	Elm Dr. W.	Mississauga	30 m
Erindale Neighbourhood and Cooksville Neighbourhood	Stavebank Rd. (Scenic Route)	Approximately 150 m south of Isabella Ave.	Premium Way	Mississauga	15 m
Fairview Neighbourhood	Kariya Dr.	Approximately 50 m south of Enfield Place (2nd leg)	Elm Dr. W.	Mississauga	30 m
Mineola Neighbourhood	Stavebank Rd. (Scenic Route)	Pinetree Way	Canadian National Railway tracks	Mississauga	12 m – 15 m
Streetsville Community Node	Church St.	Queen St. S.	Ontario St. E.	Mississauga	15 m
Streetsville Neighbourhood	Kinsmen Gate	Falconer Dr.	Argentia Rd.	Mississauga	30 m
Streetsville Neighbourhood	Ontario St. E.	Church St.	Queen St. S.	Mississauga	15 m

\* These are considered basic rights-of-way. At intersections, grade separations or major physical topographical constraints, wider rights-of-way may be required to accommodate necessary features such as embankments, auxiliary lanes, additional pavement or sidewalk widths, transit facilities, cycling facilities, or to provide for necessary improvements for safety in certain locations.

## Table 8-4: Road Classification – Local Roads

The road right-of-way (R-O-W) for local roads will be 17 m – 20 m, with the following exceptions:

Character Areas*	R-O-W Range**
Airport Corporate Centre	22 m – 26 m
Airport Special Purpose Area	n/a
Downtown Core	20 m
Gateway Corporate Centre	20 m – 24 m
Gateway Employment Area	20 m – 24 m
Northeast Employment Area	20 m – 24 m
Port Credit Community Node	17m – 22 m
Port Credit Neighbourhood	17 m – 22 m
Streetsville Community Node	15 m – 20 m
Streetsville Neighbourhood	15 m – 20 m
University of Toronto Mississauga Special Purpose Area	n/a
Western Business Park Employment Area	17 m – 24 m

\* Refers to all streets in the character area except for the streets specified below.

Character Area	Street	Jurisdiction	R-O-W Range**
Churchill Meadows Neighbourhood	Buffer Roads	Mississauga	15 m – 17 m
Churchill Meadows Neighbourhood	Public Lanes	Mississauga	8 m
Downtown Core	Mew Road	Mississauga	12.5 m
Lisgar Neighbourhood	Bishop Strachan Court cul-de-sacs	Mississauga	10 m
Lisgar Neighbourhood	Mockingbird Lanes cul-de-sacs	Mississauga	10 m
Lisgar Neighbourhood	Snow Goose Lanes cul-de-sacs	Mississauga	10 m
Lisgar Neighbourhood	Tenth Line cul-de-sacs	Mississauga	10 m
Lisgar Neighbourhood	Trelawny Circle cul-de-sacs	Mississauga	10 m

\*\* These are considered basic rights-of-way. At intersections, grade separations or major physical topographical constraints, wider rights-of-way may be required to accommodate necessary features such as embankments, auxiliary lanes, additional pavement or sidewalk widths, transit facilities, cycling facilities, or to provide for necessary improvements for safety in certain locations.

# 9 Build a Desirable Urban Form

## 9.1 Introduction

Urban form refers to the physical layout and design of the city. Urban design is the art of shaping the interaction between people and places through the arrangement, appearance and functions of cities. It addresses the natural and built environments and influences the processes that lead to successful cities. Or, more succinctly, how do buildings fit together to make quality spaces.

The focus of this chapter is on the achievement of a sustainable urban form for Mississauga, based on

the urban system and city structure, with high quality urban design and a strong sense of place that is culturally vibrant, attractive, livable and functional, and protects or enhances natural and built heritage features. It is organized on the basis of Intensification Areas where growth will be directed and other areas where limited growth will occur.

Mississauga envisions that growth will be directed to Intensification Areas comprised of the Downtown, Major Nodes, Community Nodes, Corporate Centres, **Intensification Corridors** and **Major Transit Station Areas**. Established residential Neighbourhoods, the Natural Areas



Figure 9-1: Mississauga will direct growth to Intensification Areas while protecting existing neighbourhoods. This concept plan illustrates how the Hurontario/Dundas area can be redeveloped to achieve a vibrant, pedestrian friendly destination that is rich in character.

System and valuable cultural heritage resources will be protected and strengthened with infill and redevelopment, compatible with the existing or planned character. Employment Areas, Special Purpose Areas – University of Toronto Mississauga and the Airport - and **Corridors** will also receive growth compatible with their planned vision.

Appropriate infill in both Intensification Areas and Non-Intensification Areas will help to revitalize existing communities by replacing aged buildings, developing vacant or underutilized lots and by adding to the variety of building forms and tenures. It is important that infill “fits” within the existing urban context and minimizes undue impacts on adjacent properties. Redevelopment projects include a range of scales, from small residential developments to large scale projects, such as redeveloping strip malls. Redevelopment must also be sensitive to the existing urban context and minimize undue impacts on adjacent properties.

An appropriate urban form will guide development, infill and redevelopment in a manner that protects, enhances and restores the green system and cultural heritage features, while sensitively integrating these features into the city pattern.

Mississauga will achieve an attractive, comfortable and functional public realm as an integral component of its urban form. The public realm is composed of public lands with a focus on-streets and boulevards and edges of private properties as they are visible from, and as they interface with the public streets. Policies regarding the public realm address matters such as the arrangement of streets and blocks, civic buildings, landmarks, gateways, views, public art and open spaces.

A desirable urban form frames and supports all movement systems in the City. It addresses the interdependent relationship among built form and various modes of movement, including accessibility. As greater emphasis is placed on transit and **active transportation**, urban form needs to support these various modes while still having regard for the importance of vehicular and goods movement to the economic functioning of the city.

Site development is the layout and design of all features on a property including buildings, structures, parking, driveways, landscaping and utilities. Site development policies are directed at the creation of buildings and spaces which not only satisfy the needs of its own users and those who will live and work in the area, but also the needs of future generations. Sites will be developed to:

- respect the experience, identity and character of the surrounding context;
- ensure the sustainability of natural systems and urban living;
- protect the quality of life of residents, employees and visitors;
- ensure the connectivity and integration of surrounding uses; and
- require properties to develop in a manner that contributes to the overall vision for the city.

It is recognized that the urban form envisioned by this Plan will take time to realize. As such, development may be phased provided that the proposed development contributes to and does not hinder the ultimate achievement of the policies of this Plan.

9.1.1 Mississauga will develop an urban form based on the urban system and the hierarchy identified in the city structure as shown on Schedule 1: Urban System.

9.1.2 Within Intensification Areas an urban form that promotes a diverse mix of uses and supports transit and **active transportation** modes will be required.

9.1.3 Infill and redevelopment within Neighbourhoods will respect the existing and planned character.

9.1.4 Development within Employment Areas and Special Purpose Areas will promote good urban design that respects the function of the area.



9.1.5 Development on **Corridors** will be consistent with existing or planned character, seek opportunities to enhance the **Corridor** and provide appropriate transitions to neighbouring uses.

9.1.6 The urban form of the city will ensure that the Green System is protected, enhanced and contributes to a high quality urban environment and quality of life.

9.1.7 Mississauga will promote a built environment that protects and conserves heritage resources.

9.1.8 Mississauga will transform the public realm to create a strong sense of place and civic pride.

9.1.9 Urban form will support the creation of an efficient multi-modal transportation system that encourages a greater utilization of transit and **active transportation** modes.

9.1.10 The city vision will be supported by site development that:

- a. respects the urban hierarchy;
- b. utilizes best sustainable practices;
- c. demonstrates context sensitivity, including the public realm;
- d. promotes universal accessibility and public safety; and
- e. employs design excellence.

9.1.11 A distinct character for each community will be created or enhanced through the road pattern, building massing and height, **streetscape** elements, preservation and incorporation of heritage resources and prominent placement of institutions and open spaces.

9.1.12 An urban form will be developed to take advantage of the Lake Ontario waterfront through connections, views and access.

9.1.13 Development will have positive, restorative, ecological benefits on a site through the practice of sustainable building and site design.

9.1.14 Mississauga may undertake or require studies that develop additional policies, guidelines and design control tools that may contain more specific urban form requirements.

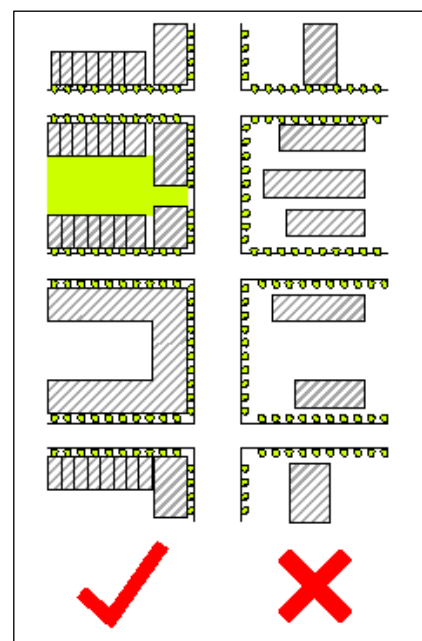


Figure 9-2: Building frontages will frame streets and provide a contiguous built form.



Figure 9-3: New development will frame adjacent streets by locating buildings parallel to the street with consistent front yard setbacks. Development on a corner should address both adjacent street frontages to give prominence to the corner.

## 9.2 City Pattern

City pattern provides the visual framework of the city. The city pattern that defines Mississauga includes:

- Intensification Areas;
- Non-Intensification Areas;
- Green System; and
- Cultural Heritage.

The city pattern is a reflection of policies and land use decisions that direct growth. It is the major driver of the city's image – it creates order, scale, a sense of place, purpose and identity.

Mississauga will develop a city pattern that is more sustainable and supports complete communities by directing growth to Intensification Areas and managing growth in other areas. It is intended to create a pattern marked by a greater mixture of land uses in a more compact form of development that supports, and is integrated with a multi-modal transportation system.

City pattern does not establish land use, but rather it directs the form in which permitted development

should be undertaken. Urban form requirements will vary in accordance with the envisioned city pattern.

The following policies for the city pattern will implement the city structure as shown on Schedule 1b: Urban System: City Structure and Schedule 1c: Urban System: Corridors, while protecting the Green System as shown on Schedule 1a: Urban System: Green System, and cultural heritage resources.

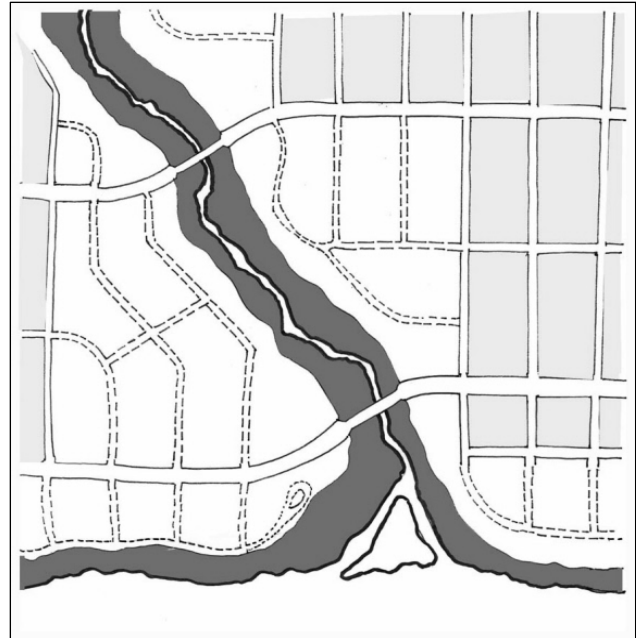


Figure 9-5: A sustainable city pattern will support complete communities.



Figure 9-4: The city pattern will respect the urban hierarchy with the Downtown containing the greatest heights, densities and mix of uses.





Figure 9-6: Over time, Eglinton Avenue and Hurontario Street will intensify with a built form that frames the streets and provides a quality public realm, ensuring a vibrant pedestrian environment.

## 9.2.1 Intensification Areas

Intensification Areas are the principal location for future growth and consist of:

- Downtown;
- Major Nodes;
- Community Nodes;
- Corporate Centres;
- ***Intensification Corridors***; and
- ***Major Transit Station Areas***.

Intensification Areas are a major building block of the city pattern and, as such, will be expected to exhibit high standards of urban design that will

result in vibrant and memorable urban places. They are intended to create order and a sense of place, with a scale that varies with their intended purpose and role in the urban hierarchy.

In order to achieve the vision for Intensification Areas as vibrant, mixed use areas, serviced by multi-modal transportation, the physical form, relationship among buildings and spaces and the quality of the built environment will be critical in making these areas successful.

9.2.1.1 Development will create distinctive places and locales.

9.2.1.2 Design excellence will create a vibrant Downtown complemented by communities that retain their own identity and contribute to an overall strong city identity.







Figure 9-7: A compact, urban built form with a mix of uses will be incorporated in Cooksville and other Intensification Areas.



9.2.1.3 Built form should provide for the creation of a sense of place through, among other matters, distinctive architecture, streetscaping, public art and cultural heritage recognition.

9.2.1.4 Mississauga will encourage a high quality, compact and urban built form to reduce the impact of extensive parking areas, enhance pedestrian circulation, complement adjacent uses, and distinguish the significance of the Intensification Areas from surrounding areas.

9.2.1.5 Small land parcels should be assembled to create efficient development parcels.

9.2.1.6 Mississauga will encourage the consolidation of access points and shared parking, service areas and driveway entrances.

9.2.1.7 Development proponents may be required to provide concept plans that show how a site will be developed with surrounding lands.

9.2.1.8 The preferred location of **tall buildings** will be in proximity to existing and planned **Major Transit Station Areas**.

9.2.1.9 Where the right-of-way width exceeds 20 m, a greater building height may be required to achieve appropriate street enclosure in relation to the right-of-way width.

9.2.1.10 Appropriate height and built form transitions will be required between sites and their surrounding areas.

9.2.1.11 **Tall buildings** will be sited and designed to enhance an area's skyline.

9.2.1.12 **Tall buildings** will be sited to preserve, reinforce and define view corridors.

9.2.1.13 **Tall buildings** will be appropriately spaced to provide privacy and permit light and sky views.

9.2.1.14 In appropriate locations, **tall buildings** will be required to incorporate podiums to mitigate wind impacts on the pedestrian environment and maximize sunlight on the public realm.

9.2.1.15 **Tall buildings** will address pedestrian scale through building articulation, massing and materials.

9.2.1.16 **Tall buildings** will minimize adverse microclimatic impacts on the public realm and private amenity areas.

9.2.1.17 Principal streets should have continuous building frontages that provide continuity of built form from one property to the next with minimal gaps between buildings.

9.2.1.18 Existing large blocks will be reconfigured to incorporate a fine-grained block structure with public roads and on-street parking to support at grade uses.

9.2.1.19 The public realm and the development interface with the public realm will be held to the highest design standards.

9.2.1.20 Mississauga will develop identifiable civic buildings, structures, and spaces as community and city focal points.

9.2.1.21 Development will contribute to pedestrian oriented **streetscapes** and have an urban built form that is attractive, compact and transit supportive.

9.2.1.22 Development will be designed to support and incorporate pedestrian and cycling connections.

9.2.1.23 Active uses will be required on principal streets with direct access to the public sidewalk.

9.2.1.24 Development will face the street.

9.2.1.25 Buildings should have active façades characterized by features such as lobbies, entrances and display windows. Blank building walls will not

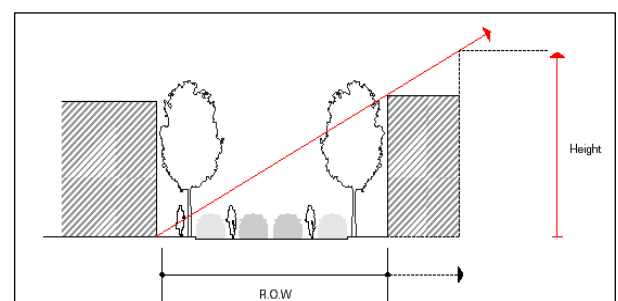


Figure 9-8: On wider streets, taller buildings may be required to provide appropriate street enclosure.

be permitted facing principal street frontages and intersections.

9.2.1.26 For non-residential uses, at grade windows will be required facing major streets and must be transparent.

9.2.1.27 Development will create a sense of gateway to the Intensification Area with prominent built form and landscaping.

9.2.1.28 Built form will relate to and be integrated with the streetline, with minimal building setbacks where spatial enclosure and street related activity is desired.

9.2.1.29 Development will have a compatible bulk, massing and scale of built form to provide an integrated **streetscape**.

9.2.1.30 Development will provide open space, including squares and plazas appropriate to the size, location and type of the development.

9.2.1.31 Buildings should be positioned along the edge of the public streets and public open spaces, to define their edges and create a relationship with the public sidewalk.

9.2.1.32 Buildings should be oriented to, and positioned along the street edge, with clearly defined primary entry points that directly access the public sidewalk, pedestrian connections and transit facilities.

9.2.1.33 Open spaces will be designed to promote social interaction.

9.2.1.34 Development will utilize **streetscape** design to provide visual connections to open space, providing enhanced sidewalk and trail connections near open spaces.

9.2.1.35 Buildings and **streetscapes** will be situated and designed so as to encourage pedestrian circulation.

9.2.1.36 **Streetscape** improvements including trees, pedestrian scale lighting, special paving and street

furniture in sidewalks, boulevards, open spaces and walkways, will be coordinated and well designed.

9.2.1.37 Developments should minimize the use of surface parking in favour of underground or aboveground structured parking. All surface parking should be screened from the street and be designed to ensure for natural surveillance from public areas. Aboveground structured parking should be lined with residential, commercial or office uses.

9.2.1.38 Parking lots and structures should not be located adjacent to major streets.

9.2.1.39 Signage will be integrated with the scale and character of built form and will follow **universal design principles**.

## 9.2.2 Non-Intensification Areas

Non-intensification areas will experience limited growth and change; consequently, intensive growth will not be directed to them. Non-Intensification Areas consist of:

- Neighbourhoods;
- Employment Areas;
- Special Purpose Areas; and
- **Corridors.**

Non-Intensification areas will have lower densities, lower building heights and more homogeneous land uses than Intensification Areas.

Neighbourhoods are stable areas where limited growth is anticipated. Development in Neighbourhoods will be required to be context sensitive and respect the existing or planned character and scale of development.

Employment Areas will continue to develop for a mixture of employment uses, including land extensive uses many of which will have a one to two storey urban form.

Where increases in density and a variety of land uses are considered in Neighbourhoods and Employment Areas, they will be directed to **Corridors**. Appropriate transitions to adjoining areas that respect variations in scale, massing and land uses will be required.

Special Purpose Areas – the University of Toronto

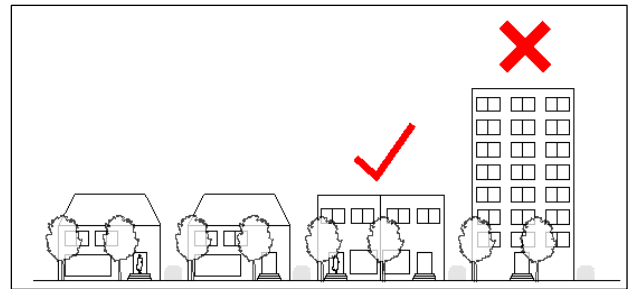


Figure 9-10: New development in neighbourhoods does not need to mirror existing development, but will be required to respect and be compatible with the surrounding area.

Mississauga and the Airport - will be developed in a manner that reflects the unique role these areas play within the city and the region. While these areas will undergo limited urban development in the traditional sense, master plans for these areas will guide development that responds to their particular functions and development needs.

9.2.2.1 Heights in excess of four storeys will be required to demonstrate that an appropriate transition in height and built form that respects the surrounding context will be achieved.

9.2.2.2 **Tall buildings** will generally not be permitted.

9.2.2.3 While new development need not mirror existing development, new development in Neighbourhoods will:

- respect existing lotting patterns;
- respect the continuity of front, rear and side yard setbacks;
- respect the scale and character of the



Figure 9-9: Development in Non-Intensification Areas will be sensitive to the existing or planned character of the neighbourhood and the natural environment.

surrounding area;

- d. minimize overshadowing and overlook on adjacent neighbours;
- e. incorporate **stormwater best management practices**;
- f. preserve mature high quality trees and ensure replacement of the tree canopy; and
- g. be designed to respect the existing scale, massing, character and grades of the surrounding area.

9.2.2.4 Employment Areas adjacent to residential areas, sensitive land uses and major roads will be required to meet higher standards of design and to mitigate adverse impacts on adjacent uses.

9.2.2.5 The City will work with landowners of Special Purpose Areas to encourage:

- a. the preservation and enhancement of the Natural Areas System;
- b. design for safety;
- c. design excellence;
- d. sustainable development; and
- e. the achievement of the overall intent, goals, objectives and policies of this Plan.

9.2.2.6 Development on **Corridors** will be encouraged to:

- a. assemble small land parcels to create efficient development parcels;
- b. face the street, except where predominate development patterns dictate otherwise;
- c. not locate parking between the building and the street;
- d. site buildings to frame the street and where non-residential uses are proposed to create a continuous street wall;
- e. provide entrances and transparent windows facing the street for non-residential uses;

- f. support transit and **active transportation** modes;
- g. consolidate access points and encourage shared parking, service areas and driveway entrances; and
- h. provide concept plans that show how the site can be developed with surrounding lands.



### 9.2.3 Green System



Figure 9-11: Kariya Park in Downtown Mississauga provides a reprieve from the built environment and is an essential component of the urban structure.

The Green System provides vital relief from the built environment. It helps satisfy human needs for rest, quiet and escape from the built environment. It is generally characterized by vegetated open areas ranging from manicured spaces and playing fields to expansive natural areas. Where buildings are sited, they tend to occupy a small proportion of the site and be small in scale except for public buildings, such as community centres and schools.

Urban form has a role to play in protecting the environment by being sensitive to natural areas in site design, utilizing sustainable design practices and supporting active modes of transportation.

9.2.3.1 Development will be sensitive to the site and ensure that Natural Areas Systems are protected, enhanced and restored.

9.2.3.2 All development will utilize sustainable design practices.

9.2.3.3 Mississauga will coordinate the design, function and location of parks with adjacent land uses.

9.2.3.4 Open space areas will be high quality, usable and physically and visually linked to streets, parks and pedestrian routes.

## 9.2.4 Cultural Heritage Resources

Cultural heritage resources are valued and should be preserved for future generations. Heritage properties, districts and landscapes create a unique sense of place and local identity. In addition to their historic associations, cultural heritage resources are landmarks and focal points that contribute to the overall city image.

9.2.4.1 Opportunities to conserve and incorporate cultural heritage resources into community design and development should be undertaken in a manner that enhances the heritage resources and makes them focal points for the community.

9.2.4.2 Development and open spaces adjacent to significant cultural heritage resources will:

- a. contribute to the conservation of the heritage attributes of the resource and the heritage character of the area;
- b. emphasize the visual prominence of cultural heritage resources; and
- c. provide a proper transition with regard to the setting, scale, massing and character to cultural heritage resources.



Figure 9-12: W.R.P. Parker had this country estate designed by the well known Canadian architect, Shy Mathers in 1918. The house was a social centre hosting such politicians as Mackenzie King at the time he was Prime Minister of Canada. In 1956 the property was sold to Hyl and Grace Chappell who lived here until the late 1980s.

9.2.4.3 **Streetscape** components such as signage, furniture and lighting, within areas with cultural heritage resources should be sympathetic to the character of the heritage area.



Figure 9-13: The Streetsville United Church, a heritage landmark, is preserved for the enjoyment of citizens today and future generations.



Figure 9-14: The Middle Road Bridge, located at the eastern end of Sherway Drive at the Etobicoke Creek, was built in 1909 and was the first concrete bow bridge of its kind in Canada.



## 9.3 Public Realm



Figure 9-15: Generous public sidewalks and open spaces will provide opportunities to enrich the pedestrian experience.

The public realm consists of streets and boulevards, public open spaces, squares and civic buildings and is an integral component of the urban form of the city. The arrangement of streets and blocks within the public realm provides a foundation for the city's built environment, which in turn influences the shape and layout of the public realm.



Figure 9-16: In the Downtown, public boulevards will be designed to incorporate street trees, on-street parking, pedestrian scale lighting, bicycle storage facilities and coordinated street furniture to enrich the urban experience.

The creation of landmarks, routes and gateways within the public realm contributes to the unique experience, legibility, sense of orientation and views and vistas. Public art, open space and landmark buildings in the public realm enriches the urban experience.

### 9.3.1 Streets and Blocks

Streets are public spaces that connect buildings, structures, parks, communities, natural resources and other significant public amenities. Blocks are the spaces between streets where buildings, structures and other elements, including parks and open spaces, are located. A **streetscape** is the image created by the buildings, sidewalks, signage, street trees, landscaping, street furnishings, open spaces, and other elements along streets.

9.3.1.1 Street patterns, development blocks and public open spaces together should create distinctive communities.

9.3.1.2 Mississauga will ensure that urban form, street patterns and public open space systems are coherent, orderly and legible.

9.3.1.3 Major roads and their **streetscapes** should be designed to create spaces that are integral parts of the adjacent communities, thus serving to link communities.

9.3.1.4 Development will be designed to:

- respect the natural heritage features, such as forests, ridges, valleys, hills, lakes, rivers, streams and creeks;
- respect cultural heritage features such as designated buildings, landmarks and districts;
- accentuate the significant identity of each Character Area, its open spaces, landmarks and cultural heritage resources;
- achieve a street network that connects to adjacent streets and neighbourhoods at regular intervals, wherever possible;



- e. meet **universal design principles**;
- f. address new development and open spaces;
- g. be pedestrian oriented and scaled and support transit use;
- h. be attractive, safe and walkable;
- i. accommodate a multi-modal transportation system; and
- j. allow common rear laneways or parallel service streets to provide direct access for lots fronting arterial roads and major collector roads, when appropriate.

design of new streets should enhance connectivity by:

- a. developing a fine-grained system of roads;
- b. using short streets and small blocks as much as possible, to encourage pedestrian movement;
- c. avoiding street closures; and
- d. minimizing cul-de-sac and dead end streets.

9.3.1.6 Where cul-de-sac and dead end streets exist, accessible paths that provide shortcuts for walking and cycling and vehicular access should be created, where possible.

#### 9.3.1.5 The improvement of existing streets and the



Figure 9-17: A fine-grained system of streets, incorporating small blocks will encourage pedestrian access and movement within Intensification Areas.





Figure 9-18: Development will incorporate street patterns of existing communities and built up areas, with a particular block structure and characteristic street pattern.

9.3.1.7 **Streetscapes** will be designed to create a sense of identity through the treatment of architectural features, forms, massing, scale, site layout, orientation, landscaping, lighting and signage.

9.3.1.8 The design of developments at intersections and along major streets should be of a highly attractive urban quality, recognizing that streets are important civic spaces and linkages.

9.3.1.9 Development and elements within the public realm will be designed to provide continuity of the **streetscape** and minimize visual clutter.

9.3.1.10 Consideration will be given to the location of utilities on private property and the public right-of-way. Utilities will be grouped or located underground where possible to minimize visual impact. The City encourages utility providers to consider innovative methods of containing utility services.

9.3.1.11 Reverse frontage lots will not be permitted, except for infill development where a street pattern has already been established.

## 9.3.2 Civic Buildings and Spaces

Civic buildings and spaces are an important component of the public realm. These elements have the opportunity to become landmark buildings and spaces and should set the standard for development within the city. Civic buildings and spaces may also act as a catalyst for further development on surrounding lands.

9.3.2.1 Civic buildings and spaces will:

- be built to a high standard of design excellence;
- incorporate sustainable site development and buildings practices;
- strive to achieve a minimum standard of LEED Silver or custom green development standards; and
- be sited for prominence, visibility and universal accessibility.



Figure 9-19: Public views of the Mississauga Civic Centre, an important landmark, must be protected as it contributes to the identity of local government, the Downtown and the city as a whole.

9.3.2.2 **Universal design principles** will be applied in the development of, or renovation to City facilities including civic buildings, open space recreation uses, transit and pedestrian facilities.

### 9.3.3 Gateways, Routes, Landmarks and Views

Gateways, routes and landmarks are important building blocks of the city and contribute to city pattern and urban experience. Some sites within the city are uniquely located, given their topography, views or gateway condition. The design and function of these sites have the opportunity and responsibility to contribute to an area's character. Public buildings and structures with a prominent role and function should stand out from their context to support their role as landmarks.

Public views of important natural or man-made features along streets and **scenic routes** need to be protected since they add value to the built form and contribute to neighbourhood identity. When opportunities arise, new development must maintain, and in some cases, enhance those views and vistas to prominent features.

9.3.3.1 An appropriate gateway treatment will be created at city boundaries, major Provincial highway interchanges and at entry points to Intensification Areas through high quality development, massing of buildings, open spaces, landscaping and **streetscape**.

9.3.3.2 **Tall buildings** have a greater presence on the skyline and are required to have the highest quality architecture.

9.3.3.3 Sites with prominence, high visibility and access should be considered as a priority for civic buildings and community infrastructure.

9.3.3.4 Buildings that serve the community such as places of religious assembly, colleges and hospitals, should be designed to be the focus of the community, highly visible, universally accessible and attractive and serve as landmarks for future generations.

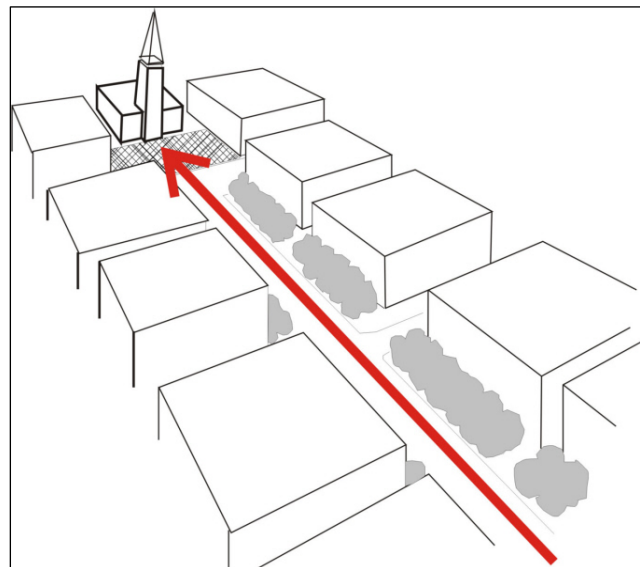


Figure 9-21: Development will preserve, promote and enhance public views to significant landmarks and natural features.



Figure 9-20: Views to Lake Ontario add value, provide a sense of orientation and preserve Port Credit's identity as a waterfront community.



9.3.3.5 Special attention will be given to major intersections to create a sense of enclosure and identity, as well as heightened architectural interest.

9.3.3.6 Developments on major corners, prominent sites or that terminate a view will be held to a higher design standard.

9.3.3.7 New streets may be introduced to create prominent view corridors.

9.3.3.8 Views of significant natural and man-made features should be created, maintained and enhanced where appropriate.

9.3.3.9 Development will preserve, promote and enhance public views to the Lake Ontario waterfront.

9.3.3.10 Special care will be taken with development along **scenic routes** to preserve and complement the scenic historical character of the street.

9.3.3.11 Lands abutting the Mississauga Road right-of-way between the St. Lawrence and Hudson Railway and Lakeshore Road West (frontage, flankage and rear yards) which is a designated **scenic route**, will be subject to the following:

- a. direct frontage lots with direct access or flankage lots with buildings that have front doors facing Mississauga Road will be encouraged;
- b. service road and reverse frontage lot development will be discouraged;
- c. existing residential lot frontages will be retained;
- d. building massing, design and setback should be consistent with buildings on surrounding lots;
- e. projecting garages will be discouraged;
- f. tree preservation, enhancement and replacement on private lands will be required;
- g. alternative on-site turn-arounds, such as hammerhead driveways, will be encouraged to reduce reverse movements and the number of driveway entrances. Circular driveways will be evaluated on an individual basis;
- h. preservation of existing landscape features (retaining walls, fences, hedgerows) will be encouraged; and
- i. the location of utilities should minimize the impact on existing vegetation.



Figure 9-22: Landmark buildings such as hospitals and places of religious assembly serve as focal points for communities as illustrated by the Credit Valley Hospital (Carlo Fidani Cancer Centre and the Richard and Annette Bloch Cancer Survivor Park) and Saviour of the World Chinese Church.

### 9.3.4 Public Art

Public art and culture enhances the quality of life for residents and visitors by contributing to the identity and unique character of the city and its various destinations. Incorporating public art into the experience of the city has the power to create a compelling, enriched environment and a place people will want to live in and return to.

Public art can range from the architecture of buildings to the design of elements within the public realm such as light features and seating. It may include memorials, sculptures, water features, murals, lighting or individual art installations; it may be integrated with building and landscape design; and art may also include functional elements such as street furniture and utility boxes.

Public art can serve as a focus in a public square or open space, or simply provide visual relief in high density areas. All gateway locations and public view terminus sites are candidates for public art.

Public art should be considered at a variety of scales and in diverse contexts. This includes larger installations at visually strategic locations such as the terminus of street corridors, at gateways, on prominent corners or in public open spaces. It also includes smaller or more unexpected installations such as along sidewalks, in interior courtyards, in association with buildings (entrances, lobbies) and in alternative public spaces such as parking lots and garages.

9.3.4.1 Mississauga will prepare a Public Art Master Plan.

9.3.4.2 Mississauga will encourage public art on public and private lands.

9.3.4.3 Public art will be incorporated into the public realm, particularly in appropriate locations to serve as landmarks and as gateway features.

9.3.4.4 Public art will be incorporated into public works, whenever feasible.



Figure 9-23: The Crown Fountain in Chicago, Millennium Park, is a significant public art piece that enhances the quality of life for residents and visitors and provides an interactive, dynamic experience.

9.3.4.5 Development proponents are encouraged to incorporate public art into their developments. Intensification Areas will be priority locations for the installation of public art.

9.3.4.6 Public art is encouraged throughout the city, particularly in Intensification Areas and along the Lake Ontario waterfront.

9.3.4.7 Public art should have a prominent presence throughout the city and contribute to a high quality urban design. Public art should:



- a. be encouraged as an integral component of public works, land development and open space planning;
- b. include pieces that serve as orienting devices for moving about and wayfinding or as focal points in public open spaces;
- c. contribute to the animation of public spaces through its design, which may include pieces that are used as street furniture, play areas and/or other interactive uses; and
- d. correspond to the visual prominence of the site on which it is located.

### 9.3.5 Open Spaces and Amenity Areas

Open spaces include both public and private space as well as on-site amenities and are one of the most significant contributors to an area's character and quality of life. It is important that they not only be well designed and beautiful, but also that they be well connected and integrated with adjacent uses and other open spaces. The provision of open space is an essential component of residential and non-residential development.

9.3.5.1 Mississauga will promote public open space design that is fully integrated with the urban design and built form of the community.

9.3.5.2 Open space will contribute to community aesthetics and enhance the Green System.

9.3.5.3 Natural features, parks and open spaces will contribute to a desirable urban form by:

- a. assisting with the protection, enhancement, restoration and expansion of the Natural Areas System, identified in Schedule 3: Natural System;
- b. connecting to the city's system of trails and pathways;
- c. connecting to other natural areas, woodlands, wetlands, parks, and open spaces, including streets, schools, cemeteries and civic spaces;
- d. ensuring that all new parks and Open Spaces address the street, providing clear visibility, access and safety;



Figure 9-24: In Port Credit, the Wave Park is the principal focus of the square with at grade retail uses located at the edges to secure a vibrant, animated open space.

- e. ensuring that adjacent uses, buildings and structures front onto them, with direct access, and encouraging natural surveillance; and
- f. appropriately sizing parks and open spaces to meet the needs of a community and ensuring they are able to accommodate social events and individual needs, inclusive of recreation, playgrounds, sports and community gardens, where possible.

9.3.5.4 Open spaces will be designed as places where people can socialize, recreate and appreciate the environment.

9.3.5.5 Private open space and/or amenity areas will be required for all development.

9.3.5.6 Residential developments of significant size, except for freehold developments, will be required to provide common outdoor on-site amenity areas that are suitable for the intended users.

9.3.5.7 Residential developments will provide at grade amenity areas that are located and designed for physical comfort and safety. In Intensification Areas, alternatives to at grade amenities may be considered.

9.3.5.8 Landscaped, outdoor on-site amenity areas will be encouraged for employment uses.

## 9.4 Movement

A guiding principle of this Plan is to connect people with places through coordinated urban design, land use and transportation planning. Development should ensure the ease of movement between the built form and transit facilities and **active transportation** routes.

While vehicular and goods movement will continue to be an essential element of the transportation system, a priority for Mississauga is to increase the appeal of transit and **active transportation** modes for people of all ages and abilities. Mississauga is committed to accessibility through barrier free universal design. The design and relationships of development and of open spaces adjacent to streets, has a significant role to play in fulfilling these objectives.



Figure 9-25: A transit and pedestrian supportive urban form with active building façades and animated public realm is anticipated for Dundas Street, along with other Intensification Areas and **Corridors** in Mississauga.

### 9.4.1 Transit and Active Transportation

Urban form is fundamental to fostering transit and **active transportation** choices. Site and building design will improve connections and accessibility for transit users and promote pedestrian and cycling transportation modes. Mississauga will consider the convenience, comfort and safety of pedestrians and cyclists through urban design.

Mississauga will ensure that pedestrian, cycling, transit and vehicular systems achieve a high standard of urban design and are an integral component of the city's visual image.



Figure 9-26: Bicycle parking is an essential urban amenity that nurtures transit usage and **active transportation** choices. (Research in Motion, Airport Corporate Centre)

9.4.1.1 The design of all development will foster the improvement of connections and accessibility for transit users and promote **active transportation** modes.

9.4.1.2 A transit and **active transportation** supportive urban form will be required in Intensification Areas and in appropriate locations along **Corridors** and encouraged throughout the rest of the city.

9.4.1.3 Development will support transit and **active transportation** by:

- a. locating buildings at the street edge, where appropriate;

- b. requiring front doors that open to the public street;
- c. ensuring active/animated building façades and high quality architecture;
- d. ensuring buildings respect the scale of the street;
- e. ensuring appropriate massing for the context;
- f. providing pedestrian safety and comfort; and
- g. providing bicycle destination amenities such as bicycle parking, shower facilities and clothing lockers, where appropriate.

9.4.1.4 Development will provide for pedestrian safety through visibility, lighting, natural surveillance and minimizing vehicular conflicts.

9.4.1.5 The design of transit facilities will consider the convenience, comfort and safety of pedestrians and cyclists.

### 9.4.2 Vehicular and Goods Movement

Although a priority for Mississauga is to increase the appeal of transit and **active transportation**, urban form must also consider the needs of vehicular and goods movement, especially in areas where it forms the dominant mode of transportation. Building and site design in employment areas must carefully consider goods movement and the potential for conflict with transit and **active transportation** modes.

9.4.2.1 Urban form will balance the needs of vehicular and goods movement with transit and **active transportation** modes.

9.4.2.2 In areas where vehicular and goods movement is the primary mode of transportation, regard for the needs and safety of transit users, pedestrians and cyclists will be required.

9.4.2.3 Where buildings and structures are separated from roadways by parking lots, efforts to



upgrade pedestrian access to buildings through landscaping, site design and the development of street related frontages is encouraged.



Figure 9-27: Mississauga's Accessibility Advisory Committee was established in 2003. Its role is to promote and facilitate a barrier free Mississauga for citizens of all abilities.

### 9.4.3 Accessibility

As Mississauga's population ages, the need for access will increase. Physical access to open spaces and buildings should be provided throughout the city. Further, the owners of existing buildings will be encouraged to retrofit them to be universally accessible.

9.4.3.1 Mississauga is committed to the creation of an accessible city. The design of the physical and built environment will have regard for ***universal design principles***.

9.4.3.2 All development will be consistent with the Mississauga Accessibility Design Handbook.

## 9.5 Site Development and Buildings

A significant part of the urban experience takes place as people move from one building to another. Focusing on the relationship between buildings and the spaces that surround them is critical to quality urban form.

The quality and character of different communities and areas will be conserved, in part, by establishing a proper transition between them.



Figure 9-28: Architecturally expressed entrance features and defined landscaping provide a welcoming gesture and denote the sense of principal access to the building. (Biovail Pharmaceutical Corporation, Meadowvale Business Park)



In the planning and design of sites for development, consideration should be given to the existing site conditions, surrounding context, the public realm and proposed uses. Protecting and enhancing the **natural features, areas and linkages including their ecological functions** in the city, will contribute to sustainable site development.

Buildings through their location and massing, help define the use and character of streets and open spaces with a memorable sense of place and history. The orientation and placement of a building on a property creates a relationship with the adjacent context and helps define the quality and character of the public realm.

Good urban form results in a pedestrian friendly, safe, inviting and aesthetically appealing urban environment. Matters such as landscaping, parking, servicing and signage need to be appropriately addressed.

### 9.5.1 Context

Context addresses how developments demonstrate compatibility and integration with surrounding land uses and vegetation by ensuring that an effective transition in built form is provided between areas of different development densities and scale, and the protection of natural features.

9.5.1.1 Buildings and site design will be compatible with site conditions, the surrounding context and

surrounding landscape of the existing or planned character of the area.

9.5.1.2 Developments should be compatible and provide appropriate transition to existing and planned development by having regard for the following elements:

- a. Natural Areas System;
- b. natural hazards (flooding and erosion);
- c. natural and cultural heritage features;
- d. street and block patterns;
- e. the size and configuration of properties along a street, including lot frontages and areas;
- f. continuity and enhancement of **streetscapes**;
- g. the size and distribution of building mass and height;
- h. front, side and rear yards;
- i. the orientation of buildings, structures and landscapes on a property;
- j. views, sunlight and wind conditions;
- k. the local vernacular and architectural character as represented by the rhythm, textures and building materials;

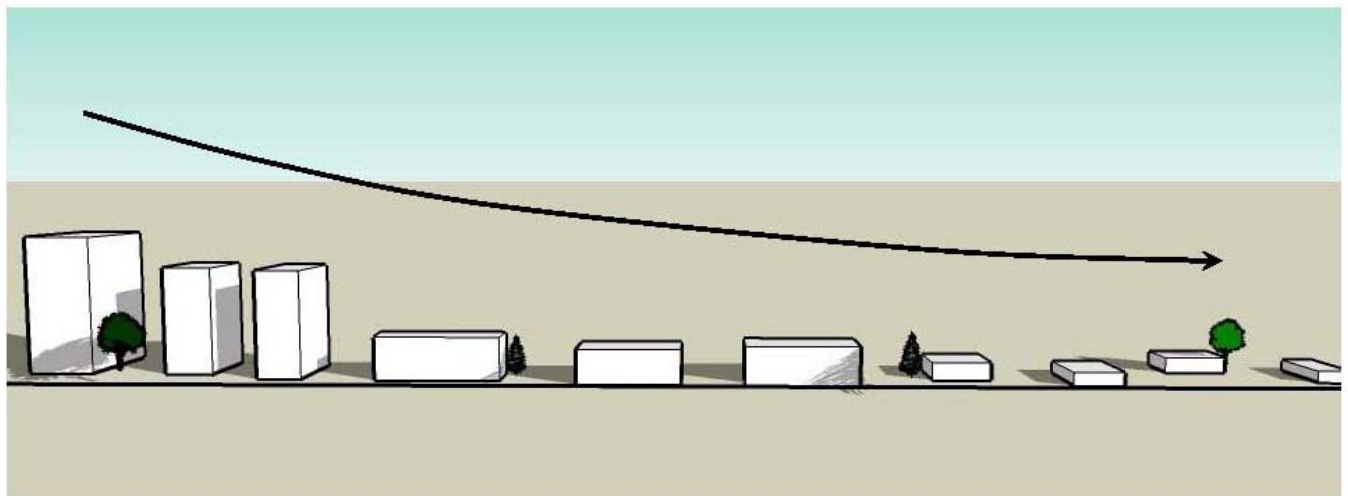


Figure 9-29: An appropriate transition of built form will be required to the existing and planned context.

l. privacy and overlook; and

m. the function and use of buildings, structures and landscapes.

9.5.1.3 Site designs and buildings will create a sense of enclosure along the street edge with heights appropriate to the surrounding context.

9.5.1.4 Buildings, in conjunction with site design and landscaping, will create appropriate visual and functional relationships between individual buildings, groups of buildings and open spaces.

9.5.1.5 Developments will provide a transition in building height and form between Intensification Areas and adjacent Neighbourhoods with lower density and heights.

9.5.1.6 Existing vegetation patterns and preservation and/or enhancement of the Urban Forest will be addressed in all new development.

9.5.1.7 Developments adjacent to public parkland will complement the open space and minimize negative impacts.

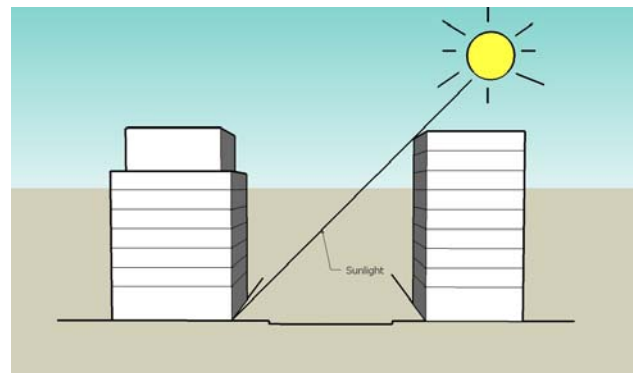


Figure 9-31: The height and massing of buildings should not obstruct sunlight from reaching the public sidewalk.

9.5.1.8 Proposed development should encourage public open space connections that link public parks and **community facilities** through the use of walkways, bikeways and bridges.

9.5.1.9 Development proposals will demonstrate compatibility and integration with surrounding land uses and the public realm by ensuring that adequate privacy, sunlight and sky views are maintained and that microclimatic conditions are mitigated.

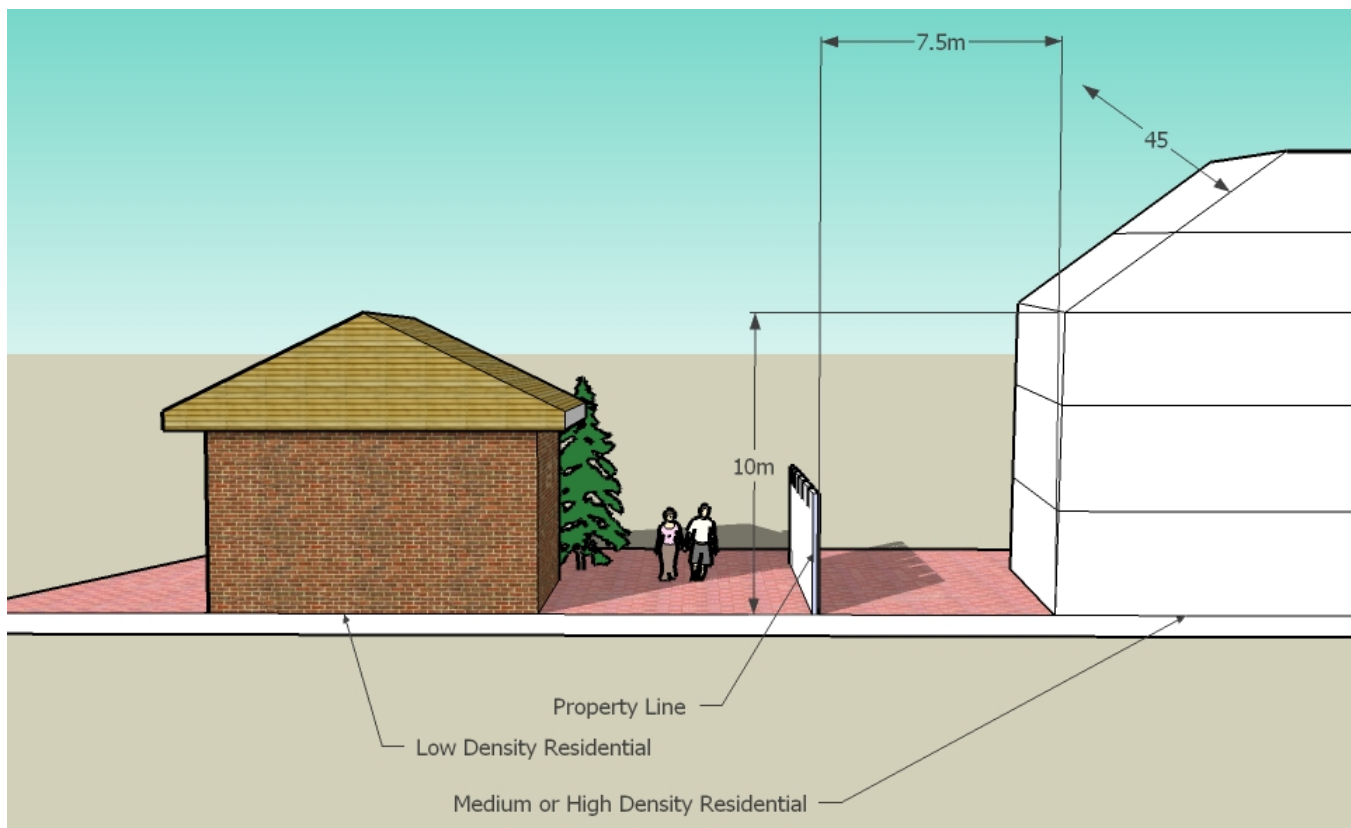


Figure 9-30: Development will respect the existing context and provide an appropriate transition in built form to low-rise residential areas.

9.5.1.10 Where employment and commercial uses are adjacent to noise sensitive uses, noise mitigation should be provided at the source of the noise to ensure compatibility and acceptable noise levels.

9.5.1.11 New residential development abutting major roads should be designed with a built form that mitigates traffic noise and ensures the attractiveness of the thoroughfare.

9.5.1.12 Noise will be mitigated through appropriate built form and site design. Mitigation techniques such as fencing and berms will be discouraged.

9.5.1.13 Buildings with exposure to Provincial Highways or public streets in areas of site plan control will be subject to a higher standard of design to achieve upgraded building elevations and landscaping, including principal doors and window fenestration.

9.5.1.14 Sites that have exposure to parks or double exposure to both Provincial Highways and public streets will be required to be designed with upgraded building elevations and landscaping facing all parks, public highways and public streets.

9.5.1.15 Development in proximity to landmark buildings or sites, to the Natural Areas System or cultural heritage resources, should be designed to:

- a. respect the prominence, character, setting and connectivity of these buildings, sites and resources; and
- b. ensure an effective transition in built form through appropriate height, massing, character, architectural design, siting, setbacks, parking, amenity and open spaces.

## 9.5.2 Site Development

The arrangement of elements on a site, as well as their massing and design, should contribute to achieving the City's vision and the intended character for the area. The development of a property may include one or more buildings or structures, services and utilities, parking areas and driveways and landscaping. Site design which incorporates **stormwater best management practices** will assist in achieving sustainable development objectives.

9.5.2.1 High quality, diverse and innovative design will be promoted in a form that reinforces and enhances the local character, respects its immediate context and creates a quality living or working environment.



Figure 9-32: Building façades should be parallel to the street edge with an appropriate architectural expression addressing the Highway 401 frontage. (Airport Corporate Centre)



9.5.2.2 Developments will be sited and massed to contribute to a safe and comfortable environment for pedestrians by:

- a. providing walkways that are connected to the public sidewalk, are well lit, attractive and safe;
- b. fronting walkways and sidewalks with doors and windows and having visible active uses inside;
- c. avoiding blank walls facing pedestrian areas; and
- d. providing opportunities for weather protection, including awnings and trees.

9.5.2.3 Development proponents will be required to ensure that pedestrian circulation and connections are accessible, comfortable, safe and integrated into the overall system of trails and walkways.

9.5.2.4 Where direct vehicular access to development is not permitted from major roads, buildings should be designed with front doors of individual units oriented towards the major road with vehicular access provided from a side street, service road or rear laneways.

9.5.2.5 Development proponents may be required to upgrade the public boulevard and contribute to the quality and character of streets and open spaces by providing:

- a. street trees and landscaping, and relocating utilities, if required;
- b. lighting;



Figure 9-34: Bio-retention features, such as **stormwater best management practices**, are encouraged to be incorporated in site development.



Figure 9-33: Permeable paving is used to enhance tree preservation efforts and to promote stormwater infiltration at Dominican University, in River Forest, Illinois.



Figure 9-35: Generous landscaped islands help to minimize the urban heat island effect.



- c. weather protection elements;
- d. screening of parking areas;
- e. bicycle parking;
- f. public art; and
- g. street furniture.

9.5.2.6 Development proponents will be required to demonstrate the successful application of **universal design principles** and compliance with legislated standards.

9.5.2.7 Site development should respect and maintain the existing grades on-site.

9.5.2.8 Site designs that conserve energy will be encouraged. Energy conservation will be addressed at the development application stage and during the preparation of building and site designs. Buildings should be designed, oriented, constructed and landscaped to minimize interior heat loss and to capture and retain solar heat energy in the winter and to minimize solar heat penetration in the summer.

9.5.2.9 Site designs will be encouraged that minimize the consumption of water.

9.5.2.10 Site development will be encouraged to meet a minimum standard of LEED Silver or custom green development standards.

9.5.2.11 Site development will be required to:

- a. incorporate **stormwater best management practices**;
- b. provide enhanced **streetscape**;
- c. provide landscaping that complements the public realm;
- d. include the use of native non-invasive plant material;
- e. protect and enhance habitat;
- f. preserve significant trees on public and private lands;
- g. incorporate techniques to minimize urban heat island effects such as providing planting and appropriate surface treatment; and
- h. provide landscaping that beautifies the site and complements the building form.

9.5.2.12 Heating, venting and air conditioning equipment and mechanical/utility functions will be



Figure 9-36: The Hazel McCallion Academic Learning Centre at the University of Toronto Mississauga demonstrates the importance of high quality architecture and the use of sustainable building materials to conserve energy. This building achieved a LEED Silver designation from the Canadian Green Building Council.

located away from the public realm and not be visible from public view.

9.5.2.13 External lighting for site development should:

- a. be energy efficient;
- b. utilize dark skylight fixtures; and
- c. not infringe on adjacent properties.

9.5.2.14 Development on a site may be phased provided that the location of buildings and services allow for future development. For projects that will be phased, applications shall be accompanied by a detailed phasing plan.



Figure 9-37: A safe and accessible pedestrian environment is created when doors front onto public sidewalks. (Port Credit)

### 9.5.3 Buildings

Buildings are often the most noticeable aspect of site development and therefore, the quality of their design and the materials selected is fundamental to good urban form. The articulation of a building is often what gives it a human scale and a sense of quality through attention to detail. The entrance of a building is often the most recognizable and used part of the façade and should be prominent, recognizable and accessible.

All buildings should be designed to incorporate sustainable technologies. Where **tall buildings**

occur, they are prominent features of the urban form and should be designed to the highest of standards.

9.5.3.1 Buildings will be designed to create a sense of identity through the site layout, massing, forms, orientation, scale, architectural features, landscaping and signage.

9.5.3.2 Buildings must clearly address the street with principal doors and fenestrations facing the street in order to:

- a. ensure main building entrances and at grade uses are located and designed to be prominent, face the public realm and be clearly visible and directly accessible from the public sidewalk:
- b. provide strong pedestrian connections and landscape treatments that link the buildings to the street; and
- c. ensure public safety.

9.5.3.3 Building façades should be articulated to include changes in materials, or material treatments, as well as the indication of transition between floors and interior spaces to provide visual interest and relief.

9.5.3.4 Principal building entrances should be covered with a canopy, awning, recess or similar device to provide visual prominence and pedestrian



Figure 9-38: Green roofs are an effective **stormwater best management practice** and contribute to the reduction of the urban heat island effect.

weather protection.

9.5.3.5 Front building façades should be parallel to the street. Consideration may be given to allow for periodic indentation for visual relief and features such as urban plazas.

9.5.3.6 Street facing façades should have the highest design quality. Materials used for the front façade should be carried around the building where any façades are exposed to the public view at the side or rear.

9.5.3.7 Buildings will be pedestrian oriented through the design and composition of their façades, including their scale, proportion, continuity, rhythms, texture, detailing and materials.

9.5.3.8 Buildings should avoid blank street wall conditions. Blank walls resulting from phased development, will require upgraded architectural treatment.

9.5.3.9 **Tall buildings** will minimize undue physical and visual negative impact relating to:

- a. microclimatic conditions, including sun, shadow and wind;
- b. noise;
- c. views;
- d. skyview; and
- e. adjacent cultural heritage resources, open spaces, the public realm, community infrastructure and residences.

9.5.3.10 The lower portion of **tall building** developments will include a built form that achieves street frontage and at grade relationships to support a pedestrian oriented environment.

9.5.3.11 Building materials should be chosen for their functional and aesthetic quality, sustainability and ease of maintenance.

9.5.3.12 The choice of building materials should minimize the risk for bird collisions.



Figure 9-39: Attractive and comfortable public spaces enhance the pedestrian experience. (Streetsville)

9.5.3.13 Where appropriate, development should be designed to incorporate measures that minimize urban heat island effects.

9.5.3.14 Buildings should be designed to conserve energy and incorporate sustainable material.

9.5.3.15 Buildings should be designed to minimize the consumption of water and to utilize **stormwater best management practices**.

9.5.3.16 Buildings should coordinate and integrate vehicular and servicing access to minimize their visual prominence.

9.5.3.17 Mechanical equipment, vents and metering devices will be integrated into the building design and will not be visible from the public realm.

9.5.3.18 Rooftop mechanicals and appurtenances will be integrated into building design and will not be visible from the public realm and residential developments.

9.5.3.19 It will be the responsibility of proponents of development applications to comply with Airport height restrictions.





Figure 9-40: The public realm in an urban environment constitutes an array of publicly accessible spaces which create a memorable sense of place. Wide public sidewalks with street trees, special pavement, animation and buildings framing the street contribute to a desirable public realm.

## 9.5.4 Relationship to Public Realm

The public realm is an integral part of any site development. The relationship between buildings, site layout and elements within the public realm has a great impact on the urban form and the experience of those who live, work and play in the city.

9.5.4.1 Development proposals should enhance public streets and the open space system by creating a desirable street edge condition.

9.5.4.2 An attractive and comfortable public realm will be created through the use of landscaping, the screening of unattractive views, protection from the elements, as well as the buffering of parking, loading and storage areas.

9.5.4.3 The sharing and reduction of access points/driveways will be encouraged to promote pedestrian safety and provide the opportunity for a continuous **streetscape**.

9.5.4.4 Along **Corridors** where an urban character is appropriate, buildings should be located close to and aligned with the street to enclose the street.

9.5.4.5 Built form will relate to the width of the street right-of-way.

9.5.4.6 Outdoor storage should not be located adjacent to, or be visible from city boundaries, the public realm or residential lands by incorporating the use of appropriate setbacks, screening, landscaping and buffering.

9.5.4.7 Display areas are to be an integral part of the overall site design and evaluated based on their visual impact on the **streetscape**.

## 9.5.5 Parking, Servicing and Loading

The design of parking, servicing and loading areas is a key component in the development of sites. These areas serve a functional need, but should be designed in a manner that screens less desirable aspects and provides high quality treatment of exposed areas while addressing safe and efficient movement of pedestrians and vehicles. Parking surfaces are a contributor to the urban heat island effect and, as such, should be designed to mitigate the heat effects.





Figure 9-41: Service and loading areas should be discreet, located on-site, recessed into the building and not be a dominant element of the **streetscape**.

9.5.5.1 Parking should be located underground, internal to the building or to the rear of buildings.

9.5.5.2 Above grade parking structures should be screened in such a manner that vehicles are not visible from public view and have appropriate directional signage to the structure.

9.5.5.3 Where surface parking is permitted, the following will apply. Parking should:

- a. not be located between the building and the street;
- b. incorporate **stormwater best management practices**, such as, permeable paving, bio-retention areas and tree clusters;
- c. provide safe and legible raised walkways, with curb ramps, within parking areas to buildings and streets;
- d. incorporate **universal design principles**;
- e. be configured to permit future development;

- f. have appropriate landscape treatment including trees and lighting, throughout parking lots;
- g. provide appropriate landscape treatment to provide shading of parking areas; and
- h. provide landscape buffering at the street edge.

9.5.5.4 Shared parking between developments will be encouraged, where appropriate.

9.5.5.5 Secure bicycle parking will be provided in developments.

9.5.5.6 Site plans will demonstrate the ability for shared servicing access between adjacent developments.

9.5.5.7 Service, loading and garbage storage areas should be internal to the building or located at the rear of the building and screened from the public realm.

## 9.5.6 Safety



Figure 9-42: The Mississauga **Crime Prevention Through Environmental Design (CPTED)** Committee was established in 1994 to provide an interdisciplinary approach to reduce opportunities for crime in the built environment.

**Crime Prevention Through Environmental Design (CPTED)** concepts and principles have the objective of promoting safe living and working environments. Following **CPTED** principles, the public and private environment will be maintained at a level that enhances the public perception of safety and buildings, landscaping and site layout and will be designed to enhance personal safety.

9.5.6.1 Site layout, buildings and landscaping will be designed to promote natural surveillance and personal safety.

9.5.6.2 Active building frontages should be designed to face public spaces including entries and windows to ensure natural surveillance opportunities.

9.5.6.3 Development should clearly define areas of access and egress to avoid the creation of entrapment areas.

9.5.6.4 Development should incorporate lighting to ensure all designated areas of circulation, entrance, and connections are appropriately illuminated.

## 9.5.7 Signage

Signage is a significant element of the city's identity and character. Signage is both public and private. Signs include street names, interpretative and commemorative plaques, advertising and identification of uses.

9.5.7.1 Signage should:

- a. orient people as they move through the city;
- b. identify businesses and services;

- c. promote and enhance an area's character;
- d. identify significant sites, community uses, destinations and landmarks;
- e. recognize cultural heritage resources; and
- f. follow ***universal design principles***.

9.5.7.2 Building and site designs will integrate signage and have regard for the character of the building, landscape and context. Signage should identify and inform as well as complement and enliven the ***streetscape***. Signage must be designed to minimize visual clutter.

9.5.7.3 Except for wayfinding and commemorative plaques, only fascia signs will be permitted within Intensification Areas.



Figure 9-43: Signage will be designed to provide identity, orientation for pedestrians and have a compatible relationship with built form and the street.

# 10 Foster a Strong Economy

## 10.1 Introduction

Mississauga is an economically strong city where people and businesses thrive. The city's economic

success is based on its ability to attract and retain a diversity of business operations. Wholesale trade; manufacturing; professional, scientific and technical services; transportation and warehousing companies, as well as various other employment



Figure 10-1: There are over 420 000 employment opportunities in Mississauga providing jobs for local as well as regional residents. While the largest concentration of these employment opportunities is in the northeast quadrant of the city at the Airport and surrounding lands, Mississauga has a number of other thriving employment areas. Mississauga has been highly successful in attracting office employment and over the last 25 years the city has developed an average of approximately one million square feet of office space annually. (Robert Speck Parkway Business Area, Hewlett-Packard Building, Airport -Terminal 1)



sectors, are all represented in the city. Many of these companies are innovative businesses, large corporations and head offices. Mississauga is home to over 60 Fortune 500 head offices.

Mississauga's strong employment base has resulted in the city becoming a net importer of talent. Two thirds of the resident labour force work in Mississauga, which demonstrates the variety of employment opportunities available to residents.

Mississauga is home to a well educated labour force. Ensuring that residents have access to opportunities to develop the skills and knowledge needed to meet the needs of the business community is critical to the city's continued economic success. Mississauga is home to The University of Toronto Mississauga and the Sheridan College campus in the Downtown. In addition to these post-secondary institutions, the City promotes the establishment of new universities and colleges so that local students have a variety of learning opportunities within their community. This will also attract new residents to the city to further their education, retain youth and encourage partnerships between academic institutions and businesses.



Figure 10-2: Retaining areas for employment purposes is vital to the health and competitiveness of the city. As the city's population grows through intensification and redevelopment, it will be critical to maintain the existing employment base and to provide opportunities for future employment.

Mississauga will continue to support existing businesses and work toward attracting new business ventures that complement the City's vision and provide a range of employment opportunities that utilize the skills and knowledge of the local labour force.

The city identifies three strategic economic development goals that support the city's vision: to be a Global Business Magnet, to have a Culture of Innovation and to be a Knowledge Economy. In this context, a number of target opportunities have been recognized in high growth knowledge sectors, including Life Sciences; Information, Communication Technologies (ICT); Finance and Insurance; and Advanced Manufacturing.

Each of these sectors has a strong presence in Mississauga today, with recognizable clusters in areas such as Airport Corporate Centre, Gateway Corporate Centre and Meadowvale Business Park. In addition, Sheridan Park is characterized by a research cluster. It is critical for the city to continue to grow and sustain these knowledge sectors within the city.

The urban system supports the business community by promoting office and employment uses within Intensification Areas, while preserving lands for a variety of activities and extensive employment functions. Employment opportunities are to be concentrated within the Downtown, Corporate Centres, Major Nodes and Employment Areas. Special Purpose Areas are also important employment centres. Within Community Nodes and Neighbourhoods, employment opportunities that provide local services to residents are anticipated.

As Mississauga continues to develop, there will be fewer opportunities for land extensive business activities and a greater reliance on office development as a generator of employment. Mississauga is fortunate to have several highly successful office areas and is well positioned to attract considerable additional office growth. Mississauga's Downtown and Corporate Centres are particularly well located for future office development as they have excellent access to



existing and planned **higher order transit** services and several 400 series highways.

The provision of infrastructure and utilities in an environmentally sustainable, timely manner, and their maintenance is key to continued economic development and growth. Mississauga will encourage energy conservation and reduce air pollution and greenhouse gas emissions through land use patterns and urban design standards that support alternative forms of transportation, energy efficient buildings and opportunities for **cogeneration**.

10.1.1 Mississauga will encourage a range of employment opportunities reflective of the skills of the resident labour force.

10.1.2 Mississauga will identify and protect lands for a diversity of employment uses to meet current and future needs.

10.1.3 An adequate supply of lands providing locations for a variety of appropriate employment uses will be maintained to accommodate the City's growth forecasts.

10.1.4 The conversion of lands designated Business Employment, Industrial, Institutional or Office within Corporate Centres and Employment Areas to permit non-employment uses is prohibited unless considered through a Phase One municipal comprehensive review and, where applicable, a Phase Two municipal comprehensive review. For the purposes of this policy, **major retail** uses are considered non-employment uses.

a. A Phase One municipal comprehensive review will be required to demonstrate that:

- there is a need for the conversion;
- Mississauga will meet the employment forecasts of this Plan;
- the conversion will not adversely affect the overall viability of the employment area and achievement of the intensification target, density targets and other policies of this Plan;

- there is existing or planned infrastructure to accommodate the proposed conversion;
- the lands are not required over the long term for employment purposes; and
- cross jurisdictional issues have been considered; and

b. Where a Phase One municipal comprehensive review has identified the potential for the conversion of lands, Phase Two of the municipal comprehensive review will determine appropriate land uses for identified areas and consider, among other matters, the following:

- alternative locations for displaced employment uses;
- land use options that result in a similar or greater number of employment opportunities;
- compatibility with surrounding land uses;
- infrastructure capacity, needs and costs; and
- municipal benefits to be realized through land conversion; and

c. Development applications for the conversion of lands to non-employment uses will be considered premature until both Phase One and Phase Two of the municipal comprehensive review has been completed.

10.1.5 Mississauga will provide for a wide range of employment activities including office and diversified employment uses. To this end Mississauga will:

- a. strive to increase office employment;
- b. encourage the establishment of knowledge based industries and support their growth; and
- c. encourage the establishment of small innovative businesses and support their growth.

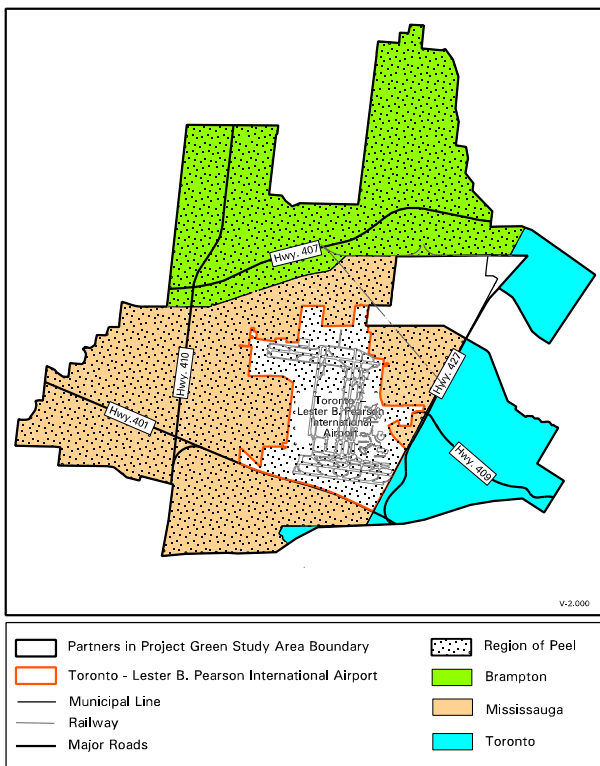


Figure 10-3: Partners in Project Green is a growing community of businesses working together to green their bottom line by creating an internationally recognized 'eco-business zone' around the Airport. Through new forms of business-to-business collaboration, Partners in Project Green delivers programming that helps businesses reduce energy and resource costs, uncover new business opportunities and address everyday operational challenges in a green and cost effective manner.

10.1.6 Mississauga will facilitate the operation and where appropriate, the expansion of existing businesses as permitted by this Plan. In some locations, alternative land uses may be identified to encourage the relocation of existing businesses to allow the lands to redevelop in accordance with the planning vision for the area. Development proponents may be required to submit satisfactory studies prior to development.

10.1.7 To encourage economic development and competitiveness, Mississauga will ensure the necessary infrastructure, for which it is responsible, is provided to support current and forecasted employment needs.

10.1.8 Transit supportive development with compact built form and minimal surface parking will be encouraged in Corporate Centres, **Major Transit Station Areas** and **Corridors**.

10.1.9 Mississauga will foster eco-industrial activity in new and existing employment areas, such as the Pearson Eco-Business Zone, which will demonstrate innovation and high levels of environmental and economic performance by:

- transforming the employment area into an eco-industrial zone;
- creating a sustainable economic area and green business areas;
- investing in green buildings, technology and practices;
- encouraging leveraged partnerships between public and private organizations; and
- establishing infrastructure with multi objective.

10.1.10 Residential uses will be prohibited within Corporate Centres and Employment Areas.

## 10.2 Office

Mississauga's success in attracting office development is an asset to the economy. Current office development is concentrated within the Corporate Centres, however, the Downtown and Employment Areas also have considerable office development. Promoting office development in the Downtown is of particular importance to the City in order to support **higher order transit** and create a lively mixed use live/work area.

10.2.1 Major office development will be encouraged to locate within the Downtown, Major Nodes, Corporate Centres and **Intensification Corridors**.

10.2.2 **Secondary office** within Employment Areas will be encouraged to locate within **Major Transit Station Areas** and **Corridors**.



Figure 10-4: Over the years Mississauga has been able to attract many diverse businesses. Many of these offices have concentrated in the city's Corporate Centres, Employment Areas and Downtown. In the future, the City will promote increased office use and activity within the Downtown.

10.2.3 Outside of Employment Areas, **secondary office** development will be encouraged to locate within Community Nodes and **Major Transit Station Areas**.

10.2.4 Within Intensification Areas, ground floor retail uses are encouraged within office buildings. Local Area Plans may determine where ground floor retail uses will be required.

10.2.5 Offices associated with employment uses such as manufacturing, research and development and warehousing will be permitted as an accessory use.

## 10.3 Industry

Industrial uses include manufacturing, assembling, processing, fabricating, research and development, sales and service, warehousing, distributing and wholesaling.

Industrial activities contribute significantly to the city's economic base. Industrial uses should locate in appropriate areas, such as Employment Areas. It is a priority for Mississauga to ensure that sensitive land uses remain separated from industrial uses.

There is only one quarry remaining in operation in the Southdown Character Area and no new quarries will be permitted. Existing areas of mineral resources will operate in compliance with legislation of the Provincial Government and the policies of this Plan.

10.3.1 Industrial uses will be permitted to locate within Employment Areas. Character Area policies may identify sites permitting industrial uses outside of Employment Areas.

10.3.2 Mississauga will protect lands within Employment Areas for industrial uses.

10.3.3 Development will minimize land use conflicts between industrial uses and sensitive land uses.

10.3.4 Within Employment Areas, Mississauga will support the continued operation of existing industrial uses and discourage employment uses and sensitive land uses in the vicinity of existing industrial land uses that would:

- a. require industrial uses to significantly modify their operations;
- b. cause industrial uses to be in non-compliance with pertinent standards; and
- c. inhibit the development of designated industrial lands for the purposes permitted by this Plan.

10.3.5 Industrial uses in proximity to residential uses and sensitive land uses will:





Figure 10-5: Mississauga is home to many warehousing and distribution centres, providing many employment opportunities within the city. These types of uses, along with other industrial uses, are best served by locating within Employment Areas away from sensitive land uses.

- a. not have outdoor storage;
- b. not generate air pollution, odour or excessive noise; and
- c. have a high standard of building design and landscaping.

10.3.6 Mississauga may identify alternative land uses and redesignate sites with existing industrial uses outside of Employment Areas or within Intensification Areas to encourage the relocation of the existing business, thus allowing the lands in the vicinity to redevelop in accordance with the existing or planned land uses.

10.3.7 The conversion of multi-unit industrial developments to industrial condominiums will be discouraged for lands within Intensification Areas and along **Corridors**.

10.3.8 Applications to expand an existing quarry must determine the area of influence and other additional information deemed necessary by the City.

10.3.9 Progressive rehabilitation of excavated pits and quarries will proceed toward a compatible after use for the site in accordance with approved site development and rehabilitation plans approved by the City and Provincial Government.

10.3.10 Processing of excavated materials and related activities must occur within, or contiguous to, the area licensed.

10.3.11 Prior to the consideration of development proposals within the area of influence of quarry operations, it will be demonstrated to the satisfaction of the City, that there will be no adverse impacts that would make it incompatible with the quarry operation so as not to preclude the continued use of the quarry.

## 10.4 Retail

Retail means the sale, lease and/or rental of goods to the public. Retail is often combined with other uses such as personal service establishments, offices, financial institutions, restaurants and overnight accommodations.

The primary locations for retail uses will be the Downtown, Major Nodes and Community Nodes. Retail uses within these locations will be encouraged to contribute to a vibrant, mixed use environment and be developed in combination with residential and office uses.

Within Corporate Centres, retail uses in conjunction with office developments will be encouraged in order to provide services to local businesses and

employees. New freestanding retail uses will not be permitted.

Within Neighbourhoods, further retail commercial will be directed to lands designated Mixed Use. Retail uses will be encouraged to develop in combination with residential and office uses.

While Employment Areas have a number of existing retail areas, they are not the preferred location for this type of use. Existing designated retail areas will be recognized by this Plan and further development of retail uses within the limits of land designated Mixed Use is permitted, however, their expansion and the establishment of new **major retail** areas will not be allowed. Existing retail areas will be encouraged to redevelop to appropriate non-retail employment uses.



Figure 10-6: Commercial uses are a staple for everyday living. These uses will be concentrated in the Downtown, Major Nodes and Community Nodes. Some retail services will be provided in Neighbourhoods. Not only is the location of commercial uses important in servicing residents, but the scale and design of these structures is important in creating a comfortable sense of place where people want to gather.



10.4.1 Retail uses are encouraged to locate primarily within the Downtown, Major Nodes and Community Nodes.

10.4.2 Retail uses will be permitted within Corporate Centres, where they support employment uses and employees. Character Area policies or local area plans will identify appropriate locations and types of uses.

10.4.3 Retail uses may be permitted within Neighbourhoods to provide retail uses convenient to the local residents. Character Area policies or local area plans will identify appropriate locations and types of uses.

10.4.4 Within the Downtown, Major Nodes, Community Nodes and Corporate Centres, existing single storey retail development will be encouraged to redevelop into multistorey mixed use developments.

10.4.5 Retail uses outside the Downtown, Major Nodes and Community Nodes will be directed to **Corridors** and **Major Transit Station Areas** or in locations as identified in Character Area policies or local area plans.

10.4.6 The dispersion of retail uses beyond designated commercial areas will be discouraged.

10.4.7 Lands designated for retail areas within Employment Areas are recognized by this Plan, however, no additional lands will be designated for retail development unless recommended through a municipal comprehensive review.

10.4.8 Local area plans or planning studies will consider alternative land uses for lands designated for retail uses within Employment Areas.

## 10.5 Post-Secondary Institutions

Post-secondary institutions can attract and support the growth of strong, innovative businesses, and further the needs and interests of youth, older adults and recent immigrants to Mississauga. Improved transit facilities and providing for a range of suitable, affordable housing choices are key to attracting new post-secondary schools, colleges and universities to Mississauga.



Figure 10-7: The University of Toronto Mississauga (UTM) has greatly expanded over the past few years. The university has built additional educational, recreational and housing structures for student use. Mississauga looks forward to working with other universities and colleges, who choose to locate within the city, to create new campuses with similar amenities as UTM.



10.5.1 Mississauga will encourage the expansion of existing and the establishment of new post-secondary institutions within the city, particularly within the Downtown, Major Nodes, and in proximity to **Major Transit Station Areas**.

10.5.2 In addition to educational, research, office and residential uses, other land uses that support the functioning of the post-secondary institution and the needs of the staff, students and visitors will be permitted.

## 10.6 Infrastructure and Utilities

The city is well served by a comprehensive network of infrastructure and utilities that supports its economic function and growth. Infrastructure and related services will be provided in a coordinated, timely manner and maintained at a level that is financially sustainable to meet the needs of the existing businesses and residents, as well as providing for future growth.

For the purposes of this Plan, infrastructure and utilities includes sanitary sewer and water supply, stormwater management facilities and systems, gas and oil transmission pipelines, **electric power distribution and transmission facilities**, telecommunications and other cabled services. These are provided by various government agencies, public bodies and the private sector.

10.6.1 In cooperation with other levels of government, appropriate agencies and the private sector, Mississauga will ensure that all development will have adequate infrastructure and utilities.

10.6.2 Mississauga will work with utility providers to ensure that networks are established and phased to serve new development in a timely and efficient manner.

10.6.3 Mississauga will undertake discussions with utility providers regarding the feasibility of servicing existing and future employment areas with leading edge telecommunications services, including

broadband technology, to attract knowledge based industries and support the economic development, technological advancement and growth of existing businesses.

10.6.4 Mississauga will encourage discussions with utility providers to determine appropriate utility design and placement within Intensification Areas prior to determining the placement and design of utilities.

10.6.5 Mississauga will ensure that full Regional and municipal services are available to serve all development.

10.6.6 Where possible, the existing conditions should be augmented by the re-establishment of native vegetation and the preservation of existing landforms, vegetation and drainage patterns. Where possible, at-source controls should be provided to reduce the need for new infrastructure. All efforts to this effect should be guided by the appropriate environmental agencies, according to all Provincial



Figure 10-8: The 27 hectare Lakeview Water Treatment Facility is located on the shore of Lake Ontario in Mississauga and is operated by the Region. The Region has identified the need for a capacity expansion of the facility as a result of increased growth to serve the eastern part of Peel and to meet servicing requirements in York Region. The expansion of the Lakeview plant will increase capacity to produce 1 150 million litres of water per day.

Government, Regional Government and municipal regulations.

10.6.7 The preservation of existing trees and the planting of new trees will be given priority and coordinated with utility placement within the public boulevard.

10.6.8 Mississauga will maintain and establish programs for renewal of infrastructure and utilities. In doing so, Mississauga will ensure that the capital cost, maintenance cost and environmental impact are minimized.

10.6.9 Mississauga expects that the Region of Peel will provide appropriate and timely water, and wastewater facilities to serve the city's development.

10.6.10 Appropriate storm sewer facilities will be installed for the safety of residents, with due regard for the need to protect **watercourses** and associated ecosystems from any possible destructive effects of stormwater runoff.

10.6.11 The sizing of storm sewer facilities will be based on the ultimate development pattern within the various drainage areas.

10.6.12 Development proposals will be subject to the recommendations of watershed studies to assess potential effects of these proposals on receiving **watercourses**. In addition, development will be required to implement stormwater management strategies in accordance with the most current stormwater management guidelines prepared by the City, the appropriate Conservation Authority and the Provincial Government.

10.6.13 Any stormwater quality/quantity facilities within Greenbelt lands must have regard for the viability of natural areas and their features and functions, and will be subject to naturalization efforts to the satisfaction of the City and the appropriate Conservation Authority, as a part of development.

10.6.14 Storm sewer services policies are to be read in conjunction with urban drainage policies.

10.6.15 Natural gas and oil pipelines are permitted in all land use designations, except Greenbelt.

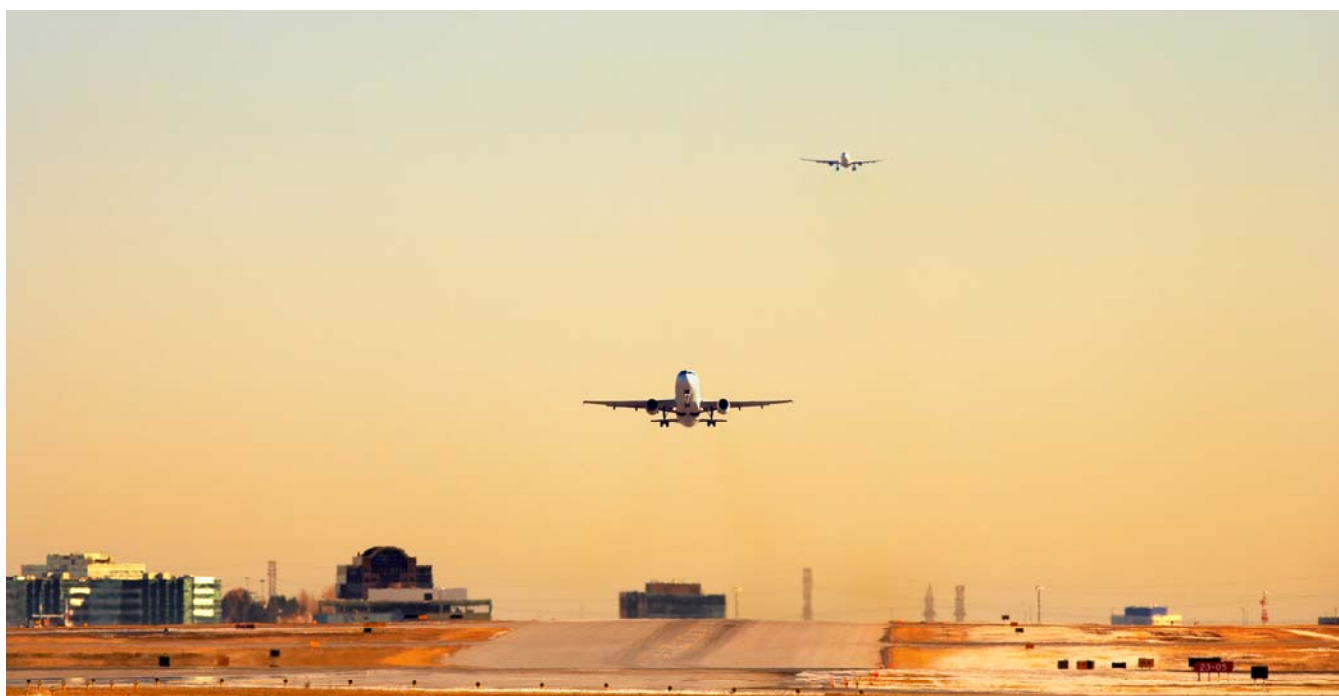


Figure 10-9: The Airport contributes to the success of the city's economy. The Airport's economic benefits transcend the Airport area to local distribution centres, logistic businesses, courier companies and transportation companies, among others.

10.6.16 The design standards used in the construction of any natural gas or oil transmission pipeline through undeveloped areas within Mississauga will take into account ultimate urbanization near or adjacent to that pipeline.

10.6.17 Setbacks of a residence, place of work, or public assembly to an oil or gas easement or associated structure, and an appropriate building design, will be determined based on the type of pipeline, stress level of the pipeline and shall take into consideration the Guidelines for Development in the Vicinity of Oil and Gas Pipeline Facilities, prepared by the Technical Standards & Safety Authority.

10.6.18 Existing or new easements accommodating gas and/or oil pipelines should be incorporated into development plans as public open space, walkways or bicycle paths, but not be incorporated into individual lots. In some cases, in consultation with the pipeline operator, certain other uses such as vehicle parking, may be considered if it forms an integral part of the development.

10.6.19 Local service power lines, telephone and other cabled services will be located underground, where feasible and desirable.

10.6.20 Telecommunication facilities, including buildings and related structures, satellite dishes and cellular antennas should be designed and located to minimize visual impact in high profile and sensitive areas.

10.6.21 Mississauga will not permit electric power distribution and telecommunications facilities within residential areas if such installations are of a magnitude, function or character incompatible with the surrounding residential environment, and are not required to serve the residential area. Where such utility installations must be located in proximity to a residential area, the infrastructure will be located in a sensitive manner and designed to be compatible with the surrounding area.

10.6.22 To ensure the integration of utilities in future subdivision designs, wherever feasible, services will be located in road rights-of-way. Where this

standard location is not possible, the provision of utility easements will be such that:

- a. the land use pattern of the area in which the easement is to be placed is minimally affected; and
- b. the environmental policies of this Plan are observed.

## 10.7 Energy and Power Generation

Energy efficiency and improved air quality through land use, development patterns and efficient transportation, are important for the health of Mississauga's population, economic prosperity and



Figure 10-10: Mississauga Official Plan provides opportunities for power generation and distribution facilities situated in appropriate locations.



protection of the environment. The City supports energy initiatives that will enable it, its residents, businesses and transportation systems to conserve energy and reduce their dependence on fossil based energy sources by changing to alternative or **renewable energy** systems.

This Plan provides opportunities for power generation facilities to accommodate current and projected needs, where feasible, and recognizes the interdependencies that exist in the built and natural environments. However, it is essential that power generating facilities be located in appropriate locations away from residential and other sensitive uses.

10.7.1 Mississauga will conserve energy by promoting energy efficient land use and development patterns, efficient transportation and alternative and **renewable energy** systems.

10.7.2 Mississauga will plan for greener, healthier communities by reducing harmful greenhouse gas emissions and improving the air quality.

10.7.3 Mississauga supports energy conservation, and the reduction of emissions from vehicles as well as from municipal, residential, commercial and industrial sources to protect air quality.

10.7.4 Mississauga supports energy demand management initiatives by other levels of government and other agencies to reduce energy consumption and improve energy efficiency.

10.7.5 Mississauga encourages appropriate land use and development patterns that are transit supportive, site and building designs that conserve energy and opportunities for district energy.

10.7.6 Mississauga encourages the creation of innovative strategies such as green site design and green buildings, which utilize technology such as green roofs, white roofs and the use of the urban tree canopy to achieve energy efficiencies.

10.7.7 Mississauga will promote public awareness and education initiatives jointly with other levels of



Figure 10-11: A 31 m wind turbine unveiled at the Lisgar GO Station in April 2009, generates as much as 80 per cent of the station's power. The site, located at Tenth Line and Argentia Road, was chosen for the wind turbine because of heavy prevailing winds from the west and its open fields.

government and other agencies on matters related to energy conservation.

10.7.8 Mississauga supports policies that promote water conservation measures and **stormwater best management practices** to ensure energy conservation.

10.7.9 Mississauga encourages district energy systems where appropriate, to reduce greenhouse gas emissions and air pollution.

10.7.10 Mississauga will work jointly with other levels of government and other agencies to investigate the need, feasibility, implications and suitable locations for **renewable energy** projects and to promote local clean energy generation, where appropriate.

# 11 General Land Use Designations

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## 11.1 Introduction

The use and development of land will reflect all components of the Urban System: The Green System; City Structure and **Corridors**. Part Three is organized by the elements of the City Structure, as shown on Schedule 1b: Urban System - City Structure. These elements are:

- Downtown;
- Major Nodes;
- Community Nodes;
- Corporate Centres;
- Neighbourhoods;
- Employment Areas; and
- Special Purpose Areas.

General policies applicable city wide for all land use designations are included in this chapter. Chapters 12 to 18 contain modifications to the general policies specific to each of the above City Structure elements. These modifications may add or delete permitted uses.

Mississauga is also divided into a series of Character Areas, the boundaries of which are shown on Schedule 9. Chapters 12 to 18 also contain policies regarding Character Areas where the general policies may be further modified. Therefore, to determine permitted land uses, the policies of Chapter 11 must be read in conjunction with the policies in Chapters 12 to 18.

Local area plans are part of this Plan. A local area plan may be prepared for all or parts of Character Areas, **Corridors** or **Major Transit Station Areas** and are not necessarily coincident with the boundaries of these areas. A local area plan will

contain general and detailed policies which elaborate on, or provide exception to the policies or schedules of this document, for the area to which it applies.

Schedule 10: Land Use Designations, identifies the uses of land permitted by Mississauga Official Plan and will be read in conjunction with Schedule 1b: Urban System - City Structure, as well as all other policies of this Plan. In the event of a conflict, the land use designations on Schedule 10 take precedence over the City Structure policies, and describe in detail the individual characteristics and policies that are distinct to each designation. Character Areas policies and local area plans are to be read in conjunction with all other policies of this Plan and take precedence in the event of a conflict.

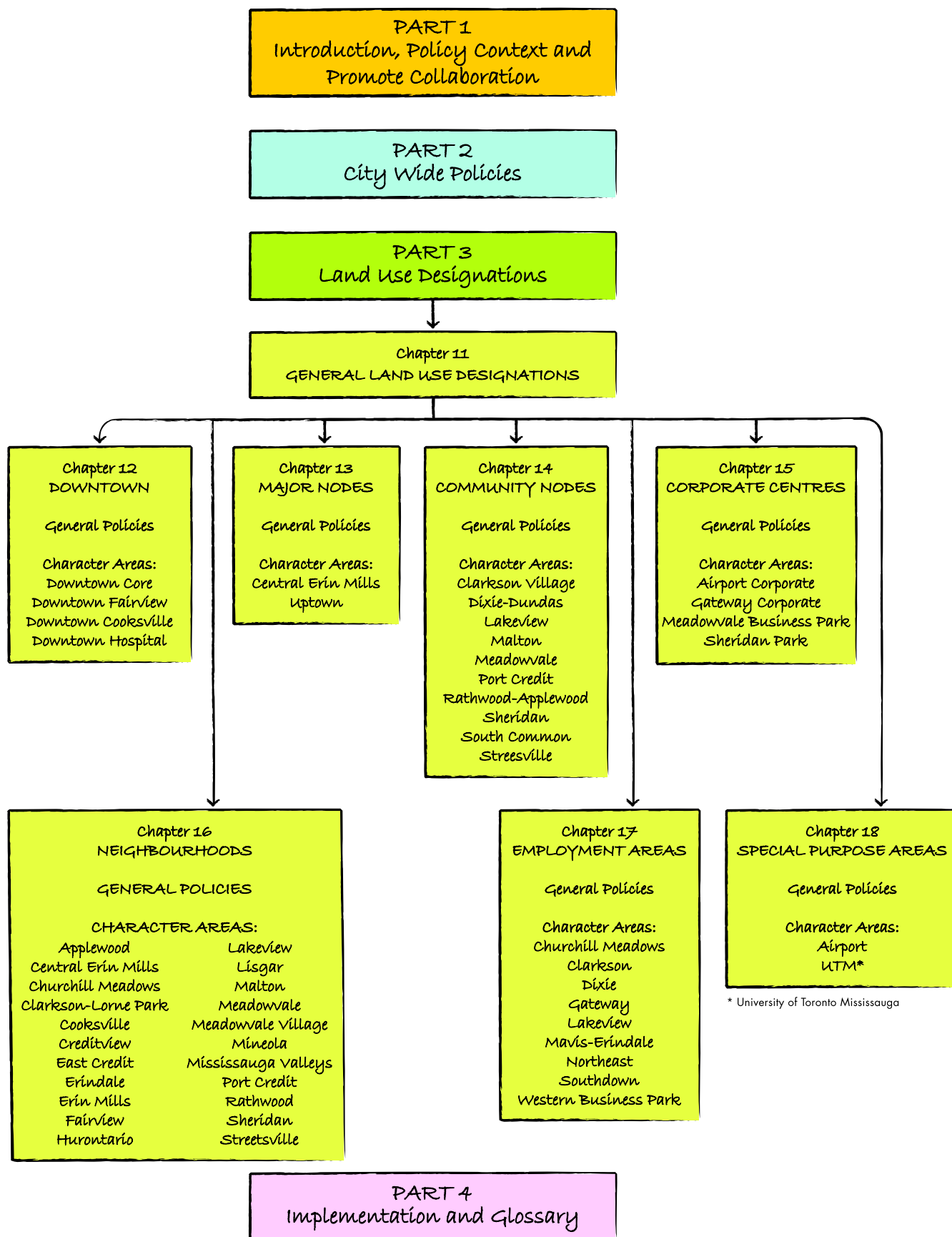


Figure 11-1: This chart shows a breakdown of the Mississauga Official Plan structure with a focus on Part 3.



## 11.2 General Land Use

### 11.2.1 Uses Permitted in all Designations

11.2.1.1 The following uses will be permitted in all land use designations, except Greenbelt unless specifically allowed:

- a. community infrastructure;
- b. community gardening;
- c. electricity transmission and distribution facility;
- d. natural gas and oil pipeline;
- e. parkland;
- f. piped services and related facilities for water, wastewater and stormwater; and
- g. telecommunication facility.

11.2.1.2 Publicly funded schools and **community facilities** are identified by symbols on Schedule 10: Land Use Designations, for information purposes only. These symbols may be deleted, moved or added to without amendment to this Plan.

### 11.2.2 Utility

11.2.2.1 In addition to the Uses Permitted in all Designations, lands designated Utility will also permit the following uses:

- a. parking; and
- b. accessory uses.

11.2.2.2 Utility uses are not permitted when lands are identified as a Provincially significant wetland.

11.2.2.3 When public works not subject to the *Environmental Assessment Act* are planned to traverse, coincide with, or otherwise affect the Natural Area System, an Environmental Impact Study will be required to be submitted to and

approved by the City and the appropriate conservation authority.

### 11.2.3 Greenbelt

11.2.3.1 Lands designated Greenbelt are generally associated with natural hazards and/or natural areas where development is restricted to protect people and property from damage and to provide for the protection, enhancement and restoration of the Natural Area System.

11.2.3.2 Lands designated Greenbelt permit the following uses:

- a. conservation;
- b. electricity transmission and distribution facilities;
- c. facilities that by their nature must locate near water or traverse **watercourses** (i.e. bridges, storm sewer outlets and stormwater management facilities);
- d. flood and/or erosion work;
- e. passive recreational activity;
- f. parkland;
- g. piped services and related facilities for water, wastewater and stormwater; and
- h. accessory uses.

11.2.3.3 Lands may be zoned Greenbelt within any land use designation.

11.2.3.4 Permitted uses will be subject to fulfilling the requirements of the appropriate conservation authority, the City and other appropriate approval agencies.

11.2.3.5 Passive recreational activities will be permitted where they are compatible with the viability of the natural area, while respecting appropriate buffers from **watercourses** and valley slopes.

11.2.3.6 Active recreational facilities will not be permitted.

11.2.3.7 Electricity transmission and distribution facilities, and piped services and related facilities used for water, wastewater and stormwater may only be permitted in Greenbelt if other options are not feasible provided that an Environmental Assessment has been completed in conformity with the *Environmental Assessment Act* or a satisfactory Environmental Impact Study has been approved by the appropriate conservation authority, the City and other appropriate approval agencies. If an Environmental Assessment is not required under the *Environmental Assessment Act*, the Environmental Impact Study shall evaluate all options available.

11.2.3.8 For existing lots of record, development will not be permitted to extend within the regulatory storm floodplain or hazard lands associated with a **valley and watercourse corridor** if there are lands suitable for development on the property beyond the hazard lands.

## 11.2.4 Open Space

11.2.4.1 The Open Space network consists of two designations:

- a. Public Open Space; and
- b. Private Open Space.

11.2.4.2 In addition to the Uses Permitted in all Designations, lands designated Public Open Space will also permit the following uses:

- a. agriculture demonstration site;
- b. cemetery;
- c. conservation;
- d. golf course;
- e. nursery gardening;
- f. recreational facility;

g. stormwater retention and stormwater quality pond; and

h. accessory uses.

11.2.4.3 Lands designated Private Open Space permit the following uses:

- a. cemetery;
- b. conservation;
- c. golf course; and
- d. accessory uses.

11.2.4.4 Crematoria, columbaria, and mausolea will be located only in cemeteries.

11.2.4.5 Stormwater retention and stormwater quality ponds are generally not appropriate uses for public parkland, however, in some instances overland flow pond stormwater facilities may be accommodated in public parkland.

11.2.4.6 Accessory uses should generally be limited to a maximum of 20% of the total Gross Floor Area (GFA) and should be on the same lot, clearly subordinate to the functioning of the permitted use.

## 11.2.5 Residential

11.2.5.1 Residential consists of four designations:

- a. Residential Low Density I;
- b. Residential Low Density II;
- c. Residential Medium Density; and
- d. Residential High Density.

11.2.5.2 In addition to the Uses Permitted in all Designations, residential designations will also permit the following uses:

- a. residential dwelling;
- b. accessory office for physicians, dentists, health professionals and drugless practitioners;

- c. home occupation;
- d. special needs housing; and
- e. urban gardening.

11.2.5.3 Lands designated Residential Low Density I will permit the following uses:

- a. detached dwelling;
- b. semi-detached dwelling; and
- c. duplex dwelling.

11.2.5.4 Lands designated Residential Low Density II will permit the following uses:

- a. detached dwelling;
- b. semi-detached dwelling;
- c. duplex dwelling, and
- d. triplexes, street townhouses and other forms of low-rise dwellings with individual frontages.

11.2.5.5 Lands designated Residential Medium Density will permit the following uses:

- a. townhouse dwelling; and
- b. all forms of horizontal multiple dwellings.

11.2.5.6 Lands designated Residential High Density will permit the following use:

- a. apartment dwelling.

11.2.5.7 Accessory offices for physicians, dentists, health professionals, and drugless practitioners will only be permitted in detached and semi-detached dwellings provided the dwelling is their principal private residence.

11.2.5.8 Home occupations will be permitted only in detached dwellings.

11.2.5.9 Second units within detached dwellings, semi-detached dwellings and townhouse dwellings will be permitted, where appropriate.

11.2.5.10 Special needs housing will be in a built form consistent with the dwelling forms permitted by the residential designation.

11.2.5.11 A horizontal multiple dwelling will not include an apartment dwelling.

11.2.5.12 In addition to the uses permitted in the High Density Residential designation, a convenience commercial facility will be permitted provided that:

- a. it forms an integral part of the ground floor of the building; and
- b. is oriented to pedestrian use.

11.2.5.13 Lands subject to minimum and maximum FSI requirements are shown on the Character Area Maps in Chapters 12, 13, 14, and 16.

## 11.2.6 Mixed Use

11.2.6.1 In addition to the Uses Permitted in all Designations, lands designated Mixed Use will also permit the following uses:

- a. commercial parking facility;
- b. conference centre;
- c. entertainment, recreation and sports facility;
- d. financial institution;
- e. funeral establishment;
- f. motor vehicle rental;
- g. overnight accommodation;
- h. personal service establishment;
- i. post-secondary educational facility;
- j. residential;
- k. restaurant;
- l. retail store; and
- m. **secondary office.**



The following uses are not permitted:

- a. self-storage facility; and
- b. detached and semi-detached dwellings.

11.2.6.2 Lands designated Mixed Use will be encouraged to contain a mixture of permitted uses.

11.2.6.3 Mixed Use development will be encouraged through infilling to consolidate the potential of these areas and to restrict their linear extension into stable, non-commercial areas.

11.2.6.4 Residential uses will be combined on the same lot or same building with another permitted use.

11.2.6.5 Residential uses will be discouraged on the ground floor.

11.2.6.6 Notwithstanding 11.2.6.4 and 11.2.6.5, development applications proposing residential uses that are not combined in the same building with another permitted use may be required to submit a development master plan to the City's satisfaction.

11.2.6.7 Notwithstanding the policies of this Plan, existing drive-through facilities will be permitted as they existed on the day these policies come into effect.

11.2.6.8 Zoning by-law amendments will be required for new drive-through facilities in Intensification Areas and may be permitted where it can be demonstrated that the drive-through facility will not interfere with the intended function and form of the Character Area. Such applications may be permitted in circumstances where the location, design and function of the drive-through facility achieves the intent of the Plan, including, but not limited to, policies regarding the following:

- a. minimum height;
- b. minimal building setbacks;
- c. continuity of the planned built form;
- d. continuity and character of the **streetscape**;
- e. no driveways or parking areas between the building and the street;

- f. active facades that address principal street frontages, and
- g. the provision for pedestrian movement into and through the site.

The application will address, among other matters, the nature of surrounding uses, the specific location of the site within the Character Area, and opportunities for the integration of the drive-through facility with other uses within the development.

11.2.6.9 Drive-through facilities may be permitted in non-intensification areas, provided that the proposed development does not interfere with the intended function and form of these Character Areas. Drive-through facilities may be permitted where the location, design and function of the drive-through facility achieves the intent of the Plan and adequately addresses to the satisfaction of the City, among other matters, the nature of surrounding areas, compatibility with surrounding uses, the specific location of the site within the Character Area and opportunities for the integration of the drive-through facility with other uses within the development.

## 11.2.7 Office

11.2.7.1 In addition to the Uses Permitted in all Designations, lands designated Office will also permit the following uses:

- a. major office;
- b. **secondary office**; and
- c. accessory uses.

11.2.7.2 Accessory uses will generally be limited to a maximum of 20% of the total Gross Floor Area.

11.2.7.3 All accessory uses should be on the same lot and clearly subordinate to the functioning of the permitted use.

## 11.2.8 Institutional

11.2.8.1 In addition to the Uses Permitted in all Designations, lands designated Institutional will also permit the following uses:

- a. hospital;
- b. post-secondary educational facility;
- c. residential dwellings associated with an institutional use; and
- d. accessory uses.

11.2.8.2 Institutional uses will be located in Intensification Areas or on a **Corridor**.

11.2.8.3 Institutional uses will be located and designed to minimize adverse effects on adjoining residential areas.

11.2.8.4 Accessory uses should generally be limited to a maximum of 20% of the total Gross Floor Area (GFA) and should be on the same lot, clearly subordinate to the functioning of the permitted use.

11.2.8.5 A **cogeneration** facility will be permitted as an accessory use.

## 11.2.9 Convenience Commercial

11.2.9.1 In addition to the Uses Permitted in all Designations, lands designated Convenience Commercial will also permit the following uses:

- a. commercial parking facility;
- b. entertainment, recreation and sports facilities;
- c. financial institution;
- d. gas bar;
- e. personal service establishment;
- f. residential;
- g. restaurant;

h. retail store; and

i. **secondary office**.

11.2.9.2 Gas bars will be permitted provided they are adjacent to a **Corridor**.

11.2.9.3 Residential uses will be combined on the same lot or same building with another permitted use.

11.2.9.4 Residential uses will not be permitted on the ground floor frontage.

11.2.9.5 Residential uses will not be permitted in combination with a gas bar.

11.2.9.6 Notwithstanding the policies of this Plan, existing drive-through facilities will be permitted as they existed on the day these policies come into effect.

11.2.9.7 Zoning by-law amendments will be required for new drive-through facilities in Intensification Areas and may be permitted where it can be demonstrated that the drive-through facility will not interfere with the intended function and form of the Character Area. Such applications may be permitted in circumstances where the location, design and function of the drive-through facility achieves the intent of the Plan, including, but not limited to, policies regarding the following:

- a. minimum height;
- b. minimal building setback;
- c. continuity of the planned built form;
- d. continuity and character of the **streetscape**;
- e. no driveways or parking areas between the building and the street;
- f. active facades that address principal street frontages, and
- g. the provision for pedestrian movement into and through the site.

The application will address, among other matters, the nature of surrounding uses, the specific location

of the site within the Character Area, and opportunities for the integration of the drive-through facility with other uses within the development.

11.2.9.8 Drive-through facilities may be permitted in non-intensification areas, provided that the proposed development does not interfere with the intended function and form of these Character Areas. Drive-through facilities may be permitted where the location, design and function of the drive-through facility achieves the intent of the Plan and adequately addresses to the satisfaction of the City, among other matters, the nature of surrounding areas, compatibility with surrounding uses, the specific location of the site within the Character Area and opportunities for the integration of the drive-through facility with other uses within the development.

### 11.2.10 Motor Vehicle Commercial

11.2.10.1 In addition to the Uses Permitted in all Designations, lands designated Motor Vehicle Commercial will also permit the following uses:

- a. gas bar;
- b. motor vehicle repair;
- c. motor vehicle service station; and
- d. motor vehicle wash.

11.2.10.2 Retail stores and take-out restaurants which may include a drive-through facility will be permitted accessory to Motor Vehicle Commercial uses.

11.2.10.3 Motor Vehicle Commercial uses will be encouraged to locate at intersections, except where such locations are important or sensitive in terms of city image, area character, **streetscape** or significant natural features.

11.2.10.4 Motor Vehicle Commercial uses will be discouraged as single uses at important intersections and should be integrated with other commercial development.

11.2.10.5 Generally more than one Motor Vehicle Commercial use at any intersection will be discouraged. However, a maximum of two Motor Vehicle Commercial uses may be permitted at any intersection. When two Motor Vehicle Commercial uses locate at the same intersection they are encouraged to locate at diagonally opposite corners, where feasible.

11.2.10.6 Motor Vehicle Commercial uses along the same street are encouraged to locate on alternate sides of the street.

## 11.2.11 Business Employment

11.2.11.1 In addition to the Uses Permitted in all Designations, lands designated Business Employment will also permit the following uses:

- a. adult entertainment establishment;
- b. animal boarding establishment which may include outdoor facilities;
- c. banquet hall;
- d. body rub establishment;
- e. broadcasting, communication and utility rights-of-way;
- f. cardlock fuel dispensing facility;
- g. commercial parking facility;
- h. commercial school;
- i. conference centre;
- j. entertainment, recreation and sports facilities;
- k. financial institution;
- l. funeral establishment;
- m. manufacturing;
- n. motor vehicle body repair facilities;
- o. motor vehicle rental;



- p. overnight accommodation;
- q. research and development;
- r. restaurant;
- s. ***secondary office***;
- t. self storage facility;
- u. transportation facilities;
- v. trucking terminals;
- w. warehousing, distributing and wholesaling;
- x. ***waste processing stations*** or ***waste transfer stations*** and composting facilities; and
- y. accessory uses.

11.2.11.2 The maximum ***floor space index (FSI)*** for ***secondary offices*** is 1.0.

11.2.11.3 Permitted uses will operate mainly within enclosed buildings.

11.2.11.4 Accessory uses will generally be limited to a maximum of 20% of the total Gross Floor Area.

11.2.11.5 All accessory uses should be on the same lot and clearly subordinate to and directly related to the functioning of the permitted use.

11.2.11.6 Notwithstanding the policies of this Plan, existing drive-through facilities will be permitted as they existed on the day these policies come into effect.

11.2.11.7 Zoning by-law amendments will be required for new drive-through facilities in Intensification Areas and may be permitted where it can be demonstrated that the drive-through facility will not interfere with the intended function and form of the Character Area. Such applications may be permitted in circumstances where the location, design and function of the drive-through facility achieves the intent of the Plan, including, but not limited to, policies regarding the following:

- a. minimum height;
- b. minimal building setback;

- c. continuity of the planned built form;
- d. continuity and character of the ***streetscape***;
- e. no driveways or parking areas between the building and the street;
- f. active facades that address principal street frontages, and
- g. the provision for pedestrian movement into and through the site.

The application will address, among other matters, the nature of surrounding uses, the specific location of the site within the Character Area, and opportunities for the integration of the drive-through facility with other uses within the development.

11.2.11.8 Drive-through facilities may be permitted in non-intensification areas, provided that the proposed development does not interfere with the intended function and form of these Character Areas. Drive-through facilities may be permitted where the location, design and function of the drive-through facility achieves the intent of the Plan and adequately addresses to the satisfaction of the City, among other matters, the nature of surrounding areas, compatibility with surrounding uses, the specific location of the site within the Character Area, and opportunities for the integration of the drive-through facility with other uses within the development.

## **11.2.12 Industrial**

11.2.12.1 In addition to the Uses Permitted in all Designations, lands designated Industrial will also permit the following uses:

- a. adult entertainment establishment;
- b. animal boarding establishment which may include an outdoor facility;
- c. banquet hall;
- d. body rub establishment;
- e. broadcasting, communication and utility rights-of-way;

- f. cardlock fuel dispensing facility;
- g. commercial school;
- h. conference centre;
- i. entertainment, recreation and sports facilities;
- j. financial institution;
- k. funeral establishment;
- l. **major power generating facility**;
- m. manufacturing;
- n. motor vehicle body repair facility;
- o. Motor Vehicle Commercial uses;
- p. motor vehicle rental facility;
- q. outdoor storage and display area;
- r. overnight accommodation;
- s. research and development;
- t. restaurant;
- u. **secondary office**;
- v. self storage facility;
- w. transportation facility;
- x. trucking terminal;
- y. warehousing, distributing and wholesaling;
- z. **waste processing stations or waste transfer stations** and composting facilities; and
- aa. accessory uses.

11.2.12.2 The maximum **floor space index (FSI)** for **secondary offices** is 0.5.

11.2.12.3 Accessory uses will generally be limited to a maximum of 20% of the total Gross Floor Area.

11.2.12.4 All accessory uses should be on the same lot and clearly subordinate to and directly related to the functioning of the permitted use.

11.2.12.5 Notwithstanding the policies of this Plan, existing drive-through facilities will be permitted as they existed on the day these policies come into effect.

11.2.12.6 Zoning by-law amendments will be required for new drive-through facilities in Intensification Areas and may be permitted where it can be demonstrated that the drive-through facility will not interfere with the intended function and form of the Character Area. Such applications may be permitted in circumstances where the location, design and function of the drive-through facility achieves the intent of the Plan, including, but not limited to, policies regarding the following:

- a. minimum height;
- b. minimal building setback;
- c. continuity of the planned built form;
- d. continuity and character of the **streetscape**;
- e. no driveways or parking areas between the building and the street;
- f. active facades that address principal street frontages, and
- g. the provision for pedestrian movement into and through the site.

The application will address, among other matters, the nature of surrounding uses, the specific location of the site within the Character Area, and opportunities for the integration of the drive-through facility with other uses within the development.

11.2.12.7 Drive-through facilities may be permitted in non-intensification areas, provided that the proposed development does not interfere with the intended function and form of these Character Areas. Drive-through facilities may be permitted where the location, design and function of the drive-through facility achieves the intent of the Plan and adequately addresses to the satisfaction of the City, among other matters, the nature of surrounding areas, compatibility with surrounding uses, the specific location of the site within the Character Area and opportunities for the integration of the

drive-through facility with other uses within the development.

### **11.2.13 Parkway Belt West**

11.2.13.1 Lands designated Parkway Belt West will be governed by the provisions of the Parkway Belt West Plan. In case of any discrepancy between the Parkway Belt West Plan and Mississauga Official Plan, the provisions of the Parkway Belt West Plan will prevail.

### **11.2.14 Airport**

11.2.14.1 Lands designated Airport permit all uses that support Airport operations, the needs of the traveling public and Airport employees.

## **11.3 Land Use Designations in Local Area Plans**

11.3.1 In addition to the general land use designations, Local Area Plans may develop land use designations specific to the area they address.

11.3.2 Policies for the following land use designations are included in Local Area Plans:

- a. Downtown Mixed Use (Downtown Core Local Area Plan);
- b. Downtown Core Commercial (Downtown Core Local Area Plan); and
- c. Land Use to be Determined (Port Credit Local Area Plan).

## **11.4 Special Study Area**

Lands west of Ninth Line will be subject to the Town of Milton and Region of Halton Official Plans in effect as of January 1, 2010, until such time as they are incorporated into this Plan.

## **11.5 To Be Determined**

The use of lands designated To Be Determined, will be addressed in future studies.



# 12 Downtown

## 12.1 Introduction

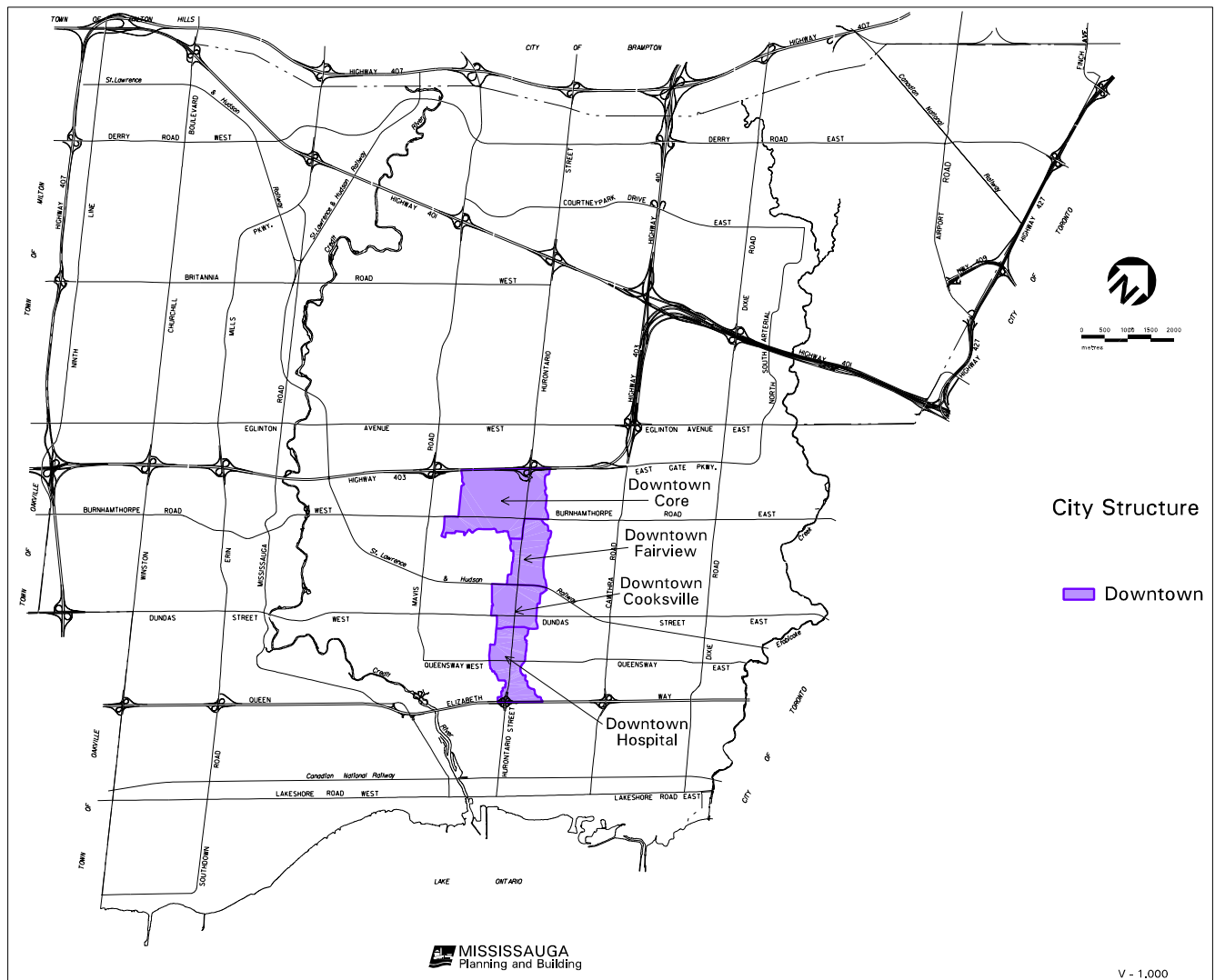
The Downtown is comprised of four Character Areas:

- Downtown Core;
- Downtown Fairview;
- Downtown Cooksville; and
- Downtown Hospital.

This section identifies the modifications to the General Land Use designations in Chapter 11 that apply to the Downtown.

### 12.1.1 General

12.1.1.1 Proponents of development applications within the Downtown may be required to demonstrate how the new development contributes to the achievement of the residents and jobs density target and the population to employment ratio.



Map 12-1: City Structure - Downtown

12.1.1.2 A minimum building height of three storeys is required on lands within the Downtown. Character Area policies may specify maximum building height requirements. Alternative minimum building heights may be established for existing areas with low density residential development.

12.1.1.3 Proposals for heights less than three storeys or different than established in the Character Area policies, will only be considered where it can be demonstrated to the City's satisfaction, that:

- a. an appropriate transition in heights that respects the surrounding context will be achieved;
- b. the development proposal enhances the existing or planned development;
- c. the City Structure hierarchy is maintained; and
- d. the development proposal is consistent with the policies of this Plan.

12.1.1.4 Lands immediately adjacent to, or within the Downtown, should provide both a transition between the higher density and height of development within the Downtown and lower density and height of development in the surrounding area.

12.1.1.5 Down-zoning of lands within the Downtown and in the transition area adjacent to the Downtown should not be permitted.

12.1.1.6 Within the Downtown, on lands designated Mixed Use, Downtown Core Commercial, Convenience Commercial or Office, ground floor retail or office uses will be provided.

## **12.1.2 Residential**

12.1.2.1 The Residential Low Density I and Residential Low Density II designations will not be permitted, except for lands designated Low Density I and Residential Low Density II at the time this Plan comes into effect.

12.1.2.2 Notwithstanding the Residential High Density policies of this Plan, the maximum building

height for lands designated Residential High Density will not exceed 25 storeys.

## **12.1.3 Mixed Use**

12.1.3.1 Notwithstanding the Mixed Use policies of this Plan, the following additional uses will be permitted:

- a. Major offices.

12.1.3.2 Mixed Use development will be pedestrian oriented and street related.

12.1.3.3 Compatible development is encouraged that recognizes the scale and enhances the form and character of Mixed Use areas.

## **12.1.4 Office**

12.1.4.1 Notwithstanding the Office policies of this Plan, the following additional uses will be permitted:

- a. Post-secondary educational facilities.

## **12.1.5 Motor Vehicle Commercial**

12.1.5.1 The Motor Vehicle Commercial designation will not be permitted, except for lands designated Motor Vehicle Commercial at the time this Plan comes into effect.

## **12.1.6 Business Employment**

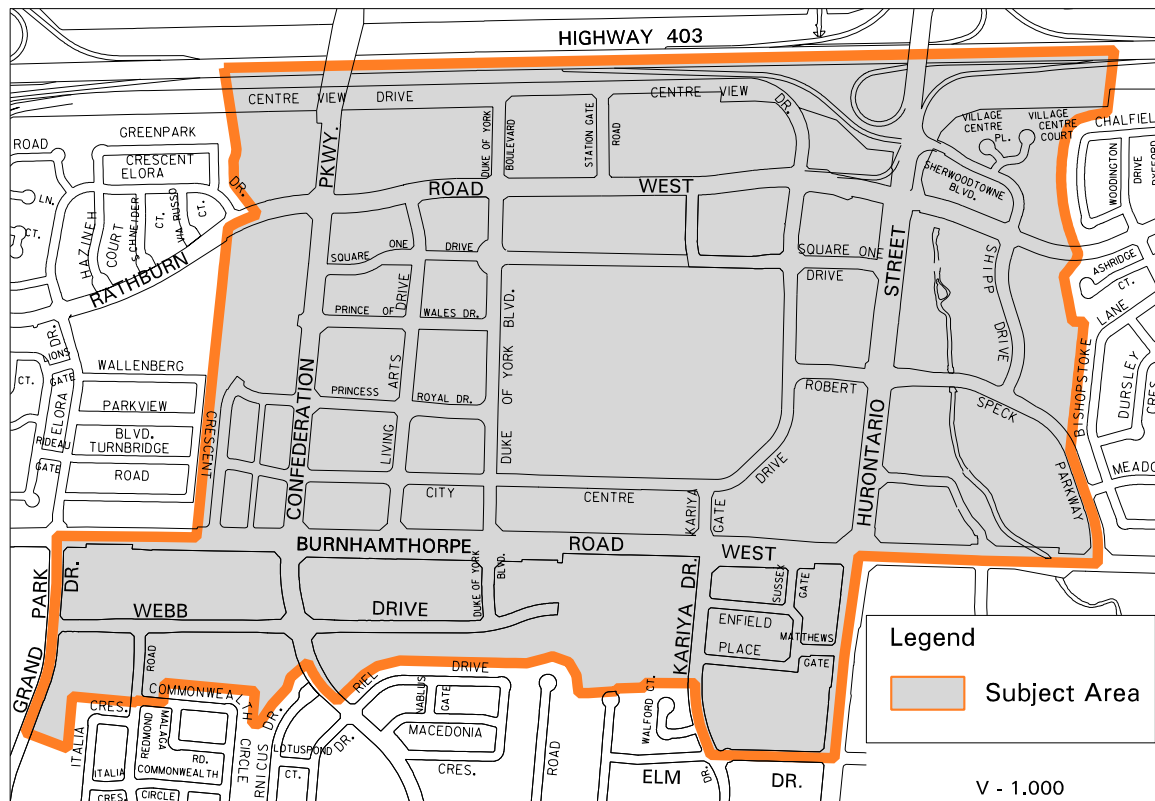
12.1.6.1 The Business Employment designation will not be permitted.

## **12.1.7 Industrial**

12.1.7.1 The Industrial designation will not be permitted.

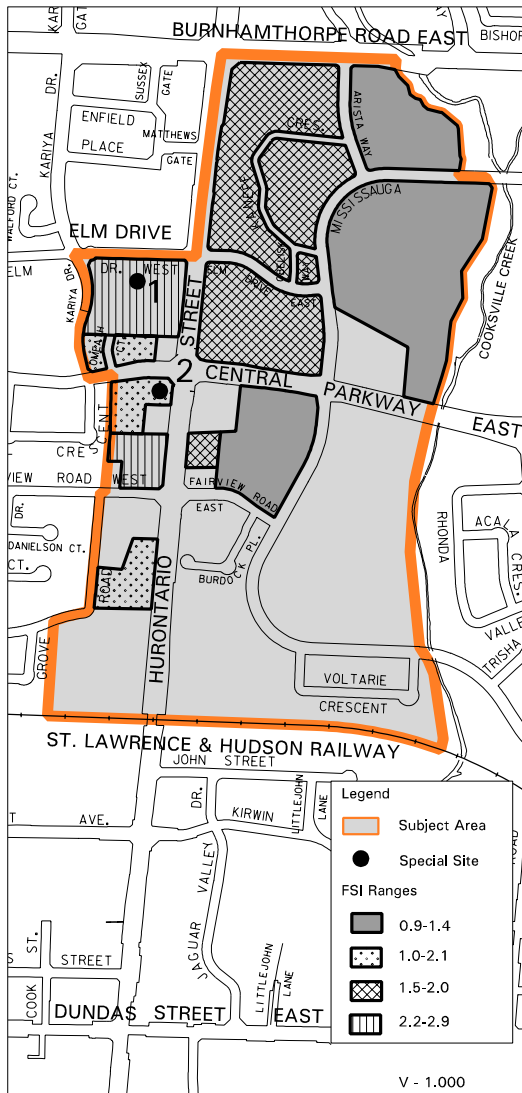
## 12.2 Downtown Core

Official Plan policies for lands within the Downtown Core are contained in the Downtown Core Local Area Plan.



Map 12-2: Downtown Core Character Area

## 12.3 Downtown Fairview



Map 12-3: Downtown Fairview Character Area

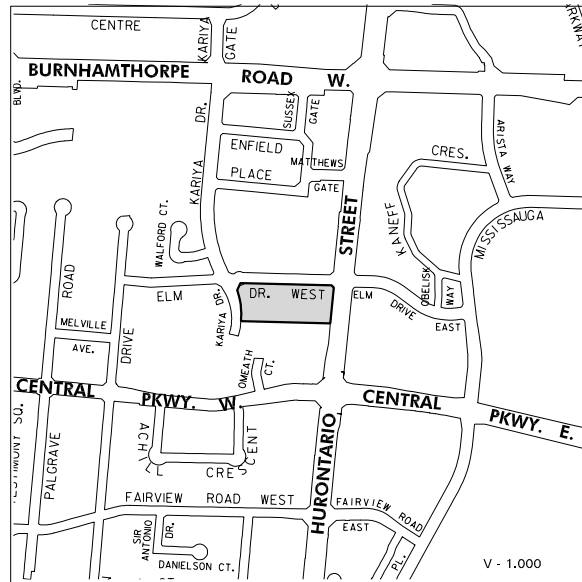
### 12.3.1 Land Use

12.3.1.1 For lands designated Residential Medium density, building height should not exceed three storeys.

### 12.3.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 12.3.2.1 Site 1



12.3.2.1.1 The lands identified as Special Site 1 are located on the west side of Hurontario Street, south of Elm Drive.

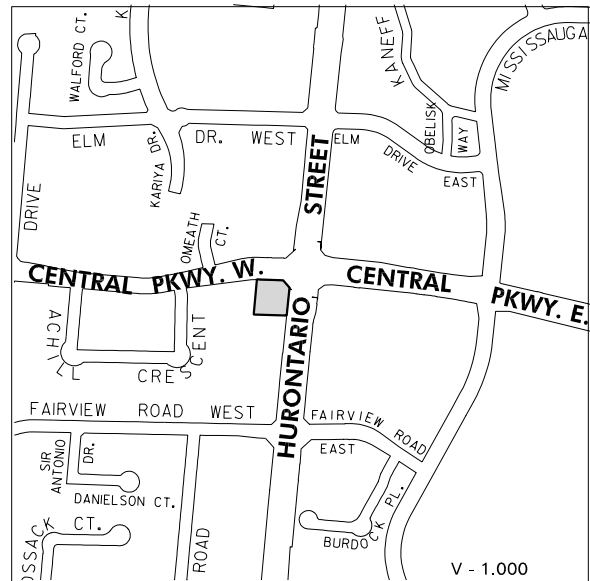
12.3.2.1.2 Notwithstanding the provisions of the Residential High Density designation and applicable policies, the following additional policies will apply:

- a concept plan for all or part of this site will be required and will address, among other matters, the following:
  - compatibility of building form and scale with existing and proposed surrounding land uses;
  - convenient pedestrian access through this site to nearby transit services on Hurontario Street;
  - traffic generated will not adversely affect the transportation system;



- acceptable ingress and egress, off-street parking, landscaping, and buffering; and
  - preservation of mature trees and other significant natural features; and
- b. Mississauga will encourage the assembly of lots fronting along Elm Drive and comprehensive redevelopment of lands in Site 1;
  - c. the redevelopment of lands will minimize access points to Hurontario Street to preserve the integrity of Hurontario Street as an arterial roadway. Alternative access to Elm Drive or the proposed Kariya Drive extension should be investigated as part of the comprehensive redevelopment of Site 1; and
  - d. apartments will be permitted at a maximum **floor space index (FSI)** of 2.2 - 2.9.

### 12.3.2.2 Site 2



12.3.2.2.1 The lands identified as Special Site 2 are located on the west side of Hurontario Street, south of Central Parkway West.

12.3.2.2.2 Notwithstanding the provisions of the Residential High Density designation and applicable policies, the following additional policies will apply:

- a. the total number of units constructed on the site will not exceed 135; and
- b. a maximum **floor space index (FSI)** of 3.75 will be permitted.

## 12.4 Downtown Cooksville

### Character of Dundas Street and Hurontario Street



Map 12-4: Downtown Cooksville Character Area

### 12.4.1 Urban Design Policies

#### Overall Character

12.4.1.1 A high level of urban design, pedestrian amenity, and intensity of development is encouraged along principal street frontages. A sense of entry to the Character Area should be articulated at these locations by prominent built form, landscaping and signage components.

12.4.1.2 The sections of Hurontario Street and Dundas Street within the Character Area should function as a focus for the Cooksville Neighbourhood, having a strong sense of place and main street character with active mixed use building frontages and highly pedestrianized nature. These street frontages should reinforce a distinctive, quality image with high standards in built form, landscaping and related pedestrian amenities.

12.4.1.3 Street Edge Uses - Development abutting the street should encourage a high level of activity along the street by incorporating grade related retail with residential and/or offices above. Retail units should be clearly oriented to, and accessed from, the public sidewalk.

12.4.1.4 Street Scale and Enclosure - Development should be closely related to, and integrated with, the public sidewalk to focus activity on the street and provide a sense of spatial enclosure for the street. Development should address the following:

- limited building setback range of three to five metres from the street line, with the larger setback in areas of high transit or pedestrian use;
- minimum building height of two to four storeys and maximum of six storeys directly abutting the street line;
- maximum continuity of street walls with built form occupying a minimum of 80% of the street frontage; and
- a minimum setback of ten metres from the street line is required for buildings exceeding six storeys in height.

12.4.1.5 Bulk and Massing - Development should be generally consistent in its bulk, massing and scale within the Character Area with use of taller more prominent buildings to highlight the Dundas and Hurontario Street intersection. A general height progression should be encouraged from this intersection to the Character Area edges and abutting the Downtown Hospital Character Area. A ratio of 1:2 (building height to eventual street width) should be provided by built form abutting Dundas and Hurontario Streets.

12.4.1.6 Development Fabric - Development fronting on the Hurontario Street and Dundas Street frontages should reinforce patterns and characteristics supportive of a main street role with highly animated pedestrian spaces. Development should address the following:

- a. commercial frontages should be broken up into smaller retail units and accessed directly from the public sidewalk with frequent access doorways;
- b. no parking should be provided between the buildings and street line;
- c. blank walls should be avoided along the street in favour of fenestration;
- d. service, loading and garbage storage areas should be accessed from rear lanes or abutting side streets;
- e. split level commercial frontages should be avoided;
- f. periodic building indentations should be provided as relief to long building walls and to provide opportunities for pedestrian spaces;
- g. storefront signage should respect the pedestrian scale and architectural character of development;
- h. pedestrian weather protection should be provided in the form of canopies and/or inset arcades; and
- i. the development and integration of rear yard parking lots to reduce the number of driveways along Hurontario Street and Dundas Street.

12.4.1.7 **Streetscape** (Open Space and Landscaping) - Development of private land abutting the sidewalk should closely coordinate with the public boulevard to create an integrated design character at the pedestrian level and to reinforce the sense of a community main street. Opportunities

for the development of entry forecourts and plazas will be encouraged in order to create a varied **streetscape**. Any existing vegetation will be considered for preservation through the redevelopment/infill process to enhance the liveability of a space.

12.4.1.8 Private Development Zone – Development should address the following:

- a. front yard setback areas should be provided with site furnishings, landscaping and paving materials, coordinate with the public boulevard design, and extend the usability of the public sidewalk; and
- b. opportunities for pedestrian spaces and circulation areas should be promoted at intersection corners, major building entrances and terminations of vistas.

12.4.1.9 Public Realm: The Design of the Public Boulevard - The following features should be encouraged to reduce the perceived visual width of the street and improve the level of pedestrian comfort, safety and convenience within the public boulevard:

- a. common paving materials and patterns, street furniture and signage;
- b. road crossings defined by special paving;
- c. intersection design to moderate speed of turning traffic in favour of pedestrian movements, i.e. avoid the construction of designated right turn lanes with islands;
- d. setting back of bus bays from intersection corners to allow sidewalks to project;
- e. curb edge parking; and
- f. provision of street trees, feature lighting and related pedestrian amenities.

## 12.4.2 Transportation

12.4.2.1 All development applications are required to address the relationship with adjacent properties and the need to minimize access onto Hurontario Street and Dundas Street.

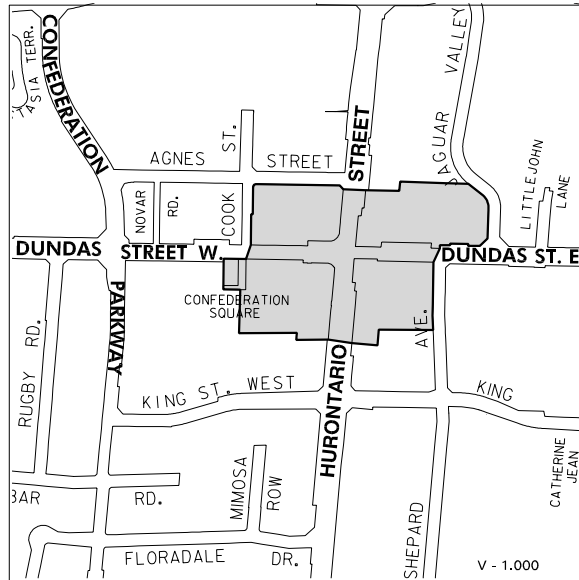
12.4.2.2 A future signalized intersection with Hurontario Street is proposed in the vicinity of the former Melissa Street right-of-way along the southerly boundary of T.L. Kennedy Secondary School. This will connect with Cook Street initially and, with the potential redevelopment of the school site, may extend to Confederation Parkway in conjunction with the possible extension of Cook Street northerly to the east GO Access Road. Redevelopment proposals in this area will consider these options.

12.4.2.3 The City of Mississauga Parking Strategy, carried out in 1993, made recommendations on the short, medium and long term strategies to meet the parking requirements and to achieve urban design objectives. These recommendations will be considered in conjunction with development applications and implementation of High Occupancy Vehicle (HOV) lanes on Hurontario Street and Dundas Street East.

## 12.4.3 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 12.4.3.1 Site 1



12.4.3.1.1 The lands identified as Special Site 1, known as Cooksville Corners, are the lands surrounding the intersection of Hurontario Street and Dundas Street.

12.4.3.1.2 Notwithstanding the provisions of the Mixed Use and Utility designations, the Urban Design Policies, and other provisions of this Plan, the following additional policies will apply:

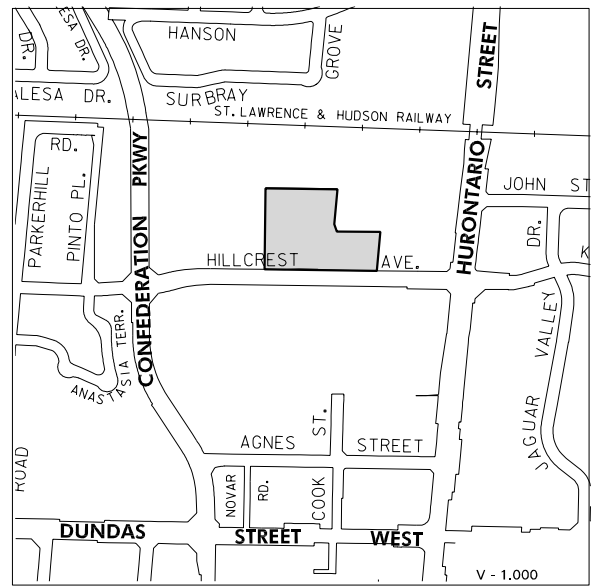
- a. development up to a **floor space index (FSI)** of 1.0 will be permitted for street related retail, office, and community uses. Development between an **floor space index (FSI)** of 1.0 and 2.9 will be permitted at a ratio of 80% residential, office, community uses and overnight accommodation uses and 20% retail uses;
- b. principal building entries and related forecourts are to be provided at the Dundas and Hurontario streets intersection. Dundas and Hurontario street frontages are to be developed with retail



uses having direct access to the public street and continuous weather protection by an arcade and/or projecting awnings;

- c. internal pedestrian courtyards with retail/restaurant uses will be permitted subject to: the provision of integral walkways to the public street; visibility from the sidewalk; and, such courtyards being clearly secondary to street related retail uses;
- d. minimum building heights of three storeys and a maximum of six storeys directly abutting Dundas and Hurontario street frontages are encouraged to create an appropriate street scale and enclosure relationship. Development exceeding six storeys to a maximum of 18 storeys will be considered subject to a minimum setback of ten metres from the building edge abutting the public sidewalk. Terracing of taller built form will be encouraged;
- e. the lands located at the southeast corner of Agnes Street and Cook Street can be utilized on an interim basis for parking for the Mixed Use development to the west. When these lands are developed, they are to be in compliance with the Residential High Density designation. The majority of parking for the mixed retail and residential development shall be accommodated in a below-grade parking structure.

#### 12.4.3.2 Site 2



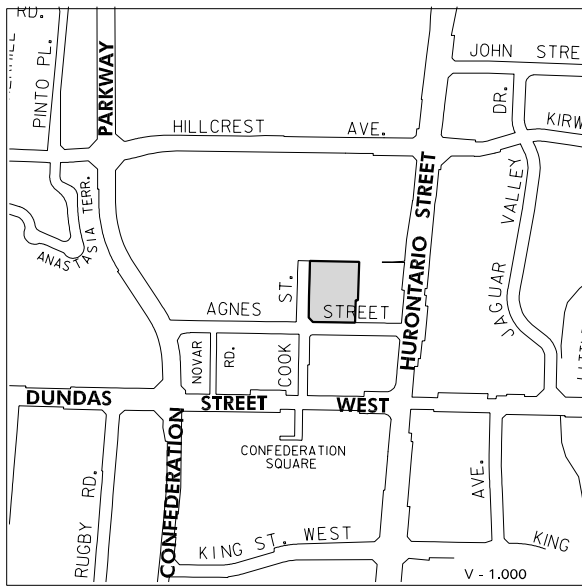
12.4.3.2.1 The lands identified as Special Site 2 are located on the north side of Hillcrest Avenue, west of Hurontario Street.

12.4.3.2.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:

- a. office development will be permitted at a maximum Gross Floor Area (GFA) of 61 439 m<sup>2</sup>, of which a maximum of 9 290 m<sup>2</sup> will be used for accessory uses; and
- b. apartments will be permitted at a maximum **floor space index (FSI)** of 1.5 - 2.9.

### 12.4.3.3

### Site 3



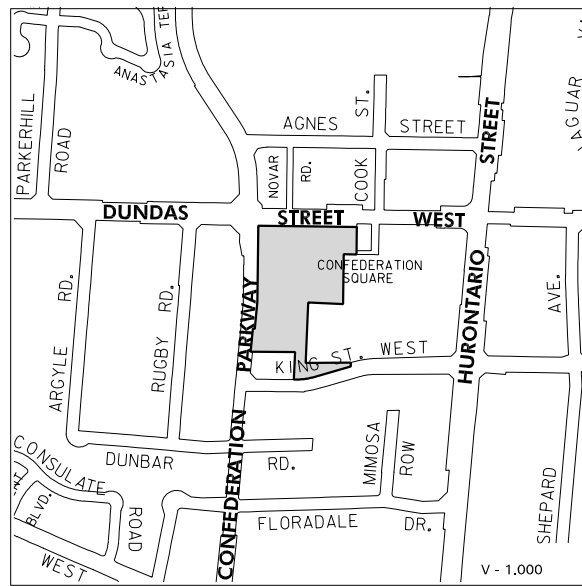
12.4.3.3.1 The lands identified as Special Site 3 are located at the northeast corner of Agnes Street and Cook Street.

12.4.3.3.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:

- a maximum of 121 apartment units and a maximum of 27 condominium townhouse units will be permitted on this site; and
- the apartment building will not exceed 13 storeys in height.

### 12.4.3.4

### Site 4

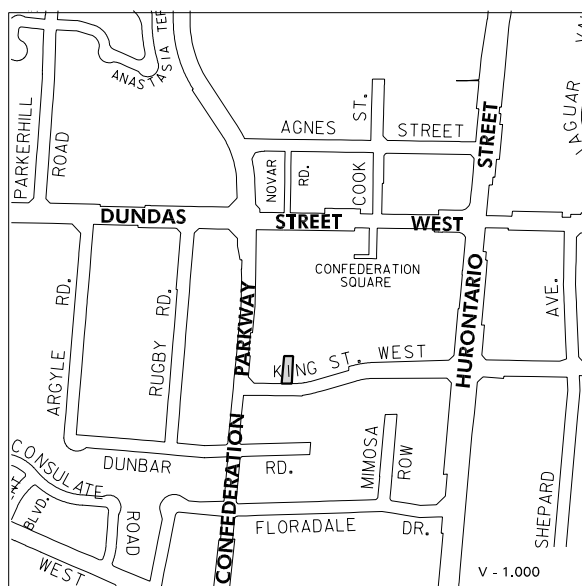


12.4.3.4.1 The lands identified as Special Site 4 are located in the southeast quadrant of the intersection of Dundas Street West and Confederation Parkway.

12.4.3.4.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a maximum of 688 apartment units; 2 541 m<sup>2</sup> of office and retail uses; a 1 152 m<sup>2</sup> community centre; and an eight storey, approximately 9 580 m<sup>2</sup> existing office building will be permitted;
- in the event that the community centre is not required, the space designated for a community centre, 1 152 m<sup>2</sup>, may be converted to office and retail uses.

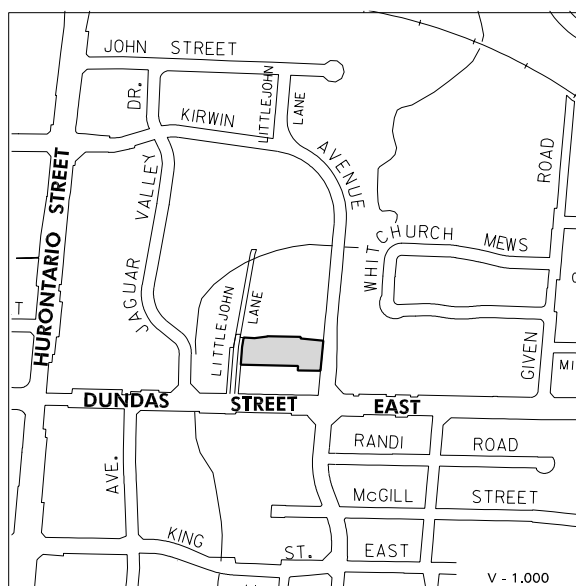
### 12.4.3.5 Site 5



12.4.3.5.1 The lands identified as Special Site 5 are located on the north side of King Street West, east of Confederation Parkway.

12.4.3.5.2 Notwithstanding the provisions of the Residential Low Density II designation, offices will be permitted.

### 12.4.3.6 Site 6



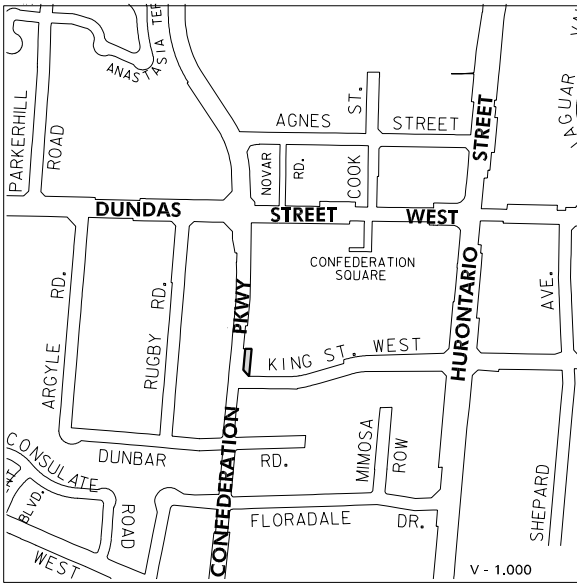
12.4.3.6.1 The lands identified as Special Site 6 are located on the west side of Kirwin Avenue, north of Dundas Street East.

12.4.3.6.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. a hotel not exceeding 42 storeys will be permitted.

### 12.4.3.7

### Site 7

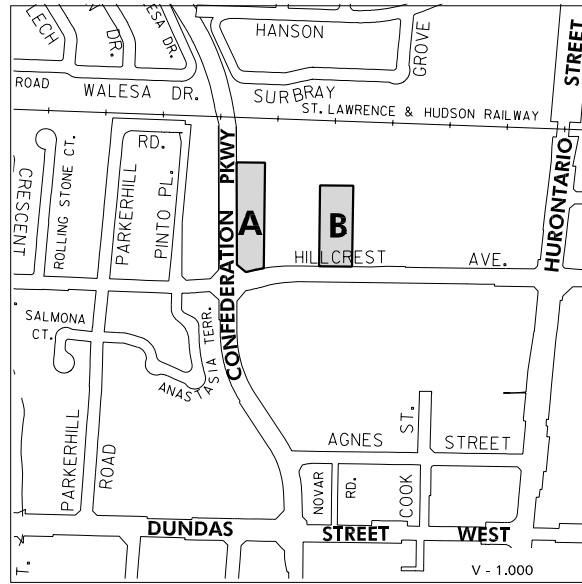


12.4.3.7.1 The lands identified as Special Site 7 are located on the northeast corner of King Street West and Confederation Parkway.

12.4.3.7.2 Notwithstanding the provisions of the Residential Low Density II designation, offices will be permitted.

### 12.4.3.8

### Site 8



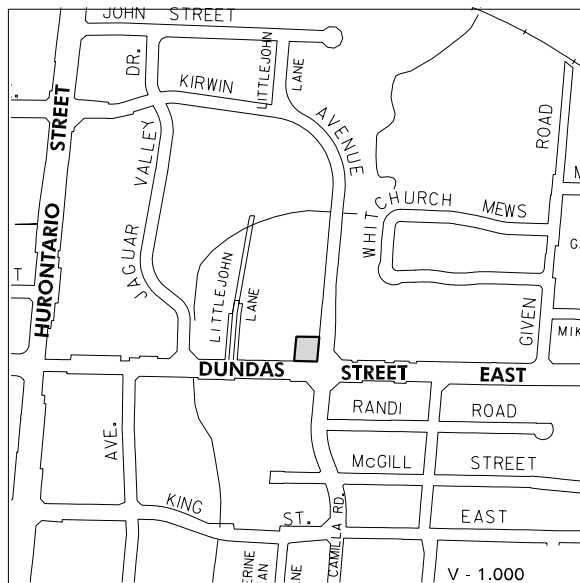
12.4.3.8.1 The lands identified as Special Site 8 are located on the north side of Hillcrest Avenue, east of Confederation Parkway.

12.4.3.8.2 Notwithstanding the provisions of the Residential High Density designation, the lands identified as Area 8A will be permitted to develop to a maximum **floor space index (FSI)** of 3.5 and the lands identified as Area 8B will be permitted to develop to a maximum **floor space index (FSI)** of 4.0.



## 12.4.4 Exempt Site Policies

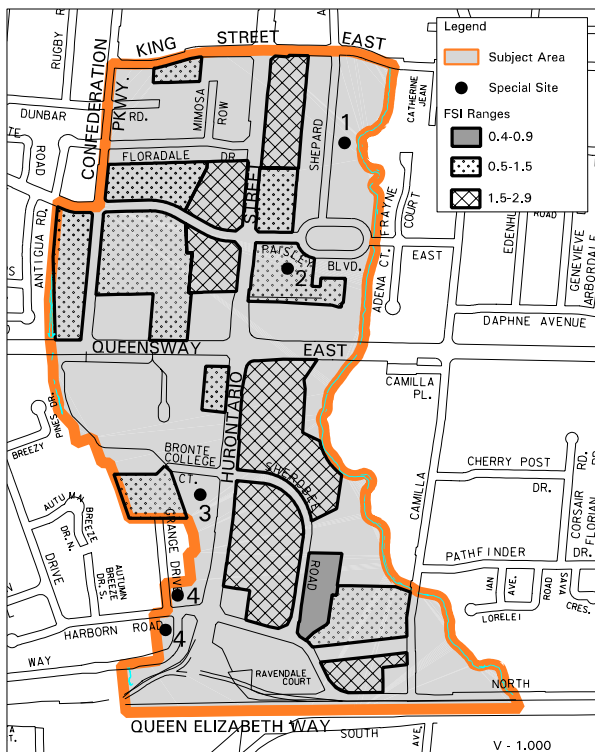
### 12.4.4.1 Site 1



12.4.4.1.1 The lands identified as Exempt Site 1 are located at the northwest corner of Dundas Street East and Kirwin Avenue.

12.4.4.1.2 Notwithstanding the provisions of the Mixed Use designation, motor vehicle commercial uses will also be permitted.

## 12.5 Downtown Hospital

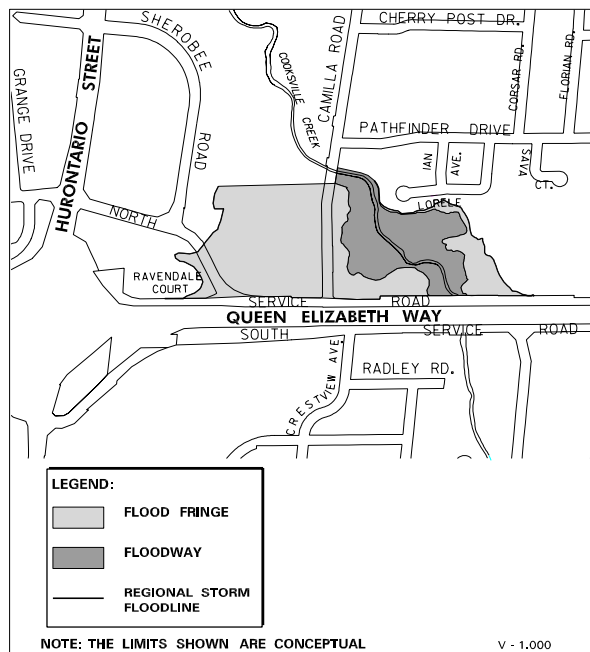


Map 12-5: Downtown Hospital Character Area

### 12.5.1 Land Use

12.5.1.1 For lands designated Residential Medium Density, building height should not exceed a maximum of four storeys unless otherwise specified in the Urban Design policies of this Plan.

## 12.5.2 Environmental Planning Area



Map 12-5.1: Cooksville Creek Floodplain Management Concept

12.5.2.1 The lands identified above are located generally between the North Service Road and where the Cooksville Creek crosses Camilla Road. The area subject to these policies within the Downtown Hospital Character Area, is generally located west of Cooksville Creek. The lands shown are subject to the **two-zone floodplain management concept**, which divides the regulatory floodplain into two portions known as the floodway and the flood fringe. The limits of the flood fringe and the floodway are conceptual, the exact limits of which will be determined through further study.

12.5.2.2 Notwithstanding the Natural Hazards policies of this Plan, the following policies will apply to those lands within the regulatory floodplain:

- the lands within the floodway are designated Residential High Density, Office, Greenbelt and Utility. Notwithstanding the Residential High Density, Office, Greenbelt and Utility policies of this Plan, the following uses will be permitted

within the floodway subject to the satisfaction of the City and Credit Valley Conservation:

- flood and/or erosion works;
  - facilities which by their nature must locate near water or traverse **watercourse** (i.e. bridges, storm sewer outlets and stormwater management facilities); and
  - passive recreation activities; and
- b. floodway lands will be zoned in an appropriate hazard category in the implementing Zoning By-law;
- c. the lands within the flood fringe are subject to their respective land use designations and the following additional policies:
- development may be permitted provided the use, building or structure is flood proofed to the regulatory flood level as required by Credit Valley Conservation;
  - ingress/egress for all development located in the flood fringe will be such that emergency vehicular and pedestrian movement is not prevented during times of flooding in order that safe access/evacuation is ensured. The determination of safe access shall be made by Credit Valley Conservation based on the depth and velocity factors;
  - enclosed underground parking will be subject to the installation of stringent flood proofing measures to the elevation of the regulatory flood level; and
  - the zoning of lands may utilize a holding zone to provide direction as to future permitted uses while ensuring flood proofing and safe access are addressed prior to development to the satisfaction of the City and Credit Valley Conservation. The Zoning By-law will be amended to remove the holding symbol when the requirements for flood proofing, the provision of safe access to the proposed development and a

detailed spill assessment and a financing agreement for the reconstruction of the culvert at the QEW has been completed to the satisfaction of the City, Credit Valley Conservation and the Ministry of Transportation; and

- d. the following uses will not be allowed within the floodplain:
- institutional services such as hospitals, nursing homes, and schools where there would be a significant threat to the safety of inhabitants involved in an emergency evacuation situation as a result of flooding or failure of flood proofing measures;
  - new uses associated with the manufacture, storage, disposal and/or consumption of hazardous substances or the treatment, collection and disposal of sewage, which would pose an unacceptable threat to public safety if they were to escape their normal containment/use as a result of flooding or failure of flood proofing measures; and
  - emergency services such as those provided by fire, police, and ambulance stations and electrical substations, which would be impaired during a flood emergency as a result of flooding or failure of flood proofing measures.

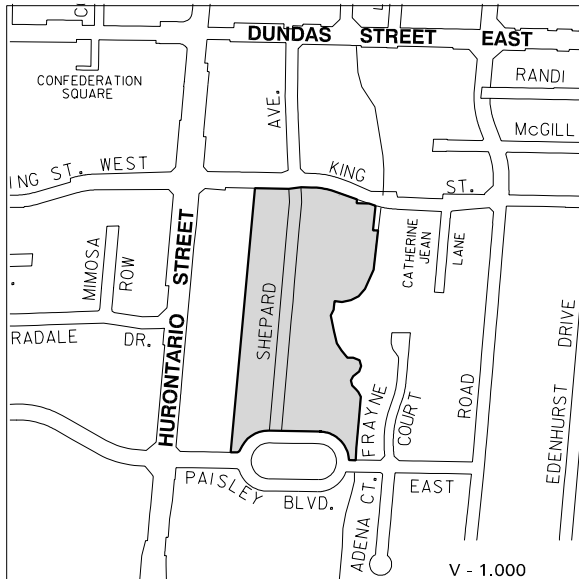
### 12.5.3 Transportation

12.5.3.1 The City of Mississauga Parking Strategy, carried out in 1993, made recommendations on the short, medium and long term strategies to meet the parking requirements and to achieve urban design objectives. These recommendations will be considered in conjunction with development applications and implementation of High Occupancy Vehicle (HOV) lanes on Hurontario Street and Dundas Street East.

## 12.5.4 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 12.5.4.1 Site 1



12.5.4.1.1 The lands identified as Special Site 1 are located on Shepard Avenue between King Street East and Paisley Boulevard East.

12.5.4.1.2 Notwithstanding the provisions of the Residential Low Density I, Residential Medium Density and Greenbelt designations, the following additional policies will apply:

- a. any proposal to alter the natural alignment of the Cooksville Creek and associated Greenbelt lands through filling or channelization will require the appropriate approvals from Credit Valley Conservation and the City of Mississauga. The final design of such alteration works will address the following:
  - criteria for erosion measures; and

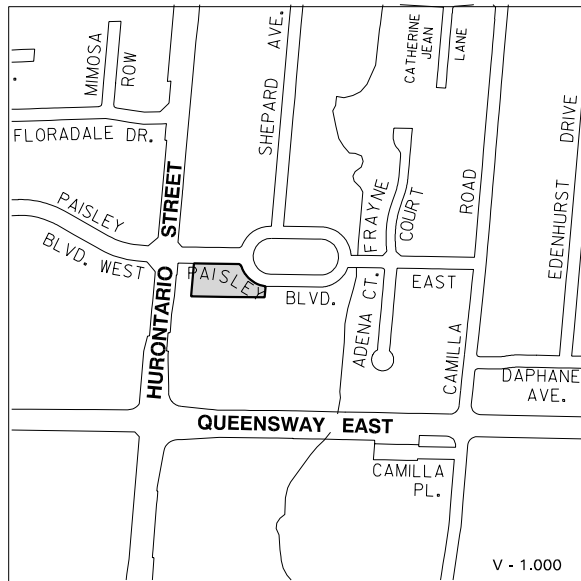
- incorporation of the recommendations of the Cooksville Creek Rehabilitation Study for the reach of the Cooksville Creek between King Street East and Paisley Boulevard East; and
- b. in the event that structural management options are not feasible and, subject to a detailed planning design and supporting engineering studies for the Special Site 1 lands, a **two-zone floodplain management concept**, or combination of a **two-zone floodplain management concept** and structural management options, may be implemented;
- c. the limits of the development will be determined through detailed studies to address limits of flooding. Lands not suitable for development will be redesignated to Greenbelt; and
- d. the preparation of a concept plan promoting the integrated redevelopment of the existing residential lots will be required in conjunction with any redevelopment applications. The concept plan will be required to address the following:
  - naturalization of the Cooksville Creek and retention of existing vegetation;
  - an alternate street access and design to limit access to the lands from only King Street East; and
  - housing forms that provide for the stepping of densities and building heights from higher at the north and west ends of the site to lower at the south and east ends of the site.

***The policies in this section and the land use designation on Schedule 10 – Land Use Designations with respect to Site 1 are under appeal under OMB Case No. PL980724 (OPA 3).***



#### 12.5.4.2

#### Site 2

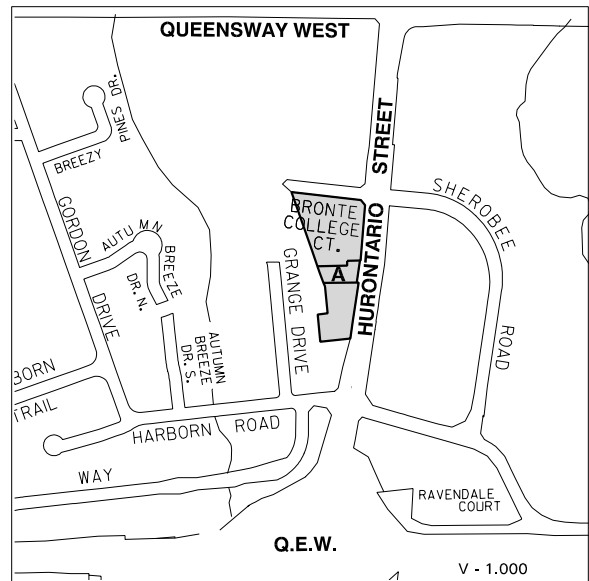


12.5.4.2.1 The lands identified as Special Site 2 are located on the south side of Paisley Boulevard, east of Hurontario Street.

12.5.4.2.2 Notwithstanding the provisions of the Residential High Density designation, apartments will be permitted at a maximum **floor space index (FSI)** of 0.5 – 1.5.

#### 12.5.4.3

#### Site 3



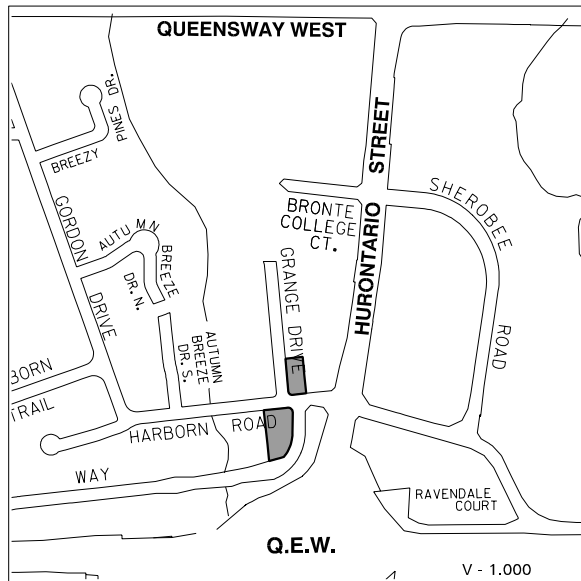
12.5.4.3.1 The lands identified as Special Site 3 are located along the west side of Hurontario Street, north of Harborn Road.

12.5.4.3.2 Notwithstanding the provisions of the Office designation, the following additional policies will apply:

- funeral homes and nursing homes will also be permitted;
- vehicular access and internal circulation and parking must respect both the visual and functional quality of the street as well as the privacy of the residential areas to the west;
- efforts must be made to retain the existing vegetation cover maintaining the continuity of the area character;
- building height and form should provide a positive scale transition between Hurontario Street and existing residential development to the west;
- the building setback from the rear lot line should provide for the retention of natural vegetation to function as a buffer from residential uses to the west; and

- f. the lands identified as Area A, located at 2150 Hurontario Street, will be permitted to develop at a maximum **floor space index (FSI)** of 1.0.

#### 12.5.4.4 Site 4



12.5.4.4.1 The lands identified as Special Site 4 are located west of Hurontario Street, south of Queensway West.

12.5.4.4.2 Notwithstanding the provisions of the Residential Low Density II designation on these lands, the following additional policies will apply:

- a. preserve and enhance the generous front, rear and side yard setbacks;
- b. ensure that existing grades and drainage conditions are preserved;
- c. encourage new housing to fit the scale and character of the surrounding areas, and take advantage of the features of a particular site, i.e. topography, contours, mature vegetation;
- d. garages should be recessed or located behind the main face of the house. Alternatively, garages should be located in the rear of the property;

- e. ensure that new development has minimal impact on its adjacent neighbours with respect to overshadowing and overlook;
- f. encourage buildings to be one to two storeys in height. The design of the building should de-emphasize the height of the house and be designed as a composition of small architectural elements, i.e. projecting dormers and bay windows;
- g. reduce the hard surface areas in the front yard;
- h. preserve existing mature high quality trees to maintain the existing mature nature of these areas;
- i. house designs which fit with the scale and character of the local area, and take advantage of the particular site are encouraged. The use of standard, repeat designs is strongly discouraged;
- j. the building mass, side yards and rear yards should respect and relate to those of adjacent lots; and
- k. a maximum height of three storeys will be permitted for street townhouses.

# 13 Major Nodes

## 13.1 Introduction

There are two Major Nodes in Mississauga:

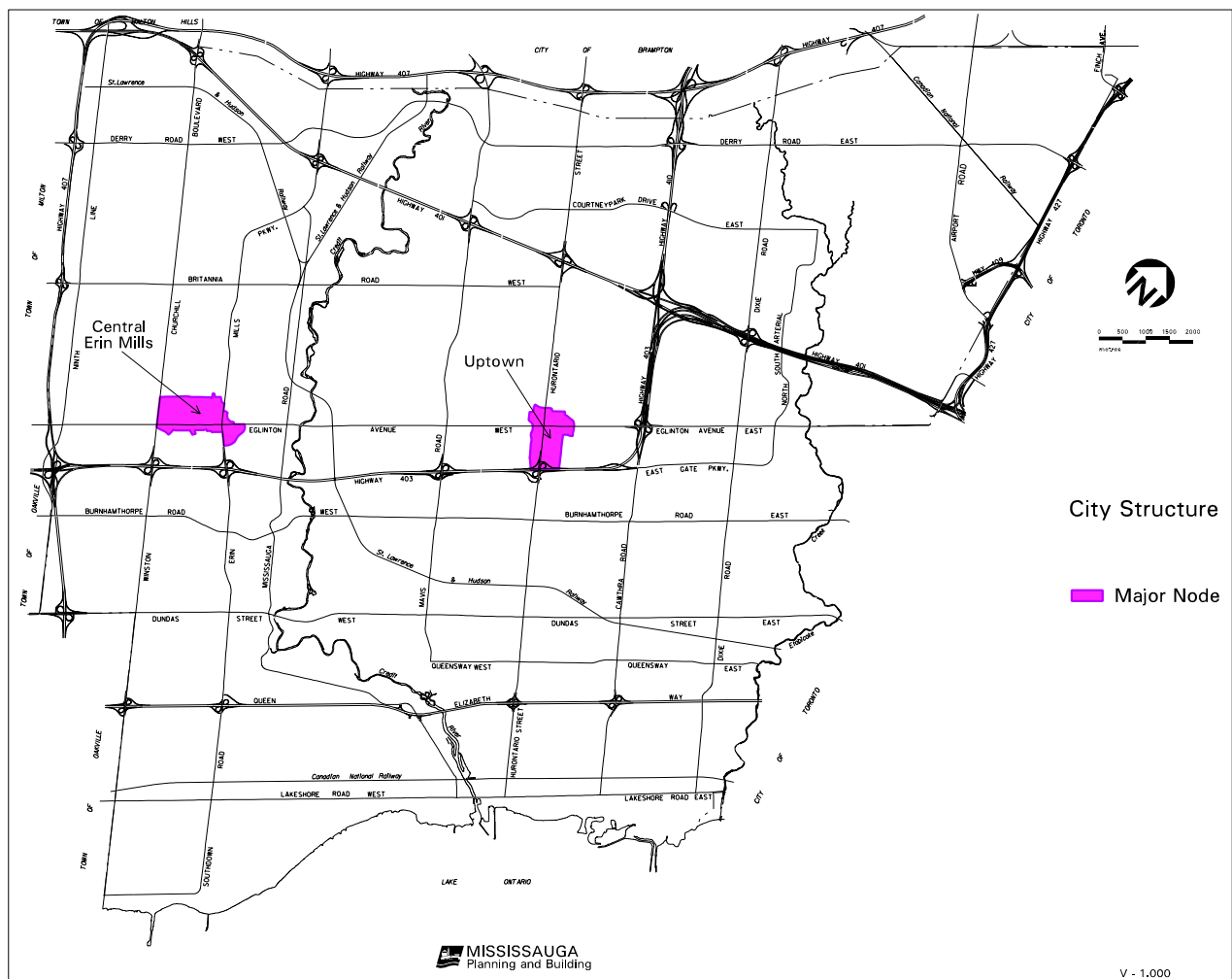
- Central Erin Mills; and
- Uptown.

This section identifies the modifications to the General Land Use designations in Chapter 11 that apply to all Major Nodes.

## 13.1.1 General

13.1.1.1 Proponents of development applications within a Major Node may be required to demonstrate how the new development contributes to the achievement of the residents and jobs density target and the population to employment ratio.

13.1.1.2 For lands within a Major Node, a minimum building height of two storeys to a maximum building height of 25 storeys will apply, unless Character Area policies specify alternative building height requirements or until such time as alternative building heights are determined through the review of Character Area policies.



Map 13-1: City Structure – Major Nodes

13.1.1.3 Proposals for heights less than two storeys, more than 25 storeys or different than established in the Character Area policies will only be considered where it can be demonstrated to the City's satisfaction, that:

- a. an appropriate transition in heights that respects the surrounding context will be achieved;
- b. the development proposal enhances the existing or planned development;
- c. the City Structure hierarchy is maintained; and
- d. the development proposal is consistent with the policies of this Plan.

### **13.1.2 Residential**

13.1.2.1 The Residential Low Density I and Residential Low Density II designations will not be permitted, except for lands designated Low Density I and Residential Low Density II at the time this Plan comes into effect.

13.1.2.2 Notwithstanding the Residential Medium Density policies of this Plan, the following additional uses will be permitted:

- a. low-rise apartment dwellings.

### **13.1.3 Mixed Use**

13.1.3.1 Notwithstanding the Mixed Use policies of this Plan, the following additional uses will be permitted:

- a. Major office.

### **13.1.4 Office**

13.1.4.1 Notwithstanding the Office policies of this Plan, the following additional uses will be permitted:

- a. Post-secondary educational facilities.

### **13.1.5 Motor Vehicle Commercial**

13.1.5.1 The Motor Vehicle Commercial designation will not be permitted, except for lands designated Motor Vehicle Commercial at the time this Plan comes into effect.

### **13.1.6 Business Employment**

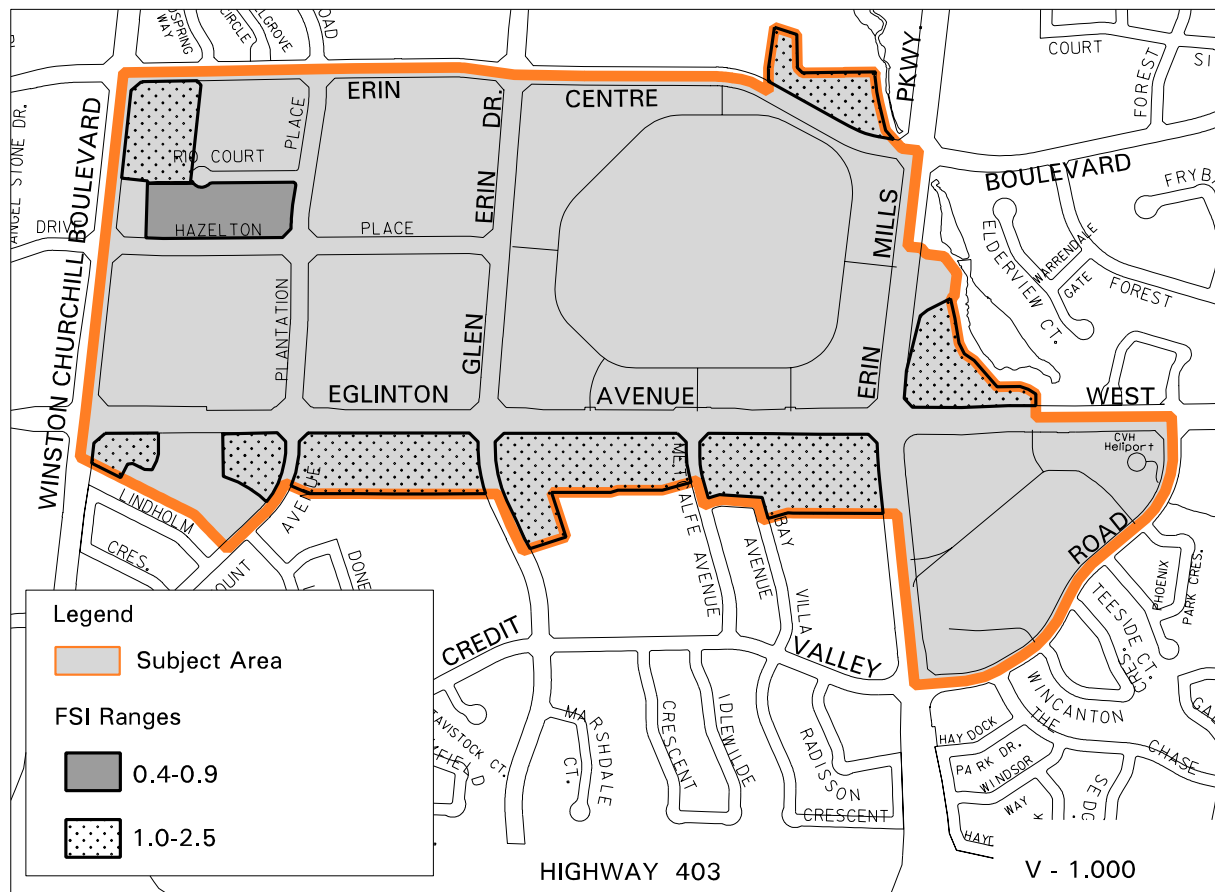
13.1.6.1 The Business Employment designation will not be permitted.

### **13.1.7 Industrial**

13.1.7.1 The Industrial designation will not be permitted.

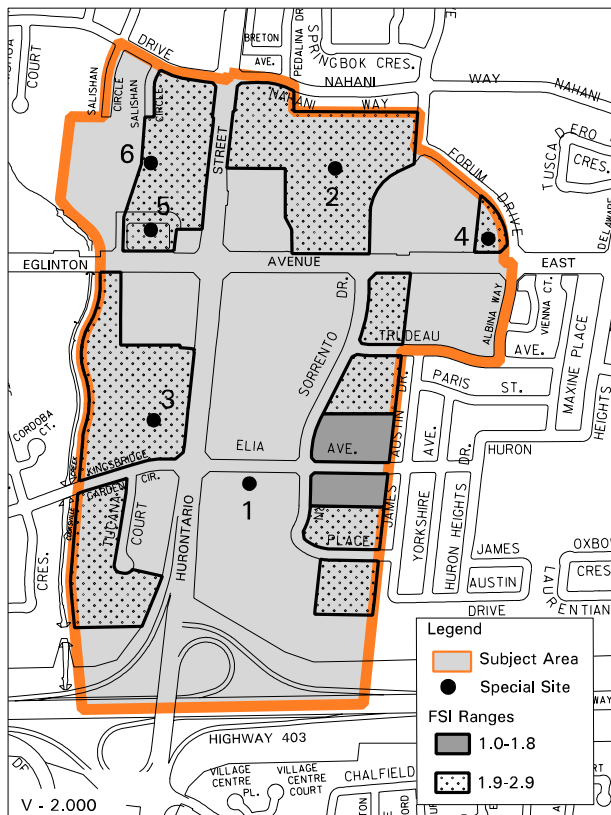


## 13.2 Central Erin Mills



Map 13-2: Central Erin Mills Major Node Character Area

## 13.3 Uptown



Map 13-3: Uptown Major Node Character Area

### 13.3.1 Urban Design Policies

13.3.1.1 To enhance a sense of community, it is proposed that a number of major **streetscapes** be developed in a manner that will impart a sense of character and identify major geographic areas of the Character Area.

13.3.1.2 Community Form and Structure Uses along Hurontario Street should be integrated with the overall community design by providing for:

- a graduated transition in development intensity and building scale; and
- orientation of buildings, related open spaces and service functions to minimize visual and functional conflicts on abutting lands.

### 13.3.2 Land Use

13.3.2.1 For lands designated Residential Medium Density, building heights will not exceed three storeys.

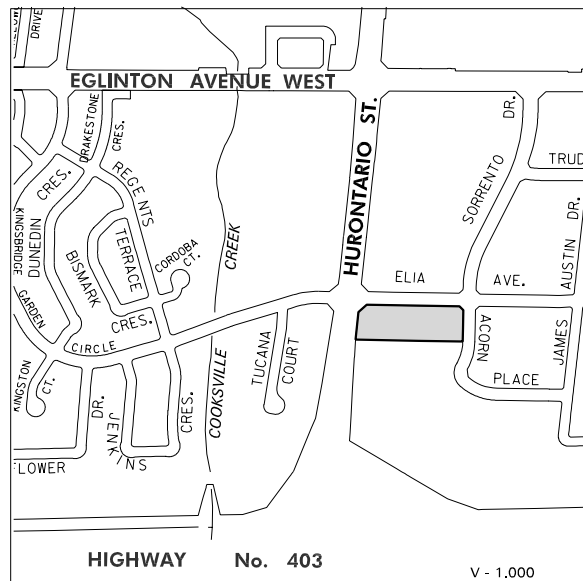
### 13.3.3 Transportation

13.3.3.1 South of the Character Area between the utility corridor and Highway 403, a new east-west roadway is proposed to provide access to the Downtown area. The proposed Highway 403 North Collector would be connected to Highway 403 west of Hurontario Street and east of Mavis Road and would be developed within the Parkway Belt West corridor as a one-way westbound two lane roadway with grade separations at Hurontario Street, Duke of York Boulevard, Confederation Parkway, the Highway 403/Mavis Road westbound off-ramp, and Mavis Road. In addition, ramp connections would be provided at Duke of York Boulevard and Confederation Parkway to provide access to and from the Downtown road network.

### 13.3.4 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 13.3.4.1 Site 1



13.3.4.1.1 The lands identified as Special Site 1 are located east of Hurontario Street and south of Elia Avenue.

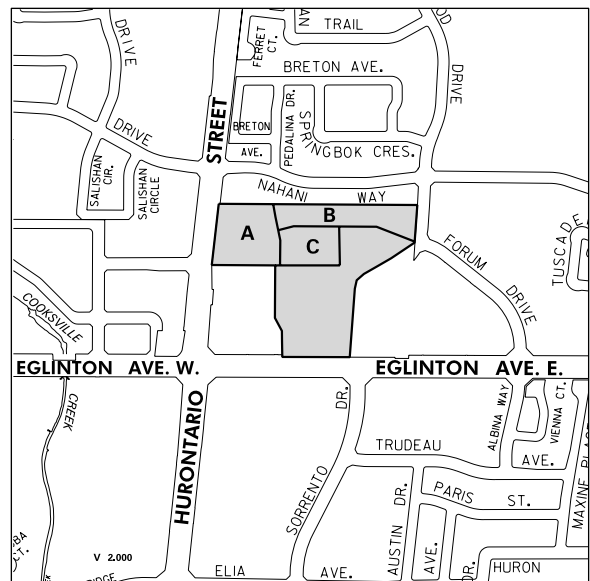
13.3.4.1.2 Notwithstanding the provisions of the Residential High Density and Office designations, the following additional policies will apply:

- Residential High Density development in combination with office uses will also be permitted, either within one building or in separate buildings. When in combination within a building or on a single site, an integrated development will be required;
- the preferred location of office development will be adjacent to Hurontario Street;
- architectural building and site plan solutions will be developed to create an interesting and identifiable street edge along Hurontario Street;
- special consideration may be given to reducing building setbacks and other requirements of the

Zoning By-law as a means of increasing the opportunities for creative development; and

- in order to achieve a continuous street frontage, buildings will be encouraged to minimize setbacks from street lines.

#### 13.3.4.2 Site 2



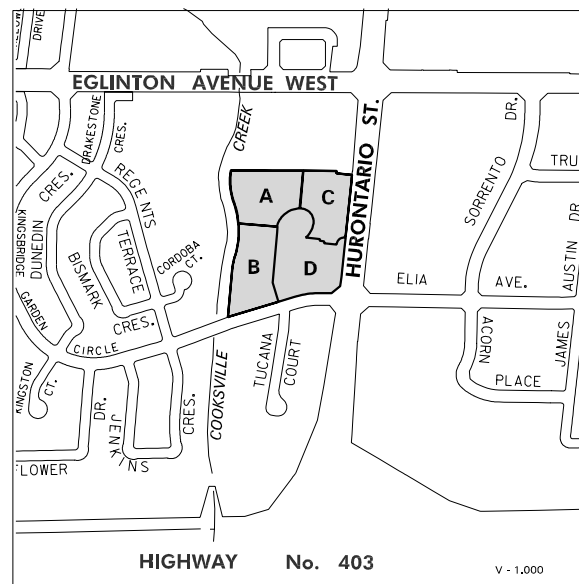
13.3.4.2.1 The lands identified as Special Site 2 are located east of Hurontario Street and north of Eglinton Avenue East.

13.3.4.2.2 Notwithstanding the provisions of this Plan, the following additional policies will apply:

- a concept plan will be required to address, among other matters: compatibility of building form and scale with existing and proposed surrounding land uses; and acceptable ingress and egress arrangements for Hurontario Street, Eglinton Avenue East, and Thornwood Drive;
- a maximum of 1,323 dwellings units will be permitted collectively on areas identified as 2A and 2C;
- a minimum of 2,750 m<sup>2</sup> and a maximum of 6,300 m<sup>2</sup> of retail commercial and office space will be provided, contained within the first three floors of the apartment buildings within Area 2A;

- d. notwithstanding the provision of the Residential High Density designation, the lands identified as Area 2A will be permitted to develop to maximum **Floor Space Index (FSI)** of 6.13, and have a maximum building height of 30 storeys; and
- e. notwithstanding the provision of the Residential Medium Density designation, semi-detached dwellings will also be permitted on the lands identified as Area 2B.

### 13.3.4.3 Site 3



13.3.4.3.1 The lands identified as Special Site 3 are located west of Hurontario Street, north of Kingsbridge Garden Circle.

#### Area 3A

13.3.4.3.2 Notwithstanding the provisions of the Residential High Density designation, the lands identified as Area 3A will be permitted to develop to a maximum **floor space index (FSI)** of 3.9.

#### Area 3B

13.3.4.3.3 Notwithstanding the provisions of the Residential High Density designation, the lands identified as Area 3B will be permitted to develop to a maximum **floor space index (FSI)** of 3.6.

#### Area 3C

13.3.4.3.4 Notwithstanding the provisions of the Residential High Density designation, the lands identified as Area 3C will be permitted to develop to a maximum **floor space index (FSI)** of 3.6. Offices will also be permitted.

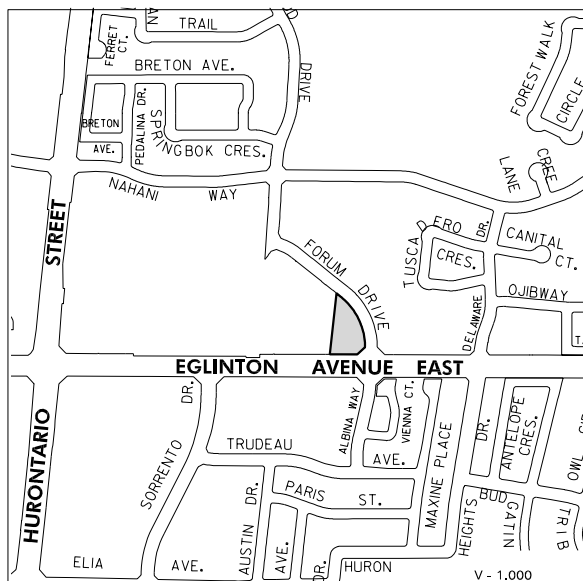
#### Area 3D

13.3.4.3.5 Notwithstanding the provisions of the Residential High Density designation, on the lands identified as Area 3D offices will also be permitted.



### 13.3.4.4

### Site 4

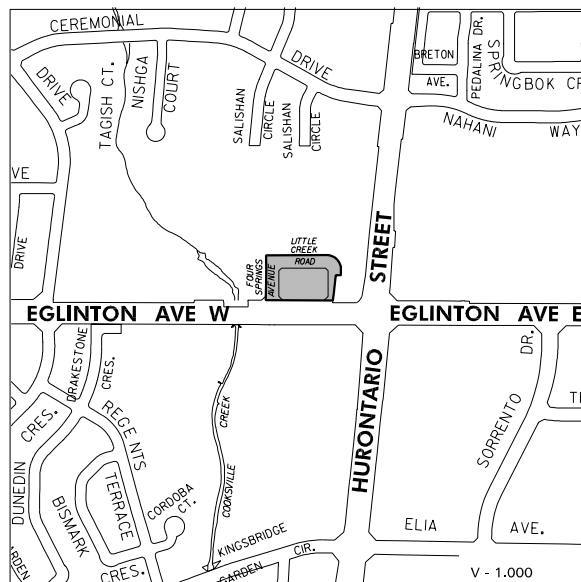


13.3.4.4.1 The lands identified as Special Site 4 are located at the northwest corner of Eglinton Avenue East and Forum Drive.

13.3.4.4.2 Notwithstanding the provisions of the Residential High Density designation, an apartment building to a maximum **floor space index (FSI)** of 3.7 and a maximum height of 20 storeys, will be permitted.

### 13.3.4.5

### Site 5



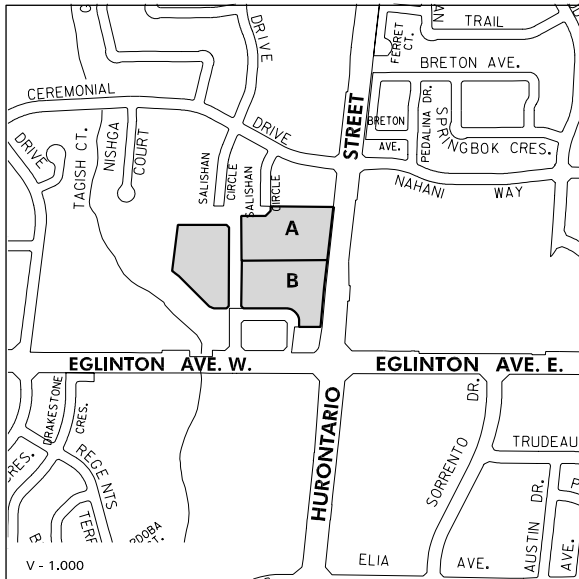
13.3.4.5.1 The lands identified as Special Site 5 are located on the north side of Eglinton Avenue West, west of Hurontario Street.

13.3.4.5.2 Notwithstanding the provisions of the Residential High Density designation, the maximum **floor space index (FSI)** permitted for all development shall be 7.5.

13.3.4.5.3 Notwithstanding the maximum height provisions, a maximum height of 28 storeys will be permitted.

### 13.3.4.6

### Site 6



13.3.4.6.1 The lands identified as Special Site 6 are located within the northwest quadrant of Eglinton Avenue West and Hurontario Street.

13.6.4.6.2 Notwithstanding the provisions of this Plan, the following additional policies will apply:

- a. A maximum of 1,969 dwelling units will be permitted;
- b. A minimum of 11,000 m<sup>2</sup> and a maximum of 25,200 m<sup>2</sup> of retail commercial and office space will be provided, contained within the first three floors of the apartment buildings within Area 6A and 6B;
- c. Notwithstanding the provisions of the Residential High Density designation, the lands identified as Area 6A will be permitted to develop to a maximum **floor space index (FSI)** of 5.19. Townhouse dwellings will also be permitted; and
- d. Notwithstanding the provision of the Residential High Density designation, the lands identified as Area 6B will be permitted to develop to a maximum **floor space index (FSI)** of 7.11 and a height of 34 storeys.

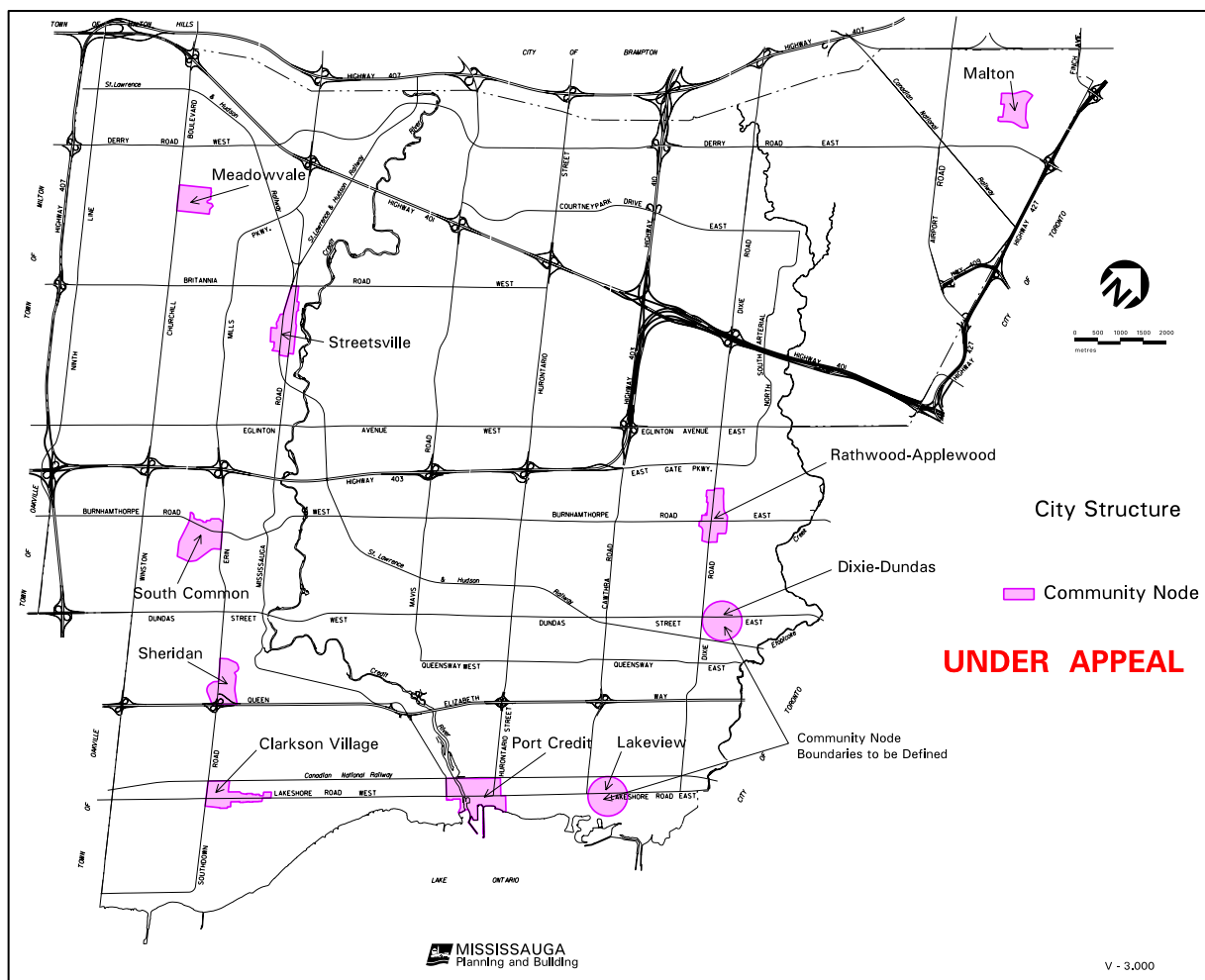
# 14 Community Nodes

## 14.1 Introduction

There are ten Community Nodes in Mississauga:

- Clarkson Village;
- Dixie-Dundas;
- Lakeview;
- Malton;
- Meadowvale;
- Port Credit;
- Rathwood-Applewood;
- Sheridan;
- South Common; and
- Streetsville.

This section identifies the modifications to the General Land Use designations in Chapter 11 that apply to all Community Nodes.



Map 14-1: City Structure – Community Node

## 14.1.1 General

14.1.1.1 Proponents of development applications within a Community Node may be required to demonstrate how the new development contributes to the achievement of the residents and jobs density target and the population to employment ratio.

14.1.1.2 For lands within a Community Node a minimum building height of two storeys to a maximum building height of four storeys will apply, unless Character Area policies specify alternative building height requirements or until such time as alternative building heights are determined through the review of Character Area policies.

14.1.1.3 Proposals for heights less than two storeys, more than four storeys or different than established in the Character Area policies will only be considered where it can be demonstrated to the City's satisfaction, that:

- a. an appropriate transition in heights that respects the surrounding context will be achieved;
- b. the development proposal enhances the existing or planned development;
- c. the City Structure hierarchy is maintained; and
- d. the development proposal is consistent with the policies of this Plan.

## 14.1.2 Residential

14.1.2.1 The Residential Low Density I and Residential Low Density II designations will not be permitted, except for lands designated Low Density I and Residential Low Density II at the time this Plan comes into effect.

14.1.2.2 Notwithstanding the Residential Medium Density policies of this Plan, the following additional uses will be permitted:

- a. low-rise apartment dwellings.

14.1.2.3 Proposals for additional development on lands with existing apartment buildings will, as a condition of development, demonstrate that the site in its entirety meets current site plan and landscaping requirements, and existing buildings meet current building code, fire code and property standards.

## 14.1.3 Office

14.1.3.1 Notwithstanding the Office policies of this Plan, the following additional uses will be permitted:

- a. Post-secondary educational facilities.

## 14.1.4 Motor Vehicle Commercial

14.1.4.1 The Motor Vehicle Commercial designation will not be permitted, except for lands designated Motor Vehicle Commercial at the time this Plan comes into effect.

## 14.1.5 Business Employment

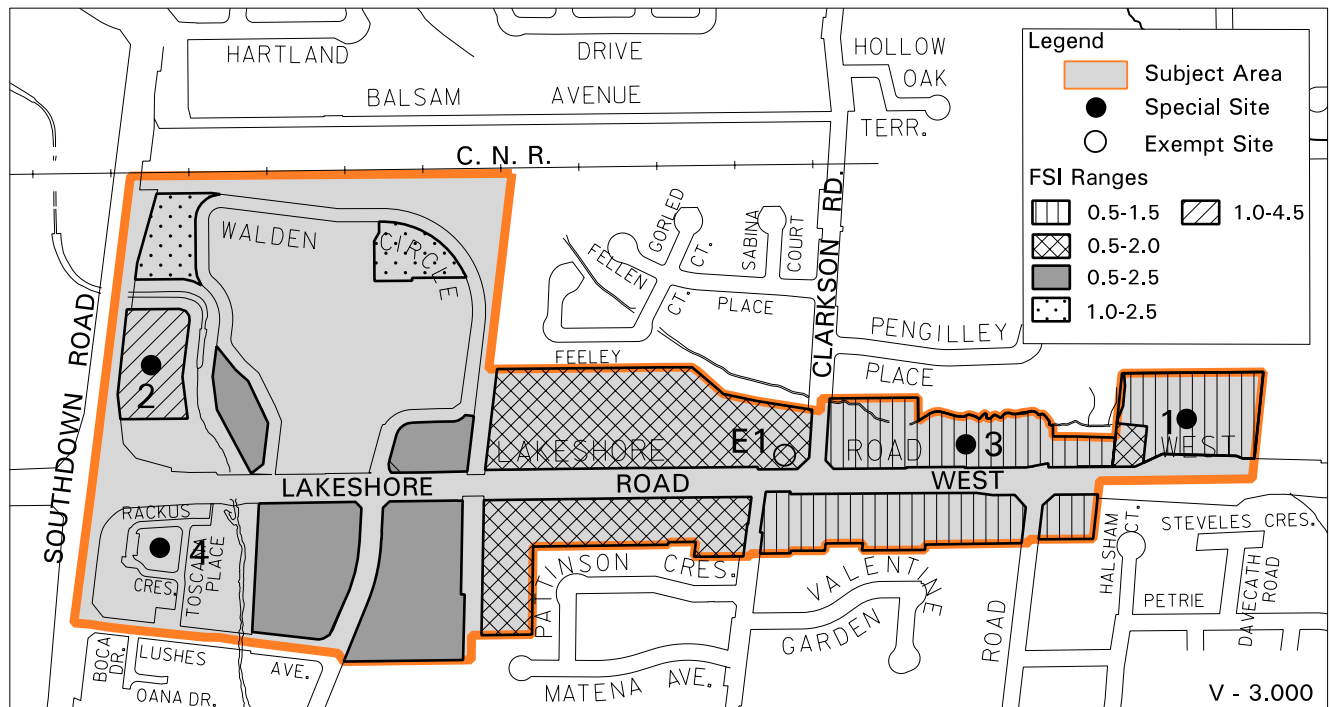
14.1.5.1 The Business Employment designation will not be permitted, except lands designated Business Employment at the time this Plan comes into effect.

## 14.1.6 Industrial

14.1.6.1 The Industrial designation will not be permitted.



## 14.2 Clarkson Village



Map 14-2: Clarkson Village Community Node Character Area

### 14.2.1 Urban Design Policies

#### Shared Community Vision and Focus

14.2.1.1 Clarkson Village Community Node is to transition into a pedestrian friendly and transit supportive community full of activity places and gathering spaces, with a mainstreet atmosphere found amidst new, contemporary, mixed use, development paying tribute to the Village's heritage and character.

14.2.1.2 The Clarkson Village Community Node will be the focus of activity for the surrounding Clarkson-Lorne Park Neighbourhood, combining residential uses, cultural activities, shopping, dining, commerce and recreation.

14.2.1.3 Precincts have been identified as shown on Map 14-2.1 Precinct Areas – Clarkson Village Community Node, to reflect the character of different areas and permitted heights within the Community Node.

14.2.1.4 Development will be compatible with and enhance the Village character as a distinct established community by integrating with and transitioning to the adjacent Clarkson-Lorne Park Neighbourhood.

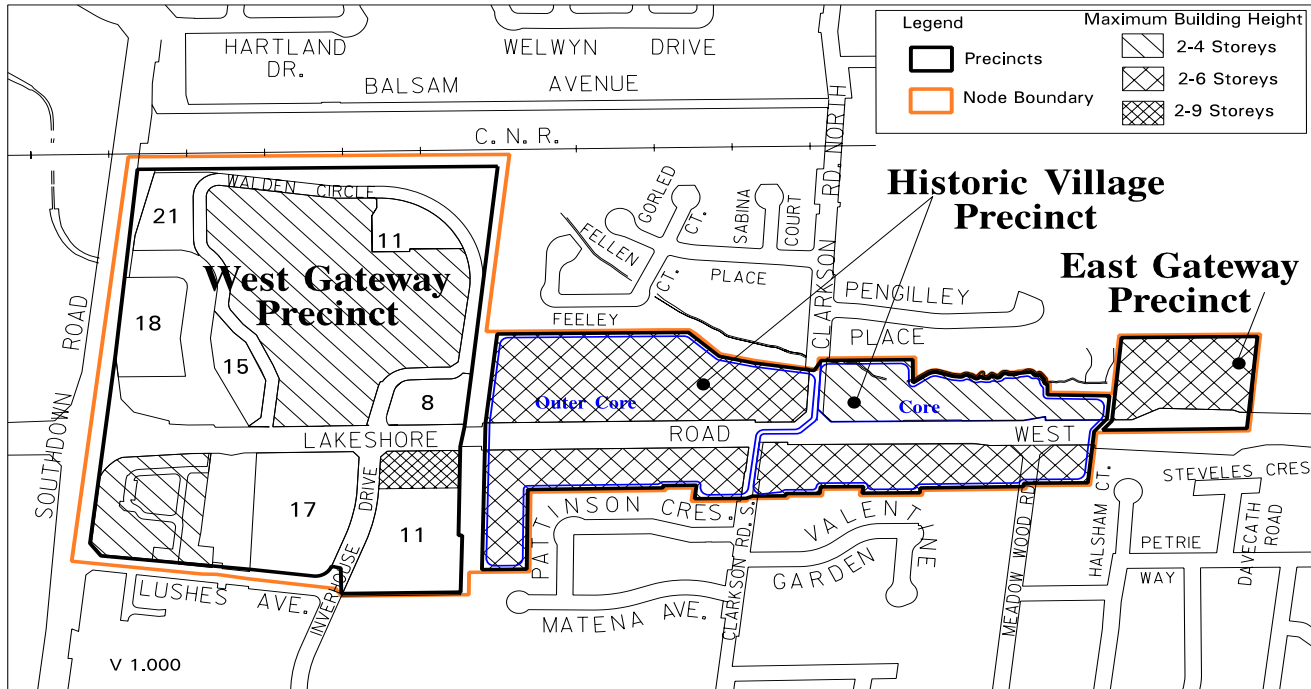
14.2.1.5 Development will incorporate a high level of urban design, pedestrian amenity, landscaping and will be of a compact form to ensure a strong sense of place, a high quality **streetscape** and reinforce the Clarkson Village Community Node as the centre of activity for the area.

14.2.1.6 Development will be in accordance with minimum and maximum height limits as shown on Map 14-2.1 Precinct Areas – Clarkson Village Community Node.

14.2.1.7 Built form will be located close to the street, with a two to three storey streetwall. Where additional height is permitted, it will be stepped back from the streetwall to minimize shadowing, maximize skyviews, maintain a desirable **streetscape** and ensure new development is

consistent and compatible with the existing building

shopping street and to encourage the visual



Map 14-2.1: Precinct Areas-Clarkson Village Community Node

fabric.

14.2.1.8 Where mixed use buildings are proposed, non-residential floor space will be excluded from the calculation of **FSI**.

14.2.1.9 New development along Lakeshore Road West will be located close to the street and promote a continuous street wall.

14.2.1.10 On-site parking will not be permitted between the streetwall and the street.

14.2.1.11 Mississauga will encourage on-street lay-by parking and the provision of well lit, on-site parking located at the rear of buildings with clear visibility and sight lines through to Lakeshore Road West.

### Historic Village Precinct

14.2.1.12 Development will respect the character of the existing building fabric as well as transition appropriately to the adjacent Clarkson-Lorne Park Neighbourhood. New development will encourage a pedestrian oriented **streetscape** and a walkable community, preserve elements along the edge of Turtle Creek to connect to the parking areas and the

enjoyment and surveillance of this natural creek feature.

14.2.1.13 Residential uses will not be permitted on the ground floor adjacent to Lakeshore Road West.

14.2.1.14 Large format retail development will be discouraged.

14.2.1.15 Lands located at the northwest corner of Lakeshore Road West and Clarkston Road North are encouraged to redevelop as a focal point of the Clarkson Village Community Node.

14.2.1.16 Within the Core area, as shown on Map 14-2 Clarkson Village Community Node Character Area, required parking for new development with a **FSI** greater than 1.0 will be within an underground structure.

14.2.1.17 Within the Outer Core area, as shown on Map 14-2 Clarkson Village Community Node Character Area, required parking for new development with a **FSI** greater than 1.0 will be within a parking structure located either above or below grade. Where an above grade structure is provided, it will be located to the rear of an active

building facade, will be no more than two storeys in height and will not be visible from public roads. Where above grade parking structures are visible from adjacent lands, they will utilize appropriate finish materials and be of a high architectural quality.

### **West Gateway Precinct**

14.2.1.18 New development will maintain the existing height transition with the highest built form at the west end, closest to the Clarkson GO Transit Station, and lowest building heights at the east end, adjacent to the CN Rail overpass and the Historic Village Precinct.

14.2.1.19 Mixed use buildings will be located close to the street to ensure a pedestrian oriented environment.

14.2.1.20 Mississauga will encourage a symbolic gateway feature on Lakeshore Road West, east of Southdown Road to define the entry and exit from Clarkson Village Community Node.

### **East Gateway Precinct**

14.2.1.21 Mississauga will encourage a symbolic gateway feature on Lakeshore Road West, west of Johnson's Lane to define the entry and exit from the Clarkson Village Community Node.

14.2.1.2 At grade, street related retail, commercial, restaurants and office uses are encouraged. Mixed use buildings will be located close to the street to ensure a pedestrian oriented environment.

## **14.2.2 Land Use**

14.2.2.1 Notwithstanding the policies of this Plan, drive-through facilities will not be permitted.

## **14.2.3 Transportation, Access and Parking**

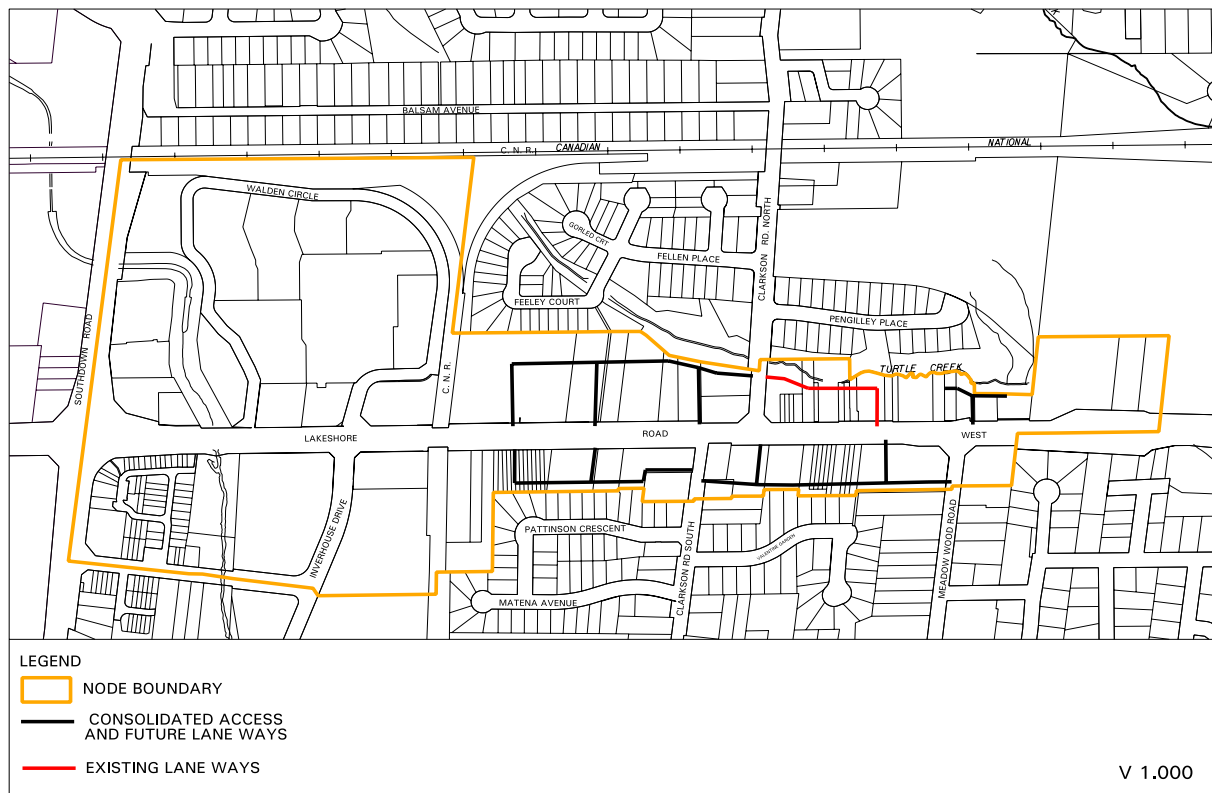
14.2.3.1 No major changes to Clarkson Road will be undertaken except minor channelization, reconstruction, bus-bay construction, and improvements of a similar nature.

14.2.3.2 Notwithstanding the classification of Clarkson Road, this road will be limited to no more than two through lanes.

14.2.3.3 Mississauga will encourage and promote better utilization of the existing parking inventory within the Clarkson Village Community Node through discussions with the members of the Clarkson Village Business Improvement Area.

14.2.3.4 Development within the Clarkson Village Community Node will implement the general intent of Map 14-2.2 Access Management Plan - Clarkson Village Community Node and will:

- a. eliminate and/or consolidate vehicular access connections to and from Lakeshore Road West to reduce vehicle turning movements onto and direct traffic towards signalized intersections;
- b. facilitate the creation of a publicly accessible laneway system by granting public use easements over internal driveways to facilitate access to and from abutting lands to the east and west and to consolidate vehicular access connections to Lakeshore Road West;
- c. contribute a proportionate share towards the construction of a continuous centre median along Lakeshore Road West; and
- d. where the ultimate condition cannot be accommodated, interim solutions will be accommodated to ensure that vehicular access rights are maintained and appropriate interim



Map 14-2.2: Access Management Plan-Clarkson Village Community Node Character Area

agreements will be executed to ensure the ultimate condition will be achieved.

14.2.3.5 Where surface parking is being provided at the rear of buildings, communal parking spaces accessed from the public laneway system will be encouraged.

14.2.3.6 Through the development review and approval process, developers will be responsible for the construction of on-street lay-by parking in accordance with the Clarkson Village Transportation/Urban Design Study – Final Report.

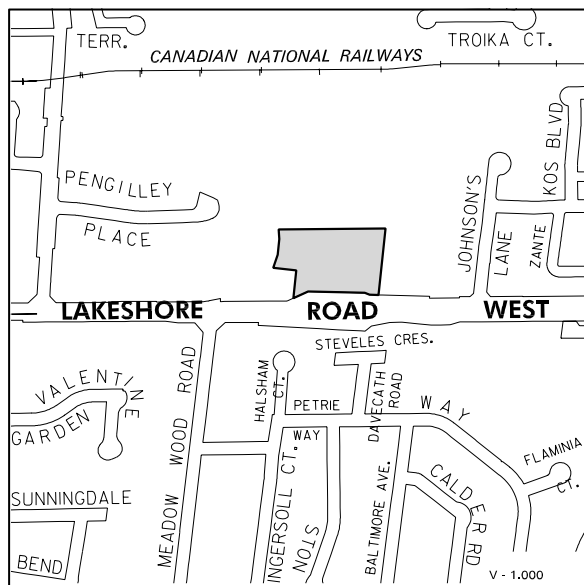
14.2.3.7 A dedicated cycling route will be provided along Lakeshore Road West.



#### 14.2.4 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

##### 14.2.4.1 Site 1

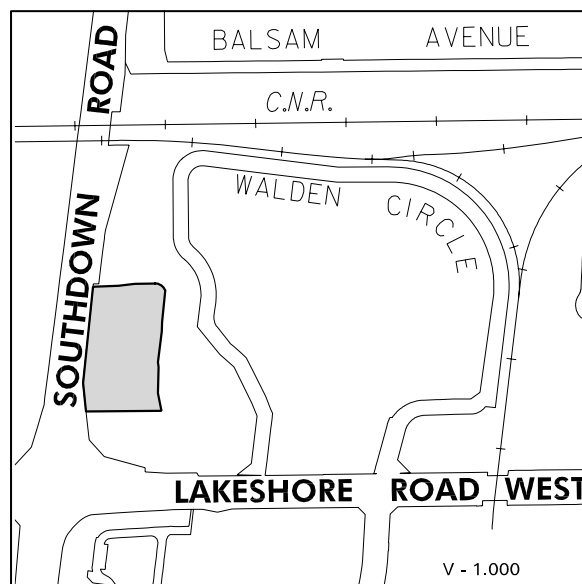


14.2.4.1.1 The lands identified as Special Site 1 are located on the north side of Lakeshore Road West, east of Clarkson Road.

14.2.4.1.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:

- the maximum **floor space index (FSI)** will be 1.5;
- the maximum number of apartment dwelling units permitted will be 210;
- the maximum number of townhouse dwellings permitted will be 32; and
- the maximum apartment building height will be six storeys.

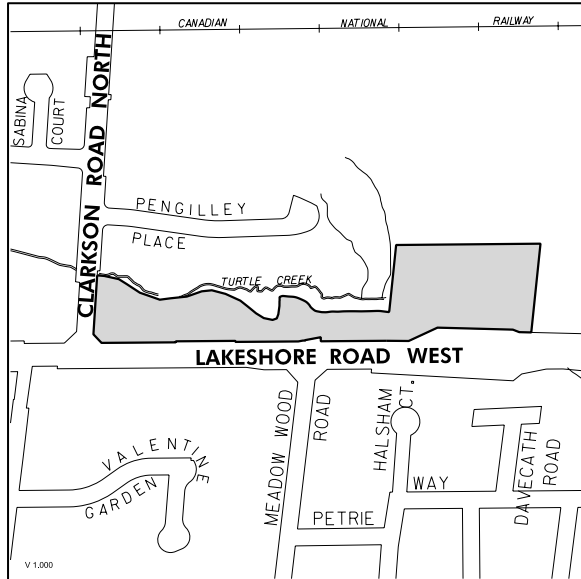
##### 14.2.4.2 Site 2



14.2.4.2.1 The lands identified as Special Site 2 are located on the east side of Southdown Road, north of Lakeshore Road West.

14.2.4.2.2 Notwithstanding the policies of this Plan, the maximum permitted number of apartment dwelling units will be 424.

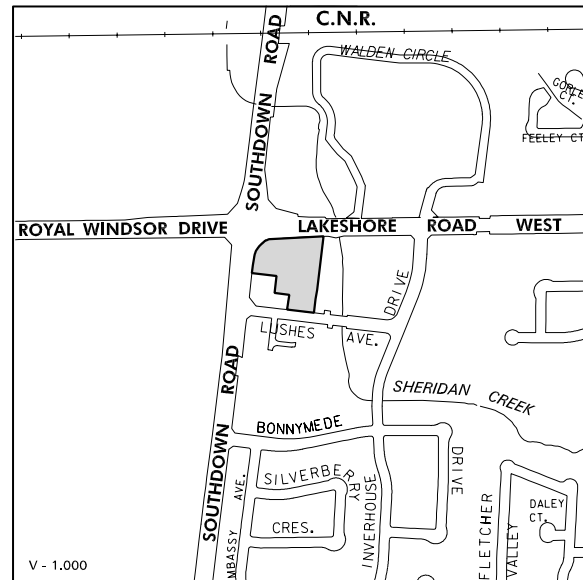
#### 14.2.4.3 Site 3



14.2.4.3.1 The lands identified as Special Site 3 are located on the north side of Lakeshore Road West between Birchwood Park and Clarkson Road North.

14.2.4.3.2 The lands are encumbered by slope stability issues associated with Turtle Creek. Satisfactory technical reports addressing these issues are required prior to any development of these lands.

#### 14.2.4.4 Site 4



14.2.4.4.1 The lands identified as Special Site 4 are located on the southeast corner of Lakeshore Road West and Southdown Road.

14.2.4.4.2 Notwithstanding the policies of this Plan, a maximum of two detached dwellings will be permitted.

## 14.2.5 Exempt Site Policies

### 14.2.5.1 Site 1

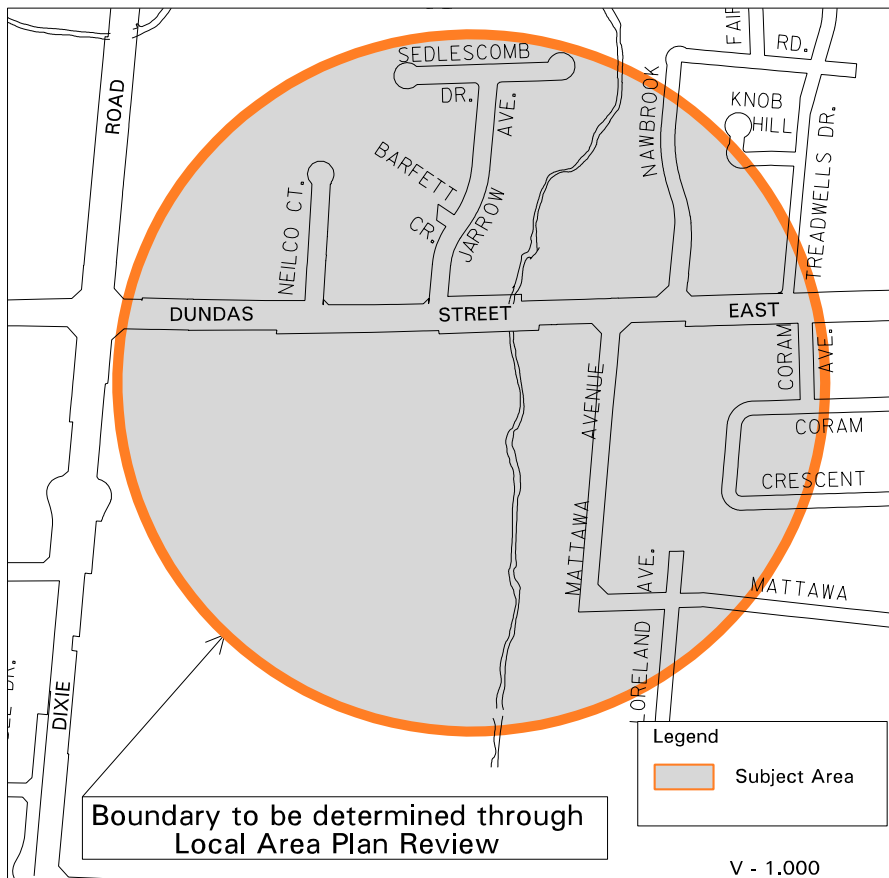


14.2.5.1.1 The lands identified as Exempt Site 1 are located at the northwest corner of Lakeshore Road West and Clarkson Road.

14.2.5.1.2 Notwithstanding the provisions of the Mixed Use designation, a motor vehicle service station and motor vehicle wash will be permitted.

## 14.3 Dixie-Dundas

The boundary of and official plan policies for lands within the Dixie-Dundas Community Node will be determined through a Local Area Plan review. Until such time as the Local Area Plan review is completed, the official plan policies are contained in the Dixie Employment Area Character Area Policies.

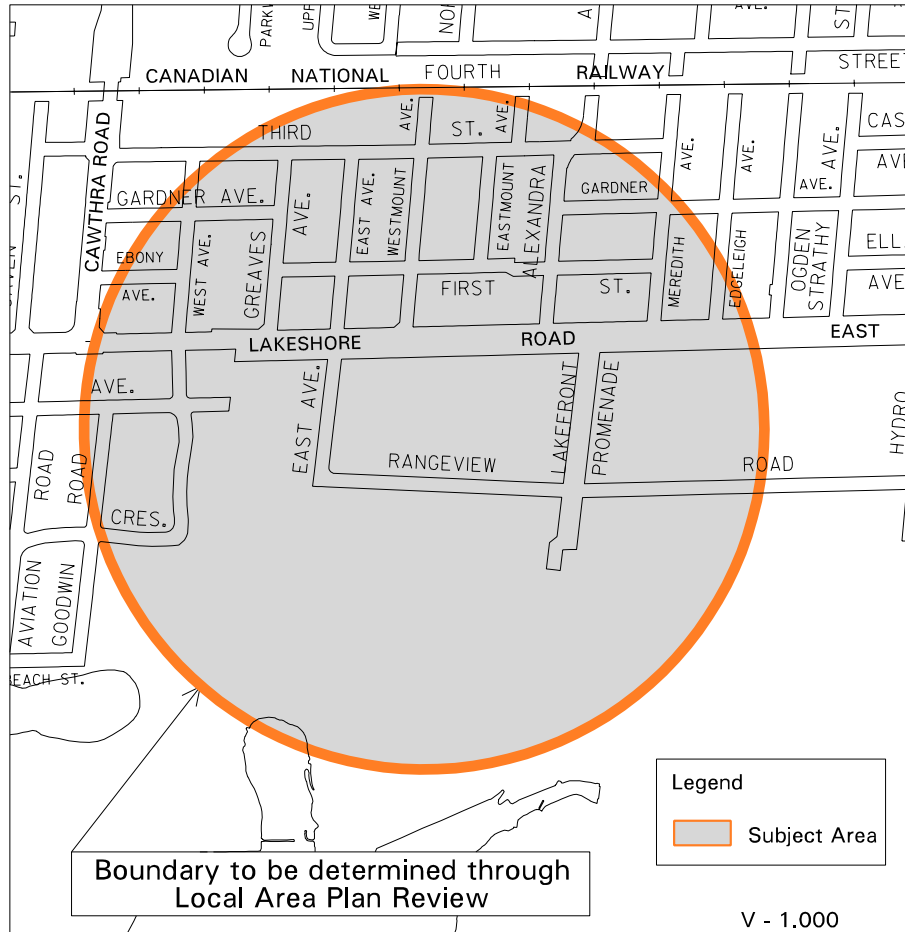


Map 14-3: Dixie-Dundas Community Node Character Area



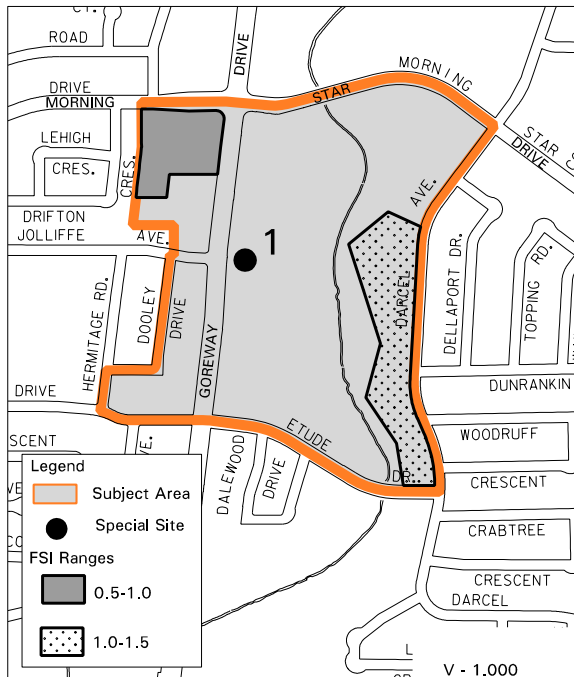
## 14.4 Lakeview

The boundary of the Lakeview Community Node will be determined through a Local Area Plan review. Until such time as the Local Area Plan review, is completed, the official plan policies for lands within the Lakeview Community Node are contained in the Lakeview Local Area Plan.



Map 14-4: Lakeview Community Node Character Area

## 14.5 Malton



Map 14-5: Malton Community Node Character Area

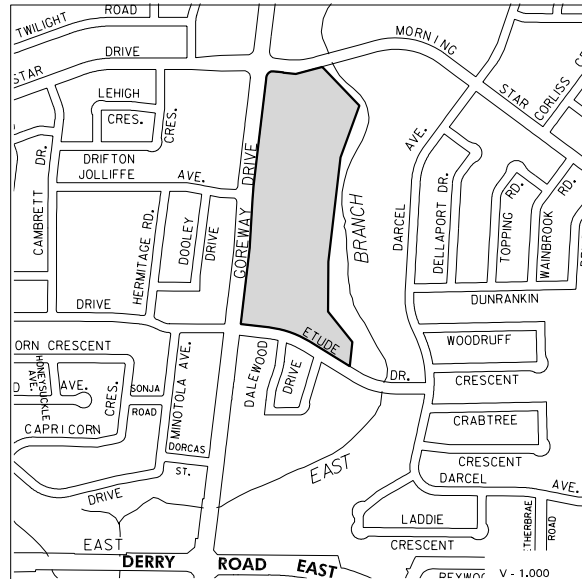
### 14.5.1 Urban Design Policies

14.5.1.1 During the evaluation of any development proposal the provision of open space connections that link commercial developments with public parks and community infrastructure through use of walkways, underpasses, bridges, **streetscape** development, and enhancement of pedestrian and cyclist access to greenbelt lands, should be encouraged wherever possible.

### 14.5.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 14.5.2.1 Site 1



14.5.2.1.1 The lands identified as Special Site 1 are located on the east side of Goreway Drive between Morningstar Drive and Etude Drive.

14.5.2.1.2 Notwithstanding the provisions of the Mixed Use designation of this Plan, the following additional policies will apply.

#### Pedestrian Linkages

14.5.2.1.3 Pedestrian connections to the commercial uses along Goreway Drive are vital to their role in the community. The following pedestrian links should be maintained and/or improved as part of any development application:

- a. open space walkway linkages to the following parks: Elmcreek Park, Malton Greenway and Derry Greenway;
- b. mid-block connection to Jolliffe Avenue; and

- c. linkages to the Malton Community Centre, the Transit Terminal and Lincoln M. Alexander Secondary School (emphasizing safety).

14.5.2.1.4 Where pedestrian linkages from the Greenbelt and Residential Community exist, they are to be strengthened.

#### **Focal Points**

14.5.2.1.5 A focal point on the north side of Westwood Mall, should be developed:

- a. to strengthen the pedestrian connection from the corner of Goreway Drive and Morning Star Drive and from the Transit Terminal to Westwood Mall; and
- b. to improve the building massing edge treatment, parking lot circulation, and **streetscape** development at the southeast corner of Goreway Drive and Morning Star Drive.

14.5.2.1.6 The main entrance to Westwood Mall from Goreway Drive, opposite Jolliffe Avenue, should be improved to contribute to the identity of the site through the use of built form, landscaping, directional signs and/or lighting.

#### **Parking Areas**

14.5.2.1.7 Parking areas around Westwood Mall should be clearly defined to facilitate the safe movement of pedestrians, cyclists and vehicles through the site. Parking areas should be improved by ensuring adequate sight lines, incorporating landscape islands that define vehicular routes throughout the site, and providing sidewalks and lighting.

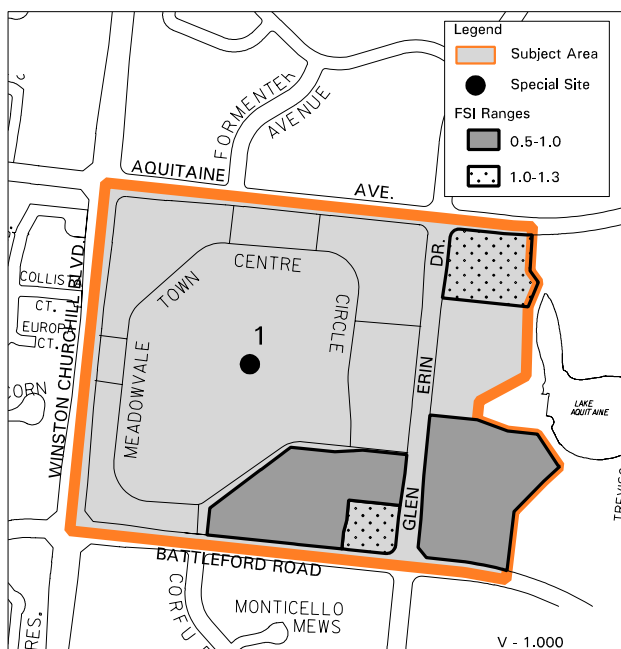
#### **Building and Massing**

14.5.2.1.8 Should additional development occur along the Goreway Drive frontage the following items should be addressed:

- a. no parking or driveway areas should be provided between the buildings and the street line;

- b. blank walls should be avoided along the street in favour of windows, building entrances and architectural detailing;
- c. service, loading, and garbage storage areas should be located on the internal side of the development, away from public streets, and should be screened from view by means of built form and landscaping; and
- d. all building entrances should be clearly articulated and linked to pedestrian walkway connections.

## 14.6 Meadowvale



Map 14-6: Meadowvale Community Node Character Area

### 14.6.1 Land Use

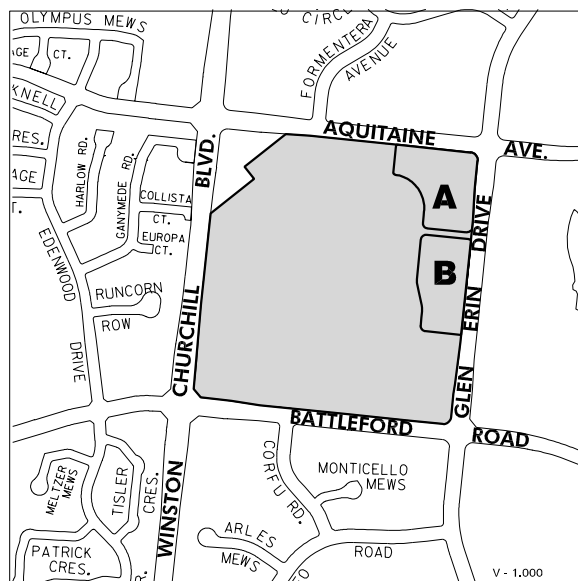
14.6.1.1 In addition to the general Residential Medium and High Density development policies of this Plan, the following additional policies will apply:

- a. in order to create acceptable built form transitions, buildings should be limited in height when adjacent to low density residential neighbourhoods; and
- b. in situations where the low density housing forms are separated from high density development by a public road or other permanent open space feature, a height of four to five storeys may be compatible.

### 14.6.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 14.6.2.1 Site 1



14.6.2.1.1 The lands identified as Special Site 1 are bounded by Aquitaine Avenue to the north, Glen Erin Drive to the east, Battleford Road to the south and Winston Churchill Boulevard to the west.

14.6.2.1.2 Notwithstanding the provisions of the Mixed Use, Residential Medium Density and Residential High Density designations, the following additional policies will apply:

- a. Area A is located at the southwest corner of Aquitaine Avenue and Glen Erin Drive. Notwithstanding the provisions of the Residential Medium Density designation, only townhouses and semi-detached dwellings with a maximum Gross Floor Area (GFA) of 0.6 times the lot area in the range of 34.6 to 44.5 units per **net residential hectare** are permitted; and
- b. Area B is located between Aquitaine Avenue and Battleford Road on the west side of Glen Erin Drive. Notwithstanding the provisions of the Residential Medium Density designation,



only townhouses with a maximum Gross Floor Area (GFA) of 0.63 times the lot area in the range of 34.6 to 44.5 units **net residential hectare** are permitted.

### Pedestrian Linkages

14.6.2.1.3 Pedestrian connections to the Town Centre are vital to its role in the community. The following pedestrian links should be maintained and/or improved as part of any development application:

- a. open space walkway linkages to the following parks: Lake Aquitaine, Hunter's Green and Settler's Green;
- b. mid-block connections to the area west of Winston Churchill Boulevard between Battleford Road and Aquitaine Avenue;
- c. linkages to the Meadowvale Community Centre, medical offices and the transit terminal; and
- d. diagonal connections to the site from the northeast corner of Battleford Road and Winston Churchill Boulevard.

14.6.2.1.4 A strong connection between the bicycle route on Winston Churchill Boulevard and the Town Centre should be provided.

### Focal Areas

14.6.2.1.5 A focal area on the east side of the Town Centre between the existing transit terminal, medical offices, church campus and Town Centre tower should be developed to strengthen the space as an outdoor amenity with seating, landscaping, lighting or staging areas; additional building massing should contribute to the central courtyard area by framing the interior space.

14.6.2.1.6 One main entrance to the Town Centre from Winston Churchill Boulevard should be developed to strengthen the site entry and contribute to the identity of the site through the use of built form, landscaping, directional signs or lighting.

### Ring Road and Parking Areas

14.6.2.1.7 The internal ring road and parking areas around the Town Centre should be clearly defined to facilitate the safe movement of pedestrians and vehicles through the site; definition of the ring road **streetscape** should be improved by ensuring adequate sight lines, incorporating landscape islands that define parking areas, and providing sidewalks and lighting.

### Building Massing

14.6.2.1.8 Development along the public road frontages should address the following:

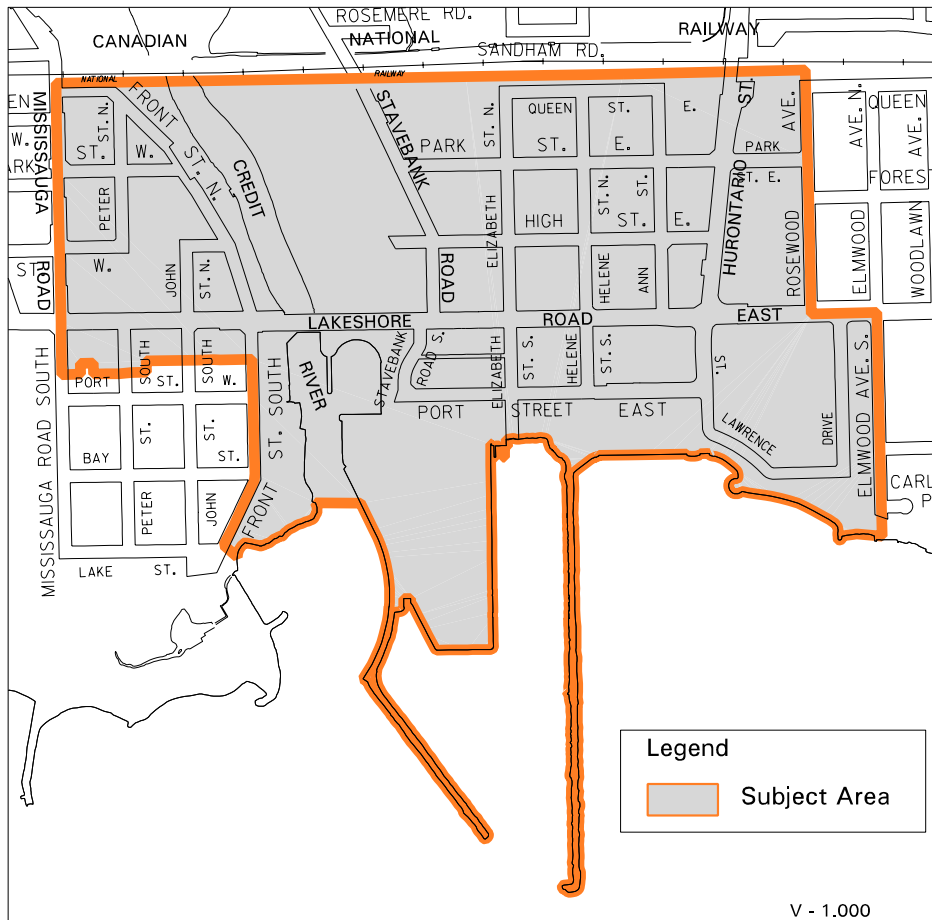
- a. no parking or driveway areas should be provided between the buildings and the street line;
- b. blank walls should be avoided along the street in favour of fenestration, building entrances and architectural detailing;
- c. service, loading and garbage storage areas should be located on the internal side of the development away from public streets and screened from view by means of built form and landscaping; and
- d. all building entrances should be clearly articulated and linked to pedestrian walkway connections.

### Streetscape

14.6.2.1.9 Landscape treatment of the Winston Churchill frontage of the Town Centre should reinforce the treatment within the municipal boulevard.

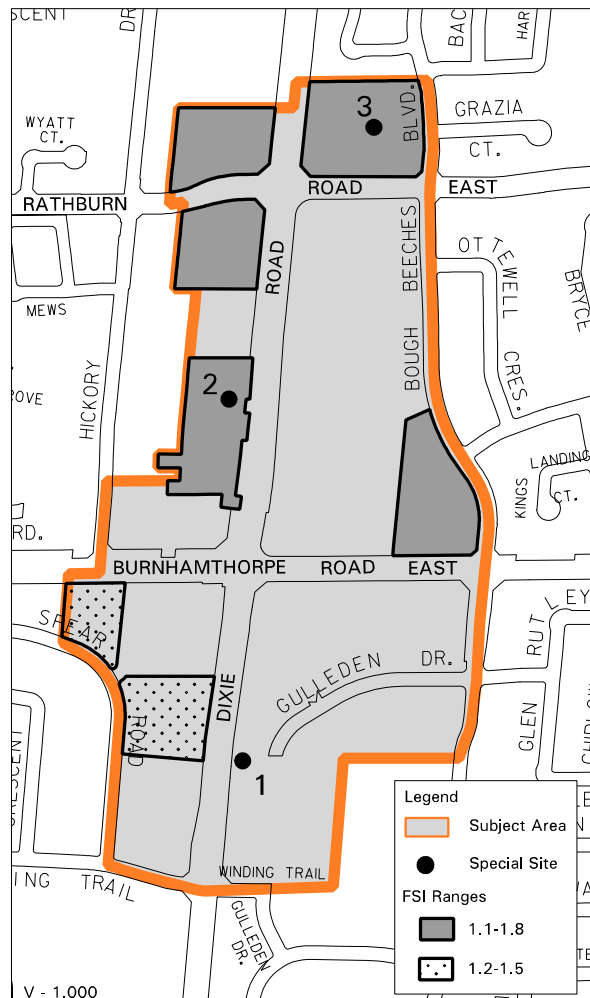
## 14.7 Port Credit

Official Plan policies for lands within the Port Credit Community Node are contained in the Port Credit Local Area Plan.



Map 14-7: Port Credit Community Node Character Area

## 14.8 Rathwood-Applewood



Map 14-8: Rathwood-Applewood Community Node Character Area

### 14.8.1 Land Use

14.8.1.1 Notwithstanding the provisions of the Residential Medium or High Density designations that are south of Burnhamthorpe Road, the following additional policies will apply:

- new development should not exceed the height of any existing buildings on the property, and should be further limited in height so as to form a gradual transition in massing when located adjacent to low density residential development;
- buildings immediately adjacent to low density housing forms should be limited to three storeys; and

- in situations where the low density housing forms are separated from the high density development by a public road, park, utility corridor or other permanent open space feature, four to five storeys may be compatible.

### 14.8.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

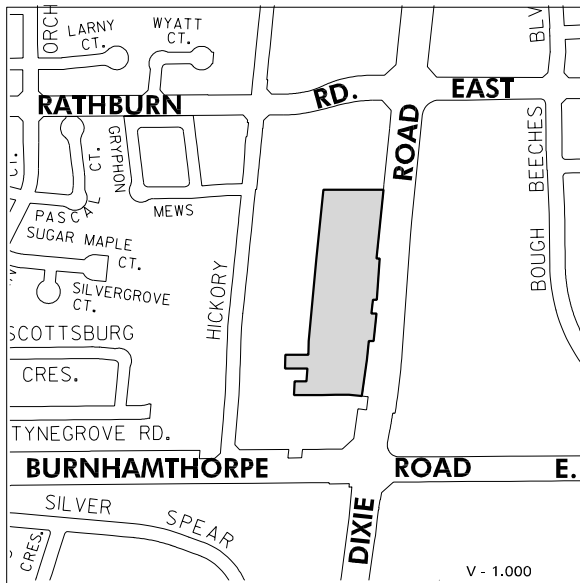
#### 14.8.2.1 Site 1



14.8.2.1.1 The lands identified as Special Site 1 are located on the east side of Dixie Road, south of Burnhamthorpe Road East.

14.8.2.1.2 Notwithstanding the provisions of the Residential Medium Density designation, detached and semi-detached dwellings may also be permitted.

### 14.8.2.2 Site 2



14.8.2.2.1 The lands identified as Special Site 2 are located west of Dixie Road and south of Rathburn Road East.

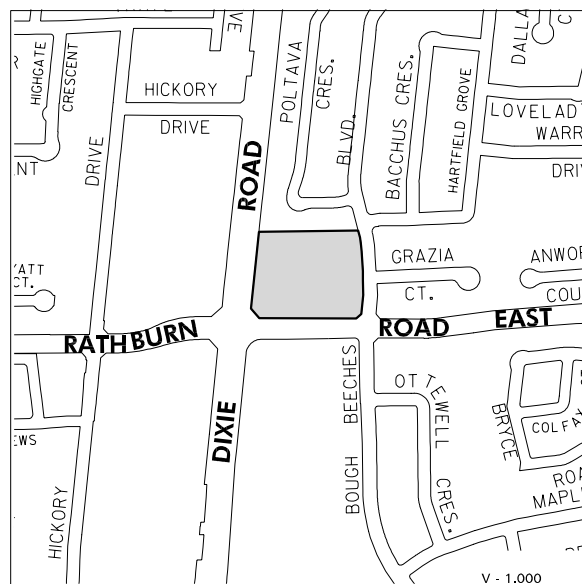
14.8.2.2.2 Notwithstanding the provisions of the Residential High Density and Convenience Commercial designations, the following additional policies will apply:

- a. a concept plan for all or part of this site may be required and will address, among other matters, the following:
  - compatibility of building form and scale with existing surrounding land uses;
  - convenient pedestrian access through the site to nearby transit service on Dixie Road and Burnhamthorpe Road East;
  - traffic generated will not adversely affect the transportation system;
  - acceptable ingress and egress, off-street parking, landscaping and buffering; and
  - preservation of all mature trees and other significant natural features; and
- b. a concept plan will be required to address, to the satisfaction of the City of Mississauga and

the Region of Peel, matters relating to transportation and access onto Dixie Road;

- c. the redevelopment of this site will minimize access points to Dixie Road to preserve the integrity of Dixie Road as a major arterial roadway; and
- d. the redevelopment of this site will allow for the construction of a centrally located street which is parallel to Hickory Drive and Dixie Road. The proposed street will connect to Dixie Road and align with the Rockwood Mall entrance on Dixie Road.

### 14.8.2.3 Site 3



14.8.2.3.1 The lands identified as Special Site 3 are located north of Rathburn Road East and east of Dixie Road.

14.8.2.3.2 Notwithstanding the provisions of the Residential High Density designation, townhouse dwellings will be permitted. The maximum **floor space index (FSI)** permitted for all development shall be 2.56.



## 14.9 Sheridan



Map 14-9: Sheridan Community Node Character Area

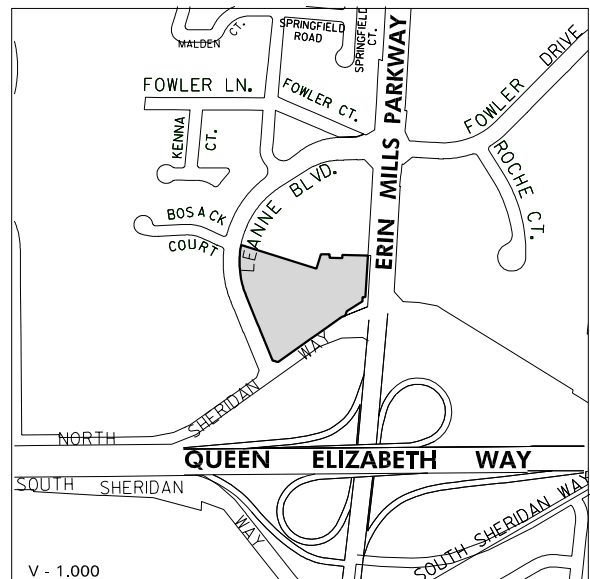
### 14.9.1 Land Use

14.9.1.1 For lands designated Mixed Use, existing major office will be permitted.

## 14.9.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

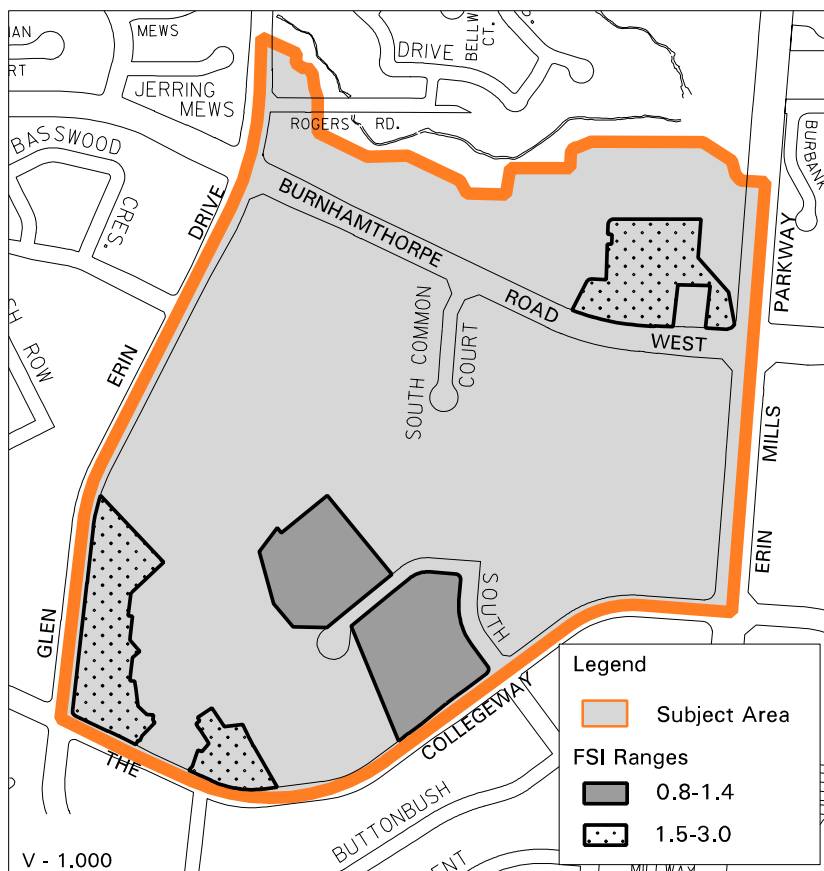
### 14.9.2.1 Site 1



14.9.2.1.1 The lands identified as Special Site 1 are located on the north side of North Sheridan Way, between Erin Mills Parkway and Leanne Boulevard.

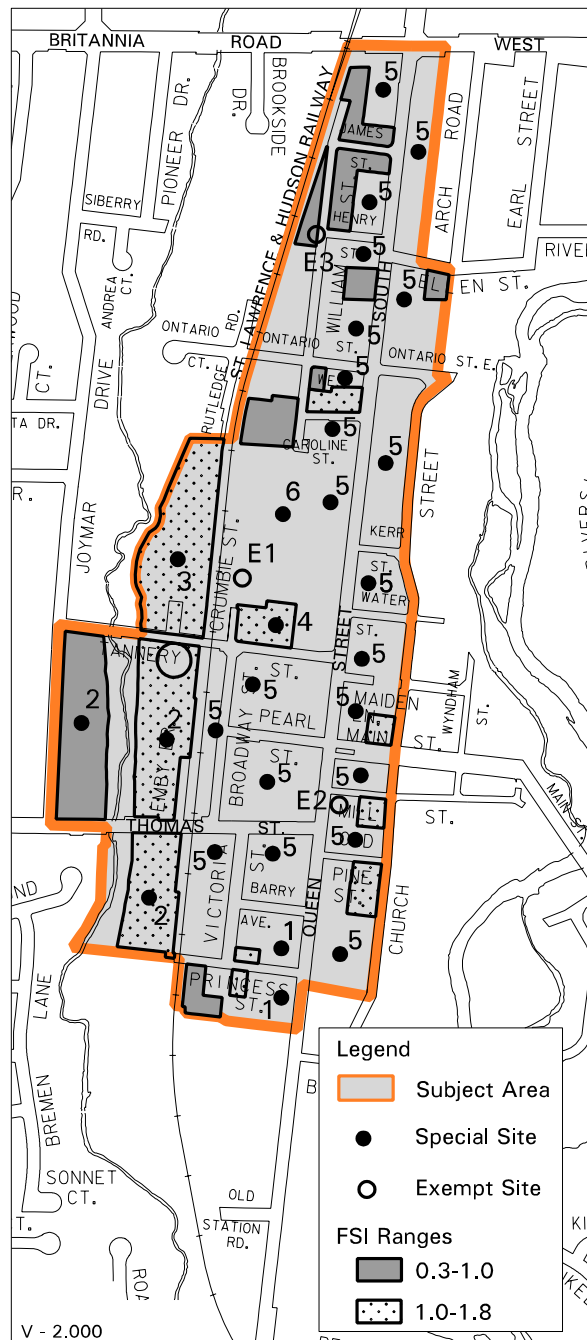
14.9.2.1.2 Notwithstanding the provisions of the Office designation, a long term healthcare facility and overnight accommodations will also be permitted. Limited commercial uses may be considered.

## 14.10 South Common



Map 14-10: South Common Community Node Character Area

## 14.11 Streetsville



Map 14-11: Streetsville Community Node Character Area

### 14.11.1 Urban Design Policies

#### Community Identity and Focus

14.11.1.1 Development will be compatible with and enhance the village character of Streetsville as a distinct established community by integrating with the surrounding area.

14.11.1.2 Development with a high level of urban design, pedestrian amenity, landscaping and compact built form will be encouraged to create a strong sense of place and reinforce the role of the Community Node as the centre of activity for the surrounding community.

14.11.1.3 Queen Street South will remain the focus of the commercial core within the Streetsville Community Node.

14.11.1.4 The development of symbolic gateways to define entry to and exit from the Streetsville Community Node will be encouraged.

#### Historic Character

14.11.1.5 Alterations to heritage structures, including building repairs and additions, and/or alterations to existing vegetation which is part of the cultural heritage landscape, will be in keeping with the original character of the heritage resources to be preserved.

14.11.1.6 Designs for new buildings and additions will enhance the historic character and heritage context of the Streetsville Community Node through appropriate height, massing, architectural pattern, proportions, set back and general appearance.

14.11.1.7 Commercial signs will reflect the historic village character in the Streetsville Community Node. Hand painted wooden signs, projecting signs as permitted by municipal by-laws and other signs which reflect late nineteenth or early twentieth century practices will be encouraged.

14.11.1.8 The established residential character of the areas generally located along Queen Street South, south of Barry Avenue, will be maintained through appropriate building masses, setbacks,

intensive landscaping, **streetscapes** with many mature trees, and a regular street grid pattern.

14.11.1.9 The following will apply to lands designated Mixed Use:

- a. development with a mix of residential and office uses on upper floors and street related commercial uses in closely spaced storefronts lining the street, will be encouraged along the principal streets in the Streetsville Community Node to promote an active pedestrian environment;
- b. new buildings will be at least two storeys but not more than three storeys in height. Building additions will not be more than three storeys in height and will be generally harmonious in style and massing with the buildings to which they are attached;
- c. the apparent height of new buildings will be reduced through massing and design;
- d. new development will be encouraged to reflect the original lot pattern and setback dimensions of surrounding properties. The front setback of new buildings should match the setback of adjacent buildings so as to create a uniform street wall. Where the building setbacks on either side are not equal, the lesser setback should be used;
- e. integrated parking opportunities, which may include rear lane access to combined parking areas and the development of private pay parking lots, will be encouraged where appropriate; and
- f. development which enhances the **streetscape** through landscaping and the provision of pedestrian amenities will be encouraged along the principal streets within the Streetsville Community Node.

#### Public Realm

14.11.1.10 The placement of parking areas to the rear of buildings and the consolidation of driveways will be encouraged along principal street frontages

in the Streetsville Community Node to increase the area available for planting and public amenity.

14.11.1.11 The rear façades of developments which back onto places accessible to the public, including parking areas or laneways, should be designed to provide a level of detail, fenestration, and direct access appropriate to their role as secondary frontages.

### 14.11.2 Land Use

14.11.2.1 Notwithstanding the Residential Low Density I policies of this Plan, the Residential Low Density I designation permits only detached dwellings.

14.11.2.2 For lands designated Residential High Density a maximum building height of seven storeys is permitted.

14.11.2.3 In addition to the uses permitted by the Land Use Policies of this Plan, bed and breakfast establishments are permitted provided that they front upon Queen Street South, Main Street, Thomas Street or Church Street.

### 14.11.3 Heritage

14.11.3.1 Heritage resources will include those properties listed on the City's Heritage Register, but will not be restricted to the list.

14.11.3.2 Mississauga will encourage the investigation of the Streetsville historic core area as an area to be examined for future designation as a Heritage Conservation District in accordance with the Ontario Heritage Act.

### 14.11.4 Stormwater Management

14.11.4.1 A stormwater drainage report will be required to ensure that the existing drainage system has the capability to convey the increased storm flow from redevelopment in accordance with current City drainage standards.



## 14.11.5 Transportation

14.11.5.1 The maximum pavement width of Queen Street South, from Britannia Road West to the St. Lawrence and Hudson Railway, will not exceed a basic two lane cross-section, except for bus bays, on-street parking lanes, turning lanes at intersections and bicycle and pedestrian routes.

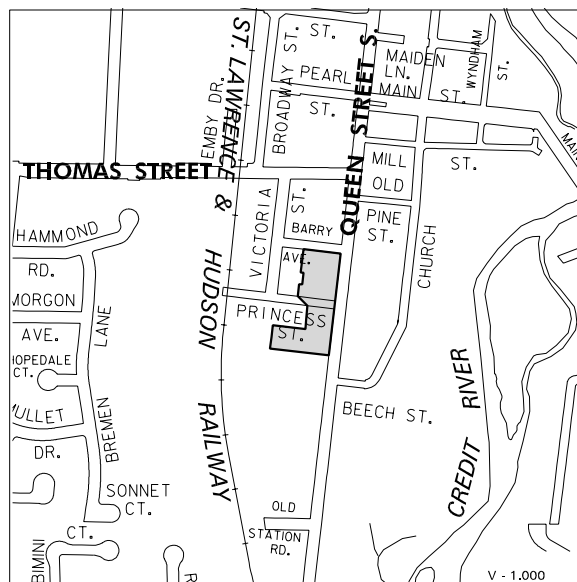
14.11.5.2 Main Street between Queen Street South and Church Street will not be widened to accommodate additional through traffic lanes. Minor modifications to this roadway for on-street parking, safety or maintenance will be permitted, however, major modifications which would have an adverse effect on the character of the roadway, will require an amendment to this Plan.

14.11.5.3 The City will continue to encourage and promote better utilization of the existing and future parking inventory through discussions with the members of the Streetsville Business Improvement Area (BIA), with a goal of developing a self-sufficient parking operation through measures such as parking charges.

## 14.11.6 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 14.11.6.1 Site 1



14.11.6.1.1 In addition to the uses permitted by their Residential Low Density I and II designations, the lands identified as Special Site 1, which front on Queen Street South, may also be used for offices.

14.11.6.1.2 Any office conversion should maintain a residential appearance in keeping with the existing scale, materials, and character of the existing dwellings in the immediate area. The existing houses should be preserved if at all possible, while the interior floor plan may be altered for office use.

14.11.6.1.3 Any additions and alterations of existing buildings should be sensitive to the existing vernacular and heritage village theme, and should be largely confined to the rear of the property.

14.11.6.1.4 Any additions, alteration, conversion or redevelopment should maintain the existing front yard setback, with the front yard used for landscaping.

14.11.6.1.5 Sufficient on-site parking, which will consist of only surface parking, as required by the Zoning By-law, should be provided in the rear yard only at grade without removal of existing trees, except at the discretion of the City arborist.

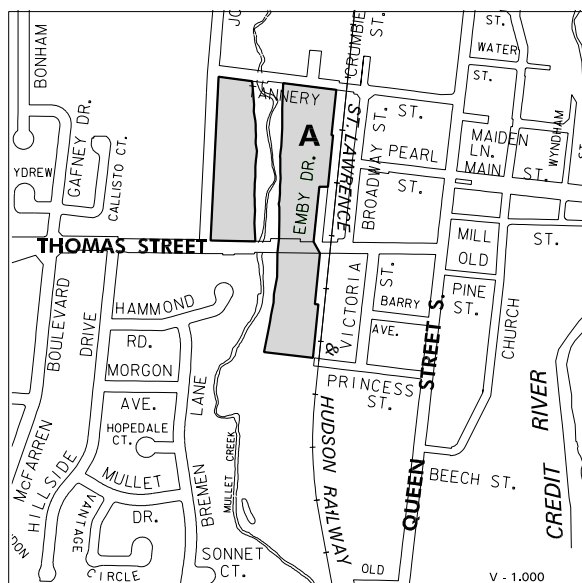
14.11.6.1.6 Vehicular entrances should be combined to minimize the number of access points on Queen Street South.

14.11.6.1.7 Minimal signage will be permitted. The design of such signage and external lighting should be compatible with the residential character of the area.

14.11.6.1.8 Existing lot sizes should be retained.

14.11.6.1.9 Rear yard drainage will be provided to the satisfaction of the City.

#### 14.11.6.2 Site 2



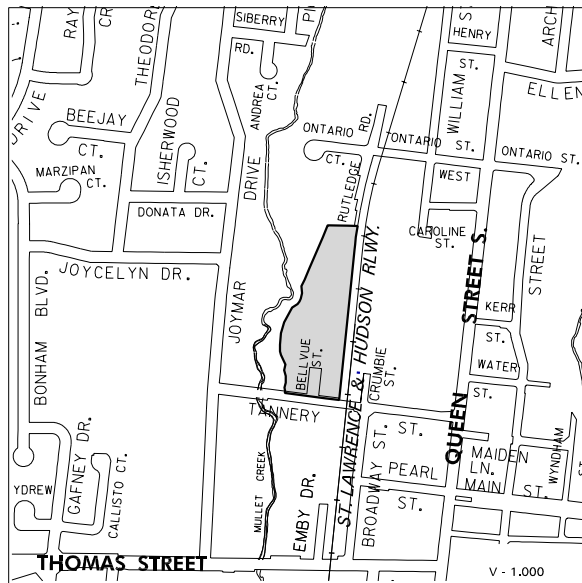
14.11.6.2.1 The lands identified as Special Site 2 are located south of Tannery Street, north of Old Station Road, east of Joymar Drive and west of the St. Lawrence and Hudson Railway.

14.11.6.2.2 Notwithstanding the provisions of the Residential High Density, Residential Medium Density and Greenbelt designations, the following additional policies will apply:

- a. the determination of the area suitable for redevelopment will have regard for the extent of the "regulatory storm" floodplain and the erosion hazards associated with Mullet Creek, whichever is greater. The extent of areas required for conservation purposes will be determined to the satisfaction of Credit Valley Conservation and the City;
- b. building forms should consist of low profile buildings ranging in height from three storeys near Mullet Creek to six storeys near the railway tracks; and
- c. redevelopment will include provisions for a community park and a multi-use recreational trail. The trail will be located adjacent to the east side of the Mullet Creek valley, from Tannery Street to the existing trail crossing Mullet Creek, south of Thomas Street. The location and size of the community park will be determined through the preparation of a parkland concept plan.

14.11.6.2.3 Redevelopment of Area A should include provision for a public road connecting Thomas Street and Tannery Street west of the St. Lawrence and Hudson Railway right-of-way.

### 14.11.6.3 Site 3

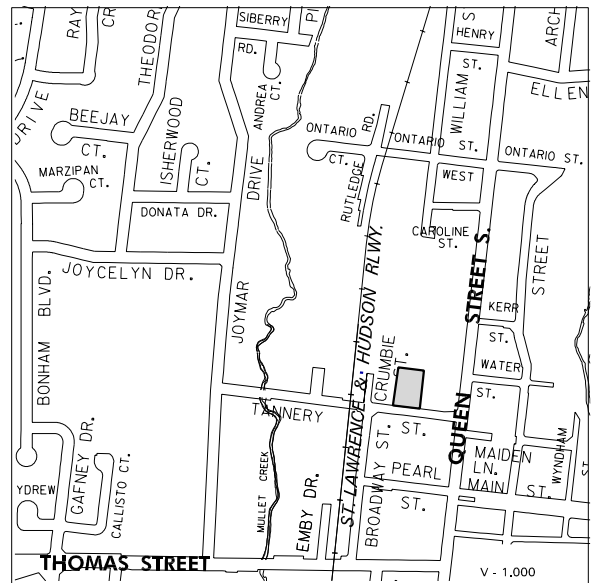


14.11.6.3.1 The lands identified as Special Site 3 are located north of Tannery Street, west of the St. Lawrence & Hudson Railway.

14.11.6.3.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:

- a. the determination of the area suitable for redevelopment will have regard for the extent of the "regulatory storm" floodplain and the erosion hazards associated with Mullet Creek, whichever is greater. The extent of areas required for conservation purposes will be determined to the satisfaction of Credit Valley Conservation and the City;
- b. redevelopment will include provision of a connecting public road between Rutledge Road and Tannery Street; and
- c. a maximum of 397 dwelling units are permitted in a building form consisting of low profile buildings ranging in height from three storeys near Mullet Creek to six storeys near the railway tracks.

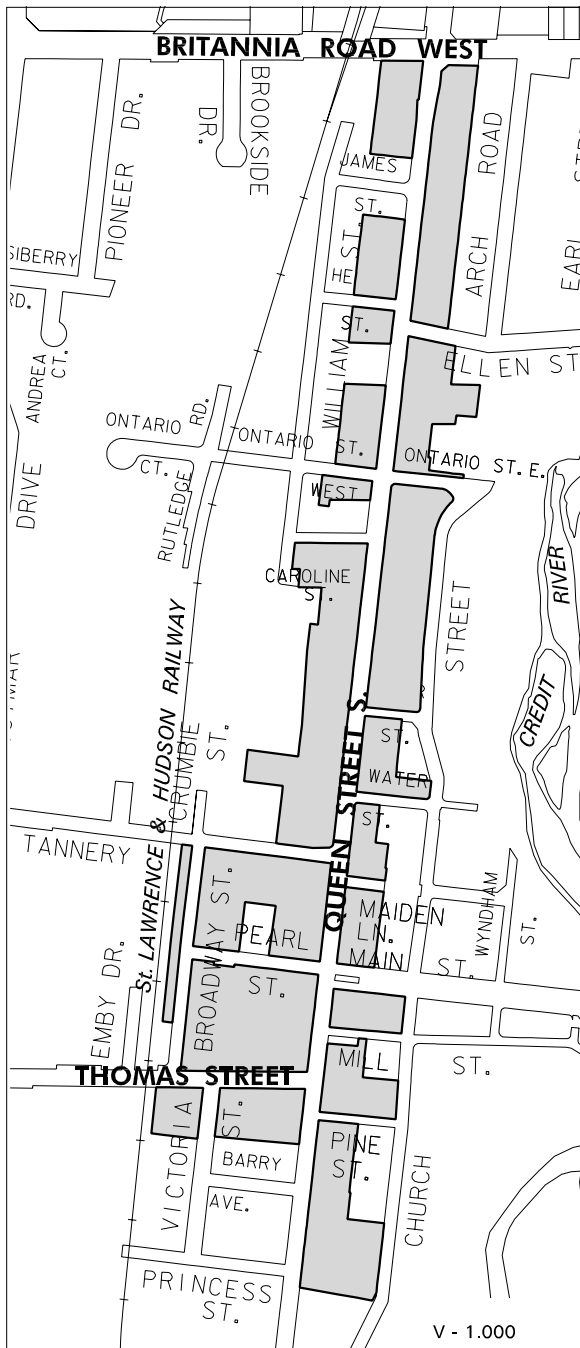
### 14.11.6.4 Site 4



14.11.6.4.1 The lands identified as Special Site 4 are located on the north side of Tannery Street, east of Crumby Street.

14.11.6.4.2 Notwithstanding the provisions of the Residential High Density designation, an apartment building with a maximum **floor space index (FSI)** of 3.1 will be permitted.

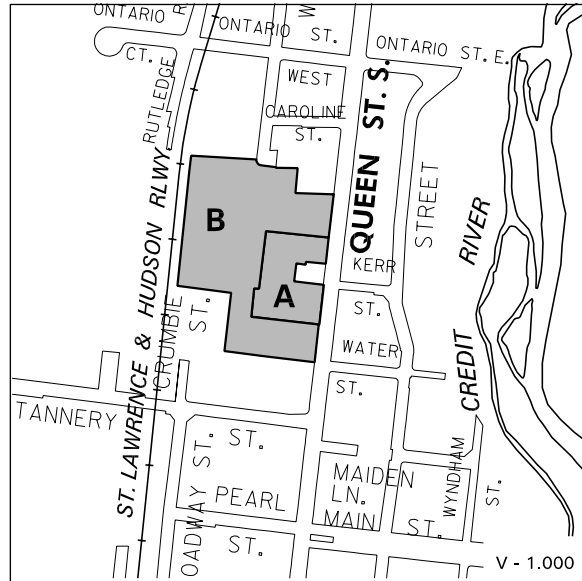
### 14.11.6.5 Site 5



14.11.6.5.1 The lands identified as Special Site 5 are located on the east and west sides of Queen Street South, the south side of Tannery Street, the east and west side of Broadway Street, the south side of Pearl Street and the north and south sides of Thomas Street.

14.11.6.5.2 Notwithstanding the provisions of the Mixed Use designations, drive-through facilities will not be permitted.

### 14.11.6.6 Site 6



14.11.6.6.1 The lands identified as Special Site 6 are located on the west side of Queen Street South, north of Tannery Street.

14.11.6.6.2 Notwithstanding the provisions of the Mixed Use designation, lands identified as Area A will be permitted to develop for a residential apartment building ranging in height from three storeys to seven storeys with ground floor commercial uses.

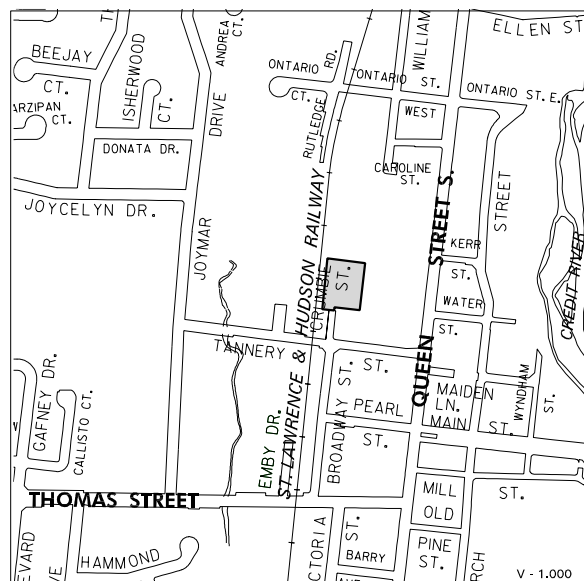
14.11.6.6.3 Prior to development of lands identified as Area B, a concept plan will be required to address among other matters:

- a connecting public road network linking Queen Street South with Crumbie Street and William Street; and
- the location of additional public open space for recreational and library purposes.



## 14.11.7 Exempt Site Policies

### 14.11.7.1 Site 1



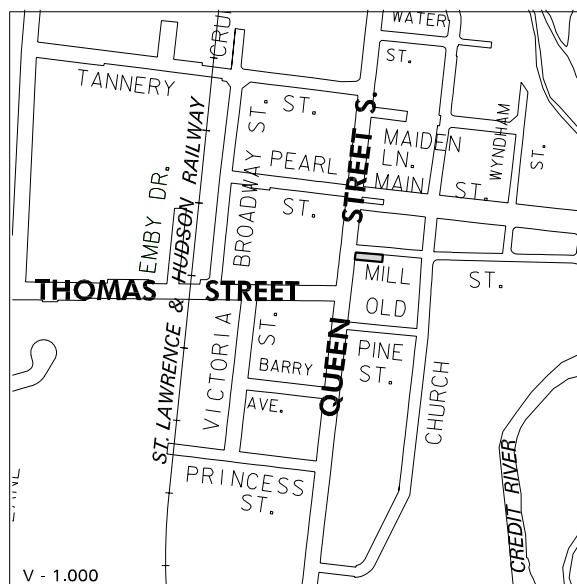
14.11.7.1.1 The lands identified as Exempt Site 1 are located east of the St. Lawrence and Hudson Railway tracks and north of Crumple Street.

14.11.7.1.2 Notwithstanding the provisions of the Mixed Use designation, motor vehicle repair facilities will also be permitted.

14.11.7.1.3 Prior to development of lands, a concept plan will be required to address among other matters:

- a. a connecting public road network linking Queen Street south with Crumple Street and William Street; and
- b. the location of additional public open space for recreational and library purposes.

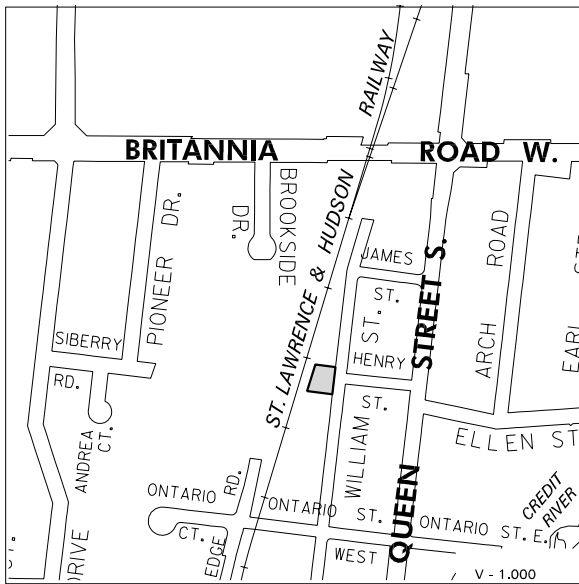
### 14.11.7.2 Site 2



14.11.7.2.1 The lands identified as Exempt Site 2 are located on the east side of Queen Street South, south of Mill Street.

14.11.7.2.2 Notwithstanding the provisions of the Mixed Use designation, motor vehicle repair facilities will also be permitted.

### 14.11.7.3 Site 3



14.11.7.3.1 The lands identified as Exempt Site 3 are located on the west side of William Street, south of James Street and are municipally known as 46 William Street.

14.11.7.3.2 Notwithstanding the provisions of the Residential Medium Density designation, outdoor storage of refrigeration trailers may be permitted on a temporary basis by a Temporary Use By-law in accordance with the provisions of the Planning Act.

# 15 Corporate Centres

## 15.1 Introduction

There are four Corporate Centres in Mississauga:

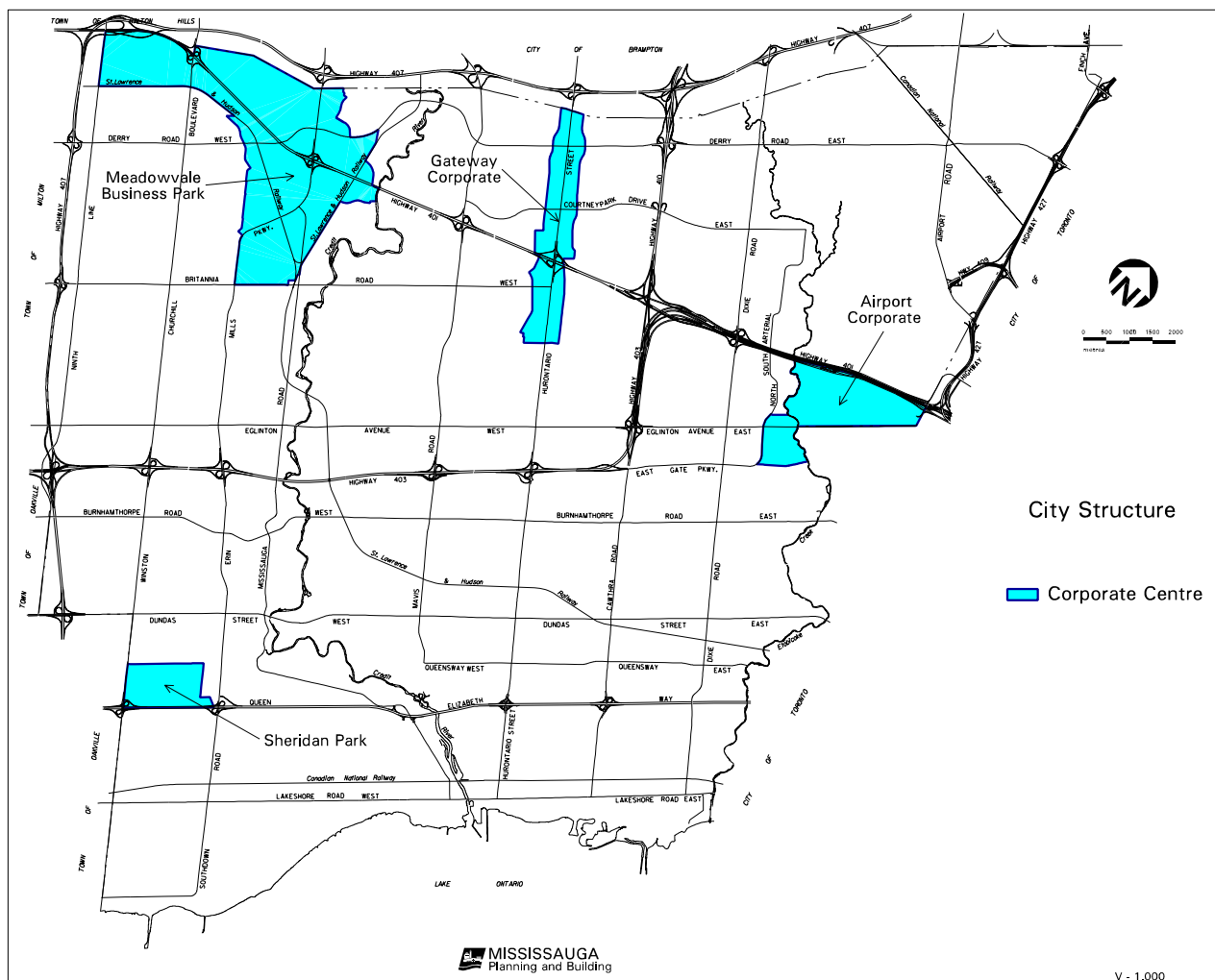
- Airport Corporate;
- Gateway Corporate;
- Meadowvale Business Park; and
- Sheridan Park.

This section identifies the modifications to the General Land Use designations in Chapter 11 that apply to all Corporate Centres.

### 15.1.1 General

15.1.1.1 Corporate Centres will develop a mix of employment uses with a focus on office development and uses with high employment densities.

15.1.1.2 Lands on a **Corridor** or within a **Major Transit Station Area** will be subject to the two storey height minimum. Local area plans or planning studies may establish maximum height requirements.



Map 15-1: City Structure – Corporate Centres

15.1.1.3 The following uses will not be permitted in freestanding buildings on a **Corridor**:

- a. financial institution;
- b. motor vehicle rental;
- c. personal service establishment;
- d. restaurant; and
- e. retail store.

15.1.1.4 Within a **Corridor** all accessory uses must be in the same building as the principal use.

## 15.1.2 Residential

15.1.2.1 Residential uses will not be permitted.

## 15.1.3 Mixed Use

15.1.3.1 Notwithstanding the Mixed Use policies of this Plan, the following additional uses will be permitted:

- a. Major offices.

15.1.3.2 Notwithstanding the Mixed Use policies of this Plan, the following uses will not be permitted:

- a. residential.

## 15.1.4 Office

15.1.4.1 Notwithstanding the Office policies of this Plan, the following additional uses will be permitted:

- a. Post-secondary educational facilities.

## 15.1.5 Institutional

15.1.5.1 Notwithstanding the Institutional policies of this Plan, residential uses will not be permitted.

## 15.1.6 Convenience Commercial

15.1.6.1 Notwithstanding the Convenience Commercial policies of this Plan, the following uses will not be permitted:

- a. Residential.

15.1.6.2 The Convenience Commercial designation will not be permitted, except for lands designated Convenience Commercial at the time this Plan comes into effect.

## 15.1.7 Motor Vehicle Commercial

15.1.7.1 The Motor Vehicle Commercial designation will not be permitted, except for lands designated Motor Vehicle Commercial at the time this Plan comes into effect.

## 15.1.8 Business Employment

15.1.8.1 Notwithstanding the Business Employment policies of this Plan, the following additional uses will be permitted:

- a. Major office; and
- b. post-secondary educational facility.

15.1.8.2 Notwithstanding the Business Employment policies of this Plan, the following uses will not be permitted:

- a. adult entertainment establishment;
- b. animal boarding establishment;
- c. body rub establishment;
- d. cardlock fuel dispensing;

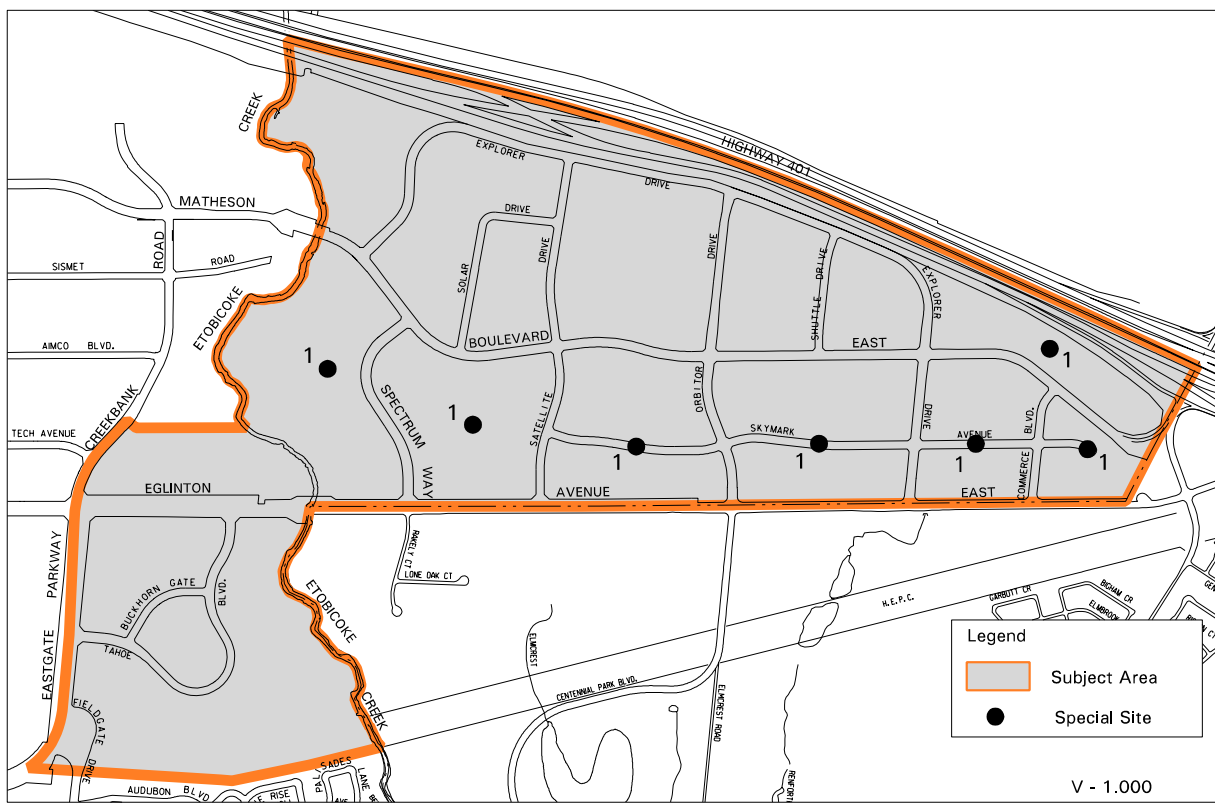


- e. composting facilities;
- f. motor vehicle body repair facility;
- g. outdoor storage and display areas related to a permitted manufacturing use;
- h. transportation facilities;
- i. trucking terminals;
- j. self storage facilities; and
- k. **waste processing stations** or **waste transfer stations**.

15.1.8.3 Notwithstanding the Business Employment policies of this Plan, **secondary offices** will not be subject to a maximum FSI.

## 15.1.9 Industrial

15.1.9.1 The Industrial designation will not be permitted, except for lands within the Meadowvale Business Park Character Area designated Industrial at the time this Plan comes into effect.



Map 15-2: Airport Corporate Centre Character Area

## 15.2 Airport Corporate

### 15.2.1 Urban Design Policies

15.2.1.1 Development of a continuous street frontage through the orientation of buildings parallel to the street and the placement of significant building mass adjacent to the street edge with transparent façades at grade will be encouraged.

15.2.1.2 Where they are related, multistorey buildings should be linked together with a podium.

15.2.1.3 Blank walls should be avoided along the street line in favour of building entrance features, fenestration, and architectural detailing.

15.2.1.4 Main building entrances should be clearly articulated and linked to pedestrian walkway systems to provide convenient access for pedestrians to public transit. Active building entrances features should be oriented to major street frontages and the BRT.

15.2.1.5 Where building side or rear elevations are exposed to roadways, these elevations should be similar to front elevations with respect to details, trim, and the orderly placement of windows, roof forms, and architectural elements.

15.2.1.6 Where continuous street planting is not possible, concentrated landscape treatment may be required.

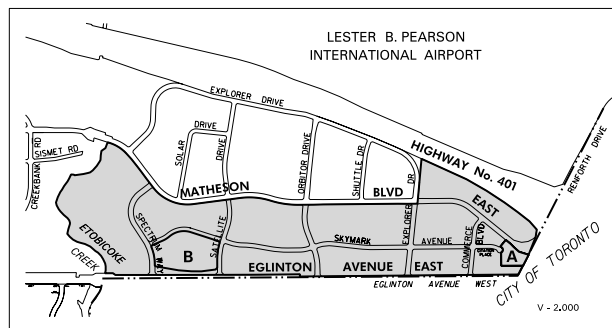
15.2.1.7 Through the site planning and development processes adequate provision should be made for

landscaping and design throughout the Character Area including the area adjacent to the Parkway Belt West and related easement north of Eglinton Avenue West, both with regard to existing uses and in the context of the ultimate use of these lands for transit purposes.

## 15.2.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 15.2.2.1 Site 1



15.2.2.1.1 The lands identified as Special Site 1 are located north of Eglinton Avenue West, south of Matheson Boulevard East, east of the Etobicoke Creek, to Explorer Drive and all lands east of Explorer Drive.

a. Notwithstanding the provisions of the Business Employment designation, the following uses will not be permitted:

- new industrial uses including manufacturing, warehousing, distributing and wholesaling, outdoor storage and display; and
- freestanding retail commercial uses and financial institutions.

However, existing industrial uses will be permitted to continue and expand.

b. Notwithstanding the Business Employment designation, the following additional policies will apply:

- buildings will be a minimum of four storeys within 500 m of the limits of Renforth BRT station;
- buildings will be a minimum of two storeys within 500 m of the limits of the Spectrum and Orbitor stations;
- development will have a minimum **floor space index (FSI)** of 0.5. In calculating **floor space index (FSI)** on large sites with multiphase development, regard may be had to the size of individual development parcels;
- prior to site plan approval, the proponent will demonstrate that sites have the ability to achieve a minimum **floor space index (FSI)** of 1.0 over time, by demonstrating the capacity of the site to accommodate additional development having regard to parking, servicing, access and landscaping;
- the provision of retail commercial uses with display windows in the at grade level is encouraged. Buildings closest to the BRT station should have active uses along most of their ground floor frontage facing public streets and/or BRT corridor;
- where it is not feasible to include retail commercial uses, the at grade level should include windows, lobbies and entrances so as to avoid blank walls facing public streets; and
- in order to achieve a continuous street wall, it is intended that as development occurs over time, a minimum of 70 percent of any lot frontage along the BRT corridor, Skymark Avenue, Commerce Boulevard and Citation Place, should be occupied with a building or buildings. In the case of lots with multiple street frontages, priority will be given to

establishing a continuous street wall along Commerce Boulevard and the BRT corridor.

c. Pedestrian Connections

Development will promote pedestrian movements to and from transit stations through the local streets and publicly accessible private pedestrian connections or private open space areas (plazas). The location, size and character of the publicly accessible connections will be determined during the site plan review process having regard for the following:

- sidewalks will be provided on both sides of all streets and form a connected system of pedestrian access to and from BRT stations;
- **streetscape** improvements will be coordinated and well designed, including trees, pedestrian scale lighting, special paving and street furniture on sidewalks, boulevards and important pedestrian and publicly accessible open space areas and walkways;
- parking areas will have appropriate landscape treatments, including trees and lighting, throughout parking lots and along their edges, in order to improve the appearance of the parking areas, to contribute to the visual continuity of the street edge. Parking areas should also incorporate defined pedestrian routes for safe and convenient pedestrian movement to building entrances and other destinations to encourage the safe use of these spaces;
- concentrated landscape treatment will be provided where continuous street planting is not possible due to the location of utilities or other constraints;
- private open space areas will be high quality, usable, and physically and visually linked to streets, park and mid-block pedestrian routes; and
- pedestrian easements will be provided through the site plan review for the achievement of a continuous pedestrian promenade adjacent to the BRT corridor.

d. Parking and Servicing

- new developments will demonstrate that they have a transportation demand management (TDM) strategy in place as a condition of site plan approval;
- prestige offices will be encouraged to provide at least one level of below-grade parking below the building;
- no parking will be provided between the building and the streetline or BRT corridor on principal building frontages. In cases of lots with multiple street frontages, priority will be given to not allowing parking along the BRT corridor and Commerce Boulevard;
- surface parking should incorporate pedestrian and environmental features including: pathways and plantings to break up large expanses of asphalt, permeable surfaces, stormwater management, clear pathways for enhanced pedestrian access, and defined future development blocks;
- loading and garbage storage areas should be located at the rear of buildings, integrated or screened from primary pedestrian routes and publicly visible areas;
- shared parking and driveways between developments will be encouraged;
- site plans will demonstrate the ability for shared servicing access with adjacent developments; and
- new development will provide secure bicycle parking for employees.

15.2.2.1.2 For the lands identified as Area 1A known municipally as 2950 Citation Place, and located at the easterly limits of Citation Place, south of Matheson Boulevard East and north of Eglinton

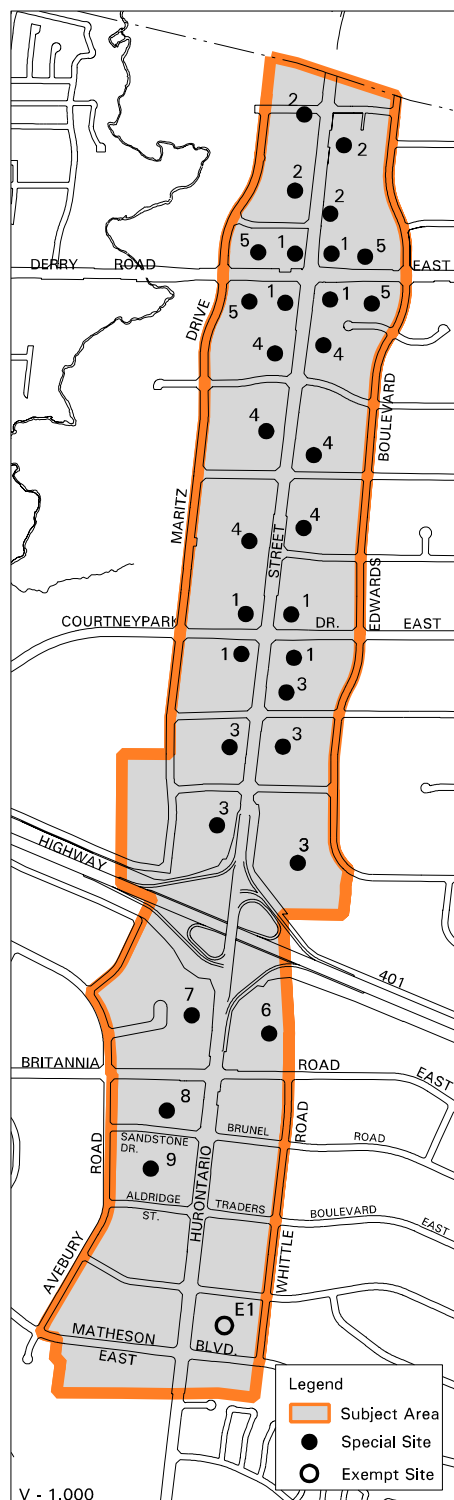


Avenue West, notwithstanding Section 15.2.2.1.1 (b), first bullet point, the City may consider through a site specific zoning amendment an initial phase consisting of a minimum two storey building on the eastern portion of the property provided:

- a. at least half of the site is reserved for a future phase(s) which consists of one or more buildings that will each be a minimum four storeys in height;
- b. the future phase(s) is located on the western portion of the property;
- c. the initial phase meets all other provisions of the Plan including a minimum **floor space index (FSI)** of 0.5 on the portion of the property proposed for the initial phase;
- d. a site specific zoning amendment and a site plan application are submitted which delineate the initial phase and the lands reserved for a future phase;
- e. the site plan application includes a plan which illustrates how the site will function through future phased development; how the individual buildings of the initial and future phase(s) will relate and integrate with one another; how the site will meet the design requirements of the Official Plan in subsequent phase(s); and how the site will achieve a **floor space index (FSI)** of 1.0 over time; and
- f. the site specific zoning amendment places a hold on the western portion of the site subject to a site plan application being submitted for that portion of the site which meets the requirements of the Official Plan and zoning by-law.

15.2.2.1.3 Notwithstanding 15.2.2.1.1.a and b, six freestanding, single storey restaurant buildings will be permitted on lands identified as B.

## 15.3 Gateway Corporate



Map 15-3: Gateway Corporate Centre Character Area

### 15.3.1 Urban Design Policies

15.3.1.1 The focus of these policies is to promote high quality urban design and built form. These policies are also intended to reinforce and enhance the image of Hurontario Street as the main north-south **Corridor** through the city.

#### Hurontario Street Corridor Development Policies

15.3.1.2 The purpose of the following urban design policies is to define principles for the physical form and character of Hurontario Street:

- encourage a high quality urban design in the built form which is distinctive and urban in character, and which contributes to the identity of Hurontario Street as a principal city thoroughfare;
- encourage a high standard of public and private realm **streetscape** design that is coordinated and comprehensive, which includes street furniture, public art, building forecourts, open space, bus shelters, tree planting, and the sensitive location of utilities;
- ensure buildings are street related with pedestrian entrances, active building elevations, and fenestration forming an integrated link between the building and the sidewalk;
- encourage the development of a unique Hurontario Street character, and enhance its image through the creation of **streetscape** design, prominent intersections, built form features, an integrated public and private realm and gateway features;
- orient the most active and architecturally detailed building façade to the public street by use of main entrances and a large percentage of fenestration addressing the **streetscape**;
- locate parking facilities at the rear and/or side of buildings instead of between the front of the building and the public street;

- g. design buildings with sufficient height, mass and width of street frontage to define and frame the street;
- h. complete the road system to improve cyclist and pedestrian movement, vehicular and servicing access, and to create usable and accessible development parcels;
- i. integrate the principal and the accessory uses within individual buildings;
- j. encourage the continued development of varied and innovative prestige buildings;
- k. encourage development that provides a safe and convenient pedestrian environment that promotes the use of Hurontario Street as a major transit corridor;
- l. minimize building setbacks from the streetline(s) while balancing continuous landscaping between the building and the street and pedestrian linkages to the public sidewalk;
- m. encourage the appropriate transition of built form between buildings;
- n. provide for safe, pleasant and convenient pedestrian movement from the public sidewalk and on-site parking areas to the principal building entrance(s);
- o. discourage the fragmentation of land parcels that will inhibit the eventual development of employment uses. Encourage land consolidation, in particular at the principal intersections to facilitate useable development parcels;
- p. priority will be given to pedestrian movement when accommodating both pedestrian and vehicular traffic. Design efficient parking facilities to avoid circuitous routes and dead end aisles;
- q. encourage built form (outside the gateway and main intersection areas) to incorporate a high level of physical continuity, cohesion and linkage

between buildings, from block to block, and from street to street;

- r. create a sense of prominence at the intersections of Hurontario Street, in addition to those subject to Special Site Policies, by integrating features such as, tall, more distinctive buildings located close to the street, unique landscape and **streetscape** treatment, elevated and distinguishing rooflines;
- s. internalize, screen and minimize visual impacts of the service and loading facilities from the **streetscape**, public view, pedestrian walkways, and abutting uses;
- t. the submission of a concept plan will be required for all development applications to demonstrate how the urban design policies will be implemented; and
- u. development applications will also have regard for the urban design guidelines in the urban design manual entitled Upper Hurontario Corridor - A design mandate for excellence.

### 15.3.2 Land Use

#### 15.3.2.1 Notwithstanding the Business Employment Policies of this Plan:

- a. single storey financial institutions and freestanding restaurants of all types which are not substantially screened from Hurontario Street by a building in place at the time of development will not be permitted on land adjacent to Hurontario Street.

15.3.2.2 Notwithstanding the above policy, existing single storey financial institutions and freestanding restaurants, which are not substantially screened from Hurontario Street by a building, will be permitted as they exist on the day these policies come into effect provided, however, that the reconstruction or alteration of these uses may be permitted if the proposal results in a visual or

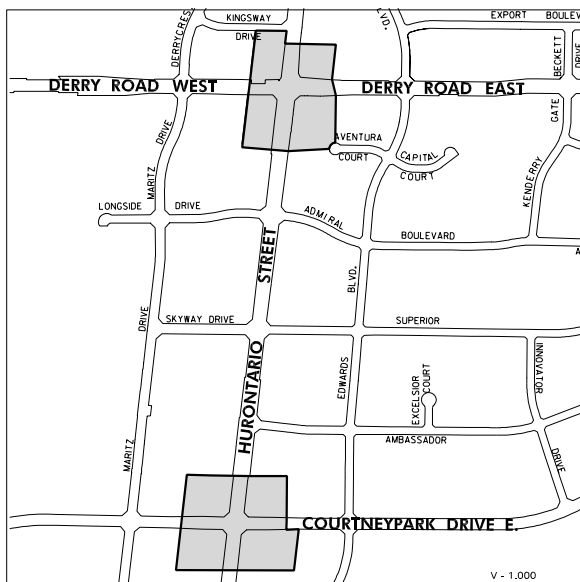
functional improvement of the site which achieves the intent and policies of the Gateway Corporate Centre Character Area Policies.

15.3.2.3 Lands designated Motor Vehicle Commercial may be developed for the permitted uses of the abutting Business Employment designations without further amendment to this Plan.

### 15.3.3 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 15.3.3.1 Site 1



15.3.3.1.1 The lands identified as Special Site 1 are located at the four corners of Hurontario Street and Derry Road East/Derry Road West, and Hurontario Street and Courtneypark Drive East/Courtneypark Drive West.

15.3.3.1.2 Notwithstanding the Business Employment designation and the Urban Design

Policies in Section 15.3.1.2 for these lands, the following additional policies will apply:

- existing motor vehicle service station/gas bar sites at the southeast and southwest corners of Derry Road East/Derry Road West and Hurontario Street are recognized, but are encouraged to be redeveloped for other permitted uses;
- expansion of the existing motor vehicle service station/gas bar sites at the southeast and southwest corners of Derry Road East/Derry Road West and Hurontario Street will be permitted. As part of the expansion of the existing gas bar at the southeast corner of Derry Road East and Hurontario Street, a car wash will also be permitted.

The reconstruction or alteration of the existing car wash at the southeast corner of Hurontario Street and Derry Road East may be permitted if the proposal results in a visual or functional improvement of the site which achieves the intent and policies of the Gateway District Policies;

- accessory commercial uses will generally be limited to a maximum of 30% of the total Gross Floor Area. Freestanding accessory commercial uses will not be permitted. Accessory commercial uses must be contained within the same building as the principal use;
- assembly of lands at the Hurontario Street/Derry Road intersection is encouraged;
- prior to development of the lands at the Hurontario Street/Derry Road intersection, an internal access concept will be prepared to the satisfaction of the Transportation and Works Department;
- these lands represent the principal intersections along the Hurontario Corridors north of

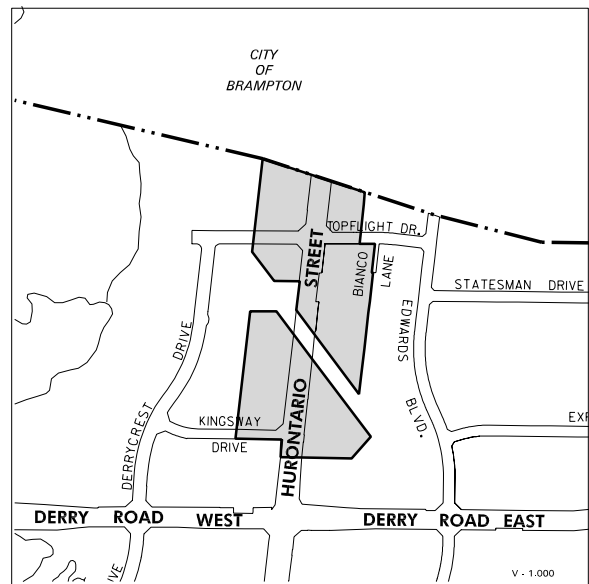


Provincial Highway 401 (Derry Road East/Derry Road West and Courtneypark Drive East/Courtneypark Drive West). Development abutting the intersections should highlight these locations as focal points within the **streetscape**, given their high profile and visibility.

In addition to the Urban Design Policies in Section 15.3.1.2, these lands will be subject to the following:

- built form at the corners of the intersections should have prominence, occupy a majority of the streetline and be a minimum of three storeys. The reconstruction of the service stations at the southeast and southwest corners of Hurontario Street and Derry Road East-West for motor vehicle commercial purposes may be permitted if it results in an improvement of the site by meeting the spirit and intent of this Plan by providing, for example, the massing, height and built form of a two storey mezzanine building; and
  - buildings with minimal frontal setbacks with active street oriented elevations, main front doors and fenestration integrated with the **streetscape**; and
- g. regard will be given to the design guidelines as outlined in the urban design manual entitled Upper Hurontario Corridor - A design mandate for excellence during the processing of development applications.

### 15.3.3.2 Site 2



15.3.3.2.1 The lands identified as Special Site 2, also known as the City Wide Gateway, are located on both sides of Hurontario Street, south of the northerly municipal boundary.

15.3.3.2.2 Notwithstanding the Business Employment designation and the Urban Design Policies in Section 15.3.1.2 for these lands, the following additional policies will apply:

- a. expansion of the existing gas bar site on the east side of Hurontario Street, north of the electric transmission lines will not be permitted;
- b. a motor vehicle service centre will be permitted on the east side of Hurontario Street south of the electric transmission lines;
- c. prior to a development proposal, the applicant will provide a concept plan demonstrating internal traffic and pedestrian circulation to the satisfaction of the City;
- d. Special Site 2 should function as the primary "gateway" into Mississauga from Brampton and areas to the north. A "gateway" should promote distinctive built form, landscaping and street furniture elements as visual landmarks to

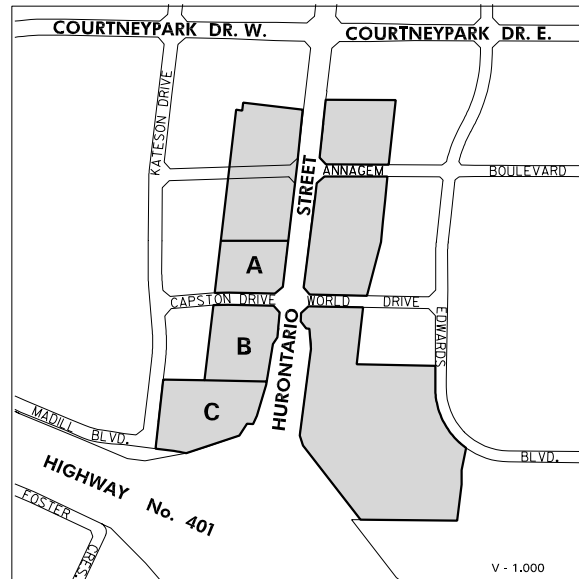
identify the City entry and reinforce a quality image.

This location is the prime opportunity to initiate a "gateway" into a civic boulevard of this calibre over the longer term. The achievement of this goal will rely on distinctive elements in both the public boulevard (i.e. feature planting, signage and decorative elements) as well as abutting development.

Built form in this location should not be seen as "background" development but should create distinctive landmarks by creative use of building massing, architectural features, higher buildings and integrated built form as a "signature" for Mississauga. Further, a transition should be provided between the highway scale of Provincial Highway 407 and the more urban scale of the street corridor through graduated change in setback, character and attention to design detail; and

- e. regard will be given to the design guidelines as outlined in the urban design manual entitled Upper Hurontario Corridor - A design mandate for excellence during the processing of development applications.

### 15.3.3.3 Site 3



15.3.3.3.1 The lands identified as Special Site 3, also known as the District Gateway, are located on both sides of Hurontario Street, north of Provincial Highway 401.

15.3.3.3.2 Notwithstanding the Business Employment designation on these lands, the following additional policies will apply:

- a. the District Gateway should provide the principal entry feature into the abutting Business Employment areas from Provincial Highway 401 and areas to the south. Development in this area should promote a quality image for this business community and reinforce its upscale image as a corporate address and destination. Opportunities for secondary landmark buildings should be promoted in order to highlight the entry point and provide orientation points. Built form should provide for a transition in scale from the broad expanses of Provincial Highway 401 to the more contained urban corridor appropriate to Hurontario Street;
- b. regard will be given to the design guidelines as outlined in the urban design manual entitled Upper Hurontario Corridor – A design mandate for excellence during the processing of development applications;

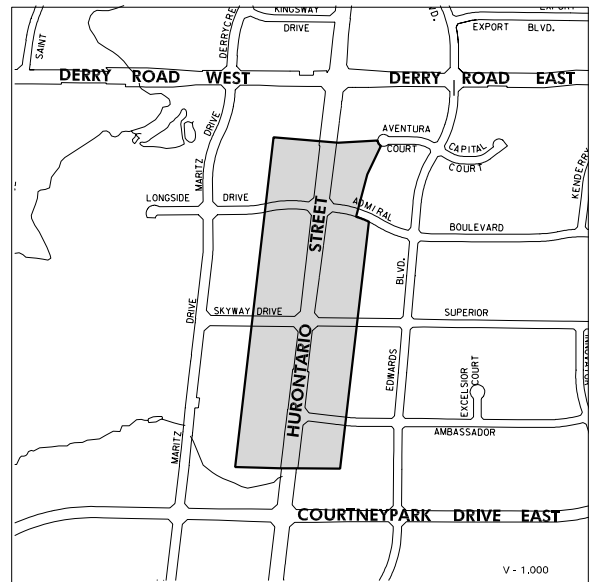
c. for the lands identified as 3A, Section 15.3.1.2 (e), Hurontario Street Corridor Development Policies shall not apply and is replaced with the following:

- the building(s) be designed with a pedestrian street entrance facing Hurontario Street; and
- a visual and functional pedestrian link be incorporated between such building entrance and the public sidewalk to encourage transit usage; and

d. for the lands identified as 3B and 3C, Sections 15.3.1.2 (e), (f) and (l), Hurontario Street Corridor Development Policies shall not apply and are replaced with the following:

- an access aisle between the building(s) and Hurontario Street, will be permitted;
- a generous landscape buffer be incorporated along the Hurontario Street frontage to screen vehicle parking areas; and
- for lands identified as 3B:
  - one row of parking between the building(s) and Hurontario Street for all permitted uses except office will be permitted;
  - the building(s) be located close to the Hurontario Street frontage on lands identified as 3B;
  - the building(s) be designed with a pedestrian street entrance facing Hurontario Street on lands identified as 3B; and
  - a visual and functional pedestrian link be incorporated between such building entrance and the public sidewalk to encourage transit usage on lands identified as 3B.

#### 15.3.3.4 Site 4



15.3.3.4.1 The lands identified as Special Site 4, also known as the urban corridor of Hurontario Street, are located on both sides of Hurontario Street, south of Derry Road East/Derry Road West.

15.3.3.4.2 Notwithstanding the Business Employment designation on these lands, the following additional policies will apply:

a. from an urban design perspective, development along the connecting corridors should establish a continuity of the urban fabric along the street and a defined "edge" and "frame" for the street volume.

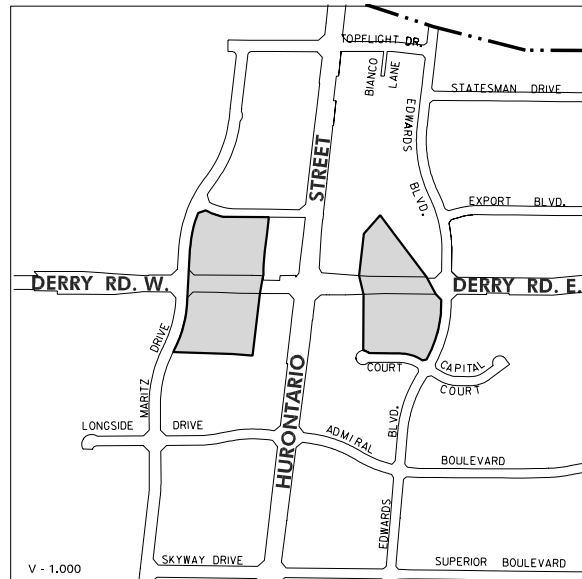
The urban corridor of Hurontario Street should provide the common denominator of built form character linking the special features outlined above within a strong overall theme. Buildings along the urban corridor should have a consistent setback, height and building street frontage. These same elements of consistency should also provide a defined scale for the street and a visual frame for the street as a foundation for a quality image; and

b. the following general principles should apply to the urban corridor of Hurontario Street:

- broader streetline setback range on development with substantial landscape area;
- substantial building coverage oriented to streetline;
- active building frontages oriented to the public street by use of pedestrian entrances and fenestration to make the building activities an integral part of the street;
- encourage consolidation of vehicular entrances;
- "background" architecture to create a unified street frame; and
- signage limited in scale and integrated with architecture (detailed guidelines have regard for Hurontario Streetscape Guidelines - south of Highway 401); and

c. regard will be given to the design guidelines as outlined in the urban design manual entitled Upper Hurontario Corridor - A design mandate for excellence during the processing of development applications.

### 15.3.3.5 Site 5



15.3.3.5.1 The lands identified as Special Site 5, also known as the urban corridor of Derry Road East/Derry Road West, are located on both sides of Derry Road East/Derry Road West, east and west of Hurontario Street.

15.3.3.5.2 Notwithstanding the Business Employment designation on these lands, the following additional policies will apply:

- from an urban design perspective, development along the connecting corridors should establish a continuity of the urban fabric along the street and a defined "edge" and "frame" for the street volume.

The urban corridor of Derry Road East/Derry Road West should provide the common denominator of built form character linking the special features outlined above within a strong overall theme. Buildings along the urban corridor should have a consistent setback, height and building street frontage;

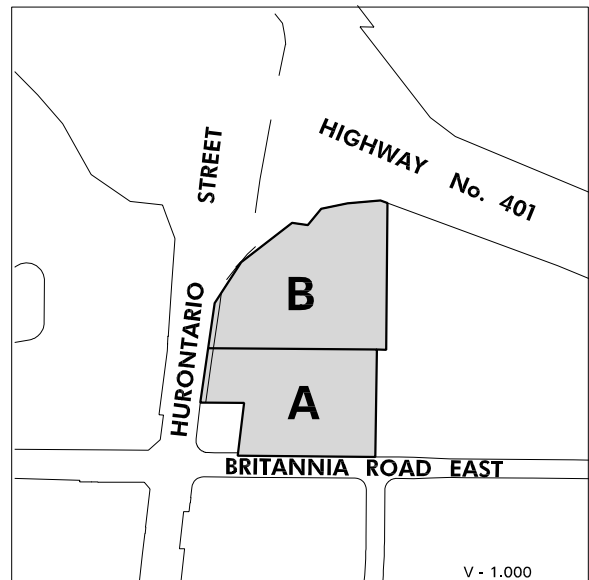
- the following general principles should apply to the urban corridor of Derry Road East/Derry Road West:



- broader streetline setback range on development with substantial landscape area;
- substantial building coverage oriented to streetline;
- active building frontages oriented to the public street by use of pedestrian entrances and fenestration to make the building activities an integral part of the street;
- encourage consolidation of vehicular entrances;
- "background" architecture to create a unified street frame; and
- signage limited in scale and integrated with architecture (detailed guidelines have regard for Hurontario Streetscape Guidelines - south of Highway 401); and

c. regard will be given to the design guidelines as outlined in the urban design manual entitled Upper Hurontario Corridor - A design mandate for excellence during processing of the development applications.

### 15.3.3.6 Site 6



15.3.3.6.1 The lands identified as Special Site 6 are located on the east side of Hurontario Street, south of Provincial Highway 401.

a. For the lands identified as 6A, Section 15.3.1.2(e), Hurontario Street Corridor Development Policies shall not apply and is replaced with the following:

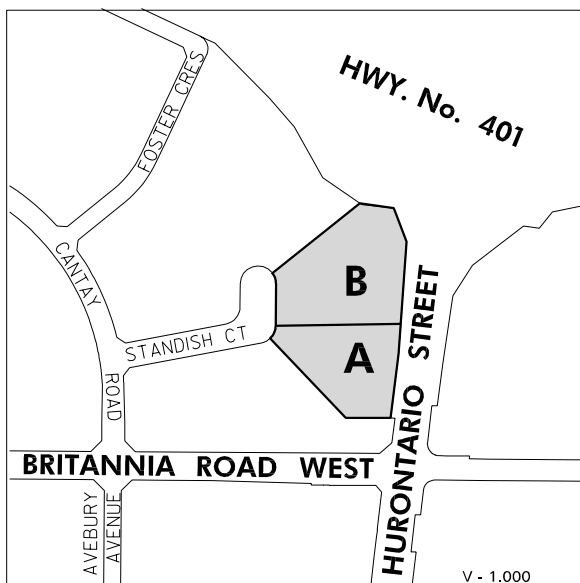
- the building(s) be designed with a pedestrian street entrance facing Hurontario Street; and
- a visual and functional pedestrian link be incorporated between such building entrance and the public sidewalk to encourage transit usage; and

b. For the lands identified as 6B, Section 15.3.1.2(e), (f) and (l), Hurontario Street Corridor Development Policies shall not apply and are replaced with the following:

- two rows of parking between the buildings(s) and Hurontario Street, will be permitted;
- an access aisle between the building(s) and Hurontario Street, will be permitted; and

- a generous landscape buffer be incorporated along the Hurontario Street frontage to screen vehicle parking areas.

#### 15.3.3.7 Site 7



15.3.3.7.1 The lands identified as Special Site 7 are located on the west side of Hurontario Street, south of Provincial Highway 401.

- For the lands identified as 7A, Section 15.3.1.2(e), Hurontario Street Corridor Development Policies shall not apply and is replaced with the following:
  - the building(s) be designed with a pedestrian street entrance facing Hurontario Street; and
  - a visual and functional pedestrian link be incorporated between such building entrance and the public sidewalk to encourage transit usage; and
- For the lands identified as 7B, Section 15.3.1.2(e), (f) and (l), Hurontario Street Corridor Development Policies shall not apply and are replaced with the following:

- an access aisle between the building(s) and Hurontario Street, will be permitted; and
- a generous landscape buffer be incorporated along the Hurontario Street frontage to screen vehicle parking areas.

#### 15.3.3.8 Site 8

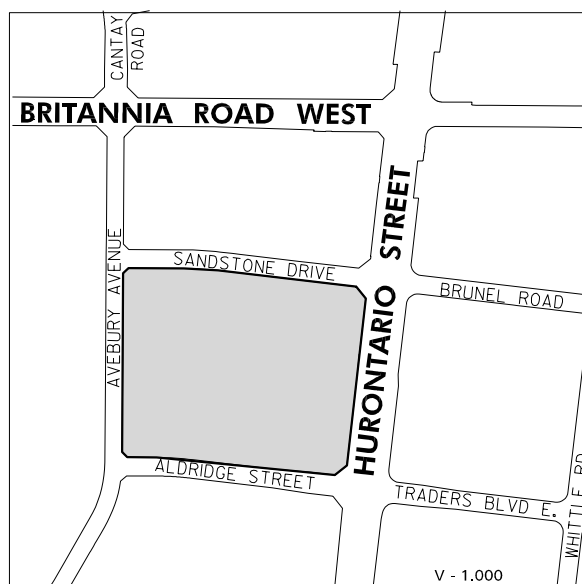


15.3.3.8.1 The lands identified as Special Site 8 are located at the northwest corner of Sandstone Drive and Hurontario Street.

- Section 15.3.1.2(e), Hurontario Street Corridor Development Policies shall not apply and is replaced with the following:
  - the building(s) be designed with a pedestrian street entrance facing Hurontario Street; and
  - a visual and functional pedestrian link be incorporated between such building entrance and the public sidewalk to encourage transit usage.

### 15.3.3.9

### Site 9



15.3.3.9.1 The lands identified as Special Site 9 are located at the southwest corner of Sandstone Drive and Hurontario Street.

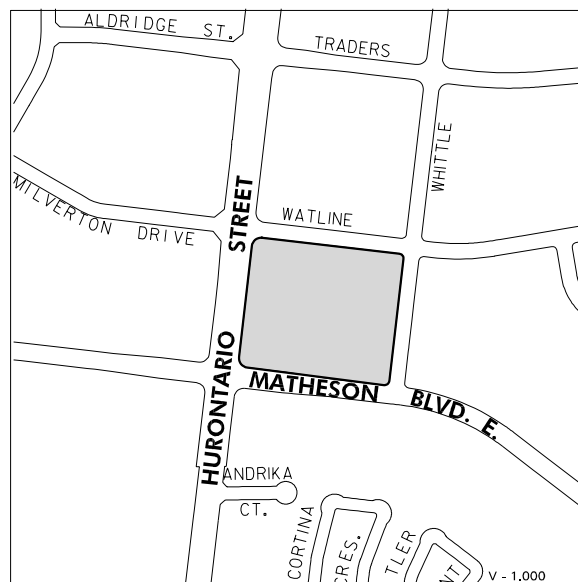
- a. Section 15.3.1.2(e), Hurontario Street Corridor Development Policies shall not apply and is replaced with the following:
  - the building(s) be designed with a pedestrian street entrance facing Hurontario Street; and
  - a visual and functional pedestrian link be incorporated between such building entrance and the public sidewalk to encourage transit usage; and
- b. Section 15.3.1.2(f) and (l), Hurontario Street Corridor Development Policies shall not apply if the existing building is expanded.

### 15.3.4

### Exempt Sites

#### 15.3.4.1

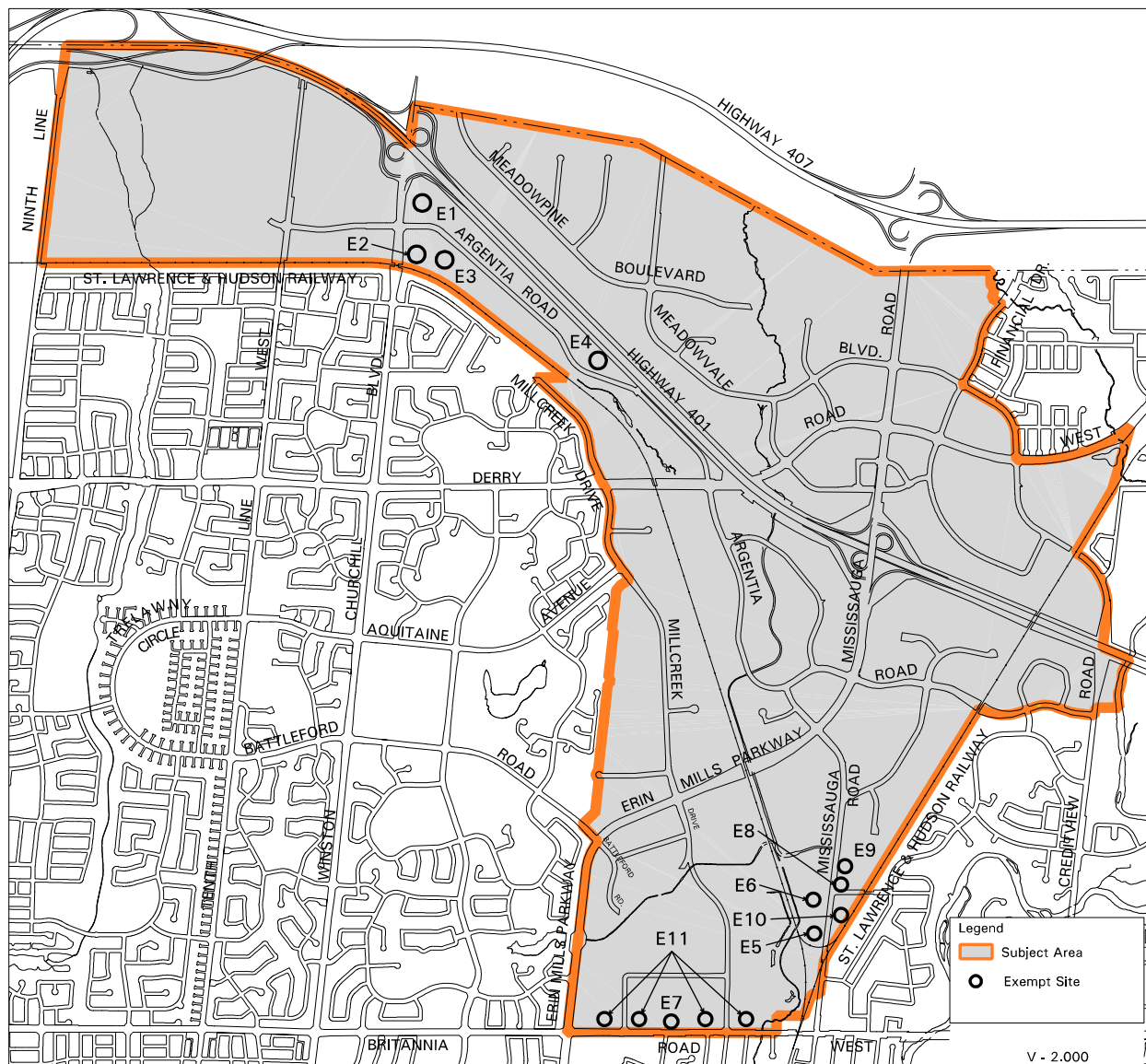
#### Site 1



15.3.4.1.1 The lands identified as Exempt Site 1 are bounded by Matheson Boulevard East, Hurontario Street, Watline Avenue and Whittle Road.

15.3.4.1.2 Notwithstanding the provisions of the Business Employment designation, commercial uses will also be permitted

## 15.4 Meadowvale Business Park



Map 15-4: Meadowvale Business Park Corporate Centre Character Area

### 15.4.1 Urban Design Policies

15.4.1.1 To achieve a high standard of building design, landscape and **streetscape**, the following design policies will be used to evaluate the design aspects of development proposals:

- a. existing natural features, particularly significant forested areas, the Mullet Creek valley and tributaries of the Credit River will be recognized

as opportunities for urban design and will be incorporated with development in a manner which will not impair their visual or ecological integrity;

- b. by taking advantage of special building forms on lands proposed for office and commercial uses, interesting and varied entrance corridors will be created; and



- c. in order to firmly establish the prestige image of the area, special attention will be given to site planning, building design and landscaping along arterial and major collector roads, at major road intersections and at other highly visible locations.

15.4.1.2 Business Employment lands which are located directly adjacent to residential lands will require acoustical barriers and landscape buffers in order to achieve adequate noise attenuation, distance separation, and landscaping between the land uses. The sensitive residential land uses must be buffered from the loading, service and parking areas associated with the Business Employment lands.

15.4.1.3 Proposals for Business Employment and Industrial uses adjacent to residential will be designed to minimize overlook conditions, be compatible with adjacent grading conditions and minimize overshadowing to any adjacent properties, and will be subject to special design controls in order to ensure upgraded building elevations and landscaped buffers.

## 15.4.2 Land Use

15.4.2.1 Notwithstanding the Business Employment policies of this Plan, the following additional uses will be permitted, except within the restricted areas identified on Figure 15.4-1: Meadowvale Business Park Restricted Areas:

- a. adult entertainment establishments;
- b. animal boarding establishments which may include outdoor facilities;
- c. body rub establishments;
- d. cardlock fuel dispensing;
- e. Motor Vehicle Commercial uses;
- f. motor vehicle rental facilities;
- g. outdoor storage and display areas related to a permitted use;
- h. self storage facilities;
- i. transportation facilities;
- j. trucking terminals; and
- k. **waste processing station** or **waste transfer stations** and composting facilities.

15.4.2.2 Outdoor storage adjacent to Provincial Highway 401, a **Corridor**, park, greenbelt or residential area will not be permitted.

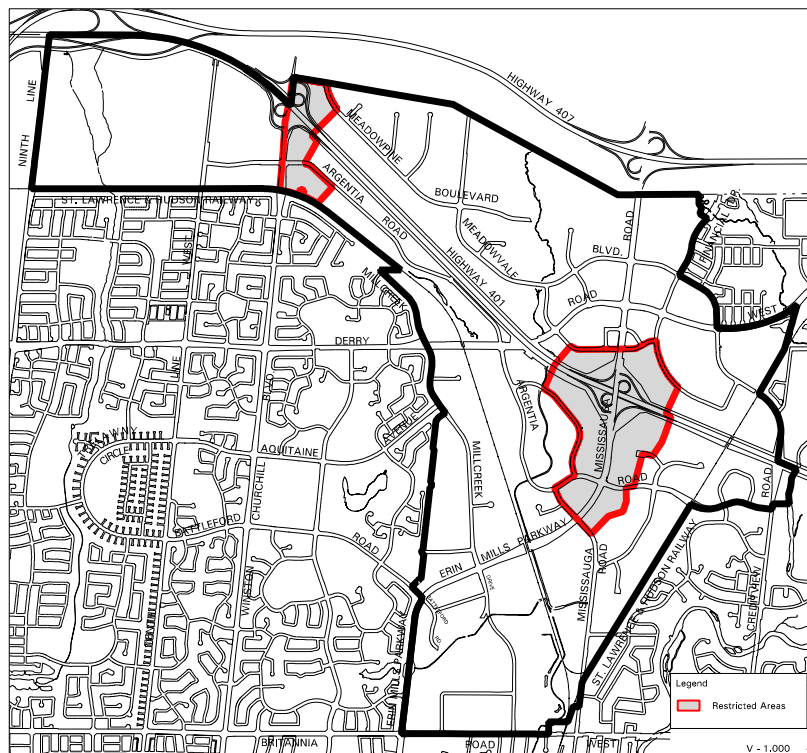
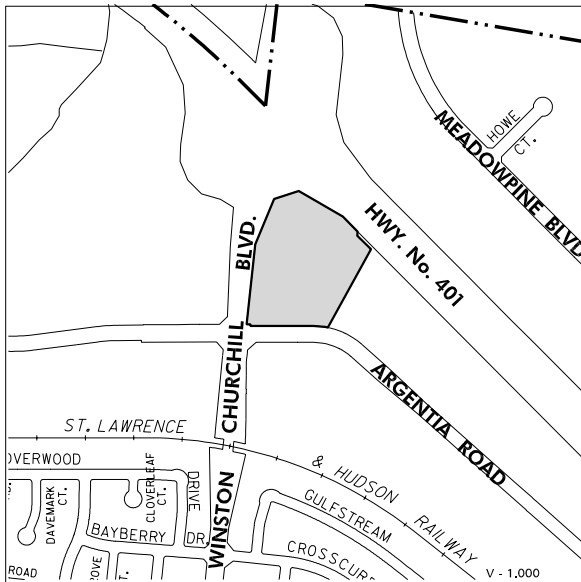


Figure 15-4.1: Meadowvale Business Park Restricted Areas

## 15.4.3 Exempt Sites

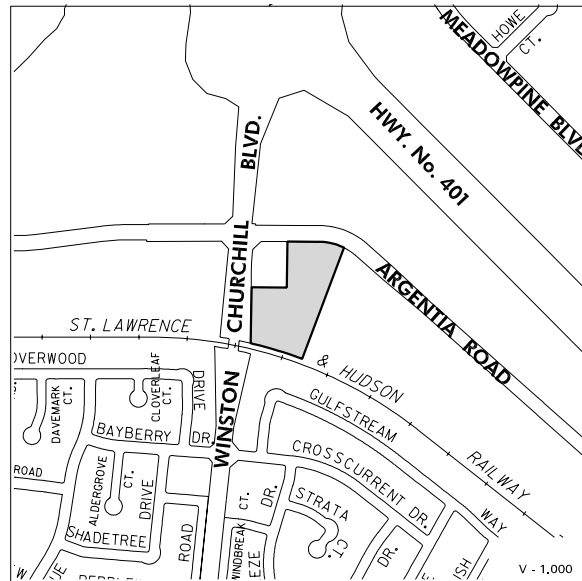
### 15.4.3.1 Site 1



15.4.3.1.1 The lands identified as Exempt Site 1 are located east of Winston Churchill Boulevard, south of Provincial Highway 401 and north of Argenta Road.

15.4.3.1.2 Notwithstanding the provisions of the Business Employment designation, all forms of general commercial uses will be permitted, except motor vehicle commercial uses.

### 15.4.3.2 Site 2

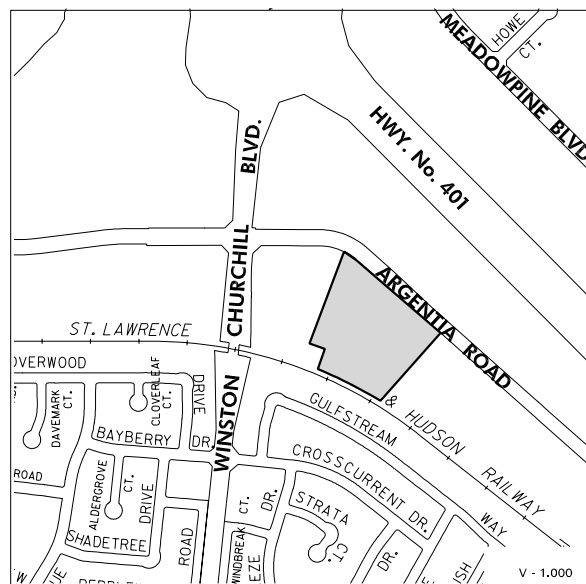


15.4.3.2.1 The lands identified as Exempt Site 2 are located east of Winston Churchill Boulevard, south of Argenta Road and north of the St. Lawrence and Hudson Railway line.

15.4.3.2.2 Notwithstanding the Business Employment and Utility designations, office gross floor area (GFA) will be limited to a maximum of 55 208 m<sup>2</sup>. An automobile dealership will also be permitted.

### 15.4.3.3

### Site 3

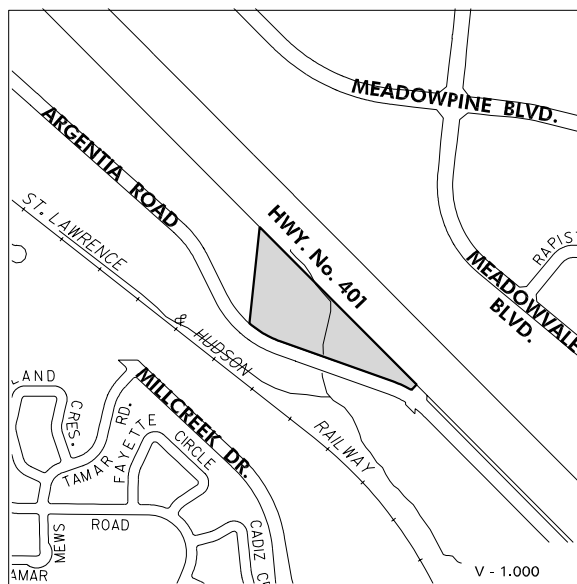


15.4.3.3.1 The lands identified as Exempt Site 3 are located south of Argentia Road and east of Winston Churchill Boulevard.

15.4.3.3.2 Notwithstanding the provisions of the Business Employment designation, a retail warehouse, and accessory outdoor storage and display areas, will also be permitted.

### 15.4.3.4

### Site 4

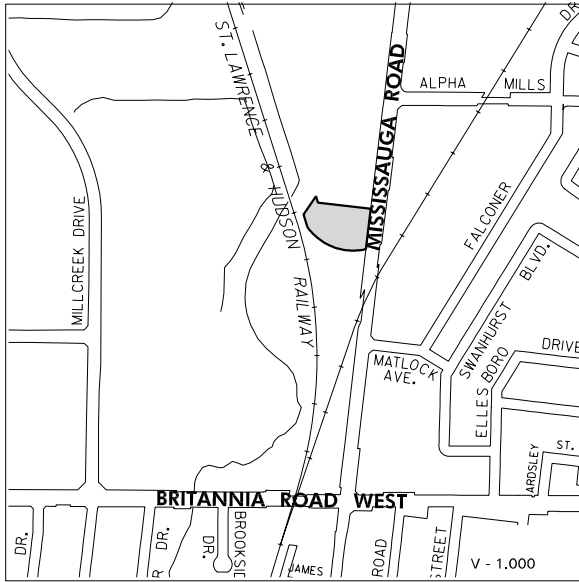


15.4.3.4.1 The lands identified as Exempt Site 4 are located north of Argentia Road and west of Fifth Line West.

15.4.3.4.2 Notwithstanding the provisions of the Motor Vehicle Commercial designation, all types of restaurants will also be permitted.

#### 15.4.3.5

#### Site 5

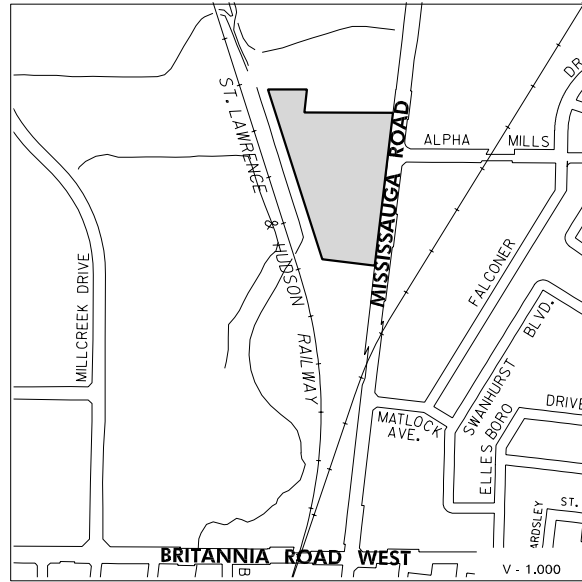


15.4.3.5.1 The lands identified as Exempt Site 5 are located on the west side of Mississauga Road, north of the St. Lawrence and Hudson Railway.

15.4.3.5.2 Notwithstanding the provisions of the Business Employment designation, a motor vehicle sales and equipment outlet will also be permitted.

#### 15.4.3.6

#### Site 6



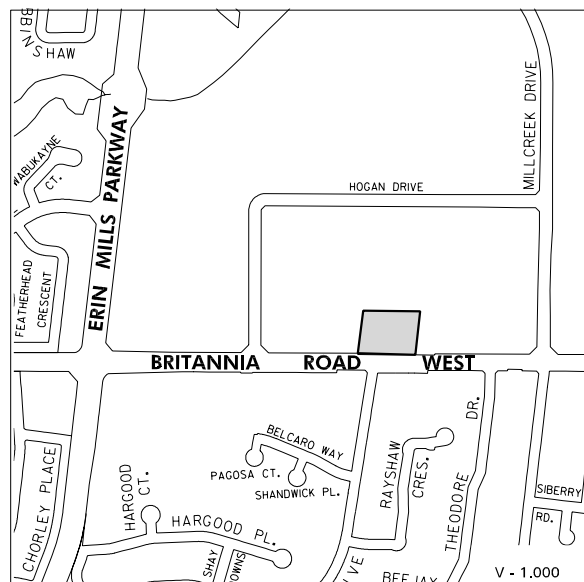
15.4.3.6.1 The lands identified as Exempt Site 6 are located on the west side of Mississauga Road, north of the St. Lawrence and Hudson Railway, opposite Alpha Mills Road.

15.4.3.6.2 Notwithstanding the provisions of the Business Employment designation, a retail warehouse, a bulk frozen food store, a bulk food store, a supermarket and a discount merchandising store will also be permitted.



### 15.4.3.7

### Site 7



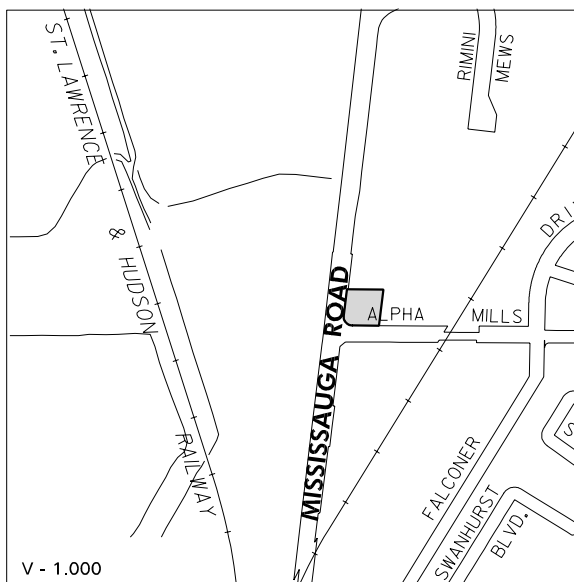
15.4.3.7.1 The lands identified as Exempt Site 7 are located on the north side of Britannia Road West, east of Erin Mills Parkway, opposite Turney Drive.

15.4.3.7.2 Notwithstanding the provisions of the Business Employment designation, the following additional policies will apply:

- a. a motor vehicle sales establishment will also be permitted;
- b. general commercial uses will be permitted to a maximum of 35% of the total gross floor area; and
- c. the following uses will not be permitted:
  - animal boarding establishments;
  - body rub and adult entertainment establishments;
  - cardlock fuel dispensing facilities;
  - transportation facilities;
  - trucking terminals; and
  - **waste processing stations** or **waste transfer stations** and composting facilities.

### 15.4.3.8

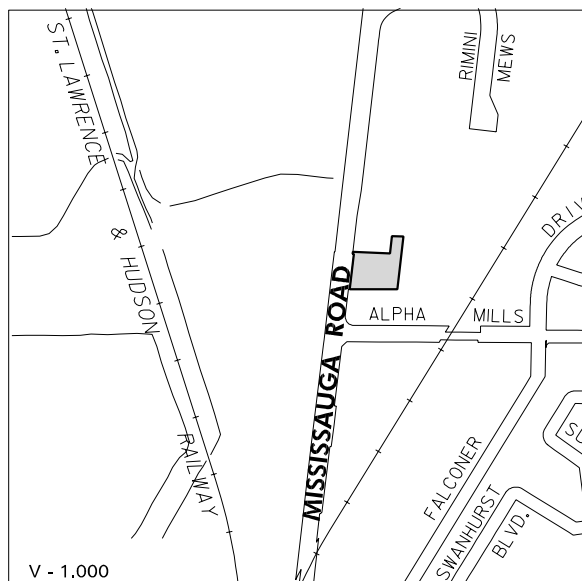
### Site 8



15.4.3.8.1 The lands identified as Exempt Site 8 are located at the northeast corner of Alpha Mills Road and Mississauga Road.

15.4.3.8.2 Notwithstanding the provisions of the Business Employment designation, a retail commercial use will also be permitted.

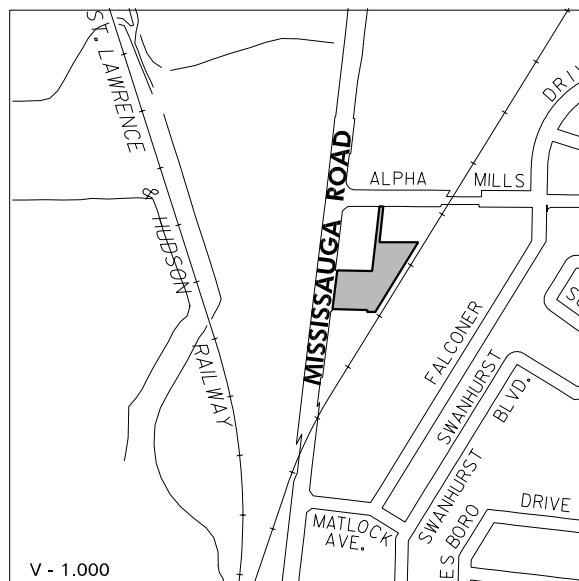
#### 15.4.3.9 Site 9



15.4.3.9.1 The lands identified as Exempt Site 9 are located on the east side of Mississauga Road, north of Alpha Mills Road.

15.4.3.9.2 Notwithstanding the provisions of the Business Employment designation, motor vehicle sales will also be permitted.

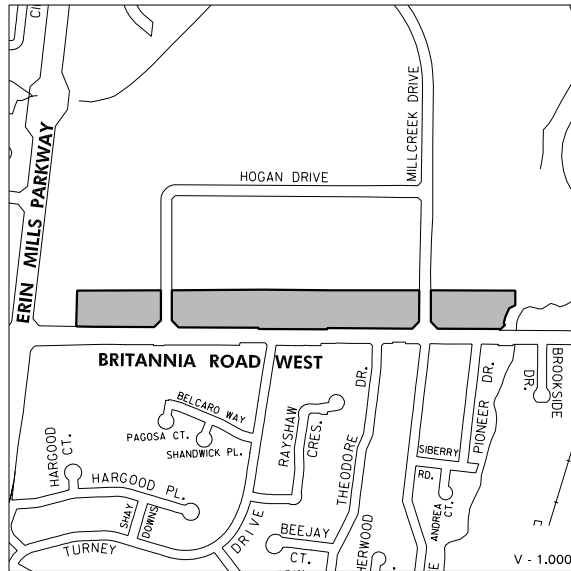
#### 15.4.3.10 Site 10



15.4.3.10.1 The lands identified as Exempt Site 10 are located on the east side of Mississauga Road, south of Alpha Mills Road.

15.4.3.10.2 Notwithstanding the provisions of the Business Employment designation, motor vehicle sales will also be permitted.

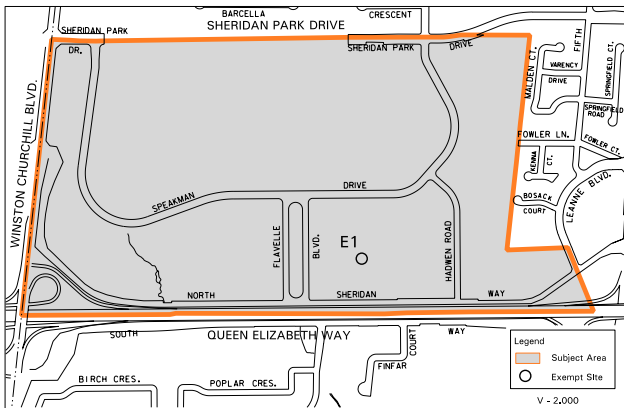
### 15.4.3.11 Site 11



15.4.3.11.1 The lands identified as Exempt Site 11 are located on the north side of Britannia Road West, east of Erin Mills Parkway.

15.4.3.11.2 Notwithstanding the policies of this Plan, one storey buildings will be permitted.

## 15.5 Sheridan Park



Map 15-5: Sheridan Park Corporate Centre Character Area

### 15.5.1 Urban Design Policies

15.5.1.1 The campus setting development is characterized by a highly developed landscape plan to enhance the building design and siting. Landscaping is an important architectural element of the overall development, thus requiring buildings to be sited on large lots with generous setbacks from streetlines to maximize landscaping opportunities. The integration of buildings through public pathways and open spaces will further strengthen this development image. Towards the achievement of City urban design objectives, development proposals should address the following:

- the enhancement of views to existing visually important natural and built features;
- the development of building forms which are sensitive to the existing character of the Character Area;
- the maintenance and enhancement of visual and physical links to the City at large, ensuring integration of the Character Area into its broader context;
- avoiding reverse frontage lots which contribute negatively to the system of public streets; and
- loading and service areas which are not located adjacent to streets and are not exposed to public areas.

### Community Identity and Design

15.5.1.2 The campus setting development image will be reinforced through appropriate standards for the siting of buildings, building heights, parking and loading spaces, site access, lighting, signage, screening and landscaping. These will be established during the rezoning and site plan approval process and should include enhanced opportunities for natural surveillance, natural access control and territorial reinforcement of the site. Special care will be exercised in the determination of lot size and building coverage in order to attain an acceptable and compatible appearance of development and ensure the preservation and integration of existing natural features and their ecological functions.

### Buildings and Spaces

15.5.1.3 To achieve and enhance the campus setting development image, the following design guidelines will be used to evaluate the design aspects of development proposals:

- buildings and structures will be sited and designed with generous setbacks from streetlines to maximize open space/landscaped areas. Vistas to the buildings may be created through the strategic location of landscape features;
- building designs are characterized by ceremonial approach features with well defined front entrances, for example a row of trees and turning circle for passenger drop-off;
- the preservation and integration of natural features such as woodlands into future development, is a priority; alterations to the existing topography, natural drainage system, and vegetation are to be minimized;
- landscaping and planting for a campus setting should incorporate the following:
  - water features, for example fountains, ponds;
  - ceremonial planting schemes;



- tree planting to define the street edge;
  - interconnecting pathways and open spaces between buildings for public and/or semi-private use;
  - define pedestrian and vehicular routes, and accent entrance ways;
  - provide year round shelter and enhancement to outdoor pedestrian areas;
  - provide summer shade and protection from winter winds; and
  - create clear visual and spatial distinction between publicly accessible and private open space; and
- e. the design of parking and service areas will be integrated with the landscape plan for the site with planting and berms to screen parking from the street. Large expanses of surface parking are to be softened by landscaped islands;
- f. visual and functional relationships between individual buildings and groups of buildings, the promotion of an open space system within the business park, and the relationship of buildings to adjacent streets will be important considerations in evaluating satisfactory design;
- g. the creation of an identifiable street edge will be encouraged on lands adjacent to, and visible from Winston Churchill Boulevard; and
- h. a high standard of building design should have regard for context, level of detail, modulation of façade and consistency of design content.
- laboratories, pilot plants and prototype production facilities;
- b. education and training facilities, but excluding a public school or private school used for elementary or secondary level education and training;
- c. data processing centres;
- d. engineering services;
- e. offices associated with science and technology uses;
- f. hotels; and
- g. accessory commercial uses, namely, conference facilities, fitness facilities, banks and restaurants within buildings provided they do not exceed 15% of the overall floor space.

15.5.2.2 Notwithstanding the above, a private school used for elementary and secondary level education and training is permitted on the lands described as Part 2 on Reference Plan 43R-25302 at 2300 Speakman Drive.

15.5.2.3 Development will be subject to the following policies:

- a. uses will not exceed a ***floor space index (FSI)*** of 0.40;
- b. operations must be carried out within enclosed buildings and structures; and
- c. private landowners will be encouraged to provide opportunities for active and passive forms of outdoor recreation.

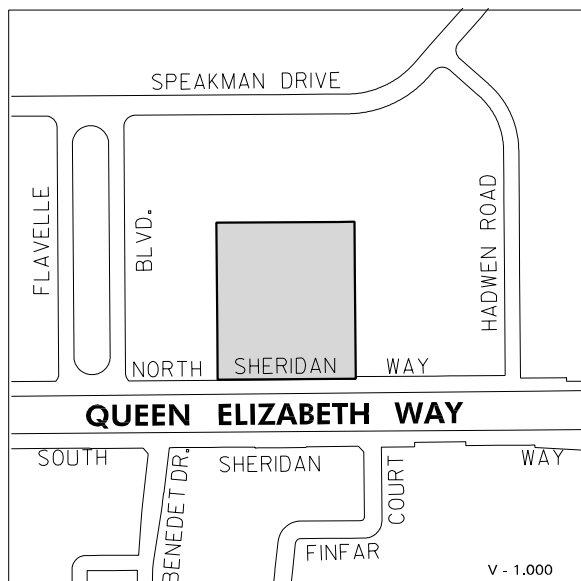
## 15.5.2 Land Use

15.5.2.1 Notwithstanding the Business Employment policies of this Plan, lands designated Business Employment will only be used for the following uses:

- a. facilities involved with scientific and engineering research and development, including:

## 15.5.3 Exempt Site Policies

### 15.5.3.1 Site 1



15.5.3.1.1 The lands identified as Exempt Site 1 are located on the north side of North Sheridan Way, east of Flavelle Boulevard and west of Hadwen Road and are municipally known as 2333 North Sheridan Way.

15.5.3.1.2 Notwithstanding the Business Employment Policies of this Plan, the following additional uses will be permitted:

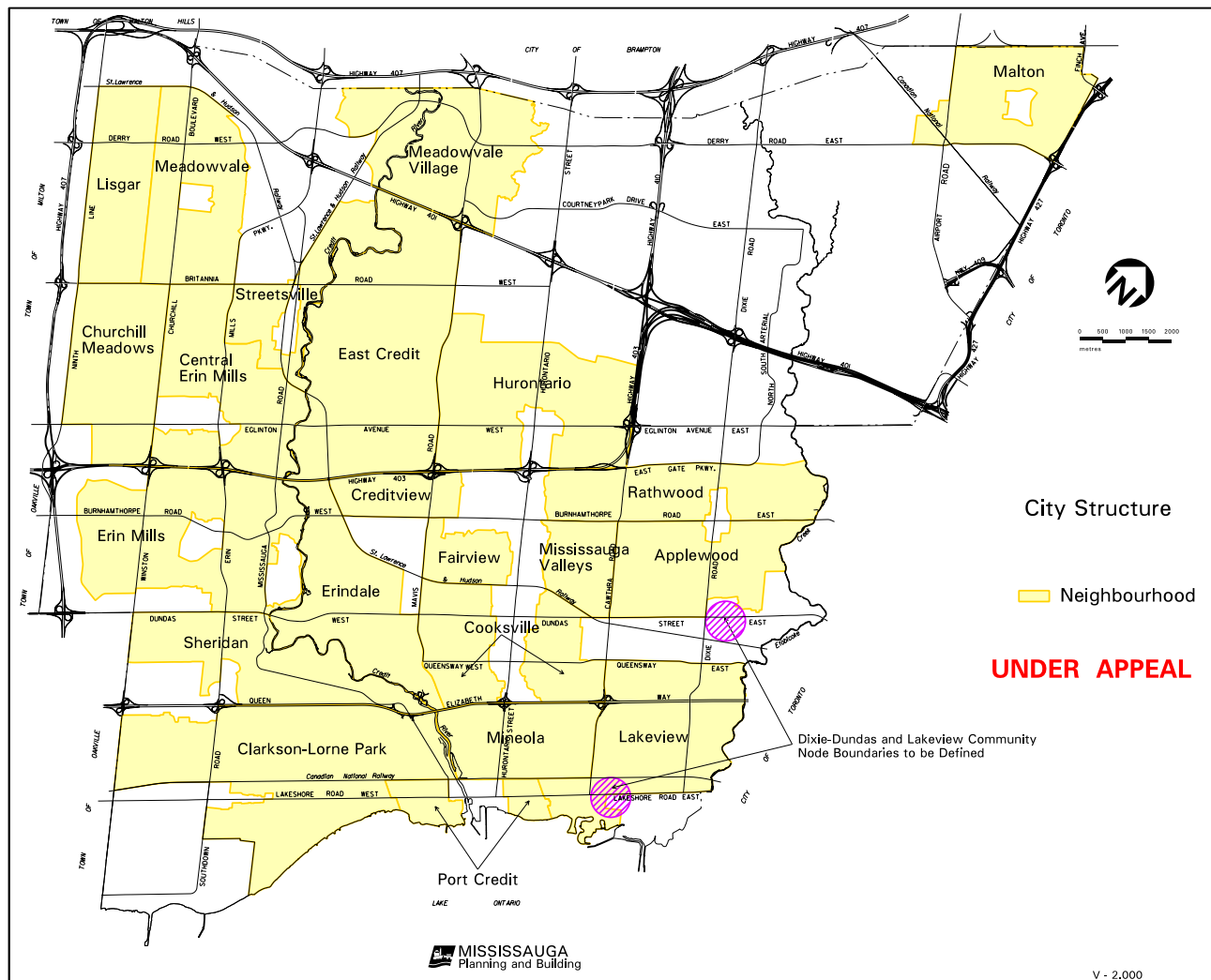
- a. industrial uses within enclosed buildings including manufacturing, warehousing, distributing and wholesaling;
- b. office and accessory uses within industrial buildings or in separate buildings within a complex of associated industrial buildings; and
- c. limited outdoor storage areas accessory to an existing permitted use provided they are screened from public view.

# 16 Neighbourhoods

## 16.1 Introduction

There are 22 Neighbourhoods in Mississauga:

- Applewood;
- Central Erin Mills;
- Churchill Meadows;
- Clarkson-Lorne Park;
- Cooksville;
- Creditview;
- East Credit;
- Erindale;
- Erin Mills;
- Fairview;
- Hurontario;
- Lakeview;
- Lisgar;
- Malton;
- Meadowvale;



Map 16-1: City Structure - Neighbourhoods

- Meadowvale Village;
- Mineola;
- Mississauga Valleys;
- Port Credit;
- Rathwood;
- Sheridan; and
- Streetsville.

This section identifies the modifications to the General Land Use designations in Chapter 11 that apply to all Neighbourhoods.

### 16.1.1 General

16.1.1.1 For lands within a Neighbourhood, a maximum building height of four storeys will apply unless Character Area policies specify alternative building height requirements or until such time as alternative building heights are determined through the review of Character Area policies.

16.1.1.2 Proposals for heights more than four storeys or different than established in the Character Area policies, will only be considered where it can be demonstrated to the City's satisfaction, that:

- an appropriate transition in heights that respects the surrounding context will be achieved;
- the development proposal enhances the existing or planned development;
- the City Structure hierarchy is maintained; and
- the development proposal is consistent with the policies of this Plan.

### 16.1.2 Residential

16.1.2.1 To preserve the character of lands designated Residential Low Density I and

Residential Low Density II, the minimum frontage and area of new lots proposed along the periphery of a draft plan of subdivision, or which are subject to a consent application, will generally represent the greater of:

- the average lot frontage and lot area of residential lots on both sides of the same street within 120 m of the subject property. In the case of a corner lot, lots on both streets within 120 m will be considered;

or

- the requirements of the Zoning By-law.

16.1.2.2 Notwithstanding 16.1.2.1, where the average lot frontage or lot area of residential lots determined pursuant to 16.1.2.1.a is less than the minimum requirements of the zoning by-law, consideration may be given to a minor variance.

16.1.2.3 Where the Residential Medium Density policies of this Plan permit low-rise apartment dwellings, they will be encouraged to locate on **Corridors**.

16.1.2.4 Existing detached and semi-detached dwellings on lands designated Residential Medium Density are deemed to conform to the Residential Medium Density designation and policies of this Plan.

16.1.2.5 Proposals for additional development on lands with existing apartment buildings will be subject to the following, in addition to other policies regarding medium and high density residential development in this Plan:

- on lands designated Residential High Density, development in addition to existing buildings will be restricted to uses permitted in the Residential Medium Density designation; and
- as a condition of development, the site in its entirety must meet current site plan and landscaping requirements, and existing buildings must meet current building code, fire code and property standards.



### 16.1.3 Office

16.1.3.1 Notwithstanding the Office policies of this Plan, the following use will not be permitted:

- a. Major office.

16.1.3.2 Notwithstanding the office policies of this Plan, the maximum ***floor space index (FSI)*** for ***secondary office*** is 0.5.

### 16.1.4 Institutional

16.1.4.1 The Institutional designation will not be permitted, except for lands designated Institutional at the time this Plan comes into effect.

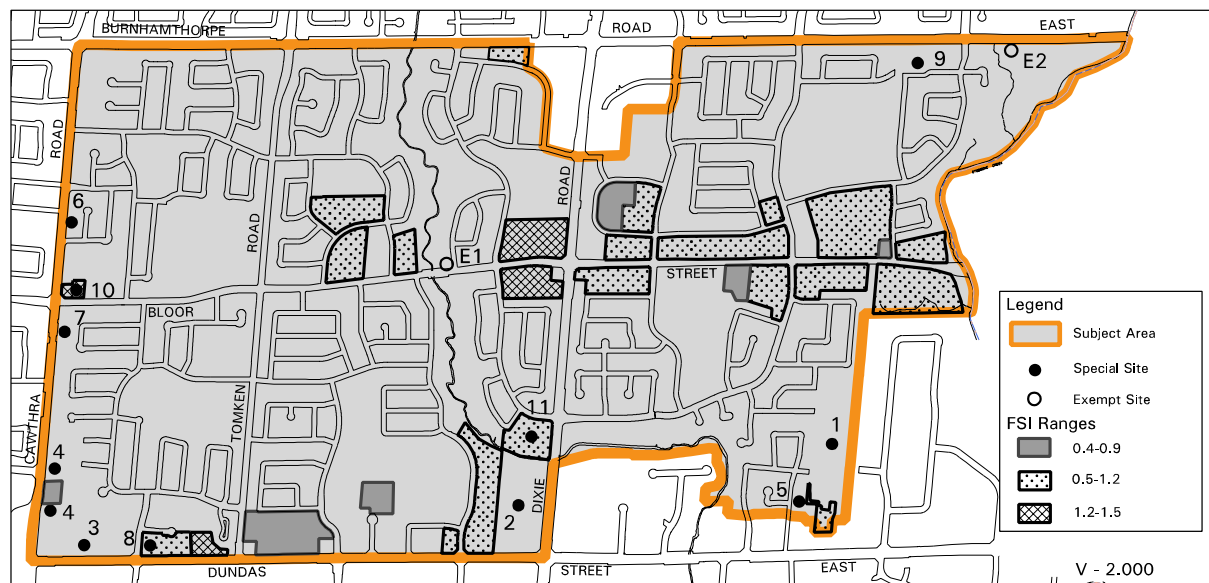
### 16.1.5 Business Employment

16.1.5.1 The Business Employment designation will not be permitted, except for lands designated Business Employment at the time this Plan comes into effect.

### 16.1.6 Industrial

16.1.6.1 The Industrial designation will not be permitted.

## 16.2 Applewood



Map 16-2: Applewood Neighbourhood Character Area

### 16.2.1 Urban Design Policies

#### Bloor Street

16.2.1.1 East of Dixie Road, development consists primarily of high density residential uses, with some mixed uses. The following policies apply to development or infilling of this segment of the street to encourage an improved pedestrian environment:

- a. High density residential development should relate to the street, with buildings sited to minimize setbacks from Bloor Street. Efforts to develop a continuous street frontage through the construction of ground related podium structures (not including above ground parking structures), intensive landscaping at the street edge, and the orientation of buildings parallel to the street are encouraged; and

- b. The mixed use properties on Bloor Street should have a stronger relationship to Bloor Street, with the main entrances of the buildings oriented towards Bloor Street and at least half of the linear frontage occupied by built form, wherever possible.

#### Dundas Street East

16.2.1.2 Creative massing solutions are encouraged in the design of built form and site layouts for mixed use lands on Dundas Street East. The height of buildings should be maximized, and the amount of parking between the front building façade and the street line decreased. Emphasis should be placed upon creative and effective landscape treatment. Signs should form an integral part of the overall concept of site and building design.

16.2.1.3 In high density residential development, efforts to develop a continuous street frontage through the orientation of buildings parallel to the street and the placement of significant building mass adjacent to the street edge are encouraged. Where this is not possible, the construction of

ground related podium structures (not including above ground parking structures) and intensive landscaping at the street edge, may be considered.

16.2.1.4 Pedestrian activity on lands designated Mixed Use at the northwest corner of Dundas Street East and Dixie Road should be encouraged by:

- a. minimizing building setbacks and the amount of parking between the front façade of buildings and the street;
- b. orienting buildings parallel to the street with a significant portion of the building mass located along the street frontage; and
- c. providing a continuous landscape area between the front wall of buildings and the street.

## 16.2.2 Land Use

16.2.2.1 For Medium and High Density Development, new development should not exceed the height of any existing buildings on the property, and should be further limited in height so as to form a gradual transition in massing when located adjacent to low density residential development. Buildings immediately adjacent to low density housing forms should be limited to three storeys. In situations where the low density housing forms are separated from the high density development by a public road, park, utility or other permanent open space feature, four to five storeys may be compatible.

16.2.2.2 Lands designated Residential Low Density II will not permit the following uses:

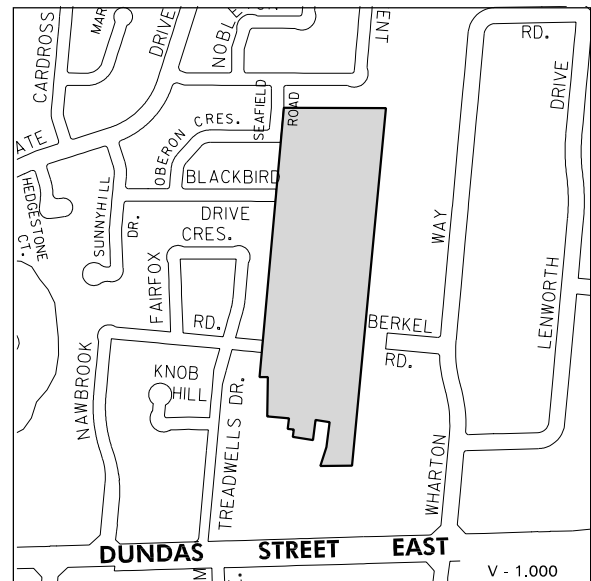
- a. triplexes, street townhouses and other forms of low-rise dwellings with individual frontages.

16.2.2.3 Lands designated Residential Medium Density will also permit low-rise apartment dwellings.

## 16.2.3 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

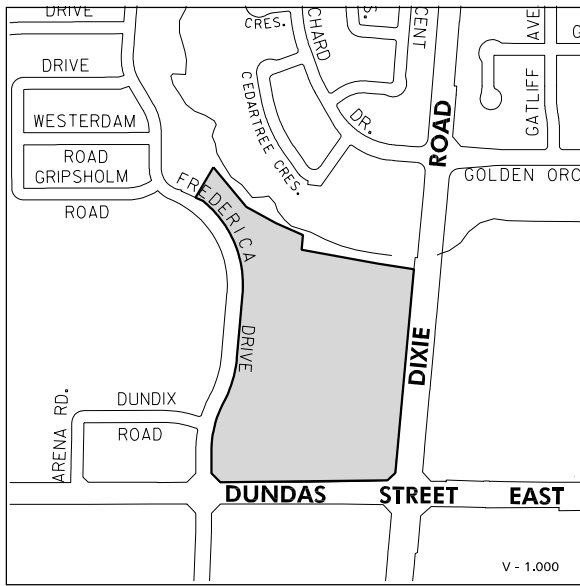
### 16.2.3.1 Site 1



16.2.3.1.1 The lands identified as Special Site 1 are located north of Dundas Street East, west of the Ontario Hydro corridor.

16.2.3.1.2 Notwithstanding the provisions of the Residential Low Density II designation, a maximum of 239 mobile homes or land lease community homes with accessory administrative facilities will be permitted.

### 16.2.3.2 Site 2



16.2.3.2.1 The lands identified as Special Site 2 are located at the northwest corner of Dundas Street East and Dixie Road. These lands are situated within a spill zone associated with the Regional Storm flood overtopping the south bank of the Little Etobicoke Creek channel and are classified as a special policy area floodplain on Schedule 3: Natural System.

16.2.3.2.2 Notwithstanding the policies of this Plan which relate to development within the Regional Storm Floodplain, and the provisions of the Mixed Use, Motor Vehicle Commercial and Residential High Density designations, the following policies will apply:

- a. the construction of any buildings or structures, including any additions to an existing building, and the placing or dumping of fill of any kind, or the alteration of any **watercourse**, will not be permitted within the special policy area without the approval of the Toronto and Region Conservation Authority (TRCA);
- b. development will not be permitted within the 100 year storm floodplain;
- c. development outside the 100 year storm floodplain will be permitted only on the basis of approvals by the City and the TRCA;

- d. ideally, new buildings and structures will be protected from flooding under Regional Storm conditions. Where this level of protection is technically or economically not feasible, Mississauga may, in consultation with the TRCA, consider a lesser level of protection. In no case, however, will the minimum level of protection be less than the 350 year storm floodplain;
- e. notwithstanding subsections c. and d. above of this section, development, or additions will not be permitted, where, as a result of a flood event equal to or less than the Regulatory Flood, structures will be subject to flows which, due to velocity and/or depth, would present conditions of hazard to life or potential damage to structures;
- f. in support of any development application, Mississauga or the TRCA may require engineering studies related to flood frequency, velocity and depth of flood flows, appropriate flood damage reduction measures, and stormwater management techniques. Based on these studies and other information, Mississauga or the TRCA may specify that any approval of the application will be subject to certain design considerations related to location, structural integrity, setbacks, coverage, elevation of ground floor openings, and other matters as may be determined by the City or the TRCA:
  - ingress and egress should be "safe" pursuant to Provincial Government flood proofing standards, and/or achieve the maximum level of flood protection determined to be feasible and practical based on existing infrastructure;
  - new development will not be permitted to locate in the floodplain where the use is:



- associated with the manufacture, storage, disposal and/or consumption of hazardous substances or the treatment, collection and disposal of sewage, which would pose an unacceptable threat to public safety if they were to escape their normal containment/use as a result of flooding or failure of flood proofing measures;
- homes and schools, which would pose a significant threat to the safety of the inhabitants (e.g. the sick, the elderly, the disabled or the young), if involved in an emergency evacuation situation as a result of flooding or failure of flood proofing measures; and
- associated with services, such as those provided by fire, police and ambulance stations and electrical substations, which would be impaired during a flood emergency as a result of flooding or failure of flood proofing measures; and
- where new development is not considered to pose an unacceptable risk to public safety, a higher level of flood protection and/or additional flood proofing precautions above the Regulatory Flood level, may still be required due to the sensitive nature of the development; and
- g. any amendment to the Zoning By-law will include provisions required by the City or the TRCA to implement the Special Site 2 policies of this Plan.

### 16.2.3.3 Site 3



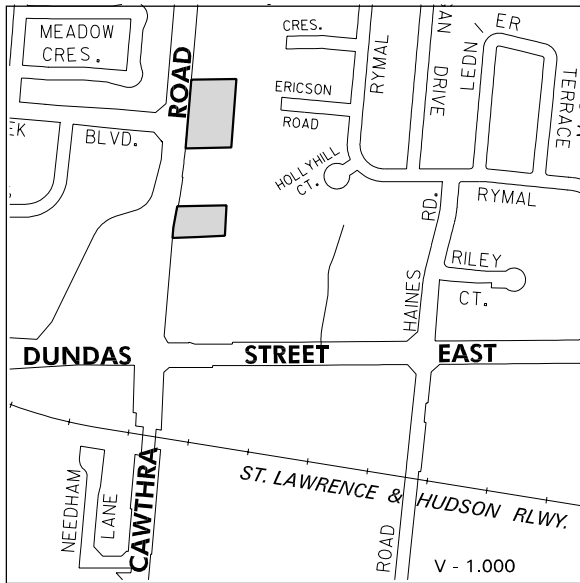
16.2.3.3.1 The lands identified as Special Site 3 are located on the north side of Dundas Street East, east of Cawthra Road.

16.2.3.3.2 Notwithstanding the provisions of the Office designation, apartment dwellings in accordance with the Residential High Density designation, will also be permitted and the following additional policies will apply:

- a. the extension of the existing cemetery is also permitted on the westerly portion of these lands; and
- b. approval for development will be subject to approval of a tree survey submission which demonstrates appropriate tree preservation measures.

#### 16.2.3.4

#### Site 4



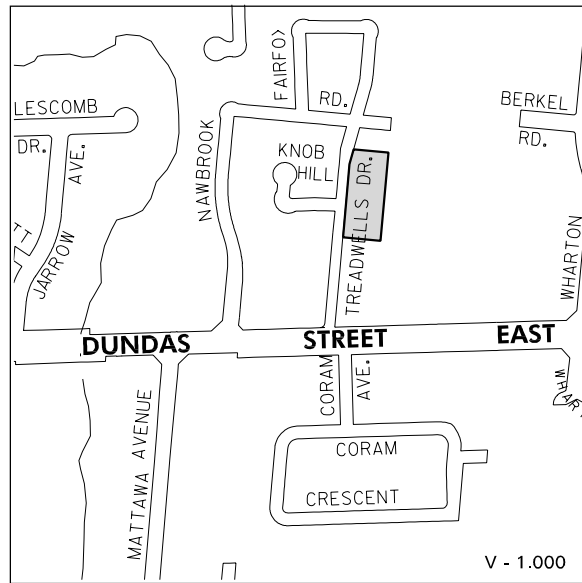
16.2.3.4.1 The lands identified as Special Site 4 are located along the east side of Cawthra Road, north of Dundas Street East.

16.2.3.4.2 Notwithstanding the provisions of the Residential Low Density I designation, offices will be permitted.

16.2.3.4.3 Approval for development will be subject to approval of a tree survey submission which demonstrates appropriate tree preservation measures.

#### 16.2.3.5

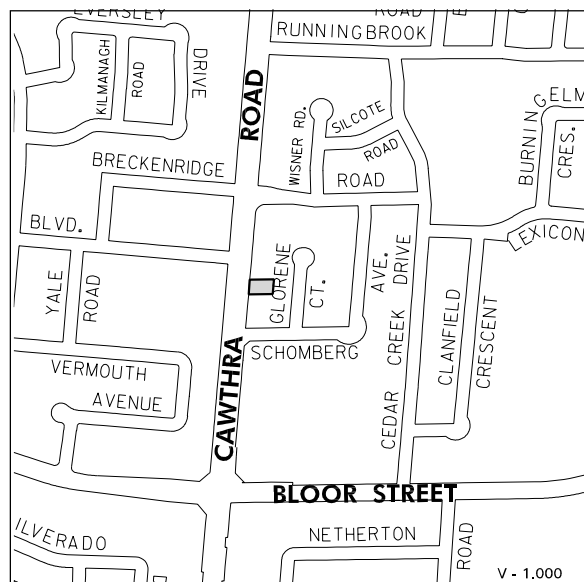
#### Site 5



16.2.3.5.1 The lands identified as Special Site 5 are located on the east side of Treadwells Drive, north of Dundas Street East.

16.2.3.5.2 Notwithstanding the provisions of the Residential Medium Density designation, a maximum of 9 detached dwellings and 23 townhouse dwellings will be permitted.

### 16.2.3.6 Site 6



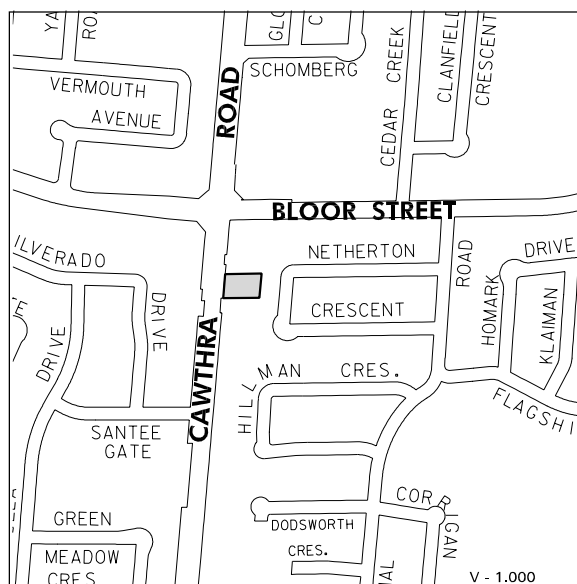
16.2.3.6.1 The lands identified as Special Site 6 are located on the east side of Cawthra Road, north of Schomberg Avenue.

16.2.3.6.2 Notwithstanding the provisions of the Residential Low Density II designation, the following additional policies will apply:

- a window and door showroom and associated office will be permitted in addition to residential uses;
- the building, including amenities and signage, whether new or modified, will have a residential appearance which is consistent with the form, design and scale of the surrounding residential area;
- all required and related parking will be accommodated at grade in such a manner that minimizes the loss of vegetation and provides for limited impact on adjoining residential development;
- to minimize the amount of hard surface area, on-site parking areas should have efficient vehicular circulation and layout which is suitably screened, preferably with vegetation; and

- new buildings or modified buildings used for a window and door showroom and associated office will not exceed 150 m<sup>2</sup>.

### 16.2.3.7 Site 7

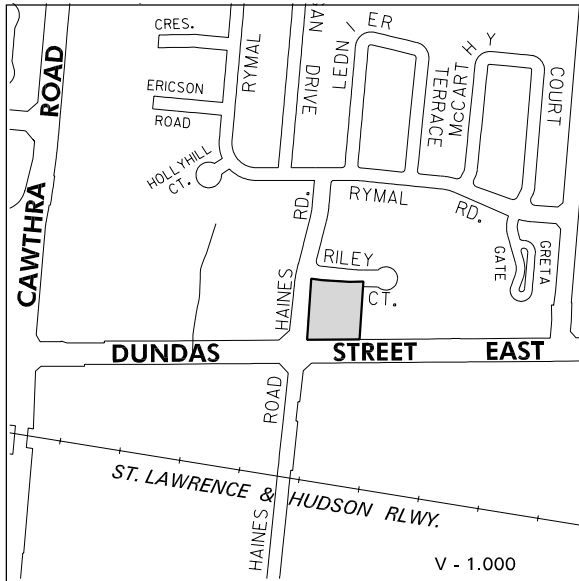


16.2.3.7.1 The lands identified as Special Site 7 are located on the east side of Cawthra Road, south of Bloor Street.

16.2.3.7.2 Notwithstanding the provisions of the Residential Low Density II designation, only a detached dwelling or an office for a maximum of three non-resident physicians, dentists and drugless practitioners will be permitted.

### 16.2.3.8

### Site 8



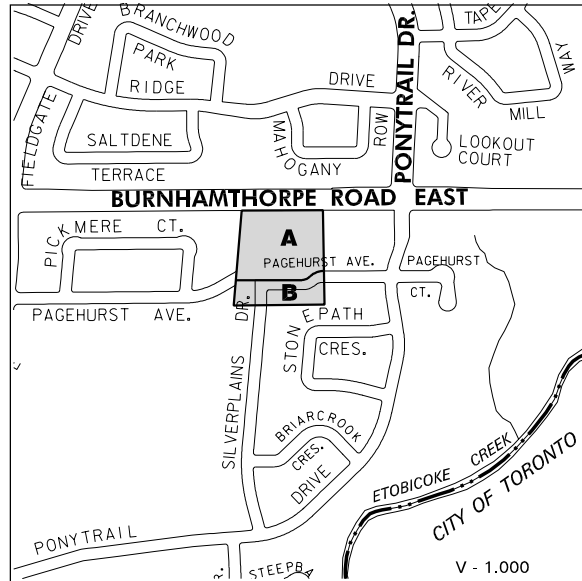
16.2.3.8.1 The lands identified as Special Site 8 are located on the east side of Haines Road, north of Dundas Street East.

16.2.3.8.2 Notwithstanding the provisions of the Residential High Density Designation, the following additional policies will apply:

- a. apartment dwellings with a maximum height of ten storeys if the lot area is equal to or greater than 6 600 m<sup>2</sup>;
- or
- b. apartment dwellings with a maximum height of four storeys if the lot area is less than 6 600 m<sup>2</sup>.

### 16.2.3.9

### Site 9



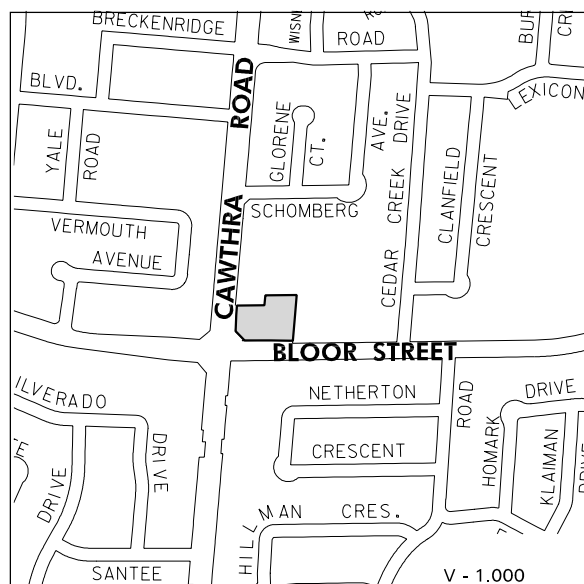
16.2.3.9.1 The lands identified as Special Site 9 are located on the south side of Burnhamthorpe Road East, west of Ponytrail Drive.

16.2.3.9.2 Notwithstanding the provisions of the Residential Medium Density designation applying to Area A, a maximum of 96 townhouse units and 24 semi-detached units will be permitted.

16.2.3.9.3 Notwithstanding the provisions of the Residential Low Density I designation applying to Area B, a maximum of 11 detached dwellings will be permitted.



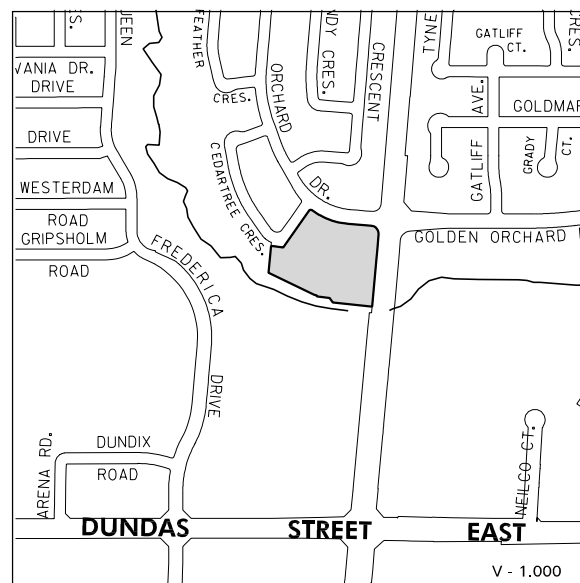
### 16.2.3.10 Site 10



16.2.3.10.1 The lands identified as Special Site 10 are located on the northeast corner of Bloor Street and Cawthra Road.

16.2.3.10.2 Notwithstanding the provisions of the Residential High Density designation, a residential apartment building will be permitted with a maximum total gross floor area of 1.65 times the lot area. No building or structure will exceed a height of 11.9 m above established grade, excluding any mechanical penthouse.

### 16.2.3.11 Site 11



16.2.3.11.1 The lands identified as Special Site 11 are located on the southwest corner of Golden Orchard Drive and Dixie Road.

16.2.3.11.2 Notwithstanding the provisions of the Residential High Density designation, apartment dwellings are permitted subject to the following additional policies:

- a. development will be restricted to apartment dwellings to a maximum of 328 units;
- b. the maximum density will be 134 units per hectare; and
- c. the maximum building height of new development will be seven storeys.

16.2.3.11.3 Townhouse development may be permitted subject to a rezoning application that addresses, among other matters, the location, number and design of units.

16.2.3.11.4 In addition to the other applicable policies, the site will incorporate the following design guidelines:

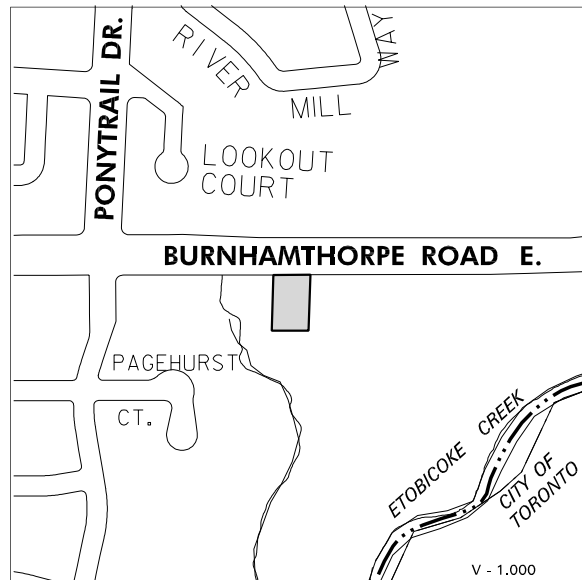
- a. Built Form - the perceived building scale of all new construction will be minimized, by incorporating the top level of apartment units

within a gable roof form with dormer window accents;

- b. Site Layout - a landscaped forecourt will be provided at principal building entrances incorporating a vehicular drop-off, visitor parking and pedestrian areas;
- c. Architectural Integration - architectural details and materials of all new construction must complement the character of existing buildings on the site; and
- d. Landscaping - extensive landscape buffering will be provided along the westerly and northerly streetlines, in combination with a decorative fence. A 1.8 m high privacy fence will be erected to provide privacy for the single family dwelling with a common property line to the west.

## 16.2.4 Exempt Site Policies

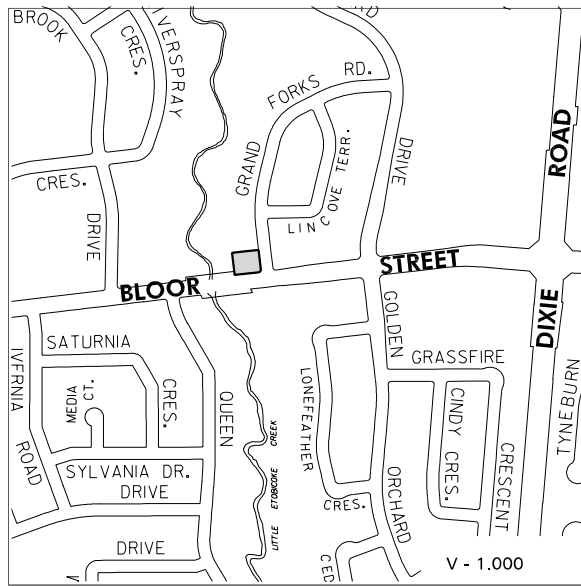
### 16.2.4.1 Site 1



16.2.4.1.1 The lands identified as Exempt Site 1 are located at the northwest corner of Grand Forks Road and Bloor Street.

16.2.4.1.2 Notwithstanding the provisions of the Residential Low Density I designation, offices will also be permitted, subject to the requirement that the external appearance of the building be visually compatible with the adjacent low density residential development.

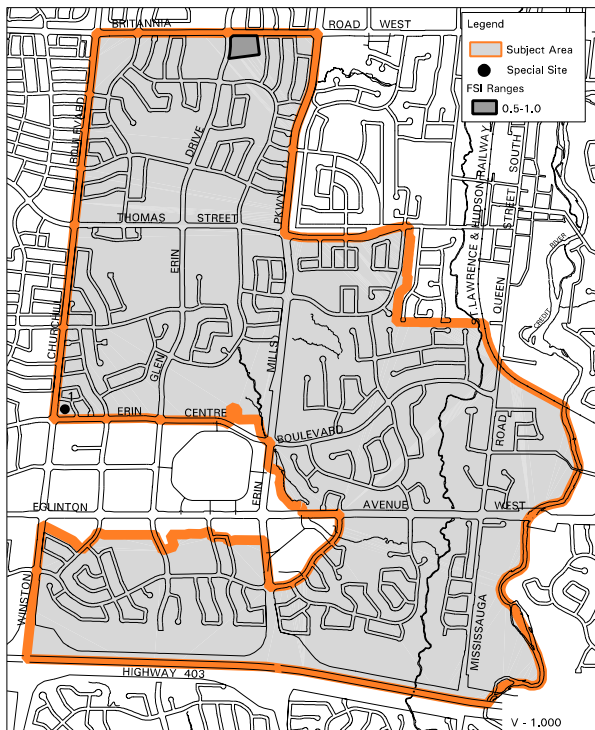
#### 16.2.4.2 Site 2



16.2.4.2.1 The lands identified as Exempt Site 2 are located on the south side of Burnhamthorpe Road East, east of Ponytrail Drive.

16.2.4.2.2 Notwithstanding the provisions of the Greenbelt designation, a veterinary clinic will also be permitted.

## 16.3 Central Erin Mills

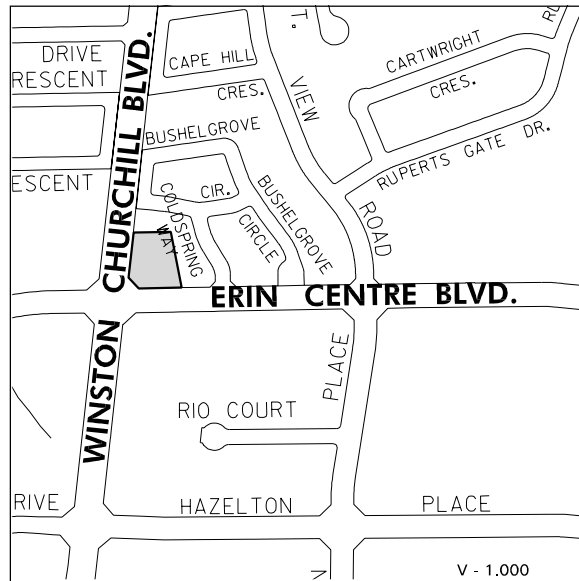


Map 16-3: Central Erin Mills Neighbourhood Character Area

### 16.3.1 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 16.3.1.1 Site 1

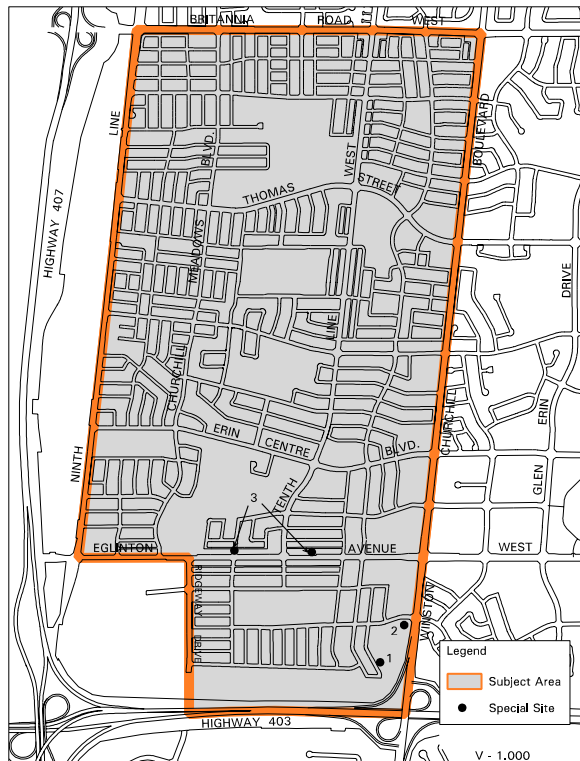


16.3.1.1.1 The lands identified as Special Site 1 are located at the northeast corner of Erin Centre Boulevard and Winston Churchill Boulevard.

16.3.1.1.2 Notwithstanding the provisions of the Motor Vehicle Commercial designation, a convenience restaurant will be permitted.



## 16.4 Churchill Meadows

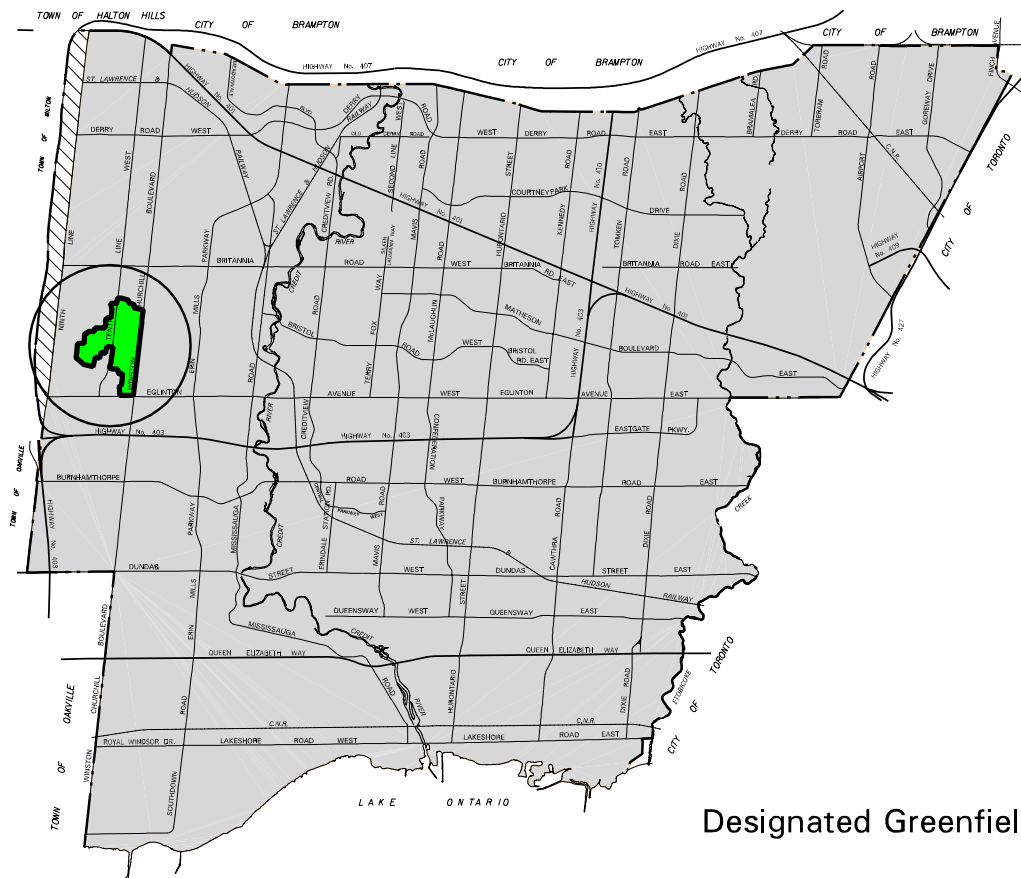


Map 16-4: Churchill Meadows Neighbourhood Character Area

### 16.4.1 Designated Greenfield Area

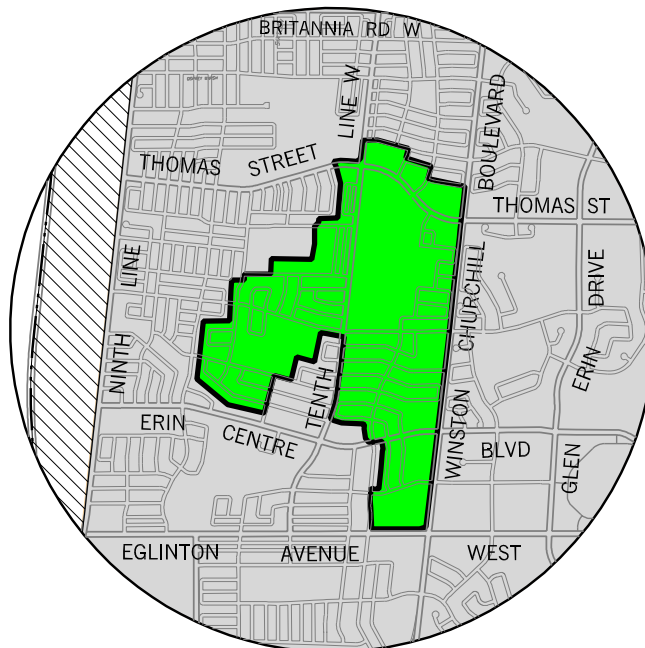
There are some lands in the Churchill Meadows Neighbourhood Area identified on Map 16-4.1 as a designated greenfield area pursuant to the Growth Plan for the Greater Golden Horseshoe.

16.4.1.1 The designated greenfield area will be planned to achieve a minimum density of 75 residents and jobs combined per hectare, excluding permitted environmental take-outs.



## Designated Greenfield Area

- Designated Greenfield Area
- Built-up Area
- Special Study Area
- City Limits



0 1000 2000 3000 4000  
metres



V - 1,000

Map 16-4.1: Designated Greenfield Area

## 16.4.2 Urban Design Policies

### 16.4.2.1 Community Design

16.4.2.1.1 For the purpose of these policies, Churchill Meadows is composed of five residential Precincts, as identified on Map 16-4.2. The Community Design policies must be read in conjunction with the Land Use policies and all other Urban Design policies including the *Neighbourhood Concept Plan Principles and Urban Design Guidelines*, July 1997.

### 16.4.2.2 Community Focus (Precinct 1)

16.4.2.2.1 The lands surrounding the intersection of Thomas Street and Tenth Line West will form a community focus for the city through the development of a retail commercial facility and Residential High Density development. The form of development is to allow for transition of conventional dwellings to mixed uses with store fronts addressing mainstreets to achieve an urban character with a pedestrian friendly environment.

- a. Buildings on Residential Low Density II and Residential Medium Density lands located north and south of Thomas Street, fronting onto the east and west sides of Tenth Line West, should have built forms located close to the street edge. While buildings may be developed as residential units, mixed commercial/residential "live/work" units will be permitted subject to: parking facilities being provided and designed in a manner that is compatible with the mainstreet character of the area; incorporation of measures such as landscape space, planters or tree grates; or other elements which reinforce the urban street wall.
- b. Building heights for Residential High Density lands located to the east and south of the Mixed Use designation will be limited to a range of five to eight storeys. Buildings located at the intersection of Tenth Line West and Thomas

Street will be a minimum of two storeys and a maximum of five storeys. Building heights for Residential High Density lands located on the west side of Tenth Line West, south of Thomas Street will be a minimum of three storeys together with end units of a block development having a minimum building height of two storeys.

- c. Development of the retail commercial sites should discourage parking and loading areas at the intersection of two streets in favour of built form.
- d. The two Residential High Density sites west of the intersection of Thomas Street and Winston Churchill Boulevard will generally be limited to a range of five to eight storeys in height. This development will represent the secondary community gateway; therefore, buildings at the higher end of the height range will be encouraged.

### 16.4.2.3 Winston Churchill Boulevard Linkage (Precinct 2)

16.4.2.3.1 This area relates to the development of the lands on the west side of Winston Churchill Boulevard between Precinct 1 and 3. Built form within this linkage area should step down in height from the two community gateway nodes at either end of this Precinct. In addition, the goal of this area is to provide a coordinated built form image linking two Precincts.

- a. Residential development should avoid reversed frontage lots abutting Winston Churchill Boulevard.
- b. Built form should frame the streetline with a consistent **streetscape** along Winston Churchill Boulevard.

#### **16.4.2.4 Primary Community Gateway (Precinct 3)**

16.4.2.4.1 This area, located at the intersection of Winston Churchill Boulevard and Eglinton Avenue West, has Residential High Density designations.

- a. Building heights for the Residential High Density designation will generally be limited to a range of five to eight storeys with the greatest heights to occur at the intersection of two streets.
- b. Open space/pedestrian corridors from these developments to the proposed transit terminal will be encouraged. In support of this objective, evaluations of microclimatic conditions may be required in the review of development applications in this Precinct.
- c. At full development these parcels of land should provide continuity in built form and landscape themes with the Residential High Density lands on the east side of Winston Churchill Boulevard.

#### **16.4.2.5 Eglinton Avenue Linkage (Precinct 4)**

16.4.2.5.1 This area is located on the north and south sides of Eglinton Avenue West between Precinct 3 and 5. The goal of this area is to provide a coordinated built form image linking two Precincts while having regard for the integration and significance of the retained woodlands west of Tenth Line West.

- a. The landscape treatment for development parcels adjacent to the retained woodlands should be in the form of naturalized character to integrate development with these open spaces. In addition, the built form along Eglinton Avenue West is to be sympathetic in height and scale to the retained woodlands/space, wherever possible.
- b. The commercial facility is to avoid, wherever possible, parking and loading areas abutting Eglinton Avenue West and Tenth Line West, in favour of built form. The preferred character is

to create continuous landscape areas between the building and the streetline.

- c. The height of buildings are to step down from the Primary Community Gateway Precinct.
- d. Buildings on Residential Medium Density lands fronting onto the north and south sides of Eglinton Avenue West should have built forms located close to the street edge. Residential development should front Eglinton Avenue West and be served by rear lane access to units. While buildings may be developed as residential units, mixed commercial/residential "live/work" units will be permitted subject to parking facilities being provided and designed in a manner that is compatible with the mainstreet character of the area; incorporation of measures such as landscape space, planters or tree grates; or other elements that reinforce the urban street wall.

#### **16.4.2.6 City Entry Point (Precinct 5)**

16.4.2.6.1 This area represents an entry point into the city and, therefore, will promote built form of high architectural and landscaping standards.

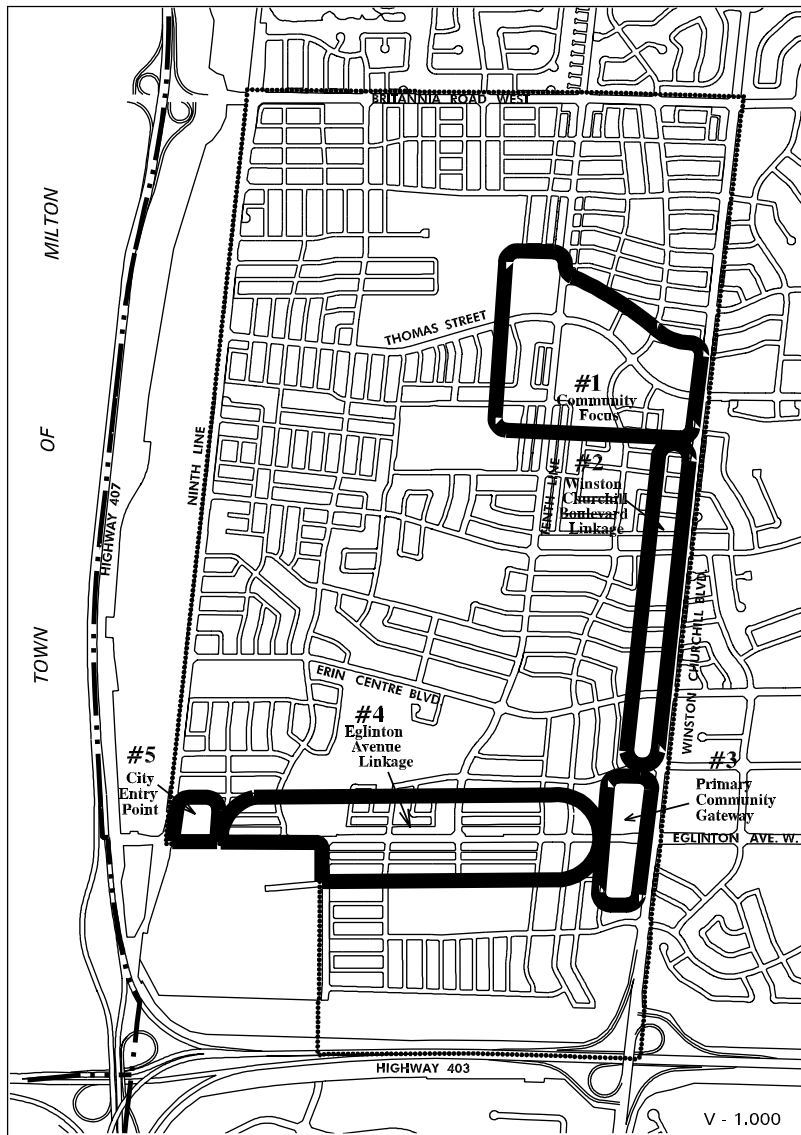
- a. Strong built form along the street frontage with massing emphasis at corners, thus discouraging parking and loading areas at the streetlines.
- b. Landscape treatments at corners and extending along Eglinton Avenue West should generally be complementary on either side of the street.

#### **16.4.2.7 General**

16.4.2.7.1 The retention/preservation of woodlands, wherever possible, will be a priority in establishing a community identity for the Character Area.

16.4.2.7.2 Buildings located on lands designated Residential Medium Density, adjacent to detached or semi-detached dwellings, will be subject to a two storey height restriction. Others will be subject to a maximum four storey height restriction.





Map 16-4.2: Churchill Meadows Precincts

### 16.4.3 Land Use

16.4.3.1 Notwithstanding the Residential Low Density I policies of this Plan, the Residential Low Density I designation permits only detached dwellings.

16.4.3.2 Lands designated Residential High Density will also permit townhouse dwellings.

### 16.4.4 Transportation

16.4.4.1 Notwithstanding the policies of this Plan, the following policies will apply:

- a. where feasible, the alignment of individual roads will recognize the need to preserve natural features, including woodland edges and hedgerows. Detailed road design will include mitigating and rehabilitation measures to address any disturbance of these features;
- b. Mississauga will be pursuing other possible opportunities for improving Provincial Highway access to Churchill Meadows. A further detailed study will be required, involving the approval of appropriate agencies, to determine the feasibility and timing of these improvements;
- c. direct access will be permitted on Major Collectors, with 30 m rights-of-way, for uses other than residential detached, semi-detached, or street townhouses. The access location will require the approval of the Transportation and Works Department;
- d. during the review of development applications, consideration may be given to reducing the right-of-way widths of minor collector roads according to the guidelines established in the *Neighbourhood Concept Plan Principles and Urban Design Guidelines*. Any such consideration would require a detailed analysis of the municipal servicing, utility and planting constraints within the right-of-way, to ensure that adequate **corridors** are maintained to the

satisfaction of the City, Region and the Public Utilities Co-ordinating Committee; and

- e. guidelines regarding the treatment of Buffer Roads and Public Lanes are established in the *Neighbourhood Concept Plan Principles and Urban Design Guidelines*. Buffer Roads will have a right-of-way width of 17 m in direct frontage situations, and consideration will be given to reducing this right-of-way width to 15 m where there is no direct frontage. Public Lanes will have a minimum right-of-way width of eight meters.

### 16.4.5 Physical Services and Utilities

- a. The area comprises two drainage systems: the Credit River and the Sixteen Mile Creek watersheds. This Plan will be subject to drainage plans for the two watersheds. Before being permitted to proceed, all new development will have adequate storm drainage facilities which conform to the requirements of the relevant drainage plans.
- b. Applicants for development of lands which drain to the Region of Halton will be required to obtain approval from Conservation Halton for the design, installation and maintenance of the storm drainage works required in the Sixteen Mile Creek watershed area.
- c. All proposed drainage works are to be compatible with the Provincial Government plans for the future extension of Provincial Highway 407 from Provincial Highway 403 to Provincial Highway 401.
- d. An Environmental Assessment Report, satisfactory to the City, Provincial Government, and other public agencies, will be prepared in support of proposed drainage works which are subject to the *Environmental Assessment Act*.

- e. All development within Churchill Meadows will be in accordance with the Sawmill Creek Sub-watershed Plan, the Sixteen Mile Creek Master Drainage Plan and the Lisgar Region Water Quality Study.
- f. Additional off-site lands associated with the construction of the stormwater management facilities recommended by the Sawmill Creek Sub-watershed Study may be needed.
- g. Stormwater management facilities are located on the west side of Ninth Line, to accommodate post development runoff from those portions of Churchill Meadows drained by the Sixteen Mile Creek.

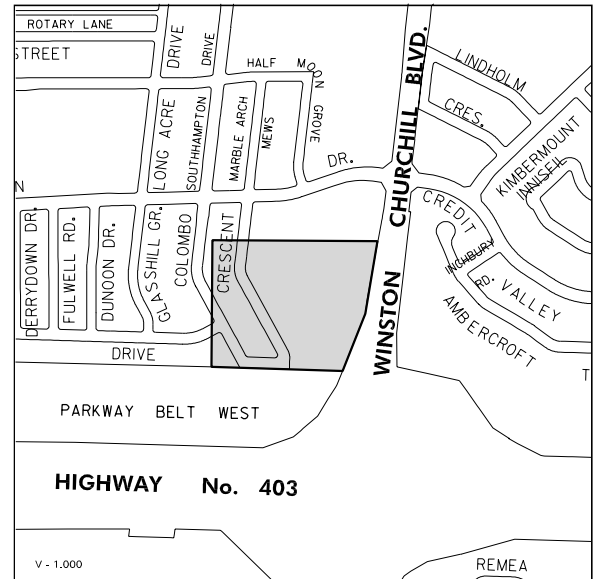
## 16.4.6 Implementation

Mississauga has the option of requiring the developers to enter into mandatory cost sharing agreements to ensure that the lands required for public purposes are available at the appropriate time.

## 16.4.7 Special Site Policies

There are sites within Churchill Meadows that merit special attention and are subject to the following policies.

### 16.4.7.1 Site 1



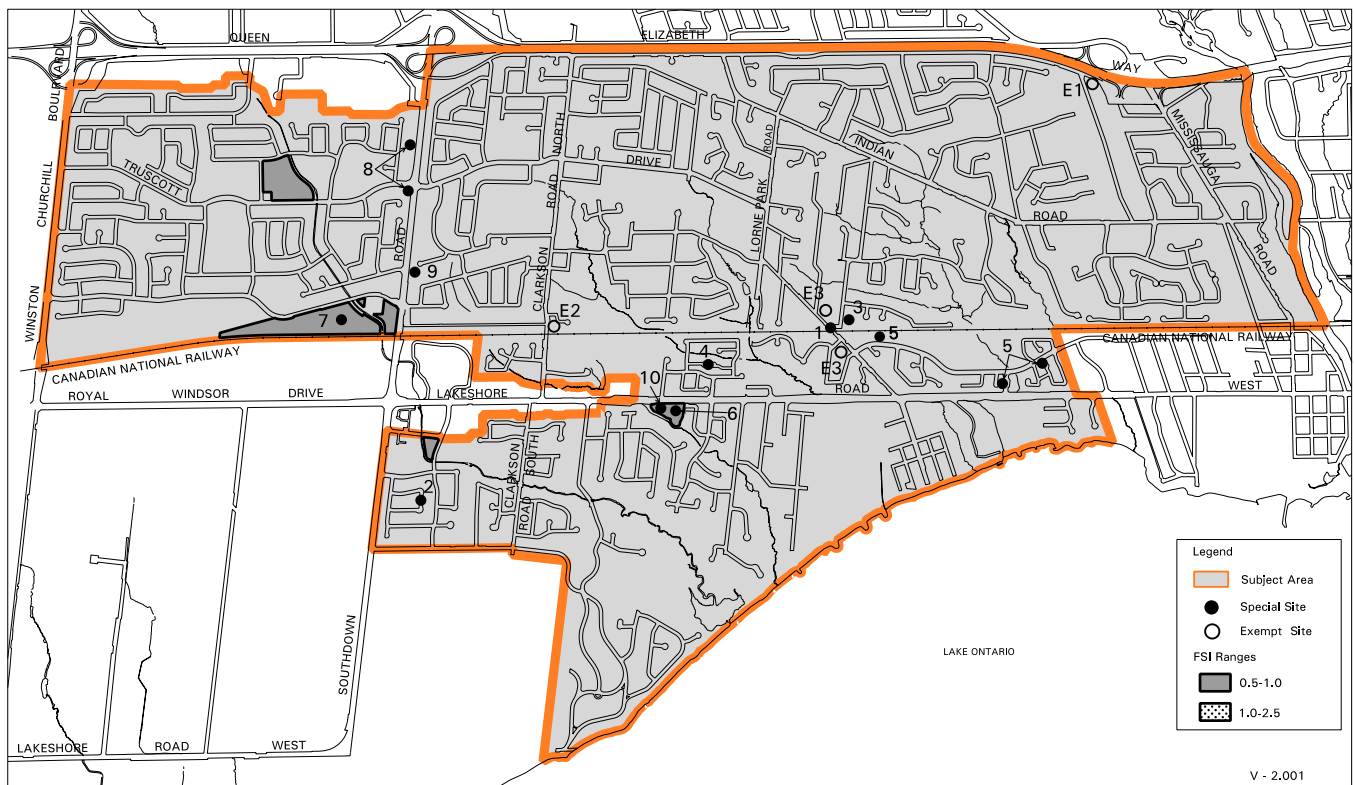
16.4.7.1.1 The lands identified as Special Site 1 are located south of Credit Valley Road and west of Winston Churchill Boulevard.

16.4.7.1.2 Notwithstanding the provisions of the Residential Low Density II, Greenbelt, and Utility designations, the physical integration of the Bus Rapid Transit (BRT) facility with adjacent development will be encouraged.





## 16.5 Clarkson-Lorne Park



Map 16-5: Clarkson-Lorne Park Neighbourhood Character Area

### 16.5.1 Urban Design Policies

#### Community Identity and Focus

16.5.1.1 Developments should be compatible with and enhance the character of Clarkson-Lorne Park as a diverse established community by integrating with the surrounding area.

16.5.1.2 Development should be designed to reflect and enhance the Clarkson Village Mixed Use area **streetscape**.

16.5.1.3 The Clarkson Node, will be the focus of activity for the Character Area, combining residential uses, cultural activities, shopping, dining, commerce and recreation.

#### Infill Housing

16.5.1.4 For development of all detached dwellings on lands identified in the Site Plan Control By-law, the following will apply:

- preserve and enhance the generous front, rear and side yard setbacks;
- ensure that existing grades and drainage conditions are preserved;
- encourage new housing to fit the scale and character of the surrounding area, and take advantage of the features of a particular site, i.e. topography, contours, mature vegetation;
- garages should be recessed or located behind the main face of the house. Alternatively, garages should be located in the rear of the property;

- e. ensure that new development has minimal impact on its adjacent neighbours with respect to overshadowing and overlook;
- f. encourage buildings to be one to two storeys in height. The design of the building should de-emphasize the height of the house and be designed as a composition of small architectural elements, i.e. projecting dormers and bay windows;
- g. reduce the hard surface areas in the front yard;
- h. preserve existing mature high quality trees to maintain the existing mature nature of these areas;
- i. house designs which fit with the scale and character of the local area, and take advantage of the particular site are encouraged. The use of standard, repeat designs is strongly discouraged; and
- j. the building mass, side yards and rear yards should respect and relate to those of adjacent lots.

## 16.5.2 Land Use

16.5.2.1 Notwithstanding the Residential Low Density I policies of this Plan, the Residential Low Density I designation permits only detached dwellings.

16.5.2.2 Notwithstanding the Residential Low Density II policies of this Plan, the Residential Low Density II designation permits:

- a. only detached dwellings for the area east of Southdown Road; and
- b. only detached, semi-detached and duplex dwellings for the area west of Southdown Road.

16.5.2.3 Notwithstanding the Residential Low Density II policies of this Plan, for the area west of Southdown Road, any lot occupied by a detached

dwelling prior to May 6, 2003 will only be developed for a detached dwelling.

16.5.2.4 Notwithstanding the Residential Medium Density policies of this Plan, the Residential Medium Density designation permits only townhouses.

16.5.2.5 Notwithstanding the provisions of the Mixed Use policies of this Plan, motor vehicle sales and rentals will not be permitted.

## 16.5.3 Natural Areas System

16.5.3.1 Notwithstanding the Natural Areas System policies of this Plan concerning residential woodlands, sites with mature trees will be subject to a review of a tree preservation plan prior to consideration of proposed development.

## 16.5.4 Transportation

16.5.4.1 No major changes to Clarkson Road, Truscott Drive, Lorne Park Road, and Indian Road will be undertaken except minor channelization, reconstruction, bus bay construction, and improvements of a similar nature.

16.5.4.2 Notwithstanding the classification of Clarkson Road, Lorne Park Road, Indian Road and Mississauga Road, these roads will be limited to no more than two through lanes.

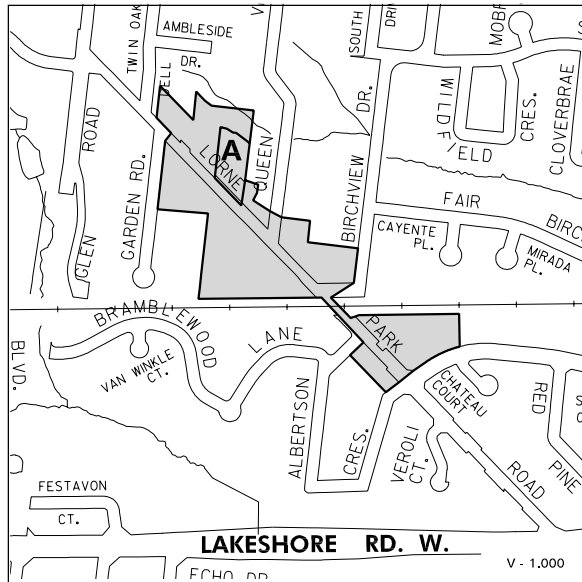
16.5.4.3 No approvals for development on the south side of South Sheridan Way, west of Southdown Road, will be given until the proponent has submitted an acceptable traffic impact study to the satisfaction of the City.

16.5.4.4 Mississauga will also encourage and promote better utilization of the existing parking inventory within the Clarkson Village Mixed Use area through discussions with the members of the Clarkson Village Business Improvement Area.

## 16.5.5 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 16.5.5.1 Site 1



16.5.5.1.1 The lands identified as Special Site 1 are located east and west of Lorne Park Road and north and south of the Canadian National Railway Line.

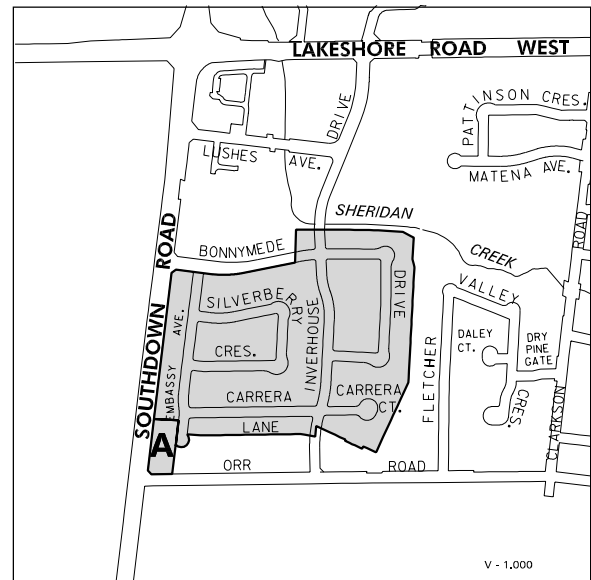
16.5.5.1.2 Notwithstanding the provisions of the Residential Low Density I and Mixed Use designations, the following policies will apply:

- this **streetscape** should emphasize older buildings which are close to the street; and
- encourage two storey development, with buildings designed to de-emphasize height, and to be consistent with the residential character of the neighbourhood.

16.5.5.1.3 Notwithstanding the Residential Low Density II designation, on lands identified as A, the subject lands may only be developed for

detached, semi-detached and townhouse dwellings or a combination thereof, up to a maximum density of 19 units **net residential hectare**.

### 16.5.5.2 Site 2



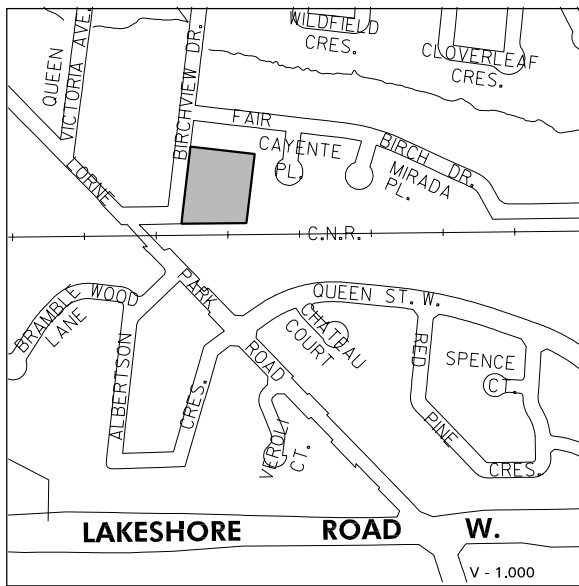
16.5.5.2.1 The lands identified as Special Site 2 are located east of Southdown Road, south of Bonnymede Drive.

16.5.5.2.2 Notwithstanding the provisions of the Residential Low Density II designation, semi-detached dwellings will also be permitted at a density of 11 - 30 units per **net residential hectare**.

16.5.5.2.3 The lands identified as Area A are located at the northeast corner of Southdown Road and Orr Road. A Shelter for Victims of Family Violence in the form of a building composed of office and residential uses with a maximum total gross floor area of 2 100 m<sup>2</sup> will also be permitted.

### 16.5.5.3

### Site 3

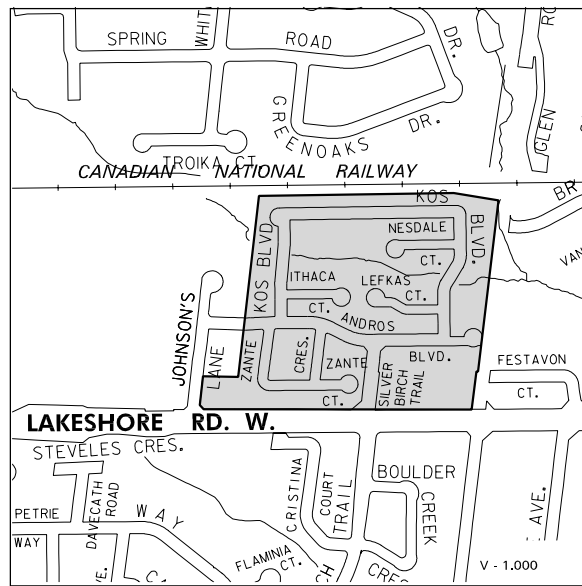


16.5.5.3.1 The lands identified as Special Site 3 are located on the east side of Birchview Drive, immediately north of the CNR tracks.

16.5.5.3.2 Notwithstanding the provisions of the Residential Low Density II designation, the subject lands may only be developed for a condominium consisting of detached and semi-detached dwellings or a combination thereof, up to a maximum density of 15 units per **net residential hectare** provided that no more than 12 dwelling units shall be permitted, of which no more than four dwelling units shall be in the form of semi-detached dwellings.

### 16.5.5.4

### Site 4



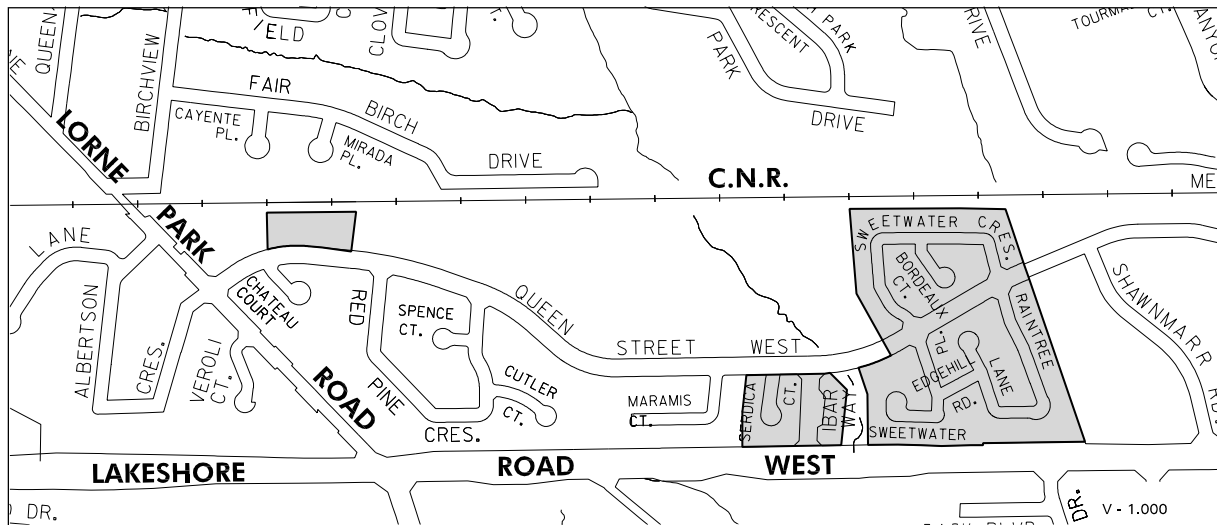
16.5.5.4.1 The lands identified as Special Site 4 are located north of Lakeshore Road West, east of Johnson's Lane.

16.5.5.4.2 Notwithstanding the provisions of the Residential Low Density I, Residential Low Density II and Residential Medium Density designations, detached, semi-detached and townhouses will be permitted.



### 16.5.5.5

### Site 5

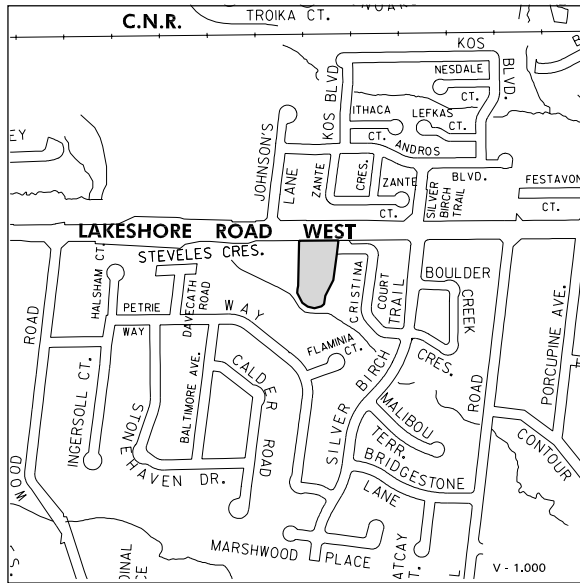


16.5.5.5.1 The lands identified as Special Site 5 are located north of Lakeshore Road West, east of Lorne Park Road.

16.5.5.5.2 Notwithstanding the provisions of the Residential Low Density II and Residential Medium Density designations, detached, semi-detached and townhouses will be permitted.

## 16.5.5.6

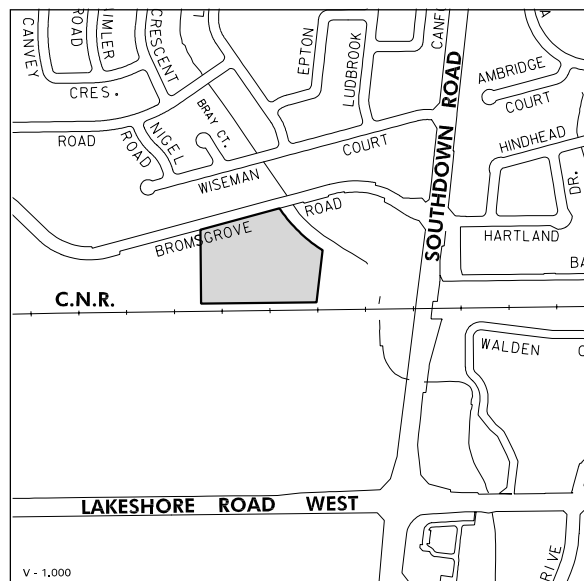
## Site 6



16.5.5.6.1 The lands identified as Special Site 6 are located on the south side of Lakeshore Road West, west of Cristina Court.

16.5.5.6.2 Notwithstanding the provisions of the Residential High Density designation, 21 one storey townhouse dwellings will be permitted.

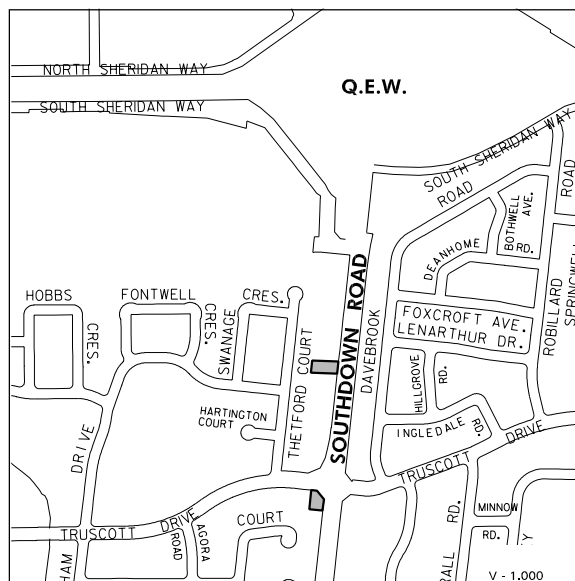
### 16.5.5.7 Site 7



16.5.5.7.1 The lands identified as Special Site 7 are located on the south side of Bromsgrove Road, west of Southdown Road.

16.5.5.7.2 Notwithstanding the provisions of the Residential High Density designation, horizontal multiple dwellings will be permitted.

### 16.5.5.8 Site 8

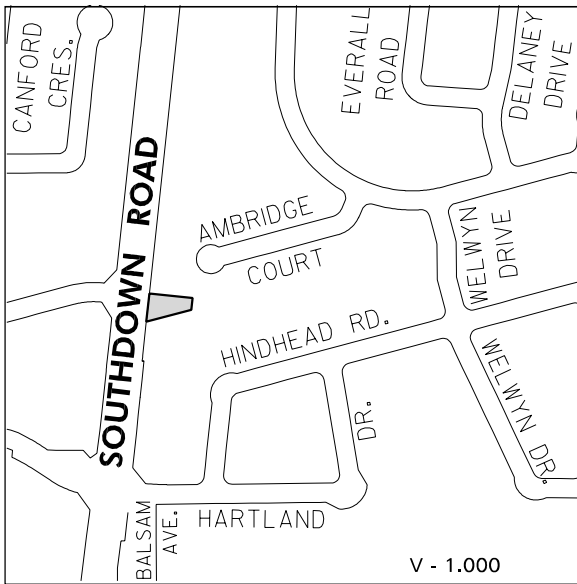


16.5.5.8.1 The lands identified as Special Site 8 are located on the west side of Southdown Road, south of Truscott Drive and north of Truscott Drive.

16.5.5.8.2 Notwithstanding the provisions of the Residential Low Density II designation, offices will be permitted.

### 16.5.5.9

### Site 9

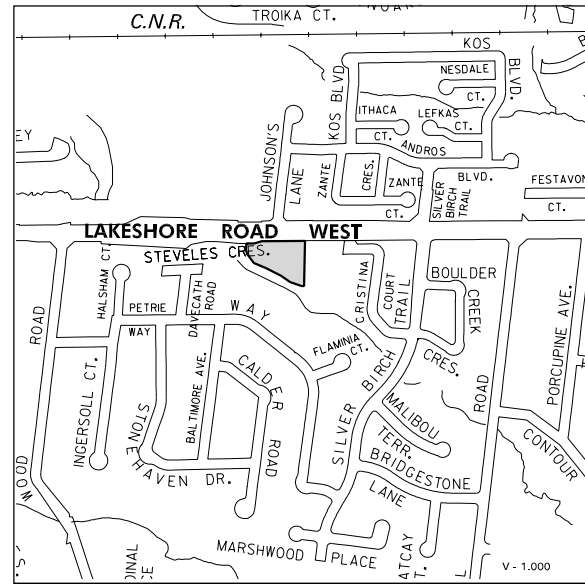


16.5.5.9.1 The lands identified as Special Site 9 are located on the east side of Southdown Road, south of Truscott Drive.

16.5.5.9.2 Notwithstanding the provisions of the Residential Low Density II designation, offices, excluding medical and real estate offices, will be permitted.

### 16.5.5.10

### Site 10



16.5.5.10.1 The lands identified as Special Site 10 are located on the south side of Lakeshore Road West, west of Cristina Court.

16.5.5.10.2 Notwithstanding the provisions of the Residential High Density designation, retirement dwellings with a **floor space index (FSI)** of 1.3, to a maximum of three storeys, will be permitted.



## 16.5.6 Exempt Site Policies

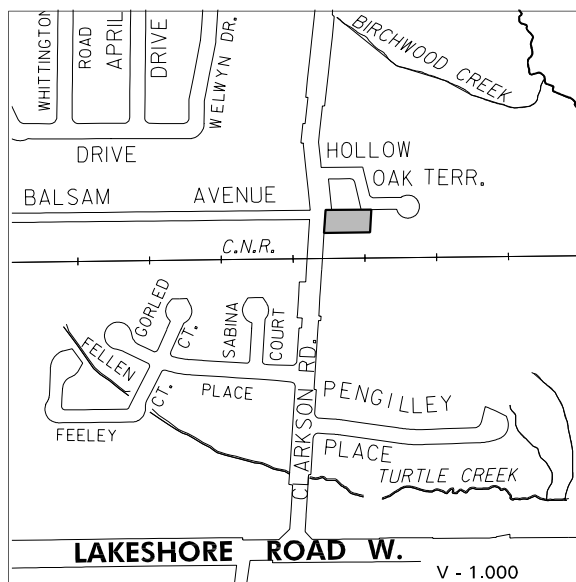
### 16.5.6.1 Site 1



16.5.6.1.1 The lands identified as Exempt Site 1 are located at the southeast corner of South Sheridan Way and Indian Grove.

16.5.6.1.2 Notwithstanding the provisions of the Motor Vehicle Commercial designation, a convenience restaurant will also be permitted.

### 16.5.6.2 Site 2

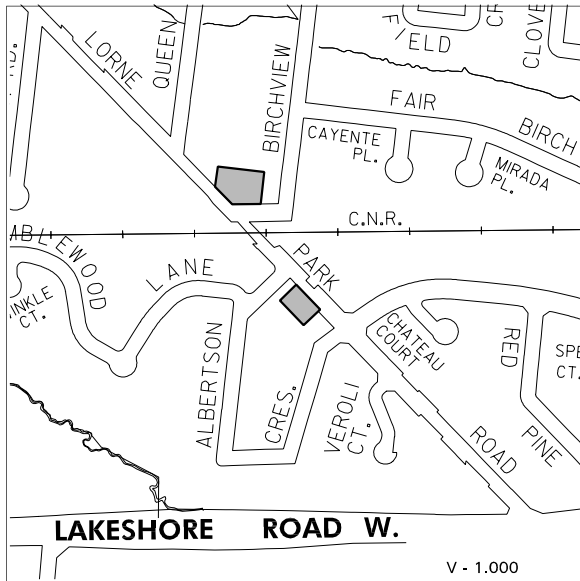


16.5.6.2.1 The lands identified as Exempt Site 2 are located on the west side of Clarkson Road North, south of Hollow Oak Terrace.

16.5.6.2.2 Notwithstanding the provisions of the Residential Low Density I designation, office use in the existing detached dwelling will also be permitted.

### 16.5.6.3

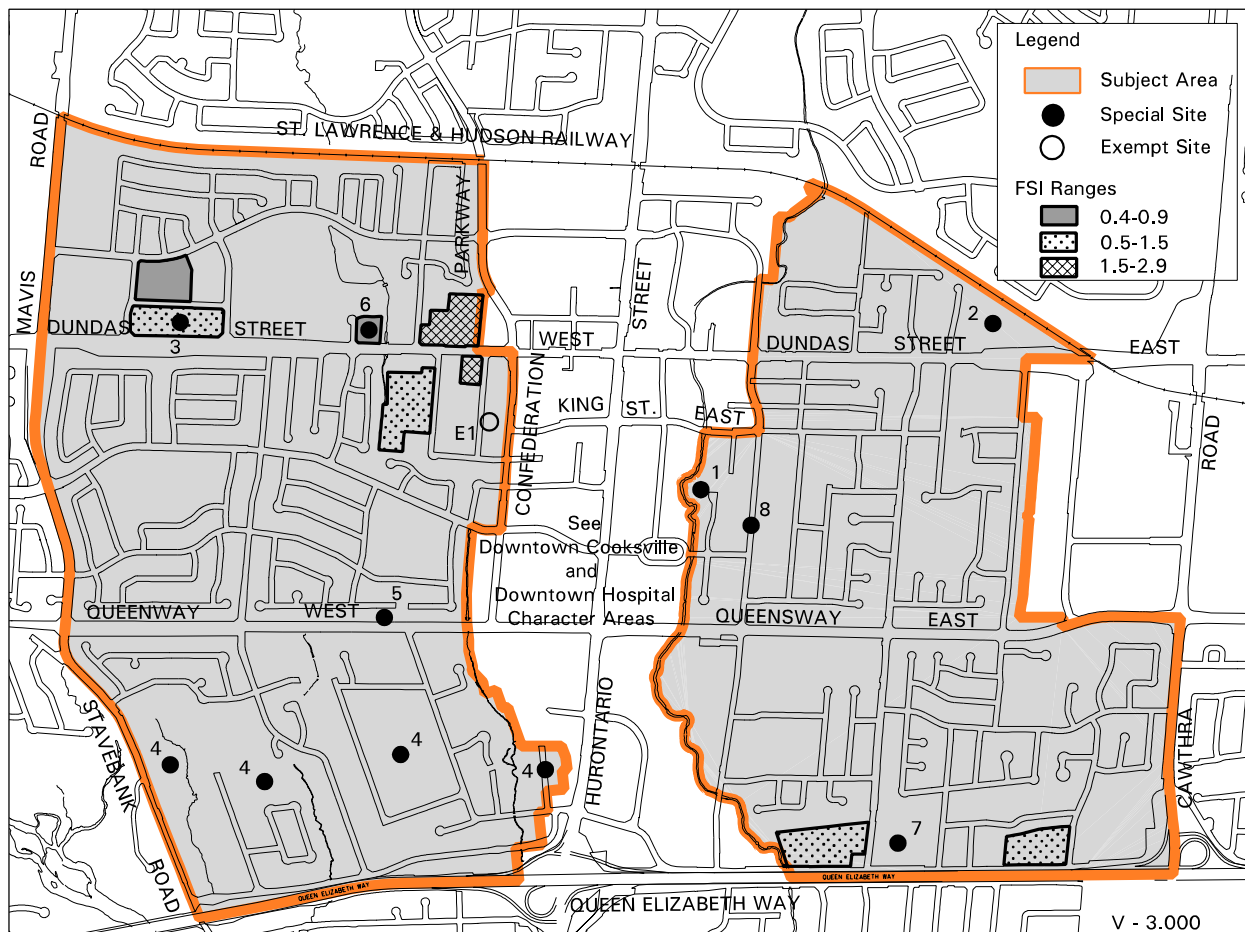
### Site 3



16.5.6.3.1 The lands identified as Exempt Site 3 are located north of Lorne Park Road, west of Birchview Drive and south of Lorne Park Road, east of Albertson Crescent.

16.5.6.3.2 Notwithstanding the provisions of the Mixed Use designation, a motor vehicle repair garage will also be permitted.

## 16.6 Cooksville



Map 16-6: Cooksville Neighbourhood Character Area

### 16.6.1 Land Use

16.6.1.1 The Residential Low Density I designation, for the area bounded by the Queensway West, Hurontario Street, the Queen Elizabeth Way and Stavebank Road, permits only detached dwellings.

16.6.1.2 Lands designated Residential Medium Density will also permit low-rise apartment dwellings.

16.6.1.3 For lands designated Residential High Density a maximum building height of eight storeys is permitted unless otherwise specified in the Urban Design policies of this Plan.

16.6.1.4 Notwithstanding the Business Employment policies of this Plan, only the following uses will be permitted:

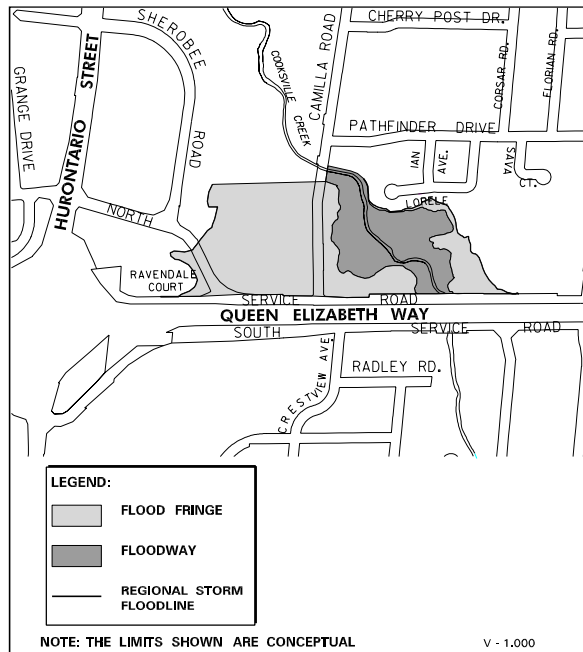
- a. **secondary office**; and
- b. outdoor storage and display areas related to the **secondary office** uses.

### 16.6.2 Urban Design

16.6.2.1 Sites fronting on Dundas Street and Hurontario Street, immediately outside of the Character Area, should serve as transitional areas to

the Character Areas, where a greater intensity of mixed use development should occur.

### 16.6.3 Environmental Planning Area



Map 16-6.1: Cooksville Creek Floodplain Management Concept

16.6.3.1 The lands within the Cooksville Creek floodplain shown above are located generally between the North Service Road and where the Cooksville Creek crosses Camilla Road. The area subject to these policies within the Cooksville Neighbourhood, is generally located east of Cooksville Creek. The lands shown are subject to the **two-zone floodplain management concept**, which divides the regulatory floodplain into two portions known as the floodway and the flood fringe. The limits of the flood fringe and the floodway are conceptual, the exact limits of which will be determined through further study.

16.6.3.2 Notwithstanding the Natural Hazards policies of this Plan, the following policies will apply to those lands within the regulatory floodplain:

- a. the lands within the floodway are designated Residential High Density, Office, Greenbelt and Utility. Notwithstanding the Residential High Density, Office, Greenbelt and Utility policies of this Plan, the following uses will be permitted

within the floodway subject to the satisfaction of the City and Credit Valley Conservation:

- flood and/or erosion works;
  - facilities which by their nature must locate near water or traverse **watercourse** (i.e. bridges, storm sewer outlets and stormwater management facilities); and
  - passive recreation activities; and
- b. floodway lands will be zoned in an appropriate hazard category in the implementing zoning by-law;
  - c. the lands within the flood fringe are subject to their respective land use designations and the following additional policies:
    - development may be permitted provided the use, building or structure is flood proofed to the regulatory flood level as required by Credit Valley Conservation;
    - ingress/egress for all development located in the flood fringe will be such that emergency vehicular and pedestrian movement is not prevented during times of flooding in order that safe access/evacuation is ensured. The determination of safe access shall be made by Credit Valley Conservation based on the depth and velocity factors;
    - enclosed underground parking will be subject to the installation of stringent flood proofing measures to the elevation of the regulatory flood level; and
    - the zoning of lands may utilize a holding zone to provide direction as to future permitted uses while ensuring flood proofing and safe access are addressed prior to development to the satisfaction of the City and Credit Valley Conservation. The Zoning By-law will be amended to remove the holding symbol when the requirements for flood proofing, the provision of safe



access to the proposed development and a detailed spill assessment and a financing agreement for the reconstruction of the culvert at the QEW has been completed to the satisfaction of the City, Credit Valley Conservation and the Ministry of Transportation; and

d. the following uses will not be allowed within the floodplain:

- institutional services such as hospitals, nursing homes, and schools where there would be a significant threat to the safety of inhabitants involved in an emergency evacuation situation as a result of flooding or failure of flood proofing measures;
- new uses associated with the manufacture, storage, disposal and/or consumption of hazardous substances or the treatment, collection and disposal of sewage, which would pose an unacceptable threat to public safety if they were to escape their normal containment/use as a result of flooding or failure of flood proofing measures; and
- emergency services such as those provided by fire, police, and ambulance stations and electrical substations, which would be impaired during a flood emergency as a result of flooding or failure of flood proofing measures.

## 16.6.4 Transportation

16.6.4.1 All development applications along the north side of Dundas Street West, between Parkerhill Road and Confederation Parkway, will require an internal access scheme, preferably interconnecting the properties to reduce ingress and egress to Dundas Street West.

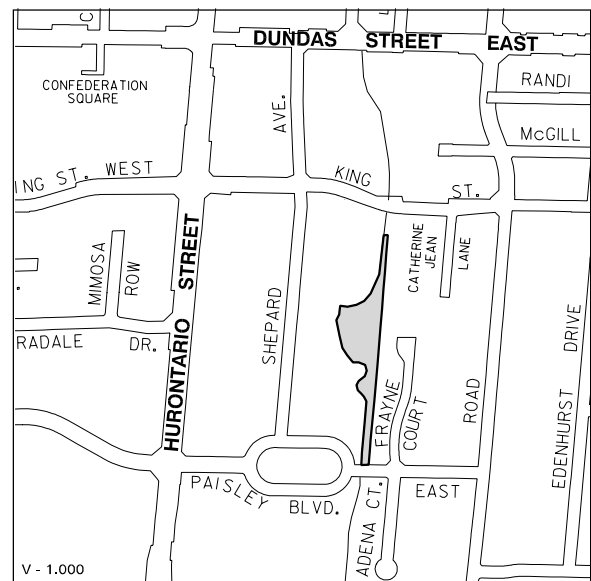
16.6.4.2 The City of Mississauga Parking Strategy, carried out in 1993, made recommendations on the short, medium and long term strategies to meet the

parking requirements and to achieve urban design objectives. These recommendations will be considered in conjunction with development applications and implementation of High Occupancy Vehicle (HOV) lanes on Hurontario Street and Dundas Street East.

## 16.6.5 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 16.6.5.1 Site 1



16.6.5.1.1 The lands identified as Special Site 1 are located east of Shepard Avenue between King Street East and Paisley Boulevard East.

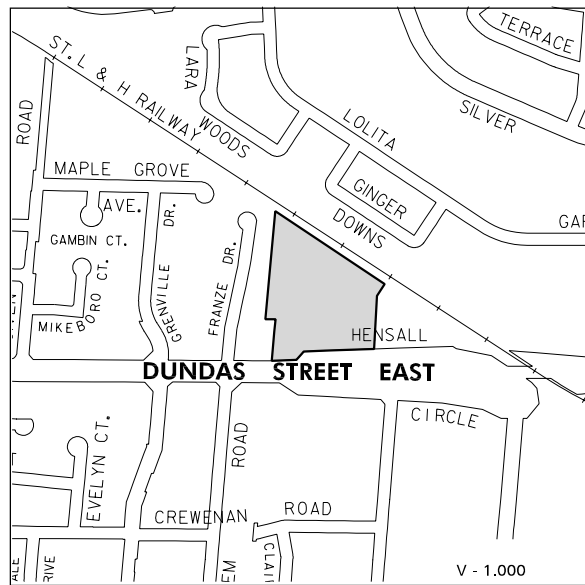
16.6.5.1.2 Notwithstanding the provisions of the Greenbelt designation, the following additional policies will apply:

- any proposal to alter the natural alignment of the Cooksville Creek and associated Greenbelt lands through filling or channelization will require the appropriate approvals from Credit Valley Conservation and the City of Mississauga. The

final design of such alteration works will address the following:

- criteria for erosion measures; and
  - incorporation of the recommendations of the Cooksville Creek Rehabilitation Study for the reach of the Cooksville Creek between King Street East and Paisley Boulevard East; and
- b. in the event that structural management options are not feasible and, subject to a detailed planning design and supporting engineering studies for the Special Site 1 lands, a **two-zone floodplain management concept**, or combination of a **two-zone floodplain management concept** and structural management options, may be implemented;
- c. the limits of the development will be determined through detailed studies to address limits of flooding. Lands not suitable for development will be redesignated to Greenbelt; and
- d. the preparation of a concept plan promoting the integrated redevelopment of the existing residential lots will be required in conjunction with any redevelopment applications. The concept plan will be required to address the following:
- naturalization of the Cooksville Creek and retention of existing vegetation;
  - an alternate street access and design to limit access to the lands from only King Street East; and
  - housing forms that provide for the stepping of densities and building heights from higher to the north and west ends of the site to lower to the south and east ends of the site.

## 16.6.5.2 Site 2

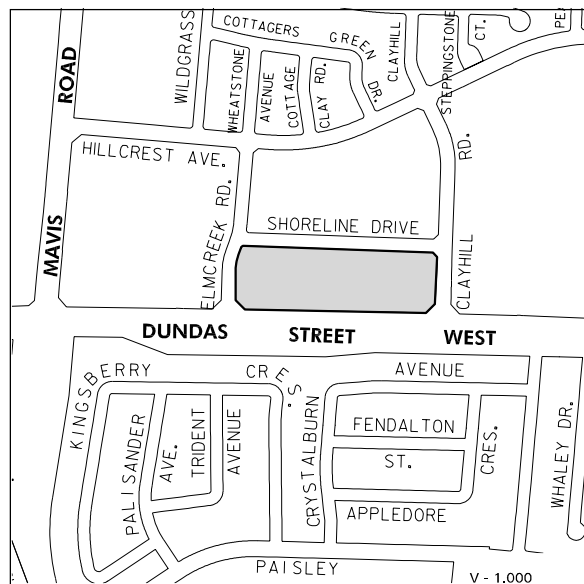


16.6.5.2.1 The lands identified as Special Site 2 are located north of Hensall Circle, abutting the CNR tracks.

16.6.5.2.2 Notwithstanding the provisions of the Residential Medium Density designation, the following additional policies will apply:

- a. the development of the site is to incorporate any planned road improvements to Dundas Street East and Hensall Circle. Access for any development will be restricted to Hensall Circle;
- b. any development is to minimize impact on the abutting low density development on Franze Drive through a sensitive design of units, building heights, open space, parking and laneways on the site, and protect and enhance existing vegetation along the westerly limits of the site; and
- c. unit layouts for the site should minimize privacy/acoustic fencing abutting the public street. Elevations facing the public street should be enhanced architecturally.

### 16.6.5.3 Site 3

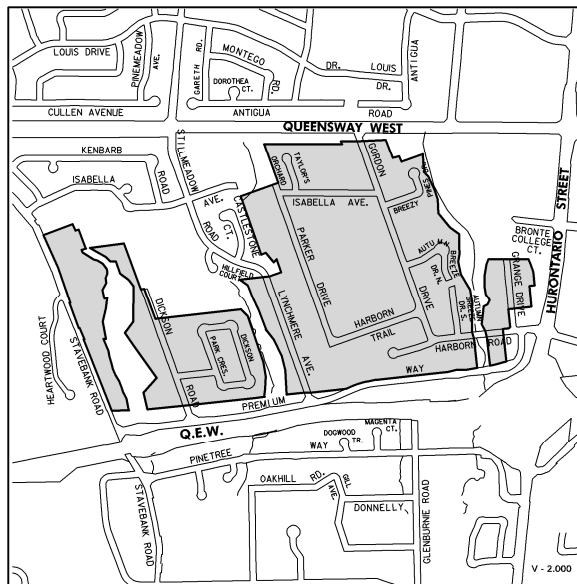


16.6.5.3.1 The lands identified as Special Site 3 are located on the north side of Dundas Street West, east of Mavis Road, in the former Canada Brick quarry site.

16.6.5.3.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:

- a. all forms of horizontal multiple dwellings as well as low-rise apartment dwellings and townhouse dwellings will be permitted in combination with Residential High Density uses provided the overall density achieves the minimum permitted **floor space index (FSI)** of 1.0 for the subject lands; and
- b. a concept plan for the entire property will be submitted by the applicant to ensure that the overall site development achieves the minimum density for the subject lands, an appropriate built form relationship defines the Dundas Street West frontage, and acceptable transitions in built form are established to surrounding land uses.

### 16.6.5.4 Site 4



16.6.5.4.1 The lands identified as Special Site 4 are located west of Hurontario Street, south of Queensway West.

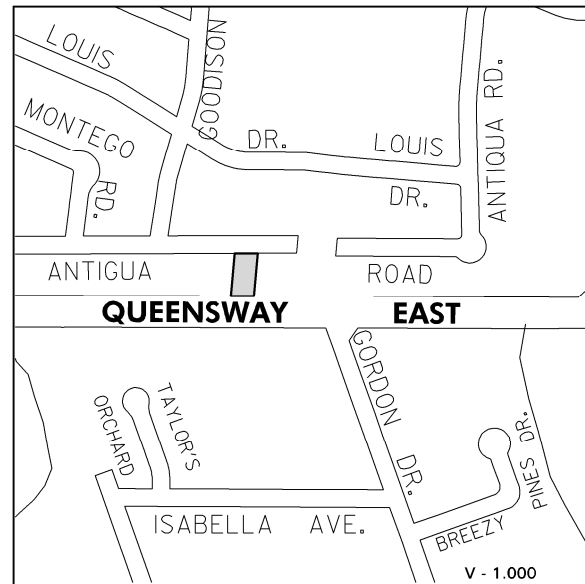
16.6.5.4.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

- a. the generous front, rear and side yard setbacks will be preserved;
- b. landscaping and natural features will be protected, enhanced, restored, and where possible, expanded;
- c. existing grades and drainage conditions will be preserved unless modified by a City approved drainage plan;
- d. new housing will be encouraged to fit the scale and character of the surrounding development, and take advantage of the features of a particular site, e.g., topography, contours, mature vegetation;
- e. garages should be recessed or be located behind the main face of the house, or in the rear of the property;
- f. new development will have minimal impact on adjacent development with respect to overshadowing and overlook;

- g. buildings will be encouraged to be one to two storeys in height. The design of the building should de-emphasize the height of the house and be designed as a composition of small architectural elements, e.g. projecting dormers and bay windows;
- h. hard surface areas in the front yard will be less than half of the front yard;
- i. existing high quality trees will be preserved to maintain the existing mature nature of these areas;
- j. house designs which fit with the scale and character of the local area, and take advantage of the particular site are encouraged. The use of standard, repeat designs is discouraged; and
- k. building mass, side yards and rear yards should respect and relate to those of adjacent lots.

16.6.5.4.3 Lot frontages should generally not be decreased. Consideration may be given to consents for lot frontages exceeding 50 m provided the resulting lots are in keeping with the frontage of adjacent lots.

#### 16.6.5.5 Site 5



16.6.5.5.1 The lands identified as Special Site 5 are located on the north side of Queensway West, west of Confederation Parkway and are municipally known as 255 Queensway West.

16.6.5.5.2 Notwithstanding the provisions of the Residential Low Density II designation and Section 19.9, Temporary Use By-law, the following additional policies will apply:

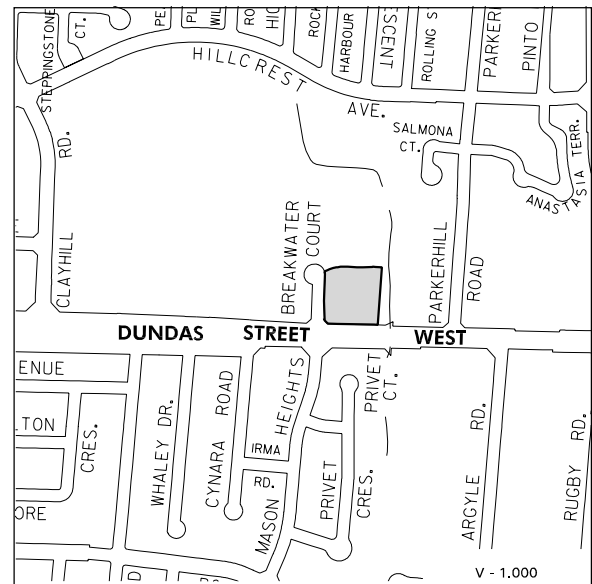
- a. City Council may pass By-laws to authorize the temporary use of the land, building and structure for a dental office that is otherwise prohibited by the Plan or the Zoning By-law, as permitted by the provisions of the *Planning Act*;
- b. the temporary use may be permitted by a Temporary Use By-law to allow the use of land, building and structure for a dental office until redevelopment of the land for a use permitted by this Plan is warranted by future market conditions; and
- c. the following conditions will apply to the use permitted by the Temporary Use By-law:
  - Council may by by-law authorize the temporary use of land, buildings and structures for a period of three years from the day of the passing of the by-law (2007 Mar. 28) as per the *Planning Act*;



- Council may by by-law grant further periods of not more than three years each during which the temporary use is authorized as per the *Planning Act*;
- the temporary use should not continue for more than a total of ten years from the day of the passing of the by-law (2007 Mar. 28);
- the temporary use will not be considered a legal, non-conforming use of the property or building; and
- the temporary use will not jeopardize the eventual planned use for Residential Low Density II purposes with vehicular access limited to Antigua Road.

#### 16.6.5.6

#### Site 6

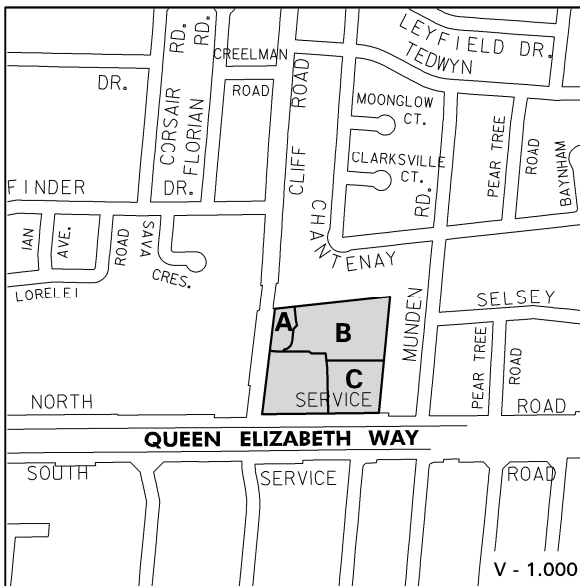


16.6.5.6.1 The lands identified as Special Site 6 are located at the northeast corner of Dundas Street West and Breakwater Court.

16.6.5.6.2 Notwithstanding the provisions of the Residential Medium Density designation, development up to a **floor space index (FSI)** of 1.1 will be permitted. A concept plan for the entire property will be submitted by the applicant to ensure that the overall site development achieves an appropriate built form relationship, which defines the Dundas Street West frontage, and acceptable transitions in built form are established to the surrounding land uses.

### 16.6.5.7

### Site 7



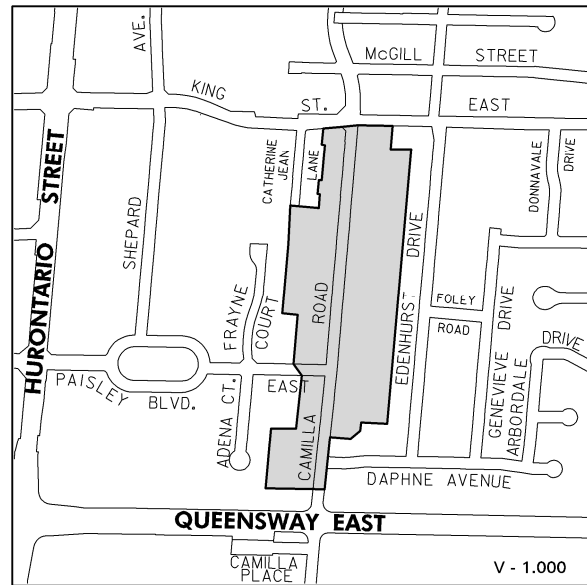
16.6.5.7.1 The lands identified as Special Site 7 are located at the northeast corner of North Service Road and Cliff Road.

16.6.5.7.2 Notwithstanding the Residential Medium Density, Residential High Density, Mixed Use and Convenience Commercial designations, the following additional policies will apply:

- The maximum **floor space index (FSI)** for the entire site shall not exceed 2.5 times the site area;
- A maximum 11 storey retirement dwelling having a maximum gross floor area of 10 702 m<sup>2</sup> shall be the only permitted use on lands identified as Area A;
- The maximum number of townhouse dwellings permitted on the lands identified as Area B shall not exceed 54 units; and
- A one storey commercial building permitting retail and personal service uses, having a maximum gross floor area of 1 075 m<sup>2</sup> shall be permitted on lands identified as Area C.

### 16.6.5.8

### Site 8



16.6.5.8.1 The lands identified as Special Site 8 are located on the east and west sides of Camilla Road between King Street East and Queensway East.

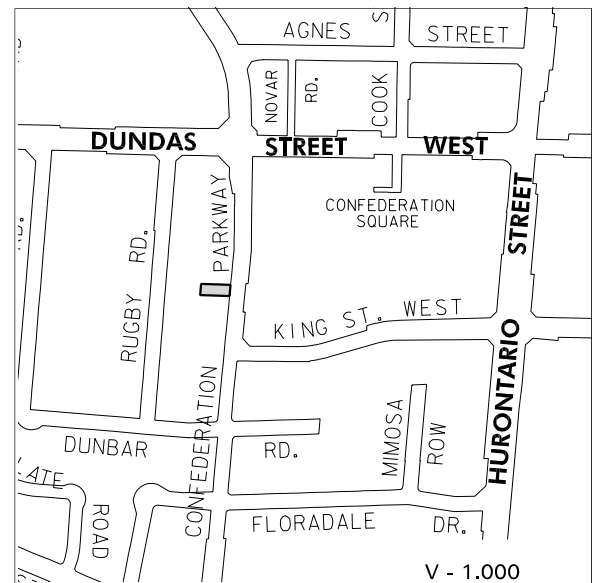
16.6.5.8.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

- the lotting, building mass, side yards and rear yards should respect and relate to those of adjacent lots;
- new lots should front onto existing public streets;
- existing grades and drainage conditions will be preserved unless modified by a City approved drainage plan;
- new housing will be encouraged to fit the scale and character of the surrounding development, and take advantage of the features of a particular site, e.g. topography, contours, mature vegetation;
- house designs which fit with the scale and character of the local area, and take advantage of the particular site are encouraged. The use of standard, repeat designs is discouraged;

- f. garages should be recessed or be located behind the main face of the house, or in the rear of the property;
- g. new development will have minimal impact on adjacent development with respect to shadowing and overlook;
- h. buildings will be encouraged to be one to two storeys in height. The design of the building should de-emphasize the height of the house and be designed as a composition of small architectural elements, e.g. projecting dormers and bay windows;
- i. hard surface areas in the front yard will be less than half of the front yard; and
- j. existing high quality trees will be preserved to maintain the existing mature nature of the area.

## 16.6.6 Exempt Site Policies

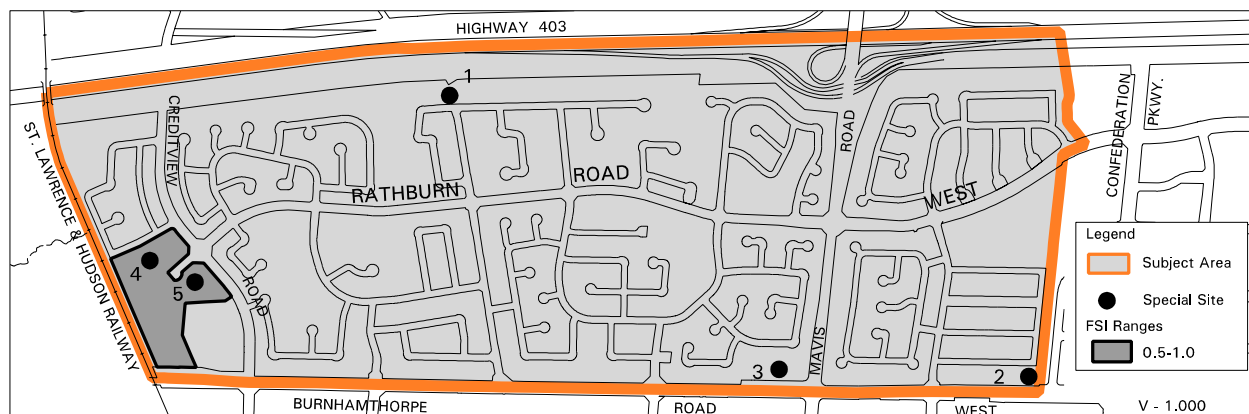
### 16.6.6.1 Site 1



16.6.6.1.1 The lands identified as Exempt Site 1 are located on the west side of Confederation Parkway, south of Dundas Street West.

16.6.6.1.2 Notwithstanding the provisions of the Residential Low Density II designation, offices will also be permitted.

## 16.7 Creditview



Map 16-7: Creditview Neighbourhood Character Area

### 16.7.1 Urban Design Policies

16.7.1.1 "Gateway" treatments including special landscape features, upgraded planting, circulation and pedestrian linkages, and attractive built form will be promoted in the easterly section of the Character Area adjacent to the Downtown Core to complement the urban design vision for the Downtown Core and to encourage the Downtown Core to be used by area residents.

16.7.1.2 Where existing noise attenuation walls or rear yard privacy fencing are exposed to public streets, supplementary planting and upgraded landscape features should be added where feasible, as a condition of development or road reconstruction. Mississauga will encourage landowners to coordinate the eventual replacement of fences to enhance the appearance of the area from the street. Special consideration should be given to Rathburn Road West and Burnhamthorpe Road West as they function as important entrance ways into the Downtown Core.

### 16.7.2 Land Use

16.7.2.1 Owners/developers will be encouraged to advise all prospective purchasers or tenants occupying new residential units within the 600 metre influence area of a chemical plant located on Mavis Road, south of Burnhamthorpe Road West, of the possibility of adverse odour emissions.

16.7.2.2 Lands designated Residential Low Density II will not permit the following uses:

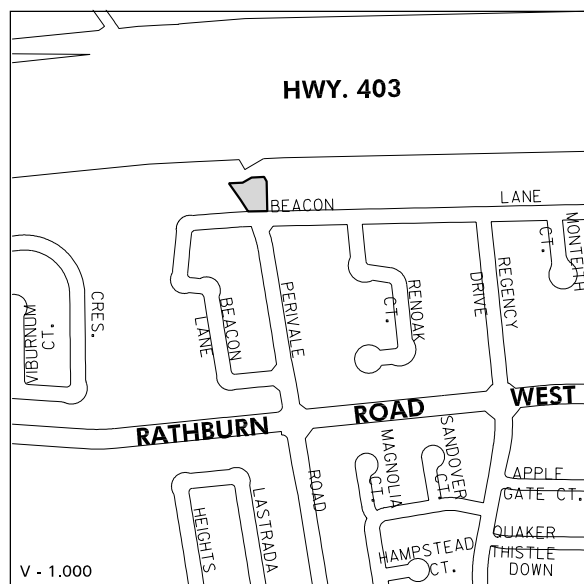
- a. triplexes, street townhouses and other forms of low-rise dwellings with individual frontages.



### 16.7.3 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 16.7.3.1 Site 1

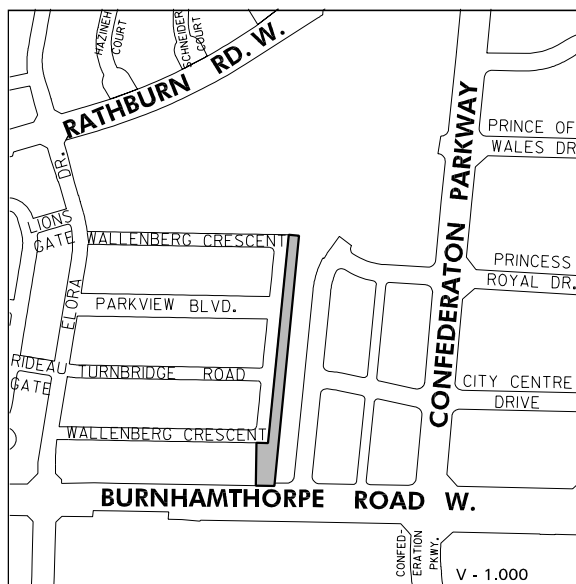


16.7.3.1.1 The property identified as Special Site 1 is located at 4265 Perivale Road and is referred to as the Crozier House. This house has been identified as a building worthy of preservation.

16.7.3.1.2 Notwithstanding the provisions of the Residential Low Density II designation, the following additional policies will apply:

- a. any development proposals will maintain and incorporate the heritage structure and any new construction or severing of property will be compatible in building form and scale with the heritage structure; and
- b. new development will be subject to site plan approval to ensure compatibility with the heritage structure and the preservation of mature trees and other significant natural features.

#### 16.7.3.2 Site 2

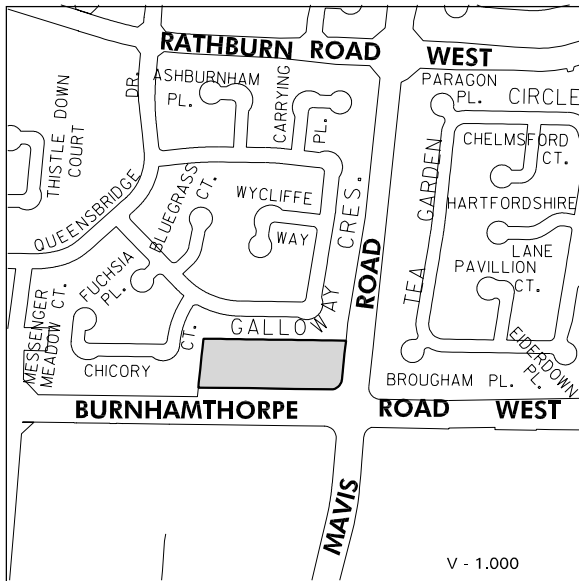


16.7.3.2.1 The lands identified as Special Site 2 are located along the east side of Wallenberg Crescent.

16.7.3.2.2 Notwithstanding the provisions of the Public Open Space designation, below-grade parking may be permitted.

### 16.7.3.3

### Site 3

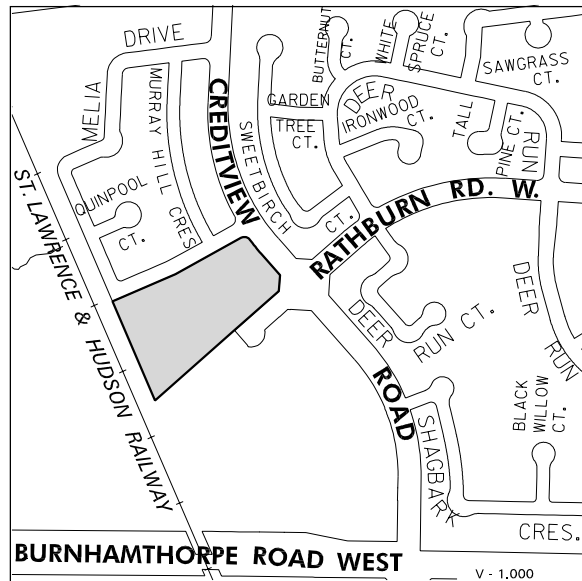


16.7.3.3.1 The lands identified as Special Site 3 are located at the northwest corner of Burnhamthorpe Road West and Mavis Road.

16.7.3.3.2 Notwithstanding the provisions of the Motor Vehicle Commercial designation, all types of restaurants will be permitted.

### 16.7.3.4

### Site 4



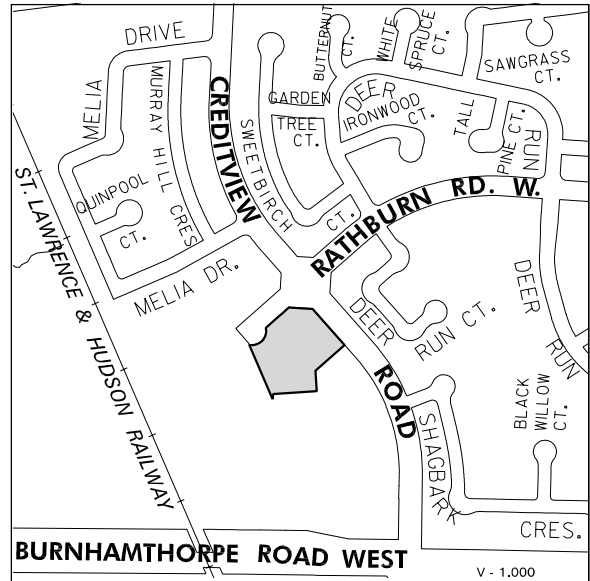
16.7.3.4.1 The lands identified as Special Site 4 are located on the west side of Creditview Road between Rathburn Road West and Melia Drive.

16.7.3.4.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:

- a. a maximum of three apartment buildings containing a maximum of 500 apartment dwelling units and an enclosed recreation centre will be permitted on the lands at a **floor space index (FSI)** of 2.09;
- b. building heights shall range from seven to ten storeys adjacent to Creditview Road to 12 storeys on the south side of Melia Drive to 18 storeys adjacent to the southern boundary of the site in the vicinity of the GO train station on the St. Lawrence and Hudson Railway; and
- c. the development of these lands will also be subject to the following:
  - no vehicular access to Melia Drive;
  - the erection of an enclosed recreation centre concurrent with the erection of the 12 storey apartment building fronting on Melia Drive; and

- provision for Rathburn Road West modifications and site access to the satisfaction of the City of Mississauga.

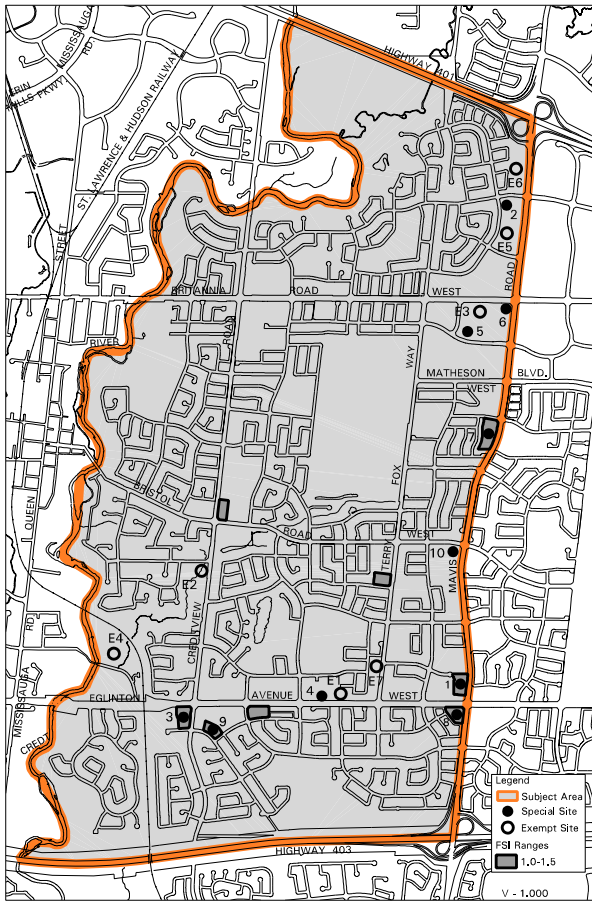
### 16.7.3.5 Site 5



16.7.3.5.1 The lands identified as Special Site 5 are located at the southwest corner of Rathburn Road West and Creditview Road.

16.7.3.5.2 Notwithstanding the provisions of the Residential High Density designation, development will be permitted up to a maximum **floor space index (FSI)** of 1.9. No building or structure will exceed a height of nine storeys, excluding any mechanical penthouse.

## 16.8 East Credit



Map 16-8: East Credit Neighbourhood Character Area

### 16.8.1 Urban Design Policies

16.8.1.1 The following urban design policies will apply to lands designated Business Employment north of Britannia Road West, west of Mavis Road:

- for the lands with direct exposure to Ivandale Drive and Bancroft Drive, upgraded building elevations will be required;
- loading bays and **waste** collection areas should not face onto Ivandale Drive or Bancroft Drive; and
- outdoor storage and display areas should not face onto Ivandale Drive or Bancroft Drive.

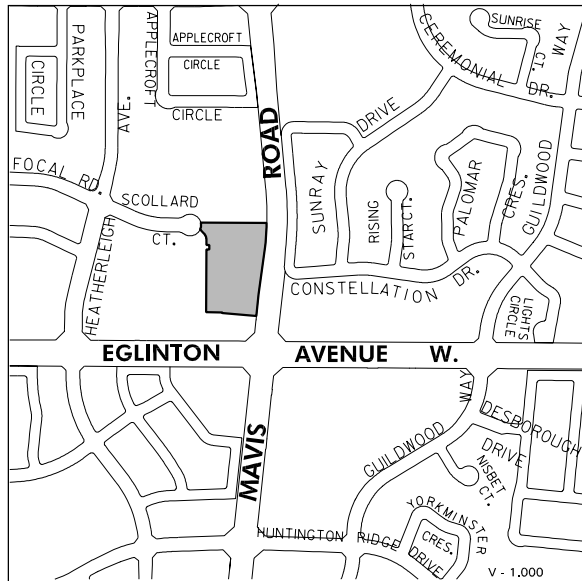
### 16.8.2 Transportation

16.8.2.1 Second Line West may be terminated north and south of Provincial Highway 401 as part of the future widening of Provincial Highway 401. The precise timing and location of these points of termination north and south of Provincial Highway 401 will be determined by the City in conjunction with the appropriate authorities.

### 16.8.3 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 16.8.3.1 Site 1

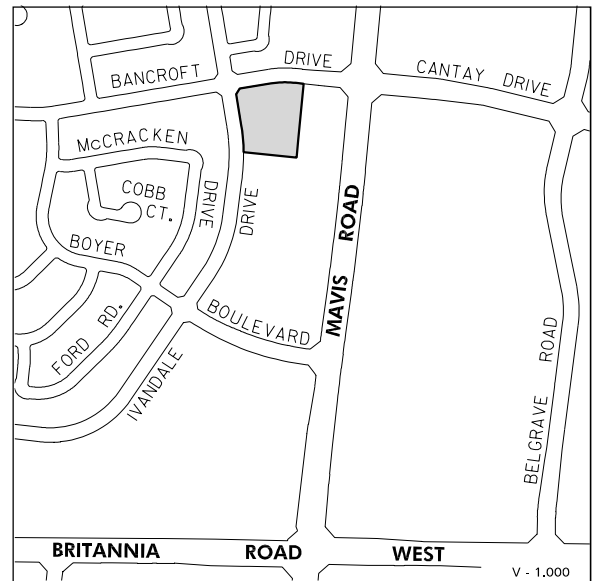


16.8.3.1.1 The lands identified as Special Site 1 are located on the west side of Mavis Road, north of Eglinton Avenue West.

16.8.3.1.2 Notwithstanding the Residential High Density designation on these lands, the following additional policies will apply:

- a. apartment buildings will be permitted up to a maximum **floor space index (FSI)** of 3.4;
- b. townhouses will be permitted; and
- c. the maximum apartment building height will be 18 storeys.

#### 16.8.3.2 Site 2



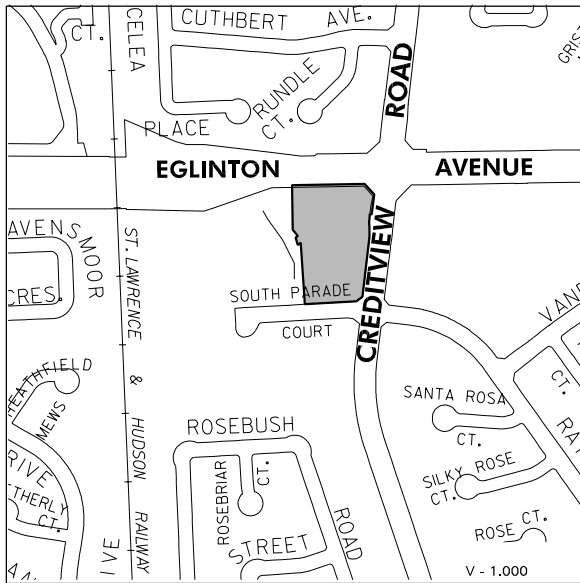
16.8.3.2.1 The lands identified as Special Site 2 are located at the southeast corner of Bancroft Drive and Ivandale Drive.

16.8.3.2.2 Notwithstanding the policies of this Plan, motor vehicle sales, leasing, rental and repair will also be permitted.



### 16.8.3.3

### Site 3



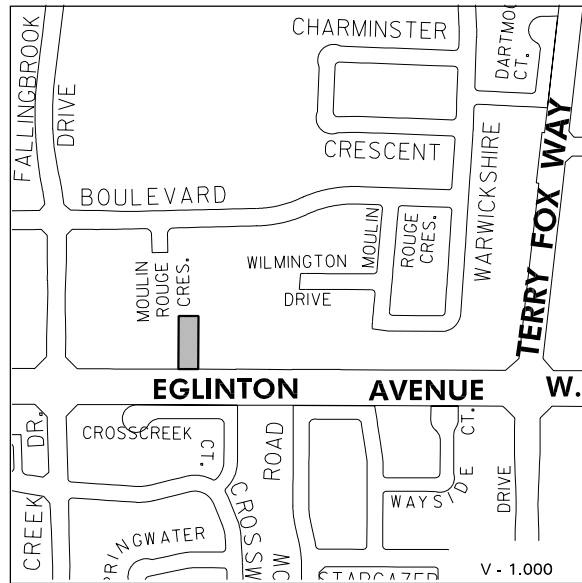
16.8.3.3.1 The lands identified as Special Site 3 are located at the southwest corner of Eglinton Avenue West and Creditview Road.

16.8.3.3.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:

- townhouse dwellings will be permitted;
- the maximum apartment building height will be 12 storeys; and
- apartment dwellings at a maximum **floor space index (FSI)** of 1.77 will be permitted.

### 16.8.3.4

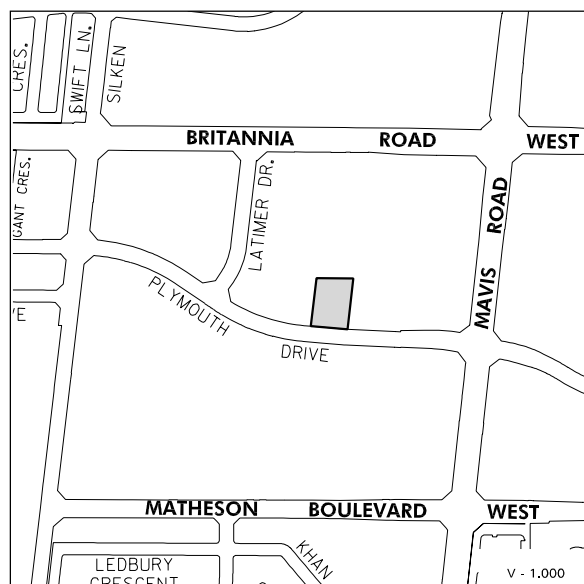
### Site 4



16.8.3.4.1 The lands identified as Special Site 4 are located on the north side of Eglinton Avenue West, east of Fallingbrook Drive.

16.8.3.4.2 Notwithstanding the provisions of the Residential Medium Density designation, a veterinary clinic will also be permitted.

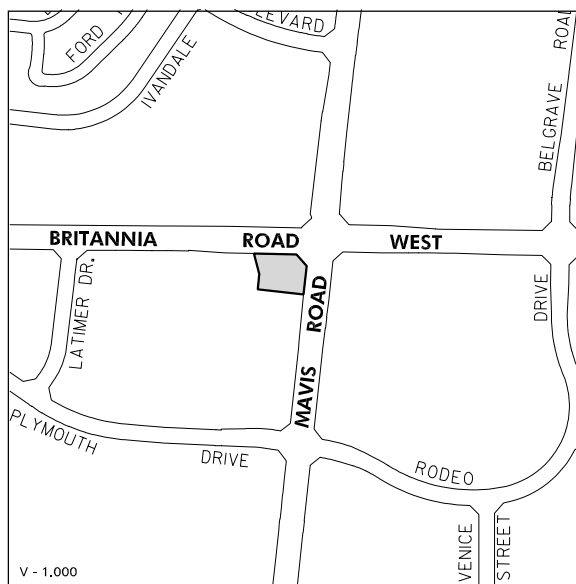
### 16.8.3.5 Site 5



16.8.3.5.1 The lands identified as Special Site 5 are located on the north side of Plymouth Drive, east of Latimer Drive.

16.8.3.5.2 Notwithstanding the provisions of the Mixed Use designation, a motor vehicle body repair facility will also be permitted.

### 16.8.3.6 Site 6

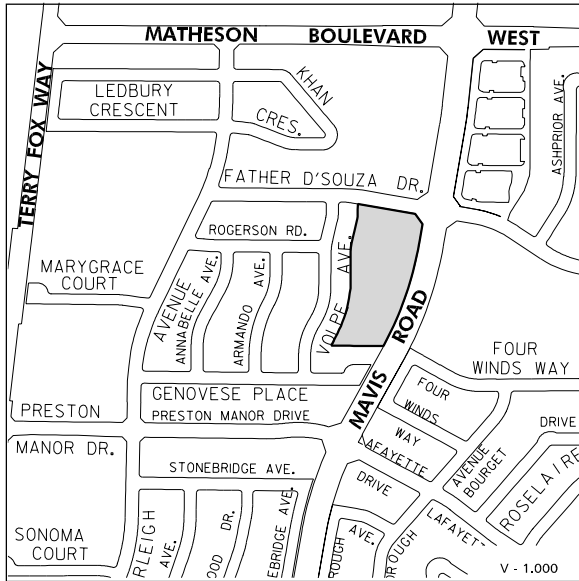


16.8.3.6.1 The lands identified as Special Site 6 are located at the southwest corner of Britannia Road West and Mavis Road.

16.8.3.6.2 Notwithstanding the provisions of the Mixed Use designation, motor vehicle commercial uses will also be permitted.

### 16.8.3.7

### Site 7



16.8.3.7.1 The lands identified as Special Site 7 are located on the west side of Mavis Road, north of Preston Manor Drive.

16.8.3.7.2 Notwithstanding the provisions of the Residential High Density designation, development of the property will be permitted up to a maximum **floor space index (FSI)** of 1.75.

### 16.8.3.8

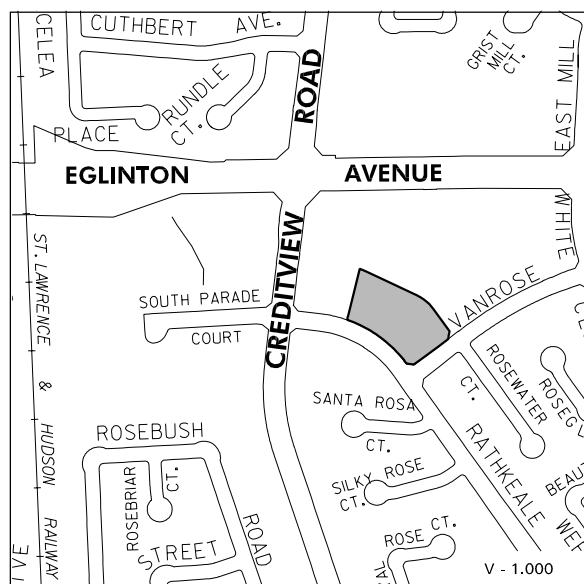
### Site 8



16.8.3.8.1 The lands identified as Special Site 8 are located at the southwest corner of Eglinton Avenue West and Mavis Road.

16.8.3.8.2 Notwithstanding the provisions of the Residential High Density designation, apartment dwellings at a maximum **floor space index (FSI)** of 3.7 will be permitted.

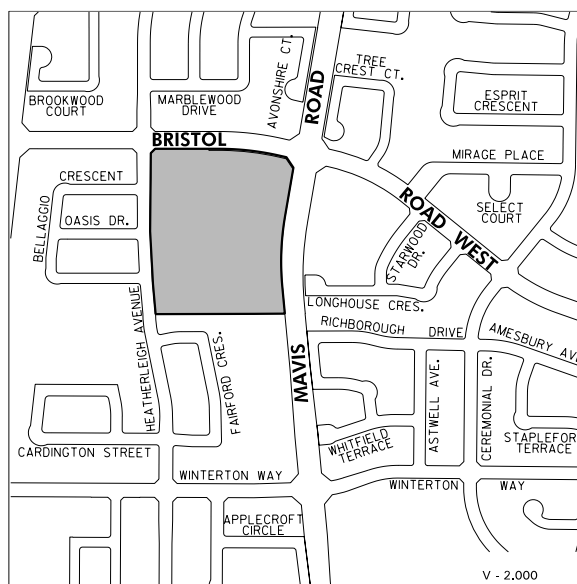
### 16.8.3.9 Site 9



16.8.3.9.1 The lands identified as Special Site 9 are located on the north side of Rathkeale Road, east of Creditview Road.

16.8.3.9.2 Notwithstanding the provisions of the Residential High Density designation, a maximum **floor space index (FSI)** of 2.1 will be permitted.

### 16.8.3.10 Site 10

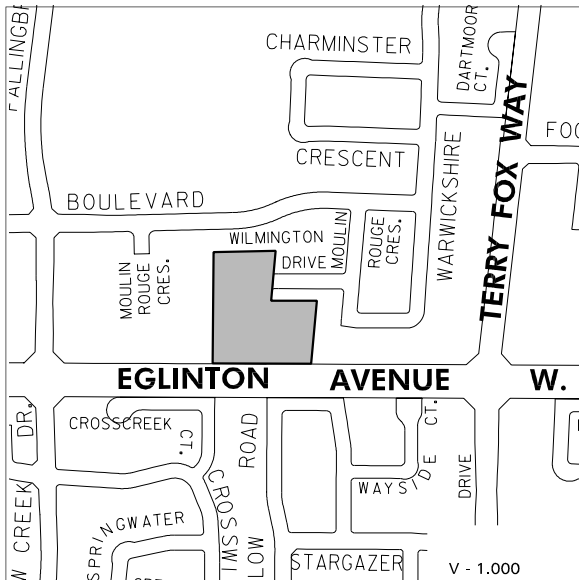


16.8.3.10.1 The lands identified as Special Site 10 are located at the southwest corner of Bristol Road West and Mavis Road.

16.8.3.10.2 Notwithstanding the policies of this Plan, a service station, gas bar and car wash may be permitted.

## 16.8.4 Exempt Site Policies

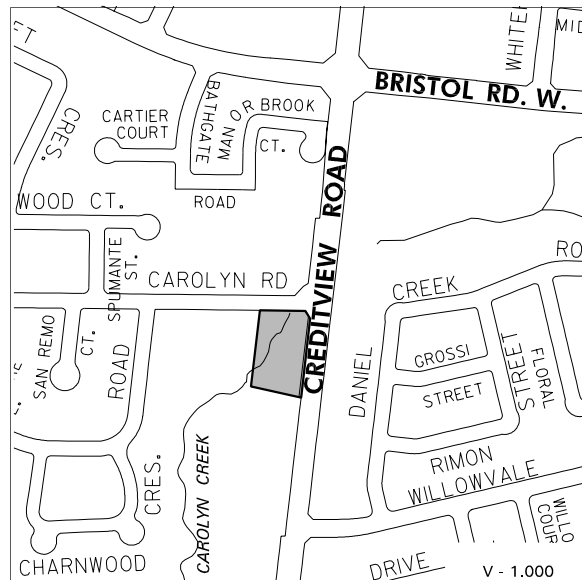
### 16.8.4.1 Site 1



16.8.4.1.1 The lands identified as Exempt Site 1 are located on the north side of Eglinton Avenue West, west of Terry Fox Way.

16.8.4.1.2 Notwithstanding the provisions of the Residential Medium Density designation, a garden centre including the sale and storage of firewood, Christmas trees and landscaping materials, excluding nursery stock, and residential accommodation accessory to the operation of the garden centre use will also be permitted for an interim period of time, until the lands are redeveloped in accordance with the Residential Medium Density I designation.

### 16.8.4.2 Site 2



16.8.4.2.1 The lands identified as Exempt Site 2 are located on the southwest corner of Creditview Road and Carolyn Road.

16.8.4.2.2 Notwithstanding the provisions of the Residential Low Density II and Greenbelt designations, a veterinary clinic within the existing detached dwelling will be permitted and a bird sanctuary structure on public greenbelt accessory to the adjacent veterinary clinic will also be permitted.



### 16.8.4.3

### Site 3

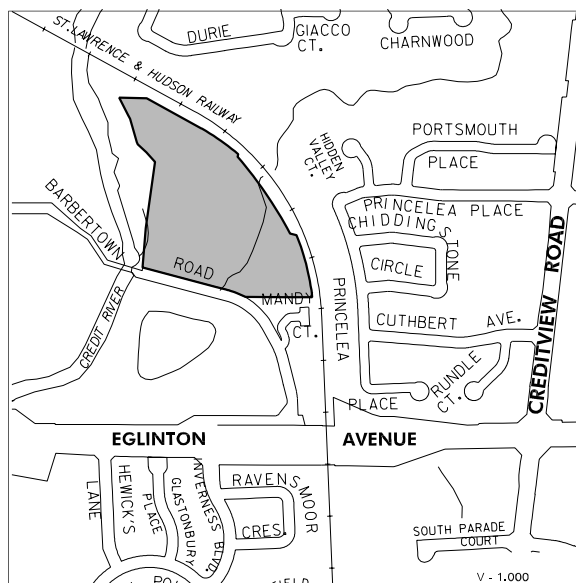


16.8.4.3.1 The lands identified as Exempt Site 3 are located on the south side of Britannia Road West between Terry Fox Way and Mavis Road.

16.8.4.3.2 Notwithstanding the provisions of the Mixed Use designation, a truck terminal with a maximum Gross Floor Area (GFA) of 510 m<sup>2</sup> will also be permitted.

### 16.8.4.4

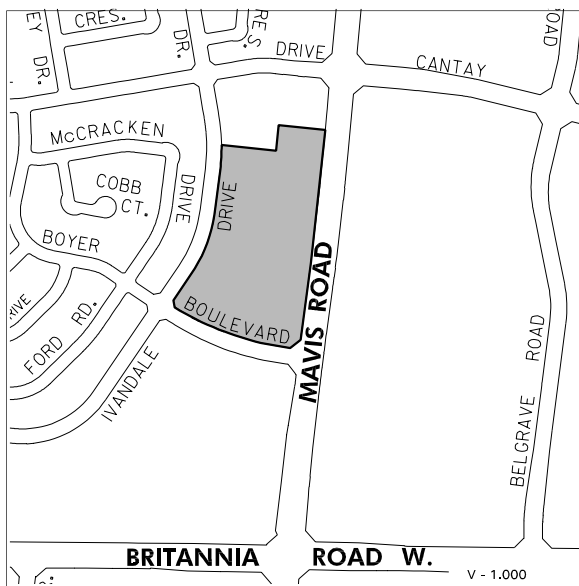
### Site 4



16.8.4.4.1 The lands identified as Exempt Site 4 are located on the north side of Barbertown Road immediately east of the Credit River.

16.8.4.4.2 Notwithstanding the provisions of the Greenbelt designation, existing residential dwellings will be permitted to remain subject to the policies of the Credit Valley Conservation.

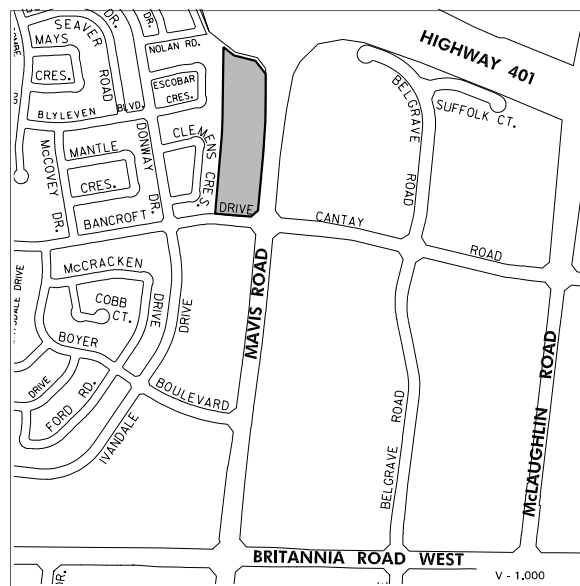
#### 16.8.4.5 Site 5



16.8.4.5.1 The lands identified as Exempt Site 5 are located on the west side of Mavis Road, north of Boyer Boulevard.

16.8.4.5.2 Notwithstanding the provisions of the Business Employment designation, motor vehicle sales, rental and repair establishments and establishments for minor motor vehicle repairs will also be permitted. Entertainment, recreation and sports facilities as well as broadcasting, communication and public utility rights-of-way, will not be permitted. Noise sensitive land uses, as defined in the Provincial Policy Statement and this Plan, will not be permitted.

#### 16.8.4.6 Site 6



16.8.4.6.1 The lands identified as Exempt Site 6 are located at the northwest corner of Mavis Road and Bancroft Drive.

16.8.4.6.2 Notwithstanding the provisions of the Business Employment designation, motor vehicle sales, service and repair establishments will also be permitted.

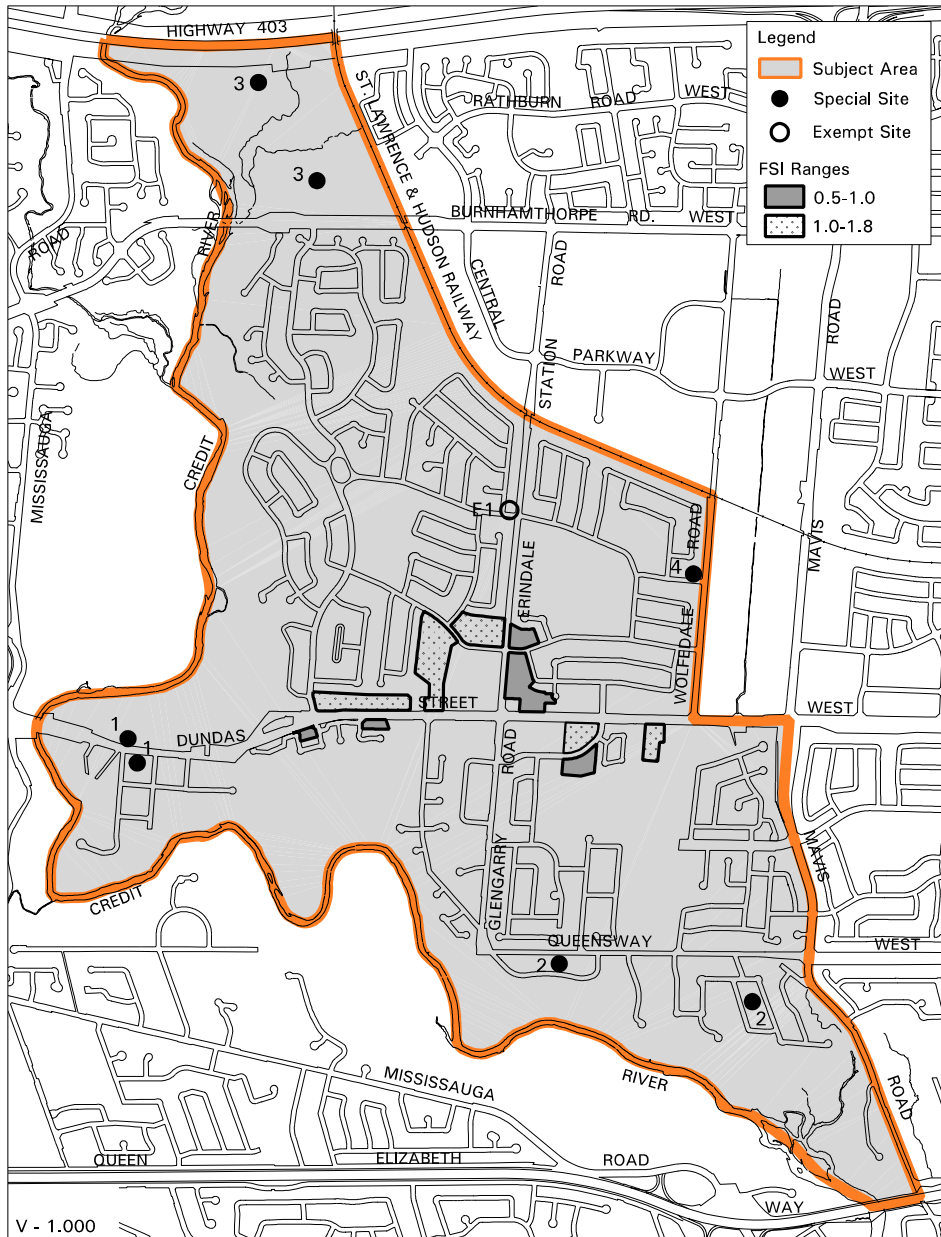
#### 16.8.4.7 Site 7



16.8.4.7.1 The lands identified as Exempt Site 7 are located on the west side of Terry Fox Way, north of Eglinton Avenue West.

16.8.4.7.2 Notwithstanding the provisions of the Residential Low Density II designation, a dental office will also be permitted.

## 16.9 Erindale



Map 16-9: Erindale Neighbourhood Character Area

### 16.9.1 Land Use

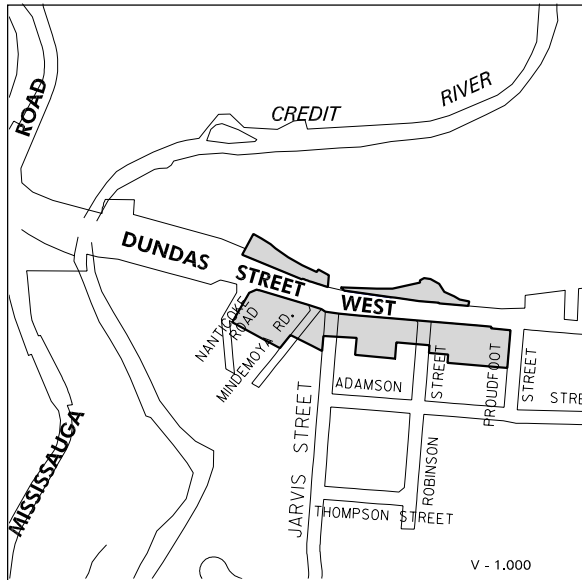
16.9.1.1 Notwithstanding the provisions of the Residential Low Density I designation, the Residential Low Density I designation permits only detached dwellings.

16.9.1.2 Notwithstanding the provisions of the Residential Medium Density designation, the Residential Medium Density designation permits only townhouse dwellings.

## 16.9.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 16.9.2.1 Site 1



- d. for those properties east of Mindemoya Road, on-site parking will consist only of surface parking and will be provided in the rear yard only;
- e. the closed road allowance for Nanticoke Road will provide access to parking and form part of a pedestrian walkway to the Credit River valley;
- f. notwithstanding the provisions of the Mixed Use designation, commercial uses and all types of restaurants will not be permitted within Special Site 1 lands on the south side of Dundas Street West, west of Robinson Street extending westerly to include the lands at the southwest corner of Dundas Street West and Mindemoya Road; and
- g. buildings should have a minimum of two storeys and a maximum of three storeys in height.

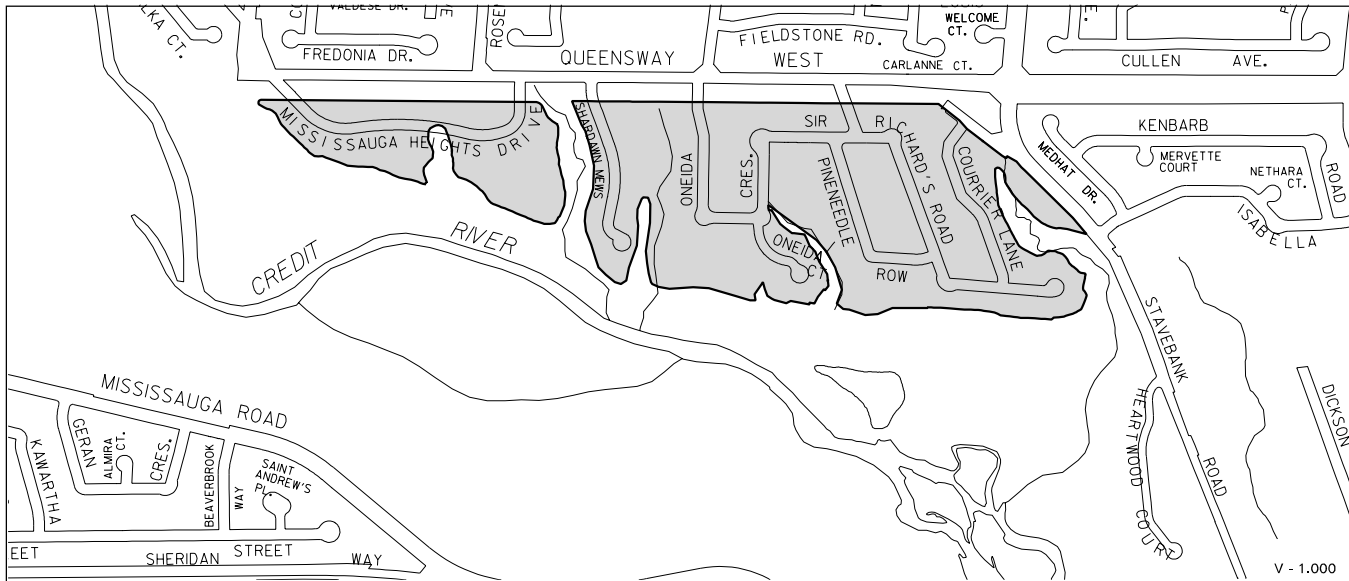
16.9.2.1.1 The lands identified as Special Site 1 are located along the north and south sides of Dundas Street West, east of the Credit River and extend easterly from Nanticoke Road to Proudfoot Street.

16.9.2.1.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. any conversion of existing residential units to office uses will maintain a residential appearance in keeping with the character of the existing dwellings in the surrounding area;
- b. any additions or alterations of existing buildings will be sensitive to the village theme of the area, and will be largely confined to the rear of the property;
- c. minimal signage will be permitted and the design of such signage and external lighting will be compatible with the residential character of the area;



## 16.9.2.2 Site 2

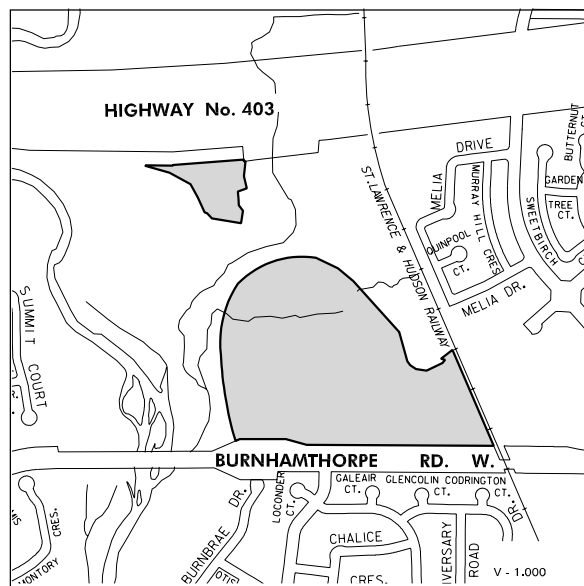


16.9.2.2.1 The lands identified as Special Site 2 are located south of Queensway West, west of Stavebank Road.

16.9.2.2.2 Notwithstanding the provisions of the Residential Low Density I designation, the following additional policies apply:

- a. preserve and enhance the generous front, rear and side yard setbacks;
- b. ensure that existing grades and drainage conditions are preserved;
- c. encourage new housing to fit the scale and character of the surrounding area, and take advantage of the features of a particular site, i.e. topography, contours, mature vegetation;
- d. garages should be recessed or located behind the main face of the house. Alternatively, garages should be located in the rear of the property;
- e. ensure that new development has minimal impact on its adjacent neighbours with respect to overshadowing and overlook;
- f. encourage buildings to be one to two storeys in height. The design of the building should de-emphasize the height of the house and be designed as a composition of small architectural elements, i.e. projecting dormers and bay windows;
- g. reduce the hard surface areas in the front yard;
- h. preserve existing mature high quality trees to maintain the existing mature nature of these areas; and
- i. the lots fronting on Mississauga Heights Drive will be required to have a minimum frontage of 30 m and a minimum area of 1 400 m<sup>2</sup>.

### 16.9.2.3 Site 3



16.9.2.3.1 The lands identified as Special Site 3 are generally located north of Burnhamthorpe Road West, east of the Credit River. The site is to be developed as part of a City Garden Park with a series of display gardens, demonstration exhibits, interpretive sites and appropriate buildings linked together by a system of walks and trails.

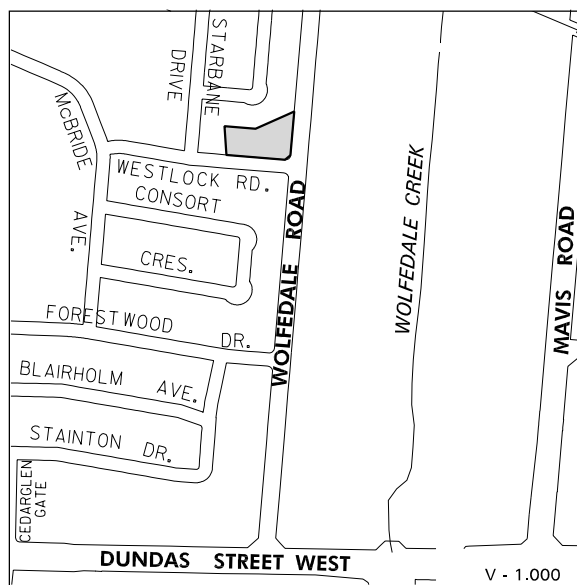
16.9.2.3.2 Notwithstanding the provisions of the Public Open Space and Greenbelt designations, the following additional policies will apply:

- a. in addition to the uses permitted in the Public Open Space designation, the following uses will also be permitted:
  - offices;
  - retail uses that are accessory to the City Garden Park;
  - conference centre;
  - banquet hall;
  - conservatory/greenhouse complex; and
  - commercial school; and
- b. any proposed structures will be limited in size and to a design compatible with the existing

structures and will be designed and located in a manner that minimizes the removal of forest cover; and

- c. the planning and development of the Mississauga Garden Park will be pursuant to the approved Mississauga Garden Park Master Plan and Implementation Strategy.

### 16.9.2.4 Site 4



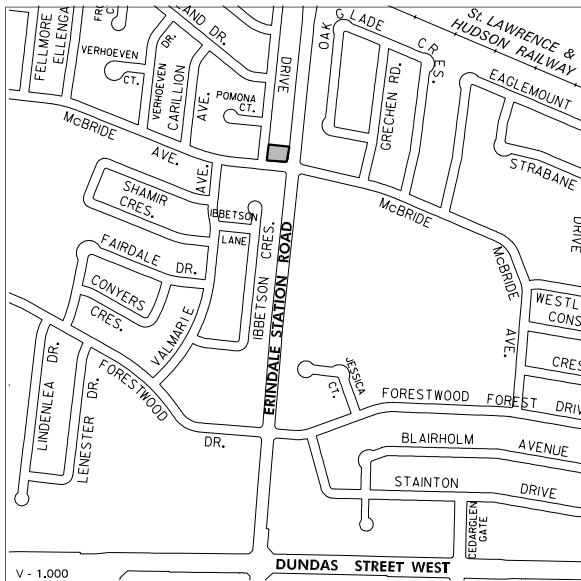
16.9.2.4.1 The lands identified as Special Site 4 are located at the northwest corner of Wolfedale Road and Westlock Road.

16.9.2.4.2 Notwithstanding the provisions of the Convenience Commercial designation, the following additional uses will also be permitted:

- a. townhouse dwellings; and
- b. motor vehicle rental facility.

## 16.9.3 Exempt Site Policies

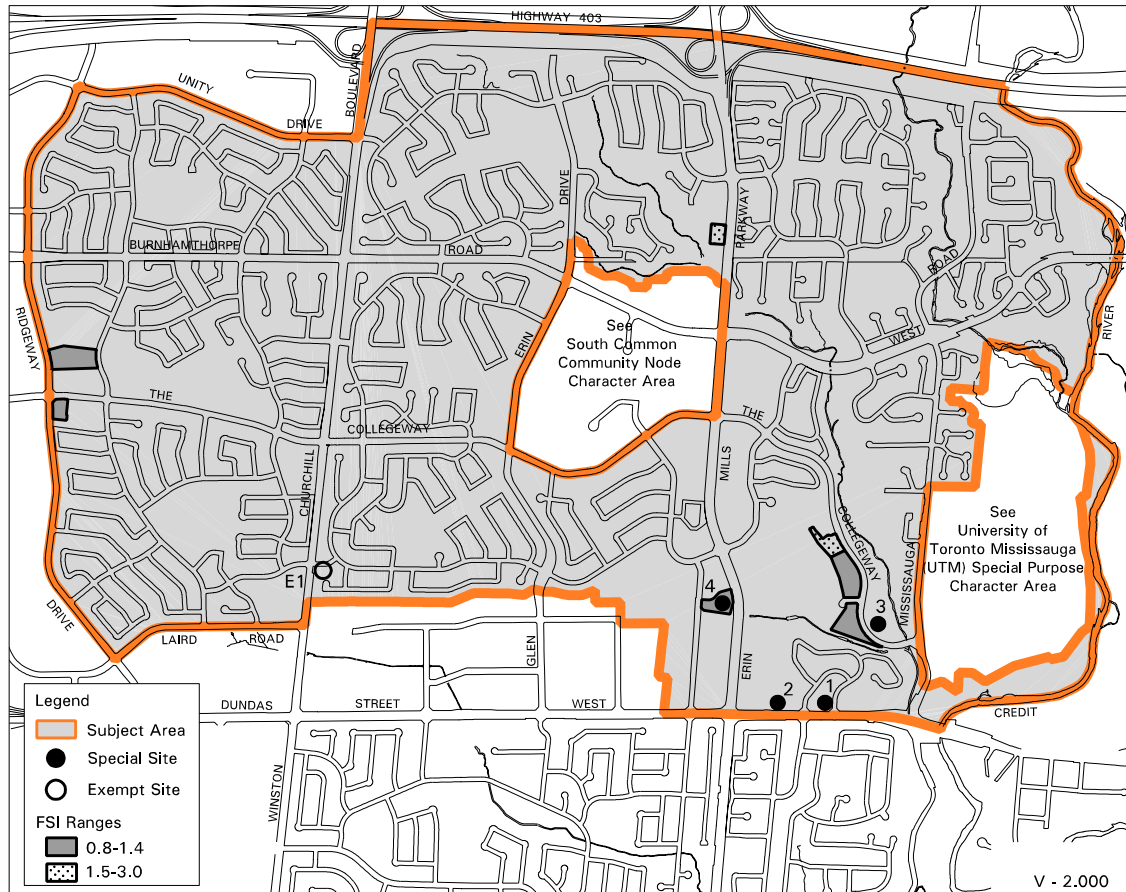
### 16.9.3.1 Site 1



16.9.3.1.1 The lands identified as Exempt Site 1 are located at the northwest corner of Erindale Station Road and McBride Avenue.

16.9.3.1.2 Notwithstanding the provisions of the Residential Low Density II designation, medical offices will also be permitted.

## 16.10 Erin Mills



Map 16-10: Erin Mills Neighbourhood Character Area

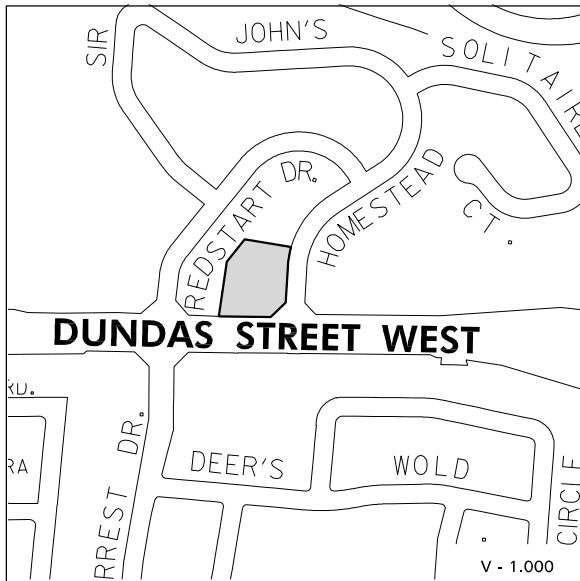
### 16.10.1 Land Use

16.10.1.1 Notwithstanding the Low Density I policies of the Plan, semi-detached and duplex dwellings will not be permitted west of Winston Churchill Boulevard.

## 16.10.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

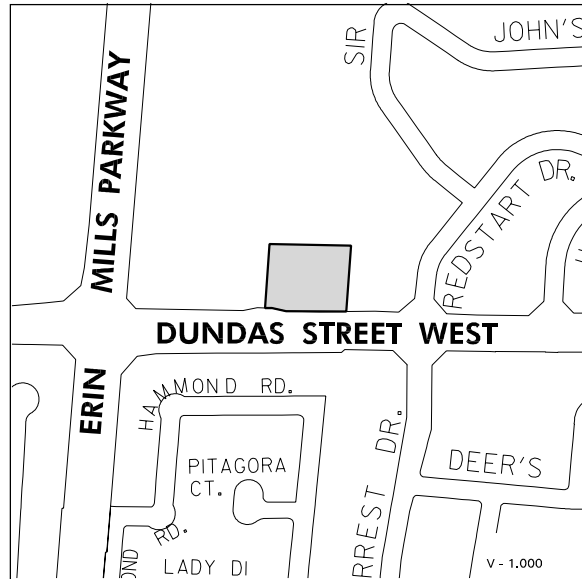
### 16.10.2.1 Site 1



16.10.2.1.1 The lands identified as Special Site 1, are located at the northwest corner of Dundas Street West and Sir John's Homestead.

16.10.2.1.2 Notwithstanding the provisions of the Residential Low Density I designation, office uses will be permitted.

### 16.10.2.2 Site 2

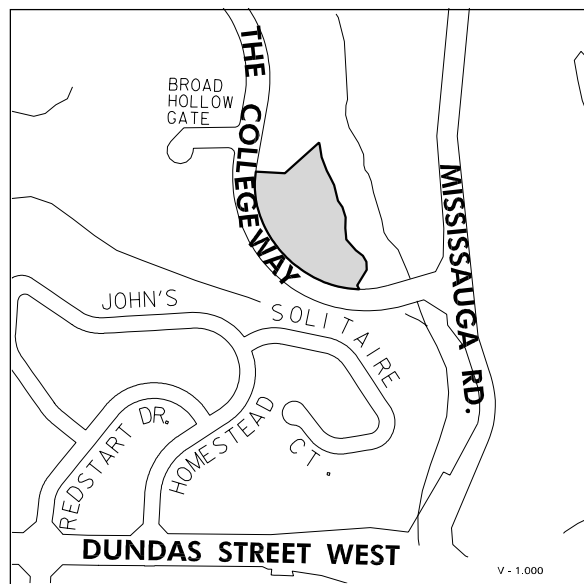


16.10.2.2.1 The lands identified as Special Site 2 are located on the north side of Dundas Street West, east of Erin Mills Parkway.

16.10.2.2.2 Notwithstanding the provisions of the Mixed Use designation, a funeral establishment will be the only permitted use.



### 16.10.2.3 Site 3

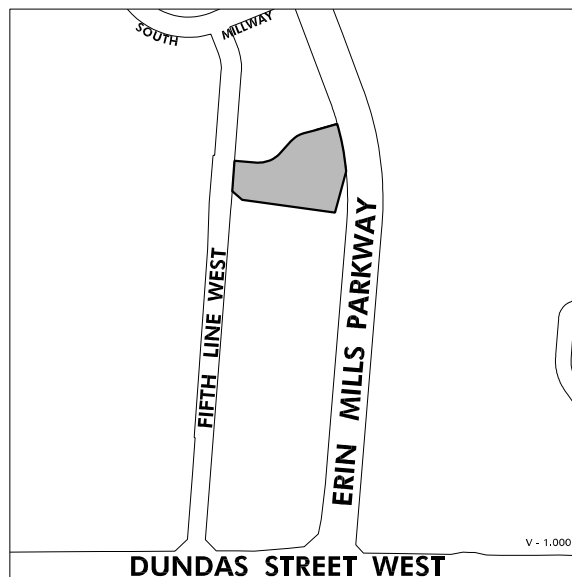


16.10.2.3.1 The lands identified as Special Site 3, are located on the north side of The Collegeway, west of Mississauga Road and contain a cultural heritage resource under the terms and conditions of Part IV of the *Ontario Heritage Act*.

a. Notwithstanding the provisions of the Mixed Use designation, uses will be limited to the following:

- overnight accommodation;
- restaurants;
- banquet halls;
- conference centres;
- spa;
- recreation facilities associated with overnight accommodation;
- residential apartment building with a maximum height of 22 storeys as measured from The Collegeway and a maximum **floor space index (FSI)** of 1.17; and
- the maximum **floor space index (FSI)** of all commercial uses shall not exceed 0.27.

### 16.10.2.4 Site 4

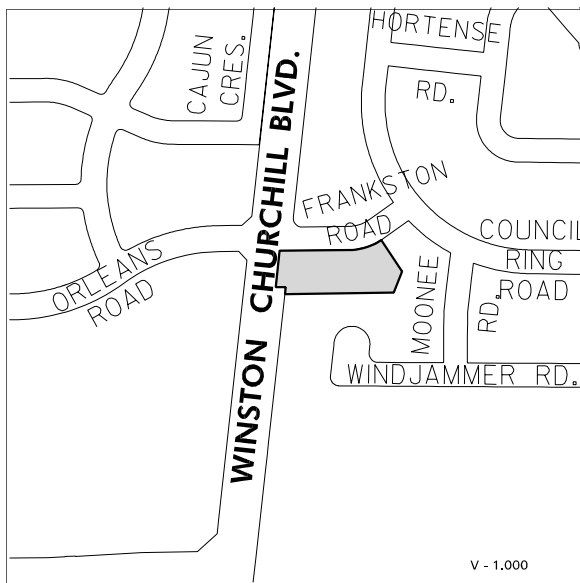


16.10.2.4.1 The lands identified as Special Site 4 are located south of South Millway, between Erin Mills Parkway and Fifth Line West.

16.10.2.4.2 Notwithstanding the policies of this Plan, a maximum height of five storeys will be permitted.

## 16.10.3 Exempt Site Policies

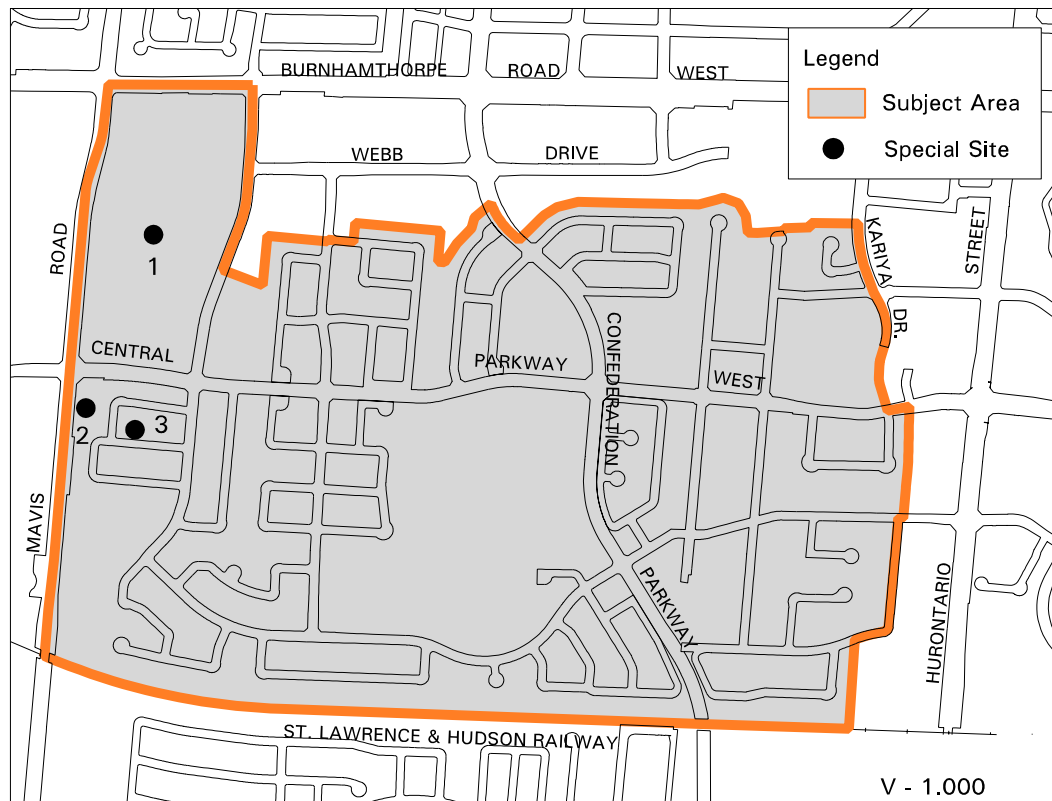
### 16.10.3.1 Site 1



16.10.3.1.1 The lands identified as Exempt Site 1 are located at the southeast corner of Frankston Road and Winston Churchill Boulevard.

16.10.3.1.2 Notwithstanding the provisions of the Residential Low Density I designation, a garden centre will also be permitted.

## 16.11 Fairview



Map 16-11: Fairview Neighbourhood Character Area

### 16.11.1 Land Use

16.11.1.1 No new residential development will be permitted within the 300 m influence area of a chemical plant located on Mavis Road, south of Burnhamthorpe Road West, until the closure of the plant, or suitable clean up of emissions has been carried out to the satisfaction of the Provincial Government.

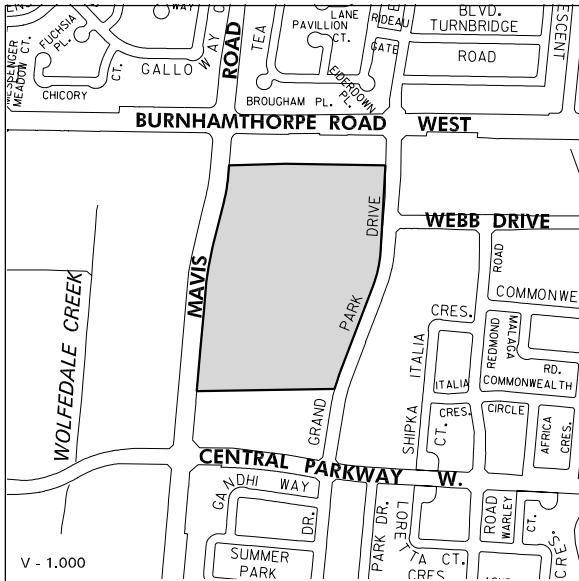
16.11.1.2 Owners/developers will be encouraged to advise all prospective purchasers or tenants occupying new residential units within the 600 m influence area of a chemical plant located on Mavis Road, south of Burnhamthorpe Road West, of the possibility of adverse odour emissions.

16.11.1.3 Notwithstanding the Residential Medium Density policies of this Plan, the Residential Medium Density designation permits only townhouse dwellings.

## 16.11.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 16.11.2.1 Site 1



16.11.2.1.1 The lands identified as Special Site 1 are located in the southeast quadrant of Burnhamthorpe Road West and Mavis Road, and comprise a large portion of the currently vacant and industrial lands.

16.11.2.1.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- the redevelopment of the lands fronting Mavis Road should recognize industries on the west side of Mavis Road. Redevelopment of the lands fronting the east side of Mavis Road will be of a mixed commercial/residential nature and any permitted high density residential development will be located internally on the site and have an easterly orientation; and
- further to subsection a. above of this section, a concept plan for all of Site 1 will be required to address, among other matters, the following:

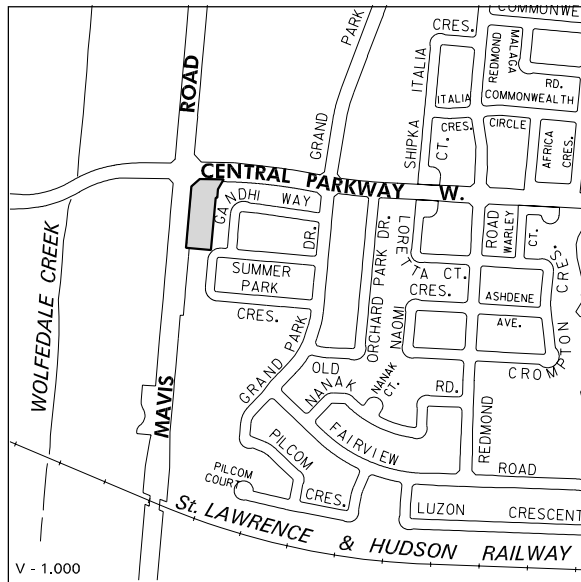
- compatibility of building form and scale with existing and proposed surrounding land uses;
- acceptable ingress and egress arrangements for Mavis Road, Burnhamthorpe Road West, Grand Park Drive, and Central Parkway West;
- the provision of off-street parking; and
- landscaping and buffering of commercial uses proposed along Mavis Road.

16.11.2.1.3 The Mixed Use designation applies to a large tract of land fronting the east side of Mavis Road, south of Burnhamthorpe Road West and north of Central Parkway West.

- The Mixed Use designation permits a mix of office, commercial and high density residential uses on this site.
- Office development will not exceed a **floor space index (FSI)** of 1.0.
- In addition to the **floor space index (FSI)** of 1.0 permitted for office uses, an additional **floor space index (FSI)** of 0.15 will be permitted for accessory street level commercial uses.
- In addition, Residential High Density development will be permitted to a maximum of 247 units per **net residential hectare** and will be oriented towards the interior and easterly boundary of this site.
- The Provincial Government restrictions regarding the buffering of new residential development from existing industrial development on Mavis Road will apply.
- A self storage facility will also be permitted.
- The lands may be used for the industrial operations and related accessory uses in existence at the time this policy comes into effect (2007 September 10). Minor alterations or additions to existing facilities, buildings and structures will be permitted.

- h. When the existing industries cease operation, the lands will be developed in accordance with the Mixed Use designation of this Plan.

### 16.11.2.2 Site 2

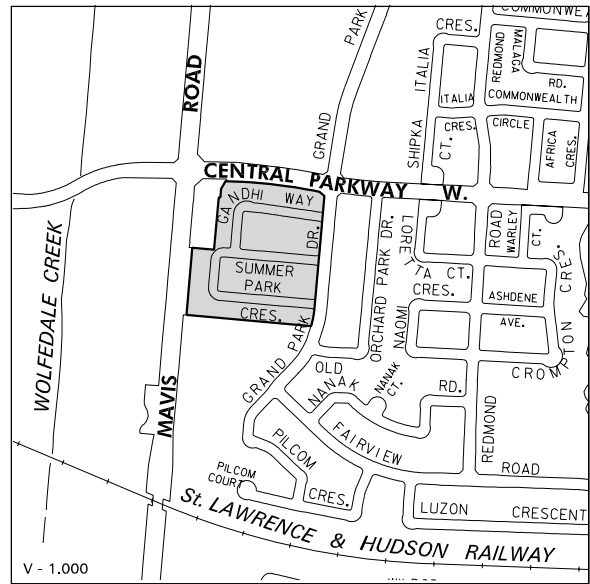


16.11.2.2.1 The lands identified as Special Site 2 are located at the southeast corner of Mavis Road and Central Parkway West.

16.11.2.2.2 Notwithstanding the provisions of the Motor Vehicle Commercial designation, the following additional policies will apply:

- a. convenience restaurants will be permitted; and
- b. car washes will not be permitted.

### 16.11.2.3 Site 3

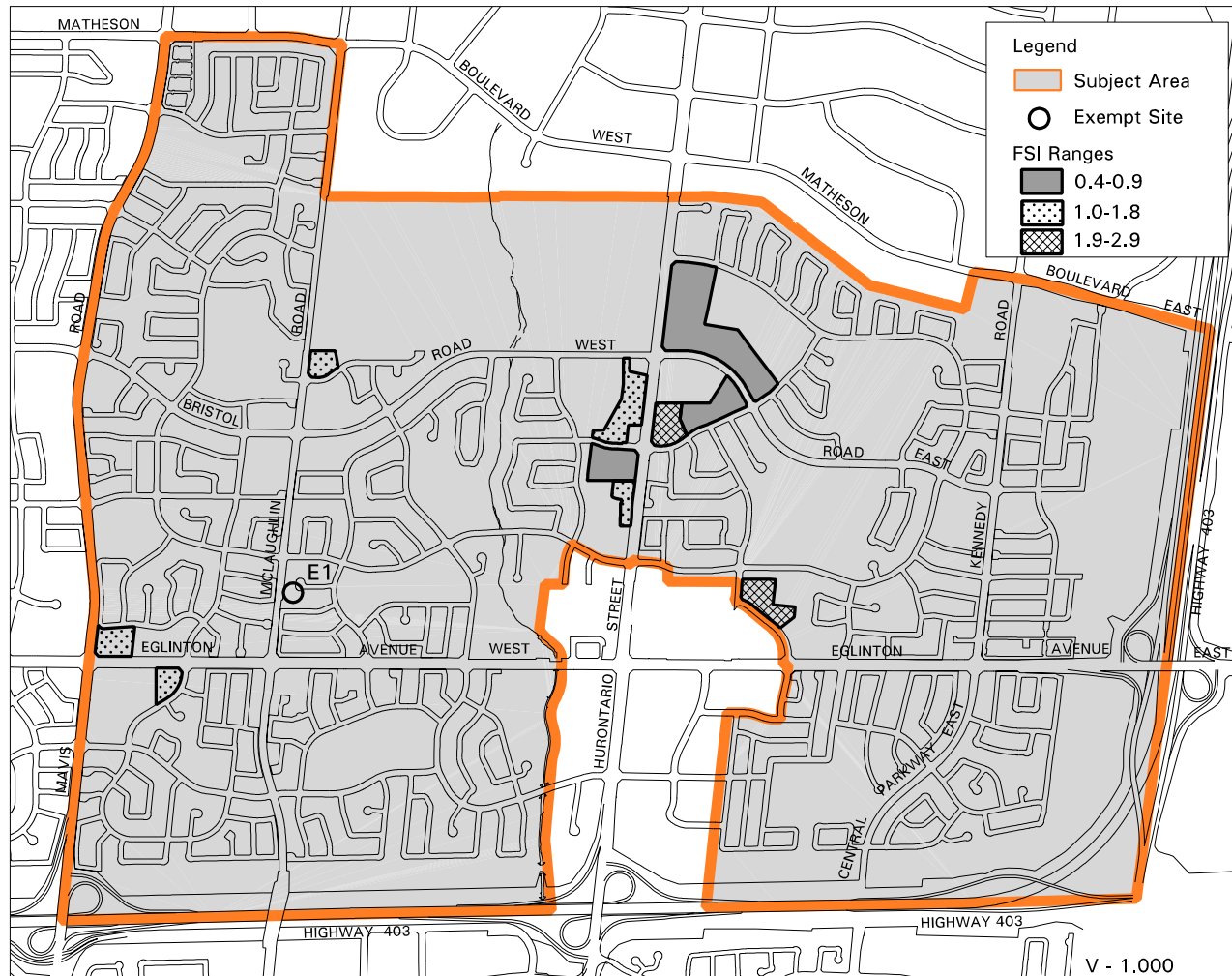


16.11.2.3.1 The lands identified as Special Site 3 are located south of Central Parkway West between Mavis Road and Grand Park Drive.

16.11.2.3.2 Notwithstanding the provisions of the Residential Medium Density designation, semi-detached dwellings will also be permitted.



## 16.12 Hurontario



Map 16-12: Hurontario Neighbourhood Character Area

### 16.12.1 Urban Design Policies

16.12.1.1 To enhance a sense of community, it is proposed that a number of major **streetscapes** be developed in a manner which will impart a sense of character and identify major geographic areas of the Character Area.

16.12.1.2 Community Form and Structure Uses along Hurontario Street should be integrated with the overall community design by providing for:

- a. a graduated transition in development intensity and building scale; and

- b. orientation of buildings, related open spaces and service functions to minimize visual and functional conflicts on abutting lands.

### 16.12.2 Land Use

16.12.2.1 Lands designated Residential Medium Density will also permit low-rise apartment dwellings.

16.12.2.2 The Institutional designation recognizes the Britannia Farm which is owned and operated by

the Peel District School Board and is a unique feature in the City. It has maintained its original rural character, and is used for agricultural and educational purposes. It will be developed in accordance with the Britannia Farm Master Plan, which recommends that the lands be developed for educational purposes to provide students with experiences with agriculture, the environment, and the historic schoolhouse.

16.12.2.3 Notwithstanding the Institutional policies of this Plan, a **cogeneration** facility will not be permitted.

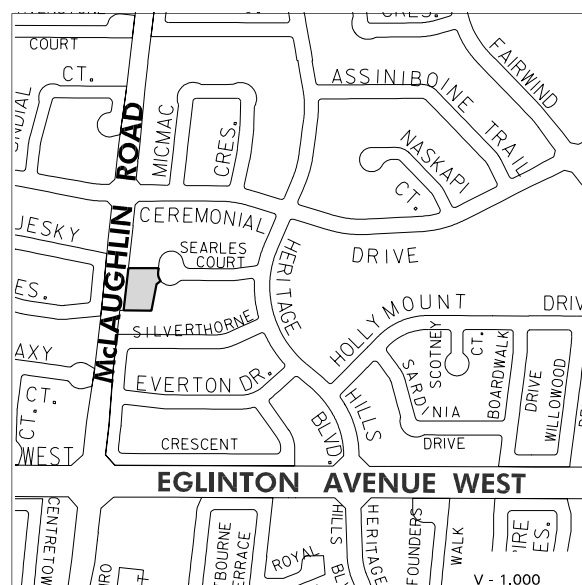
## 16.12.3 Transportation

16.12.3.1 South of the Character Area between the utility corridor and Provincial Highway 403, a new east-west roadway is proposed to provide access to the Downtown Core. The proposed Provincial Highway 403 North Collector would be connected to Provincial Highway 403, west of Hurontario Street and east of Mavis Road and would be developed within the Parkway Belt West corridor as a one-way westbound two lane roadway with grade separations at Hurontario Street, Duke of York Boulevard, Confederation Parkway, the Provincial Highway 403/Mavis Road westbound off-ramp, and Mavis Road. In addition, ramp connections would be provided at Duke of York Boulevard and Confederation Parkway to provide access to and from the Downtown Core road network.

16.12.3.2 An overpass structure is planned for Provincial Highway 403 at Duke of York Boulevard.

## 16.12.4 Exempt Site Policies

### 16.12.4.1 Site 1

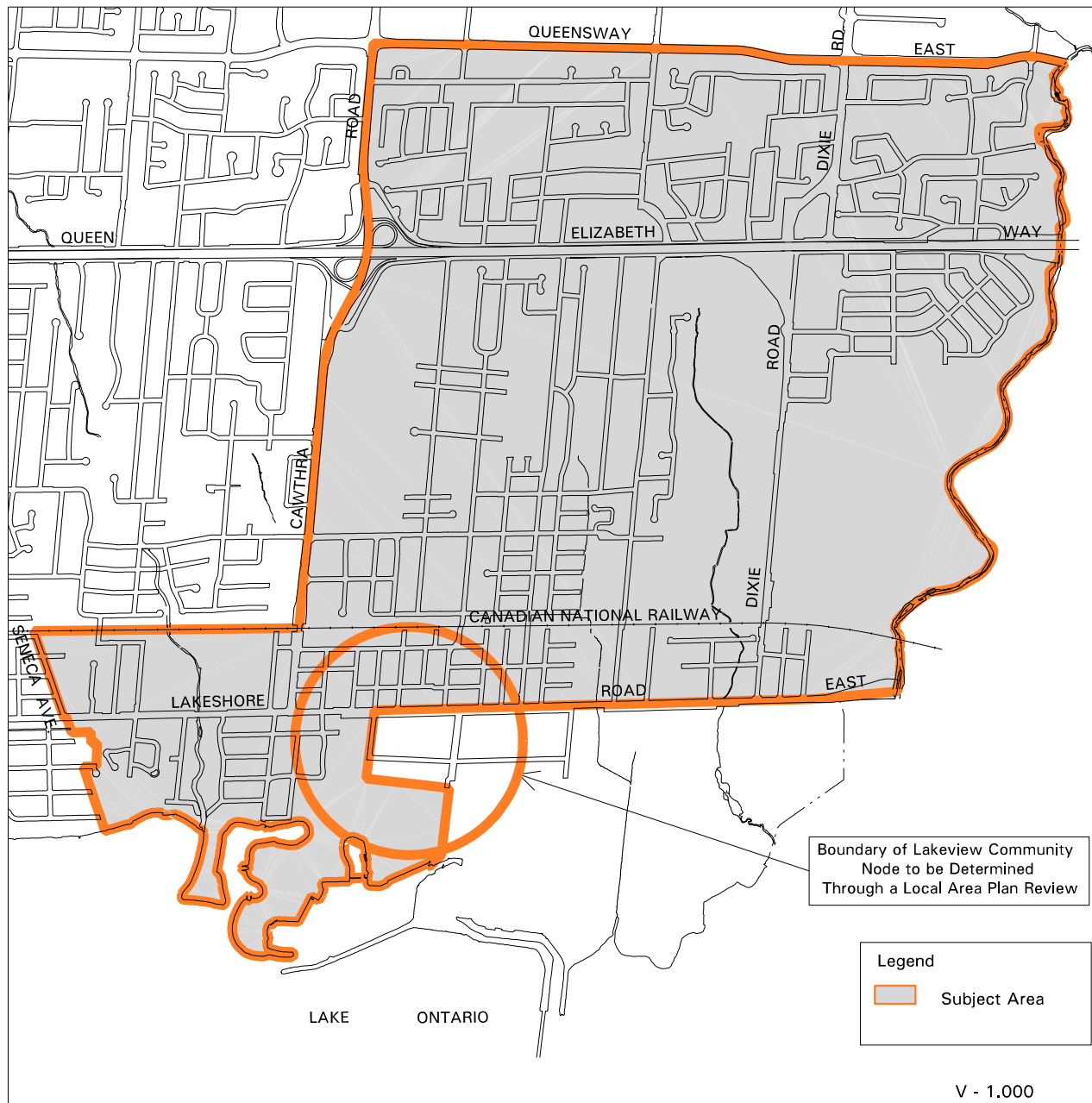


16.12.4.1.1 The lands identified as Exempt Site 1 are located on the east side of McLaughlin Road, north of Silverthorne Crescent.

16.12.4.1.2 Notwithstanding the provisions of the Residential Low Density II designation, an office for a maximum of two physicians, dentists or drugless practitioners will also be permitted.

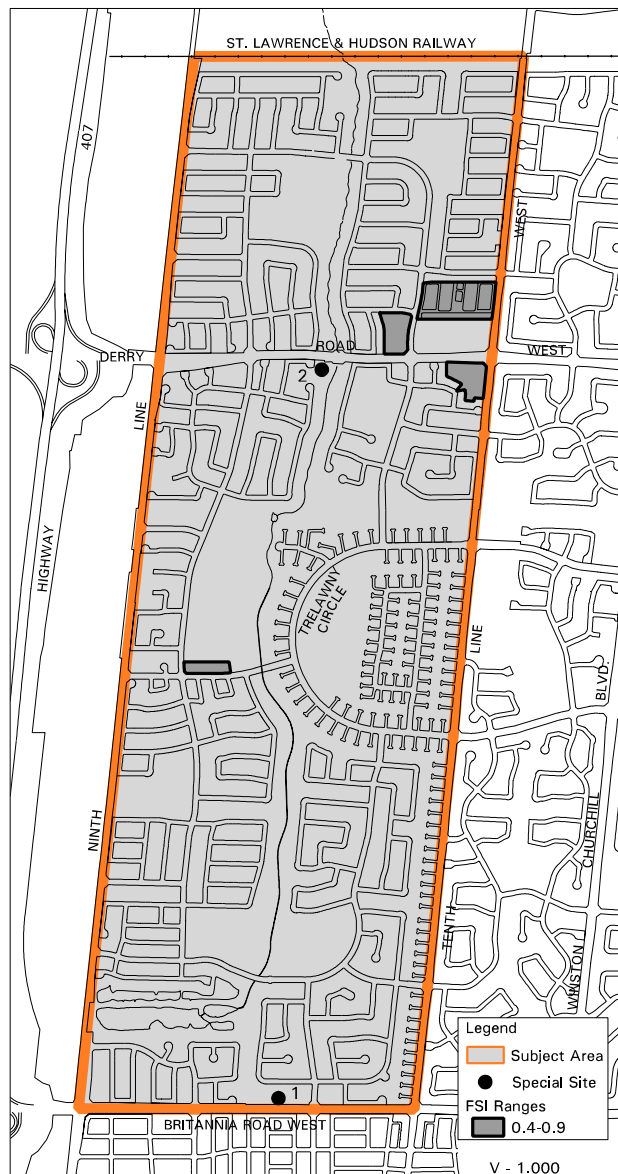
## 16.13 Lakeview

Official Plan policies for lands within the Lakeview Neighbourhood Character Area are contained in the Lakeview Local Area Plan.



Map 16-13: Lakeview Neighbourhood Character Area

## 16.14 Lisgar



Map 16-14: Lisgar Neighbourhood Character Area

### 16.14.1 Urban Design Policies

16.14.1.1 Open space connections that link public parks and **community facilities** through use of walkways, underpasses or bridges will be encouraged during the evaluation of any development proposal.

### 16.14.2 Land Use

16.14.2.1 Frontage service roads will be encouraged as a means of improving noise abatement and quality of **streetscape** aesthetics. Where service roads are not appropriate, or cannot be achieved due to certain constraints, reversed frontage lots may be permitted, provided the lots have a minimum depth of 45 m.

16.14.2.2 Notwithstanding the Residential Low Density I policies of this Plan, the Residential Low Density I designation permits only detached dwellings.

16.14.2.3 In addition to the general Residential Medium Density development policies of this Plan, in order to create acceptable built form transitions, buildings should be limited in height when adjacent to low density residential neighbourhoods and buildings abutting low density housing forms should be limited to three storeys.

16.14.2.4 Lands designated Residential Medium Density will also permit low-rise apartment dwellings.

### 16.14.3 Transportation

16.14.3.1 Prior to giving consideration to any proposal to widen Tenth Line West by City Council, a neighbourhood traffic study will be undertaken. This study should examine alternatives to widening Tenth Line West, including traffic management measures. In the event that a widening is required, its need and justification should be clearly documented in accordance with the requirements of the document entitled Class Environmental Assessment for Municipal Road Projects.

16.14.3.2 Truck traffic will be discouraged from utilizing Tenth Line West, south of the St. Lawrence and Hudson Railway in recognition of the residential character of Tenth Line West, south of the railway line.

16.14.3.3 Direct residential frontage and access for detached dwellings, semi-detached dwellings and street townhouses to Tenth Line West will not be permitted.

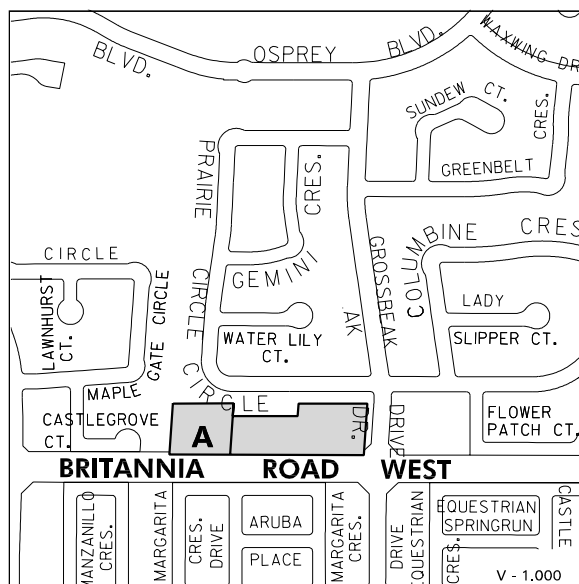
## 16.14.4 Physical Services and Utilities

16.14.4.1 Lands within the Character Area that are drained by the Sixteen Mile Creek Sub-watershed are serviced by a Foundation Drain Collector System intended for the collection of weeping tile drainage only. All buildings within this **sub-watershed** will not be permitted to have connections of roof leaders to the Foundation Drain Collector or the weeping tile, now or in the future. Roof leaders for residential units will discharge onto the lots with the use of concrete splash pads such that the side lot swales will drain this runoff to the road. Building roof leaders for commercial, community, and multiple family residential land uses will be drained similarly where practical.

## 16.14.5 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 16.14.5.1 Site 1



16.14.5.1.1 The lands identified as Special Site 1 consist of five existing detached dwellings located on the north side of Britannia Road West, west of Grossbeak Drive.

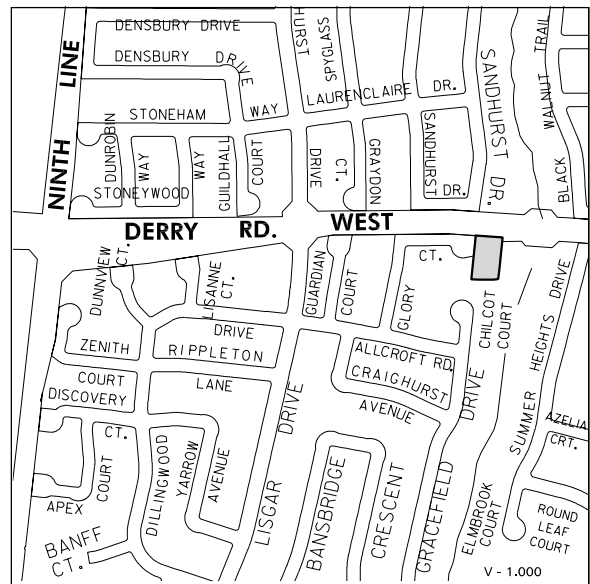
16.14.5.1.2 Notwithstanding the provisions of the Residential Low Density I designation, the following additional policies will apply:

- offices will be permitted in existing detached dwellings;
- parking areas associated with office uses located in the front yard of the existing dwellings should be screened from the public street with landscaping, low screen walls or other treatment. Additions and/or alterations to the existing dwellings will be designed to maintain the existing residential character, massing and materials;



- c. provisions should be made in any redevelopment plans for assembly of additional lands to facilitate a pedestrian walkway connecting Prairie Circle to the greenbelt lands to the west; and
- d. for the land identified as Area A, a veterinary clinic, including indoor overnight boarding of animals, will be permitted.

#### 16.14.5.2 Site 2

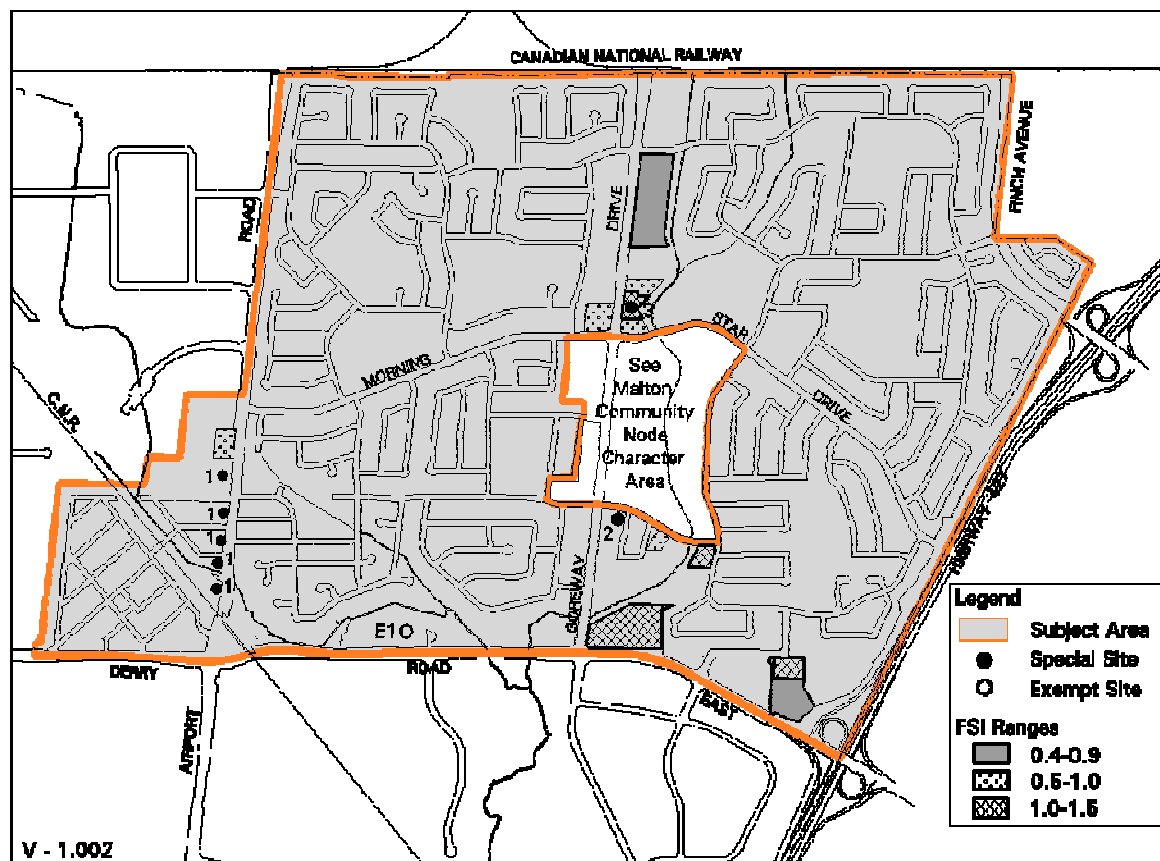


16.14.5.2.1 The lands identified as Special Site 2 are located south of Derry Road West, east of Lisgar Drive, known as 3800 Derry Road West.

16.14.5.2.2 Notwithstanding the provisions of the Residential Medium Density designation, a veterinary clinic will be permitted in the existing detached dwelling.

16.14.5.2.3 Redevelopment of Special Site 2 will be subject to an archaeological assessment due to its location adjacent to the Kindree Family Cemetery, designated under the *Ontario Heritage Act*.

## 16.15 Malton



Map 16-15: Malton Neighbourhood Character Area

### 16.15.1 Urban Design Policies

16.15.1.1 The following principles should be encouraged during the evaluation of any development proposal:

- the provision of open space connections that link commercial developments with public parks and **community facilities** through use of walkways, underpasses, bridges, **streetscape** development, and enhancement of pedestrian and cyclist access to greenbelt lands, wherever possible;
- the preservation and conservation of the former Village of Malton, bounded by Derry Road East to the south, Airport Road to the east, and

Cattrick Street to the west, be maintained with respect to its housing character, street pattern and that all public works should enhance the heritage elements; and

- the conservation of the Victory War Time Housing cultural landscape, bounded by Victory Crescent on the north, Airport Road on the west, Meritt Avenue on the south and Lancaster Avenue on the east. The neighbourhood arose, as a planned community, out of the need for affordable housing for the thousands of employees in the adjacent airplane manufacturing plants and related industries at the beginning of World War II. There is a strong character of modest one to one and a half

storey residential structures, mature trees and consistent setbacks.

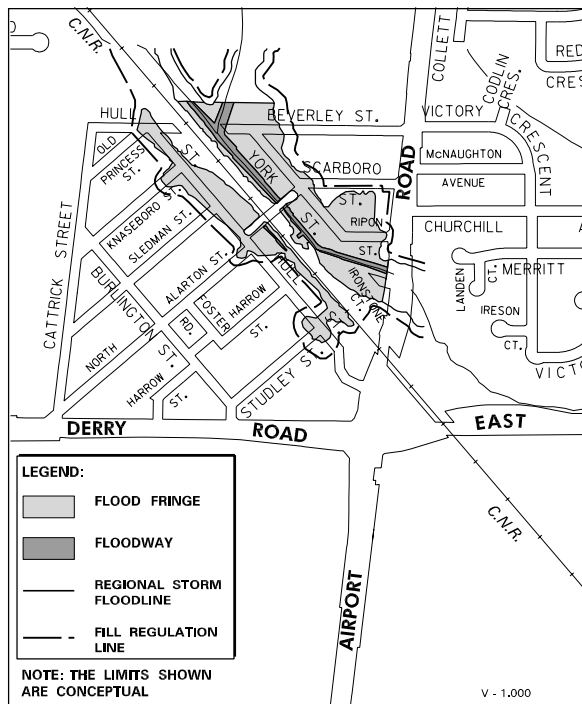
## 16.15.2 Land Use

16.15.2.1 Lands designated Residential Low Density II will not permit the following uses:

- a. triplexes, street townhouses and other forms of low-rise dwellings with individual frontages.

16.15.2.2 Lands designated Residential Medium Density will also permit apartment dwellings.

## 16.15.3 Environmental Planning Area



Map 16-15.1: Mimico Creek Floodplain Management Concept

16.15.3.1 The lands within the Mimico Creek floodplain shown above are subject to the **two-zone floodplain management concept** which divides the floodplain into two portions known as the floodway and flood fringe. The floodway contains the greatest depth and velocity and, therefore, development is restricted. The flood fringe may support development provided that regulatory flood protection is provided. The **two-zone floodplain**

**management concept** approval procedures are based on the following principles:

- a. development will be restricted to areas of the floodplain where the depth of flooding and velocities are non-life threatening and property damage can be minimized (flood fringe);
- b. regulatory flood protection will be sought for new development; and
- c. approval pursuant to the Toronto and Region Conservation Authority's (TRCA) Fill, Construction and Alteration to Waterways regulation is granted at the discretion of the Authority's Executive Committee.

16.15.3.2 Notwithstanding the Natural Hazard policies of this Plan, the following policies will apply to those lands within the flood fringe of the regulatory floodplain and outside the floodway:

- a. development must be flood protected to the level of the Regulatory Flood as defined by TRCA;
- b. in all instances, ingress and egress will be safe, pursuant to Provincial Government flood proofing standards. In addition, the maximum level of flood protection determined to be feasible, will be considered;
- c. flood damage reduction measures will be carried out by the proponent of development to achieve the required level of flood protection. The selection of flood damage reduction measures will be based on the following alternatives, listed in order of priority:

- dry, passive flood proofing measures will be implemented to the extent technically and/or practically feasible;
- wet flood proofing measures may be permissible to minimize flood risk and/or to meet the level of flood protection required; and
- dry, active flood proofing measures may be permissible to minimize flood risk; and

d. The following uses will not be allowed within the floodplain:

- uses such as hospitals, nursing homes, and schools which would pose a significant threat to the safety of the inhabitants involved in an emergency evacuation situation as a result of flooding or failure of flood proofing measures;
- uses associated with the manufacture, storage, disposal and/or consumption of hazardous substances or the treatment, collection and disposal of sewage, which would pose an unacceptable threat to public safety if they were to escape their normal containment/use as a result of flooding or failure of flood proofing measures; and
- emergency services such as those provided by fire, police, and ambulance stations and electrical substations, which would be impaired during a flood emergency as a result of flooding or failure of flood proofing measures.

16.15.3.3 Notwithstanding the above, no development will be permitted if:

- the development would be subjected to a water velocity or depth which would create an unacceptable hazard to life; or
- the development would be susceptible to major structural damage as a result of a flood less than or equal to the Regulatory Flood; or
- the necessary flood protection measures would have a negative impact on adjacent properties.

16.15.3.4 Written permission from the Toronto and Region Conservation Authority Executive Committee must be received prior to:

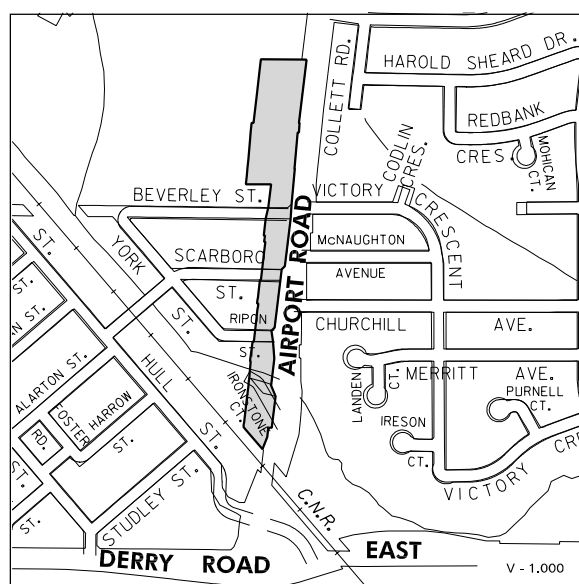
- the construction of any buildings or structures within the Regional Storm (Regulatory Flood) floodplain;
- the placement or dumping of fill within the fill regulation area; and

c. the straightening, changing, diversion or interference in any way with the existing channel of a river, creek, stream or **watercourse**.

## 16.15.4 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 16.15.4.1 Site 1



16.15.4.1.1 The lands identified as Special Site 1 are located on Airport Road, north of the Canadian National Railway and south of Morning Star Drive.

16.15.4.1.2 Pedestrian connections to the commercial uses along Airport Road are vital to its role in the community. The following pedestrian links should be maintained and/or improved as part of any development application:

- walkway linkages to the Derry Greenway Park;

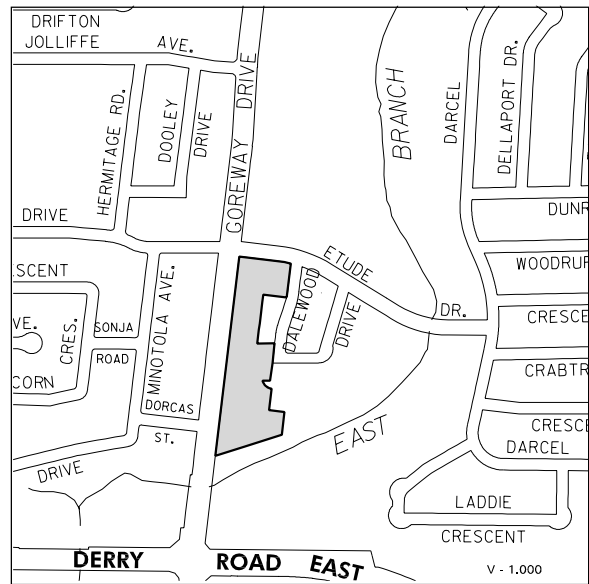
- mid-block connection in the vicinity of Churchill Avenue which may also incorporate a link to the City bicycle and pedestrian route system (recreational trail); and
- strong pedestrian linkages along both sides of Airport Road between Morning Star Drive and Derry Road East the municipal sidewalk to adjacent commercial development.

16.15.4.1.3 Parking for this area should be provided in small, distributed parking lots. Parking facilities should be located and designed to be compatible with the main street character of the area by including measures such as landscape space, planters, or other elements which reinforce the street wall and enhance public amenity.

16.15.4.1.4 Redevelopment of parking areas should not have an adverse impact upon adjacent residential uses.

16.15.4.1.5 While recognizing that parking is located between the buildings and the street in commercial development of the sort found along Airport Road, development in this area should strive to achieve a rhythm of closely spaced storefronts, in order to foster an attractive and active pedestrian environment along the roadway. Building designs, window treatment and awnings, signs, and landscape treatment, including crossovers from the municipal sidewalk to the storefronts, should act to diminish the apparent separation between buildings and the roadway, and contribute to the overall aesthetic value of the **streetscape**.

#### 16.15.4.2 Site 2



16.15.4.2.1 The lands identified as Special Site 2 are located on the east side of Goreway Drive and on the south side of Etude Drive.

16.15.4.2.2 Notwithstanding the Mixed Use designation of this Plan, the following additional policies will apply.

#### Pedestrian Linkages

16.15.4.2.3 Pedestrian connections to the commercial uses along Goreway Drive are vital to their role in the community. The following pedestrian links should be maintained and/or improved as part of any development application:

- a. open space walkway linkages to the Malton Greenway Park; and
- b. linkages to the Malton Community Centre, the Transit Terminal and Lincoln M. Alexander Secondary School (emphasizing safety).

16.15.4.2.4 Where pedestrian linkages from the Greenbelt and Residential Community exist, they are to be strengthened.



## Parking Areas

16.15.4.2.5 Pedestrian links to parking areas for the commercial uses along the east side of Goreway Drive, south of Etude Drive are encouraged. These links will facilitate the safe movement of pedestrians in the area, and create a sense of continuity between properties.

## Building and Massing

16.15.4.2.6 Should additional development occur along the Goreway Drive frontage the following items should be addressed:

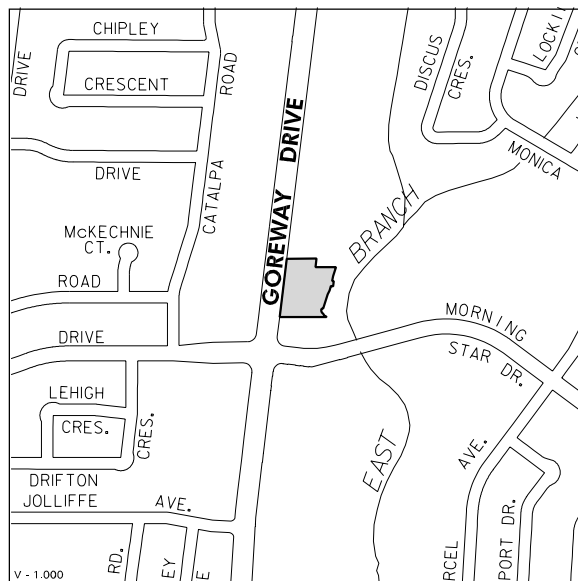
- no parking or driveway areas should be provided between the buildings and the street line;
- blank walls should be avoided along the street in favour of windows, building entrances and architectural detailing;
- service, loading, and garbage storage areas should be located on the internal side of the development, away from public streets, and should be screened from view by means of built form and landscaping; and
- all building entrances should be clearly articulated and linked to pedestrian walkway connections.

## Land Uses

16.15.4.2.7 Notwithstanding the provisions of this section, the existing drive-through facility at 7161 Goreway Drive will be permitted.

### 16.15.4.3

### Site 3



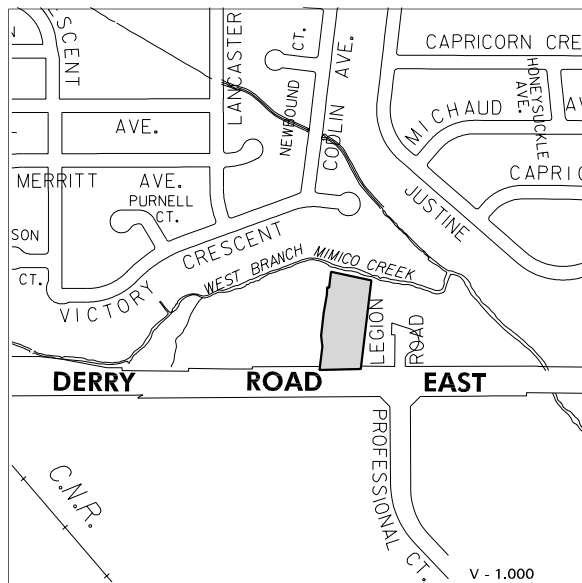
16.15.4.3.1 The lands identified as Special Site 3 are located on the east side of Goreway Drive, north of Morning Star Drive.

16.15.4.3.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:

- a maximum **floor space index (FSI)** of 1.6 will be permitted; and
- the maximum apartment building height will be seven storeys.

## 16.15.5 Exempt Site Policies

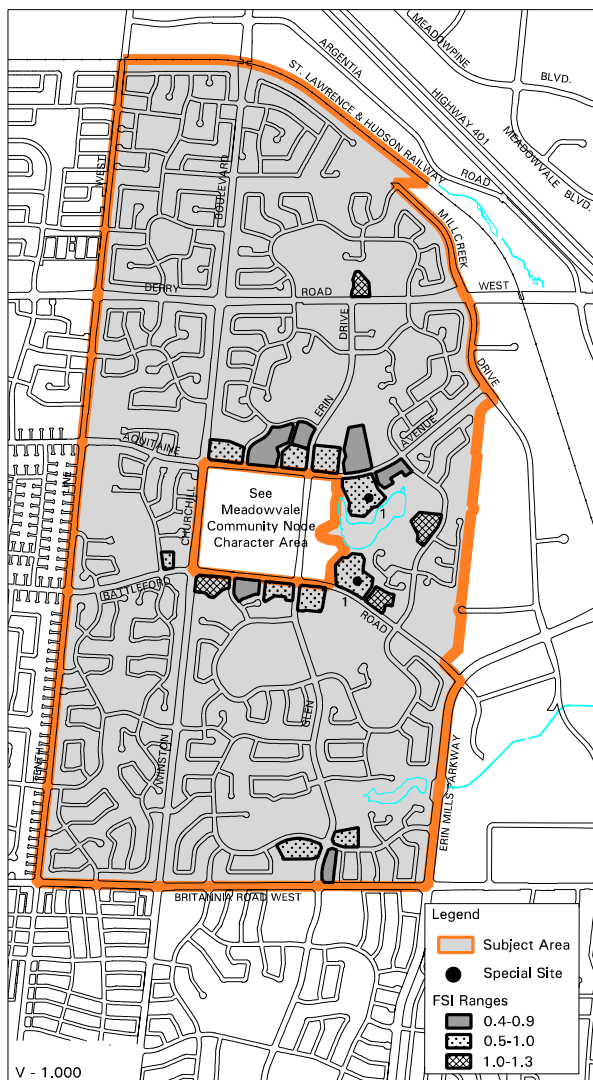
### 16.15.5.1 Site 1



16.15.5.1.1 The lands identified as Exempt Site 1 are located on the north side of Derry Road East, west of Legion Road.

16.15.5.1.2 Notwithstanding the provisions of the Mixed Use designation, the existing Malton Mobile Homes Park will also be permitted.

## 16.16 Meadowvale



Map 16-16: Meadowvale Neighbourhood Character Area

### 16.16.1 Land Use

16.16.1.1 In addition to the general Residential Medium and High Density development policies of this Plan, the following additional policy applies specifically to this Character Area:

- a. in order to create acceptable built form transitions, buildings should be limited in height when adjacent to low density residential neighbourhoods. Buildings immediately adjacent to low density housing forms should be limited to three storeys. In situations where the low density housing forms are separated from high

density development by a public road or other permanent open space feature, a height of four to five storeys may be compatible.

16.16.1.2 Lands designated Residential Low Density II will not permit the following uses:

- a. triplexes, street townhouses and other forms of low-rise dwellings with individual frontages.

16.16.1.3 Lands designated Residential Medium Density will also permit low-rise apartment dwellings.

### 16.16.2 Transportation

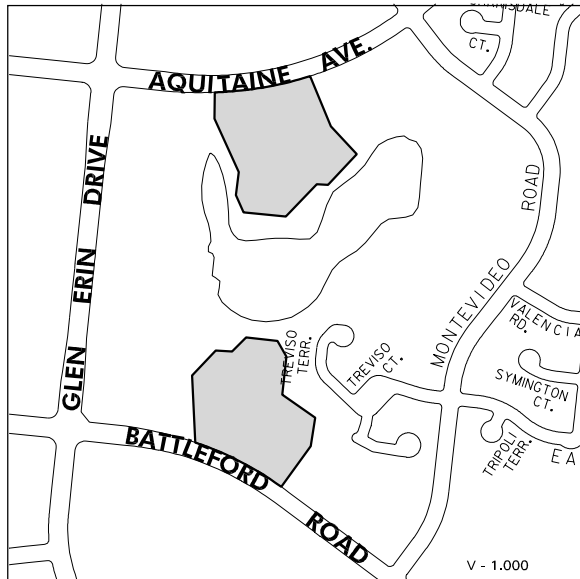
16.16.2.1 Prior to giving consideration to any proposal to widen Tenth Line, City Council will require a neighbourhood traffic study be undertaken. This study should examine alternatives to widening Tenth Line, including traffic management measures. In the event that a widening is required, its need and justification should be clearly documented in accordance with the requirements of the document entitled Class Environmental Assessment for Municipal Road Projects.

16.16.2.2 Truck traffic will be discouraged from utilizing Tenth Line West south of the St. Lawrence and Hudson Railway in recognition of the residential character of Tenth Line West south of the railway line.

### 16.16.3 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

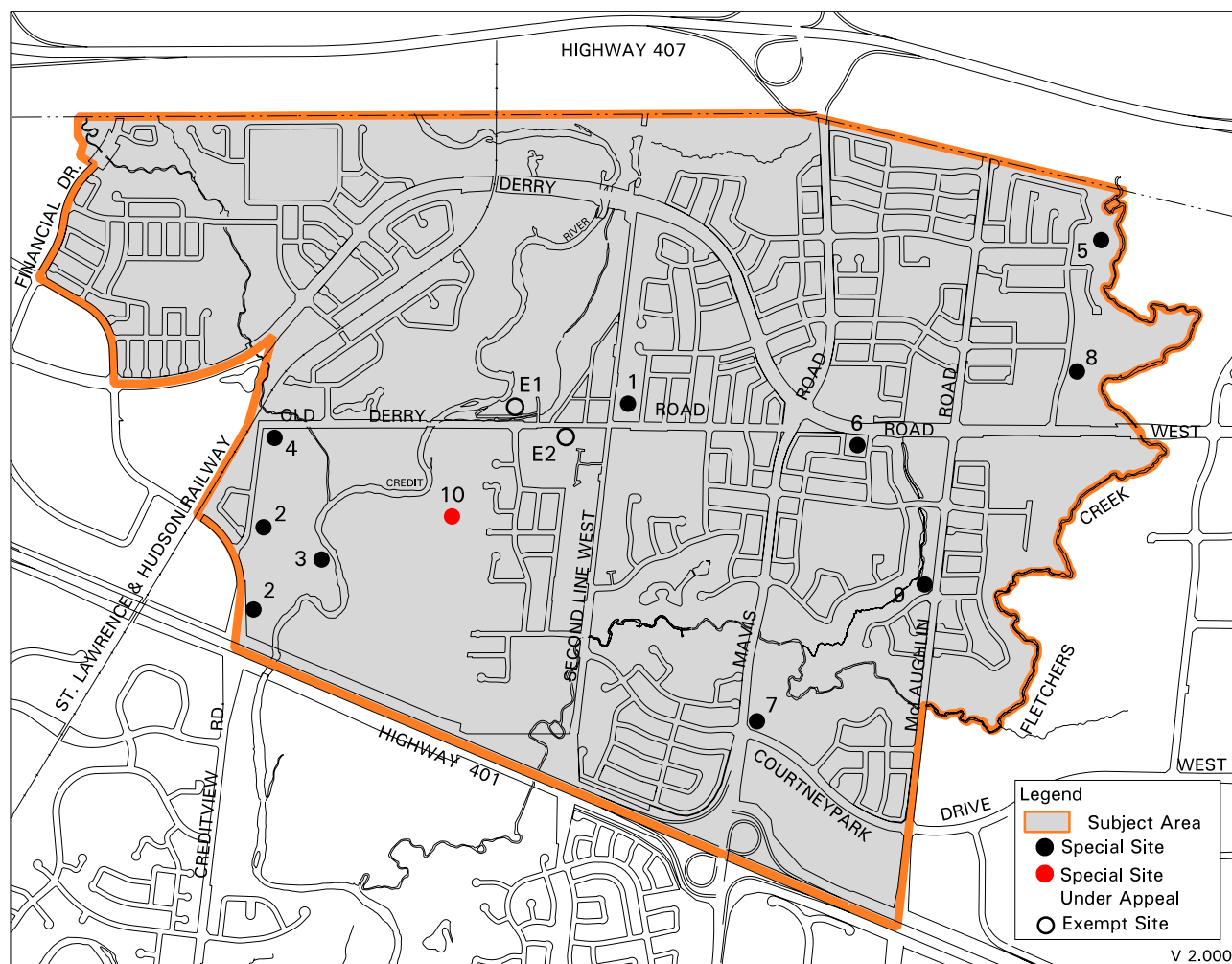
#### 16.16.3.1 Site 1



16.16.3.1.1 The lands identified as Special Site 1 are located on the south side of Aquitaine Avenue, east of Glen Erin Drive and on the north side of Battleford Road east of Glen Erin Drive.

16.16.3.1.2 Notwithstanding the provisions of the Residential High Density designation, townhouse dwellings will be permitted.

## 16.17 Meadowvale Village



Map 16-17: Meadowvale Village Neighbourhood Character Area

### 16.17.1 Context

The Meadowvale Village Neighbourhood Character Area policies apply to both the Meadowvale Village Heritage Conservation District including the lands immediately surrounding the Village, and the majority of lands more removed from the Village that have been developed through residential plans of subdivision throughout the 1990's and 2000's.

Meadowvale Village was first established as a European settlement circa 1819. Its location, adjacent the Credit River, with a natural and open meadow and shallow valley or vale, was a prime location for starting a saw mill and grist mill. Throughout the nineteenth century the mill, supported by local agrarian farming, built a strong economic community and social life for its inhabitants. In the late nineteenth century the rail lines were routed around the Village which led to its decline. The Village became the modest, small village that survived into the twentieth century. By the late 1960's, Meadowvale Village was still a rural



community with much of its nineteenth century character intact.

The *Ontario Heritage Act* was introduced in 1974 to provide municipalities with the means to protect and conserve their rural village communities. Meadowvale Village was Ontario's first Heritage Conservation District as approved by the Ontario Municipal Board in 1980. The establishment of the Meadowvale Village Heritage Conservation District officially recognized its historical associations and existing built form that makes it unique in Mississauga.

The Heritage Conservation District remains distinct within the City of Mississauga. It has retained its cultural heritage landscape and attributes due to concerned residents and its early designation as a Heritage Conservation District which has managed change over the past 30 years.

## 16.17.2 Urban Design Policies

The Urban Design Policies apply to all lands within the Meadowvale Village Neighbourhood Character Area. Urban design policies specific to the Heritage Conservation District and lands immediately surrounding the Village, are detailed in the Village Precinct policies.

16.17.2.1 New development will comply with the Heritage Conservation District Plan and integrate individual developments into a cohesive whole.

16.17.2.2 An interconnected open space network including the valleys of the Credit River, Levi Creek and Fletcher's Creek is a key feature in the identity of the Character Area which should be recognized in any development or redevelopment by enhancing visual and, where appropriate, physical public access to these open spaces.

16.17.2.3 A highly interconnected street pattern, such as a grid or modified grid, is encouraged.

16.17.2.4 A concept plan may be required as part of the processing of any development application to illustrate the location of existing trees, the road and

lotting pattern and connections to adjacent developments. Appropriate land assembly may be encouraged to achieve the objectives of this Plan.

16.17.2.5 Subdivisions which provide a mixture of lot sizes which vary in a highly intermixed, seemingly random fashion to echo the lotting fabric of the Village, are encouraged.

16.17.2.6 The development of these lands may include rolled curbs and gutters, fewer municipal sidewalks, and decorative street lighting – all of which differ from existing City standards.

16.17.2.7 Standards for street layout, parking and loading spaces, landscaping, commons, building height and location, size and dwelling unit design, including dwelling unit composition, form, massing, setbacks, and spatial relationship with adjacent buildings, site access, lighting, signage, and screening will meet the requirements of the Zoning By-law.

16.17.2.8 The design of the street right-of-way and the design of the lands along the street affect the **streetscape** and should have regard for the following:

- a. vistas and views of the Heritage Conservation District, and into and along the valleys of the Credit River, Levi Creek, and Fletcher's Creek should be created, maintained and enhanced;
- b. the creation of individual entry features to subdivisions is discouraged to avoid the creation of enclaves within the community;
- c. adjacent to Provincial Highways and elsewhere where "reverse frontages" are unavoidable and acoustic protection is required, such acoustic protection should be provided through berming to the greatest extent possible, minimizing the use of noise attenuation walls; and
- d. reverse frontage development will be prohibited along the existing alignment of Old Derry Road.

16.17.2.9 In applying the following policies, the effect of buildings and spaces on the surrounding environment should be considered equally with the function and aesthetic appeal of the site itself:

- a. the presence of garages should be minimized to create an attractive **streetscape**. Garages should not project substantially beyond the front face of any house. Garages that project beyond the front face of any house will be discouraged; small, recessed or detached garages are preferred. Additional measures may be required through the processing of development applications to ensure an acceptable **streetscape** is developed. Garages will not project beyond the face of any house located in areas designated Residential Low Density I; and
- b. reverse frontage lots may be permitted, providing the lots have a minimum depth of 45m.

16.17.2.10 The Village Precinct represents the lands in and around the Heritage Conservation District as shown on Map 16-17.1: Meadowvale Village Precincts.

16.17.2.11 The development of properties within the Heritage Conservation District and the Village Precinct will be subject to site plan control. For lands within the Heritage Conservation District, substantive alterations to properties, as defined in the Meadowvale Village Heritage Conservation District Plan, will require consultation with the Heritage Advisory Committee (HAC).

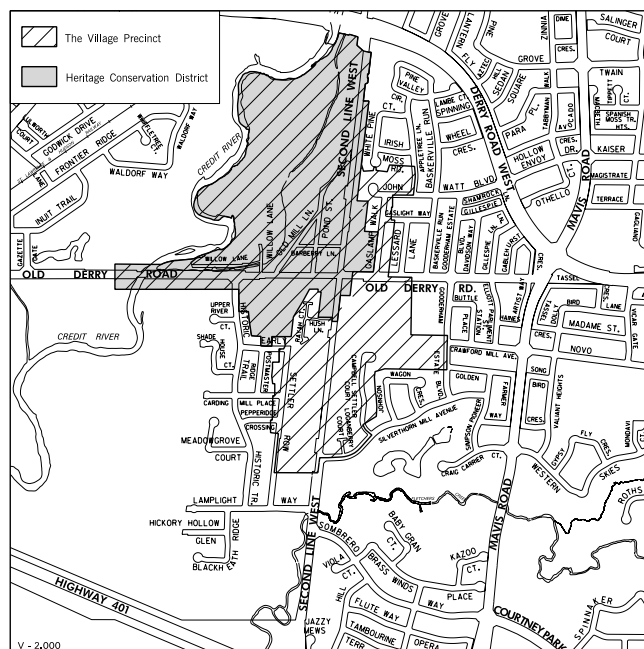
16.17.2.12 The rural village character of the Heritage Conservation District must be maintained; for example, the small houses with complex massing, the generous front, rear and side setbacks, the many mature trees and the irregular topography. These provisions should also guide new development in close proximity to the Heritage Conservation District.

16.17.2.13 The horizontal and vertical road alignments of existing roads within the Heritage Conservation District should be preserved with no widenings or significant changes to existing grades to ensure the preservation of existing hedgerow trees and Village character.

16.17.2.14 The ditched cross-sections of existing roads within the Heritage Conservation District should be maintained to retain character and to avoid disrupting the existing drainage pattern and thus affecting the health of existing trees; reconstruction of these roads to a curb and gutter cross-section will require an amendment to this Plan.

16.17.2.15 Outside the Heritage Conservation District, the street pattern should be highly interconnected to extend the street fabric of the Village, such as through a grid or modified grid street pattern with small blocks.

16.17.2.16 The existing grades should be maintained. Where acceptable drainage cannot be achieved through revised road layouts, lot sizes, lotting patterns or innovative drainage techniques,



Map 16-17.1: Meadowvale Village Precincts

regrading may be permitted, providing that the effect on topography and vegetation is minimized.

16.17.2.17 Development will comply with and ensure the protection of the Village elements as stated in the Heritage Character Statement and Heritage Attributes as contained in the Heritage Conservation District Plan.

16.17.2.18 The design of subdivisions will provide for the appropriate development of the rear yards of the existing lots fronting on both sides of Second Line West, south of Old Derry Road.

16.17.2.19 A concept plan will be required as part of the processing of any development application to illustrate the location of existing trees, the road and lotting pattern, connections to adjacent developments, existing and proposed grading, building envelopes, and garage locations.

16.17.2.20 The Precinct includes a progression of spaces and landscape features to define the edge of the Village; development near these gateways should enhance them and be in harmony with the character of the Village. The progression of spaces leading to the Village starts with a **streetscape** which is loosely enclosed by buildings or tree planting, followed by a **streetscape** which is enclosed by a canopy of trees which marks the entrance to the Village.

16.17.2.21 Lots should vary in size from street block to street block to create a varied and interesting **streetscape** sympathetic to the varied lot fabric of the Heritage Conservation District.

16.17.2.22 Limited land severances within the Heritage Conservation District that are in compliance with the applicable zoning regulations and respect the historic patterning of open space and their relationship to properties, structures and elements, will be permitted.

16.17.2.23 Building heights should be limited to two and a half storeys, lot coverage should be addressed, and provision made for generous setbacks to ensure a sense of spaciousness around the Village, with larger setbacks closer to the Village.

16.17.2.24 Any person undertaking to develop a building proposed to contain less than 25 residential dwelling units on lands within the Village Precinct will be required to provide such plans and drawings set out in the *Planning Act*, as amended.

16.17.2.25 Pedestrian access will be provided from the Community Centre through abutting land to the west of the existing park.

### 16.17.3 Transportation

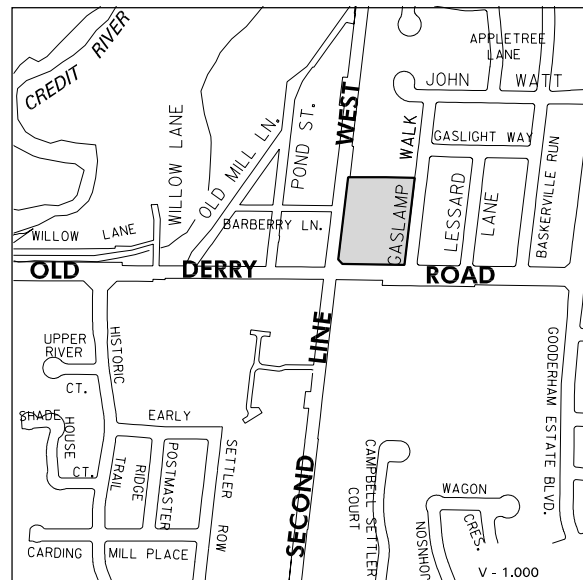
16.17.3.1 Public Lanes are considered part of the local road system and serve the rear of the properties that abut them. These Public Lanes, normally have rights-of-way less than 17 m, which will be determined during the development review process.

16.17.3.2 Second Line West may be terminated north and south of Provincial Highway 401 as part of the future widening of Provincial Highway 401. The precise timing and location of these points of termination north and south of Provincial Highway 401 will be determined by the City in conjunction with the appropriate authorities.

### 16.17.4 Special Site Policies

There are sites within the Character Area that merit special attention. Notwithstanding other policies of this Plan, any application for development of lands affected by a Special Site Policy will be subject to the provisions of this section and where applicable, consultation with the Mississauga Heritage Advisory Committee.

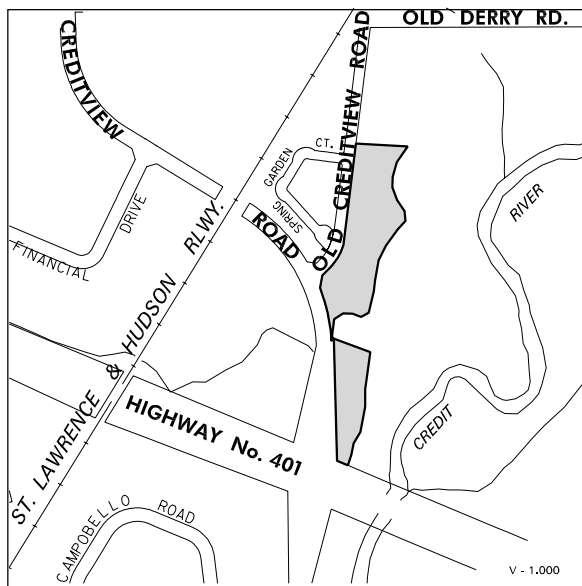
#### 16.17.4.1 Site 1



16.17.4.1.1 Within the area identified as Special Site 1, it is the intent of these Character Area Policies to establish a special site within the village. This site is located on the Gooderham Estate, located at the northeast corner of Old Derry Road and Second Line West.

16.17.4.1.2 Notwithstanding the provisions of the Residential Low Density I and Public Open Space designations, Special Site 1 may also include the integration of open space with existing buildings and structures to be used for public and/or private uses. Permitted uses within the existing building designated Residential Low Density may include but are not limited to, overnight accommodation, including bed and breakfast, restaurants, public and private community uses, an art gallery, and multiple unit housing.

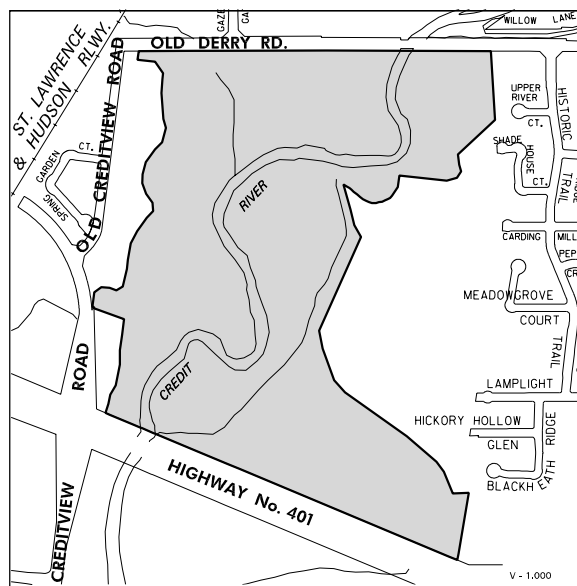
#### 16.17.4.2 Site 2



16.17.4.2.1 The lands identified as Special Site 2 are located on the east side of Old Creditview Road and the east side of Creditview Road, north of Provincial Highway 401.

16.17.4.2.2 Notwithstanding the provisions of the Residential Low Density II designation, the lands may also be used for cluster townhouses, and offices having a residential scale and character at a maximum density of 0.5 **floor space index (FSI)**. Hospitality and recreational uses will be permitted, provided such uses are accessory to the establishment of a golf course on the lands designated Special Site 3.

#### 16.17.4.3 Site 3



16.17.4.3.1 The lands identified as Special Site 3 are located south of Old Derry Road and straddle the Credit River.

16.17.4.3.2 The lands are portions of the property known in 1995 as Sanford Farm and are designated Greenbelt. Notwithstanding the Greenbelt designation, these lands may be used for a golf course.

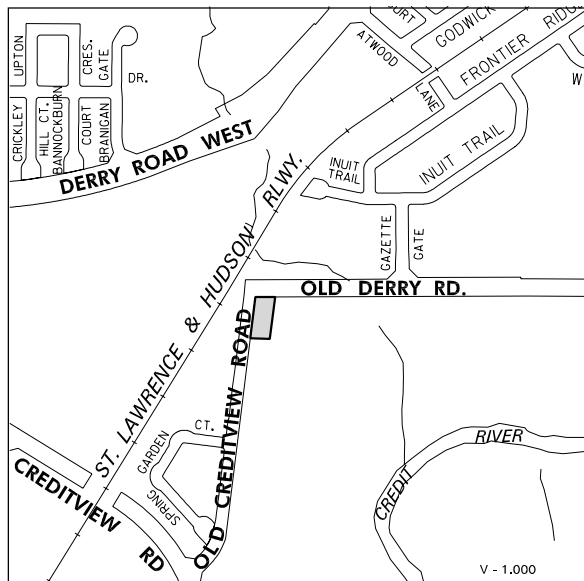
16.17.4.3.3 Development of Special Site 3 as a golf course will be subject to a comprehensive development concept for the site and any portion of Special Site 2 which would be used in connection with Special Site 3, will among other matters, satisfactorily address the following issues:

- a. compliance with Credit Valley Conservation's Policies for Floodplain Management to address concerns related to flooding hazards, flood conveyance, floodplain storage and ice jamming along the Credit River;
- b. compliance with Credit Valley Conservation's Watercourse and Valleyland Protection Policies to address environmental concerns, including the protection and preservation of native fish habitat;



- c. compliance with Credit Valley Conservation's Guidelines for the Review of Golf Course Development Proposals Within the Credit River Watershed; and
- d. the widening of Provincial Highway 401.

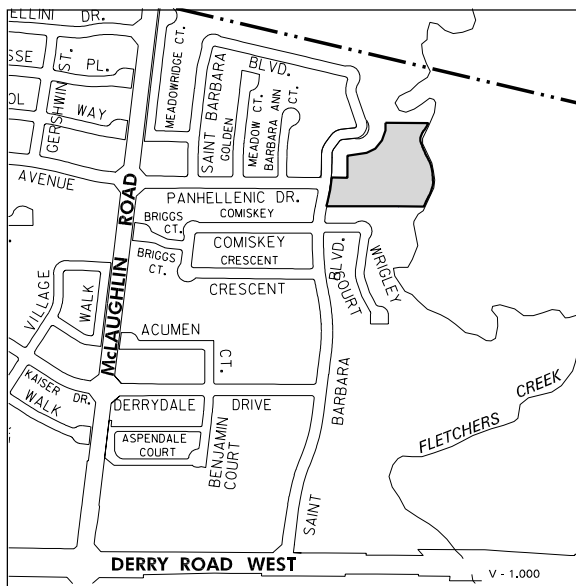
#### 16.17.4.4 Site 4



16.17.4.4.1 The lands identified as Special Site 4 are located at the southeast corner of Old Creditview Road and Old Derry Road.

16.17.4.4.2 Notwithstanding the provisions of the Public Open Space designation, the lands may be used on an interim basis, for the sale of fresh produce, vegetables, and fruit, until such time as they are acquired for park purposes by the City of Mississauga.

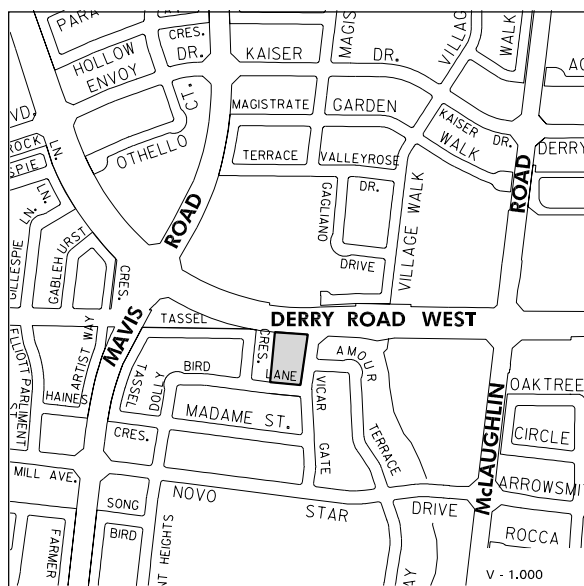
#### 16.17.4.5 Site 5



16.17.4.5.1 The lands identified as Special Site 5 are located east of McLaughlin Road and north of Derry Road West.

16.17.4.5.2 Notwithstanding the provisions of the Residential Low Density II and Greenbelt designations, the lands may be used as a place of religious assembly. Subject to the Greenbelt policies of this Plan and Credit Valley Conservation policies for valleyland protection and floodplain management, the lands may be used for place of religious assembly, related and passive recreational uses, the exact nature and extent of which will be determined during the processing of development applications.

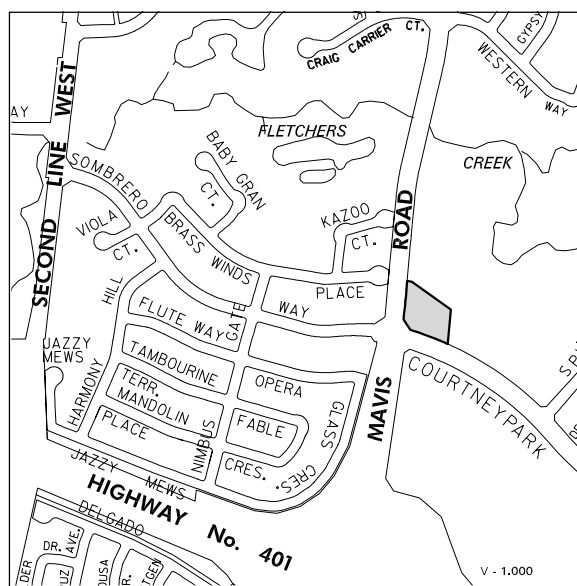
#### 16.17.4.6 Site 6



16.17.4.6.1 The lands identified as Special Site 6, comprise the Brown-Vooro House, located on the south side of Derry Road West, west of McLaughlin Road.

16.17.4.6.2 Notwithstanding the provisions of the Residential Low Density II designation, the existing house may be converted to a restaurant and/or offices subject to the provision of access to the satisfaction of the Region of Peel.

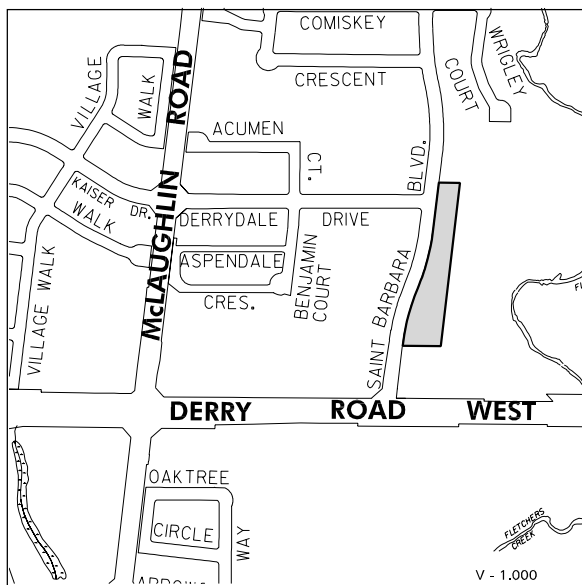
#### 16.17.4.7 Site 7



16.17.4.7.1 The lands identified as Special Site 7 are located at the northeast corner of Courtney Park Drive and Mavis Road.

16.17.4.7.2 Notwithstanding the provisions of the Motor Vehicle Commercial designation, the lands may be developed for Convenience Commercial uses.

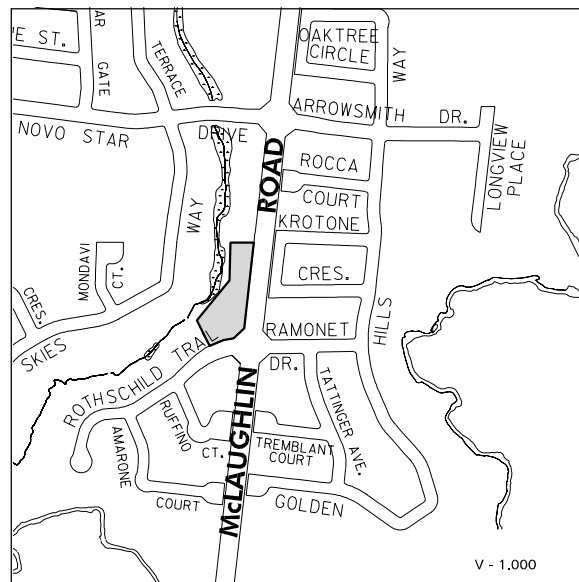
#### 16.17.4.8 Site 8



16.17.4.8.1 The lands identified as Special Site 8 are located north of Derry Road West, east of McLaughlin Road.

16.17.4.8.2 Notwithstanding the provisions of the Business Employment designation, all community infrastructure will not be permitted.

#### 16.17.4.9 Site 9



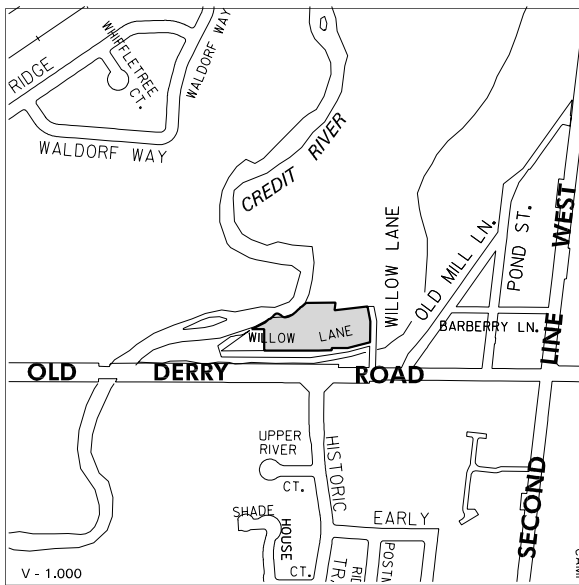
16.17.4.9.1 The lands identified as Special Site 9 are located on the west side of McLaughlin Road, south of Derry Road West.

16.17.4.9.2 Notwithstanding the provisions of the Residential Medium Density designation, apartment dwellings will be permitted.

16.17.4.10.2 Development will have regard for its relationship to the Meadowvale Village Heritage Conservation District and will be designed in a similar manner to the lands to the east of the site and incorporate similar design features.

## 16.17.5 Exempt Site Policies

### 16.17.5.1 Site 1



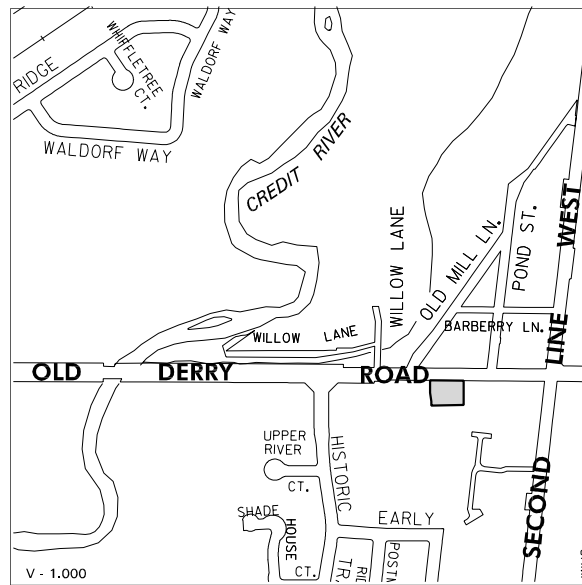
16.17.5.1.1 The lands identified as Exempt Site 1 are located on the north side of Willow Lane, and are subject to flooding from the Credit River.

16.17.5.1.2 Notwithstanding the provisions of the Greenbelt designation, detached dwellings will also be permitted in accordance with the Residential Low Density I policies of this plan.

Development of the subject lands will also be subject to the following:

- compliance with Credit Valley Conservation's *Policies for Floodplain Management* to address concerns related to flooding hazards, flood conveyance, floodplain storage and ice jamming along the Credit River; and
- compliance with Credit Valley Conservation's Watercourse and Valleyland Protection Policies to address environmental concerns, including the protection and preservation of native fish habitat.

### 16.17.5.2 Site 2

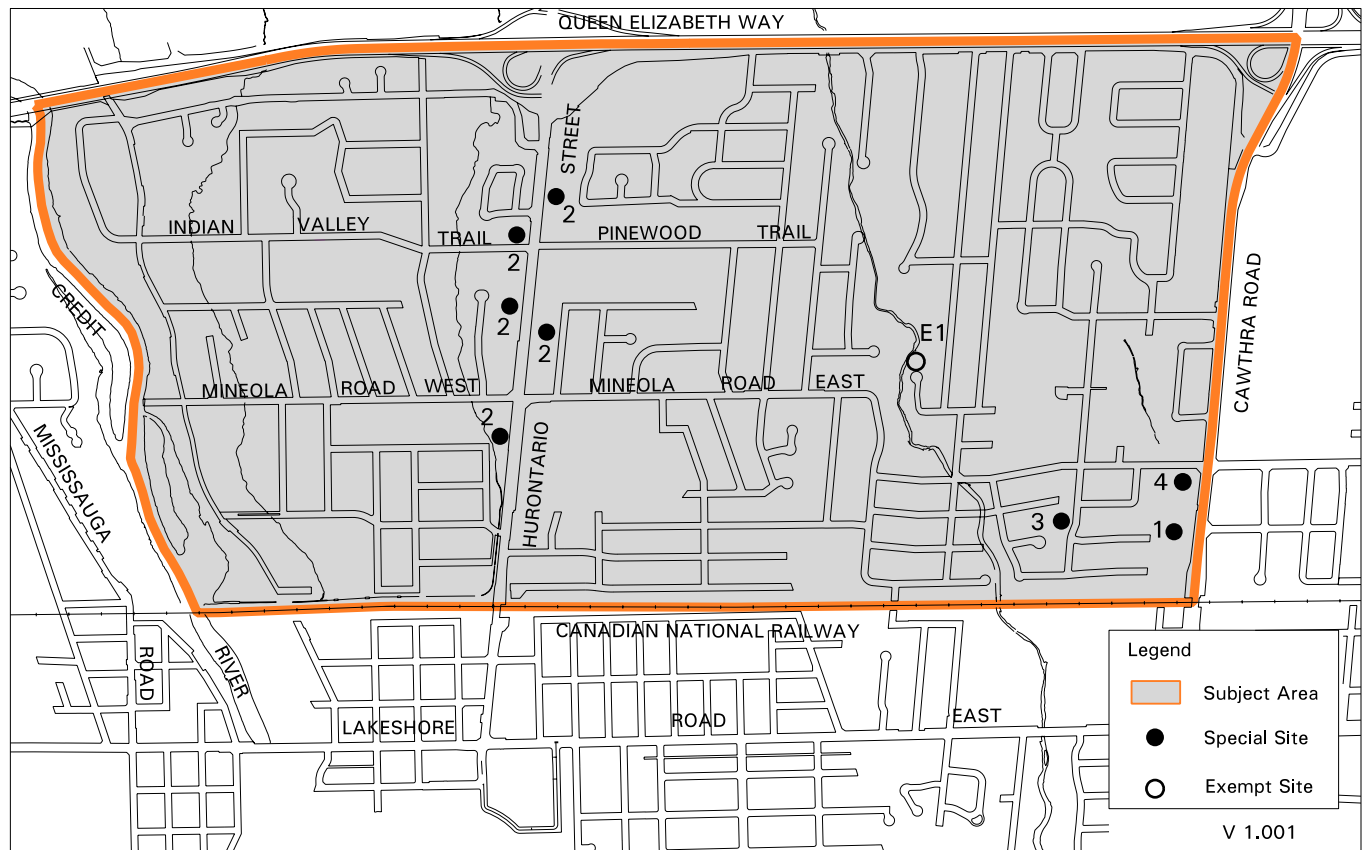


16.17.5.2.1 The lands identified as Exempt Site 2 are located on the south side of Old Derry Road, west of Second Line West.

16.17.5.2.2 Notwithstanding the provisions of the Convenience Retail Commercial designation, the existing motor vehicle service station, existing detached dwelling and the dwelling unit above a retail store in an existing detached dwelling will only be permitted, provided that it is in keeping with the historic character of the Meadowvale Village Heritage Conservation District Plan.



## 16.18 Mineola



Map 16-18: Mineola Neighbourhood Character Area

### 16.18.1 Urban Design Policies

#### Infill Housing

16.18.1.1 For development of all detached dwellings on lands identified in the Site Plan Control By-law, the following will apply:

- a. preserve and enhance the generous front, rear and side yard setbacks;
- b. ensure that existing grades and drainage conditions are preserved;
- c. encourage new housing to fit the scale and character of the surrounding area, and take advantage of the features of a particular site, i.e. topography, contours, mature vegetation;
- d. garages should be recessed or located behind the main face of the house. Alternatively, garages should be located in the rear of the property;
- e. ensure that new development has minimal impact on its adjacent neighbours with respect to overshadowing and overlook;
- f. encourage buildings to be one to two storeys in height. The design of the building should de-emphasize the height of the house and be designed as a composition of small architectural elements, i.e. projecting dormers and bay windows;

- g. reduce the hard surface areas in the front yard;
- h. existing trees, large groupings or areas of vegetation and landscape features such as retaining walls, fences, hedgerows, etc. should be preserved and enhanced, along with the maintenance of topographic features and drainage systems;
- i. large accessory structures will be discouraged, and any accessory structures will be located in side and rear yards only;
- j. house designs which fit with the scale and character of the local area, and take advantage of the particular site are encouraged. The use of standard, repeat designs is strongly discouraged; and
- k. the building mass, side yards and rear yards should respect and relate to those of adjacent lots.

### **Streetscape**

16.18.1.2 On lands adjacent to Hurontario Street, the existing mature vegetation, well landscaped appearance and generous setbacks will be maintained to reflect area character. As Hurontario Street is a gateway to the Character Area, as well as Port Credit, consideration should be given to: additional tree planting, a sodded boulevard, a bicycle route and a right-of-way design that is sympathetic to the character of the area.

16.18.1.3 On Mineola Road East and West, consideration should be given to additional tree planting.

16.18.1.4 Open ditch road cross-sections should be maintained, as they contribute to the character of the area.

## **16.18.2 Land Use**

16.18.2.1 Notwithstanding the Residential Low Density I and Residential Low Density II policies of this Plan, the Residential Low Density I and

Residential Low Density II designations permit only detached dwellings.

16.18.2.2 Notwithstanding the Residential Medium Density policies of this Plan, the Residential Medium Density designation permits only townhouses dwellings.

16.18.2.3 Notwithstanding the Office policies of this Plan, only a dispensary and cafeteria for the use of office employees will be permitted as accessory uses.

## **16.18.3 Environmental Areas**

16.18.3.1 Notwithstanding the Natural Areas System policies of this Plan concerning residential woodlands, sites characterized by native understory that is in generally good condition, will be subject to an Environmental Impact Study (EIS).

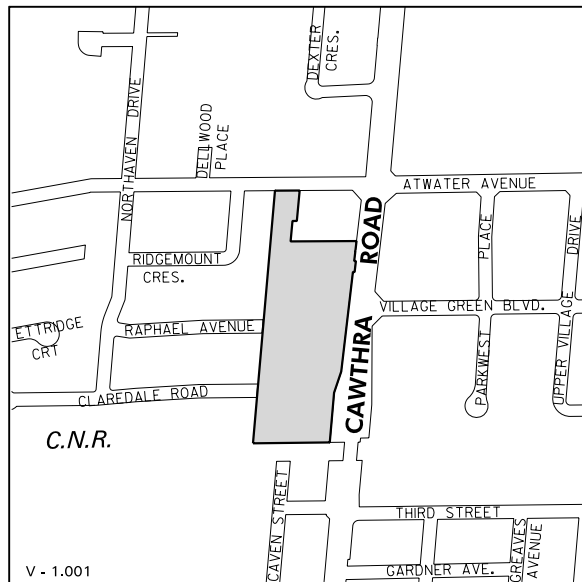
## **16.18.4 Transportation**

16.18.4.1 Hurontario Street will not be built in excess of four lanes excluding turning lanes and bus bays, unless it can be demonstrated that additional lanes will not result in a major deterioration of the neighbouring residential or commercial environment. The implementation of such major roadway modifications will require an official plan amendment.

## 16.18.5 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

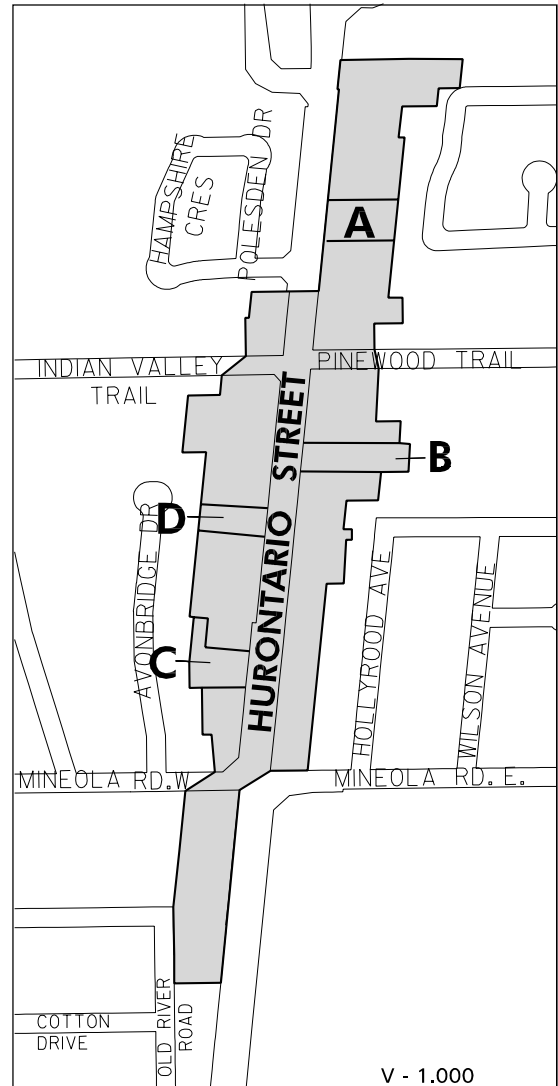
### 16.18.5.1 Site 1



16.18.5.1.1 The lands identified as Special Site 1 are located on the west side of Cawthra Road, south of Atwater Avenue, and north of the CNR line.

16.18.5.1.2 Notwithstanding the provisions of the Residential Medium Density designation, detached, semi-detached and townhouse dwellings will be permitted within a net density range of 25-37 units per **net residential hectare**. Assembly of separate parcels will be encouraged.

### 16.18.5.2 Site 2



16.18.5.2.1 The lands identified as Special Site 2 are the lots which front onto the east and west sides of Hurontario Street, south of the Queen Elizabeth Way.

16.18.5.2.2 Notwithstanding the provisions of the Residential Low Density I designation, the following additional policies will apply:

- a. The following uses will also be permitted:
  - offices, provided that medical offices are used for the consultation, examination or therapeutic treatment by a physician, dentist, drugless practitioner or health professional licensed by the Province of

Ontario. Medical offices may not include hospitals or other accessory medical uses such as, laboratories, diagnostic facilities for medical and dental purposes, a drug and optical dispensary, nor a medical supply and equipment store; and

- a detached dwelling containing a maximum of one dwelling unit in combination with office uses; and
- b. commercial uses, which include personal service uses, will not be permitted as a primary or as an accessory use.
- c. all buildings used for office or residential-office purposes, whether new or modified, will have a residential appearance which is compatible with the form, design, and scale of the surrounding residential area. All development will be designed so that it does not negatively impact abutting properties used for residential purposes, in terms of light, privacy, noise and rear yard amenity.
- d. the use must be of a nature and intensity that results in a parking demand which does not negatively impact the function of Hurontario Street nor abutting local roads. In this regard, applicants for rezoning will be required to demonstrate, through the submission of traffic studies, parking justification studies and business operation information, that the proposed use is suitable for its location. Developments which result in a reduced parking demand are preferred.
- e. all office related parking will be accommodated within the front and side yards only, with a minimal loss of vegetation or in underground facilities. Rear yard parking, with the exception of parking for residential dwellings, is not permitted. Surface parking areas are permitted to be constructed of permeable materials in order to achieve sustainable and urban design objectives and reduce impacts on drainage and grading systems. The portion of the access

driveway within the road allowance is to be constructed of hard surface pavement.

- f. where appropriate, mutual driveway access will be permitted between abutting property owners provided this arrangement is registered on title.
- g. for developments located at intersections, access to the minor streets will be discouraged, and where technically feasible, access will be permitted on Hurontario Street.
- h. street frontages are required to be enhanced with landscaping to reflect Hurontario Street's role as a gateway to the Mineola and Port Credit Character Areas. On-site parking areas should not dominate the **streetscape** and are required to be appropriately screened by vegetation and landscape treatments which are complementary to the character of the area. In this regard the following will also apply:
  - a minimum of 40% of the front yard of interior lots, will be landscaped open space. Landscaped open space may include pedestrian walkways but will exclude paved parking areas;
  - corner lots will provide a total minimum landscaped open space area of 40% of the lot;
  - a landscape buffer ranging from 4.5 m to 7.5 m in depth will be provided along the Hurontario Street frontage; and
  - a minimum landscape buffer of 4.5 m in depth will be provided along side street frontages adjacent to parking areas; and
- i. New or modified office or residential-office buildings will not exceed:
  - 420 m<sup>2</sup> Gross Floor Area (GFA);
  - a maximum lot coverage of 25%; and
  - a maximum building height of two storeys and a 10.7 m mean height level between the eaves and the ridge of a sloped roof or

7.5 m to the highest point of the surface of a flat roof.

- j. The following yards are required for office or residential-office buildings:
- minimum 1.8 m side yard for a one storey building;
  - minimum 2.4 m side yard for a two storey building;
  - minimum 7.5 m rear yard; and
  - maximum front yard equivalent to 50% of the lot depth.

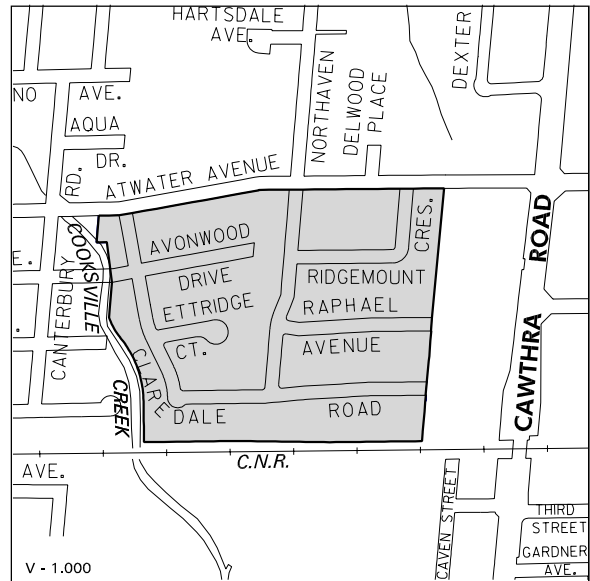
16.18.5.2.3 Notwithstanding subsection i. of this section, for the lands identified as Area A, the existing building at 1523 Hurontario Street will not exceed 585 m<sup>2</sup> Gross Floor Area (GFA).

16.18.5.2.4 Notwithstanding subsection i. of this section, for the lands identified as Area B, an office building at 1443 Hurontario Street will not exceed 552 m<sup>2</sup> Gross Floor Area (GFA).

16.18.5.2.5 Notwithstanding subsection i. of this section, for the lands identified as Area C, an office building at 1370 Hurontario Street will not exceed 465 m<sup>2</sup> Gross Floor Area (GFA).

16.18.5.2.6 Notwithstanding subsection i. of this section, for the lands identified as Area D, for the building at 1424 Hurontario Street, the maximum Gross Floor Area (GFA) will not exceed 460 m<sup>2</sup> and the maximum Gross Floor Area (GFA) used for an office will not exceed 250 m<sup>2</sup>.

### 16.18.5.3 Site 3

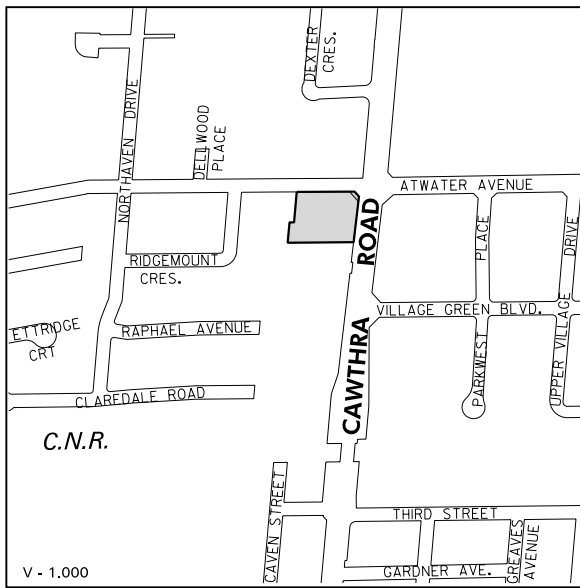


16.18.5.3.1 The lands identified as Special Site 3 are located north of the Canadian National Railway right-of-way, south of Atwater Avenue, and east of the Cooksville Creek.

16.18.5.3.2 Notwithstanding the provisions of the Residential Low Density II designation, semi-detached dwellings will also be permitted.



#### 16.18.5.4 Site 4



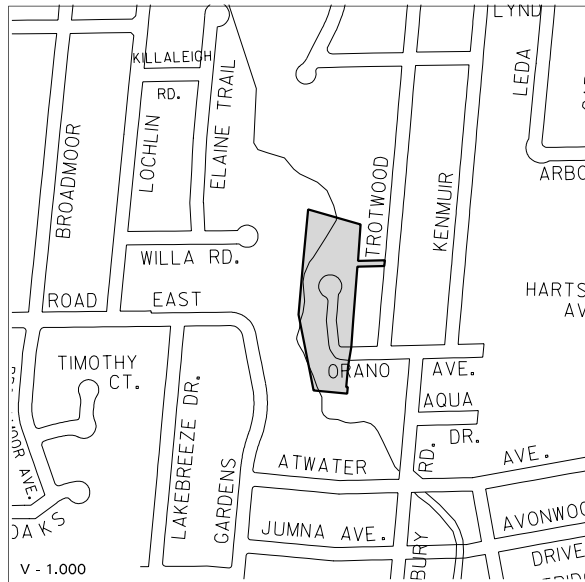
16.18.5.4.1 The lands identified as Special Site 4 are located at the southwest corner of Atwater Avenue and Cawthra Road.

16.18.5.4.2 Notwithstanding the provisions of this Plan, the following additional policies will apply:

- a. a maximum **floor space index (FSI)** of 1.46 will be permitted; and
- b. the maximum apartment building height will be four storeys.

## 16.18.6 Exempt Site Policies

### 16.18.6.1 Site 1

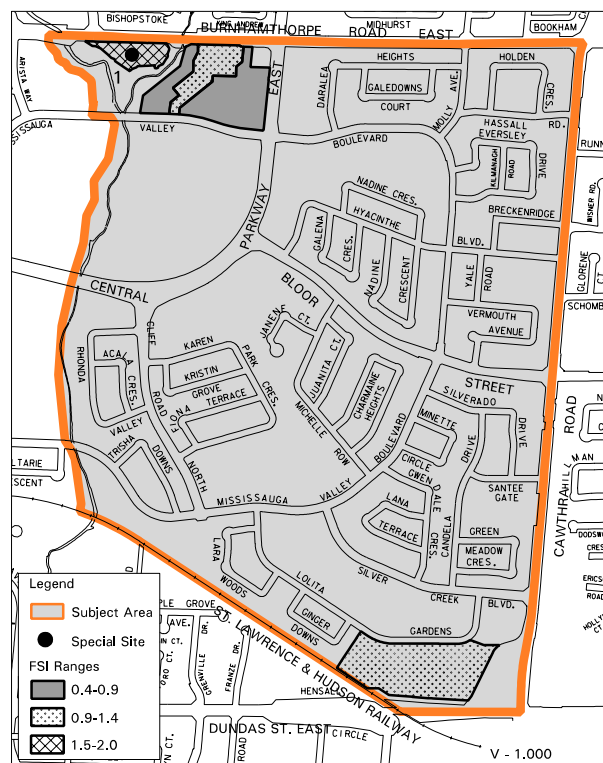


16.18.6.1.1 The lands identified as Exempt Site 1 are bounded by the rear lot lines of properties on Mineola Gardens and Willa Road to the west, and the rear lot lines of properties fronting on Trotwood Avenue to the east. Orano Avenue terminates near the southerly limit of the property. The subject site is traversed by Cooksville Creek, and is partially within the regulatory **floodplain** of Cooksville Creek.

16.18.6.1.2 Notwithstanding the Greenbelt designation, detached dwellings will also be permitted subject to the Residential Low Density II policies. New lots which extend into the **floodplain** will not be permitted. Access to approved lots must be flood free or flood proofed. In addition, a significant setback is required to address channel bank erosion (21 m).

In addition to being in the **floodplain**, this site is located partially within a woodlot. Due to both of these environmental constraints, Mississauga may require that an **Environmental Impact Study (EIS)** be undertaken for any proposed development.

## 16.19 Mississauga Valleys



Map 16-19: Mississauga Valleys Neighbourhood Character Area

### 16.19.1 Urban Design Policies

16.19.1.1 Where existing noise attenuation walls or rear yard privacy fencing are exposed to public streets, supplementary planting and upgraded landscape features should be added, where feasible, as a condition of development or road reconstruction. Mississauga will encourage landowners to coordinate the eventual replacement of fences to enhance the appearance of the area from the street. Special consideration should be given to Burnhamthorpe Road East, Cawthra Road, Bloor Street and Cliff Road North.

### 16.19.2 Land Use

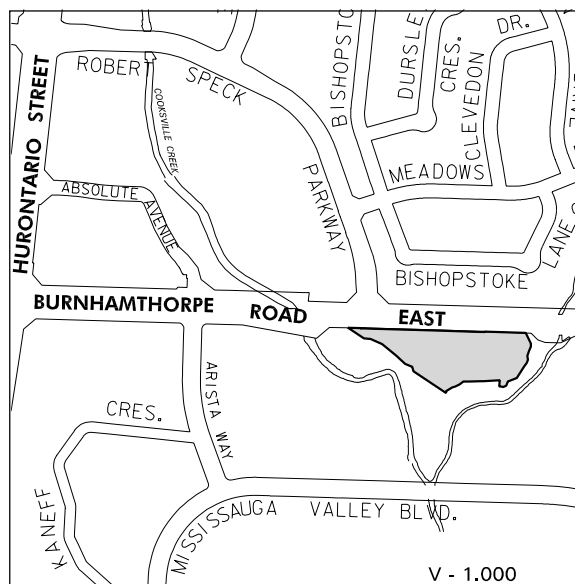
16.19.2.1 Notwithstanding the Residential Low Density I policies of this Plan, the Residential Low Density I designation permits only detached dwellings.

16.19.2.2 Lands designated Residential Medium Density will also permit low-rise apartment dwellings.

### 16.19.3 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 16.19.3.1 Site 1

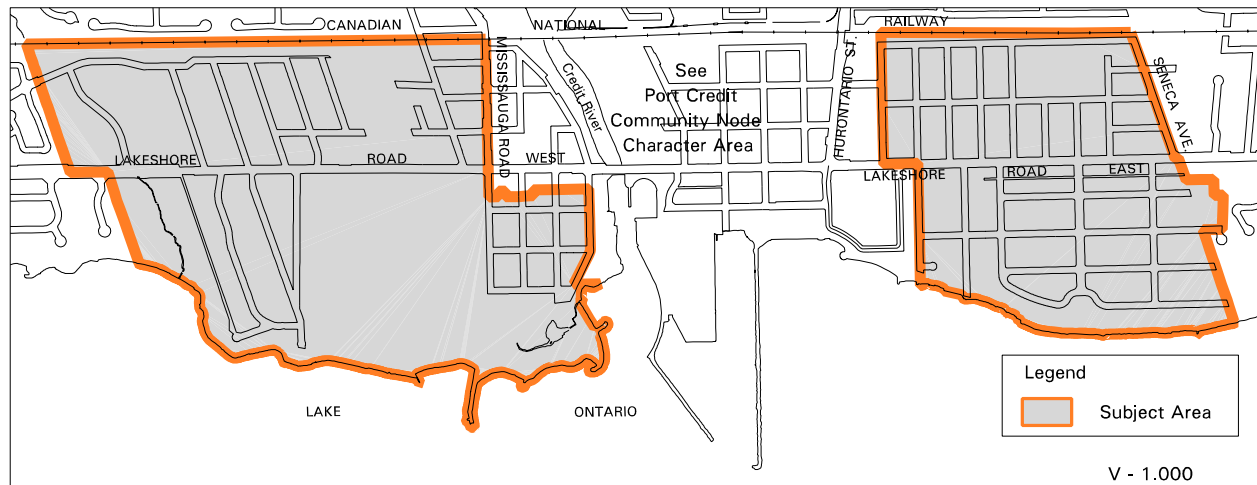


16.19.3.1.1 The lands identified as Special Site 1 are located on the south side of Burnhamthorpe Road East, west of Central Parkway East.

16.19.3.1.2 Notwithstanding the provisions of the Residential High Density designation, the maximum **floor space index (FSI)** will be 3.5.

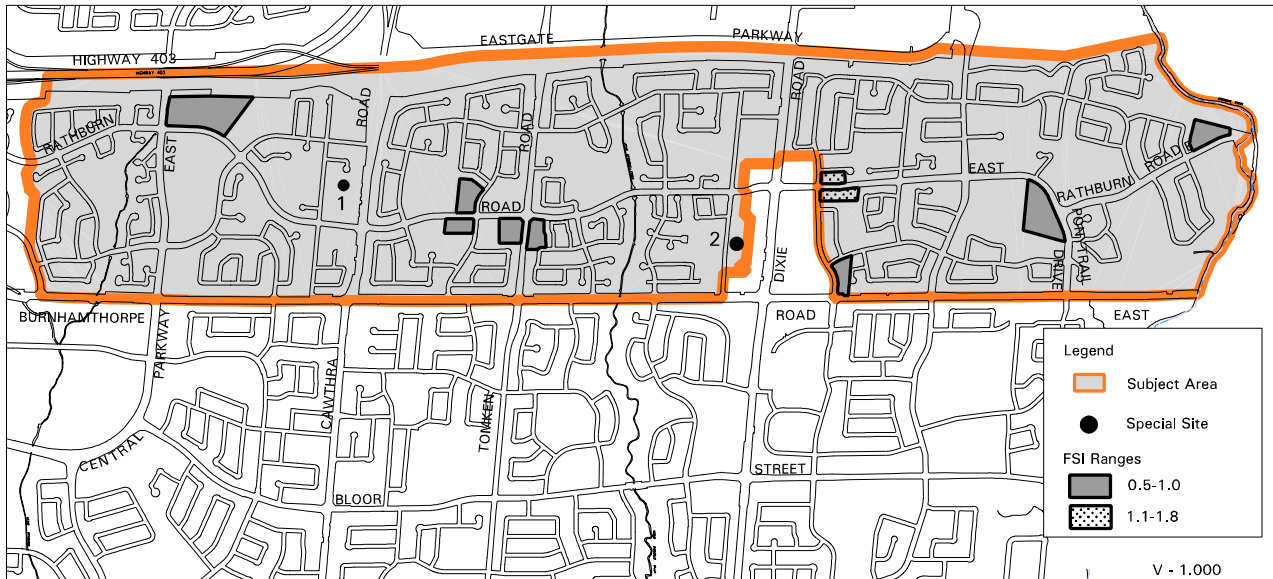
## 16.20 Port Credit

Official Plan policies for lands within the Port Credit Neighbourhood are contained in the Port Credit Local Area Plan.



Map 16-20: Port Credit Neighbourhood Character Area

## 6.21 Rathwood



Map 16-21: Rathwood Neighbourhood Character Area

### 16.21.1 Urban Design Policies

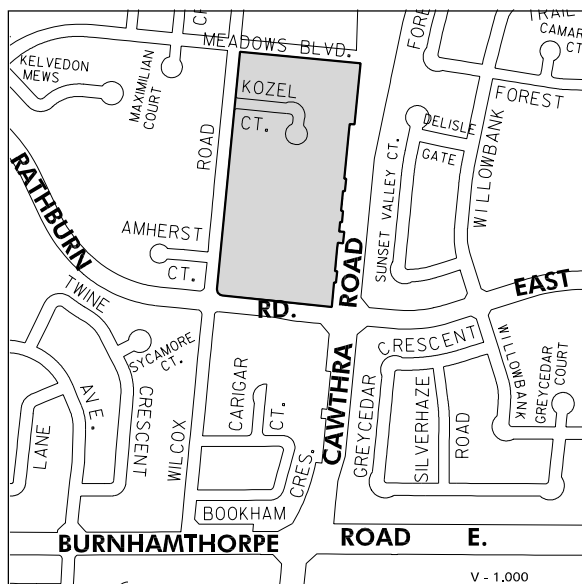
16.21.1.1 Mississauga encourages efforts to improve existing reverse frontage development along limited access roadways, particularly Burnhamthorpe Road East and Rathburn Road East, through the use of landscaping and the promotion of a uniform and well maintained rear yard fencing system.



## 16.21.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 16.21.2.1 Site 1



16.21.2.1.1 The lands identified as Special Site 1 are located west of Cawthra Road and north of Rathburn Road East.

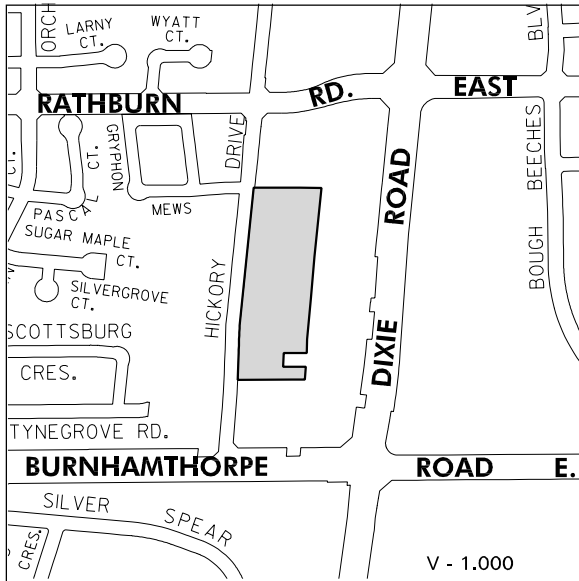
16.21.2.1.2 Notwithstanding the provisions of the Residential Low Density I designation of this Plan, the following additional policies will apply:

- a. a concept plan for all or part of this site may be required and will address, among other matters, the following:
  - compatibility of building form and scale with existing surrounding land uses;
  - convenient pedestrian access through this site to nearby transit service on Rathburn Road East, Cawthra Road and the future Mississauga Bus Rapid Transit (BRT) facility;
  - the potential to access all lands designated Residential Low Density I from Rathburn Road East;

- traffic generated will not adversely affect the transportation system;
  - acceptable ingress and egress, off-street parking, landscaping and buffering; and
  - preservation of mature trees and other significant natural features; and
- b. the concept plan will be required to address, to the satisfaction of the City of Mississauga and the Region of Peel, matters relating to transportation and access onto Cawthra Road;
  - c. Mississauga will permit the severance of the rear portions of lots fronting on Wilcox Road to encourage the assembly and comprehensive redevelopment of all Residential Low Density I lands in Special Site 1;
  - d. individual frontages on the east side of Wilcox Road will be developed in keeping with lot frontages on the west side of Wilcox Road;
  - e. the redevelopment of all lands designated Residential Low Density I will minimize access points to Cawthra Road to preserve the integrity of Cawthra Road as a major arterial roadway; and
  - f. access to individual properties on the north side of Rathburn Road East will be discouraged.

### 16.21.2.2 Site 2

the Region of Peel, matters relating to transportation.

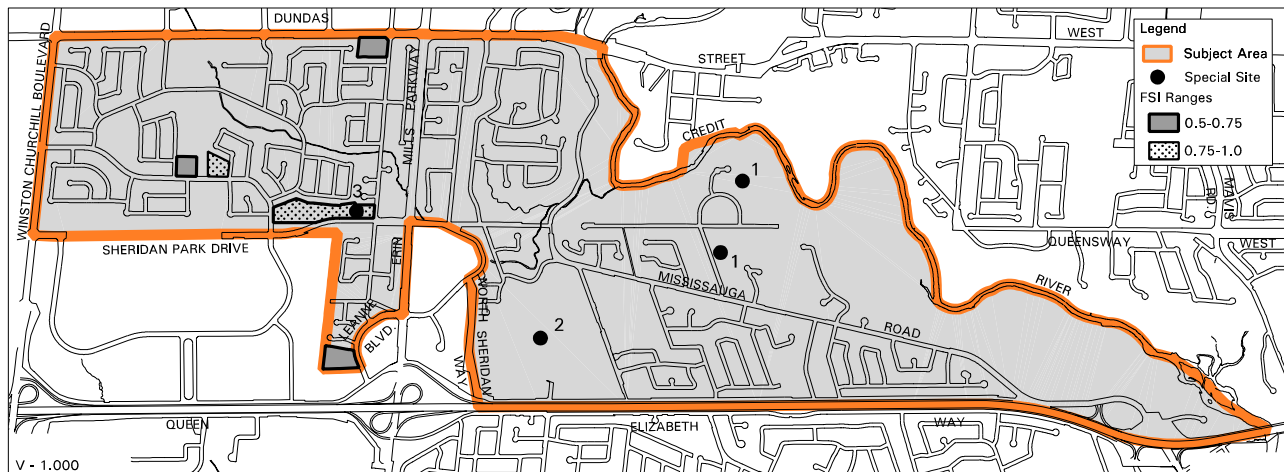


16.21.2.2.1 The lands identified as Special Site 2 are located west of Dixie Road and south of Rathburn Road East.

16.21.2.2.2 Notwithstanding the provisions of the Residential Medium Density designation, the following additional policies will apply:

- a. a concept plan for all or part of this site may be required and will address, among other matters, the following:
  - compatibility of building form and scale with existing surrounding land uses;
  - convenient pedestrian access through the site to nearby transit service on Dixie Road and Burnhamthorpe Road East;
  - traffic generated will not adversely affect the transportation system;
  - acceptable ingress and egress, off street parking, landscaping and buffering; and
  - preservation of all mature trees and other significant natural features; and
- b. a concept plan will be required to address, to the satisfaction of the City of Mississauga and

## 16.22 Sheridan



Map 16-22: Sheridan Neighbourhood Character Area

### 16.22.1 Land Use

16.22.1.1 Notwithstanding the Residential Low Density I policies of this Plan, the Residential Low Density I designation permits only detached dwellings.

16.22.1.2 For lands designated Residential Low Density I, the subdivision of lots of less than 23 m frontage will be discouraged, if it is considered to be detrimental to the character of the surrounding area.

16.22.1.3 Lands designated Residential Low Density II will not permit the following uses:

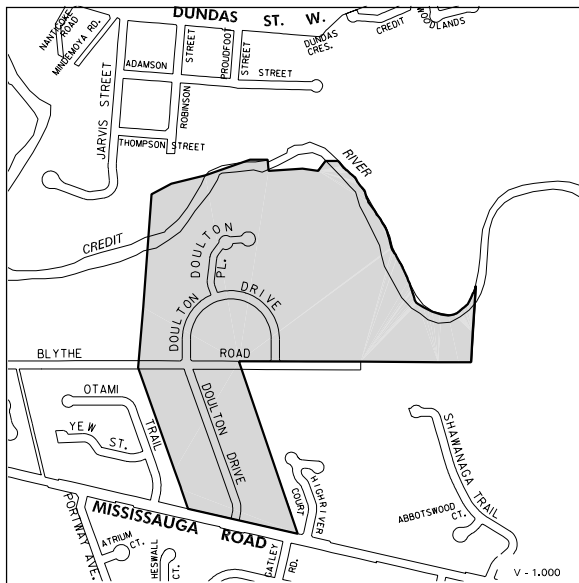
- a. triplexes, street townhouses and other forms of low-rise dwellings with individual frontages.

16.22.1.4 Notwithstanding the Business Employment policies of this Plan, overnight accommodations and conference centres will not be permitted.

### 16.22.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 16.22.2.1 Site 1

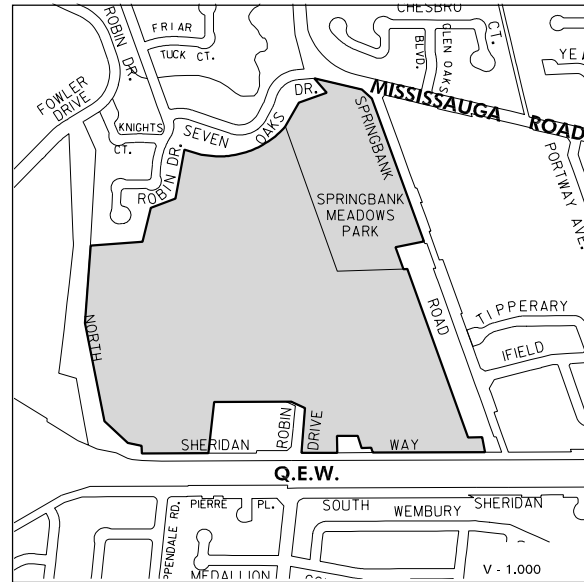


16.22.2.1.1 The lands identified as Special Site 1 are referred to as the Doulton Drive Lands.

16.22.2.1.2 Notwithstanding the provisions of the Residential Low Density I and Greenbelt designations, these lands will be subject to the following:

- Mississauga considers that the preservation of this distinctive area could be achieved with up to 47 residential lots;
- Mississauga will encourage a minimum lot area of 0.3 ha with a minimum lot frontage of 38 m, except for those lots fronting on Mississauga Road which are not corner lots, where a minimum lot area of 0.2 ha and a minimum lot frontage of 30 m are considered appropriate;
- new development will be subject to site plan approval to ensure that site areas, building, landscaping and open space are all satisfactorily located and designed;
- any redevelopment may proceed on the basis of private sewage disposal systems subject to the applicable regulations; and
- comprehensive site and environmental analyses will be required in support of any divisions of land.

### 16.22.2.2 Site 2



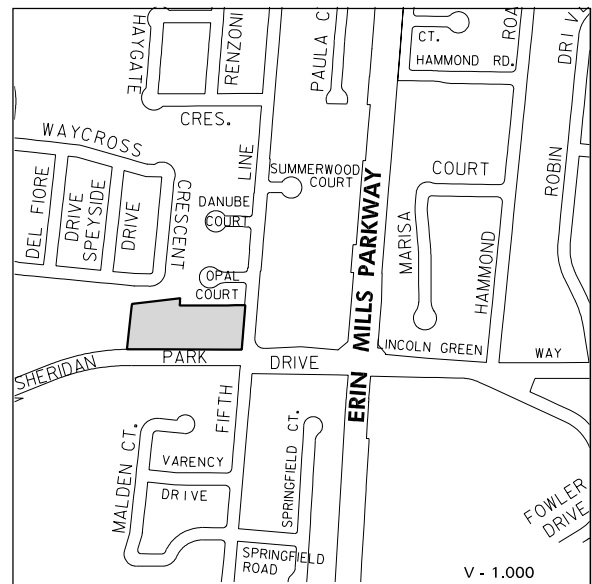
16.22.2.2.1 The lands identified as Special Site 2 are known as the North Sheridan Landfill site and Springbank Meadows Park. This site has been identified as a former **waste disposal site**. Permitted land uses include open space and privately operated uses of a recreation, conservation and agricultural nature, and other uses deemed compatible with open space.

16.22.2.2.2 Notwithstanding the provisions of the Public Open Space designation, the following additional policies will apply:

- development within or adjacent to these lands will be restricted where such activity would constitute a hazard to human or ecosystem health;
- prior to the consideration of development proposals on, or in proximity to, these lands, a detailed study will be submitted by the applicant to determine the impact and area of influence of the **waste disposal site**. Where no information is available on the influence area of the site, a 500 m radius surrounding the **waste** cell is recommended by the Provincial Government. Any remediation that must be undertaken will be in accordance with Provincial Government legislation and guidelines;

- c. development on lands formerly used for the disposal of **waste**, within a period of 25 years or less, requires approval of the Provincial Government;
- d. the site will be planned, designed, operated and maintained to the satisfaction of the City, the Region of Peel, and the Provincial Government in such a way as to ensure compatibility with adjacent, existing and future land uses, and to ensure minimal adverse impact on the natural environment;
- e. there should be no development permitted on the site until the long term care, maintenance and security of the gas control system is established by the Region of Peel;
- f. a buffer zone of 20 m must be established around the perimeter of the site to be used exclusively as a corridor for the gas control system;
- g. any use that would allow the accumulation of methane gas requires the approval of the City and the Region of Peel;
- h. engineering studies must accompany development applications to determine any hazards of development and damage that may result to the subject property and surrounding lands from methane gas, leachate and settling;
- i. development proposals must demonstrate that they will not interfere with the migration gas control system for the site; and
- j. land uses and activities that may have an adverse impact on adjacent, existing and future residential land uses in terms of noise, safety, and traffic generation should be discouraged.

### 16.22.2.3 Site 3



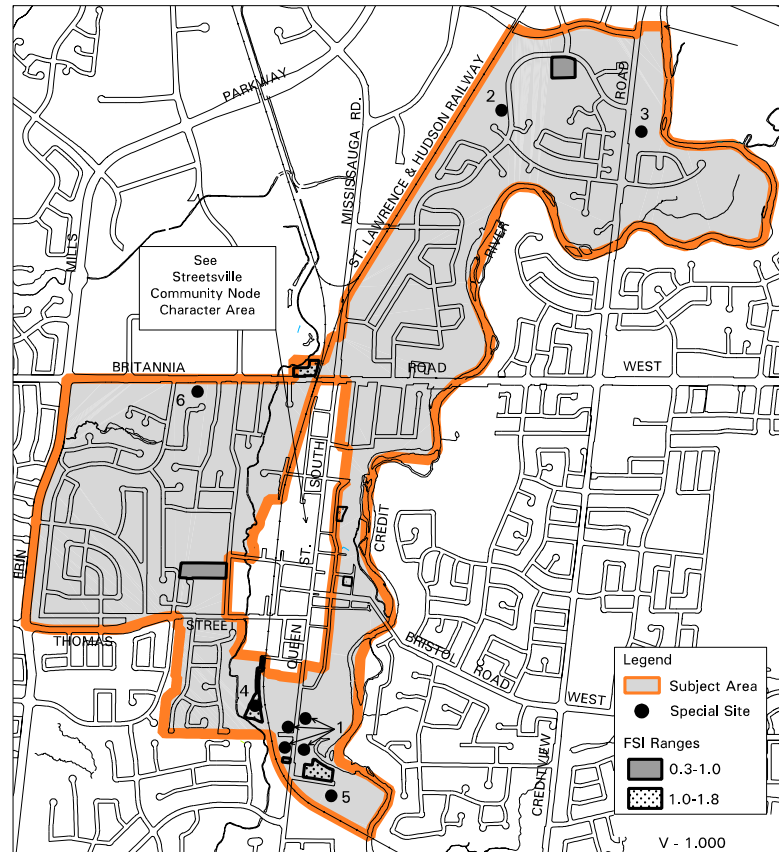
16.22.2.3.1 The lands identified as Special Site 3 are located at the northwest corner of Sheridan Park Drive and Fifth Line West.

16.22.2.3.2 Notwithstanding the provisions of the Residential High Density policies, a **floor space index (FSI)** of 0.5 will be permitted.



## 16.23 Streetsville

of Barry Avenue, will be maintained through appropriate building masses, setbacks, intensive landscaping, **streetscapes** with many mature trees, and a regular street grid pattern.



Map 16-23: Streetsville Neighbourhood Character Area

### 16.23.2 Land Use

16.23.2.1 In addition to the uses permitted by the Land Use Policies of this Plan, bed and breakfast establishments are permitted provided that they front upon Queen Street South, Main Street, Thomas Street or Church Street.

16.23.2.2 Notwithstanding the Residential Low Density I policies of this Plan, the Residential Low Density I designation permits only detached dwellings.

16.23.2.3 Lands designated Residential Medium Density will also permit low-rise apartment dwellings.

16.23.2.4 Notwithstanding the Business Employment designation only the following uses will be permitted:

- a. community infrastructure;
- b. entertainment, recreation and sports facility;
- c. financial institution;
- d. manufacturing;
- e. motor vehicle rental facility;
- f. research and development;
- g. sales and service accessory to a permitted use;
- h. **secondary office**; and
- i. warehousing, distribution and wholesaling.

### 16.23.1 Urban Design Policies

16.23.1.1 Development will be compatible with and enhance the village character of Streetsville as a distinct established community by integrating with the surrounding area.

16.23.1.2 Alterations to heritage structures, including building repairs and additions, and/or alterations to existing vegetation which is part of the cultural heritage landscape, will be in keeping with the original character of the heritage resources to be preserved.

16.23.1.3 The established residential character of the areas generally located along Main Street east of Church Street and along Queen Street South, south

### 16.23.3 Heritage

16.23.3.1 Heritage resources will include those properties listed on the City's Heritage Register, but will not be restricted to the list. There are heritage characteristics throughout Streetsville that may warrant study and the establishment of heritage conservation districts.

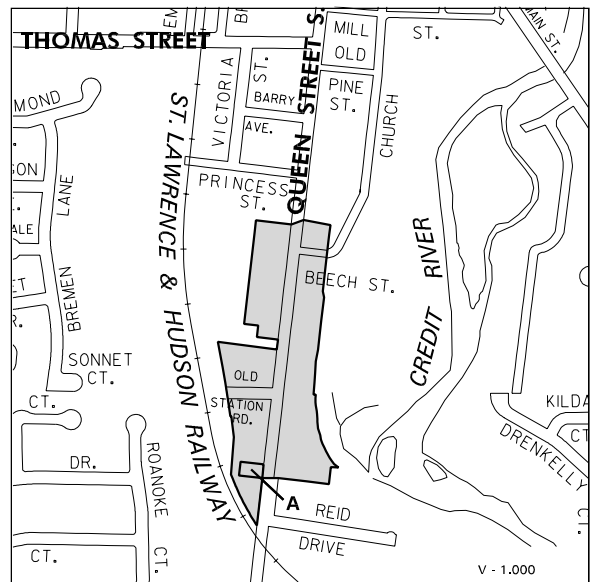
### 16.23.4 Stormwater Management

16.23.4.1 A stormwater drainage report will be required to ensure that the existing drainage system has the capability to convey the increased storm flow from redevelopment in accordance with current City drainage standards.

### 16.23.5 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 16.23.5.1 Site 1



16.23.5.1.1 Notwithstanding the provisions of the Low Density I and II designations, the lands identified as Special Site 1 which front onto Queen Street South, may also be used for offices.

16.23.5.1.2 Any office conversion should maintain a residential appearance in keeping with the existing scale, materials, and character of the existing dwellings in the immediate area. The existing houses should be preserved if at all possible, while the interior floor plan may be altered for office use.

16.23.5.1.3 Any additions and alterations of existing buildings should be sensitive to the existing vernacular and heritage village theme, and should be largely confined to the rear of the property.

16.23.5.1.4 Any additions, alteration, conversion or redevelopment should maintain the existing front yard setback, with the front yard used for landscaping.

16.23.5.1.5 Sufficient on-site parking, which will consist of only surface parking, as required by the Zoning By-law, should be provided in the rear yard only at grade, without removal of existing trees, except at the discretion of the City arborist.

16.23.5.1.6 Vehicular entrances should be combined to minimize the number of access points on Queen Street South.

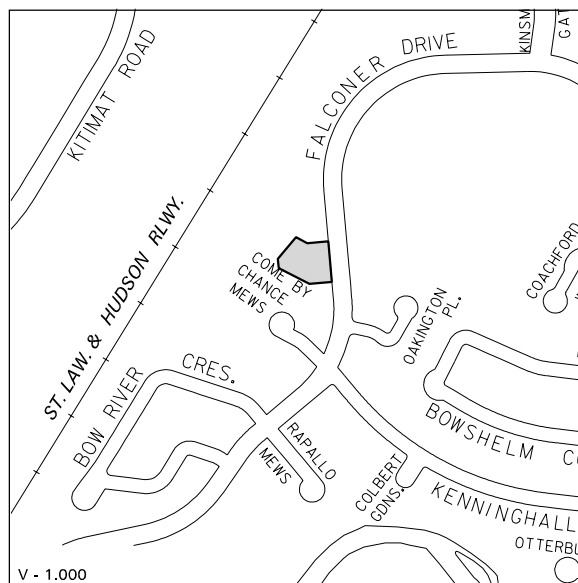
16.23.5.1.7 Minimal signage will be permitted. The design of such signage and external lighting should be compatible with the residential character of the area.

16.23.5.1.8 Existing lot sizes should be retained.

16.23.5.1.9 Rear yard drainage will be provided to the satisfaction of the City.

16.23.5.1.10 Notwithstanding the provisions of the Residential Medium Density designation, the lands identified as Area A may be used for a medical office and a maximum of three dwelling units at a maximum density of 33 units per ***net residential hectare***.

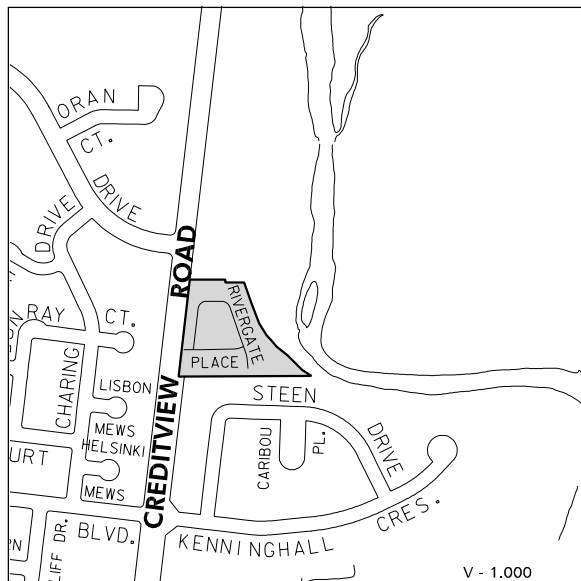
## 16.23.5.2 Site 2



16.23.5.2.1 The lands identified as Special Site 2 are located on the west side of Falconer Drive, north of Oakington Place.

16.23.5.2.2 Notwithstanding the provisions of the Mixed Use designation, only a day care facility will be permitted.

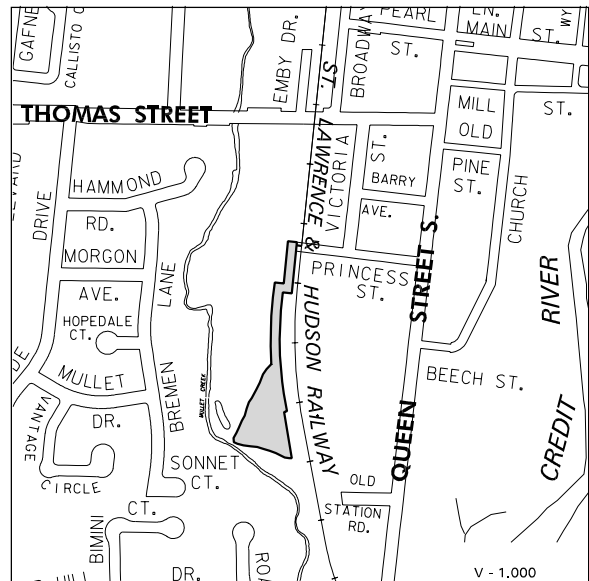
### 16.23.5.3 Site 3



16.23.5.3.1 The lands identified as Special Site 3 are located east of Creditview Road, north of Kenninghall Crescent.

16.23.5.3.2 Notwithstanding the provisions of the Residential Low Density II designation, the lands may be developed for townhouse development and semi-detached dwellings, or any combination thereof, with a maximum density of 15.5 units per **net residential hectare**.

### 16.23.5.4 Site 4



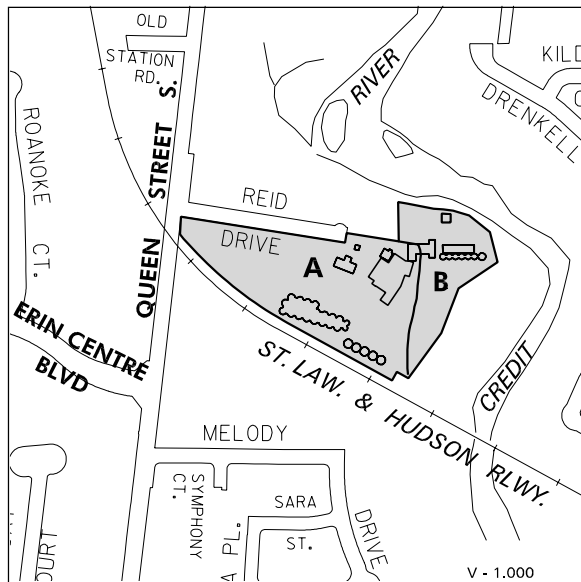
16.23.5.4.1 The lands identified as Special Site 4 are located south of Thomas Street, north of Old Station Road and west of the St. Lawrence and Hudson Railway.

16.23.5.4.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:

- a. the determination of the area suitable for redevelopment will have regard for the extent of the "regulatory storm" floodplain and the erosion hazards associated with Mullet Creek, whichever is greater. The extent of areas required for conservation purposes will be determined to the satisfaction of Credit Valley Conservation and the City;
- b. building forms should consist of low profile buildings ranging in height from three storeys near Mullet Creek to six storeys near the railway tracks; and
- c. redevelopment will include provisions for a community park and a multi-use recreational trail. The trail will be located adjacent to the east side of the Mullet Creek valley, from Tannery Street to the existing trail crossing Mullet Creek, south of Thomas Street. The location and size

of the community park will be determined through the preparation of a parkland concept plan.

#### 16.23.5.5 Site 5



16.23.5.5.3 Notwithstanding the provisions of the Greenbelt designation, on the lands identified as Area B, the existing buildings may be used for grain processing, grain milling and related accessory uses. Repairs that do not enlarge or extend the existing buildings or structures and which are for the purpose of maintenance or restoration of the buildings or structures to a safe condition will be permitted. Subject to the approval of the Credit Valley Conservation Authority, minor alterations or additions to existing buildings and structures to accommodate the replacement or upgrading of equipment, buildings or structures will be permitted. Subject to the approval of the Credit Valley Conservation Authority, existing buildings and structures may be replaced with new buildings and structures, provided that any new building or structure is not larger or higher than the building or structure it replaces and is in the same location as the building it replaces.

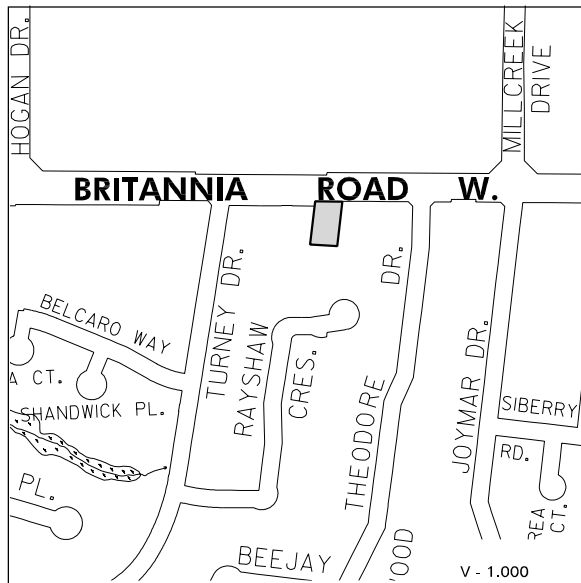
16.23.5.5.1 The lands identified as Special Site 5 are located east of Queen Street South, south of Reid Drive.

16.23.5.5.2 Notwithstanding the provisions of the Business Employment designation, the following additional policies will apply:

- a. the lands identified as Area A, may be used for the existing flour mill operation, and related accessory uses, including accessory offices, outdoor storage located to limit its visibility to Queen Street, Reid Drive and the Credit River valley, and the temporary storage of motor vehicles related to the primary use of the property. Any new construction will be limited to a construction envelope identified in the applicable implementing zoning by-law; and
- b. Amendments to the Official Plan and Zoning By-law will be required prior to the development of Area A for any purpose other than grain processing, grain milling, and related accessory uses.



**16.23.5.6 Site 6**



16.23.5.6.1 The lands identified as Special Site 6 are located on the south side of Britannia Road West, east of Turney Drive.

16.23.5.6.2 Notwithstanding the provisions of the Residential Low Density II designation, a dental office will be permitted.

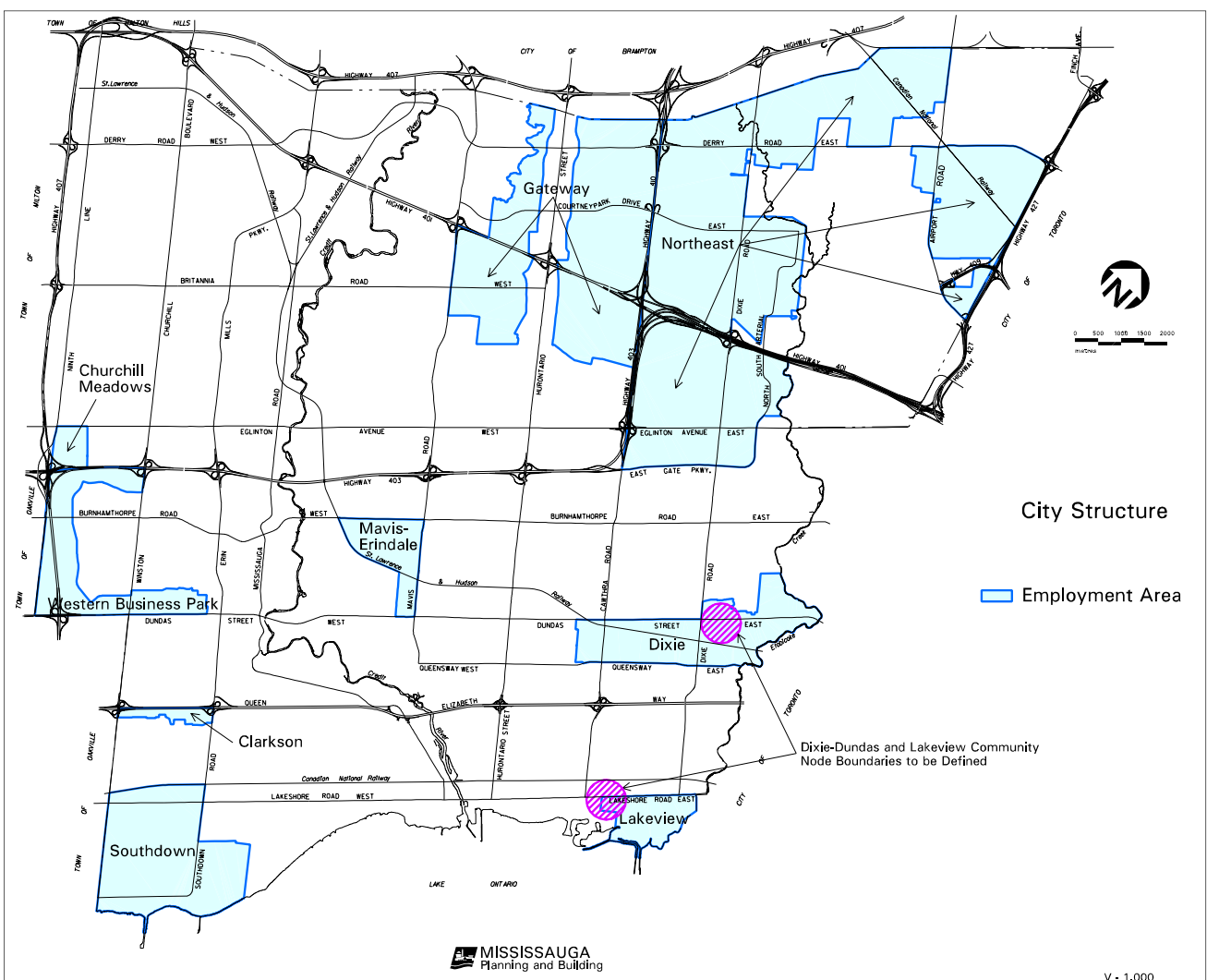
# 17 Employment Areas

## 17.1 Introduction

Employment Areas in Mississauga are located within the following nine Character Areas.

- Churchill Meadows;
- Clarkson;
- Dixie;
- Gateway;
- Lakeview;
- Mavis-Erindale;
- Northeast;
- Southdown; and
- Western Business Park.

This section identifies the modifications to the General Land Use designations in Chapter 11 that apply to all Employment Areas.



Map 17-1: City Structure – Employment Areas

### 17.1.1 General

17.1.1.1 Lands on a **Corridor** will not be subject to the two storey height minimum except along **Intensification Corridors** and within **Major Transit Station Areas**. Local area plans or planning studies may establish height requirements.

### 17.1.2 Utilities

17.1.2.1 Outdoor storage will be permitted accessory to a permitted land use adjacent to electric power rights-of-way.

### 17.1.3 Residential

17.1.3.1 Residential designations will not be permitted, except for permitted residential designations in the Dixie Employment Area in existence at the time this Plan comes into effect.

### 17.1.4 Mixed Use

17.1.4.1 Notwithstanding the Mixed Use policies of this Plan, the following additional uses will be permitted only in the Dixie, Gateway, Mavis-Erindale, Northeast, Southdown and Western Business Park Character Areas:

Business Employment Uses;

Motor Vehicle Commercial Uses; and

motor vehicle sales.

17.1.4.2 Notwithstanding the Mixed Use policies of this Plan, the following uses will not be permitted:

a. residential

17.1.4.3 Notwithstanding the Mixed Use policies of this Plan, the maximum **floor space index (FSI)** for **secondary offices** is 1.0.

17.1.4.4 The Mixed Use designation will not be permitted, except for lands designated Mixed Use at the time this Plan comes into effect.

### 17.1.5 Offices

17.1.5.1 Notwithstanding the Office policies of this Plan, the following uses will not be permitted:

a. Major office

17.1.5.2 Notwithstanding 17.1.5.1, existing major office is permitted and major office will be permitted in **Major Transit Station Areas**.

17.1.5.3 Notwithstanding the Office policies of this Plan, the maximum **floor space index (FSI)** for **secondary offices** is 1.0.

### 17.1.6 Institutional

17.1.6.1 Institutional uses will not be permitted.

### 17.1.7 Convenience Commercial

17.1.7.1 Notwithstanding the Convenience Commercial policies of this Plan, the following uses will not be permitted:

a. residential

17.1.7.2 The Convenience Commercial designation will not be permitted, except for lands designated Convenience Commercial at the time this Plan comes into effect.

### 17.1.8 Business Employment

17.1.8.1 Notwithstanding the Business Employment policies of this Plan, existing major office will be permitted.

### 17.1.9 Industrial

17.1.9.1 The Industrial designation will not be permitted in the following Character Areas:

- Churchill Meadows;
- Clarkson; and
- Lakeview.

## 17.2 Churchill Meadows



Map 17-2: Churchill Meadows Employment Area

### 17.2.1 Land Use

17.2.1.1 Notwithstanding the Business Employment policies of this Plan, the following uses will not be permitted:

- a. adult entertainment establishments;
- b. animal boarding establishments which may include outdoor facilities;
- c. body rub establishments;
- d. motor vehicle body repair facilities;
- e. transportation facilities;

f. trucking terminals; and

g. **waste processing stations** or **waste transfer stations** and composting facilities.

### 17.2.2 Urban Design

17.2.2.1 Eglinton Avenue West represents an entry point into the city and, therefore, will promote built form of high architectural and landscaping standards.

17.2.2.2 Strong built form should be achieved along the street frontage with massing emphasis at corners, thus discouraging parking and loading areas at the streetlines.

17.2.2.3 Landscape treatments at corners and extending along Eglinton Avenue West should generally be complementary on either side of the street.

## 17.2.3 Transportation

17.2.3.1 Notwithstanding the policies of this Plan, the following policies will apply:

- a. where feasible, the alignment of individual roads will recognize the need to preserve natural features, including woodland edges and hedgerows. Detailed road design will include mitigating and rehabilitation measures to address any disturbance of these features;

Mississauga will be pursuing other possible opportunities for improving Provincial Highway access to Churchill Meadows. A further detailed study will be required, involving the approval of appropriate agencies, to determine the feasibility and timing of these improvements; and

direct access will be permitted on Major Collectors, with 30 m rights-of-way. The access location will require the approval of the Transportation and Works Department.

## 17.2.4 Physical Services and Utilities

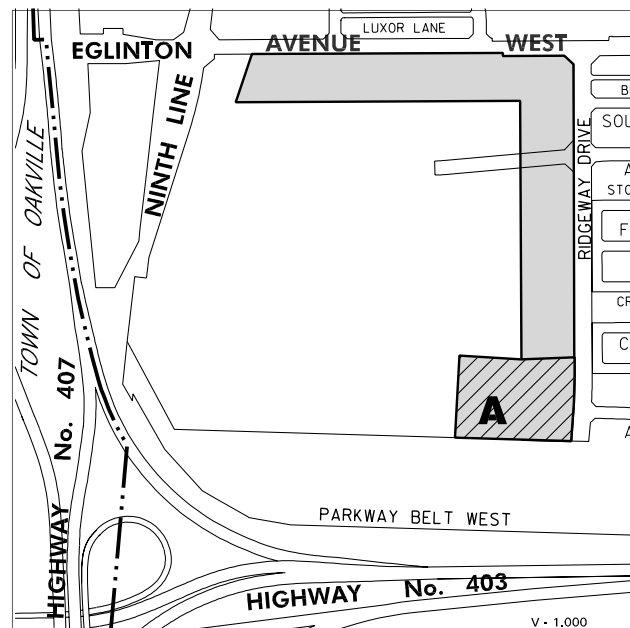
17.2.4.1 All development will be in accordance with the Sawmill Creek Sub-watershed Plan, and the Lisgar Region Water Quality Study.

17.2.4.2 Additional off-site lands associated with the construction of the stormwater management facilities recommended by the Sawmill Creek Sub-watershed Study may be needed.

## 17.2.5 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 17.2.5.1 Site 1



17.2.5.1.1 The lands identified as Special Site 1 are located south of Eglinton Avenue West and west of Ridgeway Drive.

17.2.5.1.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

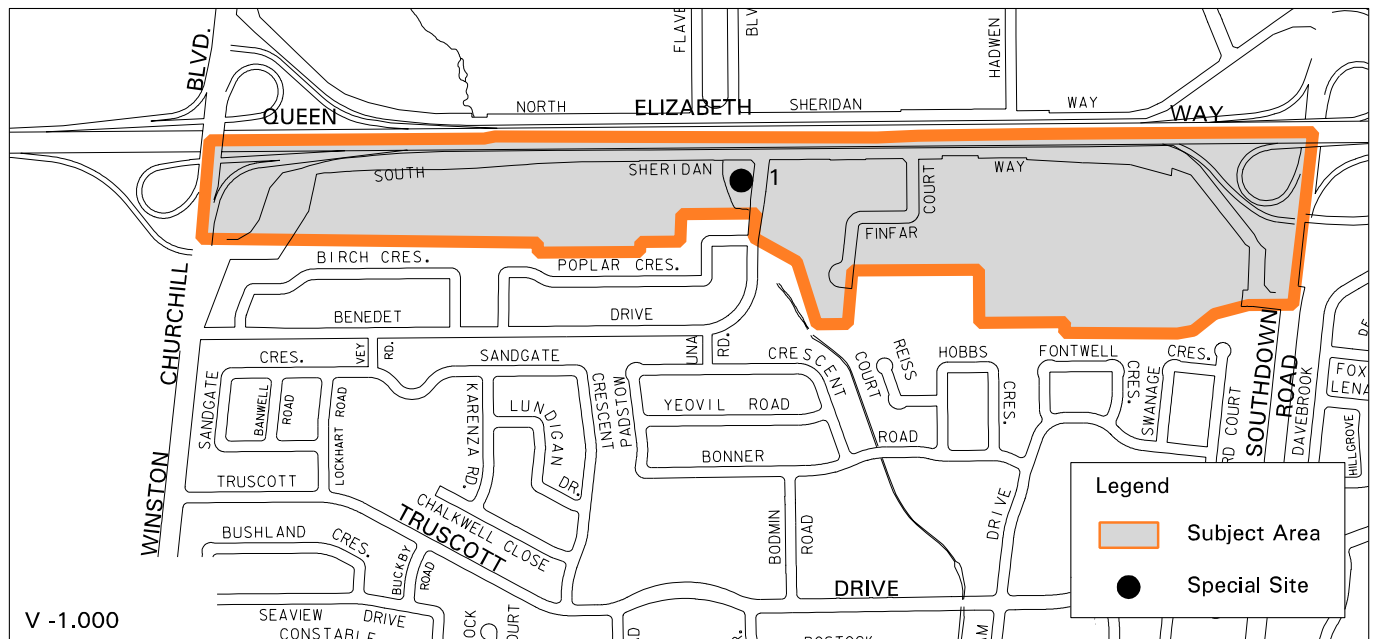
- a. the following additional uses will be permitted:
  - broadcasting station;
  - communications and broadcasting establishment;
  - radio and television transmission facility;
  - telephone sales and service establishment; and
- b. retail warehouses will not be permitted.

17.2.5.1.3 Area A is located north of the utility corridor and west of Ridgeway Drive. Notwithstanding the Mixed Use designations, the following additional policy will apply:

- a. the physical integration of the Bus Rapid Transit (BRT) facility with adjacent development will be encouraged.



## 17.3 Clarkson



Map 17-3: Clarkson Employment Area

### 17.3.1 Land Use

17.3.1.1 Notwithstanding the Business Employment Policies of this Plan, only the following uses will be permitted:

- a. manufacturing;
- b. **secondary office**;
- c. warehousing, distributing and wholesaling; and
- d. accessory uses.

17.3.1.2 The maximum **floor space index (FSI)** of a **secondary office** is 0.5.

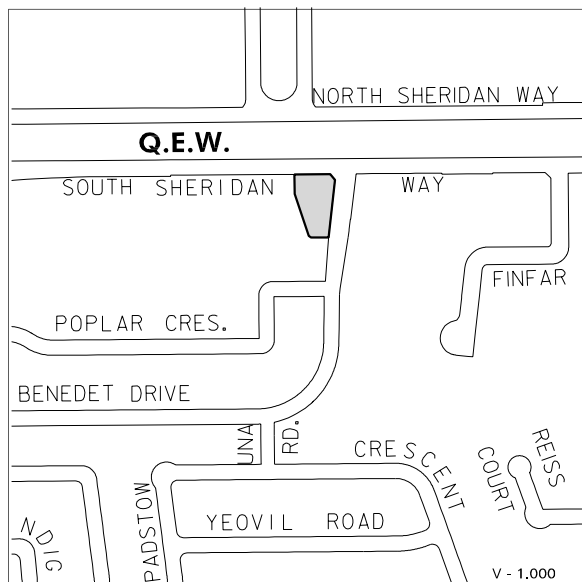
### 17.3.2 Transportation

17.3.2.1 No approvals for development on the south side of South Sheridan Way, west of Southdown Road, will be given until the proponent has submitted an acceptable traffic impact study to the satisfaction of the City.

### 17.3.3 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 17.3.3.1 Site 1



17.3.3.1.1 The lands identified as Special Site 1 are located at the southwest corner of South Sheridan Way and Benedet Drive.

17.3.3.1.2 Notwithstanding the provisions of the Motor Vehicle Commercial designation, only a gas bar will be permitted.

## 17.4 Dixie



Map 17-4: Dixie Employment Area

### 17.4.1 Urban Design Policies

#### Community Identity and Design

17.4.1.1 An appropriate gateway treatment should be created at the city boundaries at Dundas Street East and the Queensway East through the massing of buildings, landscape design, or the design of the **streetscape**. A gateway treatment for Dundas Street East is outlined in the Special Site policies. The Queensway East gateway should use adjacent greenbelt lands and the median on the Queensway East for feature landscaping to frame the entrance to the city.

17.4.1.2 The **streetscape** along Dundas Street East should be enhanced by the punctuation of built form with architectural interest at designated sites. These locations include the gateways at the city boundary and the intersections of Dundas Street with Dixie Road, Haines Road and Stanfield Road. These sites should be developed with a unique and/or significant built form along the street to act as orienting landmarks. The built form and site layout should punctuate the **streetscape** with architectural interest, which can be achieved by maximizing building heights, encouraging creative building massing solutions to break up the dominant

rhythm of one storey structures, reducing front yard parking, emphasis on enhanced landscape treatment, and the inclusion of signage as an integral part of the overall architecture of the building(s).

17.4.1.3 Planting should be provided appropriate to the circumstances and existing vegetation will be preserved in order to maintain the character of the Character Area. Where continuous street planting along Dundas Street East is not possible, concentrated landscape planting may be required.

17.4.1.4 Development along Dixie Road between Dundas Street East and the GO Transit Station should attempt to encourage active pedestrian use, by minimizing building setbacks and the amount of parking between the building and the street. Development should be designed with a significant portion of the building mass located along the linear street frontage. The space between the front wall of the building and the streetline should be treated as a continuous landscape area with no parking or driveway areas. A pedestrian connection should be provided from the public sidewalk to the principal building entrance.

## 17.4.2 Land Use

17.4.2.1 Notwithstanding the Mixed Use policies of this Plan, the following additional uses will be permitted within enclosed buildings:

- a. manufacturing;
- b. research and development; and
- c. warehousing, distributing and wholesaling.

17.4.2.2 Notwithstanding the Mixed Use policies of this Plan, outdoor flea markets will not be permitted.

17.4.2.3 Notwithstanding the Business Employment policies of this Plan, existing industrial uses that require extensive outdoor processing and storage will be permitted, and may expand, except where adjacent to residentially designated lands.

## 17.4.3 Transportation

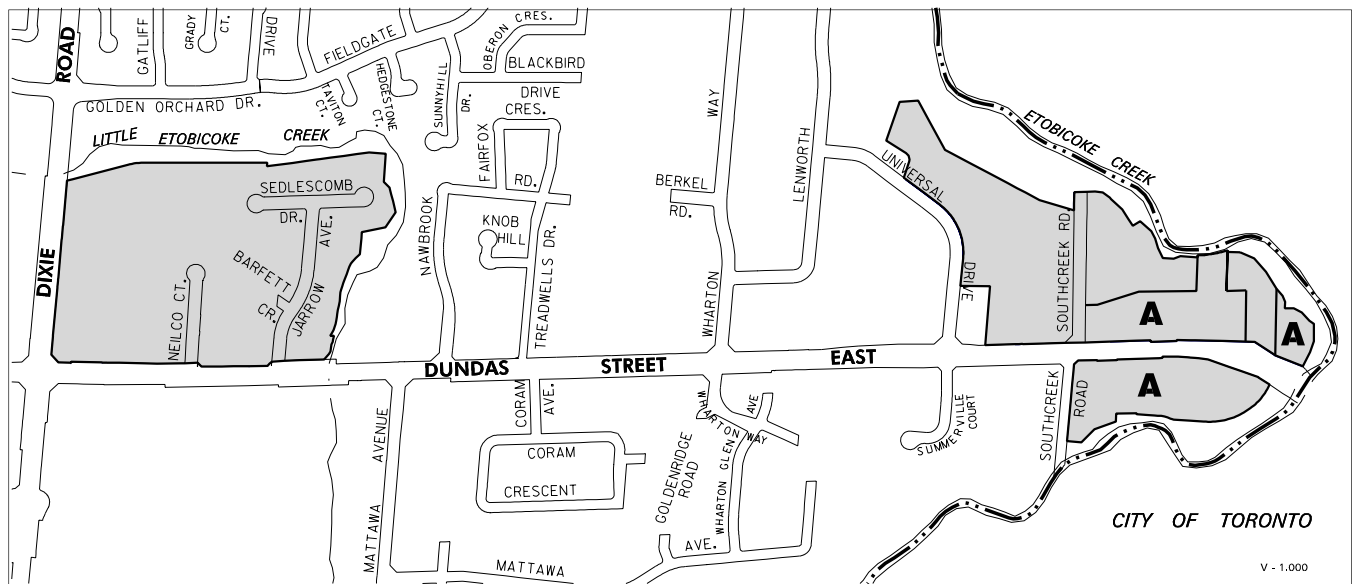
17.4.3.1 Access control will be emphasized along arterial roadways such as Dundas Street East, where High Occupancy Vehicle (HOV) lanes are currently in operation between Dixie Road and the Etobicoke Creek.

17.4.3.2 Mississauga will consider the extension of Mattawa Avenue westward to Dixie Road, and a connection of Jarrow Avenue southward to the Mattawa Avenue extension.

## 17.4.4 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 17.4.4.1 Site 1



17.4.4.1.1 The following lands are identified as Special Site 1:

- the lands on the northeast corner of Dixie Road and Dundas Street East, bounded by the Little Etobicoke Creek. This site is situated within a spill zone associated with the Regional Storm flood overtopping the south bank of the Little Etobicoke Creek channel; and
- the lands west of Etobicoke Creek at Dundas Street East. This site is entirely within the Regional Storm floodplain of the Etobicoke Creek. These lands are shown conceptually and are subject to further review by the Toronto and Region Conservation Authority (TRCA).

Site 1 is a special policy area defined by Provincial Government criteria and as established by the Toronto and Region Conservation Authority (TRCA).

Notwithstanding the policies of this Plan which relate to development within the Regional Storm floodplain, and the provisions of the Mixed Use, Business Employment and Greenbelt designations, the following policies will apply:

- the construction of any buildings or structures, including any additions to an existing building, and the placing or dumping of fill of any kind, or the alteration of any **watercourse**, will not be permitted within the special policy area without the approval of the appropriate Conservation Authority;
- development will not be permitted within the 100 year return storm floodplain;
- development outside the 100 year return storm floodplain will be permitted only on the basis of approvals by the City and the appropriate Conservation Authority;
- ideally, new buildings and structures will be protected from flooding under Regional Storm conditions. Where this level of protection is technically or economically not feasible, Mississauga may, in consultation with the appropriate Conservation Authority, consider a lesser level of protection. In no case, however, will the minimum level of protection be less than the 350-year return storm floodplain;
- notwithstanding subsections c. and d. of this section, development, or additions will not be



permitted, where, as a result of a flood event equal to or less than the Regulatory Flood, structures will be subject to flows which, due to velocity and/or depth, would present conditions of hazard to life or potential damage to structures;

f. in support of any development application, the City or the appropriate Conservation Authority may require engineering studies related to flood frequency, velocity and depth of flood flows, appropriate flood damage reduction measures, and storm water management techniques. Based on these studies and other information, Mississauga or the TRCA may specify that any approval of the application will be subject to certain design considerations related to location, structural integrity, setbacks, coverage, elevation of ground floor openings, and other matters as may be determined by the City or the appropriate Conservation Authority;

- ingress and egress should be "safe" pursuant to Provincial Government flood proofing standards, and/or achieve the maximum level of flood protection determined to be feasible and practical based on existing infrastructure; and

- new development not be permitted to locate in the floodplain where the use is:

- associated with the manufacture, storage, disposal and/or consumption of hazardous substances or the treatment, collection and disposal of sewage, which would pose an unacceptable threat to public safety if they were to escape their normal containment/use as a result of flooding or failure of flood proofing measures; and

- associated with uses, such as hospitals, nursing homes and schools, which would pose a significant threat to the safety of the

inhabitants (e.g. the sick, the elderly, the disabled or the young), if involved in an emergency evacuation situation as a result of flooding or failure of flood proofing measures;

- associated with services such as those provided by fire, police and ambulance stations and electrical substations, which would be impaired during a flood emergency as a result of flooding or failure of flood proofing measures; and

- where new development is not considered to pose an unacceptable risk to public safety, a higher level of flood protection and/or additional flood proofing precautions above the regulatory flood level, may still be required due to the sensitive nature of the development; and

g. any amendment to the Zoning By-law will include provisions required by the City or the appropriate Conservation Authority to implement the Special Site Policies of this Plan.

17.4.4.1.2 The lands identified as Area A are located north and south of Dundas Street East, from Southcreek Road to the municipal boundary, Etobicoke Creek.

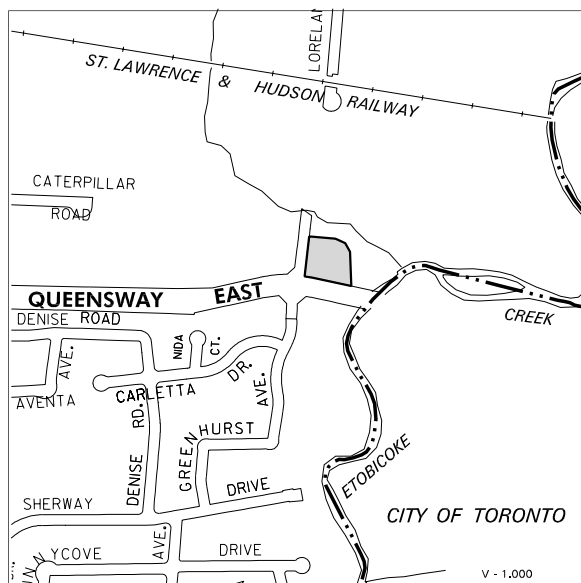
These lands function as the primary gateway into Mississauga from Toronto and areas to the east, and should promote distinctive built form, landscaping and street furniture elements as visual landmarks to identify the City entry and reinforce a quality image.

Notwithstanding the provisions of the Mixed Use designation of the lands, the following additional policies will apply:

a. motor vehicle sales and rentals and other commercial or industrial uses with outdoor storage, flea markets, and motor vehicle commercial uses will not be permitted;

- b. consolidation of vehicular entrances should be encouraged;
- c. active building frontages should be oriented to the public street, maximizing the linear street frontage occupied by the building;
- d. building definition is to be strengthened on the street façade by use of prominent roof forms, upper level window bands, architectural projections, and other design elements to accentuate the height and prominence of single storey buildings; and
- e. special formal street tree planting at regular intervals is encouraged along the frontage of the gateway properties.

#### 17.4.4.2 Site 2



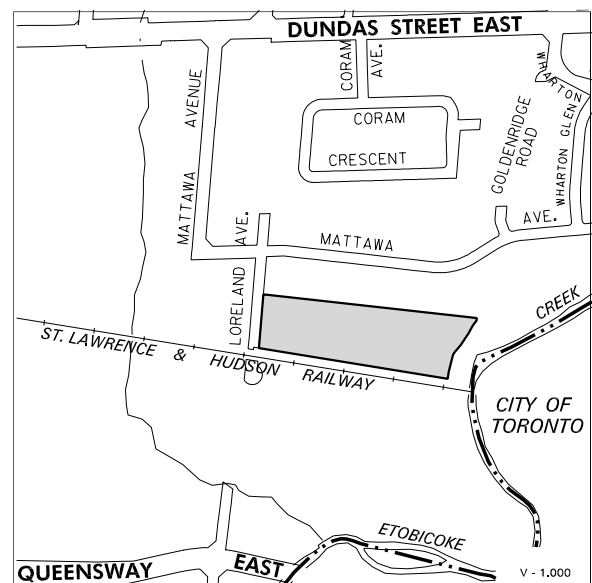
17.4.4.2.1 The lands identified as Special Site 2 are located on the north side of Queensway East, west of Etobicoke Creek.

17.4.4.2.2 These lands function as a gateway into Mississauga from Toronto and should promote distinctive built form and landscaping elements as visual landmarks to identify the City entry and reinforce a quality image.

17.4.4.2.3 Notwithstanding the Business Employment designation of the lands, the following additional policies will apply:

- a. 50% of the building's gross floor area is permitted to be used for retail commercial uses in addition to the uses permitted in the Business Employment designation; and
- b. built form should minimize setbacks from Queensway East and include enhanced landscape features to promote the building on site as a highly visible landmark structure.

#### 17.4.4.3 Site 3

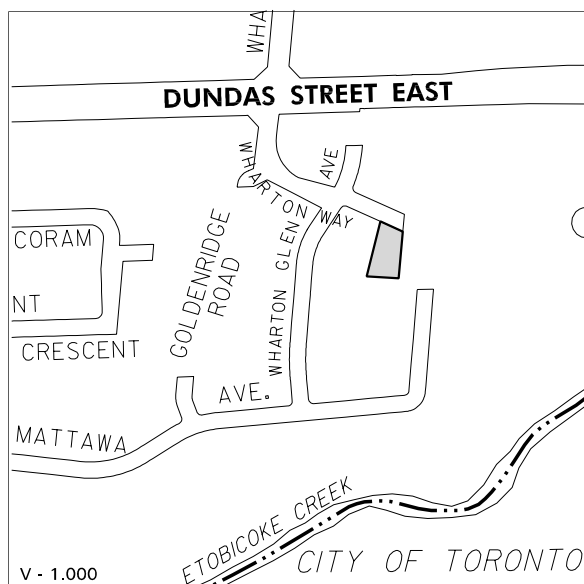


17.4.4.3.1 The lands identified as Special Site 3 are located on the east side of Loreland Avenue, south of Dundas Street East, at 2315 Loreland Avenue.

17.4.4.3.2 Notwithstanding the provisions of the Business Employment designation on the lands a **major power generating facility** will be permitted.

## 17.4.5 Exempt Site Policies

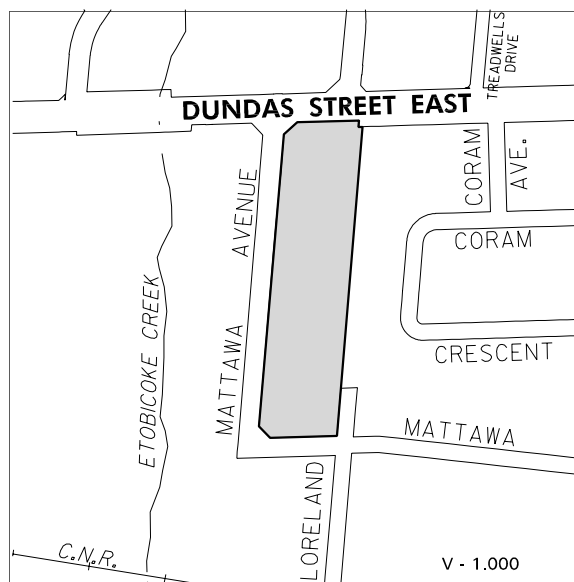
### 17.4.5.1 Site 1



17.4.5.1.1 The lands identified as Exempt Site 1 are located on the south side of Wharton Way east of Wharton Glen Avenue, at 2001 Wharton Way.

17.4.5.1.2 Notwithstanding the provisions of the Business Employment designation, a food store will also be permitted with a maximum gross floor area of 320 m<sup>2</sup>.

### 17.4.5.2 Site 2



17.4.5.2.1 The lands identified as Exempt Site 2 are located at the southeast corner of Dundas Street East and Mattawa Avenue, at 1650 Dundas Street East.

17.4.5.2.2 Notwithstanding the provisions of the Mixed Use designation, residential uses will also be permitted on the second floor of the existing building, provided that they take the form of live/work units and are limited to a maximum of 42 units and 6 140 m<sup>2</sup>.

## 17.5 Gateway

17.5.1.3 Notwithstanding the Industrial policies of this Plan, the following use will also be permitted subject to a rezoning:

- a. crematorium.

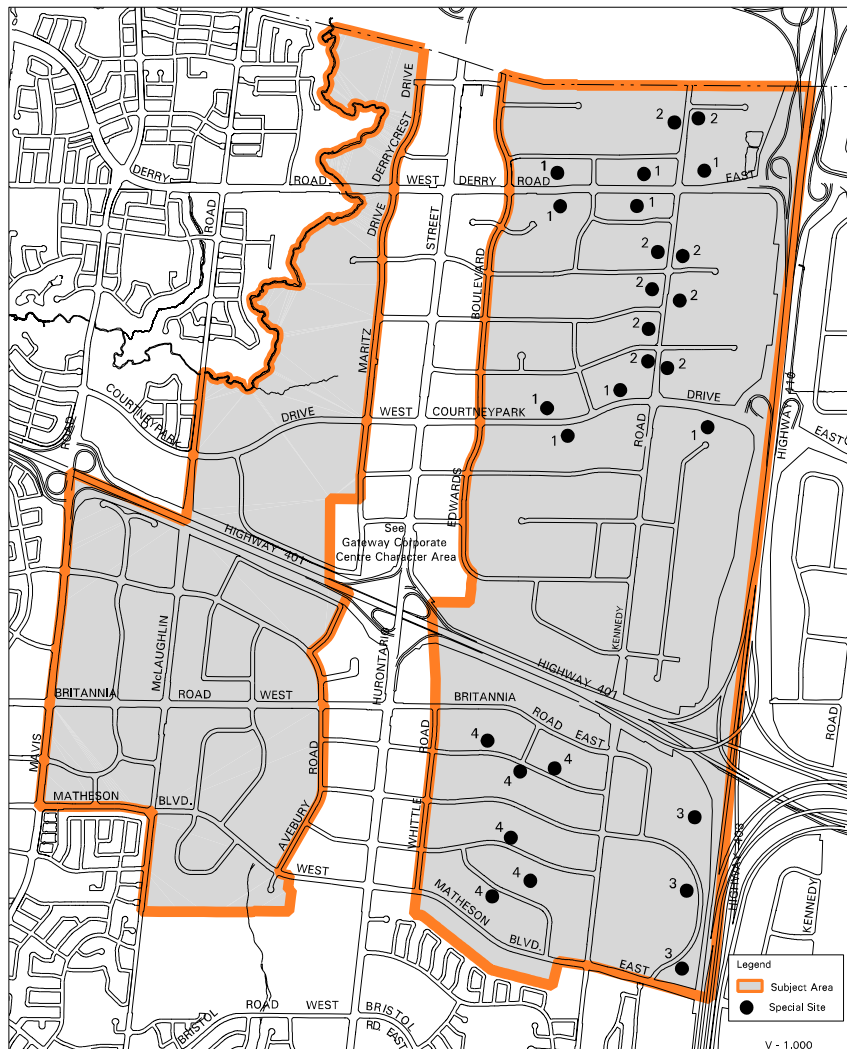
### 17.5.2 Transportation

17.5.2.1 Access to Mavis Road in general, will be limited to signalized intersections. Restricted

intersections. Restricted right-in/right-out accesses along this arterial road may be permitted. These arterial access/intersection points will be assessed in more detail at the development review stage. The Transportation and Works Department may require development concept plans.

17.5.2.2 The following road connections may be constructed without further amendment to this Official Plan subject to appropriate studies and Provincial Government approval:

- the extension of Madill Boulevard to the Provincial Highway 401 westbound off-ramp at Hurontario Street;
- the extension of Belgrave Road to the Provincial Highway 401 eastbound off-ramp at Mavis Road;



Map 17-5: Gateway Employment Area

### 17.5.1 Land Use

17.5.1.1 Notwithstanding the Business Employment policies of this Plan, the following uses will not be permitted:

- a. motor vehicle body repair facilities.

17.5.1.2 Lands designated Motor Vehicle Commercial may be developed for the permitted uses of the abutting Business Employment designations without further amendment to this Plan.

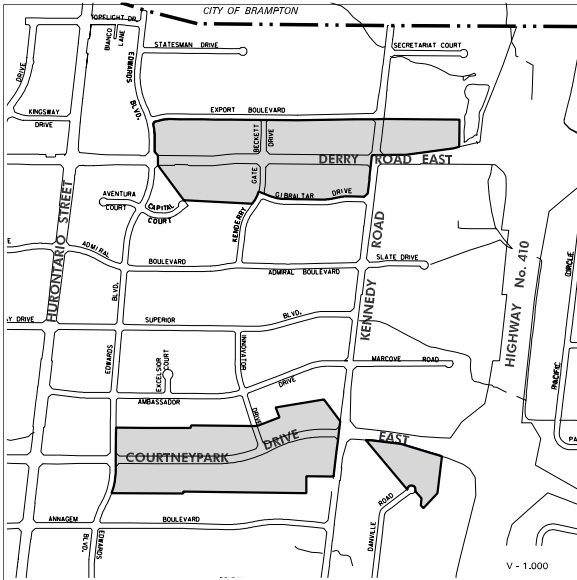
- the extension of Whittle Road to the Provincial Highway 401 eastbound off-ramp at Hurontario Street; and
- the extension of Edwards Boulevard to the Provincial Highway 407 eastbound off-ramp at Hurontario Street (subject to approval by the City of Brampton).

This statement is intended to protect these potential ramp connections for future consideration and does not represent a commitment on the part of the Provincial Government.

### 17.5.3 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 17.5.3.1 Site 1



17.5.3.1.1 The lands identified as Special Site 1, are located on both sides of Derry Road East and Courtney Park Drive East between Provincial Highway 410 and Edwards Boulevard.

17.5.3.1.2 Notwithstanding the Business Employment designation on these lands, the following additional policies will apply:

- a. industrial uses which may have some outside storage, such as transportation related activities, and motor vehicle sales (including trucks and truck trailers), leasing, repairing and servicing, will be permitted; and
- b. any site development plans will address the following built form expectations:
  - that the principal built form model is one of enclosure (i.e. as viewed from Derry Road East and Courtney Park Drive East) of the major portion of materials/trucks/truck trailers storage, service and docking uses by

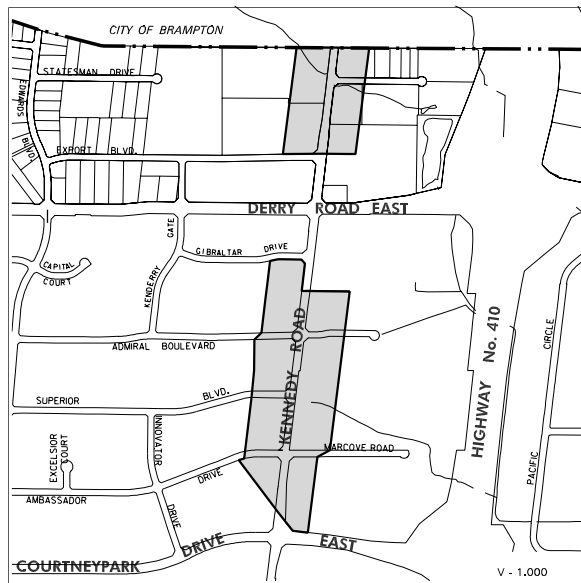
buildings and/or wing walls (e.g. in a quadrangle, "U" shaped or "T" shaped footprint);

- that the street frontage, in linear terms, be defined by built form (i.e. buildings or projecting walls compatible with the architecture) to the greatest extent possible and maintaining a general consistent, and limited setback to relate the buildings both to each other and to the street edge (minimum guideline of at least two thirds of the linear street frontage is to be occupied by building walls with a setback range of four-and-a-half to thirteen (4.5-13) m);
- that street front building walls should be highly articulated by use of creative massing, entry features, fenestration, high quality materials and interesting roof form (unrelieved walls with no concession to the visual character of the street realm should be avoided); and
- that high profile locations such as intersections or gateway locations be recognized by the siting, design and height of respective buildings.



### 17.5.3.2

### Site 2



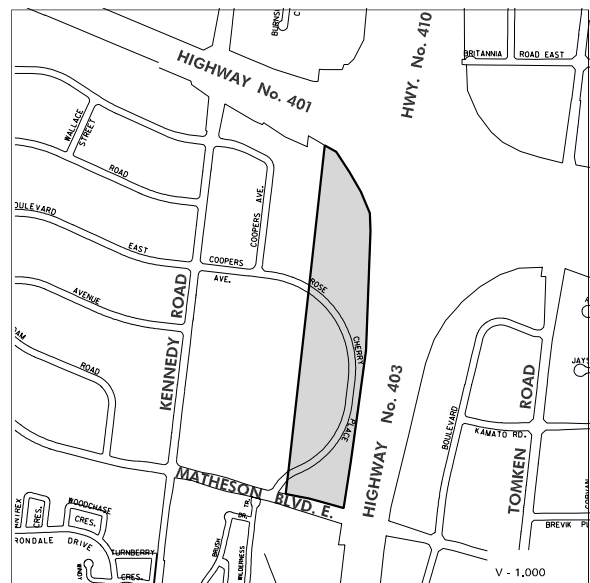
17.5.3.2.1 The lands identified as Special Site 2, are located on both sides of Kennedy Road, north and south of Derry Road East.

17.5.3.2.2 Notwithstanding the Business Employment designation on these lands, the following additional policy will apply:

- a. industrial uses which may have some outside storage, such as transportation related activities, and motor vehicle sales (including trucks and truck trailers), leasing, repairing and servicing, will be permitted.

### 17.5.3.3

### Site 3

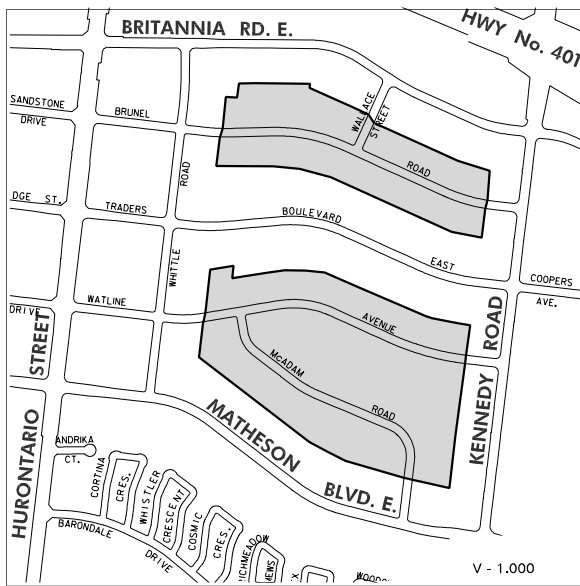


17.5.3.3.1 The lands identified as Special Site 3, known as the Hershey Centre, are located west of Provincial Highway 403, between Provincial Highway 401 and Matheson Boulevard East.

17.5.3.3.2 Notwithstanding the provisions of the Public Open Space designation, the following additional policy will apply:

- a. land uses permitted within Special Site 3 include recreational and spectator facilities such as, spectator arenas, outdoor stadiums, community arenas, and indoor sport complex, as well as complementary uses such as, but not limited to, retail commercial uses, restaurants, sports medical clinic, health and fitness centre, accessory offices, and overnight accommodations.

#### 17.5.3.4 Site 4

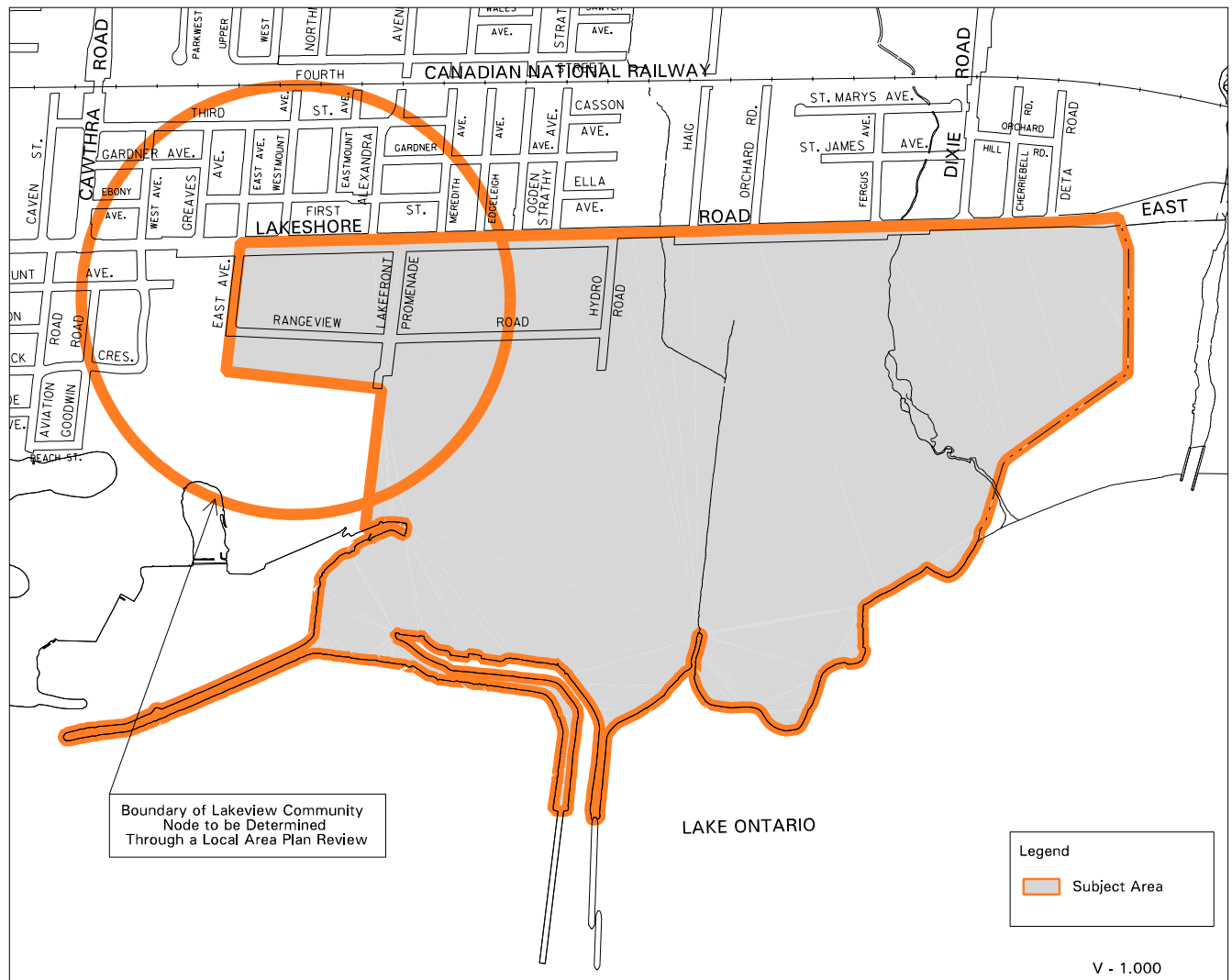


17.5.3.4.1 The lands identified as Special Site 4 are located on the north and south sides of Brunel Road, west of Kennedy Road and on the north and south sides of Watline Avenue and McAdam Road, west of Kennedy Road.

17.5.3.4.2 Notwithstanding the provisions of the Business Employment designation, all uses in existence at the time this policy comes into effect (2007 September 10), and outdoor storage will be permitted.

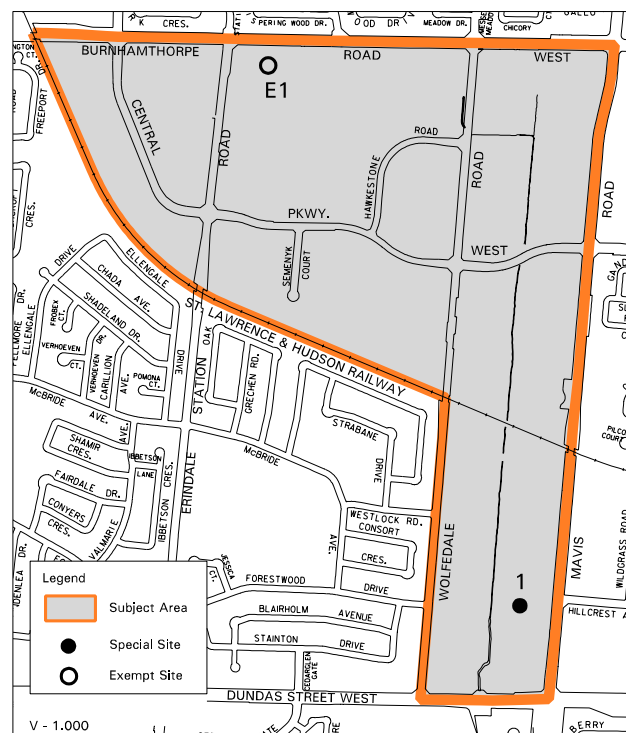
## 17.6 Lakeview

Official Plan policies for lands within the Lakeview Employment Area are contained in the Lakeview Local Area Plan.



Map 17-6: Lakeview Employment Area

## 17.7 Mavis-Erindale



Map 17-7: Mavis-Erindale Employment Area

### 17.7.2 Land Use

17.7.2.1 Notwithstanding the Business Employment policies of this Plan, existing manufacturing and warehousing, distributing, and wholesaling uses that require extensive outdoor processing and storage will be permitted, and may expand subject to the development objectives of this Plan and current site plan control requirements.

### 17.7.3 Special Site Policies

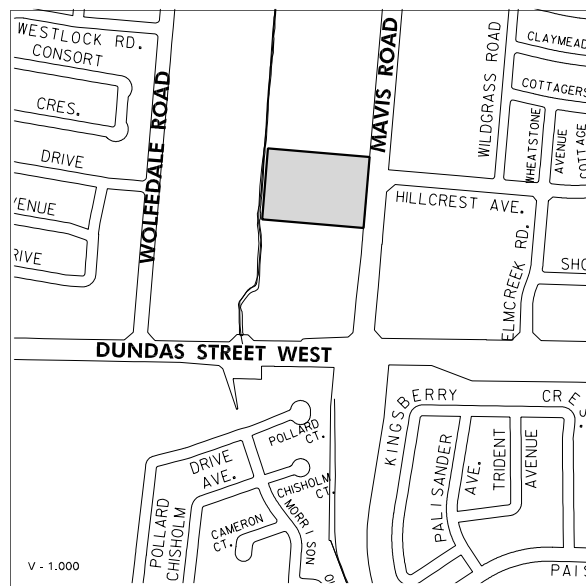
There are sites within the Character Area that merit special attention and are subject to the following policies.

### 17.7.1 Urban Design Policies

17.7.1.1 To achieve a high standard of building design, landscape and **streetscape**, the following design guidelines will be used to evaluate the design aspects of development proposals:

- redesign of the lands on the west side of Mavis Road across from the lands designated for residential purposes on the east side of Mavis Road, will require upgraded landscape and **streetscape** treatment; and
- redesign of the lands on the east side of Wolfedale Road will require upgraded landscape and **streetscape** treatment in order to address the residential nature of the lands on the west side of Wolfedale Road, south of the St. Lawrence and Hudson Railway tracks.

### 17.7.3.1 Site 1



17.7.3.1.1 The lands identified as Special Site 1 are located on the west side of Mavis Road, north of Dundas Street West.

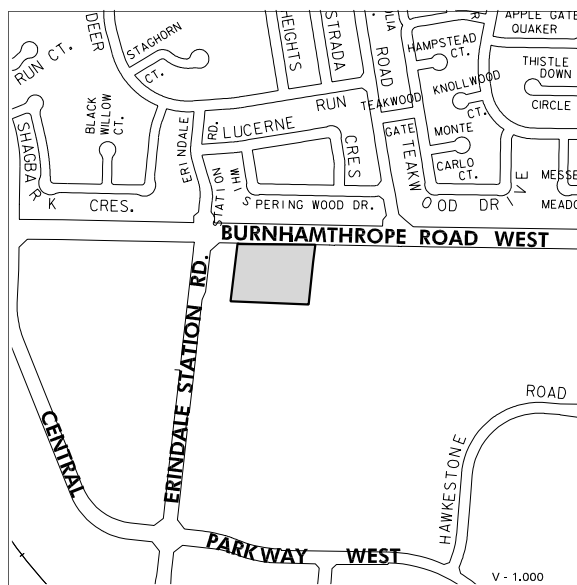
17.7.3.1.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

a. the following uses will not be permitted:

- a department store;
- a food supermarket;
- a drug store; and
- an automotive parts and accessories store.

### 17.7.4 Exempt Site Policies

#### 17.7.4.1 Site 1

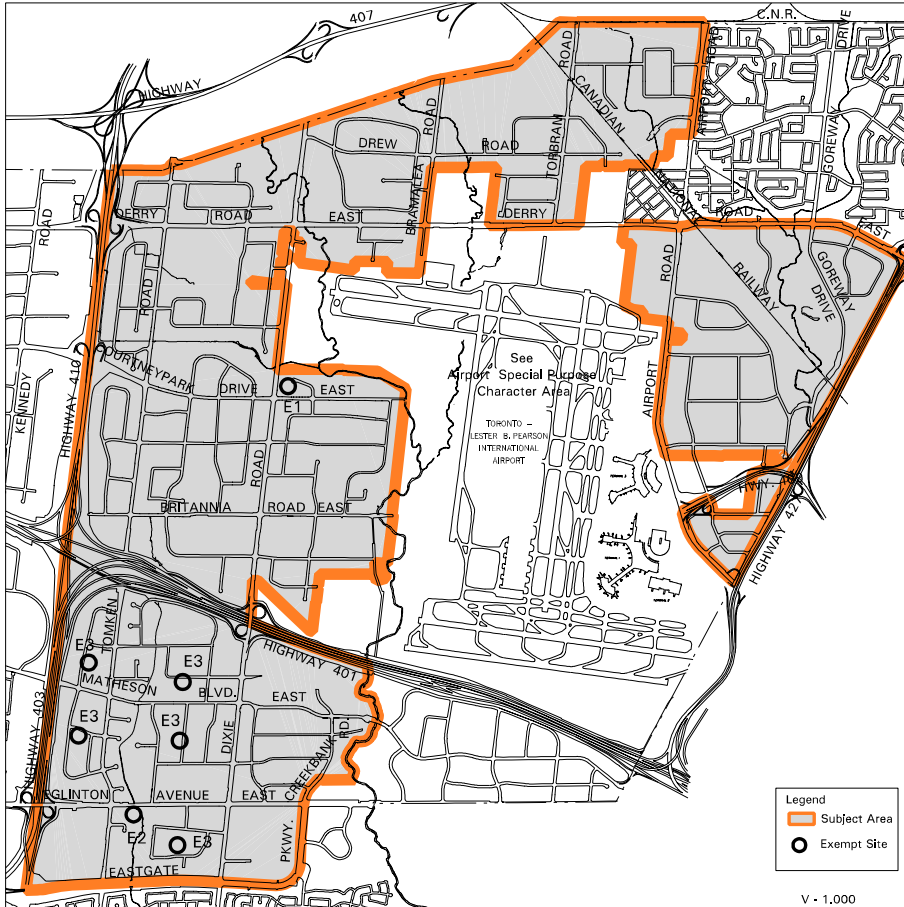


17.7.4.1.1 The lands identified as Exempt Site 1 are located on the south side of Burnhamthorpe Road West, east of Erindale Station Road.

17.7.4.1.2 Notwithstanding the provisions of the Mixed Use designation, motor vehicle body repair uses will also be permitted.



## 17.8 Northeast



Map 17-8: Northeast Employment Area

### 17.8.1 Land Use

17.8.1.1 Notwithstanding the Mixed Use and Business Employment policies of this Plan, existing industrial operations which have extensive outdoor processing or storage areas will be permitted to continue and expand in accordance with the policies of this Plan.

17.8.1.2 Notwithstanding the provisions of the Business Employment designation, for the lands fronting on Airport Road the following uses will not be permitted:

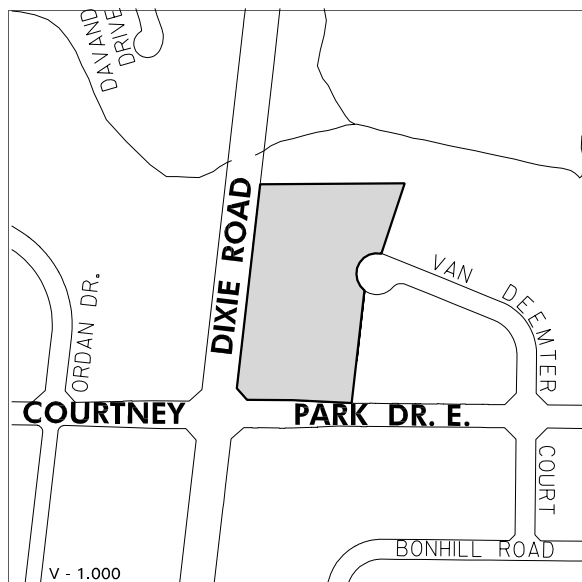
- broadcasting, communication, and utility rights-of-way;
- trucking terminals; and
- waste processing stations** or **waste transfer stations** and composting facilities.

17.8.1.3 Notwithstanding the Industrial policies of this Plan, the following use will also be permitted on lands west of Airport Road subject to a rezoning:

- crematorium.

## 17.8.2 Exempt Site Policies

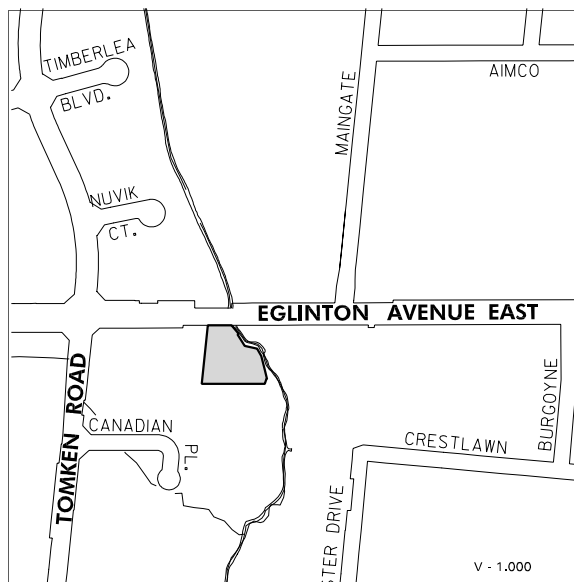
### 17.8.2.1 Site 1



17.8.2.1.1 The lands identified as Exempt Site 1 are located on the east side of Dixie Road north of Courtneypark Drive East.

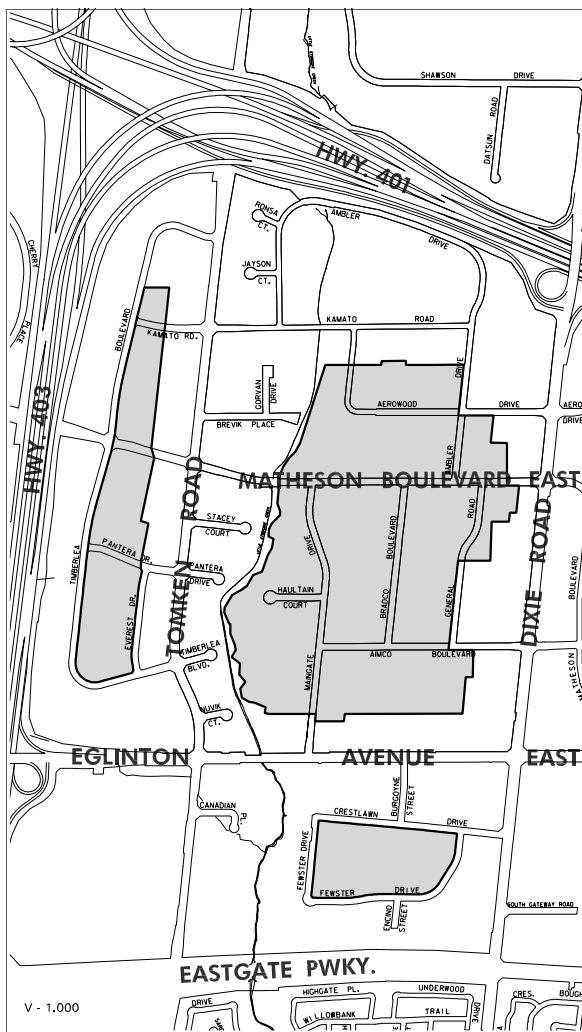
17.8.2.1.2 Notwithstanding the provisions of the Business Employment designation, retail commercial uses will also be permitted. Motor vehicle commercial uses including a truck detailing establishment and automobile sales and leasing accessory to an automobile repair garage will also be permitted. Private schools, places of religious assembly and day care centres will not be permitted.

### 17.8.2.2 Site 2



17.8.2.2.1 The lands identified as Exempt Site 2 are located on the south side of Eglinton Avenue East, east of Tomken Road.

17.8.2.2.2 Notwithstanding the provisions of the Business Employment and Greenbelt designations, housing for the elderly will also be permitted.



17.8.2.3.1 The lands identified as Exempt Site 3 are located north of Eastgate Parkway, west of Dixie Road, south of Highway 401, and east of Highway 403.

17.8.2.3.2 Notwithstanding the provisions of the Business Employment designation, all uses in existence as of September 10, 2007 and outdoor storage will also be permitted.

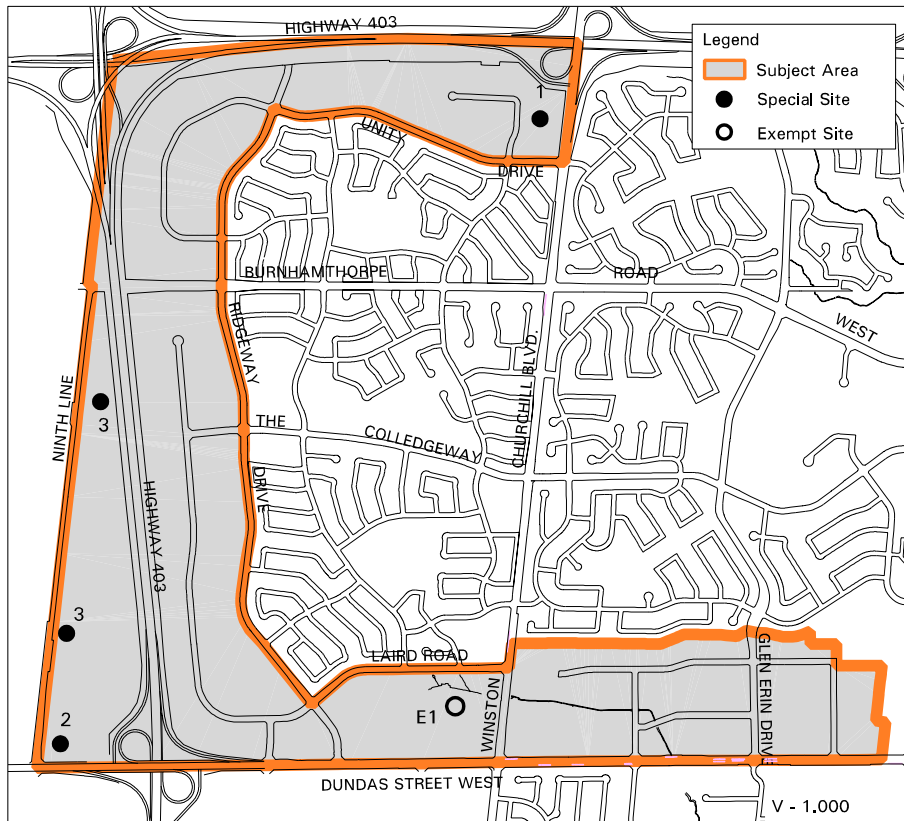
## 17.9 Southdown

Official Plan Policies for lands within the Southdown Employment Area are contained in the Southdown Local Area Plan.



Map 17-9: Southdown Employment Area

## 17.10 Western Business Park



Map 17-10: Western Business Park Employment Area

### 17.10.1 Urban Design Policies

17.10.1.1 For lands with direct exposure to Provincial Highway 403, the following design policies will be used to evaluate the design aspects of development proposals:

- limited parking will be permitted between the building and Provincial Highway 403 property line; and
- upgraded building elevations will be required for lots abutting Provincial Highway 403, Ridgeway Drive and Ninth Line.

17.10.1.2 Loading bays and **waste** collection areas should not face onto Provincial Highway 403, Ridgeway Drive, Winston Churchill Boulevard, Dundas Street West or Ninth Line.

17.10.1.3 Outdoor storage and display areas should not face onto Ridgeway Drive.

### 17.10.2 Land Use

17.10.2.1 Notwithstanding the Business Employment policies of this Plan, the following uses will not be permitted:

- motor vehicle body repair facilities;
- trucking terminals; and
- waste processing stations** or **waste transfer stations** and composting facilities.



### 17.10.3 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 17.10.3.1 Site 1



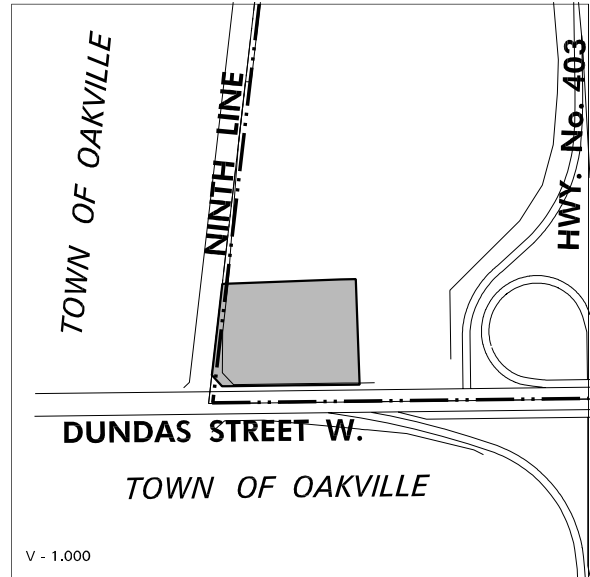
17.10.3.1.1 The lands identified as Special Site 1 are located in the northwest quadrant of the intersection of Winston Churchill Boulevard and Unity Drive.

17.10.3.1.2 Notwithstanding the provisions of the Business Employment designation, the following additional policies will apply:

- a. Mixed Uses, excluding motor vehicle commercial uses, will be permitted; and
- b. the preparation of a concept plan promoting the integrated development of the existing lots will be required in conjunction with any development applications. The concept plan will be required to address the following:
  - street access;
  - impacts on residential lands will be addressed in accordance with the Urban Design policies of this Plan; and

- appropriate gateway treatment through the massing of buildings and landscape design.

#### 17.10.3.2 Site 2

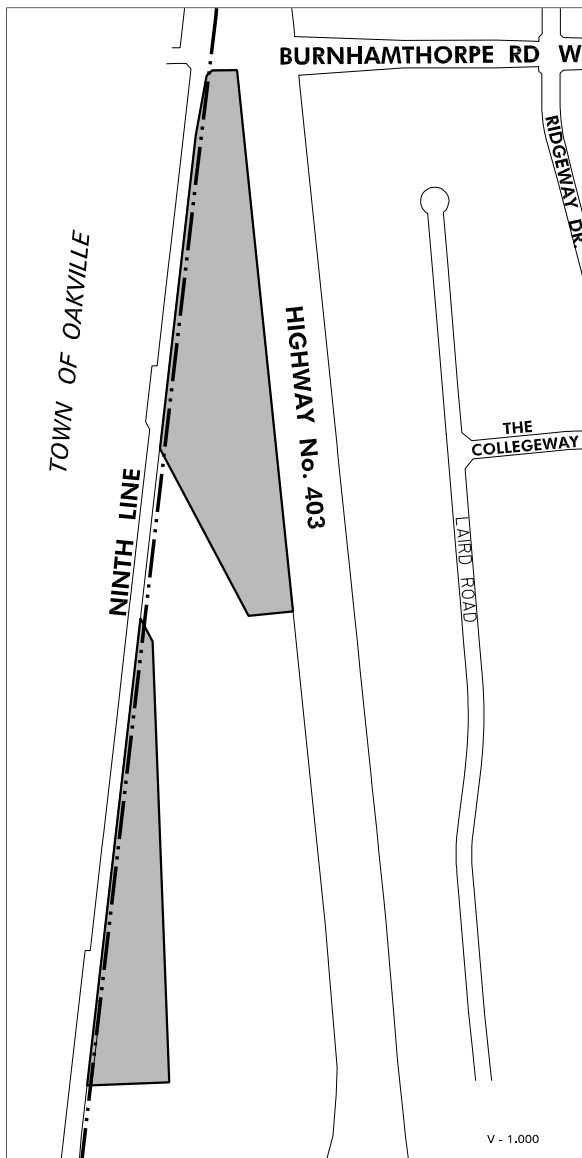


17.10.3.2.1 The lands identified as Special Site 2 are located on the north side of Dundas Street West, east of Ninth Line, and west of Provincial Highway 403.

17.10.3.2.2 Notwithstanding the provisions of the Business Employment designation, Mixed Uses and cemetery use will be permitted.

### 17.10.3.3

#### Site 3



17.10.3.3.1 The lands identified as Special Site 3 are located on the east side of Ninth Line, south of Burnhamthorpe Road West.

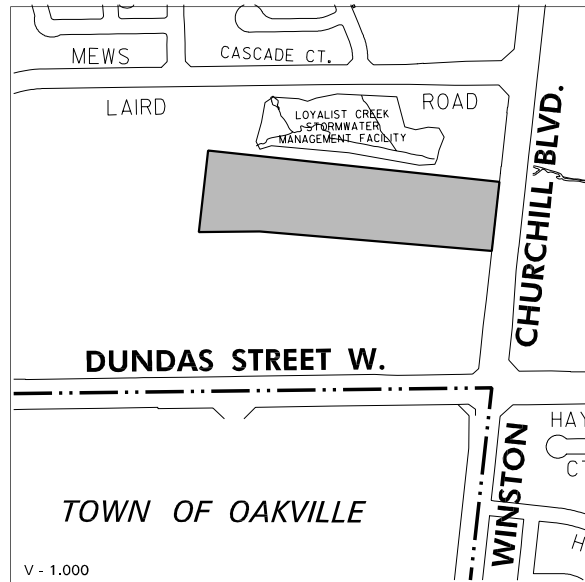
17.10.3.3.2 Notwithstanding the provisions of the Business Employment designation, cemetery use will be permitted.

### 17.10.4

#### Exempt Site Policies

#### 17.10.4.1

##### Site 1



17.10.4.1.1 The lands identified as Exempt Site 1 are located south of Laird Road and west of Winston Churchill Boulevard.

17.10.4.1.2 Notwithstanding the provisions of the Mixed Use designation, housing for the elderly will also be permitted.

# 18 Special Purpose Areas

## 18.1 Introduction

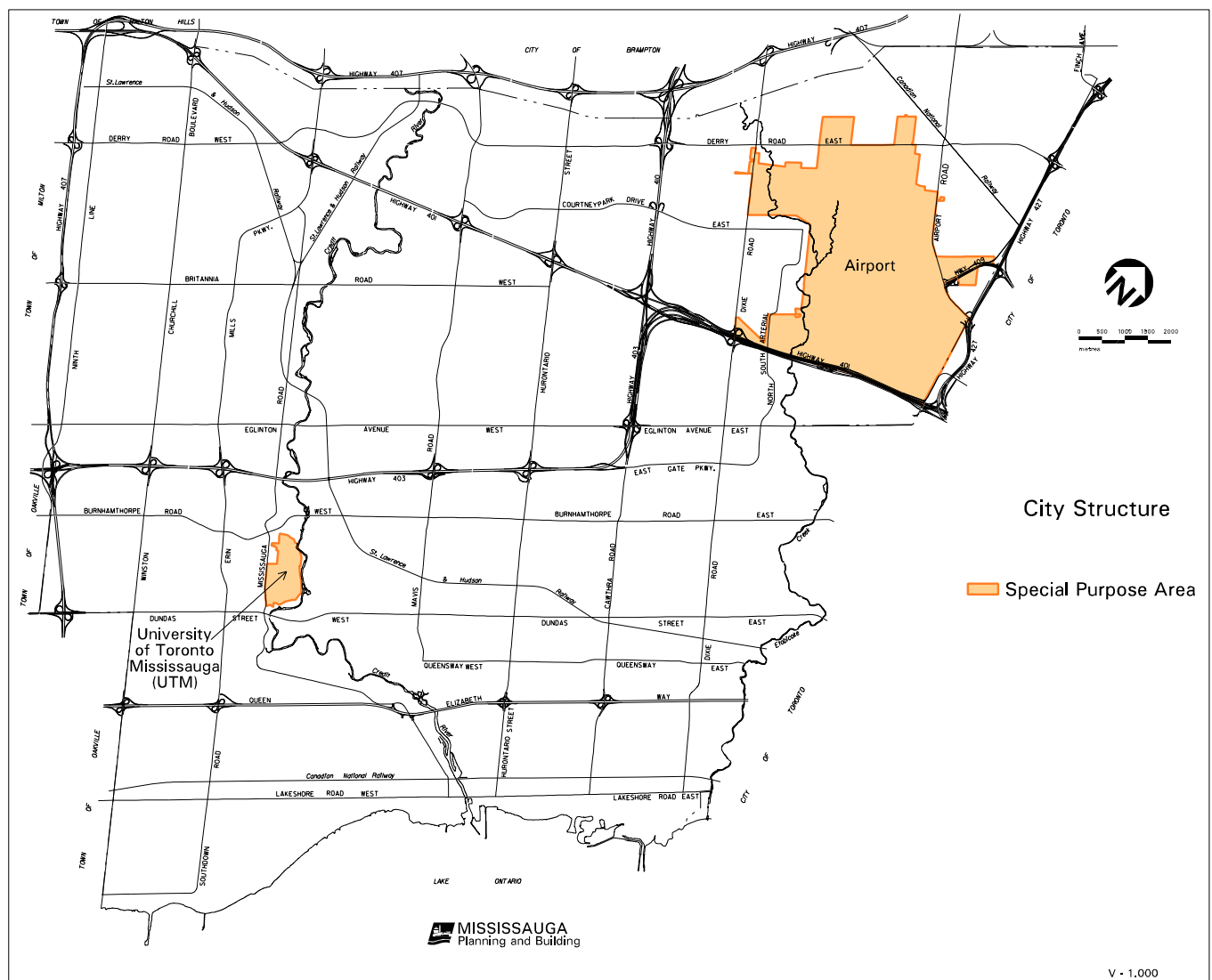
There are two Special Purpose Areas in Mississauga:

- the Airport; and
- University of Toronto Mississauga.

This section identifies the modifications to the General Land Use designations in Chapter 11 that apply to all Special Purpose Areas.

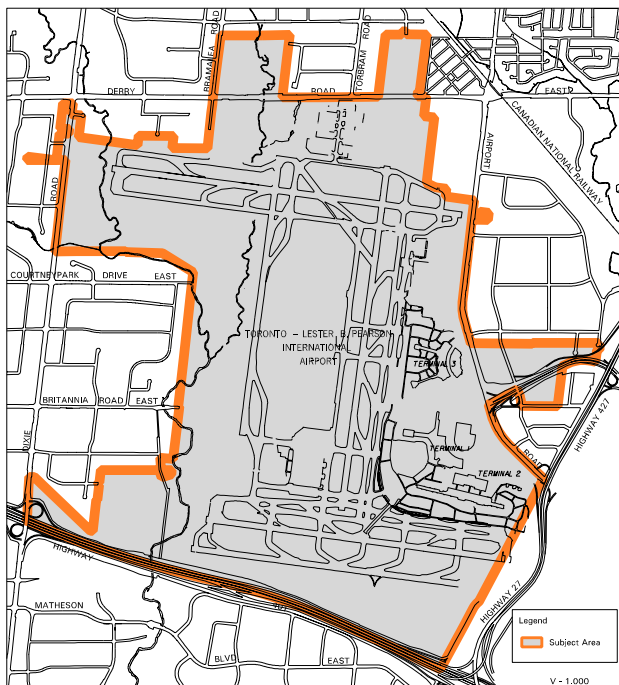
### 18.1.1 General

18.1.1.1 Special Purpose Areas will develop as unique destinations of city and regional significance.



Map 18-1: City Structure – Special Purpose Areas

## 18.2 Toronto-Lester B. Pearson International Airport



Map 18-2: Toronto-Lester B. Pearson International Airport Special Purpose Area

### 18.2.1 Introduction

It is recognized that the impacts of Airport operations, including noise, air navigation facilities and procedures, and airport obstacle limitation surface restrictions, extend far beyond its geographic boundaries. This is a permanent circumstance that this Plan acknowledges.

Land use and development can impact the safe operation of aircraft in an area extending well beyond the limits of the Airport. The Airport Influence Area, shown on Appendix B represents the total area where new construction has the potential to impact on civil navigation for aircraft using the Airport or which has the potential to impact airport or airspace capacity due to interference with signals, communications, and instrument flight procedures.

Further, there are areas of Mississauga which are subject to high levels of aircraft noise. As a result, Chapter 6 contains policies which set out the restrictions on development within the areas subject to high levels of aircraft noise. The policies of this Plan are based on a six runway configuration of the Airport.

### 18.2.2 Land Use

18.2.2.1 Mississauga will cooperate with the Federal Government, or successors, to ensure that new construction within the Airport Influence Area is compatible with the requirements of the Airport for:

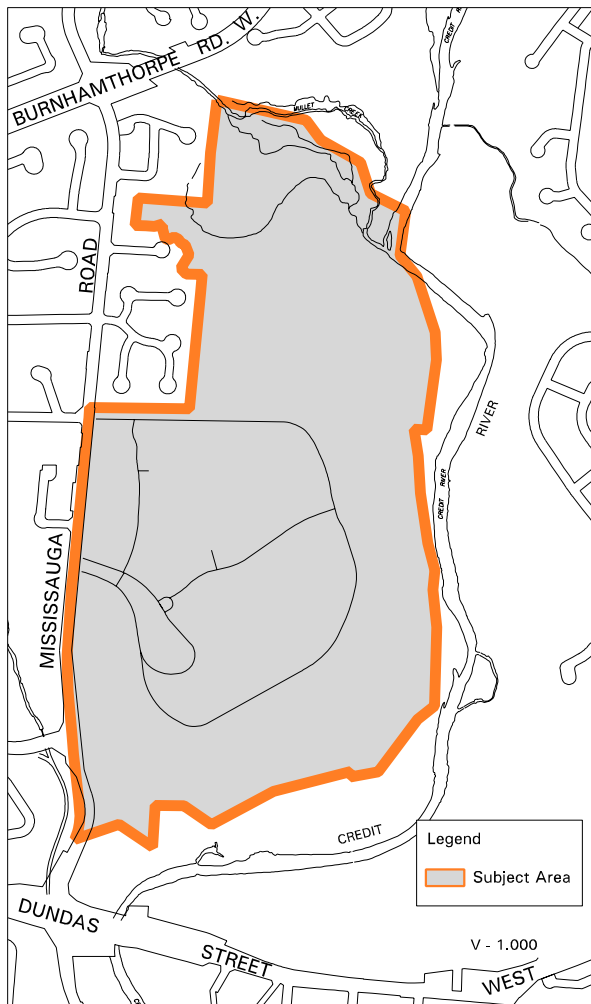
- height limitations;
- protection of navigational aids, surveillance equipment and communications;
- visibility; and
- protection from wildlife hazards.

18.2.2.2 All applications to amend the Official Plan or zoning by-law, draft plans of subdivision, and site development plans within the Airport Influence Area, will be circulated to the GTAA for comments.

18.2.2.3 Because of the economic importance of the Airport to the city, region and province, development should be either complementary or compatible with Airport operations and allow the Airport to function at maximum efficiency to achieve full economic potential having regard for:

- existing and future industry;
- business and employment opportunities; and
- the interests of existing and future residents.

## 18.3 University of Toronto Mississauga



Map 18-3: University of Toronto Mississauga Special Purpose Area

### 18.3.2 Land Use

18.3.2.1 Expansion of the UTM campus by the acquisition of adjacent properties will be permitted without an amendment to this Plan.

18.3.2.2 Development should be located and designed to minimize adverse effects on adjacent residential areas.

18.3.2.3 Development should have regard for the Mississauga Road **Scenic Route** Policies.

18.3.2.4 As part of any significant future development, the need for additional or alternative access will be reviewed.

### 18.3.1 Introduction

The University of Toronto Mississauga (UTM) is located on the east side of Mississauga Road, north of Dundas Street. The campus is situated on 91 ha of land adjacent to the Credit River.



# 19 Implementation

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The purpose of this chapter is to establish how the policies of Mississauga Official Plan are implemented and translated into programs. This Plan will be implemented by the powers conferred upon City Council by the *Planning Act*, the *Municipal Act*, and other statutes as may be applicable. For instance, this Plan will be implemented by zoning by-laws, heritage designation by-laws, subdivision control, site plans, municipal legislation and the construction of public works.

## 19.1 Jurisdiction

19.1.1 The policies of this Plan apply to all lands within the City of Mississauga, except for those owned by the Federal Crown or the Provincial Crown. Should lands owned by the Federal Crown or the Provincial Crown be sold to an agency that is not a crown agency of the Federal or Provincial governments or to a private owner, the policies of this Plan will apply.

## 19.2 Monitoring

19.2.1 The Official Plan will be reviewed every five years or earlier, if warranted.

19.2.2 Mississauga may develop a set of indicators to measure the implementation of the policies of this Plan.

## 19.3 Finance

Development will support itself in terms of capital requirements. The implementation of the policies of this Plan will be subject to the capital budget and financial policies and procedures of City Council, as

well as availability of Regional and Provincial Government funding. The Official Plan also acknowledges that some services are provided to the City by other levels of government.

The City has adopted a Development Charges By-law as authorized by the *Development Charges Act*. The by-law implements the charges required to support the emplacement of capital facilities, as determined by various needs assessments, over the next ten to twenty years. The by-law also contains the capital costs to be charged per industrial/commercial and residential hectare developed and per housing unit constructed.

The by-law will be reviewed periodically as required by the *Development Charges Act* and revised, as necessary.

19.3.1 Mississauga will strive to maintain an appropriate relationship between residential and non-residential assessment in order to maximize City revenues and minimize City expenditures. To this end, staff will monitor the appropriate financial indicators, such as tax rates, capital contribution levels, residential to commercial/industrial assessment ratio, vacancy rates, and overall growth rates.

19.3.2 Mississauga will provide quality services to the residents and businesses of Mississauga that are accessible throughout the city and that recognize specific community needs.

## 19.4 Development Applications

19.4.1 Development applications will be evaluated and processed in accordance with the policies of this Plan, approved **streetscape** studies and design guidelines and other relevant City Council policies and Provincial policies.



Figure 19-1: Development applications are subject to a number of criteria and require extensive review. Once the application is reviewed with all supporting documentation, the application is presented before City Council for approval.

19.4.2 To ensure that the policies of this Plan are being implemented, the following controls will be regularly evaluated:

- a. Mississauga Official Plan;
- b. Mississauga Zoning By-law;
- c. Site Plan Control By-law;
- d. Urban Design Guidelines;
- e. City of Mississauga Heritage Register; and
- f. all other practices and procedures involved in processing development applications.

19.4.3 To provide consistent application of planning and urban design principles, all development applications will address, among other matters:

- a. the compatibility of the proposed development to existing or planned land uses and forms, including the transition in height, density, and built form;
- b. conformity with the policies in this Plan;
- c. the sustainability of the development to support public transit and to be oriented to pedestrians;
- d. in circumstances where medium and high density residential uses are in proximity to developments of a lower density, measures, such as increased setback; sensitive building location, transition and design; and landscaping, may be required to ensure compatibility with the lower density designations;
- e. the adequacy of engineering services;
- f. the adequacy of community infrastructure;
- g. the adequacy of the multi-modal transportation systems;
- h. the suitability of the site in terms of size and shape, to accommodate the necessary on site functions, parking, landscaping, and on site amenities;
- i. the relationship of the proposed development to the street environment and its contribution to an effective and attractive public realm;

- j. the impact of the height and form of development, in terms of overshadowing and amenity loss, on neighbouring residential and park uses;
- k. site specific opportunities and constraints;
- l. sustainable design strategies; and
- m. urban form and public health.

19.4.4 Prior to the submission of a development application, the City may require a meeting with the development proponent and/or their agent.

19.4.5 Some or all of the following studies, reports and/or documents may be required as part of a complete application submission for an official plan amendment, rezoning, draft plan of subdivision or condominium or consent application, dependent on the type of application, the property location and adequacy of services.

Submitted material must be satisfactory to the City and relevant agency, if applicable, such as conservation authorities in the case of an Environmental Impact Study. Further, the required material must be deemed satisfactory in the early stages of application review. This list is not inclusive, and other material may be requested when the application is reviewed.

The scope of the studies and the terms of reference will be determined at the pre-application meeting prior to application submission, and/or appropriate staff contacts will be provided for scoping purposes. If the requested material is not provided, the application will be deemed incomplete by the City.

- Development Application Review Checklist, as completed at the Development Application Review Committee (pre-application) meeting
- Notice signage erected on site in accordance with the City of Mississauga's guidelines and regulations
- Complete application form and required fees

- Planning Justification Report, which may include a plan of survey and a concept plan for the subject site and surrounding lands
- Draft official plan amendment
- Draft zoning by-law amendment
- Draft plan of subdivision or condominium
- Development Master Plan
- Urban Design Study
- Arborist's Report (including Tree Survey/Tree Preservation Plan)
- Parking Utilization Study
- Sun/Shadow/Wind Study
- Environmental Impact Study
- Slope Stability Study/Top of Bank Survey
- Downstream Erosion Impact Report/Investigation
- Functional Storm Drainage Report
- Stormwater Management Study
- Stream Erosion Assessment
- Implementation study for Two Zone Floodplain Policies
- Environmental Site Screening Questionnaire and Declaration
- Site Remediation Studies, including Phase I Environmental Site Assessment, Phase II Environmental Site Assessment, Remedial Work Plan, Site Clean Up Report
- Acoustical Feasibility Study (for stationary, road, rail and/or airport noise sources)
- Vibration Analysis
- Air Quality Study
- Geotechnical Report

- Transportation Impact Study (including scoped studies such as gapping, signal operations, and/or other relevant traffic issues)
- Traffic Safety Impact Study (including access review, sight lines, queuing, gapping and collisions)
- On Street Parking Analysis (including type, availability and feasibility of the on street parking arrangement)
- Park Concept Plan
- **Heritage Impact Statement**
- Heritage Conservation Plan
- Community Uses Needs Assessment
- Above and below ground Utility Plans (existing and proposed) on City lands
- Archaeological Assessment
- Restrictions on Title (where the creation of Common Element Condominium lots precedes the creation of the private road)
- Transportation Demand Management (TDM)
- Health Impact Study
- Sustainability Design

19.4.6 Proposals for buildings higher than three storeys will be designed to minimize overlook conditions; obstructions of grade level vistas and overshadowing of any adjacent properties. In this regard, sun and shadow studies, view studies and microclimatic studies may be required to determine the impacts of the proposal. For the purpose of this policy, the above noted studies generally would not be required for adjacent lands used for industrial purposes.

19.4.7 To provide consistent, efficient, and predictable application of environmental planning principles, all applications will have regard for:

- a. promotion of public transit;

- b. promotion of cycling and walking;
- c. management of **waste** ;
- d. energy and water conservation;
- e. quality and quantity of stormwater management;
- f. noise and vibration minimization;
- g. habitat protection and enhancement;
- h. erosion and sediment control;
- i. tree preservation;
- j. land form conservation;
- k. air quality;
- l. soil and groundwater quality, and
- m. public health.

19.4.8 Provincial Government policies and guidelines will be used in reviewing development applications.

19.4.9 Where appropriate, the City will coordinate environmental performance criteria with the programs, policies, and legislation of appropriate Provincial Government agencies, conservation authorities, and the Region.

19.4.10 Rights-of-way or additions thereof, and private roads will not be included in the calculation of density for on site building coverage.

19.4.11 A development master plan may be required when a development proposal may set a precedent for the use, scale and form of future development of a site or area (e.g., Community Node, **Major Transit Station Area**). In consultation with the development proponent, the City will identify the lands for which the development master plan is required and the matters to be addressed. The development master plan will identify how the site or area may be developed to achieve the intent of this Plan and address, where applicable, matters such as:

- a. height, scale and location of proposed uses;

- b. how density and population to employment ratio requirements will be achieved;
- c. community and physical infrastructure requirements (e.g., parks, roads, water and sewers);
- d. environmental requirements (e.g., green development standards);
- e. transition and connectivity to surrounding development; and
- f. treatment of the public realm.

The development proponent may be required to consult with other landowners in the development master plan area. A development master plan may be endorsed by Council as part of a development application.

## **19.5 Criteria for Site Specific Official Plan Amendments**

19.5.1 City Council will consider applications for site specific amendments to this Plan within the context of the policies and criteria set out throughout this Plan. The proponent of an official plan amendment will be required to submit satisfactory reports to demonstrate the rationale for the amendment; including, among other matters:

- a. that the proposed redesignation would not adversely impact or destabilize the following:
  - the achievement of the overall intent, goals, objectives, and policies of this Plan; and
  - the development or functioning of the remaining lands that have the same designation, or neighbouring lands; and
- b. that a municipal comprehensive review of land use designations or a five year review is not required;

- c. that the lands are suitable for the proposed use, and a planning rationale with reference to the policies of this Plan, other applicable policies, and sound planning principles is provided, setting out the merits of the proposed amendment in comparison with the existing designation;
- d. land use compatibility with the existing and future uses of surrounding lands; and
- e. the adequacy of engineering services, community infrastructure and multi-modal transportation systems to support the proposed application.

19.5.2 Mississauga may initiate site specific amendments to this Plan through local area plans or other planning studies. In order to demonstrate the viability of a proposal, development proponents may be required to submit satisfactory studies prior to the development of the site.

## **19.6 Zoning**

19.6.1 The zoning for all properties will conform to this Plan within three years of it coming into force and effect.

## **19.7 Zone Subject to a Holding Provision**

19.7.1 A holding provision may be used in conjunction with any zoning category to specify the use to which lands, buildings or structures may be put at such time in the future as the holding provision is removed by amendment to the zoning by-law.

19.7.2 A holding provision will be used to implement this Plan for staging of development and additionally may be used to implement specific requirements, such as, but not limited to:



- a. the adequacy of engineering services;
- b. the adequacy of community infrastructure;
- c. the adequacy of multi-modal transportation systems;
- d. the adequacy of access/ingress to a site and in respect of adjacent properties;
- e. development of affordable housing;
- f. protection of the Natural Areas System;
- g. the remediation of **contaminated sites**;
- h. the provision of parkland; and
- i. the provision of flood free ingress/egress.

19.7.3 Generally, uses existing at the time a site specific by-law with a holding provision is enacted will be allowed to continue, however, restrictions on existing uses and/or alternative uses may be specified in the by-law with a holding provision.

19.7.4 Mississauga may apply a holding provision to lands within Intensification Areas to ensure that the policies of this Plan are implemented. The removal of the holding provision will be conditional on the applicant satisfying the requirements of the policies of this Plan.

19.7.5 The policies of this Plan should not be construed to require the gratuitous dedication of land for new public roads, including realignments of roads or impose an obligation upon a landowner to construct or pay for the construction of new roads, where not otherwise permitted by the *Planning Act*.

## 19.8 Bonus Zoning

19.8.1 City Council may pass by-laws permitting increases in height and/or density for development permitted by this Plan and/or the zoning by-law to enable the City to secure specific amenities that benefit the city. These by-laws are intended to allow the community to tangibly share in benefits that

landowners accrue from achieving increased height and/or density permissions on their lands. To be eligible for increases in height and/or density, the proposed development must constitute good planning, be consistent with the intent and objectives of this Plan, and comply with all other City of Mississauga policies.

19.8.2 City Council may grant bonuses in height and/or density of site specific development proposals in exchange for facilities, services or matters, above and beyond that that would be otherwise provided under the provisions of the *Planning Act*, the *Development Charges Act* or other statute, such as, but not limited to:

- a. protection of significant views and vistas of Lake Ontario;
- b. provision of parkland above that required by the *Planning Act*;
- c. enhancement of the Natural Areas System;
- d. provision of additional road or servicing improvements;
- e. provision of multi-modal transportation facilities;
- f. provision of community infrastructure;
- g. provision of a wide range of housing types, including affordable, assisted and special needs housing;
- h. preservation of heritage resources;
- i. provision of public art;
- j. enhanced urban design features;
- k. provision of **streetscape** improvements;
- l. contributions to city wide funds for public art or affordable housing;
- m. environmental development performance standards or LEED certification that exceeds that required by the Official Plan; and

- n. inclusion of office space in high density areas to meet population-to-employment ratios.

19.8.3 In all cases, the increase in height and/or density will be based on a site specific review. In reviewing the proposed increase in height and/or density, City Council will ensure that:

- a. the proposed development is compatible with the scale and character of the surrounding area and has minimal impact on neighbouring uses;
- b. there are adequate engineering services and community services;
- c. the transportation system can accommodate the increase in density;
- d. the site is suitable in terms of size and shape, to accommodate the necessary on site functions, parking, landscaping, and recreational facilities; and
- e. a special study is required from the applicant that establishes a reasonable relationship between the benefit to the owner of the value of the density increase that may be permitted and the value of the facility, service, or matter to the public.

19.8.4 When considering bonusing, and allowing the provision of benefits off-site, the positive impacts of the exchange should benefit the surrounding areas experiencing the increased height and/or density.

19.8.5 By-laws permitting bonusing of height and/or density will:

- a. specify the amount by which the height and/or density of the development would be increased in exchange for certain facilities, services, or matters; and
- b. contain the detailed development standards that would apply to the site to lessen the impact the proposed increase in height and/or density may have on the surrounding area.

19.8.6 The facilities, services, or matters will be transferred to the City or secured by agreements

entered into by the developer and the City, prior to or in conjunction with the enactment of the bonus zoning by-law.

19.8.7 Mississauga may develop bonusing policies applicable to specific areas of the city.

## 19.9 Temporary Use By-law

19.9.1 City Council may pass by-laws to authorize the temporary use of land for a purpose that is otherwise prohibited by the zoning by-law, as permitted by the provisions of the *Planning Act*.

19.9.2 A temporary use which conforms to this Plan may be permitted by a temporary use by-law to allow:

- a. an unfamiliar use on a trial basis;
- b. the use of an available building until the rehabilitation or redevelopment of the building for a use permitted by this Plan is warranted by future market conditions; or
- c. the use of vacant land for a parking lot that would otherwise not be permitted.

19.9.3 The following conditions will apply to all uses permitted by a temporary use by-law:

- a. extensions of the period of temporary use may be permitted by subsequent by-laws but should generally not continue for more than a total of ten years for a temporary use of a garden suite and three years in all other cases as per the *Planning Act*;
- b. no new buildings or expansion of buildings, except for temporary or movable structures, will be permitted;
- c. the temporary use permitted must be compatible with adjacent land uses, or measures to mitigate any adverse impacts must be applied;
- d. no adverse impacts on traffic or transportation facilities in the area may result, and sufficient parking must be provided on-site;
- e. no adverse impact on community infrastructure;



Figure 19-2: Although most of Mississauga is built out, there are still portions of the city that will require a plan of subdivision. Churchill Meadows is one of Mississauga's most recent communities that exemplifies good planning, with appropriate road connections, built form, servicing and a mix of uses.

- f. no adverse impacts on the assessment base;
- g. the temporary use will not jeopardize the eventual planned land use; and
- h. temporary buildings must conform to the property standards by-law.

## 19.10 Interim Control By-law

19.10.1 City Council may pass an Interim Control By-law prohibiting the use of land, buildings or structures within the city or within a defined area of the city for a limited period of time as specified by the *Planning Act*. Interim Control measures may be considered and enacted for the purposes of undertaking studies where development pressures warrant a review of zoning.

## 19.11 Development Zone

19.11.1 Vacant lands and legally existing land uses that do not conform to this Plan, may be recognized in the zoning by-law as a “D” (Development) Zone. It is intended that these lands will eventually be redeveloped in accordance with the policies contained in this Plan, but in the meantime allow legally existing uses to continue without a non-conforming status.

## 19.12 Development Permits

19.12.1 Consideration will be given to the enactment of a Development Permit System as authorized by the *Planning Act*.

## 19.13 Subdivisions

19.13.1 Draft plans of subdivision will comply with the policies of this Plan and relevant City Council approved policies, including those regarding the provisions of adequate services and transportation facilities, and the maintenance of a sound financial position for the City.

19.13.2 A condition of draft approval of a plan of subdivision will require that a servicing and development agreement be entered into by the developer to ensure the provision of services, facilities, and other matters to the satisfaction of City Council and the Region of Peel.

19.13.3 By-laws may be passed to exempt all, or part of registered plans of subdivision from part-lot control. Such exemptions will eliminate the need for further subdivision or consents to convey portions of lots within the registered plan of subdivision.

## 19.14 Site Plans

19.14.1 As permitted by the *Planning Act*, all lands in the city are designated as a Site Plan Control Area. By-laws may be passed to designate the whole or any part of the city as a Site Plan Control Area, or identify where site plan control does not apply. By-laws may also be passed to designate a Site Plan Control Area by reference to one or more land use designations and/or zone categories.

19.14.2 As all lands in the city are designated as a Site Plan Control Area, drawings showing plan elevation and cross-section views may be required for each building to be erected on any lands in the city and to be used for residential purposes including buildings containing less than 25 dwelling units. Applications for site plan approval will be required to contain sufficient information to ensure compliance with all relevant matters contained in the *Planning Act*.





Figure 19-3: Applications for Site Plan Approval will be required to contain sufficient information to ensure compliance with all relevant matters contained in the *Planning Act*.

19.14.3 Energy conservation, aesthetic, and functional design guidelines will be established to assist in the preparation of site plans and the design of buildings.

19.14.4 Site plan applications will address matters relating to exterior design such as, but not limited to, the character, scale, appearance and design features of all buildings, and their sustainable design.

19.14.5 Site plan applications will address the sustainable design elements on the development site and adjoining highways under Mississauga's jurisdiction including without limitation trees, shrubs, hedges, plantings or other ground cover, permeable paving materials, street furniture, curbs, ramps, **waste** and recycling containers, and bicycle parking facilities.

## 19.15 Condominiums

19.15.1 Condominium applications will be required to comply with the policies of this Plan and relevant City Council approved policies.

19.15.2 Application for approval of a condominium application for new residential development or conversion of existing rental accommodation will be considered with reference to standards and specifications regarding safety, driveways, traffic, parking control, parking facilities, sidewalks and walkways, open space, recreation, utilities, storm drainage, grading, and internal road construction.

19.15.3 Common element condominiums and vacant land condominiums will not be permitted if the City requires public ownership of the lands for pedestrian or vehicular access to create public road connections to existing developed or undeveloped lands. Regard shall be given to the scale of the development and its integration with the surrounding community.

## 19.16 Non-Conforming Uses

19.16.1 The use of land or a building or structure that does not comply with the Zoning By-law but which lawfully existed prior to the approval of the Zoning By-law is a legal non-conforming use. If such legal non-conforming use ceases, then the rights derived from the legal non-conforming use will terminate. Over time, it is the intention and expectation of the City that most non-conforming uses will cease.

19.16.2 Certain non-conforming uses, particularly industrial or commercial uses in residential areas that detract from the character and quality of a complete neighbourhood, will be encouraged to relocate or redevelop so that the subject land may be used in conformity with the policies of this Plan and the provisions of the implementing zoning by-law. In this regard, special attention will be given to the re-establishment of the use in a different location, where it will be able to perform and



produce under improved conditions, in accordance with the policies of this Plan.

19.16.3 To encourage the elimination of certain non-conforming uses of significant detrimental impact on their surroundings, the City may consider the desirability and feasibility of acquiring the legal non-conforming property and holding, selling, leasing, or redeveloping it in accordance with sound financial management and good planning principles. In order to acquire such land the City may consider an exchange of other City owned lands.

19.16.4 Applications may be made to the Committee of Adjustment to permit the extension or enlargement of a building or structure utilized for a non-conforming use or to permit a different but similar use to replace the original legal non-conforming use. The City will consider the following criteria in determining its position with respect to such applications:

- a. the primary intention that non-conforming uses will cease over time;
- b. the length of time the existing non-conforming use has been in operation;
- c. the record of the non-conforming use in terms of its adherence to all municipal by-laws and other regulations to which its operation may be subject;
- d. the nature and characteristics of the existing non-conforming use;
- e. whether the use has achieved an acceptable level of compatibility with adjacent uses;
- f. that any permitted expansion or alteration is minor in nature, is moderate in scale, and will not detract from the intent of this Plan;
- g. whether the proposed expansion or alteration unduly aggravates the situation created by the existence of the use;
- h. the characteristics of the existing non-conforming use and the proposed extension or enlargement will not contribute to air, noise, or

water pollution and will not result in nuisances such as, vibration, fumes, smoke, dust, odour, or lighting infringement, or will not affect the control of flooding or conservation of land;

- i. the existing use or proposed expansion or alteration does not involve a threat to the safety of its neighbours;
- j. that neighbouring uses can be adequately protected by the provision of landscaping, buffering, or screening; appropriate setbacks for buildings and structures; and devices and measures for reducing nuisances;
- k. traffic and parking conditions in the vicinity will not be adversely affected;
- l. adequate provisions will be made for off-street parking and loading facilities;
- m. all engineering services and community infrastructure will be adequate; and
- n. whether endorsement establishes a significant precedent for further amendments to this Plan or for similar uses.

19.16.5 City Council may also in special and appropriate circumstances, consider the enactment of an amending zoning by-law to permit the extension or enlargement of a legal non-conforming use, although the preferred procedure will be to encourage the use of applications to the Committee of Adjustment.

## 19.17 Design Excellence



Figure 19-4: A Design Review Panel may be created to provide advice on applications, specifically design related matters that may affect the public realm. Development proponents may be required to submit their application to this Panel for review.

19.17.1 Mississauga will encourage and recognize creativity; sustainability and design excellence in architecture; landscape and urban design and stimulate public awareness by:

- a. administering an awards program that promotes; recognizes and honours development projects and initiatives that set the highest standard for sustainable practices; architectural; landscape; urban design and construction in the city; and
- b. encouraging design competitions for sustainable practices; architecture; landscape and urban design.

19.17.2 Mississauga may establish a Design Review Panel to provide advice on design related matters that affect the public realm.

19.17.3 Mississauga may require development proponents to submit their application to the Design Review Panel.



Figure 19-5: Mississauga has many beautiful parks and recreational facilities, such as the Frank McKechnie Community Centre and Library, located in the Hurontario Neighbourhood Character Area. In addition to City reserves, development contributions also play an important role in the creation of open spaces and recreational facilities for all residents to enjoy.

## **19.18 Greenbelt**

19.18.1 As a condition of development approval, **natural hazard lands** may be placed in public ownership for their long term protection.

19.18.2 Greenbelt is determined on a site by site basis and is defined by natural hazards associated with **watercourse** corridors and Lake Ontario, and the limits of identified natural areas. The limits of the Greenbelt are determined in consultation with the City and appropriate conservation authority and through studies, where required, completed by the proponent to the satisfaction of the City and the appropriate conservation authority.

19.18.3 Surface drainage and stormwater management facilities and associated hazards will be designated Greenbelt. Where possible, surface drainage and stormwater management facilities should be designed in a manner that restores natural habitat links or buffers natural areas. The location of these facilities will not be detrimental to the natural area system.

19.18.4 Greenbelt lands will be conveyed to the City or other public agency. Such lands will not be accepted as part of the dedication of land for park or other public recreational purposes contribution or credited against any cash in lieu for park or other public recreational purposes or be included in the calculation of density for building coverage.

19.18.5 Development adjacent to Greenbelt lands will be subject to the delineation of natural hazards, natural areas, buffers and setbacks by the City in consultation with the appropriate conservation authority. Dedication and/or restrictive zoning of buffers to Greenbelt may also be required by the City in consultation with the appropriate conservation authority.

19.18.6 Prior to conveyance of Greenbelt lands, the proponent may be requested to conduct a Phase 1 Environmental Site Assessment (ESA) in accordance with the latest standards of the Canadian Standards Association and the Ministry of Environment. Should

the Phase 1 ESA indicate that further investigation is required, the proponent may be requested to conduct further investigation, perform site clean up and provide the appropriate environmental documentation. The conveyance of the Greenbelt lands will occur after the proponent has completed all requirements to the satisfaction of the City.

19.18.7 The Greenbelt designation applies to both public and privately owned lands. Where Greenbelt land is privately owned, this Plan does not imply that it is free and open to the general public or that it will be acquired by the City or any other public agency. Consideration will be given, however, to public acquisition of these areas.

19.18.8 Reconstruction, minor additions, and maintenance of legal non-conforming facilities, buildings, and structures may be permitted in Greenbelt. With respect to reconstruction, the development should be relocated to an area that is least susceptible to natural hazards.

## **19.19 Public Open Space and Recreational Facilities**

19.19.1 Mississauga will provide for public open space and/or recreational facilities in accordance with the following means:

- a. dedication of land for park or other public recreational purposes, or cash-in-lieu for park or other public recreational purposes, under the provisions of the *Planning Act*;
- b. receipt of levies, under the provisions of the *Development Charges Act*; or
- c. other sources.

19.19.2 As a condition of development of land, the City may require that a portion of the land proposed to be developed or redeveloped be conveyed to the City for park or other public recreational purposes, in a form satisfactory to the City, to ensure that the lands are clean at the time of conveyance.

- a. In the case of land proposed to be developed or redeveloped for predominately residential purposes, the amount of land to be conveyed will be calculated using:
  - a rate not exceeding five percent of the developable land; or
  - a rate not exceeding one hectare for each 300 dwelling units proposed; or,
  - whichever amount is greater.
- b. In the case of land proposed to be developed or redeveloped for predominately non-residential purposes, the City will require the conveyance of land at a rate not exceeding two percent of the developable land.
- c. In instances where a land dedication is not required by the City, the City may alternatively require the payment of cash in lieu of such conveyance. The value of the land will be determined as of the day before the day of the issuance of the first building permit in respect of the development or redevelopment. The payment will be made prior to the issuance of the first building permit.

19.19.3 As a condition of approval of a plan of subdivision, Mississauga will require that a portion of the land in the plan be conveyed to the City for park or other public recreational purposes in a form satisfactory to the City, to ensure that the lands are clean at the time of conveyance.

- a. In the case of a subdivision for predominately residential purposes, the amount of land which the City will require to be conveyed will be the amount calculated using:
  - a rate not exceeding five percent of the land included in the plan of subdivision; or,
  - a rate not exceeding one hectare for each 300 dwelling units proposed; or whichever amount is greater.
- b. In the case of a subdivision for predominately non-residential purposes, the City will require

the conveyance of land at a rate not exceeding two percent of the land included in the plan of subdivision.

- c. In instances where a land dedication is required that only partially satisfies the dedication of land for park or other public recreational purposes yield for the subdivision, the payment of cash in lieu for park or other public recreational purposes will be required for the balance owing on the yield. For the purpose of determining the amount of any payment, the value of the land will be determined as of the day before the day of the approval of the draft plan of subdivision. The payment will be made prior to the registration of the plan of subdivision.
- d. In instances where a land dedication or cash in lieu for park or other public recreational purposes was not required by the City as a condition of subdivision approval, the City may require payment of cash in lieu for park or other public recreational purposes as a condition of development or redevelopment of the land in accordance with the *Planning Act*, as amended, prior to the issuance of building permits. The value of the land will be determined as of the day before the day of the issuance of the first building permit in respect to the development or redevelopment. The payment will be made prior to the issuance of the first building permit.

19.19.4 Mississauga will provide recreational facilities as specified in the Future Directions for Library, Recreation, Parks and Natural Areas Master Plan, subject to any of the following:

- a. limitations in the area and/or configuration of established parks that inhibits construction of such facilities, and which cannot be expanded through land acquisition due to surrounding development;
- b. limited opportunity to acquire new parkland in established areas;
- c. restrictions on parkland development dictated by the location of the park, environmental



constraints, parking availability, access, or other reasons;

- d. specialized, identified recreational needs for parkland in certain areas of the city;
- e. the timing and pace of development which may affect the City's ability to acquire parkland through the development process; and
- f. financial circumstances and the establishment of priorities through the City's capital budget process.

19.19.5 The facility objectives contained in Future Directions for Library, Recreation, Parks and Natural Areas Master Plan, may change as market demand and other studies determine that the public's recreational needs have changed. Failure to achieve facilities at the levels contained in the Future Directions for Library, Recreation, Parks and Natural Areas Master Plan, will not constitute lack of conformity with this Plan.

19.19.6 Mississauga is not required to utilize any or all of the cash in lieu for park or other public recreational purposes that may be required to be paid as a condition of a particular development, to acquire land for park or other public recreational purposes or develop recreation facilities in the vicinity or neighbourhood of that development. The City will consolidate all cash in lieu for park or other public recreational purposes in a special reserve fund and use the funds on a city wide basis based upon priorities determined by the City for any purposes permitted under the *Planning Act*.

19.19.7 Mississauga will participate with representatives of the school boards to coordinate the planning, acquisition, and administration of sites and facilities that will be shared by park and school activities.

19.19.8 Mississauga will cooperate with other levels of government or the private sector to establish one or more parks having regional significance and containing major sports and recreational facilities suitable for competition and public use, including major spectator and competitive sports facilities.

19.19.9 Where lands are designated Private Open Space, it is not intended that they be free and open to the general public nor that they will be necessarily acquired by the City or any other public agency. Consideration will be given, however, to public acquisition of these lands through the development approval process or through the City's land securement program.

19.19.10 Mississauga will encourage the Conservation Authorities to acquire lands for conservation and recreation purposes beyond that required for flood control purposes.

19.19.11 Mississauga will encourage the Conservation Authorities to formulate acquisition and development strategies for conservation areas within the city to ensure that lands acquired will form integral components of the municipal public open space system.

19.19.12 Where lands owned by conservation authorities have value for recreation and conservation, and are not required for flood control purposes, the City will seek the cooperation of the conservation authorities to lease or convey such lands to the City for park purposes.

19.19.13 Mississauga will encourage other levels of government to assist in the provision of recreational facilities which have a National, Provincial or Regional significance.

19.19.14 Mississauga may request that the Provincial Government lease or convey to the City for public open space purposes any lands contained within the Parkway Belt West that are residual to the essential functions of the Parkway Belt West Plan or within areas designated for Public Open Space, within this Plan.



## 19.20 Property Standards

19.20.1 Mississauga will maintain and enforce a Property Standards By-law prescribing maintenance standards and conditions of occupancy for all types of property throughout the city.

19.20.2 Mississauga may make provision for public services in areas where it can be demonstrated that the lack or inadequacy of such services is a factor in the deterioration of properties.

## 19.21 Demolition Permits

19.21.1 Mississauga may enact a by-law creating areas of demolition control as authorized by the *Planning Act*.

19.21.2 Permits to demolish heritage designated buildings and structures will be considered in accordance with the provisions of the *Ontario Heritage Act* and this Plan.

19.21.3 An approved development plan, archaeological assessment and tree permit may be required prior to the release of a demolition permit.



Figure 19-6: The Lakeview Generating Station, also known as the Four Sisters, ceased operations in April 2005 after 43 years and was demolished in June 2006. The former coal burning station had been in operation since the early 1960's.

## 19.22 Community Improvement

19.22.1 In accordance with the *Planning Act*, all or a portion of the lands within the city, may, by by-law, be designated as a Community Improvement Project Area.

19.22.2 Mississauga may by by-law identify Community Improvement Project Areas, prepare and adopt Community Improvement Plans, and implement Community Improvement projects pursuant to the provisions of the *Planning Act*.

19.22.3 Mississauga may become involved in improving municipally owned lands, services, and facilities and encourage private property owners in these areas to undertake similar improvements to the benefit of the entire area.

19.22.4 Mississauga may acquire lands or buildings in order to undertake community improvement initiatives.

19.22.5 The Region of Peel may be a planning and/or financial partner in a Community Improvement Plan for matters within its jurisdiction.

19.22.6 Community Improvement Plans may consider the following, among other matters:

- a. deficiencies in the physical infrastructure of the area including sanitary sewers, water or storm sewer systems, roads, sidewalks, curbs, street lighting, and electrical facilities;
- b. deficiencies in the provision of off street parking areas;
- c. inadequate park space, open space, recreation, and other **community facilities**;
- d. for commercial areas, evidence of economic decline such as unstable uses or high vacancy rates;
- e. the existence of conflicting land uses;
- f. the condition of the housing and building stock if poor and in need of repair;

- g. identification of the need to provide affordable housing;
- h. the potential of creating a Business Improvement Area (BIA) or expanding an existing BIA;
- i. identification of the need to improve **streetscape** amenities;
- j. identification of the need to conserve heritage resources;
- k. identification of the need to provide cultural infrastructure;
- l. opportunities for infilling and development of underutilized sites;
- m. soil and water conditions, based on past industrial and/or commercial uses, resulting in potential for contamination and need for remediation;
- n. identification of the need to encourage office and other employment opportunities;
- o. identification of the need to encourage energy improvements; and
- p. opportunities to support the growth management objectives of this Plan and encourage transit supportive communities.

19.22.7 Community Improvement Plans may be implemented by the following methods:

- a. participation in funding programs with senior levels of government that provide assistance in undertaking Community Improvement projects;
- b. the formation and continuation of BIAs to maintain and improve commercial areas;
- c. the preparation of design guidelines which outline necessary **streetscape** improvements and beautification plans for the area;
- d. the encouragement of site remediation and/or infill and development that is in harmony with the existing pattern and character of the surrounding lands;

- e. the acquisition and assembly of lands for public facilities and infrastructure, and possible development;
- f. the application of the *Ontario Heritage Act* to preserve and enhance heritage buildings, where appropriate;
- g. the application and enforcement of Property Standards By-laws for the maintenance and occupancy of residential, commercial, industrial, and institutional properties within Community Improvement Areas; and
- h. allocation of public funds, in the form of grants, loans or other financial instruments for the physical rehabilitation or improvement of land and/or buildings including the remediation of contaminated properties.

19.22.8 The following will be considered when determining the timing and sequence of Community Improvement projects:

- a. the opportunity to coordinate improvements with other Capital Budget projects;
- b. the existence of a recognized BIA;
- c. the efforts of local business associations to upgrade and promote the area; and
- d. availability of other government funds through programs in which the City may wish to participate.

19.22.9 The formation and continuation of BIAs will be encouraged and supported; when possible, assistance will be provided to such organizations.

## 20 Glossary

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All terms listed in the glossary, where used, are bolded and italicized in the Official Plan text.

### ACTIVE TRANSPORTATION

means any form of self-propelled transportation, which relies on human energy and mobility assisted devices such as, walkers, wheel chairs and scooters. ***Active transportation*** modes include: walking, jogging, cycling, and in-line skating. Typically, these modes utilize on-road and off-road facilities such as sidewalks, cycling lanes, and multi-use trails and may also be combined with public transit.

### ANCHOR HUBS

means the ***Major Transit Station Area*** associated with the regional transportation system, that has significant potential to attract and accommodate new growth and development. These hubs have the potential to be planned for major institutions, employment centres, shopping centres. ***Anchor hubs*** also have strategic importance due to their relationship with the Downtown and Airport.

### BIODIVERSITY

means the variety of life in all forms, levels, and combinations. It includes ecosystem and landscape diversity, species diversity, and genetic diversity.

### COGENERATION

means the generation of two or more forms of heat energy, electrical power and mechanical power from one fuel source, such as gas or renewable sources, but excluding oil, coal and nuclear.

### COMMUNITY FACILITIES

means a facility operated by or on behalf of a public authority for the provision of community activities such as, but not limited to recreation, libraries, arts, crafts, museums, social and charitable activities. This includes pools, outdoor rinks and arenas. Private gyms, banquet halls/conference centres or convention centres are not considered ***community facilities***.

### CONTAMINATED SITES

means property or lands that have not been rehabilitated and, for reasons of public health and safety or environmental quality, are unsafe for use as a result of human activities, particularly those activities that have left a chemical or radioactive residue. Such sites include some industrial lands, some transportation facilities, electrical facilities, and some abandoned mine hazards.

## CORRIDOR

means lands adjacent to and framing a right-of-way. (Corridors are shown on Schedules 1: Urban System and 1c: Urban System - Corridors.)

## CREMATORIUM

means a building, structure or part thereof fitted with a retort(s) for the purpose of cremating human remains and may include a processing area and body storage area.

## CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN (CPTED)

means the proper design and effective use of the built environment which may lead to a reduction in the fear and incidence of crime, and an improvement of the quality of life.

## dBA

means a sound pressure level indicated by a measurement system that includes an A-weighting network. The A-weighting network approximates the relative sensitivity of the normal human ear to different frequencies of sound. The resulting value is in decibels and is commonly labelled **dBA**.

## ECOSYSTEM APPROACH

means an approach to planning and management which recognizes that economy, community, and environment are inextricably linked and equally important for the health of the city.

## ELECTRIC POWER DISTRIBUTION AND TRANSMISSION FACILITY

means buildings, structures and infrastructure that facilitate the distribution of electric power such as hydro lines, transformers, transfer stations and switch gear but does not include a **major power generating facility** or **minor power generating facility**.

## ENVIRONMENTALLY-SENSITIVE OR SIGNIFICANT AREA

means places where ecosystem functions or features warrant special protection. These may include but are not limited to rare or unique plant or animal populations or habitats' plant or animal communities, or concentrations of ecological functions. In the city, **environmentally-sensitive or significant areas** are inventoried and designated by Conservation Authorities and the Provincial Government.

## FLOODLINE

means a line as determined in accordance with criteria specified by the Provincial Government.

## FLOOR SPACE INDEX (FSI)

means the ratio of the gross floor area of all buildings on a site to the net developable area of that site. The gross floor area calculated for purposes of **floor space index (FSI)** is generally measured from the exterior of outside walls, but does not generally include mechanical areas, stairwells, washrooms, elevators, storage, and parking or other items as defined in specific Zoning By-laws.



## FLORISTIC CO-EFFICIENT

are restricted to those habitats. For detailed information regarding *floristic co-efficients*, the City of Mississauga Natural Areas Survey, should be consulted.

## FLORISTIC QUALITY INDEX (FQI)

means a system that allows for an objective numerical evaluation of an area based on the quality of its flora. For detailed information regarding the methodology for calculating the *Floristic Quality Index (FQI)*, the City of Mississauga Natural Areas Survey, should be consulted.

## GATEWAY HUB

means a *Major Transit Station Area* with high current or potential development capacity that makes it a significant origin and/or destination on the transit system. *Gateway hubs* may also locate at the interchange between two or more current or planned regional rapid transit lines.

## GROUND WATER RECHARGE

means an area in which there is significant addition of water by natural processes to ground water.

## HERITAGE EASEMENT

means a legal agreement between the property owner and the City or the property owner and the Ontario Heritage Trust whereby a set of regulations regarding the heritage conservation of the property are established in perpetuity.

means a system whereby the flora of Ontario are ranked in descending order by a plants needs for the quality of habitat in which they will persist and thus

## HERITAGE IMPACT STATEMENT

means a statement that will identify all heritage resources of a property; describe and evaluate their heritage significance; and, evaluate their sensitivity to a proposed development, use or reuse, including, where possible, measures to mitigate deleterious consequences.

## HIGHER ORDER TRANSIT

means transit that generally operates on its own dedicated right-of-way, outside of mixed traffic, and therefore can achieve a frequency of service greater than mixed traffic transit. *Higher order transit* can include heavy rail (such as subways), light rail (such as streetcars), and buses in dedicated rights-of-way.

## INTENSIFICATION CORRIDOR

means the lands within approximately 200 to 300 metres of the centre line of roads identified as having the potential for higher density mixed use development consistent with planned transit service levels.

## LANDFILL

means the disposal of *waste* by deposit, under controlled conditions, on land or on land covered by water, and includes compaction of the *waste* into a cell and covering the *waste* with materials at regular intervals.

## Leq

means the equivalent sound level of a fluctuating sound expressed in the same terms as the level of a steady sound carrying the same total energy within the same time interval.

## MAJOR POWER GENERATING FACILITY

means a building or structure used for the generation of electrical power, where output is ten megawatts or greater and where the method of production is limited to: **renewable energy**; natural gas fired; and **cogeneration**. Natural gas fired means the generation of electrical power through the combustion of natural gas and may include single cycle, combined cycle and **cogeneration** technologies.

## MAJOR RETAIL

means one or more retail establishments that provide goods or services for sale to the public that are not accessory to or associated with employment uses and have a combined gross floor area of greater than 600 m<sup>2</sup>.

## MAJOR TRANSIT STATION AREA

means the area including and around any existing or planned **higher order transit** station. Station areas generally are defined as the area within an approximate 500 m radius of a transit station, measured from the station building, representing about a 10 minute walk.

## MINOR POWER GENERATING FACILITY

means a building or structure used for the generation of electrical power, where output is less than ten megawatts and where the method of production is limited to: **renewable energy**; the combustion of natural gas, oil and propane; and **cogeneration**.

## NATURAL FEATURES, AREAS AND LINKAGES, INCLUDING THEIR ECOLOGICAL FUNCTIONS

refers to ecological features, processes, and systems within Natural Areas and as connections between Natural Areas.

## NATURAL HAZARD LANDS

means property or lands that could be unsafe for development due to naturally occurring processes. Along the shoreline of Lake Ontario, this means the land between a defined offshore distance or depth and the furthest landward limit of the flooding hazard, erosion hazard or dynamic beach hazard limits. Along river and stream systems, this means the land, including that covered by water, to the furthest landward limit of the flooding hazard or erosion hazard limits.

## NET RESIDENTIAL HECTARE

means:

- for detached, semi-detached, duplex, townhouse, and other dwelling types with individual frontages, a **net residential hectare** includes the land for residential lots and

- common element roads, but excludes public and other forms of private roadways; and
- for condominium units and apartment blocks, a **net residential hectare** includes the land for residential units, private internal roads and parking, landscaped areas, private open space, and other associated amenities

## NOISE EXPOSURE FORECAST (NEF)

means the value at a ground position providing an estimate of the integrated noise exposure produced by all types of aircraft at an airport, based on the actual or projected number and type of aircraft as well as the yearly runway utilization. The NEF system takes into consideration the number of flights, the duration of noise, the time of day, the frequency components of the noise and the noise potential of different types of aircraft operating under specific conditions. NEF values increase or decrease in a logarithmic manner, and the resultant scale is aimed at approximating the human response to a complex noise exposure situation.

## NOISE EXPOSURE PROJECTION (NEP)

similar to the NEF with the exception that it provides authorities with long range guidance in land use planning based on a projection of aircraft traffic levels, aircraft types, and runway configurations over a specific future time period.

## NOISE EXPOSURE PROJECTION (NEP)/NOISE EXPOSURE FORECAST (NEF) COMPOSITE NOISE CONTOUR

means a line linking specific locations predicted to be subject to the same noise exposure value based on the most stringent of an airport's NEF and NEP.

## ONE-ZONE CONCEPT

means the approach whereby the entire floodplain, as defined by the regulatory flood, is treated as one unit, and all development is prohibited or restricted.

## OTHER WETLAND

means any wetland that is not a provincially significant wetland.

## RENEWABLE ENERGY

means the generation of electrical power from an energy source that is renewed by natural processes including, but not limited to, wind, water, biomass, biogas, biofuel, solar energy or geothermal energy.

## RUNOFF CONTROL

means the regulation of the rate of flow of surface runoff.

## SCENIC ROUTES

means routes designed to preserve existing woodlands and greenbelts along roadways. **Scenic routes** are also designated to maintain or restore historic scenic nature of roadways.

## SECONDARY OFFICE

means business, professional, and administrative offices, less than 10,000 m<sup>2</sup> or accommodating less than 500 jobs.

## SPECIAL CONCERN

means a wildlife species that may become a threatened or endangered species because of a combination of biological characteristics and identified threats.

## SPECIES AT RISK

means any plant or animal threatened by, or vulnerable to, extinction and listed in regulations under the *Endangered Species Act*.

## STREETSCAPE

means the character of the street, including the street right-of-way, adjacent properties between the street right-of-way and building faces. Thus, the creation of a **streetscape** is achieved by the development of both public and private lands and may include planting, furniture, paving, etc.

## STORMWATER BEST MANAGEMENT PRACTICES

means a set of practices which includes techniques, measures, structural and non-structural controls that are used to manage the volume, discharge rate and quality of stormwater runoff, promote groundwater infiltration and reduce the release of pollutants into waterbodies and in-stream erosion. **Stormwater best management practices** may include low impact development techniques to replicate the natural hydrologic cycle through infiltration, evapotranspiration, reuse and storage such as innovative site design and landscaping to minimize imperviousness, permeable paving, greenroofs, rainwater harvesting and bioretention. **Stormwater**

**best management practices** may also include roadway bioretention and stormwater management ponds.

## SUB-WATERSHED

means the watershed of a tributary stream, sub-unit of a major watershed.

## SURFACE DRAINAGE FACILITIES

means any facility or facilities associated with drainage or control of storm drainage that is ultimately directed to a Street or Storm Drainage System, and includes, but is not limited to:

- a grass swale;
- a concrete or asphalt walkway, gutter or swale;
- a drainage control fence or structure; or
- the sloping and contouring of land to facilitate or control storm drainage.

## TALL BUILDING

means a building having a height greater than the width of the street on which they front. **Tall buildings** are defining elements in the city structure; becoming icons and landmarks in the skyline and **streetscape**. They have a greater opportunity and responsibility to contribute towards defining an area's identity and success. Further, when appropriately sited and designed, **tall buildings** can accommodate transit supportive densities and facilitate the viability of a successful, well used public transit system.

## TOP-OF-SLOPE

means the point of the slope where the downward inclination of the land begins, or the upward inclination of the land levels off. This point is situated at a higher topographic elevation of land than the remainder of the slope. There may be situations where there are interruptions in the valley slope by plateau (terrace) areas.

## TRANSIT ORIENTED DEVELOPMENT

refers to moderate to high density development, located within an easy walk of a major transit station. It is comprised of a mix of uses and is designed with pedestrians in mind.

## TWO-ZONE FLOODPLAIN MANAGEMENT CONCEPT

means the approach whereby certain areas of the floodplain are considered to be less hazardous than others such that development potentially could safely occur. The flood fringe is the outer portion of the floodplain where development may be permitted, subject to appropriate flood proofing. The floodway is the inner portion of the floodplain, wherein development is prohibited or restricted representing that area required for safe passage of the flood flow and/or that area where flood depths and/or velocities are considered to be such that they pose a potential threat to life or property damage.

## UNIVERSAL DESIGN PRINCIPLES

means the principles by which the environment can be designed in order to accommodate the abilities of all. For detailed information regarding the principles of universal design, the Mississauga Accessibility Design Handbook should be consulted.

## VALLEY AND WATERCOURSE CORRIDORS

means the natural resources associated with the river systems characterized by their landform, features, and functions. Valley corridors are distinguished from stream corridors by the presence of a distinct land form. The exact limit of **valley and watercourse corridors** will be determined, jointly with and on a site specific basis by the appropriate Conservation Authority.

## WASTE

includes ashes, garbage, refuse, domestic **waste**, industrial **waste** or municipal refuse, and such other **wastes** as may be designated under the *Environmental Assessment Act*.

## WASTE DISPOSAL SITE

means any land or land covered by water upon, into or through which, or building or structure in which, **waste** is deposited or processed and any machinery or equipment or operation required for the treatment or disposal of **waste**.

## WASTE PROCESSING STATION

means a facility that receives, stores and/or processes **waste** materials for the purpose of creating new products or materials.

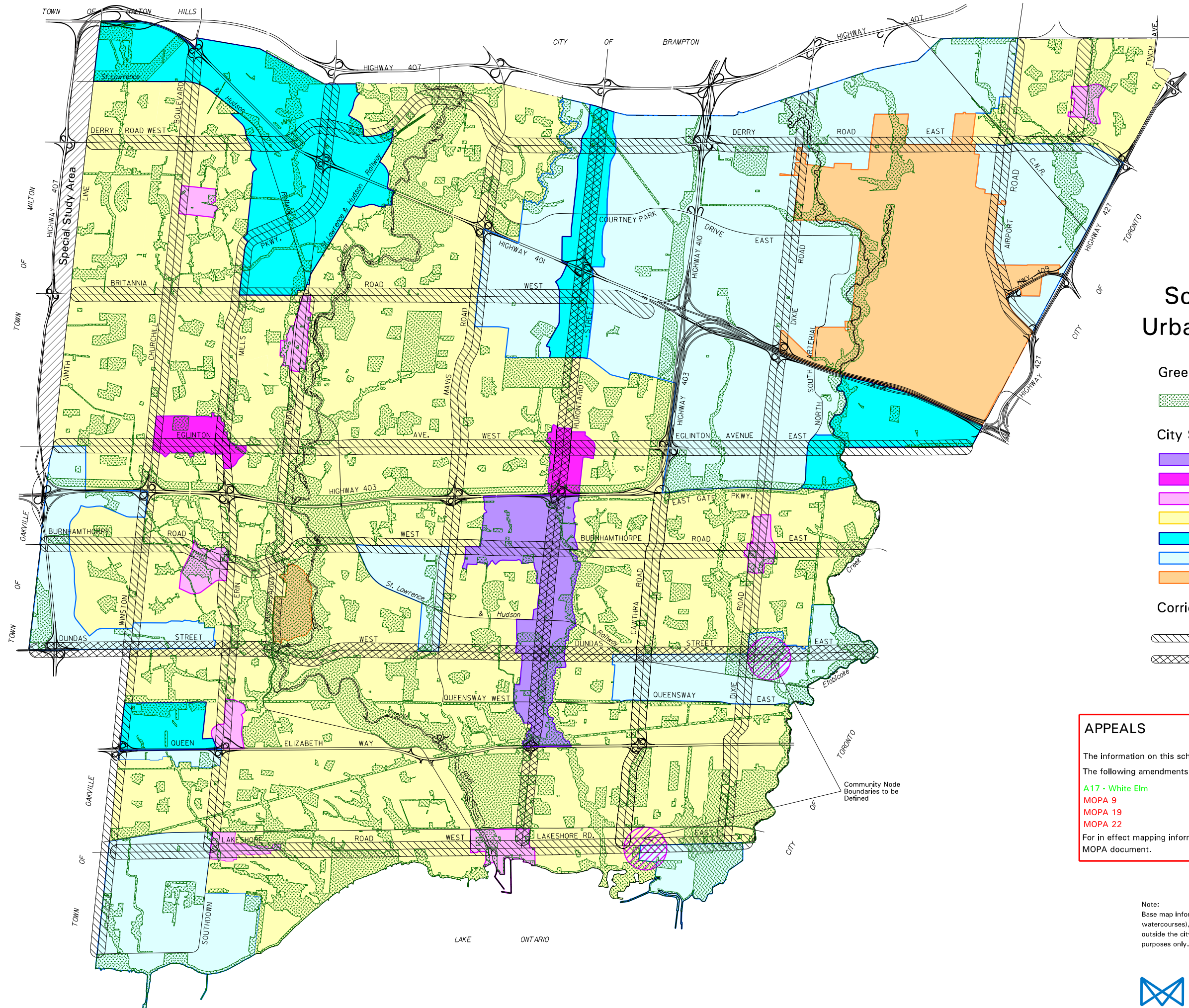


## WASTE TRANSFER STATION

means a facility where **waste** materials are collected for shipment and may be sorted and/or prepared for transportation.

## WATERCOURSE

means an identifiable depression in the ground in which water flows regularly or continuously.



# Schedule 1 Urban System

- Green System**
- Green System
- City Structure**
- Downtown
  - Major Node
  - Community Node
  - Neighbourhood
  - Corporate Centre
  - Employment Area
  - Special Purpose Area
- Corridors**
- Corridor
  - Intensification Corridor

## APPEALS

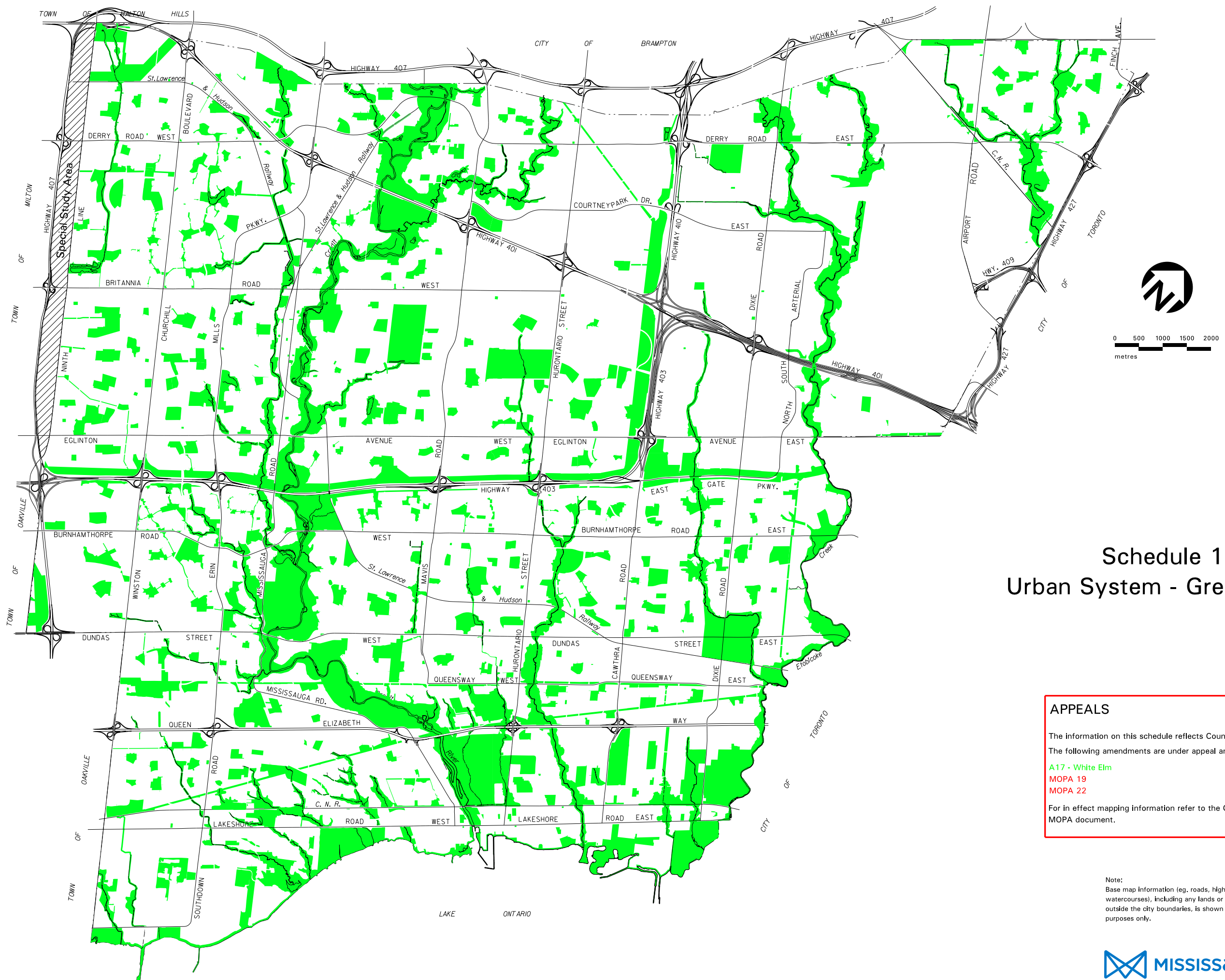
The information on this schedule reflects Council adopted amendments.  
The following amendments are under appeal and affect this schedule:

- A17 - White Elm
- MOPA 9
- MOPA 19
- MOPA 22

For in effect mapping information refer to the Consolidation Tables and MOPA document.

**Note:**  
Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.





# Schedule 1a Urban System - Green System

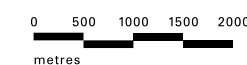
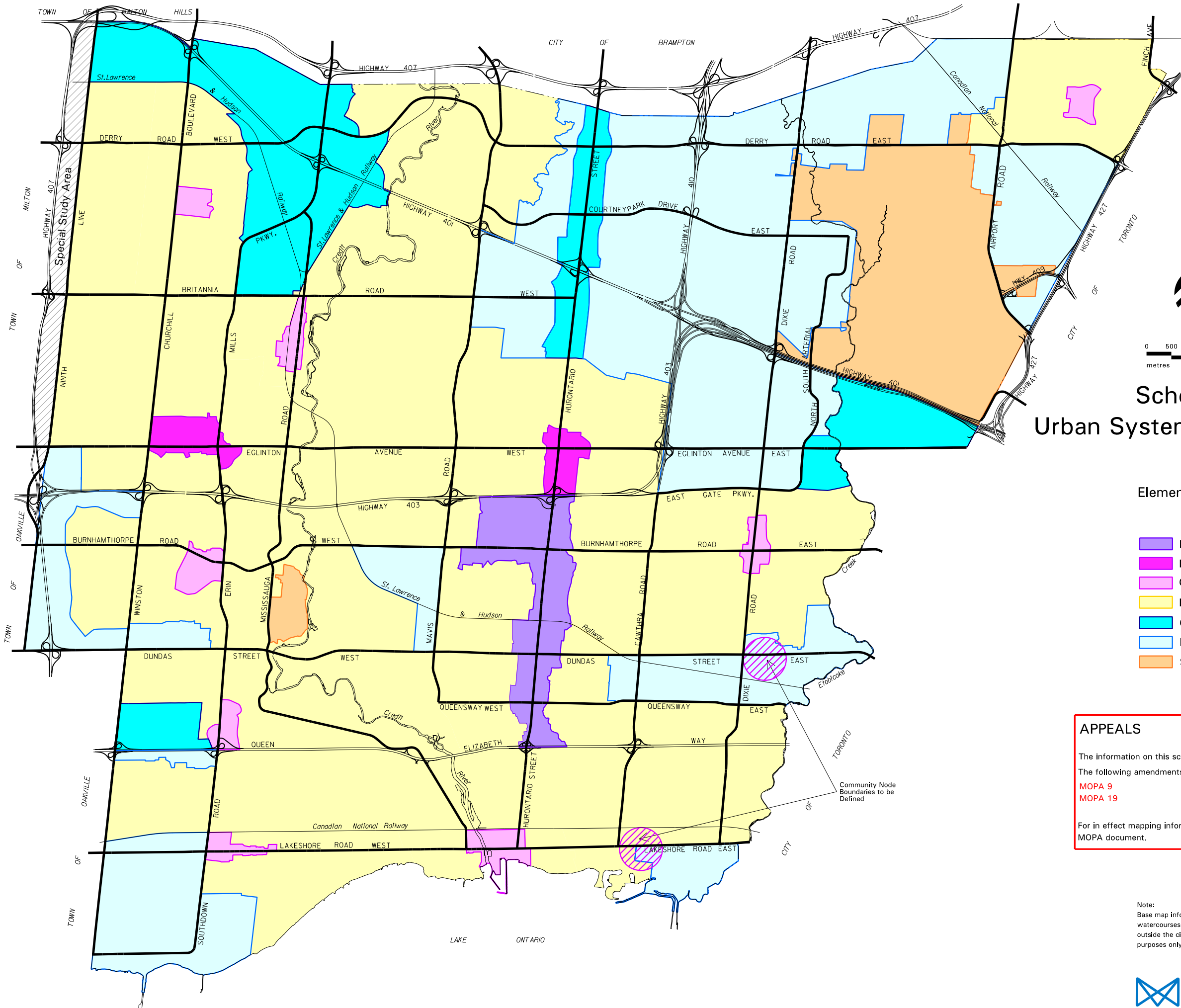
## APPEALS

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- A17 - White Elm
- MOPA 19
- MOPA 22

For in effect mapping information refer to the Consolidation Tables and MOPA document.

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# Schedule 1b

## Urban System - City Structure

### Elements

- Downtown
- Major Node
- Community Node
- Neighbourhood
- Corporate Centre
- Employment Area
- Special Purpose Area

### APPEALS

The information on this schedule reflects Council adopted amendments.

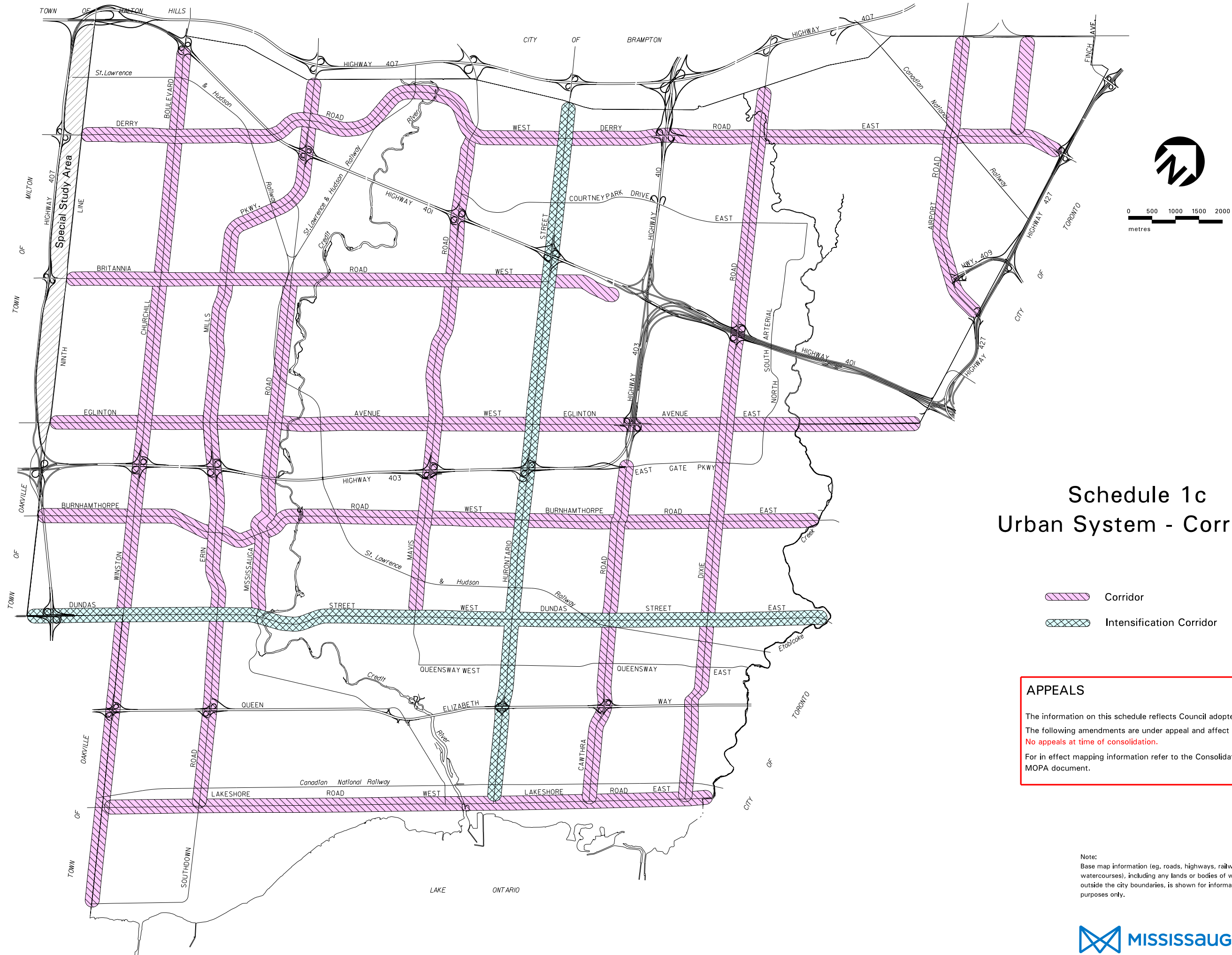
The following amendments are under appeal and affect this schedule:

**MOPA 9**

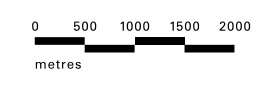
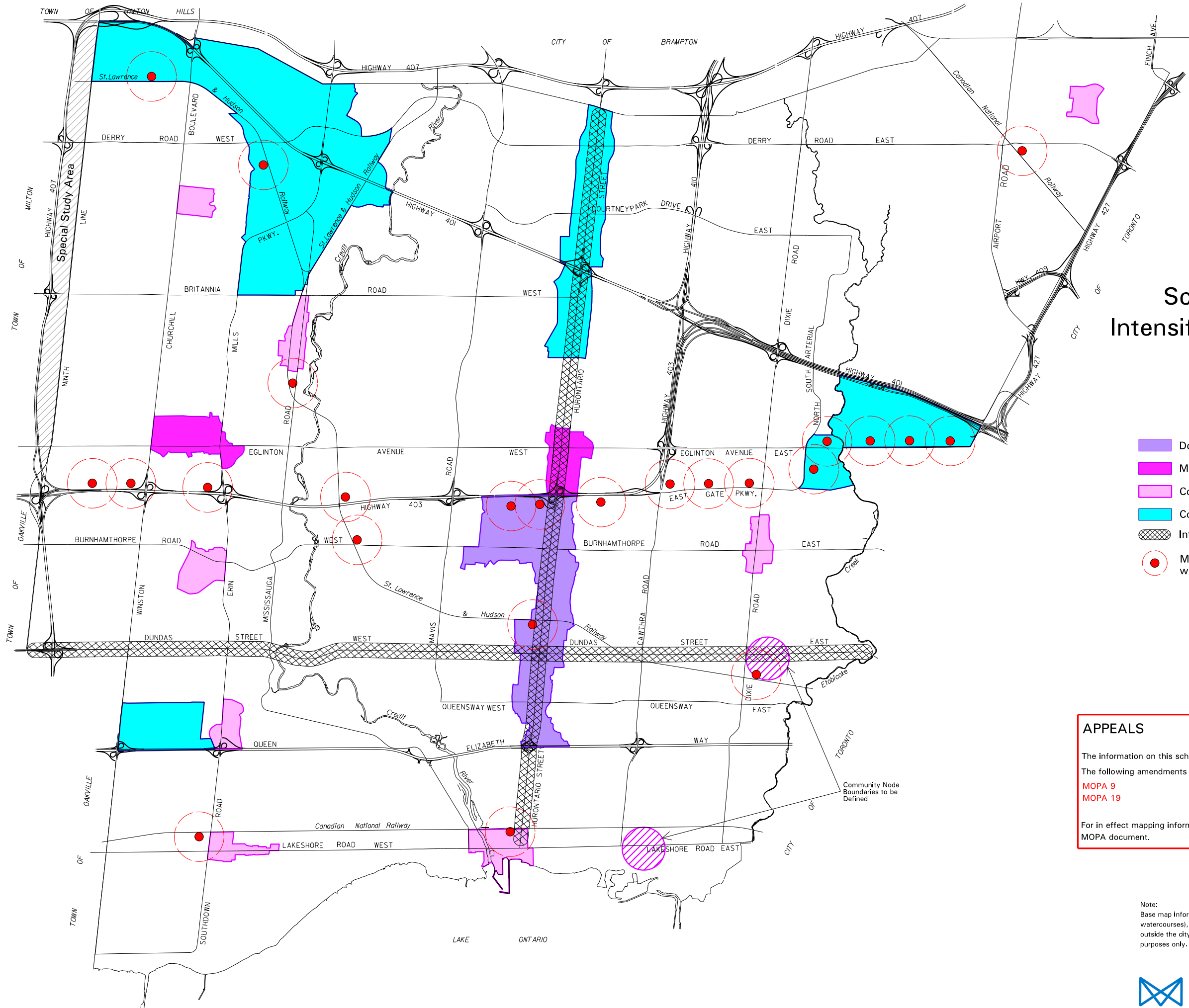
**MOPA 19**

For in effect mapping information refer to the Consolidation Tables and MOPA document.

**Note:**  
Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.







## Schedule 2 Intensification Areas

- Downtown
- Major Node
- Community Node
- Corporate Centre
- Intensification Corridor
- Major Transit Station Area with 500m radius circle

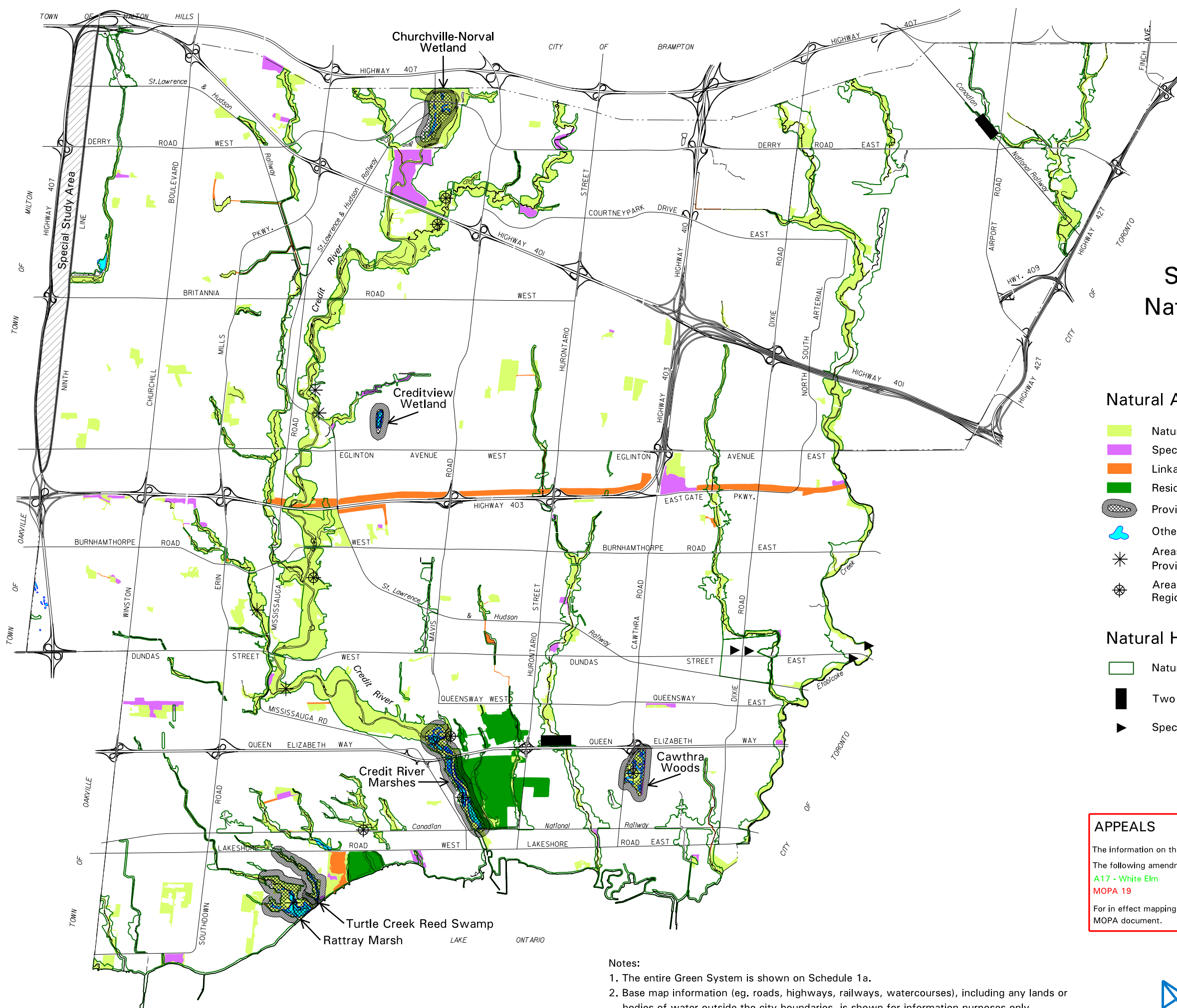
**APPEALS**

The information on this schedule reflects Council adopted amendments.  
The following amendments are under appeal and affect this schedule:

MOPA 9  
MOPA 19

For in effect mapping information refer to the Consolidation Tables and MOPA document.

Note:  
Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.



0 500 1000 1500 2000  
metres

## Schedule 3 Natural System

### Natural Areas System:

- Natural Areas
- Special Management Areas
- Linkages
- Residential Woodlands
- Provincially Significant Wetlands
- Other Wetlands
- Areas of Natural and Scientific Interest - Provincial Significance
- Areas of Natural and Scientific Interest - Regional Significance

### Natural Hazards:

- Natural Hazards
- Two Zone Floodplain Regulations
- Special Policy Area Floodplain

### APPEALS

The information on this schedule reflects Council adopted amendments.

The following amendments are under appeal and affect this schedule:

A17 - White Elm

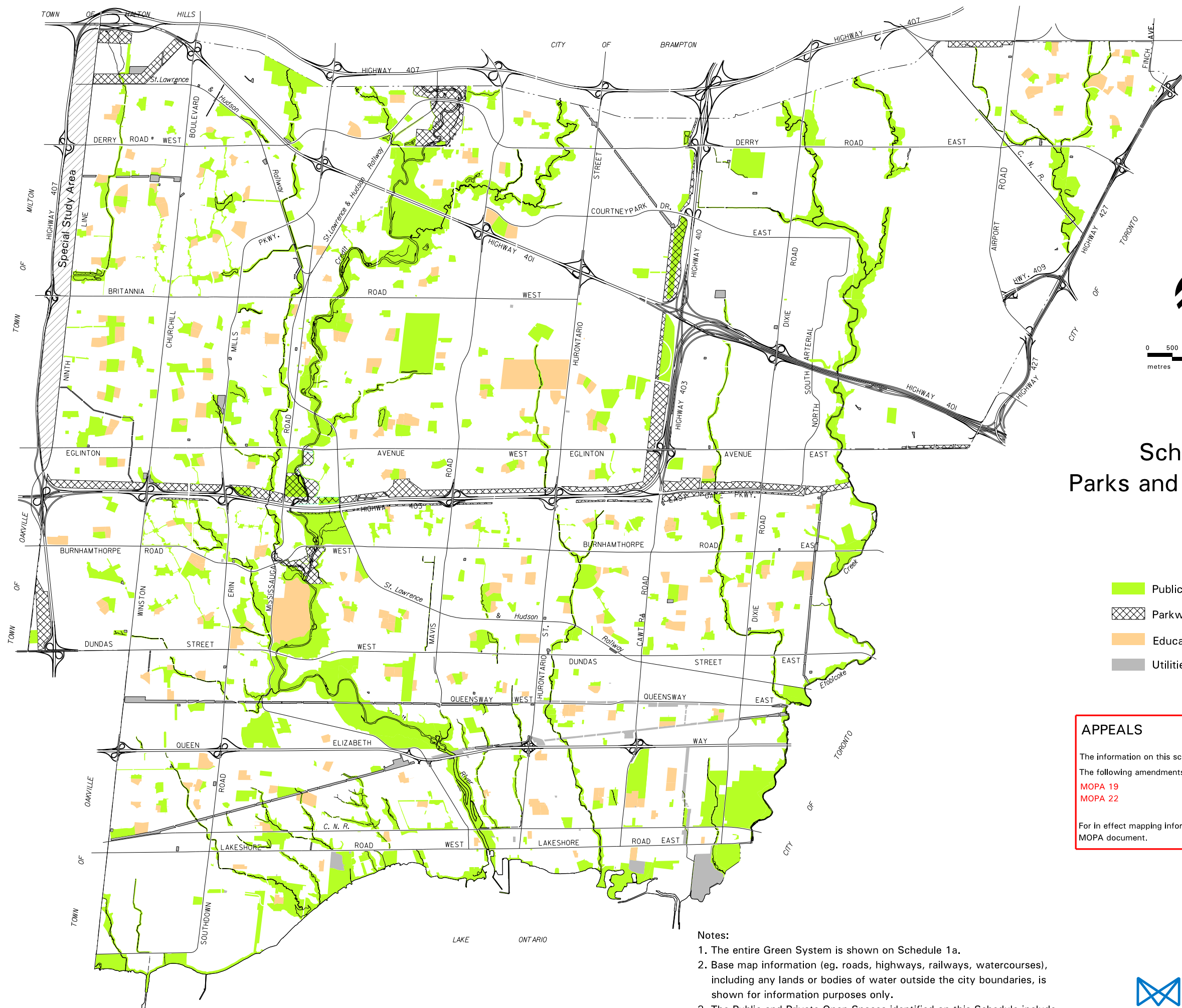
MOPA 19

For in effect mapping information refer to the Consolidation Tables and MOPA document.

### Notes:

1. The entire Green System is shown on Schedule 1a.
2. Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.
3. The limits of the Natural Hazards shown on this Schedule are for illustrative purposes only. The appropriate Conservation Authority should be consulted to determine their actual location.





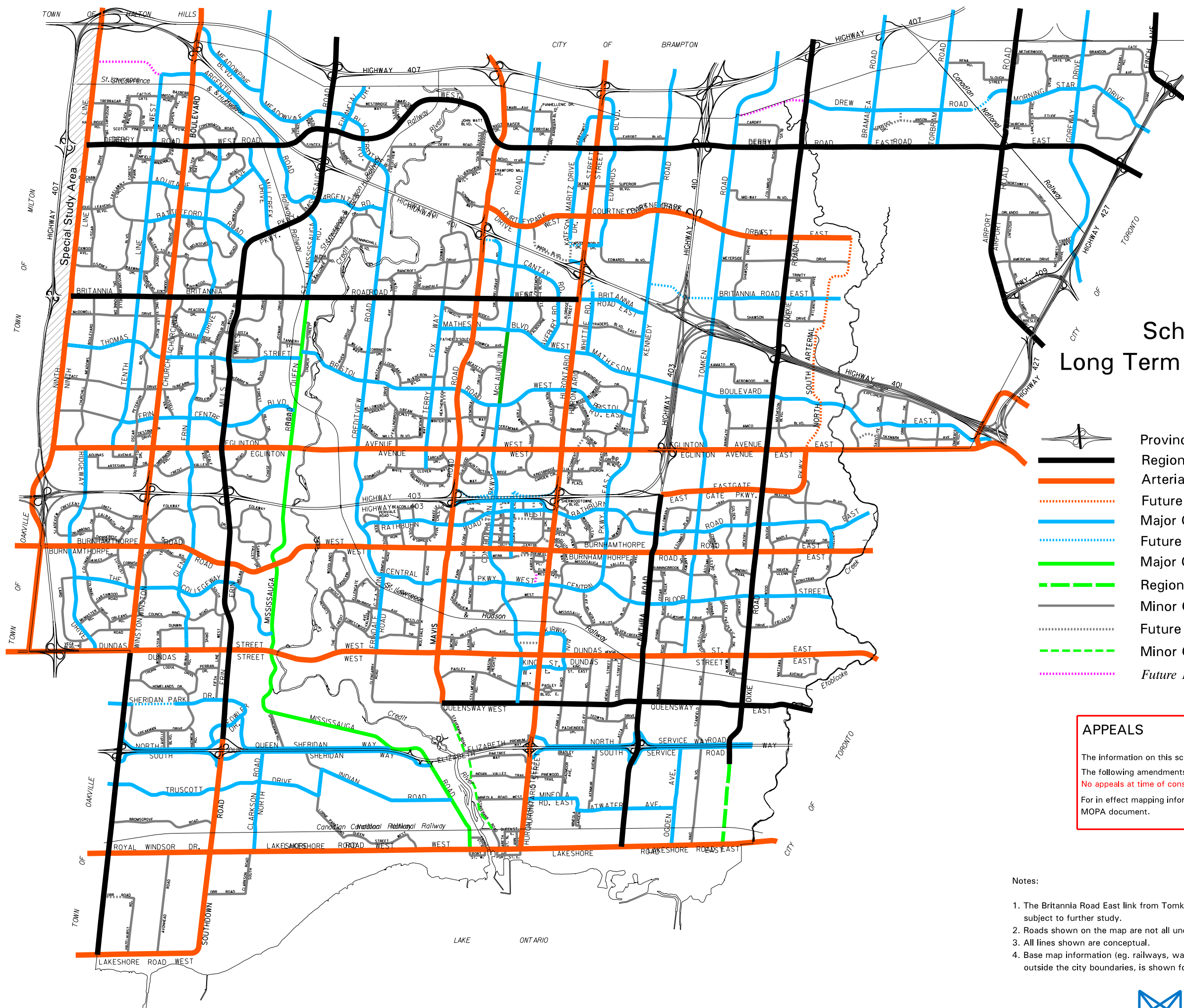
# Schedule 4 Parks and Open Spaces

- Public and Private Open Spaces
- Parkway Belt West
- Educational Facilities
- Utilities













## APPEALS

The information on this schedule reflects Council adopted amendments.  
The following amendments are under appeal and affect this schedule:  
MOPA 19  
MOPA 22  
For in effect mapping information refer to the Consolidation Tables and MOPA document.

- Notes:
- The entire Green System is shown on Schedule 1a.
  - Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.
  - The Public and Private Open Spaces identified on this Schedule include lands designated Public Open Space, Private Open Space and Greenbelt as shown on Schedule 10.



## Schedule 5 Long Term Road Network

-  Provincial Highway and Interchange
-  Regional Arterial
-  Arterial
-  Future Arterial (conceptual)
-  Major Collector
-  Future Major Collector (conceptual)
-  Major Collector (Scenic Route)
-  Regional Major Collector (Scenic Route)
-  Minor Collector
-  Future Minor Collector
-  Minor Collector (Scenic Route)
-  Future Road Link to be added.

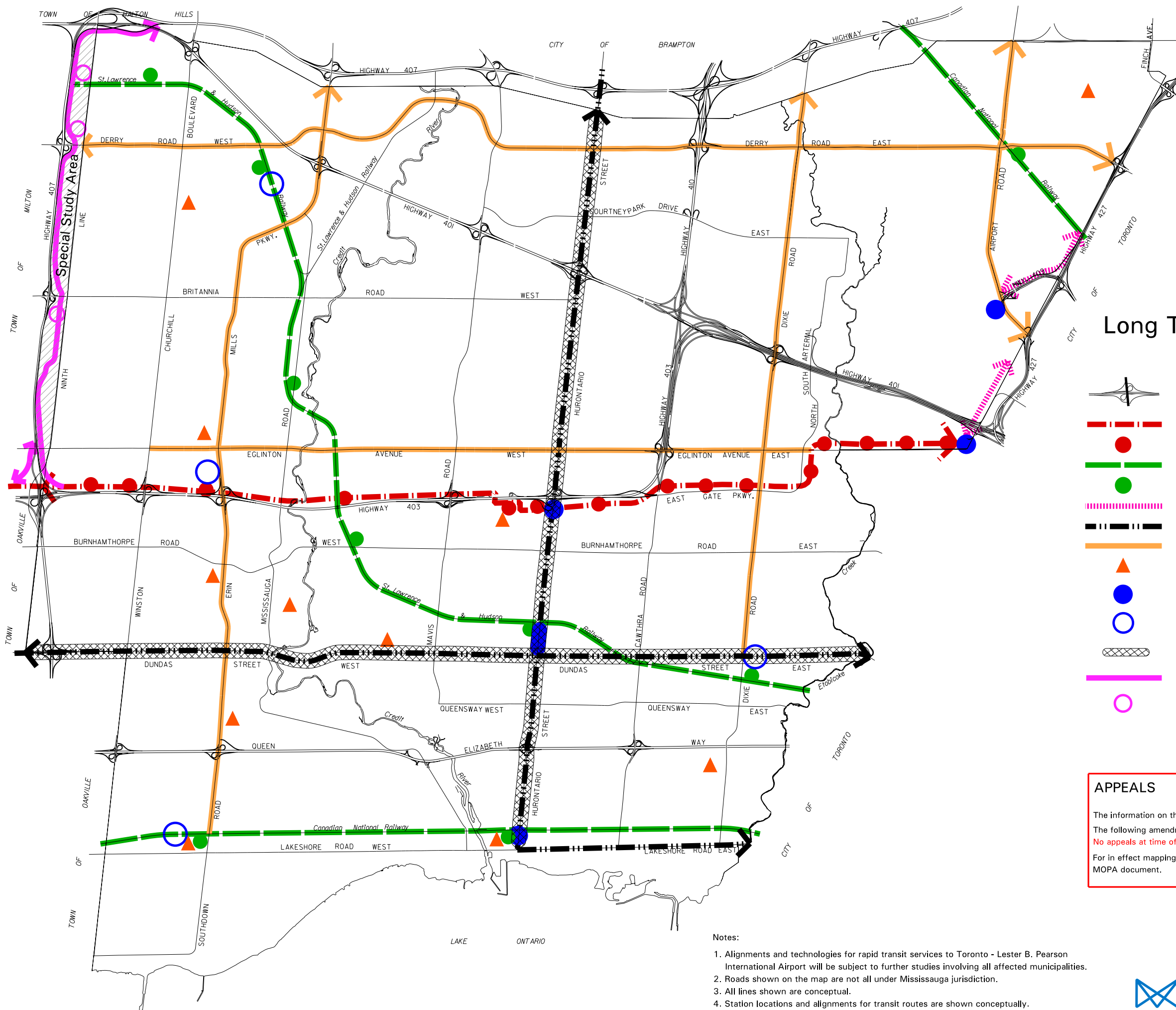
### APPEALS

The information on this schedule reflects Council adopted amendments.  
The following amendments are under appeal and affect this schedule:  
**No appeals at time of consolidation.**  
For in effect mapping information refer to the Consolidation Tables and MOPA document.

### Notes:

- The Britannia Road East link from Tomken Road to Kennedy Road is conceptual and is subject to further study.
- Roads shown on the map are not all under Mississauga jurisdiction.
- All lines shown are conceptual.
- Base map information (eg. railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.





0 500 1000 1500 2000  
metres

## Schedule 6 Long Term Transit Network



Provincial Highway and Interchange



Bus Rapid Transit Corridor



Bus Rapid Transit Station



Existing Commuter Rail



Existing Commuter Rail Station



Transit Airport Connection



Higher Order Transit Corridor



Transit Priority Corridor



Existing Mississauga Transit Terminal



Mobility Hub



Potential Mobility Hub



Intensification Corridor



Potential 407 Transitway



Potential 407 Transitway Station

### APPEALS

The information on this schedule reflects Council adopted amendments.

The following amendments are under appeal and affect this schedule:

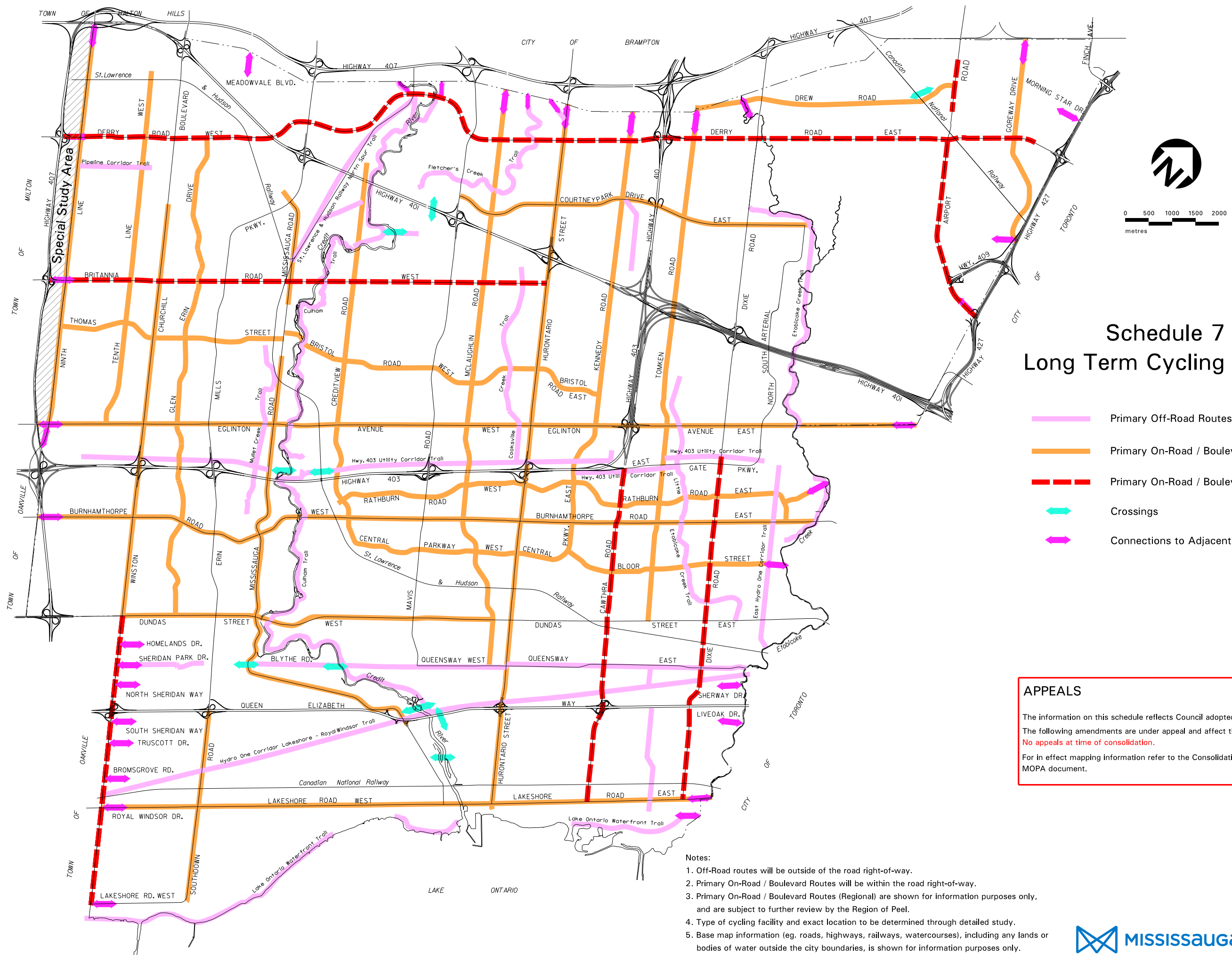
**No appeals at time of consolidation.**

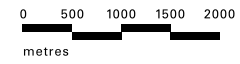
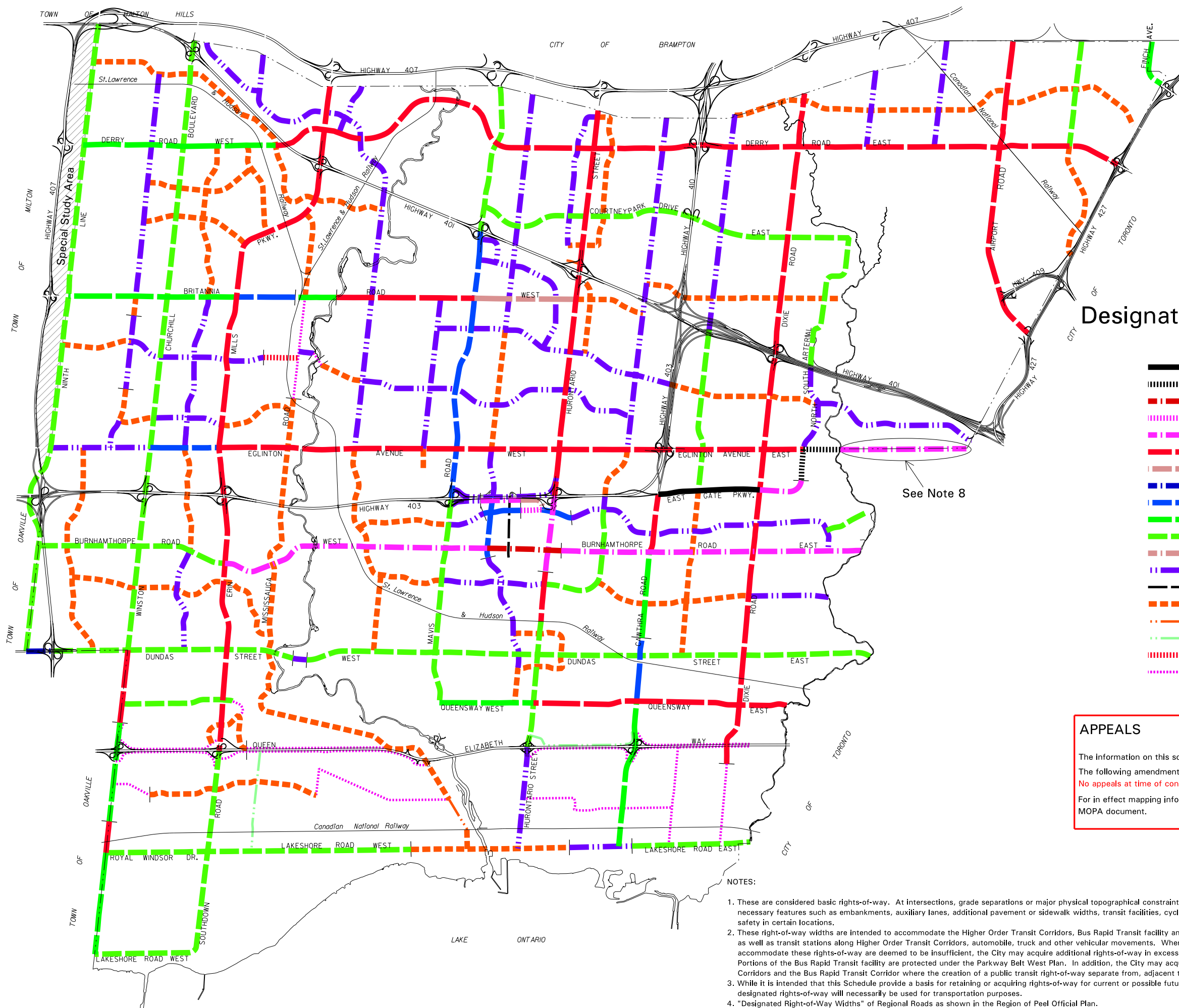
For in effect mapping information refer to the Consolidation Tables and MOPA document.

### Notes:

- Alignments and technologies for rapid transit services to Toronto - Lester B. Pearson International Airport will be subject to further studies involving all affected municipalities.
- Roads shown on the map are not all under Mississauga jurisdiction.
- All lines shown are conceptual.
- Station locations and alignments for transit routes are shown conceptually.
- Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.







## Schedule 8 Designated Right-of-Way Widths

	67 m
	65 m
	60 m
	55 m
	50 m
	45 m
	43.5 m
	42 m
	40 m
	36 m
	35 m
	30 m-50 m
	30 m
	27.5 m
	26 m
	23 m-26 m
	22 m
	20 m-26 m
	20 m

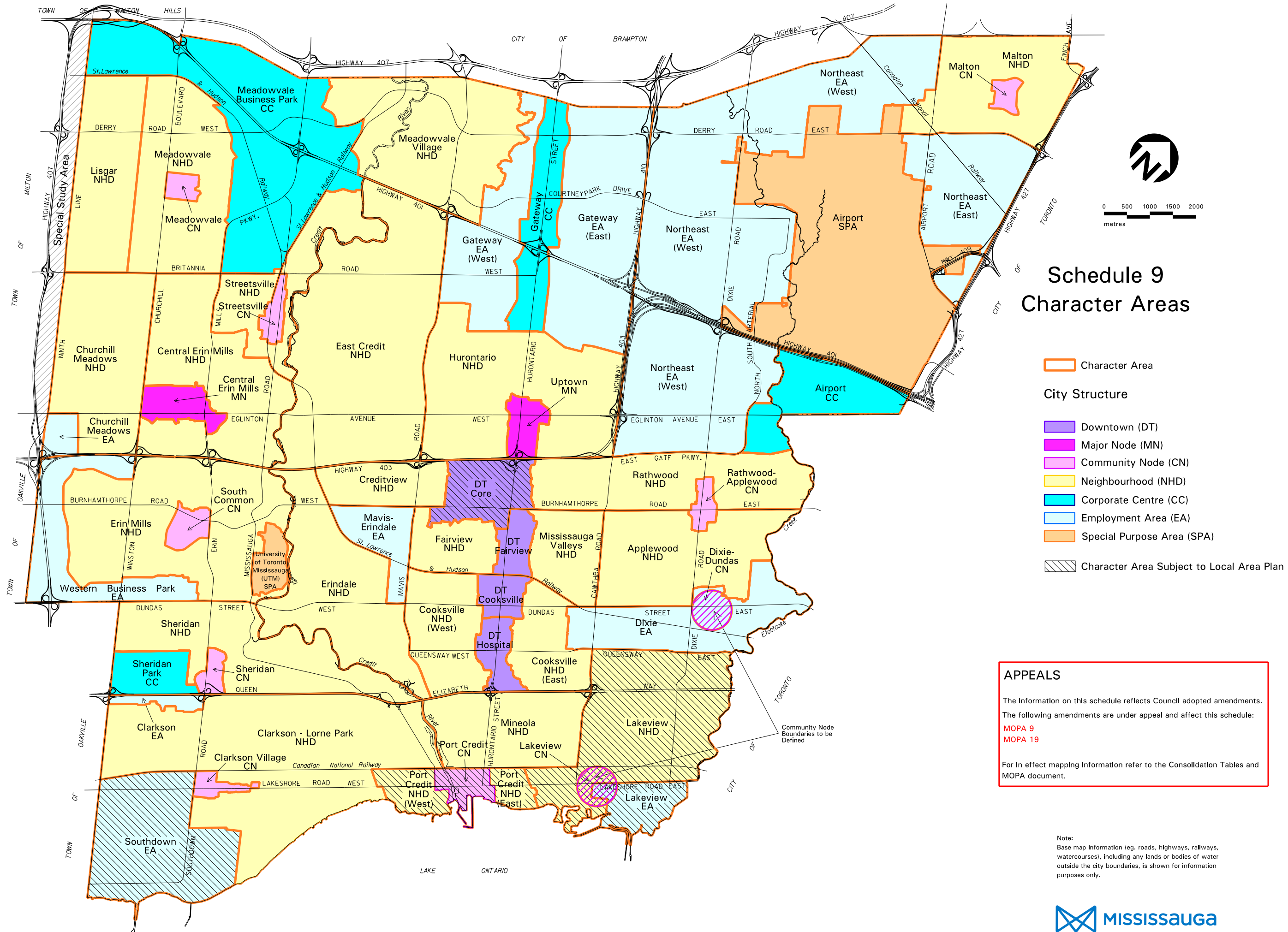
See Note 8

### APPEALS

The information on this schedule reflects Council adopted amendments.  
The following amendments are under appeal and affect this schedule:  
**No appeals at time of consolidation.**  
For in effect mapping information refer to the Consolidation Tables and MOPA document.

### NOTES:

1. These are considered basic rights-of-way. At intersections, grade separations or major physical topographical constraints, wider rights-of-way may be required to accommodate necessary features such as embankments, auxiliary lanes, additional pavement or sidewalk widths, transit facilities, cycling facilities, or to provide for necessary improvements for safety in certain locations.
2. These right-of-way widths are intended to accommodate the Higher Order Transit Corridors, Bus Rapid Transit facility and Bus Rapid Transit stations shown schematically on Schedule 6, as well as transit stations along Higher Order Transit Corridors, automobile, truck and other vehicular movements. Where as a result of further studies the land areas required to accommodate these rights-of-way are deemed to be insufficient, the City may acquire additional rights-of-way in excess of the basic right-of-way widths shown on Schedule 8. Portions of the Bus Rapid Transit facility are protected under the Parkway Belt West Plan. In addition, the City may acquire lands for a public transit right-of-way along Higher Order Transit Corridors and the Bus Rapid Transit Corridor where the creation of a public transit right-of-way separate from, adjacent to, or in addition to a road right-of-way is deemed appropriate.
3. While it is intended that this Schedule provide a basis for retaining or acquiring rights-of-way for current or possible future transportation purposes, it is not intended to imply that all designated rights-of-way will necessarily be used for transportation purposes.
4. "Designated Right-of-Way Widths" of Regional Roads as shown in the Region of Peel Official Plan.
5. "Designated Right-of-Way Widths" shown on the map are not all under Mississauga jurisdiction.
6. Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.
7. The rights-of-way for Minor Collector Roads are identified in Table 8-3.
8. The Eglinton Avenue right-of-way east of Etobicoke Creek consists of a 36m road right-of-way (Toronto) and a 14m right-of-way for the Bus Rapid Transit.
9. All lines shown are conceptual.



**APPEALS**

The information on this schedule reflects Council adopted amendments.

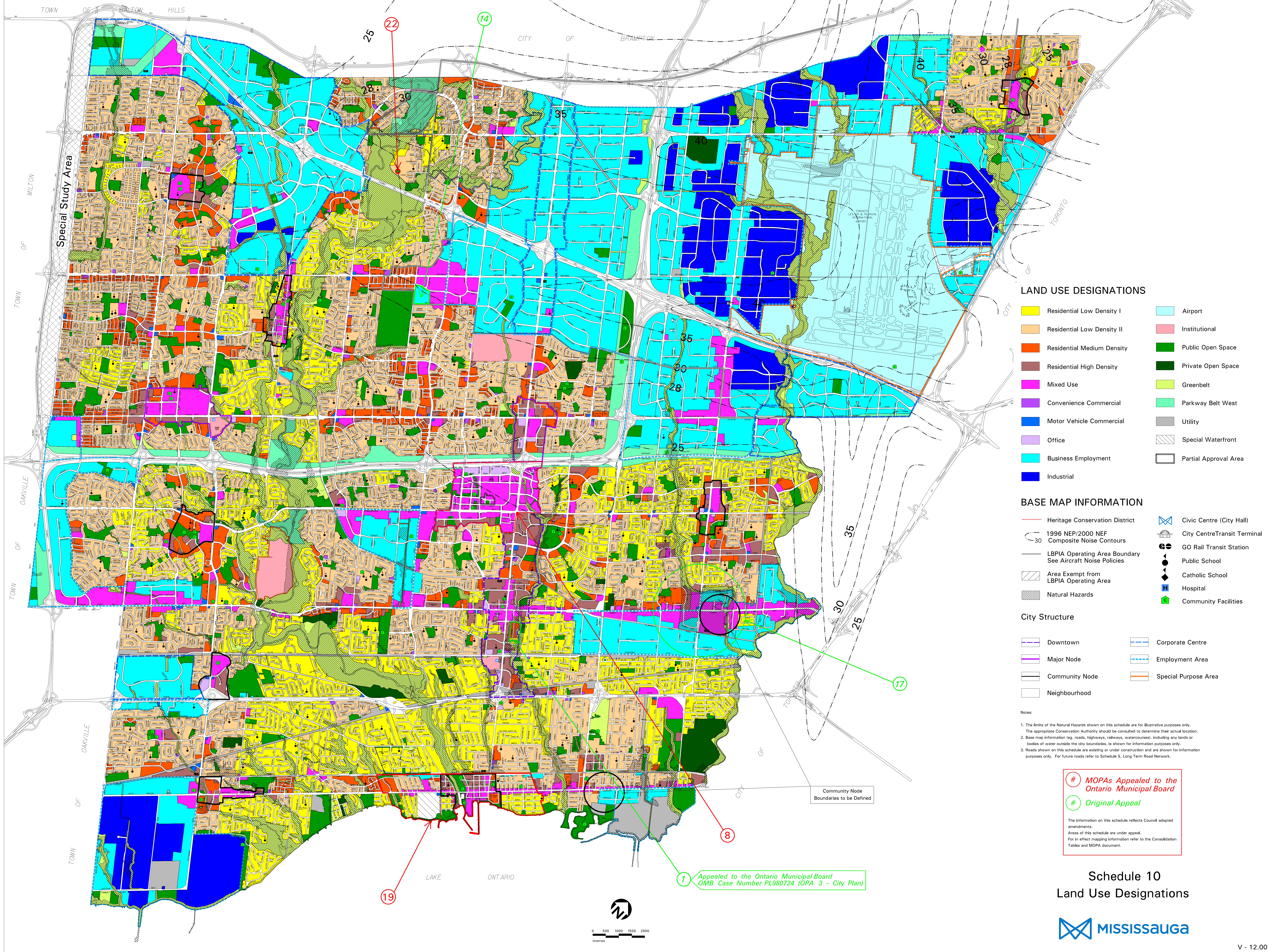
The following amendments are under appeal and affect this schedule:

**MOPA 9**  
**MOPA 19**

For in effect mapping information refer to the Consolidation Tables and MOPA document.

Note:  
Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.





LAND USE DESIGNATIONS

- |                            |                       |
|----------------------------|-----------------------|
| Residential Low Density I  | Airport               |
| Residential Low Density II | Institutional         |
| Residential Medium Density | Public Open Space     |
| Residential High Density   | Private Open Space    |
| Mixed Use                  | Greenbelt             |
| Convenience Commercial     | Parkway Belt West     |
| Motor Vehicle Commercial   | Utility               |
| Office                     | Special Waterfront    |
| Business Employment        | Partial Approval Area |
| Industrial                 |                       |

BASE MAP INFORMATION

- |   |                              |
|---|------------------------------|
| Heritage Conservation District                            | Civic Centre (City Hall)     |
| 1996 NEP/2000 NEF Composite Noise Contours                | City Centre Transit Terminal |
| LBPIA Operating Area Boundary See Aircraft Noise Policies | GO Rail Transit Station      |
| Area Exempt from LBPIA Operating Area                     | Public School                |
| Natural Hazards   | Catholic School              |
|   | Hospital                     |
|   | Community Facilities         |

City Structure

- |                |                      |
|----------------|----------------------|
| Downtown       | Corporate Centre     |
| Major Node     | Employment Area      |
| Community Node | Special Purpose Area |
| Neighbourhood  |                      |

- Notes:
- The limits of the Natural Hazards shown on this schedule are for illustrative purposes only. The appropriate Conservation Authority should be consulted to determine their actual location.
  - Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.
  - Roads shown on this schedule are existing or under construction and are shown for information purposes only. For future roads refer to Schedule 5, Long Term Road Network.

**MOPAs Appealed to the Ontario Municipal Board**

**Original Appeal**

The information on this schedule reflects Council adopted amendments.  
Areas of this schedule are under appeal.  
For in effect mapping information refer to the Consolidation Tables and MOPA document.

Schedule 10  
Land Use Designations



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## Tables and Maps

### Downtown Core Local Area Plan Land Use Map



# DOWNTOWN CORE

## 1.0 How to Read the Downtown Core Local Area Plan

Mississauga Official Plan is composed of a principal document and a series of local area plans, provided under separate cover. This is the Downtown Core Local Area Plan and provides policies for lands located in central Mississauga, as shown on Figure 1. It includes lands identified in the City Structure as the Downtown Core.

There are some instances where the policies and schedules of the principal document do not address all circumstances particular to the Downtown Core. In these cases, the Downtown Core Local Area Plan elaborates on, or provides exceptions to the policies or schedules of the principal document.

The Downtown Core Local Area Plan must be read in conjunction with the Mississauga Official Plan principal document. Parts 1 to 4, the schedules and the appendices of the principal document, are

applicable to the Downtown Core area, unless modified by this local area plan. Thus, for example, the policies of this local area plan must be read in conjunction with the environmental, multi-modal, urban form and land use policies of Parts 2 and 3 of the principal document. In the event of a conflict, the policies of this local area plan take precedence.

The Downtown Core Local Area Plan incorporates the policies of the City Centre District Policies of Mississauga Plan (2003), modified to conform with Mississauga Official Plan (2010).

The City will further refine these policies through the Downtown21 Master Plan to require a truly pedestrian-friendly and transit supportive urban environment. The Master Plan will, among other matters, identify opportunities and strategies for economic development, multi-modal transportation, placemaking and environmental sustainability. Further, it will review the Downtown Core boundaries, establish priorities for development and infrastructure investment, and propose policies to

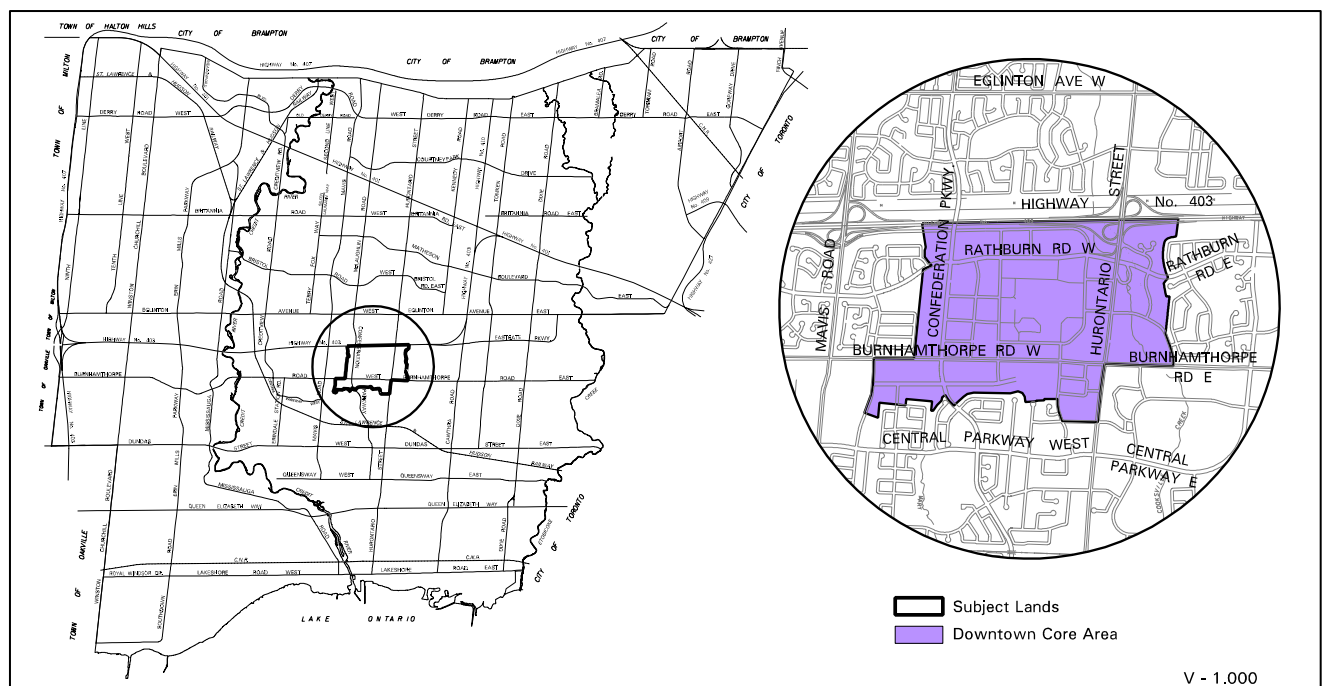


Figure 1: The Downtown Core Local Area is located in central Mississauga and is identified in the City Structure as Downtown.

direct development to achieve the Downtown21 vision. The Master Plan will provide the basis for a new local area plan and amendments to Mississauga Official Plan.

## 2.0 Introduction

The focus of these policies is to promote a high quality of urban design and an urban built form by integrating the public realm and private development. These policies are also intended to reinforce and enhance the image of the Downtown Core as a destination and a major regional centre. Mixed use development is encouraged in the Downtown Core so that its position as an employment, retail commercial and civic centre will be balanced by residential development, entertainment, and facilities for business visitors and tourists. These policies encourage and facilitate the dynamic growth and change of the Downtown Core as it matures.

### 2.1 Development Objectives

- a. Encourage a high quality of urban design in the built form which is unique and pedestrian-friendly, and which contributes to the unique identity and prominence of the Downtown Core.
- b. The location of streets, high standard of urban and **streetscape** design, and the development and enhancement of a comprehensible public/private system of pathways and open space, including public art will:
  - integrate retail commercial, office, civic and other facilities;
  - facilitate pleasant and safe pedestrian and cyclist movement throughout the Downtown Core;
  - provide opportunities for passive outdoor recreation within the Downtown Core;
  - provide access to and from the Cooksville Creek Greenbelt; and
  - reinforce and expand the role of the Downtown Core as the focus of citywide cultural, community and civic activities.
- c. Encourage street related development, with particular attention to pedestrian access and amenities.
- d. Encourage the development of a unique Downtown Core character, and enhance its image through the creation of view corridors and gateway features, and visibility of the Civic Centre and public gathering places.
- e. Complete the Downtown Core road system to improve vehicular, cyclist and pedestrian movement, and to create usable development parcels.
- f. Encourage the development of parking facilities that will be sensitive to pedestrian friendly and active **streetscapes** throughout the Character Area, and discourage the creation of structures with no pedestrian interest or protection at the sidewalk level.
- g. Encourage a mix of uses throughout the Downtown Core, within buildings and along individual streets.
- h. Encourage residential development throughout the Downtown Core. Require appropriate transition of built form adjacent to existing low density residential areas.
- i. Foster the continued development of a variety of entertainment activities within the Downtown Core which will contribute to activity both day and night.
- j. Encourage development and activities that enhance the Downtown Core as a destination.
- k. Encourage development which will expand tourism in the Downtown Core and the region.

- l. Maintain the prominence of the Downtown Core as a retail commercial centre.
- m. Facilitate vehicular and transit access to and from the Downtown Core to the Toronto Lester B. Pearson International Airport and interregional transit facilities, such as the Cooksville GO Transit station.
- n. Encourage pedestrian related development that is accessible to all, including those with disabilities.

## 3.0 Urban Design

### 3.1 General

- a. The intent of the urban design policies is to define principles for the management of the physical form and character of the Downtown Core. In this respect, urban design policy is intended to:
  - promote a distinctive image for the Downtown Core which is predominantly urban in character;
  - promote the highest standard of urban design in both the public and private realm;
  - promote a recognition of a sense of entry into a special area by encouraging gateway treatments such as taller, more prominent buildings located close to the street, distinctive landscape and **streetscape** treatment, entry signage, distinctive precincts and skyline elements;
  - encourage built form which has a high level of physical continuity, cohesion and linkage between buildings, from block to block, and from street to street;
  - reinforce and extend the public realm - streets, public open spaces, and boulevards - as a significant public resource of the highest design standard which encourages a sense of place, civic identity, and physical continuity in the built environment;
- promote urban scale land blocks, streets and built form appropriate to a pedestrian core;
- promote elements of linkage to the abutting communities while ensuring positive integration of building scale and vehicular traffic;
- promote a balance of vehicular traffic and pedestrian priorities with features and amenities critical to pedestrian comfort, convenience and safety for all, including those with disabilities;
- generally discourage above grade parking structures, blank building walls abutting the street, surface parking and loading areas between buildings and street edges, and extensive surface parking. Site specific circumstances may warrant alternative design solutions; and
- recognize utility corridors required for hard service components; and
- b. Urban form in the Downtown Core will develop incrementally. The submission of a concept plan will be required for all development applications and must demonstrate how the urban design policies will be implemented. Development applications will have regard for the Downtown Core Urban Design Guidelines.
- c. The design of the built environment should recognize that the Downtown Core will mature over time and flexible built form conditions should aim at the end product.

## 3.2 Urban Design Components

The urban form of the Downtown Core is created by both the public realm - streets, boulevards and public open spaces - and the private development abutting their edges. The integrated development of these components is critical to the development of the Downtown Core based on the following.

### 3.2.1 The Design of the Public Realm

#### a. Streets

In addition to providing routes for vehicular and pedestrian travel, streets are a significant public resource within the Downtown Core. The design of street space should:

- promote a high level of urban design in all components;
- promote pedestrian amenity, comfort, convenience and safety for all, including those with disabilities;
- promote minimal visual width and scale of the street;
- promote design and functional elements which reduce traffic speed;
- encourage on-street parking in public rights-of-way, where operationally feasible;
- incorporate coordinated street furniture and lighting systems;
- provide co-ordinated traffic signage which is designed in scale and character with the Downtown Core **streetscape** elements; and
- provide for a proposed north/south bicycle route along the public right-of-way of Confederation Parkway.

#### b. Edge Boulevards

In addition to providing corridors for utilities, boulevards provide the framework for public amenity on the Downtown Core streets and

integrate diverse areas of character. The design of boulevards should:

- promote consistent design themes, materials and landscaping throughout the Downtown Core;
- be designed as active public spaces integrating and linking development;
- should facilitate pedestrian linkage between the curb and abutting private lands;
- promote enclosure and definition of the street space;
- encourage the provision of places to meet, sit and socialize;
- should promote pedestrian comfort, weather protection and safety, particularly at transit stops; and
- provide for east-west bicycle routes along the north side of Burnhamthorpe Road and a proposed route along the south side of Rathburn Road.

#### c. Public Open Spaces

Parks and related public open space provide urban amenities for the Downtown Core. These features should:

- be passive in nature and oriented to urban activities;
- promote a sense of identity and place;
- provide pedestrian connection between the public sidewalk, significant buildings and features;
- ensure connection and linkage between the active face of abutting development and the public street; and
- frame a view, a significant building or terminate a vista, where appropriate.

### 3.2.2 The Design of the Private Realm

Individual development projects define many of the critical design elements and characteristics of the public streets and open spaces in the Downtown Core. This includes the framing of the street space, the definition of scale, the character of space and the level of comfort for the pedestrian. In this regard, development in the Downtown Core should demonstrate attention to the following principles.

#### a. The Design of Built Form in the **Streetscape**

The design, location and character of built form should encourage:

- built form as the most dominant visual element on the public street;
- building activities and functions focused on the public street;
- significance at corner and intersection locations;
- continuity of character, form, and street scale with abutting buildings;
- roof form having visual interest, identity and character;
- screening of rooftop mechanical equipment from grade level;
- screening of service facilities from the street; and
- scale and enclosure of the public street.

#### b. Landscape Design and Public Art in the **Streetscape**

The design, location and character of landscape features should encourage:

- connection and linkage with the public street;
- formal landscape elements respective of the urban setting;

- continuity with the design elements of the public boulevard;
- public art as an integral component of the street providing visual interest and a sense of place;
- architectural and landscape features reinforcing common objectives; and
- fencing and walls contributing to a high quality street image and limit the potential for graffiti by minimizing readily accessible, large smooth, blank surfaces.

#### c. Transit in the **Streetscape**

The design of built form and landscape elements should encourage:

- integration of transit stations and bus stops with development;
- weather protection in waiting areas;
- walkways from transit stops to principal building entrance(s); and
- visibility of transit stops from the public street and abutting development.

#### d. Pedestrians in the **Streetscape**

The design of built form and landscape elements should encourage:

- integration of spaces between the building wall and the streetline with the public boulevard for design continuity;
- protection from the effects of built form on wind and overshadowing;
- identification of principal building entrances from the street and linkage to the sidewalk;
- integral pedestrian comfort and weather protection;
- integration of vehicular and pedestrian circulation areas to promote pedestrian safety and comfort;



- identification of internal building spaces and exterior courtyards accessible to the public from the public street;
- ease of pedestrian movement and minimal grade changes through the site integrating with the public **streetscape**; and
- access for all, including those with disabilities, to and from building entrances and the public sidewalk, parking areas, and open spaces, where appropriate.

e. Parking and Garage Design in the **Streetscape**

The design and location of parking facilities should encourage:

- parking to be located to minimize any negative visual impact from the street and on-site development having regard for personal safety;
- walkways, traffic islands and pedestrian refuges as integral components of parking facilities;
- identified access to parking facilities from the public street;
- below-grade structured parking;
- street walls of parking garages designed as positive architectural elements;
- integration of parking on abutting sites; and
- safe vehicular circulation and orientation within the parking areas.

f. Vehicular Access and Facilities in the **Streetscape**

The design of facilities for vehicular access to properties should encourage:

- clarity, safety and internal overlook opportunities to properties;
- limited driveway access points to the public street and crossings of the sidewalk; and

- recognition of the needs of pedestrians and cyclists.

g. Signage in the Built Environment

The design, location, size and character of signs should encourage:

- visual integration of signage with the architectural components of the built form;
- compatibility of scale and character of the signage with the building and the context of the site; and
- clear identification of the site address for all, including those with disabilities.

### 3.2.3 Central Pedestrian System

- a. Priority locations are identified for pedestrian linkages and connections to core area functions such as the civic buildings, transit terminal, **major retail** commercial locations and open space features.
- b. This system will focus on the public street and its boulevards, having an upgraded boulevard treatment, an identifiable theme, a continuous treatment, and integrated public/private landscaping. Development abutting this route should reinforce a quality pedestrian scaled street through architectural treatment such as:
  - defined building base;
  - street edge definition; and
  - visually interesting and active building walls.
- c. Development abutting these routes will be encouraged to:
  - promote features such as pedestrian plazas

and courtyards which are publicly accessible and promote amenities including public art;

- promote a strong street orientation by having building entrances oriented to the public street; and
- promote a weather protected pedestrian environment incorporating features such as arcades or architectural canopies; avoid blank walls abutting streets; locate uses at grade that encourage visual interest and street activity.

### 3.3 Precincts

Precinct policies articulate in greater detail the Urban Design policies for specific areas of the Downtown Core. These policies apply to the built form, public boulevard and **streetscape** elements and are intended to realize an urban form and character which supports the Development Concept and preceding Urban Design policies for the Character Area.

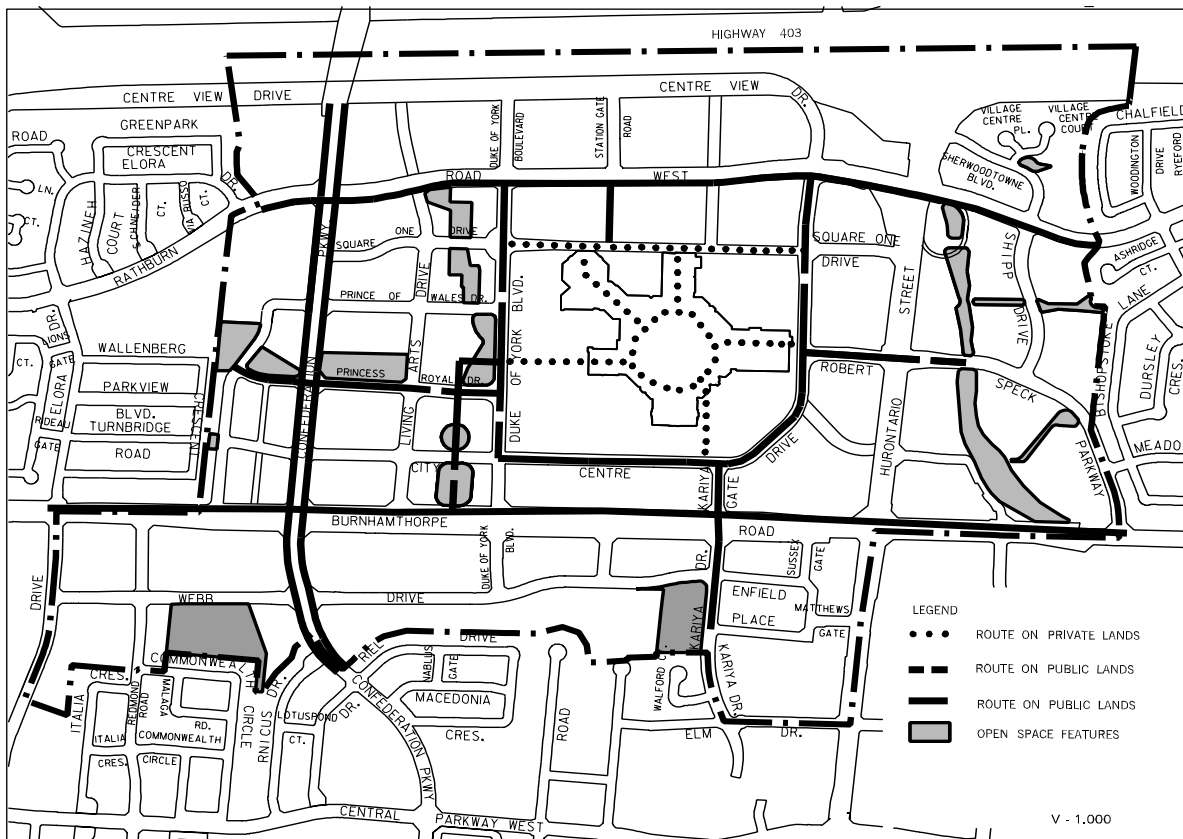


Figure 2: Central Pedestrian System

### 3.3.1 Central Urban Core

#### a. Urban Design Vision

The Central Urban Core contains the **major retail** commercial facility for the city and the Downtown Core Transit Terminal.

The design of the built environment should encourage:

- intensive pedestrian/public environments, including pedestrian corridor linkages from perimeter roads to the retail commercial core;
- high animation;
- frequent through access and public roads, as illustrated on the Downtown Core Land Use Map;
- smaller, more urban blocks/parcels;
- development that reinforces a quality, pedestrian scaled street through architectural treatment such as, a defined building base and street edge definition;
- street related built form;
- continuity of built form;
- unified boulevard design and related amenities;
- formal landscaping and boulevard treatment;
- pocket landscape parks (parkettes/open space/plazas);
- public art;
- on-street/metered parking; and
- structured parking integrated with building and landscaped area and active edges where oriented toward the street.

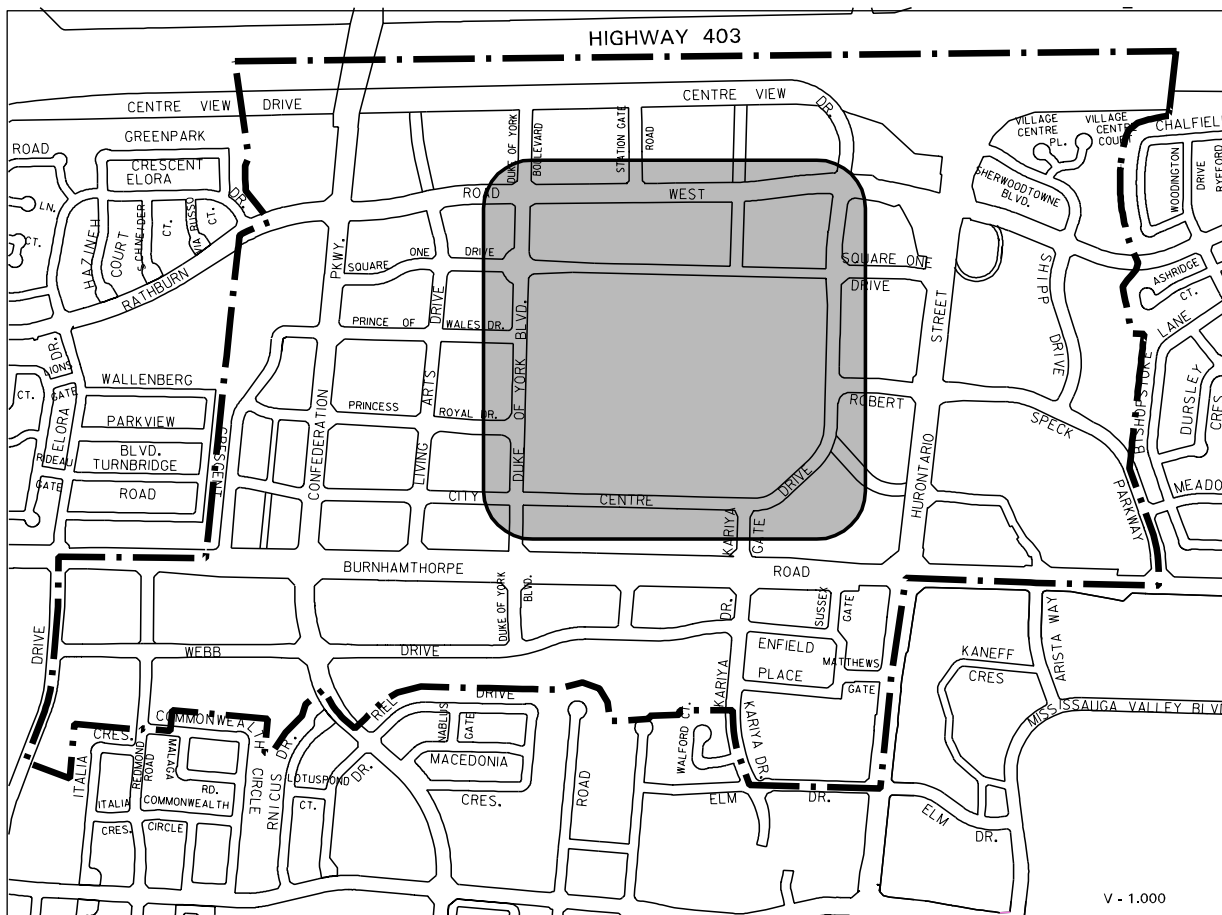


Figure 3: Civic Centre

### 3.3.2 Civic Centre

#### a. Urban Design Vision

The Civic Centre Precinct contains the Living Arts Centre, Civic Centre and Central Library which function as the principal civic destination and landmark in the city. To enhance the civic character and sense of continuity of built form and open space.

The design of the built environment should encourage:

- built form defining the majority of the streetline;
  - development that reinforces a quality pedestrian scaled street through architectural treatment such as, a defined building base and street edge definition;
- strong pedestrian interconnection - with frequent and convenient pedestrian links;
  - visual openness - maintain visual corridors from public streets to buildings;
  - development of the Central Pedestrian System; and
  - introduction of active uses at the street edge.

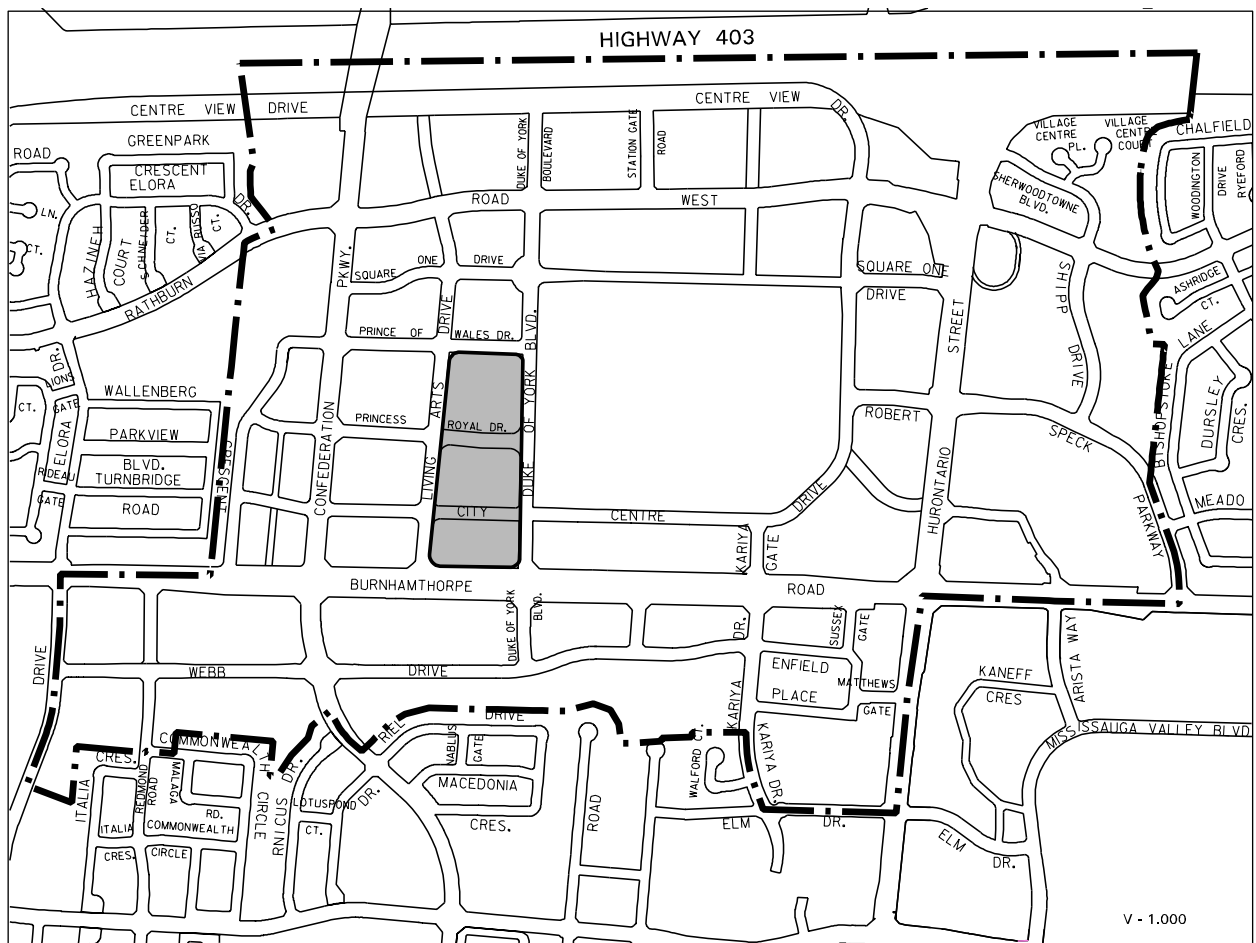


Figure 4: Civic Centre

### 3.3.3 Confederation Parkway Area

#### a. Urban Design Vision

Development in the Confederation Parkway Area will promote a distinct and unique place with a combination of high and medium scale built form together with an emphasis of medium scale form adjacent to existing low-rise development immediately to the west of the Downtown Core. Along the length of Confederation Parkway a 1.5 m bicycle path will be located within the public right-of-way along both sides of the road.

The design of the built environment should encourage:

- a distinctive sense of place, a focal point, and an orientation point for the Character Area;

- development of the Central Pedestrian System;
- public art;
- vistas to the Civic Centre Precinct;
- built form defining the street edge;
- minimal building setbacks;
- smaller scale urban blocks;
- continuity of built form;
- visually interesting and active building walls; and
- high and medium scale coverage development abutting streets and public spaces.

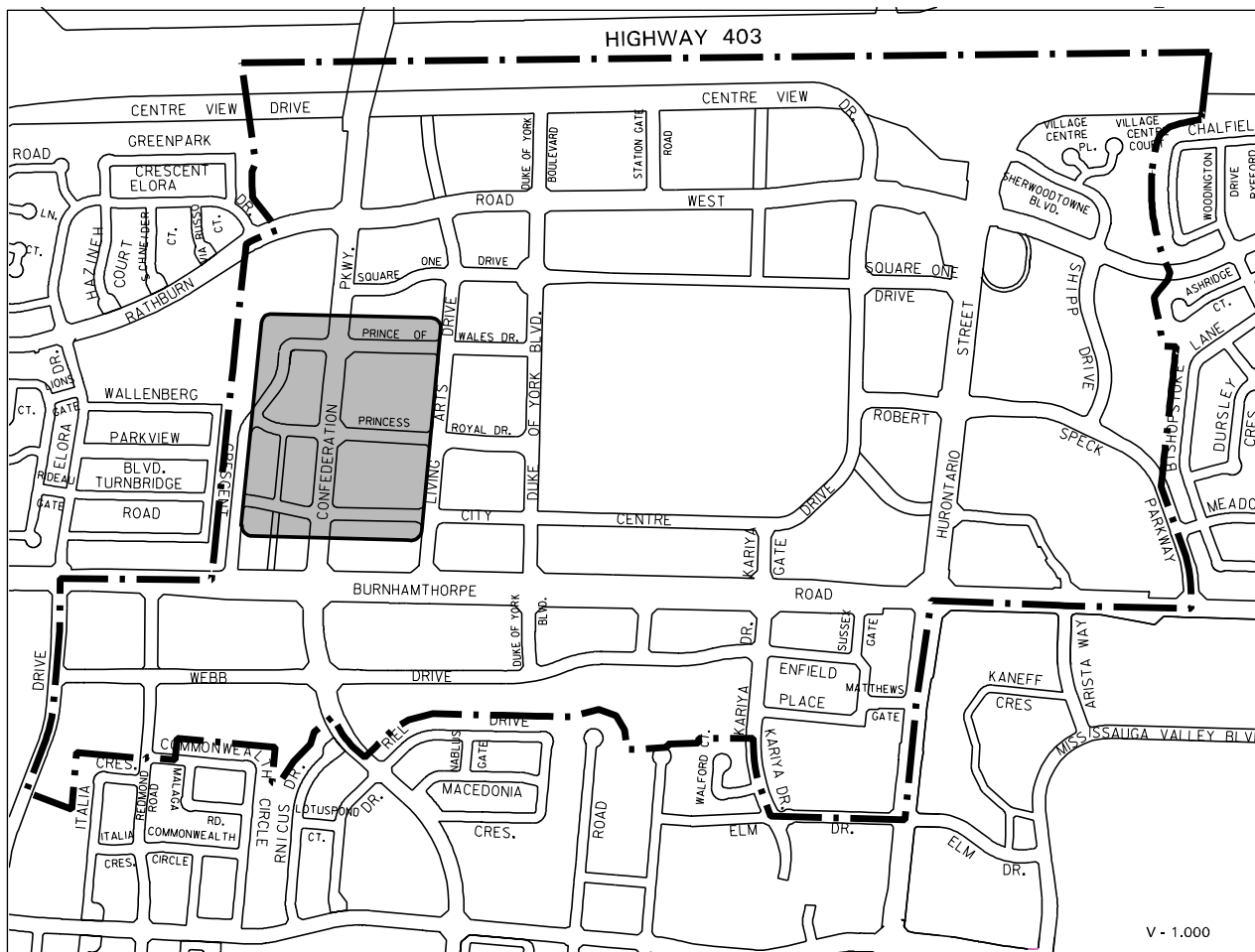


Figure 5: Confederation Parkway Area



### 3.3.4 Duke of York Boulevard Corridor

#### a. Urban Design Vision

Development abutting Duke of York Boulevard will promote the principal ceremonial and active pedestrian corridor in the Character Area.

The design of the built environment should encourage:

- vistas to the Civic Centre Precinct;
- entrance features at Burnhamthorpe Road West and Centre View Drive;
- high pedestrianization;
- a high standard of public amenity;
- public art;
- development that reinforces a quality pedestrian scaled street through

architectural treatment such as, a defined building base and street edge definition;

- Street related built form;
- visually interesting and active building walls;
- orient principal entrances and principal façades to Duke of York Boulevard and orient rear building façades or service areas away from Duke of York Boulevard;
- an integrated character; and
- development of the Central Pedestrian System.

b. In the event of conflict between these precinct policies and those of any other precincts, these policies will take precedence.

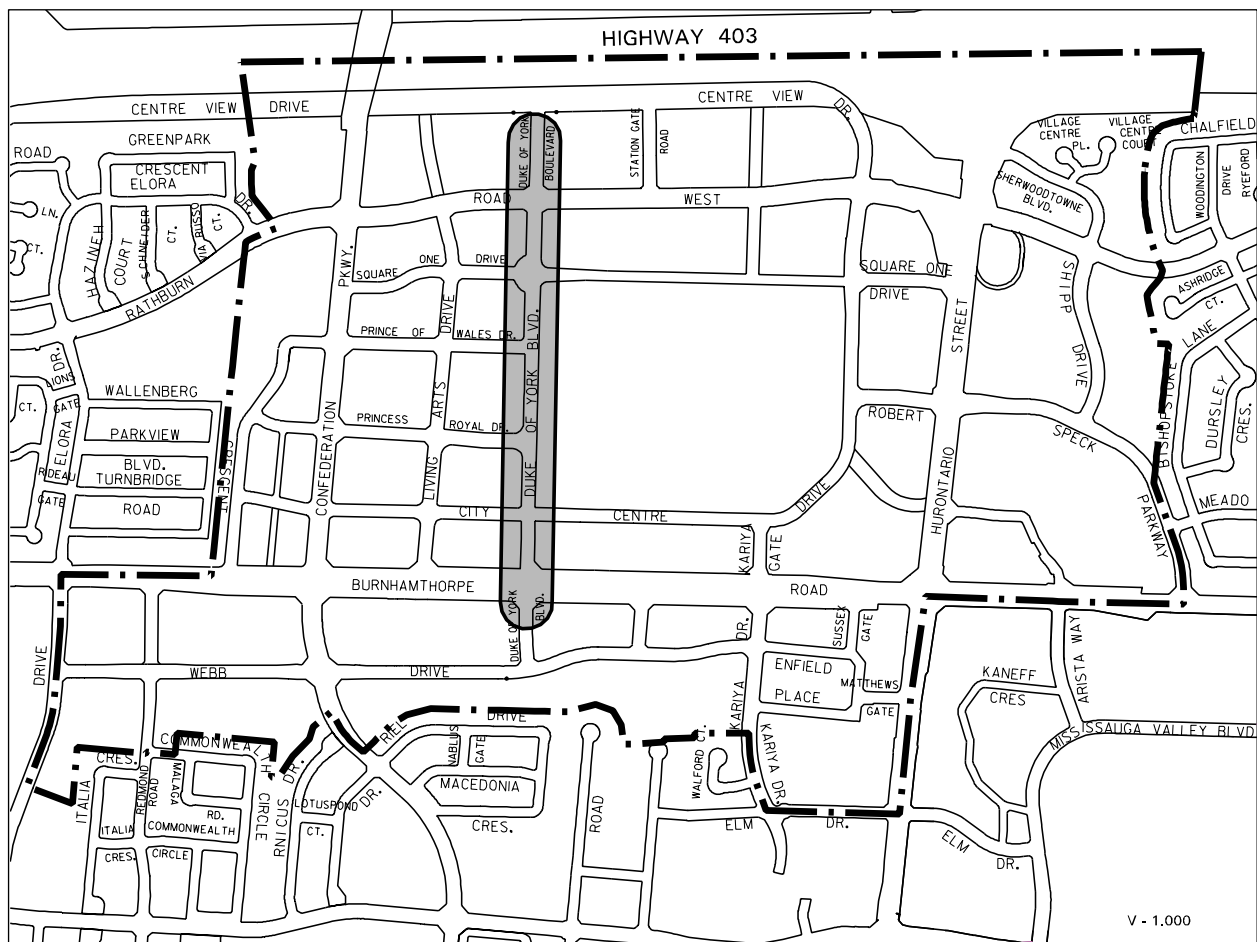


Figure 6: Duke of York Boulevard Corridor

### 3.3.5 Rathburn Road Corridor

#### a. Urban Design Vision

The Rathburn Road Corridor recognizes the Downtown Core Transit Terminal as a key element to the future of this area and Rathburn Road West as an important east-west pedestrian street and transition area to the future high-rise development on Centre View Drive. The portion of this corridor from Duke of York Boulevard to City Centre Drive will be more active and pedestrianized. Along the length of the south side of Rathburn Road a proposed 3.5 m bicycle path will be located within the existing public right-of-way.

The design of the built environment should encourage:

- pedestrian scale development fronting Rathburn Road West;

- street related activity;
- low to medium scale development;
- continuity of built form;
- pedestrian linkages;
- an improved sense of connection to the east; and
- development of the Central Pedestrian System

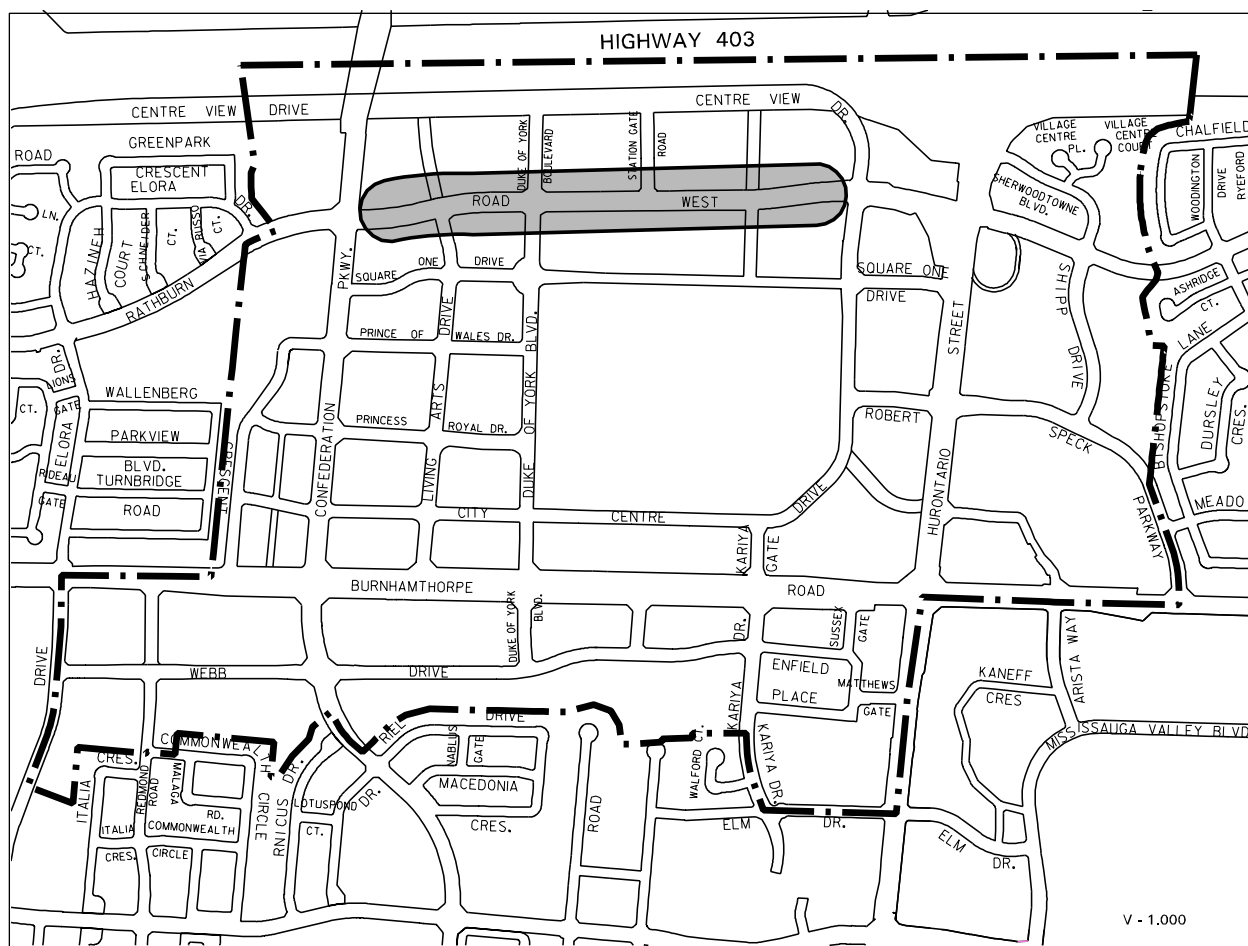


Figure 7: Rathburn Road Corridor

### 3.3.6 Burnhamthorpe Road Corridor

#### a. Urban Design Vision

The Burnhamthorpe Road Corridor is the principal east-west urban boulevard, functioning as a destination and providing entries to the Downtown Core. It promotes a focus and sense of place for the civic buildings, and will integrate development north and south of Burnhamthorpe Road. Along the length of the north side of Burnhamthorpe Road a 3.5 m bicycle path will be located within the designated public right-of-way.

The design of the built environment should encourage:

- development that reinforces a quality pedestrian scaled street through architectural treatment such as, a defined

building base and street edge definition;

- pedestrian amenities at principal entrances;
- predominantly medium scale, street related development rising to higher buildings at Hurontario Street;
- a combination of high and medium scale, street related development on the north side of Burnhamthorpe Road West, west of Confederation Parkway;
- urban development at main intersections (e.g. block between Hurontario Street and Kariya Drive);
- additional mid-block road connections; and
- major landscaped boulevard and centre median, presenting a prestige image.

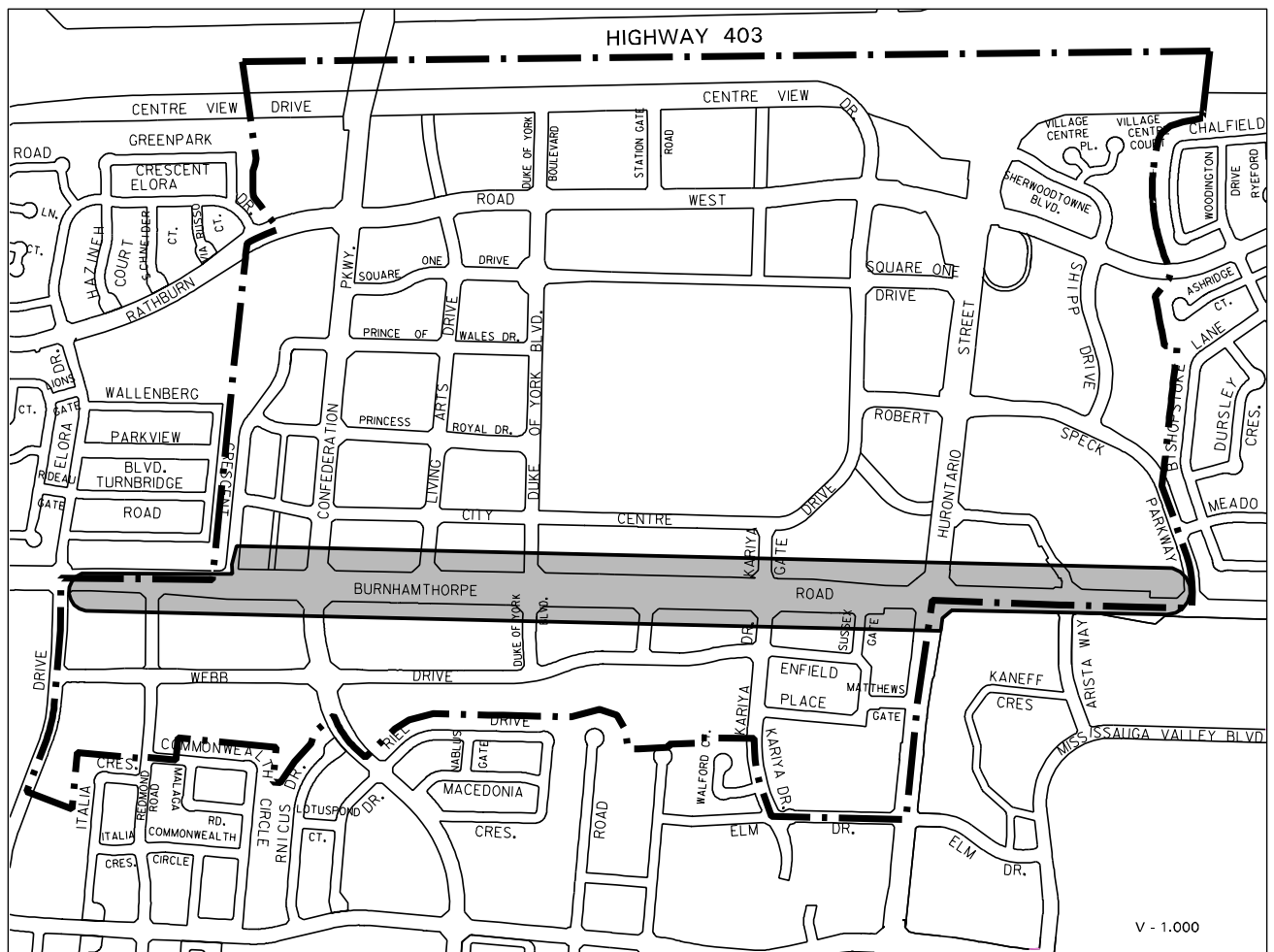


Figure 8: Burnhamthorpe Road Corridor

Across from Civic Plaza:

- development that reinforces a quality pedestrian scaled street through architectural treatment such as, a defined building base and street edge definition;
- minimal building setbacks; and
- formal landscaped plaza.

South side:

- landscaped promenade/forecourt;
- at intersections, buildings should be located close to the streetline;
- visually interesting and active building walls; and
- principal entry doors oriented to the street.

### 3.3.7 Hurontario Street Corridor

#### a. Urban Design Vision

The Hurontario Street Corridor is the principal north-south urban boulevard, functioning as a destination and providing major vehicular entrances to the Downtown Core. It projects a high profile, distinctive image with prominent buildings and extensive landscaping in a park like setting.

The design of the built environment should encourage:

- prestige buildings and dramatic landscaping;
- moderate setback range consistent with the park like setting;
- continuous landscaping between building and street;

- urban development at main intersections, with buildings closer to the streetline;
- pedestrian linkages to public sidewalks and intersections;
- pedestrian amenities at principal entrances;
- structured parking fully integrated with building form and landscaping;
- additional mid-block road connections;
- more intersections, as illustrated on the Downtown Core Land Use Map; and
- central landscaped median.

b. In the event of conflict between these precinct policies and those of any other precincts, these policies will take precedence.

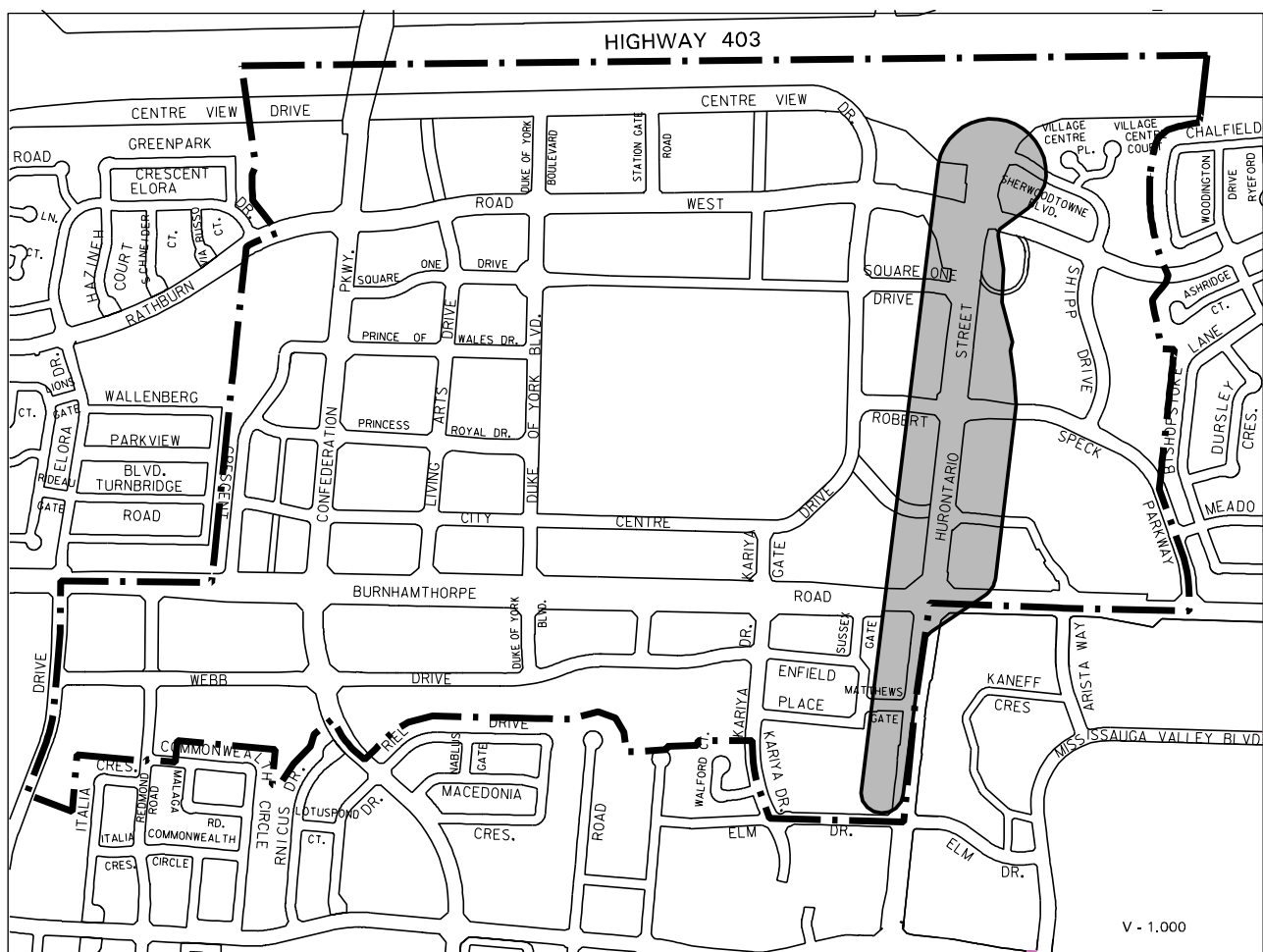


Figure 9: Hurontario Street Corridor



### 3.3.8 Provincial Highway 403 Frontage

#### a. Urban Design Vision

Development along the Provincial Highway 403 Frontage will promote a high profile skyline, projecting a distinctive identity and recognizable image for the Downtown Core. Visibility to parking will be minimized and properly screened from Centre View Drive and no service areas will be exposed to Provincial Highway 403. Parking and service areas that cannot be located elsewhere shall be screened so as to prevent negative visual impacts.

The design of the built environment should encourage:

- a high profile skyline, projecting a distinctive identity and recognizable image for the Downtown Core; and
- prominent, prestige buildings and extensive landscaping.

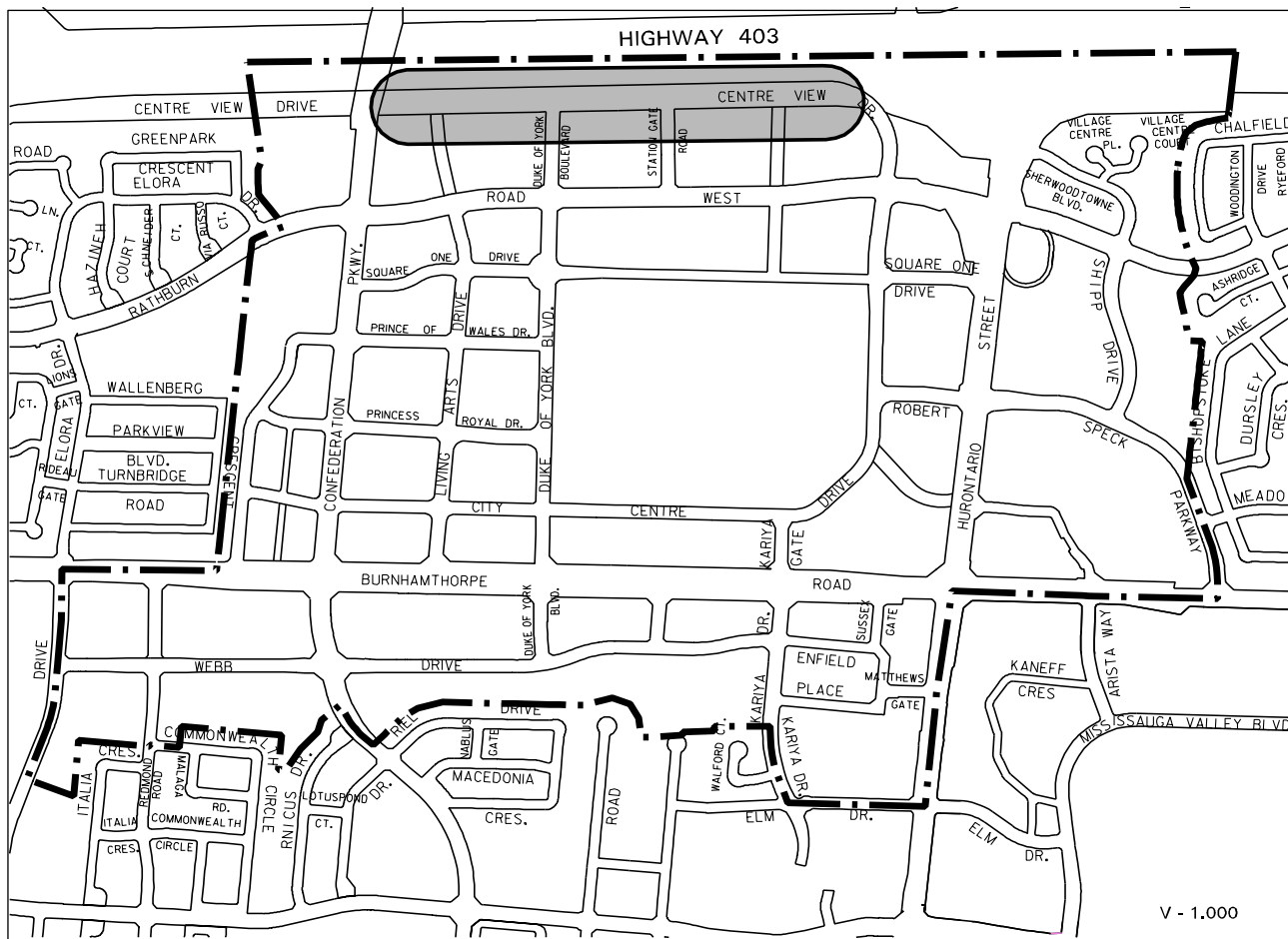


Figure 10: Provincial Highway 403 Frontage

### 3.3.9 Transition Areas

#### a. Urban Design Vision

The Transition Areas will promote positive integration of building scale and intensity of development between the Downtown Core and abutting communities.

The design of the built environment should encourage:

- mid-rise built form adjacent to the Downtown Core boundaries and north of Burnhamthorpe Road West, with height transitions to a maximum of five storeys, on lands shown as Area 'A';
- low-rise built form adjacent to Downtown Core boundaries, with height transitions from a maximum of two storeys at the Downtown Core boundaries to a maximum of eight storeys at the inner edge of the

Transition Areas for lands abutting the easterly limit of the Downtown Core, shown as Area 'B' and lands immediately south of Rathburn Road West, show as Area 'C';

- orientation of buildings enabling a sensitive integration with adjacent low density development;
- internalized service areas and parking to minimize impact on adjacent residential;
- pedestrian and bicycle links to adjacent communities;
- appropriate setbacks and landscaping adjacent to the existing walkways;

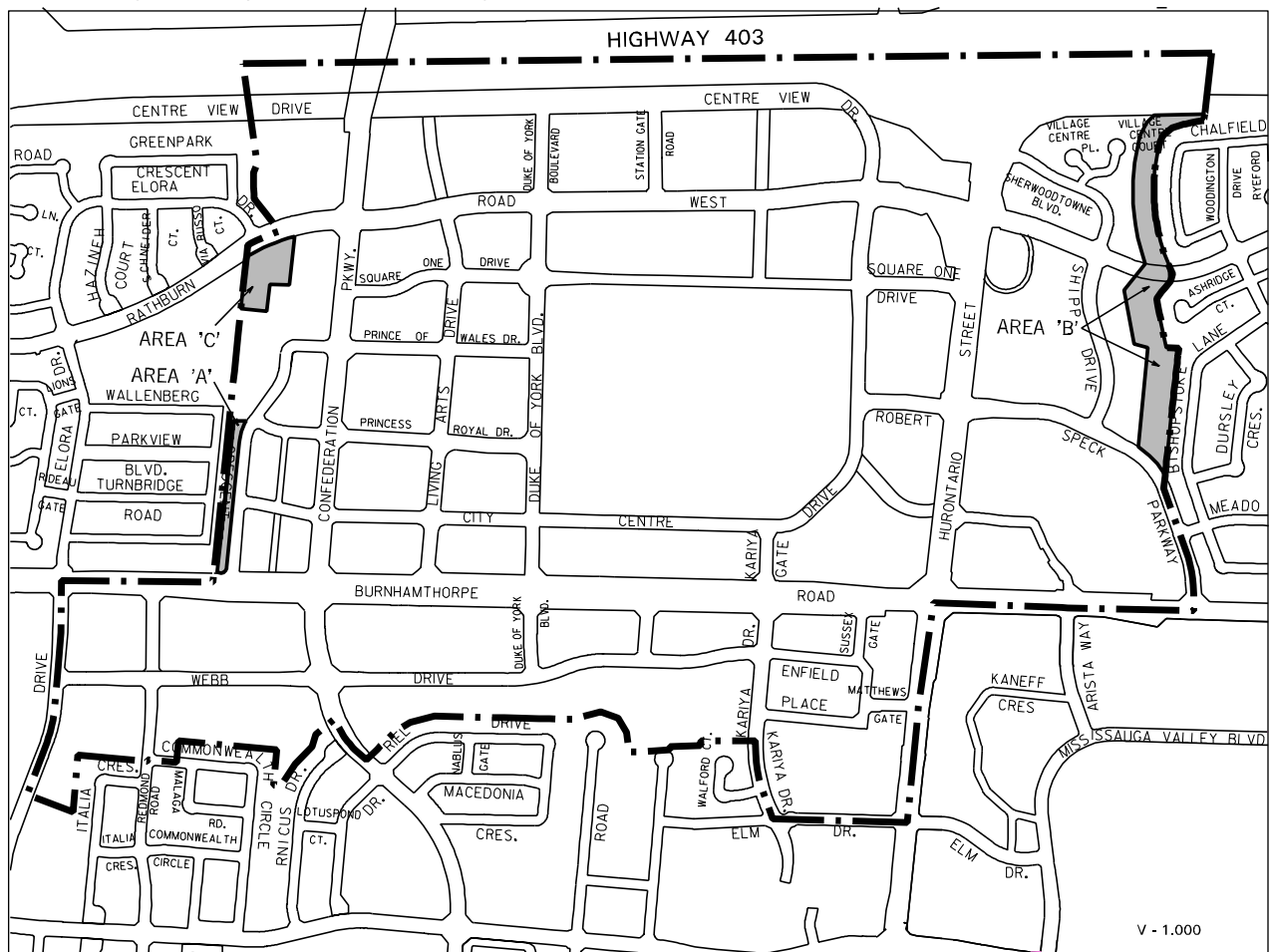


Figure 11: Transition Areas

- appropriate building interface including setbacks and landscaping, within the public open space block at the terminus of City Centre Drive within lands shown as Area 'A'; and
  - appropriate building interface with the public open space linear park to the west of lands shown as Area 'A'.
- b. In the event of conflict between these precinct policies and those of any other precinct, these policies will take precedence.

### 3.3.10 Cooksville Creek Corridor

#### a. Urban Design Vision

The Cooksville Creek Corridor is recognized as a public amenity and resource. To preserve and enhance this natural feature, the design of the built environment should encourage:

- appropriate setback of buildings from corridor (landscaping only);
- improved visibility, access and linkage to the corridor from development parcels and public streets;
- views/informal surveillance to the corridor;
- integration of public/private open space and landscape areas;

- positive relationship between built form and the corridor; and
- parking structures designed and located to complement the corridor.

The fencing of the edge of property abutting this corridor is discouraged.

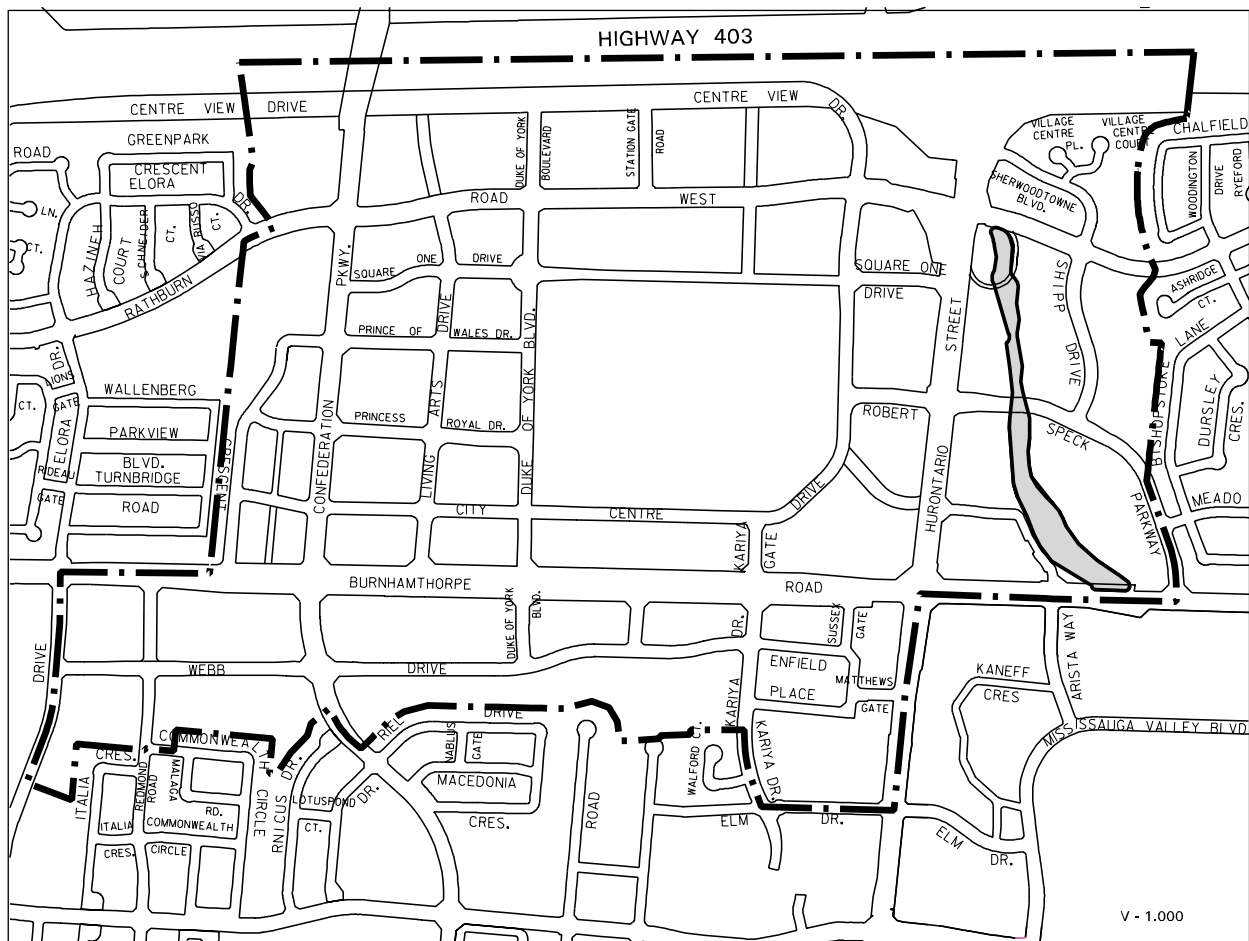


Figure 12: Cooksville Creek Corridor

## 4.0 Land Use

### 4.1 Downtown Mixed Use

- a. Lands designated Downtown Mixed Use permit a wide range of land uses and activities. These uses may be grouped either within a development parcel or an individual building.
- b. Owners/developers will be encouraged to advise all prospective purchasers or tenants occupying new residential units within the 600 m influence area of a chemical plant located on Mavis Road, south of Burnhamthorpe Road West, of the possibility of adverse odour emissions.

#### 4.1.1 Permitted Uses

- a. All forms of high density residential development. Townhouses will only be permitted in the Transition Areas. Detached and semi-detached dwellings will not be permitted.
- b. Uses permitted in accordance with the general Residential policies of this Plan.
- c. Major and **secondary offices**.
- d. Civic/cultural facilities.
- e. Hotel and conference facilities, all types of restaurants, except those with drive-through facilities, and entertainment facilities, except in the Transition Areas.
- f. Retail Commercial uses, except those with a drive-through facility, motor vehicle sales, motor vehicle commercial uses, motor vehicle body repair uses, motor vehicle wrecking and truck washes.
- g. Community infrastructure and parkland.

### 4.1.2 Policies

- a. Driveway access to individual residential units will not be permitted from a public street.
- b. Retail Commercial uses, including restaurants, will be limited to a maximum of 20% of the total Gross Floor Area (GFA).

### 4.2 Downtown Core Commercial

Lands designated Downtown Core Commercial will accommodate the highest concentration of retail commercial activities in the city in addition to a mix of other land uses.

#### 4.2.1 Permitted Uses

- a. Retail Commercial uses, except those with a drive-through facility, motor vehicle sales, motor vehicle commercial uses, motor vehicle body repair uses, motor vehicle wrecking and truck washes.
- b. All types of restaurants except those with drive-through facilities.
- c. Major and **secondary offices**.
- d. Residential apartments.
- e. Hotel and conference facilities.
- f. Entertainment, recreational facilities and parkland.
- g. Civic/cultural facilities.

### 4.3 Public Open Space

Notwithstanding the Public Open Space policies of this Plan, lands designated Public Open Space in the Civic Centre Precinct and Special Site 3 will be urban in character and may accommodate parking and a variety of programs, such as music and theatre events, displays and exhibitions, festivals and other special events.



## 5.0 Transportation

### 5.1 Road System

5.1.1 Consideration will be given to reducing or eliminating parking requirements during the implementation of the Downtown Core Policies.

5.1.2 All roads shown on the Downtown Core Land Use Map will be public.

5.1.3 Rights-of-way may be increased without an amendment to this Plan when development applications are evaluated or further transportation studies are carried out.

5.1.4 Daylight triangles of 15 m will be required.

5.1.5 The basic rights-of-way for minor collector roads and local roads may be reduced without an amendment to this Plan subject to the City being satisfied that the role and function of such roads are maintained.

5.1.6 The proposed east-west road alignment from Confederation Parkway to Hurontario Street, south of Rathburn Road West will be subject to a functional design review, to identify the following: potential traffic signal requirements along this road; intersection geometric design requirements to support the proposed intersections at Confederation Parkway, City Centre Drive and Hurontario Street; right-of-way requirements for this road; and, the intersection spacing along Confederation Parkway, City Centre Drive and Hurontario Street to support the new east-west road. This spacing review may impact the alignment of this minor collector road. In addition, this functional study is to also include the potential relocation of Part Block 18 in Registered Plan 43M-1010.

5.1.7 The transit system will expand as the Downtown Core is developed; bus service will be the predominant transit mode for a considerable period of time.

As part of the proposed Mississauga Bus Rapid Transit (BRT) facility, the existing transit terminal

may be expanded to serve future development growth and to connect with the BRT facility. In the long term, an additional BRT station is also proposed near Hurontario Street, between Rathburn Road West and Provincial Highway 403.

Transit facilities will be designed to accommodate the city wide transit network; an internal transit service operating on the perimeter roadway around Square One; the east-west BRT facility in the Provincial Highway 403 corridor, interurban transit systems; and the pedestrian system linking office concentrations, Square One and cultural activities.

As traffic volumes increase, preferential transit lanes consisting of a single curbside lane in each direction and intended to serve transit vehicles and right turning vehicles may be necessary.

5.1.8 Access to the Downtown Core to and from Provincial Highway 403 will be provided by the existing interchanges at Mavis Road and Hurontario Street and by the proposed Provincial Highway 403 North Collector and Centre View Drive. Both of the parallel roads will be connected to Provincial Highway 403 east of Hurontario Street and west of Mavis Road.

The proposed Provincial Highway 403 North Collector, located north of the Downtown Core, will be developed within the Parkway Belt West as a one-way westbound two lane roadway with grade separations at Hurontario Street, Duke of York Boulevard, Confederation Parkway, the Provincial Highway 403 westbound off-ramp at Mavis Road. In addition, ramp connections will be provided at Duke of York Boulevard and Confederation Parkway to provide access to and from the Downtown Core road network.

Centre View Drive will be developed within the Parkway Belt West and will operate predominately as a basic four lane two-way roadway (with turning lanes), with grade separations at Mavis Road, Confederation Parkway, Duke of York Boulevard, the Provincial Highway 403 eastbound off-ramp at Hurontario Street and Hurontario Street. In addition, five at grade signalized intersections will provide

access to the Downtown Core road network. The two-way section of this road will be connected to Provincial Highway 403 via a two lane one-way link extending from the eastbound off-ramp at Mavis Road and a one-way eastbound roadway joining Provincial Highway 403 east of City Centre Drive. The Downtown Core Land Use Map shows a conceptual alignment on how this one-way eastbound roadway is proposed to connect to Provincial Highway 403.

All roads in the network will be public roadways. The preliminary designs of the proposed connections/interfaces of these new roads to the adjacent Provincial Highway 403 have been endorsed in principle by the Provincial Government. The detailed design of these facilities is subject to the review and approval of the Ministry of Transportation to ensure all aspects, including safety, operations and level of service and capacity for Provincial Highway 403 are not compromised.

The placement of structures over and under public roadways consistent with this Plan will be permitted. Such structures may include, but are not necessarily limited to, open and enclosed pedestrian bridges and elevated pedestrian plazas.

development flows to pre-development levels and will contain appropriate erosion and quality control measures. One facility will be sited at the outlet of the Downtown Core where it discharges to the Cooksville Creek, immediately north of Central Parkway East. A second facility will be situated just north of Provincial Highway 403, east of Mavis Road.

## **6.0 Stormwater Management**

6.1 Any retail commercial, office, industrial, community or high density residential land development within the Cooksville Creek watershed will require the implementation of on-site stormwater management techniques to control the post development stormwater discharge to pre-development levels for the 2 year and 5 year storm events. These controls are subject to the approval of the City, in accordance with Council adopted policies.

6.2 Construction of centralized stormwater management control facilities is proposed at two locations, both of which are outside the Downtown Core. These facilities will control the 100 year post

## 7.0 Special Site Policies

There are sites within the Downtown Core that merit special attention and are subject to the following policies.

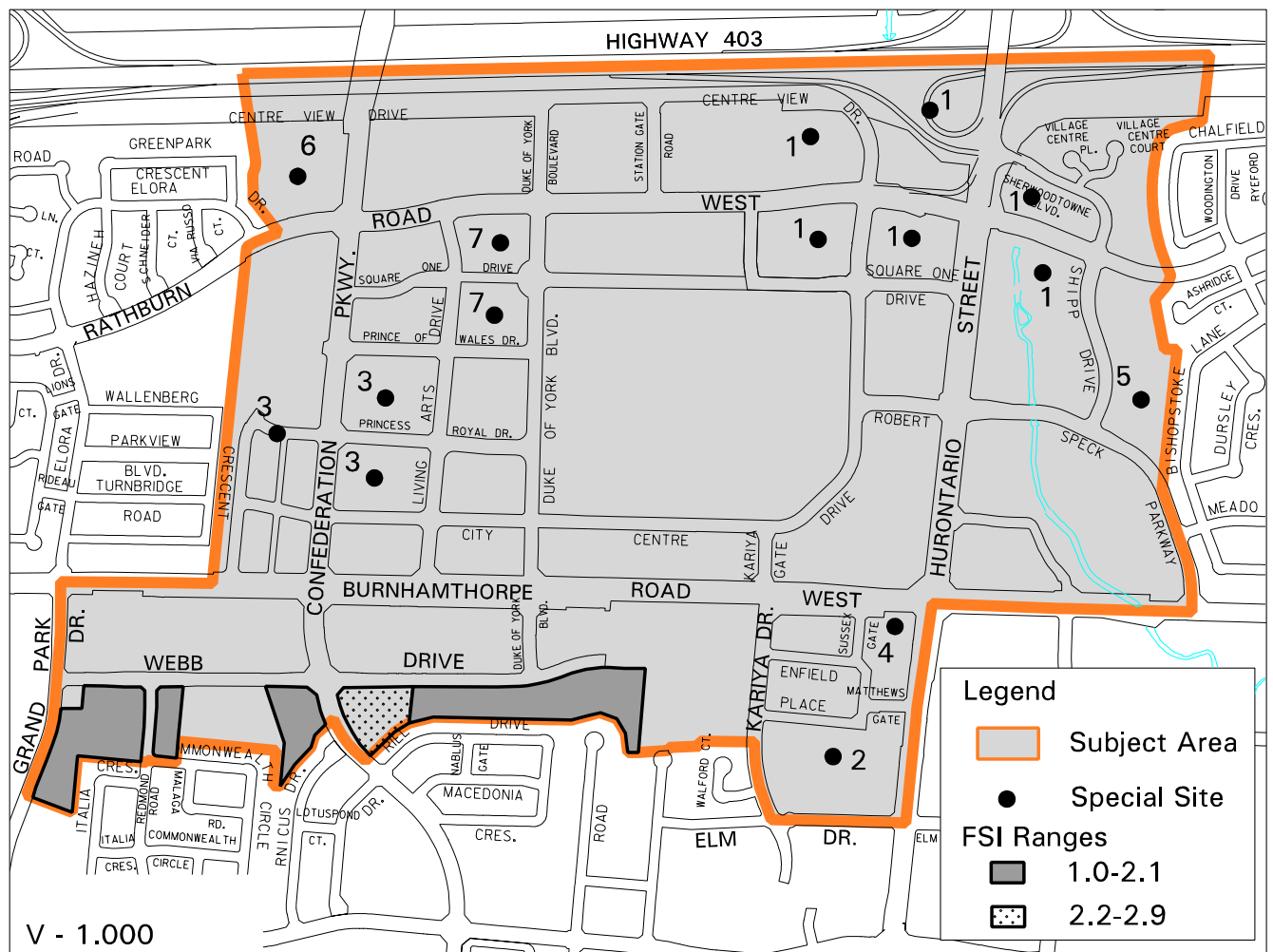
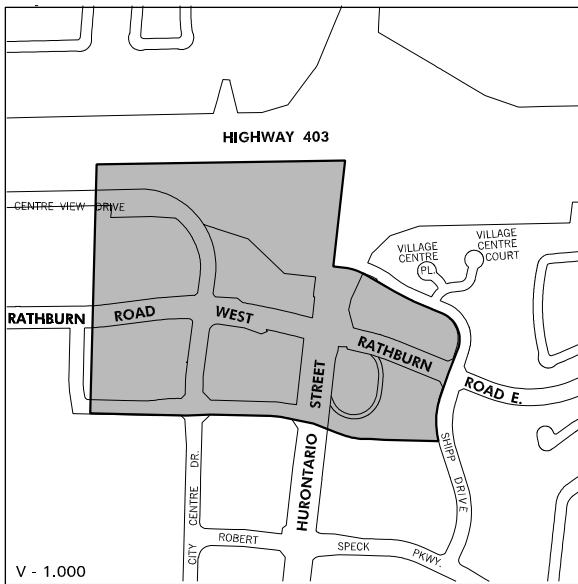


Figure 13: Location of Special Sites within the Downtown Core Local Area.

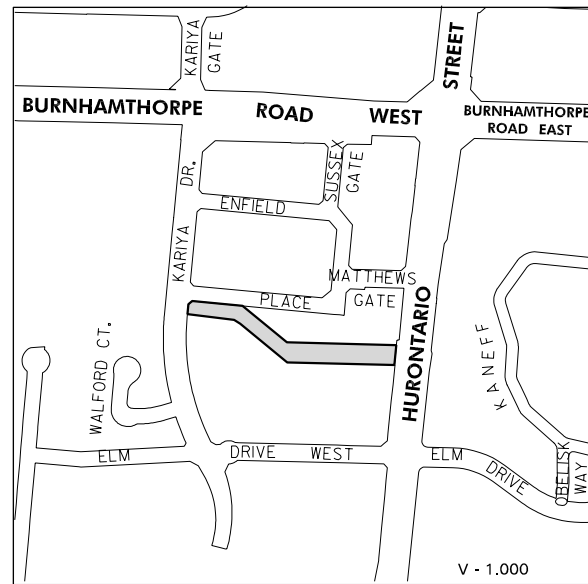
## 7.1 Site 1



17.1.1 The lands identified as Special Site 1 are located in the four quadrants adjacent to the Hurontario Street and Rathburn Road intersection.

17.1.2 Prior to the finalization of the road network for this site area, the appropriate transportation studies will be carried out in conjunction with the Provincial Government and the Region of Peel.

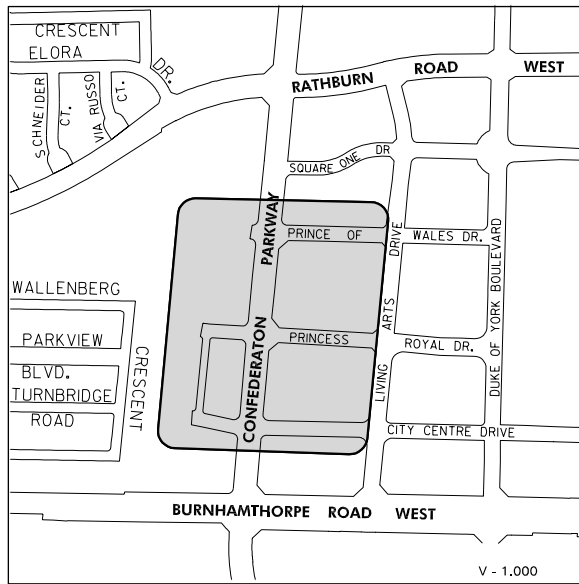
## 7.2 Site 2



17.2.1 The lands identified as Special Site 2 are located south of Enfield Place between Hurontario Street and Kariya Drive.

17.2.2 This tributary to the Cooksville Creek and its related valley feature will remain open and will be incorporated as a landscape element into the design of future development.

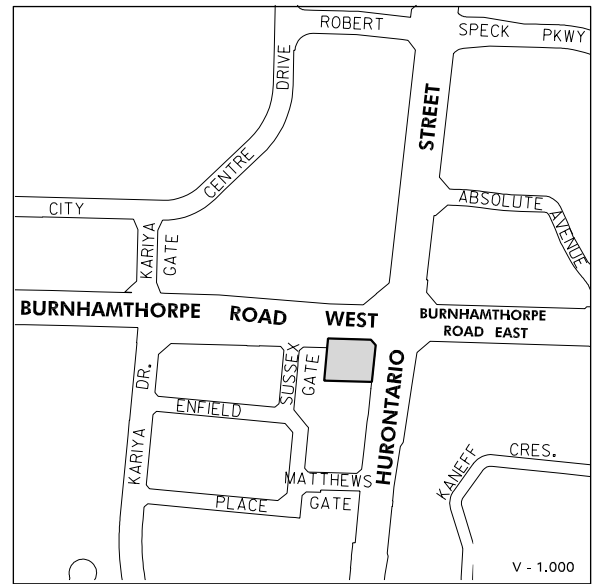
### 7.3 Site 3



17.3.1 The lands identified as Special Site 3 are defined as the Confederation Parkway Area.

17.3.2 Special Site 3 will accommodate one or more areas of public parkland. The location, number and size of the park(s) block(s) shall be determined in conjunction with the development of the lands in this Area. In order to achieve parkland of acceptable size, configuration and location, Mississauga will encourage consolidation of parkland dedication requirements of multiple sites within the Area. Prior to the approval of any draft plan of subdivision, severance, or any other development application within this Area, property owners shall demonstrate to the satisfaction of the City how the City's parkland objectives within this Area will be satisfied, through the provision of development and parkland concept plans. Prior to final approval of any severance, subdivision or any other development application within this Area, property owners shall dedicate sufficient parkland or enter into appropriate agreements with the City and other owners of lands to secure the provision of adequate parkland from other development parcels within the Area.

### 7.4 Site 4

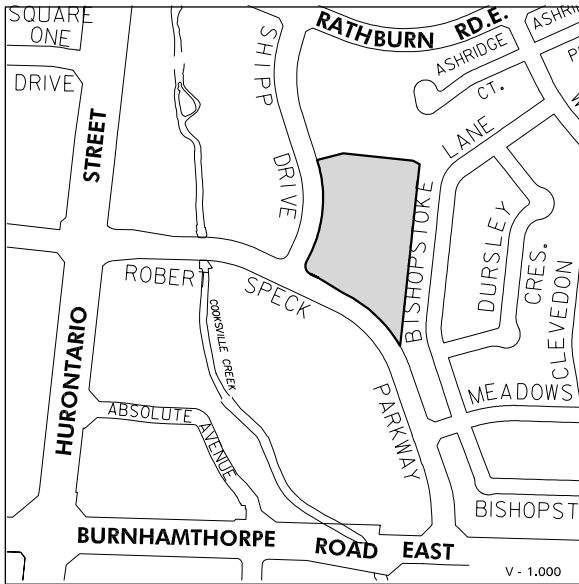


17.4.1 The lands identified as Special Site 4 are located at the southwest corner of Burnhamthorpe Road West and Hurontario Street.

17.4.2 Notwithstanding the provisions of the Mixed Use designation, a motor vehicle service station will be permitted.



## 7.5 Site 5

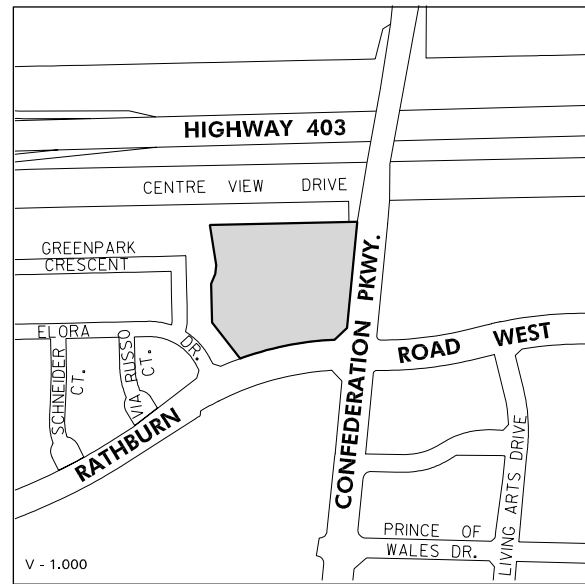


7.5.1 The lands identified as Special Site 5 are located at the northeast quadrant of Robert Speck Parkway and Shipp Drive.

7.5.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. townhouses will be permitted on the entire site.

## 7.6 Site 6



7.6.1 The lands identified as Special Site 6 are located north of Rathburn Road, west of the Confederation Parkway extension.

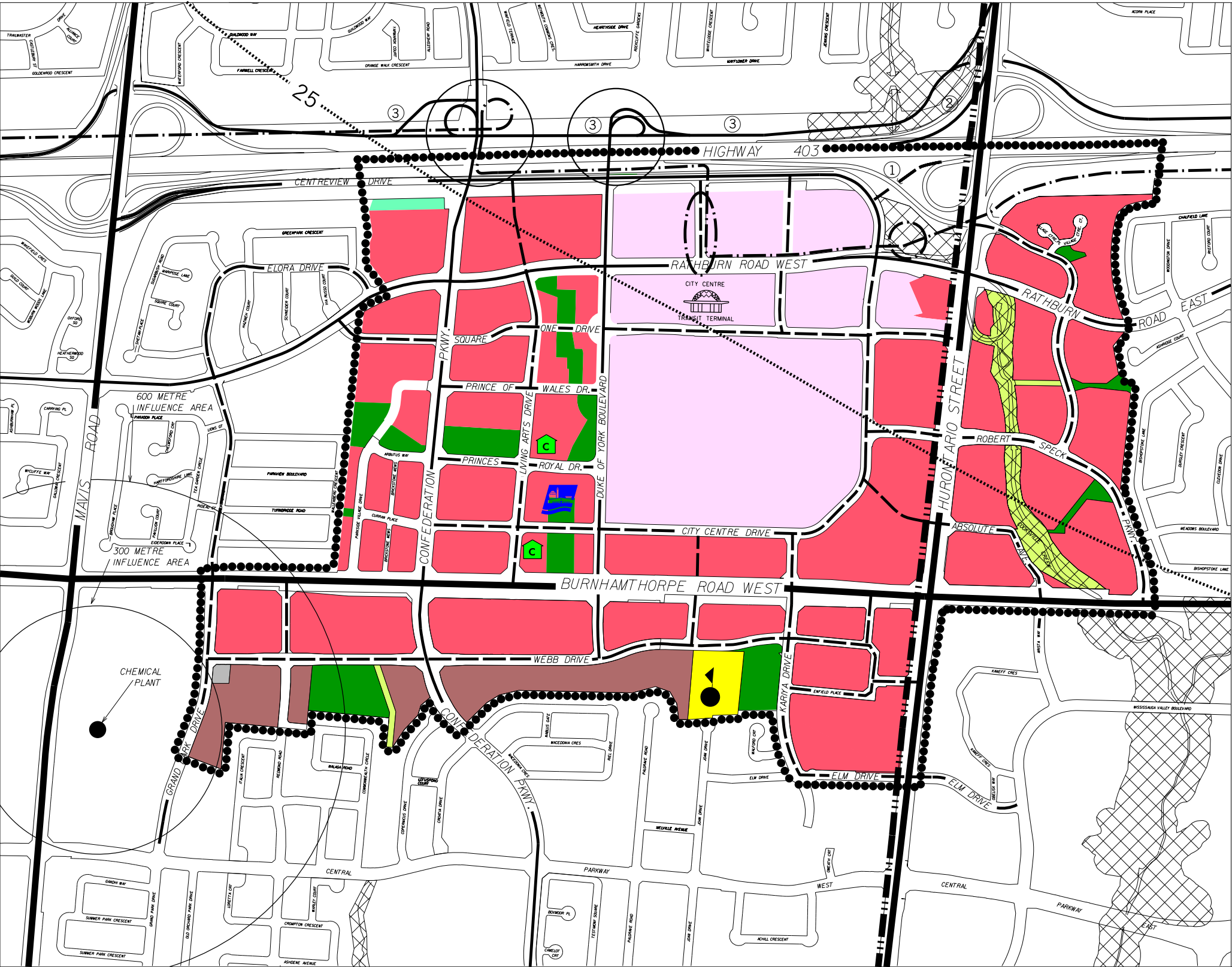
7.6.2 Notwithstanding the provisions of the Mixed Use designation, townhouse dwellings will be permitted.

## 7.7 Site 7



7.7.1 The lands identified as Special Site 7 are bounded by Rathburn Road West to the north, Duke of York Boulevard to the east, Prince of Wales Boulevard to the south and Living Arts Drive to the west.

7.7.2 Notwithstanding the Public Open Space and Downtown Mixed Use designations shown on the Land Use Plan, the lands designated Public Open Space and Downtown Mixed Use may be increased or decreased in size, reconfigured or relocated within Special Site 7 without an amendment to this Plan and these lands may also be used for parking.



**LAND USE DESIGNATIONS**

- Downtown Mixed Use
- Downtown Core Commercial
- Public Open Space
- Greenbelt
- Parkway Belt West
- Residential Low Density I
- Residential High Density
- Utility

**TRANSPORTATION LEGEND**

- Provincial Highway and Interchange
- Arterial
- Major Collector
- Minor Collector
- Local Road
- Bus Rapid Transit Corridor
- Bus Rapid Transit Station
- Higher Order Transit Corridor
- Proposed Grade Separation

**LAND USE LEGEND**

- 1996 NEP / 2000 NEF Composite Noise Contours
- Natural Hazards

**Notes:**

- Any part of the road network shown outside the city boundaries is shown for information purposes only.
- All road proposals within the Highway 403 Corridor are conceptual and require review and approval of the Provincial Government.
- Refer to Schedule 3, Natural System for the location of the Natural Areas System and Natural Hazards.
- The limits of the Natural Hazards shown on this map are for illustrative purposes only. The appropriate Conservation Authority should be consulted to determine their actual location.
- The 1996 Noise Exposure Projection (NEP)/2000 Noise Exposure Forecast (NEF) Composite Noise Contours are shown for information purposes only and are not part of this Local Area Plan. These contours will change from time to time as new information becomes available. For accurate reference the composite NEP/NEF map produced by Transport Canada at a scale of 1:50 000 should be consulted.

**TRANSPORTATION LEGEND**

- Civic Centre (City Hall)
- Public School
- City Centre Transit Terminal
- Community Facilities
- Local Area Plan Boundary

# Downtown Core Local Area Plan Land Use Map

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**Schedule 2: Downtown Core Long Term Road network and Classification**

**Schedule 3: Downtown Core Long Term Transit Network**

**Schedule 4: Downtown Core A & B Street Frontage**

**Schedule 5: Downtown Core Retail Activation**

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# Downtown Core

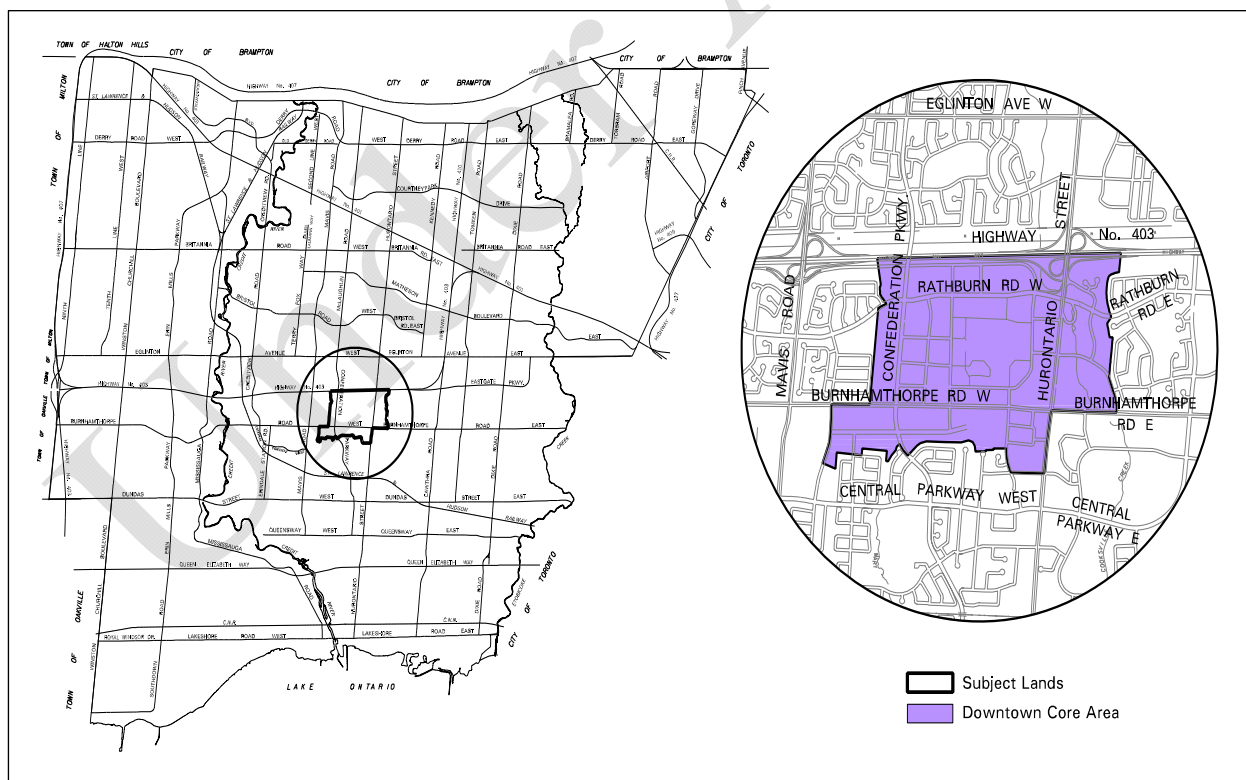
## 1.0 HOW TO READ THE DOWNTOWN CORE LOCAL AREA PLAN

Mississauga Official Plan (the Plan) consists of a principal document and a series of local area plans, provided under separate cover. This is the Downtown Core Local Area Plan (this Area Plan) and provides policies for lands located in central Mississauga, as shown on Figure 1. It includes lands identified in the City Structure as the Downtown Core.

There are some instances where the policies and schedules of the principal document do not address all circumstances particular to the Downtown Core. In these cases, the Downtown Core Local Area Plan elaborates on, or provides exceptions to the policies

or schedules of the principal document.

This Area Plan must be read in conjunction with the Mississauga Official Plan principal document. Parts 1 to 4, the schedules and the appendices of the principal document, are applicable to the Downtown Core area, unless modified by this Area Plan. For example, the policies of this Area Plan must be read in conjunction with the environmental, multi-modal, urban form and land use policies of Parts 2 and 3 of the principal document. In the event of a conflict, the policies of this Area Plan take precedence.



Map 1: The Downtown Core Local Area is located in central Mississauga and is identified in the City Structure as Downtown Core.

## 2.0 CONTEXT

Since its rural beginning as farmland in the 1970s, Downtown Mississauga has been evolving into an exciting urban place. Today the Downtown Core consists of high density residential developments, office buildings, mixed use developments, parks, post secondary institutional and cultural facilities, civic uses and recreational and entertainment uses developed around the periphery of the Square One Shopping Centre.

Over the last decade, flexible zoning provisions combined with favourable market conditions have resulted in extensive private investment in high density residential and mixed use condominium development such as the internationally acclaimed Absolute/Marilyn buildings. During this time, there has been some expansion of the Square One Shopping Centre but limited new office development.

Recent public investments, including the new Sheridan College campus, regional transit infrastructure Bus Rapid Transit (BRT), planning for Hurontario Light Rail Transit (LRT) and significant city wide public realm investments and amenities such as Celebration Square continue to enhance the Downtown's role as a regional destination in the Greater Toronto Area. The Downtown Core is located within a major designated growth area and identified as an Anchor Hub in the Regional Transportation Plan. The Downtown Core continues to be a significant focus for investment in the Greater Toronto Area.

## 3.0 COLLABORATION

The Downtown 21 Master Plan process was the basis for reviewing the Local Area Plan policies of this Plan. The goal of this Area Plan is to establish a framework for the development of a vibrant, urban downtown that serves as the commercial, business and cultural centre of Mississauga and is a place where people choose to live, work and play. The public were engaged throughout the process in open houses, workshops and presentations and will

continue to be consulted as changes to the plans evolve.

## 4.0 VISION

A key goal of the City's Strategic Plan is to Create a Vibrant Downtown that will be the civic and cultural soul of the city as well as a strong economic centre.

The basis for the Downtown Core Local Area Plan is the Downtown21 Master Plan, which was received by Council in April 2010. The Downtown21 Master Plan sets the direction for transforming the Downtown Core from a suburban model to an urban mixed use centre.

The objective is to create a high quality, pedestrian friendly, human scaled environment that is a meaningful place for all citizens and also continues to attract lasting public and private investment in the Downtown to support existing and planned infrastructure, particularly higher order transit.

Additional investment is being made to transit in the Downtown, including the Light Rail Transit system along Hurontario Street and through the core. The Bus Rapid Transit system will also serve the Downtown Core connecting it with areas to both the east and west. The current focus for local and interregional public transit is in the area of Rathburn Road West and Station Gate Road consisting of the City Centre Transit Terminal and GO platforms. The future BRT station will also be constructed in this area. With the ongoing commitment to public transit infrastructure combined with transit supportive developments this area will develop as a key mobility hub within the Greater Toronto Area.

The existing road pattern is made up of super blocks. An expanded road network is proposed that will create an urban pattern of development blocks that are walkable in scale and well connected. The new road network will result in urban scaled blocks, providing routing options for vehicular, servicing and goods movement, pedestrian and cycling movement within the Core. The scale of the streets is to be narrower with special attention paid to the public realm and elements within the boulevard to ensure

a comfortable, enjoyable and safe pedestrian environment.

Downtown Mississauga has already begun its transformation from a suburban auto-oriented centre, to one that is truly urban, rich in character, memorable and geared to the pedestrian experience.

The Downtown which forms a part of the Urban Growth Centre is expected to be the focus for population and employment growth in the City to 2031. The Core is expected to maintain its prominence as the city's highest density mixed use centre. Going forward, it will be necessary to ensure a balance between residential and employment uses including retail, in order to achieve a sustainable downtown core.

## **4.1 Guiding Principles**

This Area Plan aims to support the following key opportunities for the Downtown Core:

- a. strengthen the Core's role as the primary location for major office, highest concentration of retail commercial, mixed use, civic and cultural uses;
- b. accommodate forecasted growth without impacting the natural environment and quality of life;
- c. attract new jobs, particularly in the office sector to balance population and employment;
- d. create a fine-grained, well connected road network that supports multi-modal transportation modes: walking, cycling, transit, servicing and goods movement, and the car;
- e. develop an integrated urban place that achieves design excellence in buildings, the public realm and a distinctive, memorable character in mixed use districts; and
- f. provide a new development framework and policy regime founded on greater predictability and certainty of outcomes, to better direct

growth and support existing and planned services.

The vision for the Downtown Core is to be a mixed use urban centre, with a balance of residential, office and retail employment uses, complemented by civic and cultural uses. Continued development of a variety of entertainment activities that will contribute to activity both day and night will be encouraged where appropriate.

Additional office development is critical to achieve a strong employment base and move the Downtown in a forward direction.

This Plan will better direct development, achieve a stronger economic base, strengthen the transportation systems, create linkages and access, improve urban design and enhance the pedestrian experience resulting in a more vibrant and active mixed use Downtown Core.

## **4.2 Community Concept**

The focus of these policies is to reinforce and enhance the image of the Downtown Core as a prominent destination and urban mixed use centre in the Greater Toronto Area. These policies also address the role of the Downtown Core as a community within Mississauga.

The Downtown Core's position as an employment, retail commercial, civic and cultural centre will be balanced by residential development, education, entertainment, and facilities for business visitors and tourists. New districts are proposed within the Downtown Core to support these existing and emerging activities. Each district will develop with its own character, mix of uses, function and scale. The policies of this Area Plan will serve to facilitate the dynamic growth and change of the Downtown Core as it matures.



## 4.3 Districts

The City will prepare detailed plans and policies for the various Districts identified on Schedule 1, Downtown Core Districts, each with its own identity, scale and character, through future amendments to this Area Plan. The following provides a general outline of the intended character for each District within the Downtown Core. The Districts are:

- Main Street
- Civic
- Confederation
- Rathburn
- Cleary Park
- Hurontario
- Square One
- Sussex

### 4.3.1 Main Street District

The Main Street District is envisioned as the heart of the Downtown Core, with a broad mix of active street level retail, restaurants, cafes, patios and entertainment facilities with a mix of uses above. The District will be a vibrant, walkable place that is active at all times of the day. The Main Street District is intended to bring together the commercial activity of the north, the Civic Centre district to the west, with the residential developments to the south. The District straddles Burnhamthorpe Road West, extending southward to Kariya Drive and northward to Square One Shopping Centre. The Civic District and Duke of York are the western boundary with Kariya Drive and the Office District being the eastern boundary.

The vision for this District is to create an active lively pedestrian oriented urban place in the heart of the emerging “downtown” that would serve as a model, catalyst and attractor for ongoing investment

in the larger area. Key objectives include the following:

- a. to create at its heart a “main street” that connects from Square One Shopping Centre across Burnhamthorpe Road West to the larger residential communities to the south of Burnhamthorpe Road West;
- b. to incorporate a broad mix of uses and users that would contribute to make this a vibrant part of the city fabric;
- c. to create a critical mass of at least 5,000 residents, 5,000 employees, with neighbourhood-oriented retail/restaurant uses; and
- d. to realize this vision in a relatively short time horizon.

### The Main Street Transit Station Area

4.3.1.1 The Main Street Station serves as a destination node for the Main Street and is shown on Schedule 3, Long Term Transit Network of this Plan. This station area will be a vibrant, walkable, mixed use place.

### 4.3.2 Civic Centre District

The Civic Centre District is a five block public campus and public open space that is home to the city’s premiere public institutions including the Civic Centre, Celebration Square, Central Library, Living Arts Centre, Scholars’ Green and the Sheridan College Campus. This district is envisioned as a singular civic space comprised of smaller parks and plazas and institutional uses, linked together by the pedestrian street design of Duke of York Boulevard, Living Arts Drive and the flush street design of City Centre Drive, Princess Royal Drive, Prince of Wales Drive and Square One Drive.

### Civic Centre Station Area

The Civic Centre Station Area serves as a destination node for the concentration of civic uses along Living Arts Drive including Celebration Square, Civic Centre, Central Library, YMCA, Living Arts Centre and Sheridan College. Shown on Schedule 3, Long Term Transit Network of this Plan, the station area will provide connection to the public open space network and serve a significant residential population in the Confederation Parkway District. Placemaking and wayfinding objectives will guide the development in this station area.

### 4.3.3 Confederation Parkway District

The Confederation Parkway District serves as the western gateway to the Downtown Core. This is a rapidly growing urban residential neighbourhood that will be home to over 20,000 people, centred along Confederation Parkway. The district will contain a series of new park spaces that will connect Zonta Meadows Park to the Civic Centre District. The Confederation Parkway District will serve as the northwestern gateway into the Downtown Core and will include a mix of uses which encourage a sense of place and pedestrian movement.

### 4.3.4 Rathburn District

The Rathburn District is currently a pattern of suburban retail uses and surface parking lots. This district will intensify over the long term with an emphasis on office and employment uses that can take advantage of highly designed, efficient, comfortable pedestrian amenities and access to future higher order transit. Visibility and access to Highway 403 and transit make this a prestigious employment district.

#### Rathburn Transit Station Area

4.3.4.1 The Rathburn Station Area is served by the LRT and Bus Rapid Transit and, is adjacent to the City Centre Transit Terminal. It connects with the future development north of Square One Shopping Centre along Rathburn Road. The station area is

characterized as an office district, with some entertainment uses.

### 4.3.5 Cleary Park District

This future neighbourhood extends west to Grand Park Drive and forms the south western gateway to the Downtown Core. The extension of John Cleary Park north to Burnhamthorpe Road will serve the future residential community and completes the Mary Fix Greenway connection, which will link this neighbourhood to the balance of the Downtown Core.

### 4.3.6 Hurontario District

The Hurontario District includes the Downtown's highest concentration of existing office uses, providing the foundation for new office development in the short term. The district has the potential to increase the amount of office space through new development and intensification of existing sites.

#### Robert Speck Station Area

4.3.6.1 This station serves the concentration of office at Robert Speck Parkway and Hurontario Street and future office development along City Centre Drive.

### 4.3.7 Square One District

The Square One District is home to the Square One Shopping Centre, one of the largest and most successful shopping centres in Canada. It is expected that the shopping centre will continue to draw people from across the city and the larger region. It is also expected that it will evolve, intensify and adapt to develop under-utilized surface parking lots. Office uses combined with ground floor retail and possibly residential on the upper floors are encouraged, particularly along City Centre Drive, Duke of York, Robert Speck Parkway and Square One Drive. In the process of intensifying, the connectivity of the shopping centre to the

surrounding area and the street network will be strengthened and a more urban built form will emerge.

Additions to Square One Shopping Centre will:

- be expressed in an outward fashion and designed to support a pedestrian friendly environment along the frontages;
- be designed in accordance with 'A' street frontage requirements when additions surround, have proximity or have frontage facing public or private open space;
- have the highest level of architectural expression, articulation and use of materials at the frontage;
- have active ground floor uses to animate the adjoining walkway area, street or public or private open space; and
- have ground floor uses that incorporate transparent windows and entrances that have direct access to the adjoining walkway, street, or public or private open space.

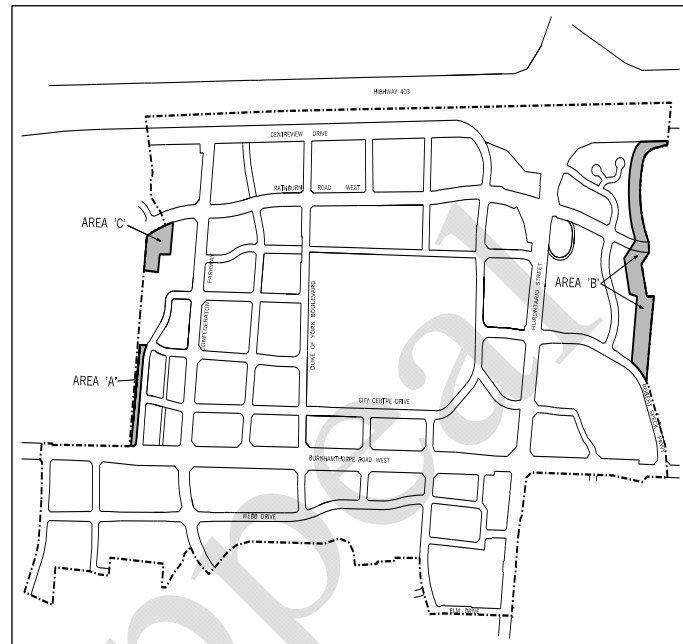
### 4.3.8 Sussex District

This is currently the densest, mixed use district in the Downtown Core and includes significant office and residential development. Some infill opportunities exist for additional office, ancillary uses, and residential development.

#### Matthews Gate Station Area

4.3.8.1 This station area at Hurontario Street and Matthews Gate serves the concentration of office and residential lands in the Sussex and Mississauga Valley Precincts. The station area will be designed to facilitate pedestrian connections among the Sussex Centre, Kariya Park, and the school board lands while also serving the needs of a dense residential area population.

### 4.3.9 Transition Areas



Map 2: Transition Areas

4.3.9.1 The Transition Areas will promote positive integration of building scale and intensity of development between the Downtown Core and abutting communities.

4.3.9.2 The design of the built environment should encourage:

- a. mid-rise built form adjacent to the Downtown Core boundaries and north of Burnhamthorpe Road West, with height transitions to a maximum of five storeys, on lands shown as Area 'A' on Map 2;
- b. low-rise built form adjacent to the Downtown Core boundaries, with height transitions from a maximum of two storeys at the Downtown Core boundaries to a maximum of eight storeys at the inner edge of the Transition Areas for lands abutting the easterly limit of the Downtown Core, shown as Area 'B' on Map 2 and lands immediately south of Rathburn Road West, shown as Area 'C' on Map 2;
- c. orientation of buildings enabling a sensitive integration with adjacent low density development;

- d. internalized service areas and parking to minimize impact on adjacent residential;
- e. pedestrian and bicycle links to adjacent communities; and
- f. appropriate setbacks and landscaping adjacent to the existing walkways.

## 5.0 DIRECT GROWTH

### 5.1 Introduction

The Downtown Core constitutes one of the four Character Areas within the greater Downtown, which is the City's Urban Growth Centre (UGC). The Downtown is intended to contain the highest densities, tallest buildings and greatest mix of uses. The entire UGC is planned to achieve a minimum gross density of 200 residents and jobs combined per hectare by 2031. The City will strive to achieve a gross density of 300 to 400 residents and jobs combined per hectare. The Downtown is also planned to achieve an average population to employment ratio of 1:1 measured as an average across the entire Downtown.

The existing gross density in the Downtown Core is approximately 210.7 residents and jobs combined per hectare. <sup>1</sup>Based on existing development and that under construction, the current population to employment ratio is 1.6:1.<sup>2</sup> It is estimated that existing planned and approved residential development will add up to 10,000 additional residents to the Downtown Core in the next decade.

With a gross density of 210.7 residents and jobs combined per hectare, the Downtown Core exceeds the minimum density target. As the Downtown Core will likely need to offset lower densities elsewhere in the Downtown, additional overall intensification is desirable.

Given the unlimited height and density permitted in the Downtown Core and prevailing market conditions, high density residential development has

predominated. Employment growth has not kept pace with residential development in the Downtown Core. The expected additional residential population will further skew the population to employment ratio. The challenge will be to meet the employment targets in order to achieve a sustainable balance between residents and employees. Additional employment which includes retail, office, public sector, institutional and other jobs will be required to provide more opportunities to live and work in the City and to support transit and other investments.

5.1.1 The City will monitor the gross density and population to employment ratio in the Downtown Core and assess its ability to accommodate further growth through the development approval process.

5.1.2 Increases in employment opportunities will be accommodated on lands designated mixed use, office and the non-residential component of high density residential development where applicable.

5.1.3 Strategies to encourage, incent and support employment uses may be pursued including consideration of Community Improvement Plans and other planning tools.

<sup>1</sup>Focus on Mississauga 2012, City of Mississauga

<sup>2</sup>Focus on Mississauga 2012, City of Mississauga

## 6.0 VALUE THE ENVIRONMENT

### 6.1 Parks and Open Spaces

Open space in the Downtown Core has a distinct character from other city parks with a focus on place making, social interaction and district identity. Open space and publicly accessible ground related amenity areas may take the form of civic gathering spaces, neighbourhood parks, naturalized spaces, public squares and private amenity areas. They should be accessible to people of all ages and abilities as facilities for passive enjoyment and urban life.

6.1.2.1 Public open space within the Downtown Core should be flanked by public roads to ensure they are open to all and not considered private spaces.

6.1.2.2 Public open spaces will be connected and continuous to form an 'Emerald Necklace' throughout the Downtown Core and connect the downtown to existing adjacent parkland such as Zonta Meadows Park, John Cleary Park, Civic View Walk and Kariya Park, and as greenbelt lands such as Bishopstoke Walk along Cooksville Creek. The size and configuration of all proposed parks will be subject to further review through a Local Area Plan update or through the review of individual development applications.

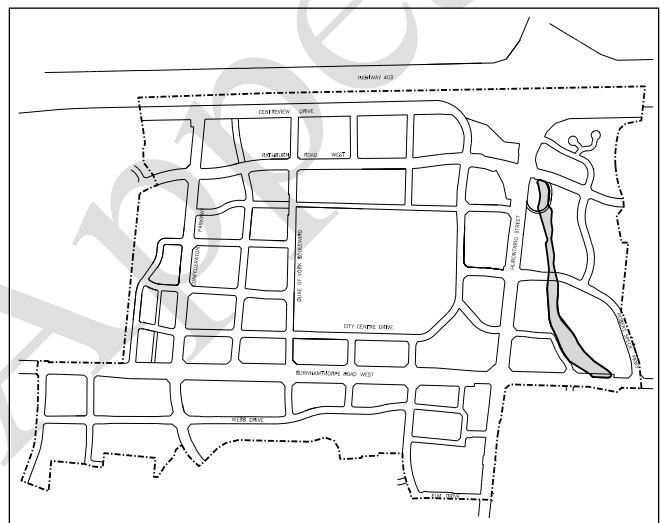
6.1.2.3 Stormwater retention and stormwater quality ponds are generally not appropriate uses for public parkland, however, in some instances overland flow pond stormwater facilities may be accommodated in public parkland.

### 6.2 Cooksville Creek Corridor

6.2.1 The Cooksville Creek Corridor as shown on Map 3 is recognized as a public amenity and resource. To protect, enhance and restore this natural feature, the design of the built environment will ensure:

a. appropriate setback of buildings from the corridor (landscaping only);

- b. improved visibility, access and linkage to the corridor from development parcels and public streets;
- c. views/informal surveillance to the corridor;
- d. integration of public/private open space and landscape areas;
- e. compatible positive relationship between the built form and the corridor;
- f. parking structures are designed and located to complement the corridor.



Map 3: Cooksville Creek Corridor

### 6.3 Stormwater Management

6.3.1 Any development within the Cooksville Creek watershed will require the implementation of on-site stormwater management techniques to control the 100 year post development stormwater discharge to 2 year pre-development levels.

6.3.2 Development applications will follow the Green Development Standards as they relate to storm water management measures. All practices will be designed and implemented to the satisfaction of the City and CVC Authority.



## **7.0 Complete Communities**

### **7.1 Introduction**

The Downtown Core contains many elements of a complete community including:

- a mixture of land uses and an assortment of activities such as retail stores, employment uses, community and facilities, parks and public transit;
- a mixture of housing forms and densities;
- cultural and educational resources such as the Living Arts Centre, the Mississauga Art Gallery, the Central Library and Sheridan College; and
- opportunities for people to engage in community life through a high quality public realm in the Civic District, vibrant cultural activities and meeting facilities.

7.1.1 In order to achieve a complete community in the Downtown Core, Mississauga will guide development toward:

- a compact urban form that is walkable and provides convenient access to a variety of land uses and public transit;
- a range of housing options;
- additional cultural resources, community infrastructure and educational institutions; and
- opportunities for enhancing the public realm.

### **7.2 Community and Cultural Infrastructure**

Community and cultural infrastructure in the Downtown Core is unique in that it functions both as a city wide and sometimes regional draw as well as serving the local community who live in the core. The Downtown contains a significant concentration of distinctive cultural resources such as facilities, buildings and events which represent a cultural

infrastructure cluster considered to be a cultural node.

7.2.1 Mississauga will encourage partnerships and collaboration to further develop community and cultural infrastructure resources in the Downtown Core.

### **7.3 Distinct Identity and Character**

The Downtown Core exhibits a distinct identity and character from the rest of the city.

7.3.1 Opportunities to further the significance of the Downtown Core through the design of the road and transit network, the public realm and built form will be addressed through the review of development applications and City initiatives.

## **8.0 CREATE A MULTI-MODAL CITY**

The Downtown Core will accommodate all modes of transportation to enable the movement of people and vehicles to, from and within the Downtown Core.

### **8.1 Multi-Modal Network**

A multi-modal transportation system is key to ensure the efficient movement of people and goods within the Downtown Core. Arterial roads, Highway 403 and a system of local collectors and roads facilitate the movement of vehicles, transit, cyclists, pedestrians and goods into, out of and within the Core.

### **8.2 Road System**

8.2.1 All roads shown on the Downtown Core Long Term Road Network and Classification, Schedule 2 will be public. The design, access requirements and public/private responsibilities will be determined through the development application process or through a Local Area Plan update.

8.2.2 Minor adjustments to the basic rights-of-way for minor collector roads and local roads may be made without an amendment to this Plan subject to

the City being satisfied that the role and function of such roads are maintained.

8.2.3 Below-grade encroachments into the road system are generally not permitted. On an exception basis, limited encroachment into the road allowance may be considered by the City, where the City is satisfied that its servicing, streetscape, transportation and design objectives are not compromised.

8.2.4 Additional rights-of-way will be required for the proposed roundabouts identified on Schedule 2, Downtown Core Long Term Road Network and Classification.

8.2.5 As traffic volumes increase, dedicated transit lanes may be necessary.

8.2.6 The proposed Highway 403 North Collector (Northern Distribution Road) , located north of the Downtown Core, is intended to provide access to and from the Downtown Core road network.

8.2.7 Centre View Drive is intended to provide access to eastbound Highway 403 via a link east of City Centre Drive.

8.2.8 A future functional feasibility study will be undertaken to review the north collector, Centre View Drive link to Highway 403 and other road or ramp proposals contained in the Downtown21 Master Plan which may impact Highway 403.

8.2.9 Cycling facilities will be incorporated per the Mississauga Cycling Master Plan. The City may in the future identify secondary cycling routes to be integrated with the design of the public realm.

## 8.3 Parking and Transportation Demand Management

8.3.1 The City will undertake a strategy to evaluate the provision of parking in the Downtown.

8.3.2 Parking for new development will be accommodated in below ground or above ground structures.

8.3.3 Surface parking lots for new development will not be permitted.

8.3.4 Mississauga will encourage Transportation Demand Management measures as part of development applications within the Downtown Core.

## 8.4 Anchor Hub

8.4.1 The Downtown Core will be served by a **higher order transit** system that provides service within the Downtown with connections to other parts of the City, neighbouring municipalities and interregionally as identified on Schedule 3, Downtown Core Long Term Transit Network.

8.4.2 An **Anchor Hub** is designated at the interchange of the Bus Rapid Transit Corridor and the Hurontario Light Rail Corridor. The exact location will be determined through a future study.

8.4.3 As part of the proposed Mississauga Bus Rapid Transit (BRT) facility, the existing transit terminal may be modified to serve future development growth and to connect with the BRT facility. In the long term, an additional BRT station is also proposed near the intersection of Rathburn Road West and City Centre Drive.

8.4.4 Transit facilities will be designed to serve the city wide transit network; internal transit services; the BRT facility, interregional transit systems; and the pedestrian system linking developments, Square One Shopping Centre and cultural facilities.

8.4.5 **Higher order transit** stations will be situated within the Downtown Core.

8.4.6 The precise location of **higher order transit** stations within the areas illustrated above will be determined through the Hurontario Light Rail Transit (LRT) Project.

8.4.7 Within transit station areas, the City will require that development applications address Transit Oriented Development (TOD) objectives. These include moderate to high density

development, comprised of a mix of uses and designed with an emphasis on the pedestrian experience and opportunities to maximize transit ridership.

8.4.8 Development applications will be supported by studies that include a review of the land use and urban design components of the station area as a whole, including surrounding privately held lands, the configuration and design of intersections and other pedestrian design elements within the public realm.

8.4.9 Ground level retail development will be required within Transit Station Areas to activate the area and street.

8.4.10 Parking will be managed carefully within Transit Station Areas. The City will consider reducing parking requirements within Transit Station Areas.

8.4.11 A distinct character for each Transit Station Area will develop, guided by the District character descriptions of this Area Plan. The City may also develop Transit Station Area Plans as a basis for guiding and assessing development applications in Transit Station Areas.

8.4.12 Development will address the ease and comfortable movement of people between transit modes and to other destinations within the Downtown Core, reinforcing this area as an anchor hub.

## **9.0 BUILD A DESIRABLE URBAN FORM**

### **9.1 General**

Vibrant downtowns are diverse places, rich in character, experiences and complexity. To achieve a desirable urban form in this context, downtowns require a coordinating framework to create predictability in the built environment, to ensure highly accessible and social places that are healthy,

comfortable, attractive, dynamic, memorable and geared to the pedestrian experience.

The intent of the urban design policies of this Area Plan is to provide direction and define principles for the physical design of the Downtown Core. Urban form including the location of streets, mix of uses, a high standard of urban design, streetscape design, and the development and enhancement of a comprehensive public/private system of linkages and open space, including public art will be achieved in the Downtown Core through the following:

- a fine-grain grid network of urban scale land blocks and streets that supports transit and a pedestrian oriented Core;
- the highest standard of design in the public and private realms that establishes a sense of place and civic identity, seamlessly integrating to produce a prominent high quality Downtown Core environment;
- a connected and well designed urban open space system that provides opportunities for public gathering and passive outdoor recreation;
- well defined skyline elements, views, gateway treatments and landmarks, distinctive character areas, districts and neighbourhoods, open space, high quality landscapes and streetscape treatment, public art and signage;
- a compact built form that has a high level of physical continuity and cohesion between buildings, from block to block, and from street to street;
- buildings that generate a high degree of animation, enabling active ground floors, vibrant streets and walking;
- the incremental transition of large surface parking lots into more intensive, urban scale development incorporating structured parking that supports the built form and multi-modal objectives of this plan;

- linkages that ensure compatible integration, transition and contextual fit with the character and scale of adjacent communities; and
- ensuring that development combines high quality design, scale, massing and the use of materials together with landscape and streetscape to create a high standard of urbanity.

## 9.2 The Public Realm

The public realm consists of those areas that are shared and accessible to the public such as streets, boulevards, squares and open space. It is considered a significant asset in achieving a desirable urban form.

Elements of the public realm also include pedestrian paths, cycling amenities, bridges, plazas, transportation hubs, gateways, natural features, view corridors and public landmarks. Together, these convey the image, identity and character of the Downtown Core setting.

### 9.2.1 Streets and Blocks

A fine-grain grid and network of streets and blocks is fundamental to this Plan and will serve as the framework for achieving urbanism.

A great benefit of a well connected network of streets and small blocks is that it makes a downtown understandable to residents and visitors. There is order and clarity to the downtown's organization if its streets connect and allow intelligible ways to conceive the downtown and travel within and between places. Streets in the Downtown Core will also be defined by public and private buildings and open space to create an important sense of enclosure and outdoor 'rooms' that function as public spaces themselves.

They serve pedestrians, transit, vehicles, cycling and provide space for public utilities and services, trees and landscaping, building access, framing of views, and access to sky views and sunlight.

9.2.1.1 The Downtown Core will be developed to:

- a. create a fine-grain grid of streets;
- b. provide high quality designs for streets and intersections that achieve, definition, enclosure and comfort for pedestrians and street life;
- c. accommodate on-street parking and cycling amenity in public rights of way, where feasible; and
- d. incorporate coordinated street furniture, street trees, lighting systems and traffic signage appropriate to the character of the Downtown Core.

## 9.2.2 Boulevards and Sidewalks

9.2.2.1 Boulevards and sidewalks provide an important framework for pedestrian movement, connectivity and street life. As public amenities, they integrate the special districts within the Downtown Core and provide connectivity to adjacent communities. The design of boulevards in the Downtown Core will:

- a. incorporate coordinated design themes, high quality materials, street trees and landscaping;
- b. be designed as active, animated public spaces that seamlessly integrate with buildings and other components of the public realm to define the street space;
- c. maximize opportunities to incorporate places to meet, sit and socialize; and
- d. achieve pedestrian comfort, weather protection



Figure 1: Boulevards incorporate street trees for rhythm and definition, along with generous sidewalks and pedestrian scale lighting for comfort and safety.

and safety, particularly at transit stops.

## 9.2.3 Streets and Frontage Categories

9.2.3.1 A hierarchy distinguishes the function and attributes of streets in the Downtown Core. Existing and proposed roads in the Downtown Core are identified for the purpose of:

- a. establishing 'A' and 'B' Street Frontage categories that identify:
  - streets having the highest pedestrian character; and
  - where vehicular access to development for parking, loading and servicing will occur on street frontages;
- b. identifying urban design policies that articulate the intent of the street frontage categories; and
- c. specifying Built Form Standards that provide direction and detailed development guidance in relation to the urban design policies and frontage categories.

## 9.2.4 Gateways, Routes, Landmarks and Views

9.2.4.1 The Downtown Core is characterized by a number of landmark buildings and sites such as the Civic Centre, the Central Library, the Living Arts Centre, Celebration Square and the Absolute 'Marilyn' buildings. As visual symbols of the Downtown Core, they create memorable experiences for local residents, workers and visitors alike.

9.2.4.2 The development of landmark buildings in the Downtown Core will be encouraged on sites having the following attributes:

- high visibility;
- major views and vistas toward the site;
- unique topographic characteristics;
- gateway locations and intersections; and



- where the proposed use includes a major destination.

9.2.4.3 The development of future buildings will ensure that important views along Burnhamthorpe Road and Hurontario Street, and views towards the Civic Centre District are preserved.

### 9.2.5 Open Space and Amenity Areas

Open Space and Amenity Areas in the Downtown Core will:

- consist of a variety of open spaces, parks, squares, plazas, naturalized green areas and publicly accessible ground related private amenity areas;
- positively contribute to the image and character of the Downtown Core with high standards and quality materials;
- reinforce and frame a view, a significant building or terminate a vista, where appropriate; and
- provide connections to the larger pedestrian and cycling network in the Downtown Core and adjacent areas, where appropriate.

### 9.2.6 Transit and Active Transportation

To achieve the transit and active transportation objectives of this plan, development will need to interface seamlessly with the public realm by incorporating design elements that integrate with proposed transit stops and stations.

Design of the built form, streetscape and landscape areas will:

- integrate transit stations and bus stops with development;
- provide walkways from transit stops to main front building entrance(s); and
- incorporate weather protection in waiting areas;

## 9.3 Buildings and Site Development

### 9.3.1 Buildings

Buildings are the most pronounced element of the urban fabric and create the sense of place. Buildings in the Downtown Core will shape and articulate the streets and open spaces by forming edges and streetwalls to establish definition and enclosure. Collectively they create the pedestrian environment, frame the public realm and establish the urban setting.

9.3.1.1 The design, location and character of buildings in the Downtown Core will:

- have regard for scale and enclosure of the public street;
- mitigate the perceived mass of large buildings and long frontages by:
  - using special massing to articulate the built form; and
  - providing a change in materials, textures, patterns, colours and details to create a sense of smaller scale buildings;
- design corner lot buildings with special massing and architectural treatments on both streets to give prominence along the frontages and visually distinguish these sites;
- prohibit blank building walls abutting the street; and
- have buildings that deploy high quality, resilient and durable materials such as stone, clay brick, wood, glass and metals.

### 9.3.2 Relationship to the Public Realm

The Downtown Core will evolve with built form that contains the streets with well designed buildings and groups of buildings to create a predictable pattern of development, and define the public realm and open space system.

General policies to establish how buildings interface with the public realm apply to all streets in the Downtown Core. Specific policies articulate the role of 'A' and 'B' Street Frontages and provide further direction on access and servicing of development blocks, at grade retail, residential uses, buildings facing open space and structured parking.

9.3.2.1 To create a desirable relationship with the public realm, buildings will be subject to the following:

- a. locate streetwalls of buildings within build-to areas on development blocks at the build-to line to contain the street and provide enclosure;
- b. coordinate build-to lines with adjacent properties in order to create consistent edges and street walls along frontages;
- c. ensure a variation in setbacks along the building frontages to articulate façade emphasis at the build-to line, thereby allowing for visual interest, outdoor patios, recessed entries and landscaped areas;
- d. design active ground floor uses to animate the public realm as an extension of the pedestrian environment;
- e. incorporate transparent windows in ground floor uses;
- f. locate functioning main front entrances to buildings so that they are clearly identifiable and prominent with direct access to the public sidewalk, pedestrian connections and transit facilities;
- g. design and locate lobby, retail and commercial entrances so that they are flush with the sidewalk elevation; and
- h. design and locate utilities, exhaust vents and municipal services to minimize negative impacts on the public realm.

### 9.3.3 Frontage Provisions

#### 9.3.3.1 Role of 'A' & 'B' Streets

All existing and new roads are categorized as 'A' and/or 'B' Streets and are shown on Schedule 4, Downtown Core A and B Street Frontage of this Area Plan. The categories differentiate the streets by their role and function, character, built form treatments and overall design. They underpin the Downtown Core's urban vision and implement the urban design and public realm objectives of the Downtown21 Master Plan.

'A' Streets have a critical role and function in the Downtown Core. They are the most important for securing animation and character, a comfortable pedestrian environment with access to sunlight and sky views, street activity and vibrancy. Development fronting onto 'A' Streets will require the greatest attention to urban design, ensuring a cohesive built form and streetscape treatment to achieve the highest standard in the public realm.

'B' Streets allow for site servicing and access, but also act to support street activity, a pedestrian friendly environment and a high quality built form and streetscape treatment.

New development will follow specified 'A' and/or 'B' street standards for each street in the Downtown Core. The site organization and character of built form varies according to the 'A' or 'B' Street frontage category as follows:

### 9.3.3.2 Provisions for 'A' Streets

9.3.3.2.1 Development fronting onto 'A' Streets will:

- a. have continuous buildings along development blocks to provide a continuity of built form from one property to the next;
- b. provide functioning main front entrances to buildings on 'A' streets; and
- c. be prohibited from locating curb cuts, driveways and laneways on 'A' streets.



Figure 2: Buildings on 'A' Streets are continuous along the length of development blocks and integrate seamlessly with the public realm to produce lively, animated, pedestrian oriented frontages.

### 9.3.3.3 Provisions for 'B' Streets

9.3.3.3.1 Development fronting onto 'B' Streets will:

- a. have buildings along development blocks, with provision for vehicular access to off-street parking, access for deliveries, garbage pick-up, servicing and loading; and
- b. provide functioning main front entrances on 'B' streets.

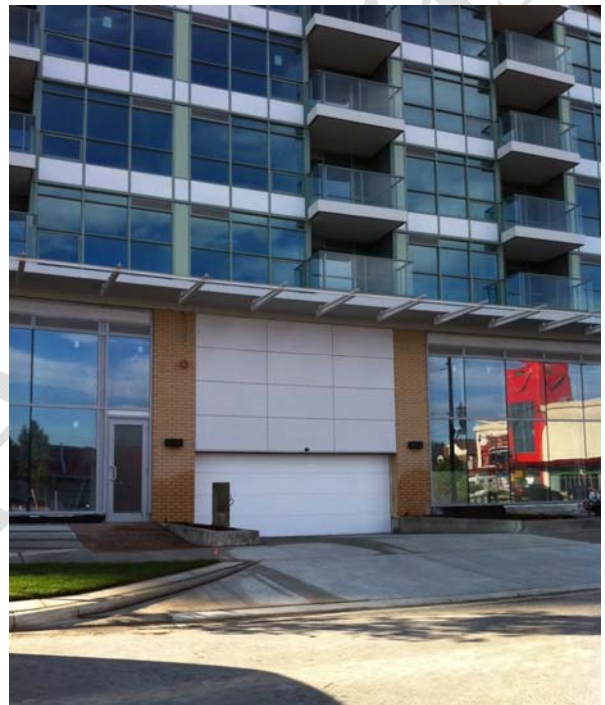


Figure 3: 'B' Streets allow for vehicular access to off street parking in addition to deliveries, garbage pick-up, servicing and loading.

### 9.3.3.4 Exceptions to Access Provisions

9.3.3.4.1 The following exceptions will apply to all Streets in the Downtown Core:

- a. curb cuts, driveways and laneways may be located on 'A' streets provided that:
  - a site or block does not have access from a 'B' street, or if there is a requirement for emergency vehicle access; and
  - impacts on the pedestrian environment are minimized through a high standard of design treatment consistent with the quality of the public realm;
- b. where a development block has an 'A' and 'B' Street frontage, the most prominent building entrance will be located on the 'A' Street frontage; and
- c. corner entrances may be required where development is located at corner sites or prominent street intersections.

### 9.3.3.5 Buildings with At Grade Retail

9.3.3.5.1 Development incorporating at grade retail frontages will be designed so that:

- a. the street is lined with continuous active retail uses;
- b. storefronts are expressed with a narrow width and frequent rhythm that supports the



Figure 4: Storefronts collectively incorporate a strong sense of rhythm and pattern across the building frontage, assisting pedestrians with spatial perception, orientation and accessibility.

pedestrian experience at the street level;

- c. uses will be directly related to the public sidewalk and street level, and will incorporate operating entrances, doorways and storefront windows oriented to the street, along with signage and lighting to provide animation, interest and variety in the streetscape;
- d. storefronts incorporate the highest standard of materials and design with clear transparent windows and doors that provide clear views into and out from ground floor uses;
- e. such uses will incorporate fixed canopies, awnings, cantilevers, or similar features that are architecturally compatible with the design of the building in order to achieve pedestrian scale, comfort and weather sheltered pedestrian routes; and
- f. where appropriate, patios and outdoor amenity space should be provided adjacent to retail and commercial entrances to promote activation and street vibrancy.

### 9.3.3.6 Residential Entrances Located On Retail Activation Streets

9.3.3.6.1 Where areas of retail activation have been identified in accordance with Schedule 5, Retail Activation of this Area Plan, main front entrances and lobbies providing access to residential uses above the ground storey will be:

- a. limited in width;
- b. located to establish and reinforce patterns of access between blocks of development; and
- c. positioned so that there is minimal interruption of retail units along the block.



### 9.3.3.7 Residential Uses

9.3.3.7.1 Development incorporating residential uses at grade or expressed in townhouse form will be designed to:

- a. provide functioning, individual entrances to residential units and to animate the frontage with windows on the ground floor that look out onto the street;
- b. create a high quality interface and transition between the public sidewalk and front door to ensure adequate separation, definition and privacy;
- c. provide high quality landscape architecture treatments within frontages, transition zones and setbacks; and
- d. consider designs that allow for the potential conversion of ground floor residential uses to commercial and retail uses over time.



Figure 5: Transition areas between the front doors of residential uses and the public sidewalk incorporate landscaping and architectural treatment.

### 9.3.3.8 Buildings Facing Open Space



Figure 6: Buildings incorporate high quality materials and articulated facades, framing open space to create green, robust and attractive park settings.

9.3.3.8.1 Buildings that surround and face parks and open space will be designed according to 'A' street frontage requirements and will have the highest level of architectural expression, articulation and use of materials.

### 9.3.3.9 Private Open Space

Private open space may be incorporated into new developments along 'A' or 'B' Street frontages in part or whole, provided that:

- the design of private open space will integrate seamlessly with the adjoining street network, pedestrian environment and overall open space system;
- the design of the private open space contributes positively to the identity of the Downtown Core;
- the private open space interfaces with existing and/or proposed development in a legible and cohesive manner;
- the design of the private open space is executed to the highest urban design and landscape architecture standards;
- private open space is accessible, safe and is being provided for the use and enjoyment of the



general public on a year-round, seasonal and 24 hour basis.

#### **9.3.3.10 Structured Parking**

9.3.3.10.1 Structured parking will be designed so that:

- a. structured parking and vehicular access will be located to minimize impacts on the property and on surrounding properties and will improve the safety and attractiveness of adjacent streets, parks and open spaces;
- b. above grade parking structures will not directly front onto 'A' streets, but will be entirely screened by liner buildings incorporating a mix of uses between the parking structure and street space;
- c. above grade parking structures will have active uses on the ground floor such as retail with an appropriate scale and architectural expression to support activity on the streets, parks and/or open spaces;
- d. above grade parking structures will only directly front onto one 'B' street, where more than one 'B' street frontage exists;
- e. where facing 'B' streets, above grade parking structures and their facades will be designed to the highest level of architectural treatment and façade animation to mask the parking and screen views of the interior; and
- f. entrances, lobbies and passageways that provide a convenient means of pedestrian access to parking facilities from the sidewalk will be enclosed, limited in size, appropriately signed and integrated into the façade design without appearing as dominant elements on the street frontages.

#### **9.3.3.11 Exceptions to Frontage Provisions**

9.3.3.11.1 The City at its discretion may consider exceptions, in part or in whole, to the frontage provisions where:

- a. the City has determined that there are extenuating site constraints; and/or
- b. where development proposals are able to demonstrate urban design excellence.

## **9.4 Buildings**

To achieve a variety of character and diversity in built form, a wide range of building forms will be encouraged across the Downtown Core to include the following:

### **9.4.1 Low-Rise Buildings**

9.4.1.1 Low-rise buildings (3 - 5 storeys) will be designed to provide a minimum of 3 storeys both at the street frontage and across the entire building area.

### **9.4.2 Mid-Rise Buildings**

9.4.2.1 Mid-rise buildings (6 – 12 storeys) will be designed to:

- a. incorporate special massing, setbacks, pedestrian perception lines, and articulation at the streetwall and upper storeys to contain the street or public space, and support a pedestrian scale; and
- b. mitigate the impacts of wind, shade and shadow and demonstrate how proposals adhere to the City of Mississauga Standards for Shadows Studies.

### 9.4.3 Tall Buildings

Tall buildings characterize a good part of the Downtown Core and define the city pattern and urban structure. To ensure high quality of life, they must be designed to enhance the pedestrian environment, provide access to natural light, sky views and privacy for residents, employees and visitors to the Downtown Core.

#### General

9.4.3.1 Tall building proposals in the Downtown Core will be designed to:

- a. encourage a variation in heights on large development parcels; and
- b. integrate with adjacent buildings to ensure a seamless interface within blocks of development.



Figure 7: Tall buildings articulate the city pattern and urban structure, while visually reinforcing the civic importance of the Downtown Core and the image of the City as a whole.

## 9.4.4 Form of Tall Buildings

9.4.4.1 Tall buildings in the Downtown Core will be designed and massed in the form of a podium middle shaft/tower and top.

9.4.4.2 The Podium will be designed to:

- have a height that is generally equal to the width of the right of way, but not less than 3 storeys;
- incorporate a street wall placed at the build-to-line, having a minimum height of 3 storeys; and
- incorporate step backs at the upper storeys to articulate the massing in combination with the streetwall.

9.4.4.3 The middle shaft/tower will be:

- located in relationship to the Podium and adjacent buildings;
  - designed to maintain generous spatial separation between towers to maximize access to sky views, natural daylighting, and adequate privacy;
  - have a limited floor plate size;
  - articulating the floor plates to break down the mass of the building; and
  - create street interest and enhance skyline character.
- c.

9.4.4.4 The tower top, consisting of the upper floors of a tall building, will be designed to:

- achieve a distinctive skyline profile; and
- use materials, finishes and patterns that are consistent with the overall building design and architectural expression.

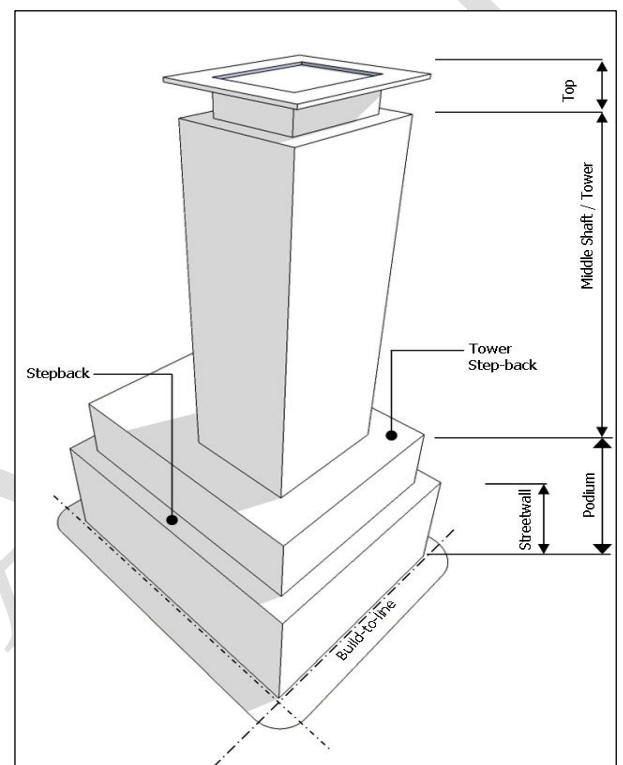


Figure 8: Massing of Tall Buildings

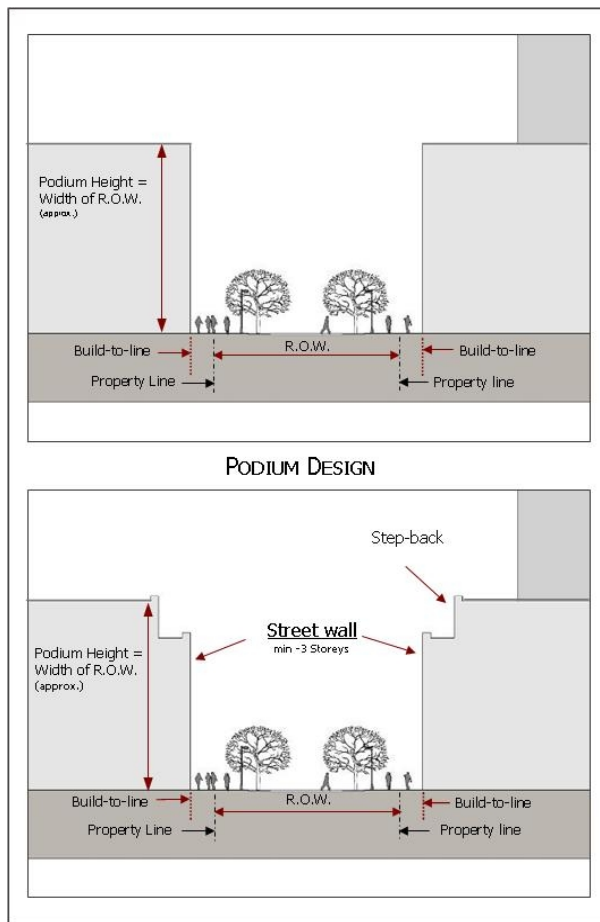


Figure 9: Tall buildings will incorporate a Podium that is generally equal to the width of the right of way. Streetwalls will also be incorporated into Podiums to contain the street, assist with pedestrian scale, sunlighting provisions on the public realm and mitigate the overall height of podiums.



Figure 10: Tall buildings in Downtown Core will be massed in the form of the podium, middle shaft/tower and top.

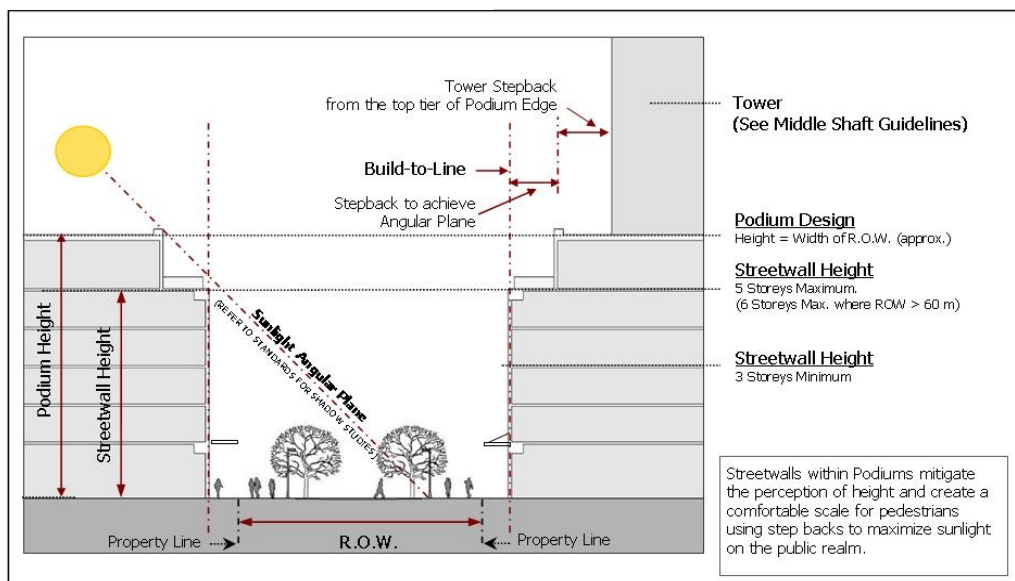


Figure 11: Podiums and their streetwalls will be designed with stepbacks at the upper storeys to provide light filled sidewalks and boulevards in the Downtown Core.

### **9.4.5 Parking, Servicing and Loading**

9.4.5.1 Parking facilities to support new development will:

- a. be accommodated in below-grade or above grade structured parking;
- b. identify access from the street;
- c. encourage shared parking between developments where appropriate;
- d. incorporate walkways, traffic islands and pedestrian refuges as integral components of parking facilities; and
- e. have recognition of the needs of cyclists.

9.4.5.2 Service, loading and garbage storage facilities will be located internally within buildings.

### **9.4.6 Signage**

9.4.6.1 The design, location, size and character of building signage will:

- a. be visually integrated with development as an extension of the building's architectural expression; and
- b. ensure compatibility of scale and character of the signage with the building and the context of the site.

### **9.4.7 Site Development**

9.4.7.1 Development proponents will be required to upgrade the public boulevard and contribute to the quality and character of streets and open spaces by providing:

- a. street trees and landscaping and relocating utilities, if required;
- b. lighting;
- c. weather protections elements;
- d. screening of parking areas;

- e. bicycle parking;
- f. public art;
- g. street furniture; and
- h. sustainable design elements

## **10.0 FOSTER A STRONG ECONOMY**

The Downtown Core is important to the economy of the City of Mississauga. Square One Shopping Centre contains the primary retail concentration in the City and is a strong regional economic draw within the Downtown and the city as a whole. The further development of post-secondary education, cultural, entertainment, tourist and business activities including retail and major offices in the Downtown will contribute to the economic success and sustainability of this area.

Higher order transit initiatives will also support the development of the downtown as the economic centre of the city and a prominent regional destination.

10.1 Mississauga will promote collaboration through public private partnerships to achieve office development, post-secondary educational institutions and higher order transit infrastructure within the downtown.

10.2 Mississauga will consider a range of planning tools to achieve these objectives.

10.3 Mississauga will encourage development and other activities that will expand business opportunities and tourism in the region.



## 11.0 LAND USE

### 11.1 General Land Use

#### 11.1.1 Uses Permitted in all Designations

11.1.1.1 Notwithstanding the Uses Permitted in all Designations policies of this Plan, the following additional uses will also be permitted:

- a. **cogeneration** facility will be permitted as an accessory use; and
- b. commercial parking facility.

#### 11.1.2 Residential

11.1.2.1 Residential consists of the following designation:

- a. Residential High Density.

11.1.2.2 Special needs housing will be in a built form consistent with the dwelling forms permitted by the residential designation.

#### 11.1.3 Mixed Use

11.1.3.1 Notwithstanding the Mixed Use policies of this Plan, the following additional uses will also be permitted:

- a. horizontal multiple dwellings accessory to an apartment dwelling;
- b. hospital;
- c. long term care dwelling;
- d. major office;
- e. residential dwellings associated with an institutional use; and
- f. retirement dwelling.

11.1.3.2 Special needs housing will be in a built form consistent with the dwelling forms permitted by the residential designation.

11.1.3.3 The following uses are not permitted:

- a. convenience restaurant with drive-through facility; and
- b. motor vehicle sales and rental facility.

11.1.3.4 Townhouses will be permitted in Transition Areas B and C as shown on Map 2, Transition Areas of this Area Plan.

#### Retail Activation

11.1.3.5 Existing and new streets in the Downtown Core have been identified on Schedule 5, Retail Activation of this Area Plan for the purpose of:

- a. establishing retail objectives within mixed use buildings;
- b. identifying districts and areas of focus for which retail has been determined to be a strategic priority in the Downtown Core;
- c. setting out requirements for the amount of at grade retail to be achieved on street frontages in accordance with the Schedule 5 Retail Activation included in this Area Plan; and
- d. identifying permitted retail uses as outlined below.

11.1.3.6 Notwithstanding the Mixed Use designation, only the following uses will be permitted in units on the ground floor of buildings fronting a Retail Activation street:

- a. financial institution;
- b. personal service establishment;
- c. restaurant; and
- d. retail store.

11.1.3.7 Notwithstanding the Mixed Use designation, future development will accommodate linkages that will form part of an extensive system of linked parks and open spaces. The provision of

linkages may be in the form of boulevards, plazas and park spaces which will be made accessible to the public. The location and size will be determined in conjunction with development. In order to achieve open space of acceptable size, configuration and location, the City will consider how the proposed open space enhances connectivity between parks and the Districts. Prior to the approval of any draft plan of subdivision, severance or any other development application within this designation, property owners will demonstrate to the satisfaction of the City, how open space objectives within this area will be satisfied through the submission of development and open space concept plans.

11.1.3.8 Notwithstanding the Mixed Use designation of lands located on the south side of Rathburn Road West, between Hurontario Street and the future extension of Duke of York Boulevard, future development will accommodate one or more areas of public parkland and will form part of an extensive system of linked parks and open spaces. The location, number and size of the park block(s) will be determined in conjunction with the development of lands in this area. In order to achieve parkland of acceptable size, configuration and location, the City will encourage consolidation of parkland of multiple sites within the Area, either through dedication or acquisition, having consideration for matters including the proximity to

residential uses and to the transit hub.

Prior to approval of any draft plan of subdivision, severance or any other development application within this area, property owners will demonstrate to the satisfaction of the City how the City's parkland objectives within this area will be satisfied, through the provision of development and parkland concept plans. Notwithstanding the provision of public recreational facilities provided through the registration of Plan M-1010, prior to final approval of any severance, subdivision or any other development application within this area, property owners will dedicate sufficient parkland or enter into appropriate agreements with the City and other owners of lands to secure the provision of adequate parkland from other development parcels within the area.

#### **11.1.4 Office**

11.1.4.1 Notwithstanding the Office policies of this Plan, the following additional uses will also be permitted:

- a. hospital; and
- b. post-secondary educational facility.



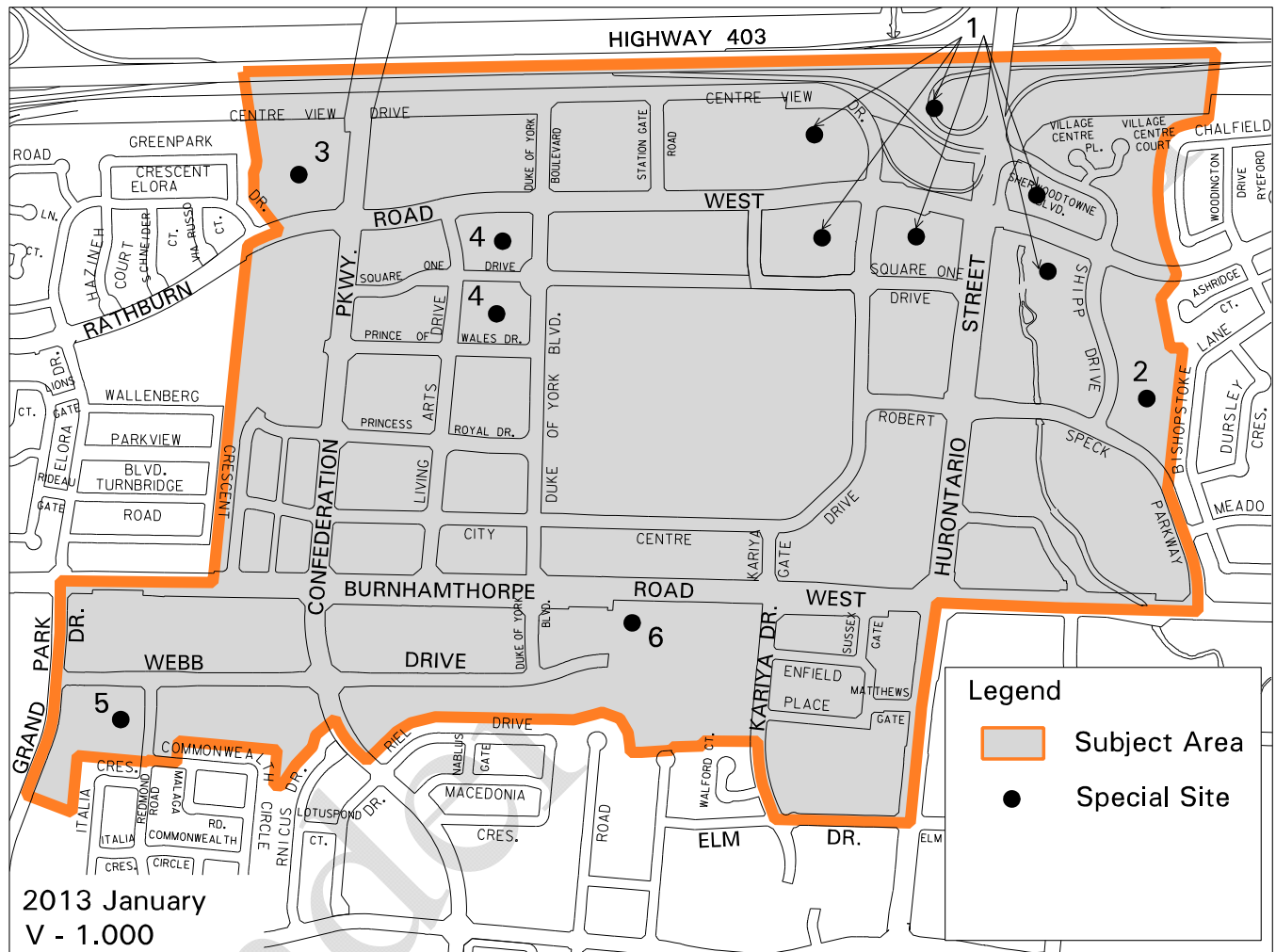
Figure 12: Retail Streets in the Downtown Core will contribute to area vibrancy, economic vitality and life on the streets.

11.1.4.2 Notwithstanding the Office policies of this Plan, the total Gross Floor Area of accessory uses will not be limited.

11.1.4.3 Notwithstanding the Office policies of this Plan, development adjacent to and the proposed east-west street north of Rathburn Road West, west of City Centre Drive to Living Arts Drive will serve as an important linkage in the parks and open space system. Future development along this corridor will be designed to provide a high quality pedestrian experience along boulevards and through private and public open spaces such as plazas and parks. Notwithstanding the provision of public recreational facilities provided through the registration of Plan M-1010, prior to final approval of any severance, subdivision or any other development application within this area, property owners will dedicate sufficient parkland or enter into appropriate agreements with the City and other owners of lands to secure the provision of adequate parkland from other development parcels within the area.

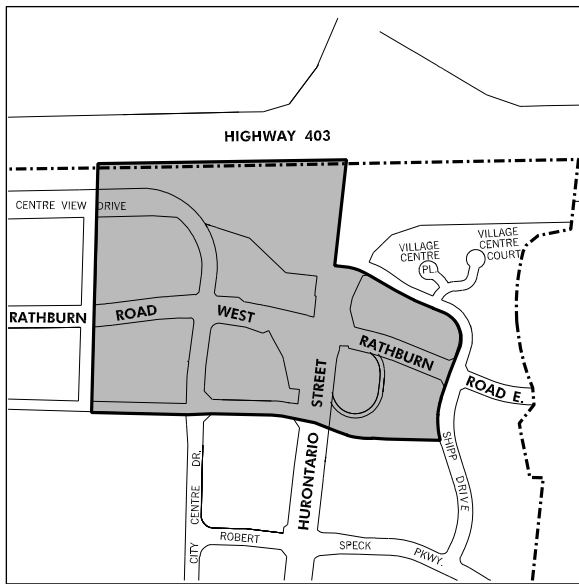
## 12.0 SPECIAL SITE POLICIES

There are sites within the Downtown Core that merit special attention and are subject to the following policies:



Map 4: Location of Special Sites within the Downtown Core Local Area Plan

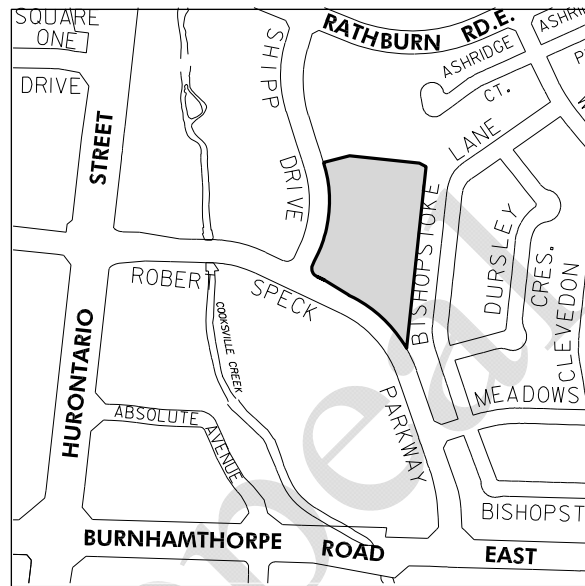
## 12.1 Site 1



12.1.1 The lands identified as Special Site 1 are located in the four quadrants adjacent to the Hurontario Street and Rathburn Road intersection.

12.1.2 Prior to the finalization of the road network for this site area, the appropriate transportation studies will be carried out in conjunction with the Provincial Government.

## 12.2 Site 2



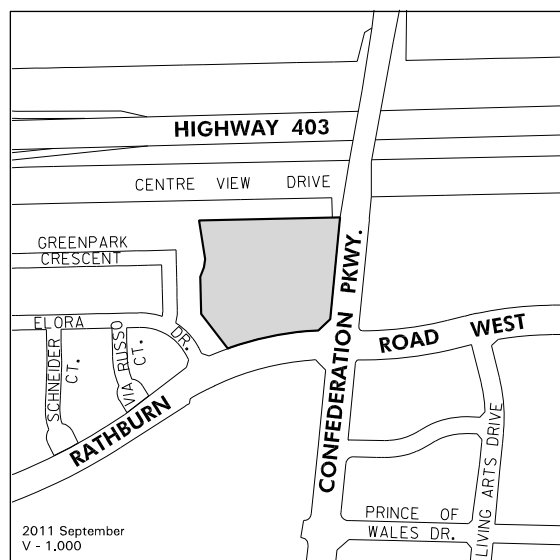
12.2.1 The lands identified as Special Site 2 are located at the northeast quadrant of Robert Speck Parkway and Ship Drive.

12.2.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. townhouses will be permitted on the entire site.



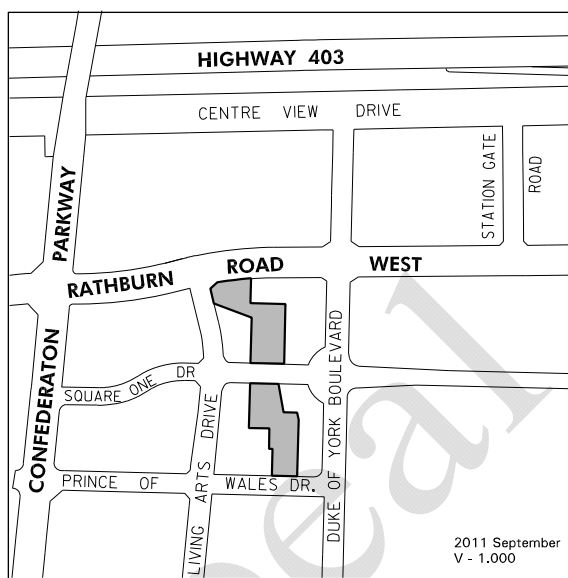
### 12.3 Site 3



12.3.1 The lands identified as Special Site 3 are located north of Rathburn Road, west of the Confederation Parkway extension.

12.3.2 Notwithstanding the provisions of the Mixed Use designation, townhouse dwellings will be permitted.

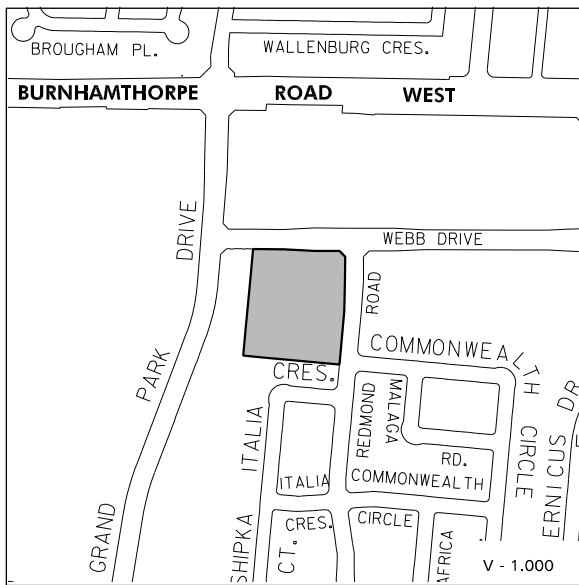
### 12.4 Site 4



12.4.1 The lands identified as Special Site 4 are bounded by Rathburn Road West to the north, Duke of York Boulevard to the east, Prince of Wales Boulevard to the south and Living Arts Drive to the west.

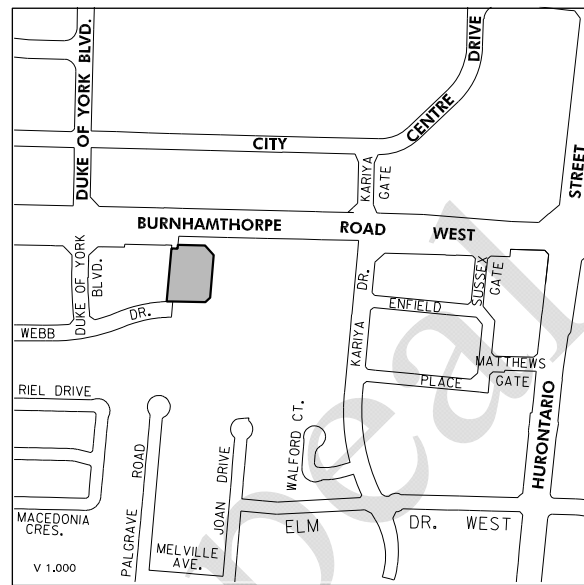
12.4.2 Notwithstanding the Open Space and Mixed Use designations shown on the Land Use Plan, the lands designated Open Space and Mixed Use may be increased or decreased in size, reconfigured or relocated within Special Site 4 without an amendment to this Plan and these lands may also be used for parking.

## 12.5 Site 5



12.5.1 Notwithstanding the Residential High Density designation, an apartment dwelling with a maximum height of 27 storeys will be permitted.

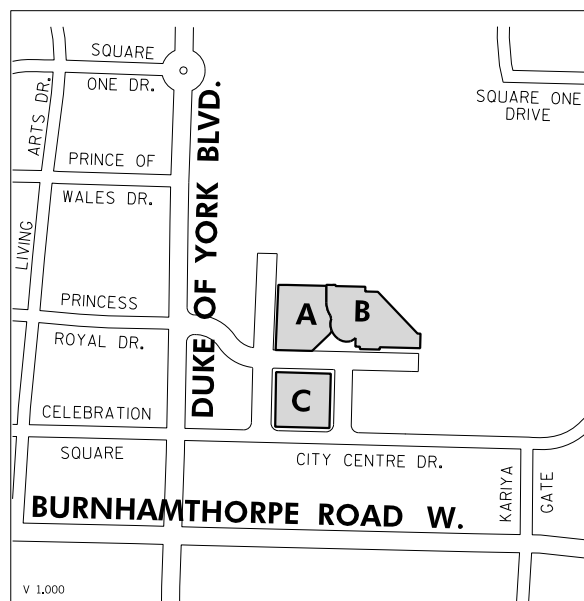
## 12.6 Site 6



12.6.1 The lands identified as Special Site 6 are located at the southwest corner of Burnhamthorpe Road East and the future extension of Main Street.

12.6.2 Notwithstanding the 'A' Street policies of this Area Plan, access for servicing will be permitted from Burnhamthorpe Road West.

## 12.7 Site 7



12.7.1 The lands identified as Special Site 7 are located east of Duke of York Boulevard and north of City Centre Drive.

12.7.2 Notwithstanding the policies in this Plan, commercial uses to a maximum of 925m<sup>2</sup> GFA will be permitted in a building of less than 3 storeys, accessory to Open Space within Area C.

12.7.3 Below-grade parking structures will be permitted in Areas C and D.

12.7.4 Storefronts may vary in width within Area A.

12.7.5 Buildings in Areas A and B will be a minimum of one storey and have the following minimum heights:

- Area A – 13.5 metres
- Area B – 7.5 metres

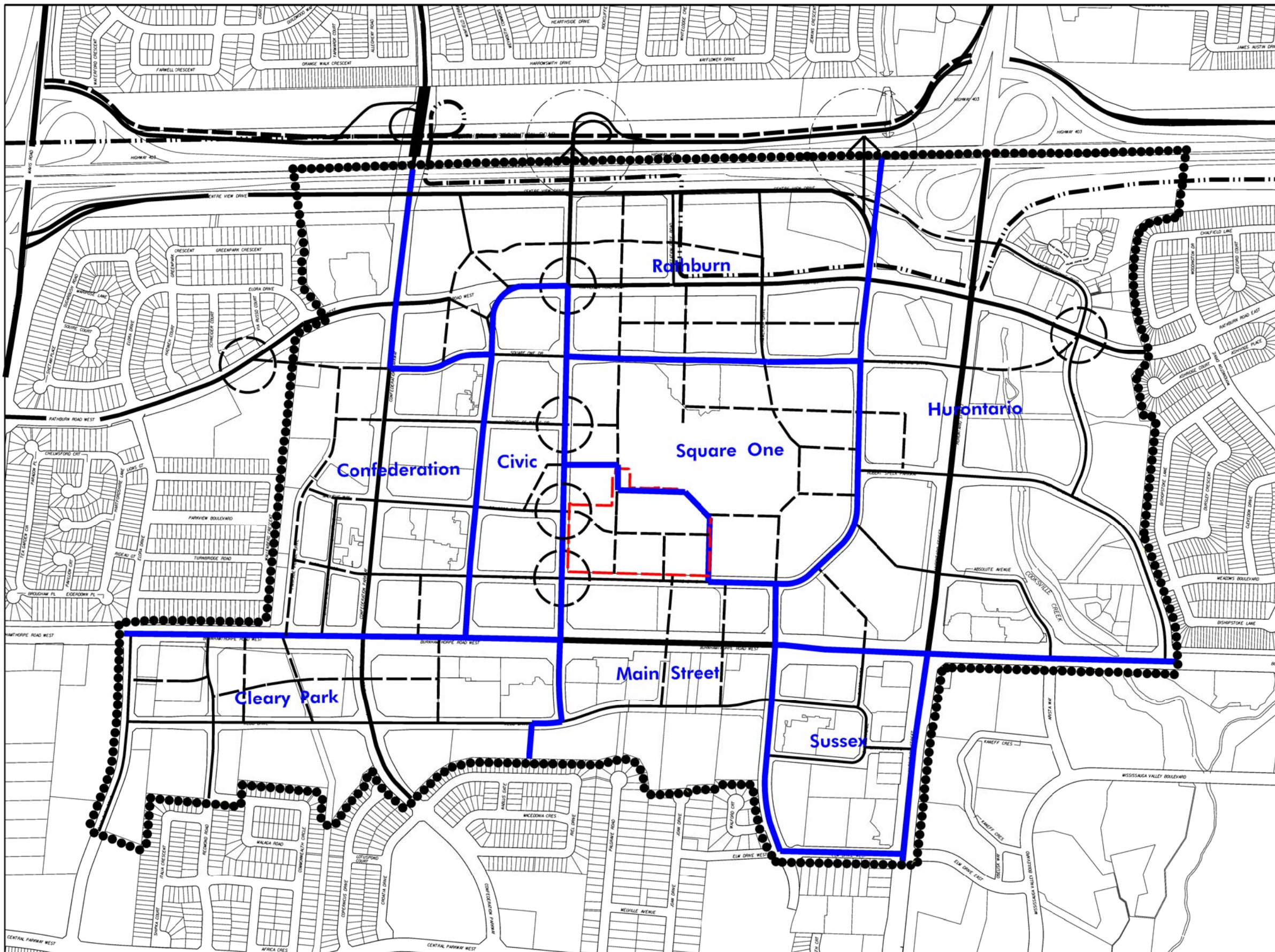
## 13.0 IMPLEMENTATION

13.1 Mississauga may apply a holding provision to lands within the Downtown Core to ensure that the policies of this Plan are implemented. The removal of the holding provision will be conditional on the applicant satisfying the requirements of the policies of this Plan, including:

- a. requiring the conveyance of land for new public roads, including realignments of roads or impose an obligation upon a landowner to construct or pay for the construction of new roads, where not otherwise permitted by the *Planning Act*;
- b. requiring the gratuitous dedication of land for new public roads, including realignments of roads or impose an obligation upon a landowner to construct or pay for the construction of new roads where not otherwise permitted by the *Planning Act*;
- c. the submission of a concept plan for all development applications proposing phased development and must demonstrate how the policies of this Plan and this Area Plan will be implemented through the development review process; and
- d. the submission of a site plan application which demonstrates compliance with the built form policies of the Plan and this Area Plan.

13.2 Site plan control is a key mechanism to implement the Council approved Built Form Standards and will form the basis for site plan approval including the review and approval of building elevations, and sustainable design elements in the adjoining public right-of-way.





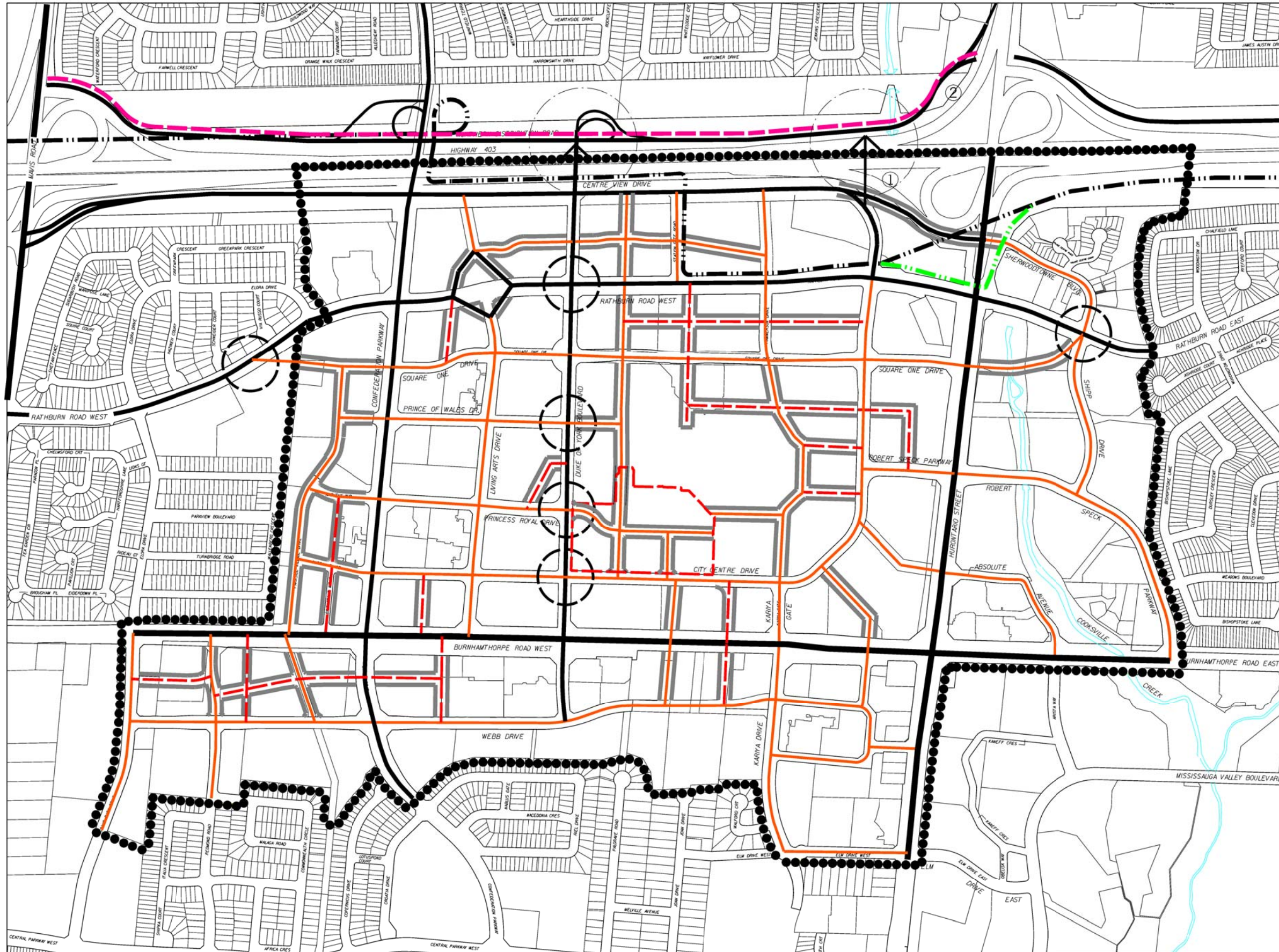
Legend

-  Downtown Core District Boundaries
-  Existing Roads
-  New Roads
-  Proposed Roundabout
-  Proposed Grade Separated Crossing
-  Partial Approval Area

Schedule 1  
Downtown Core Districts







## Legend

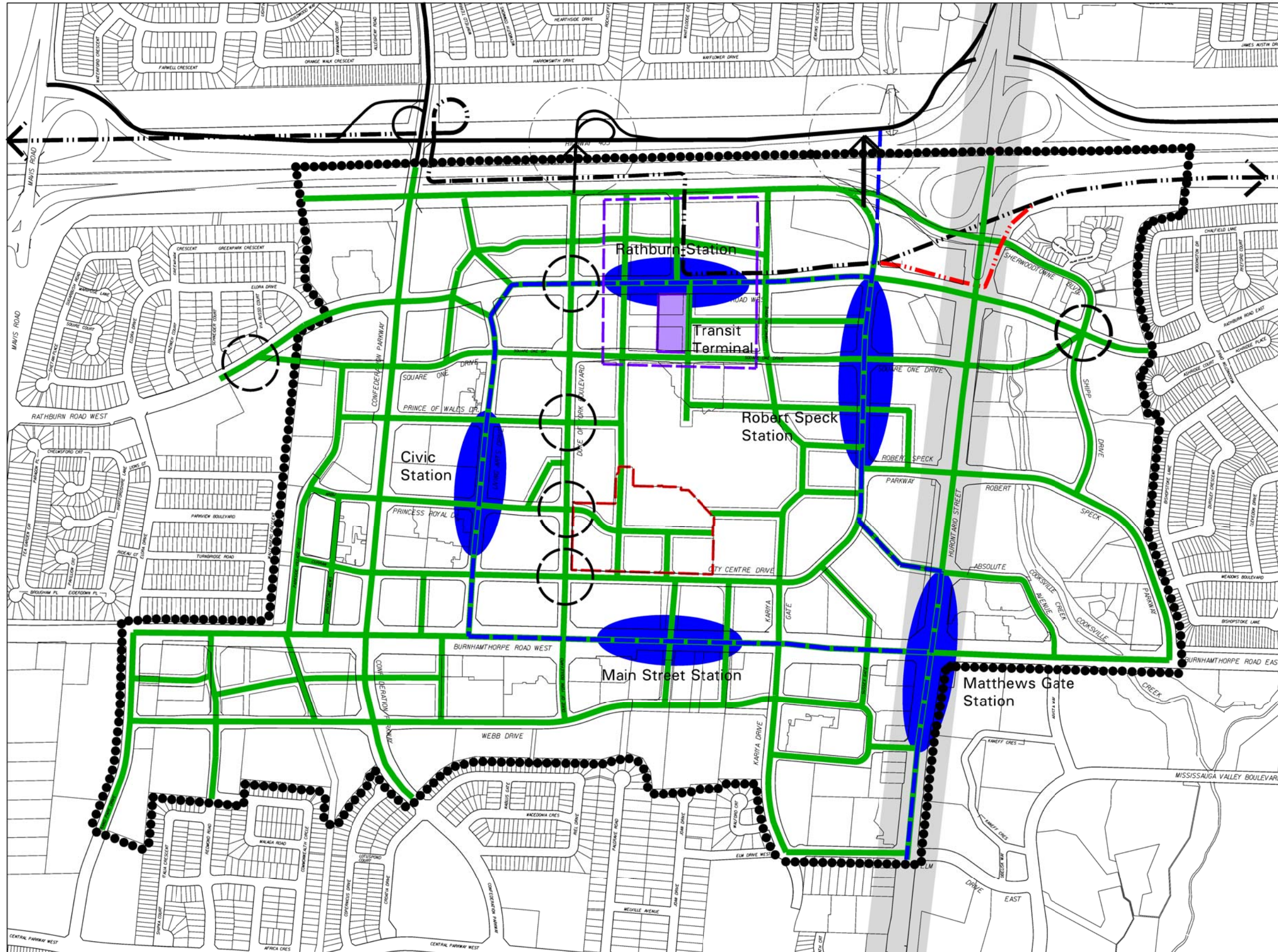
- Northern Distribution Road
- Arterial
- Major Collector
- Minor Collector
- Local Road
- Proposed Roundabout
- Bus Rapid Transit Corridor
- Interim Bus Rapid Transit Corridor
- Proposed Grade Separated Crossing
- New Roads
- Partial Approval Area

- Options
- ① Conceptual Centre View Drive connection to EB Hwy. 403 concept.
  - ② Conceptual N-W ramp terminal relocation.
- Notes:
- 1. Any part of the road network shown outside the city boundaries is shown for information purposes only.
  - 2. All road proposals within the Highway 403 Corridor are conceptual and require review and approval of the Provincial Government.
  - 3. New Transportation Elements will be Subject to a Future Transportation Study.
  - 4. The Interim Bus Rapid Transit Corridor depicts the interim alignment of the Bus Rapid Transit as per 2010 Environmental Assessment Approvals.

## Schedule 2 Downtown Core Long Term Road Network and Classification







## Legend

- Bus Rapid Transit Corridor
- Interim Bus Rapid Transit Corridor
- Light Rail Transit
- Light Rail Transit Station Area
- Transit Terminal
- Hurontario Street - Intensification Corridor (OP)
- Anchor Hub
- Road Network
- Proposed Roundabout
- Proposed Grade Separated Crossing
- Partial Approval Area

## Note:

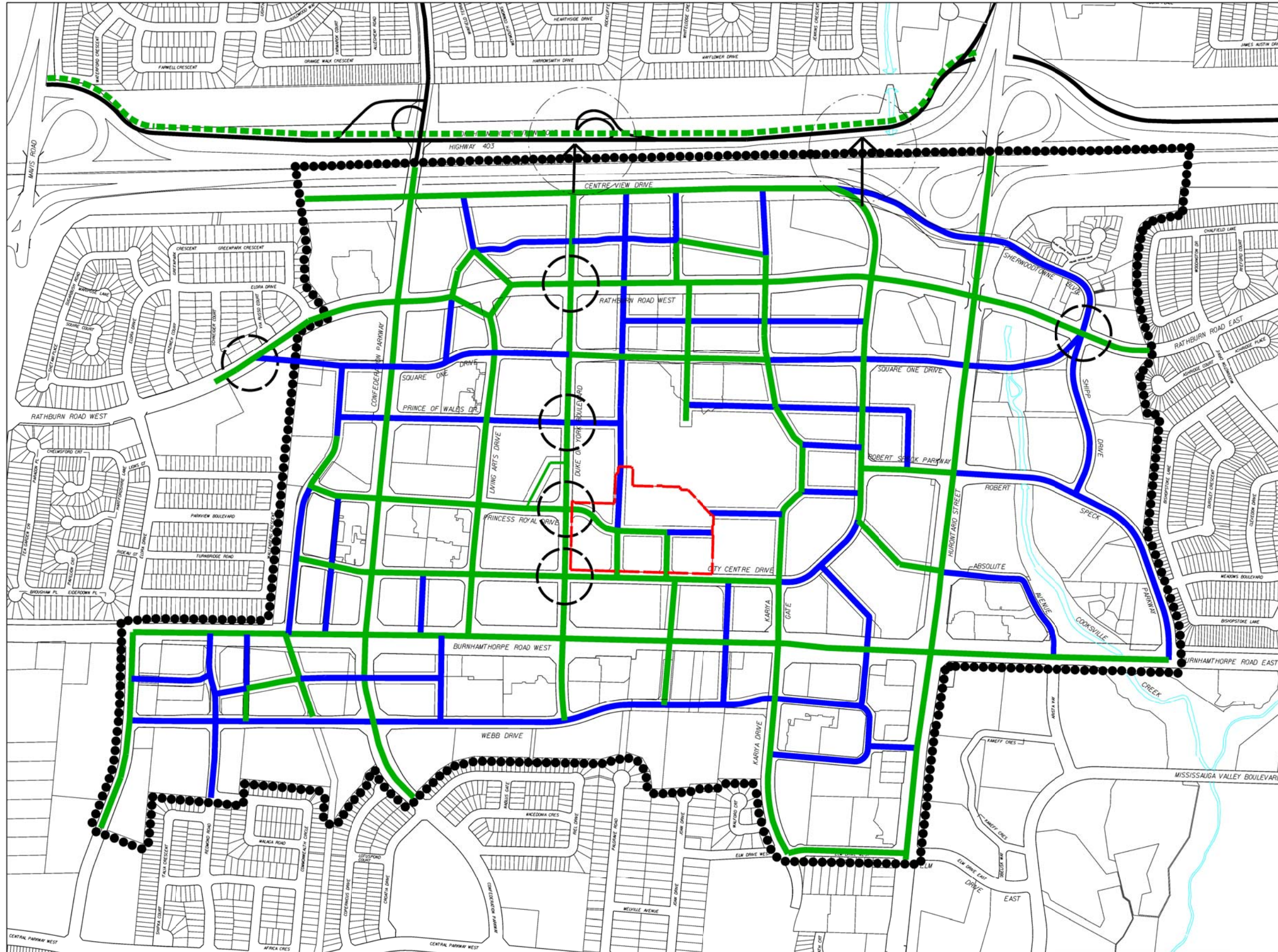
Light Rail Transit Alignment and Station Locations are subject to the approval of the Hurontario-Main LRT Transit Project Assessment Process.

The Interim Bus Rapid Transit Corridor depicts the interim alignment of the Bus Rapid Transit as per 2010 Environmental Assessment Approvals.

## Schedule 3 Downtown Core Long Term Transit Network







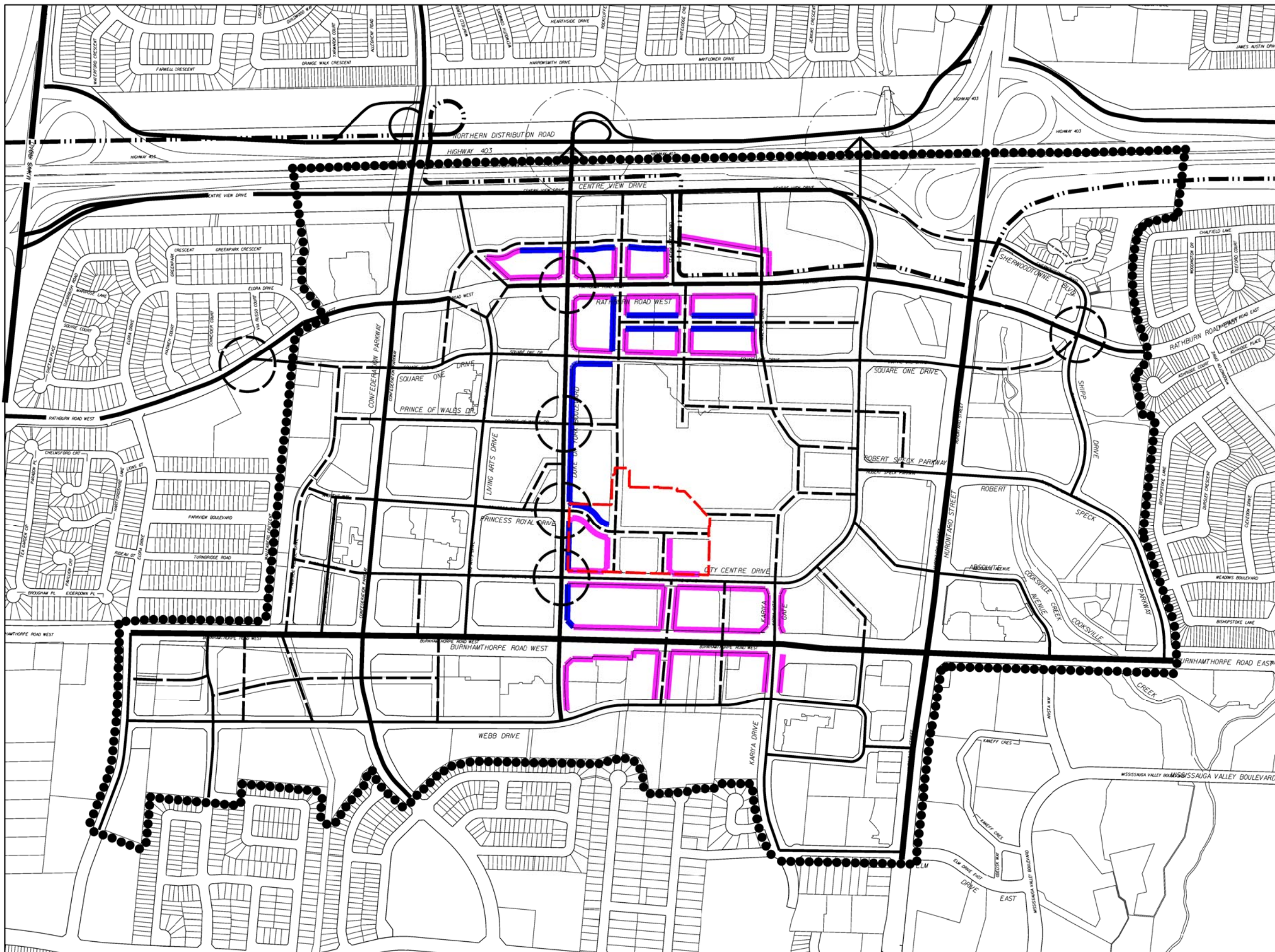
# Legend

- Northern Distribution Road
- A - Frontage
- B - Frontage
- ↑ Proposed New Crossings of Highway 403
- Proposed Roundabout
- ↑ Proposed Grade Separated Crossing
- Partial Approval Area

## Schedule 4 Downtown Core A & B Street Frontage







## Legend

Percentage of ground floor along the street frontage required to contain retail uses:

75%

50%



Proposed Roundabout



Proposed Grade Separated Crossing



Partial Approval Area

## Schedule 5 Downtown Core Retail Activation



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## Tables and Maps

### Lakeview Local Area Plan Land Use Map



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# LAKEVIEW

## 1.0 How to Read the Lakeview Local Area Plan

Mississauga Official Plan is composed of a principal document and a series of local area plans, provided under separate cover. This is the Lakeview Local Area Plan and provides policies for lands located in southeast Mississauga as shown in Figure 1. It includes lands identified in the City Structure as a Community Node, Neighbourhood and Employment Area.

There are some instances where the policies and schedules of the principal document do not address all circumstances particular to Lakeview. In these cases, the Lakeview Local Area Plan elaborates on, or provides exceptions to the policies or schedules of the principal document.

The Lakeview Local Area Plan must be read in conjunction with the Mississauga Official Plan principal document. Parts 1 to 4, the schedules and the appendices of the principal document, are applicable to the Lakeview area, unless modified by this local area plan. Thus, for example, the policies of this local area plan must be read in conjunction with the environmental, multi-modal, urban form and land use policies of Parts 2 and 3 of the principal document. In the event of a conflict, the policies of this local area plan take precedence.

The Lakeview Local Area Plan incorporates the policies of the Lakeview District Policies of Mississauga Plan (2003), modified to conform with Mississauga Official Plan (2010).

A planning study is currently being conducted for the Lakeview community that will result in a new local area plan.

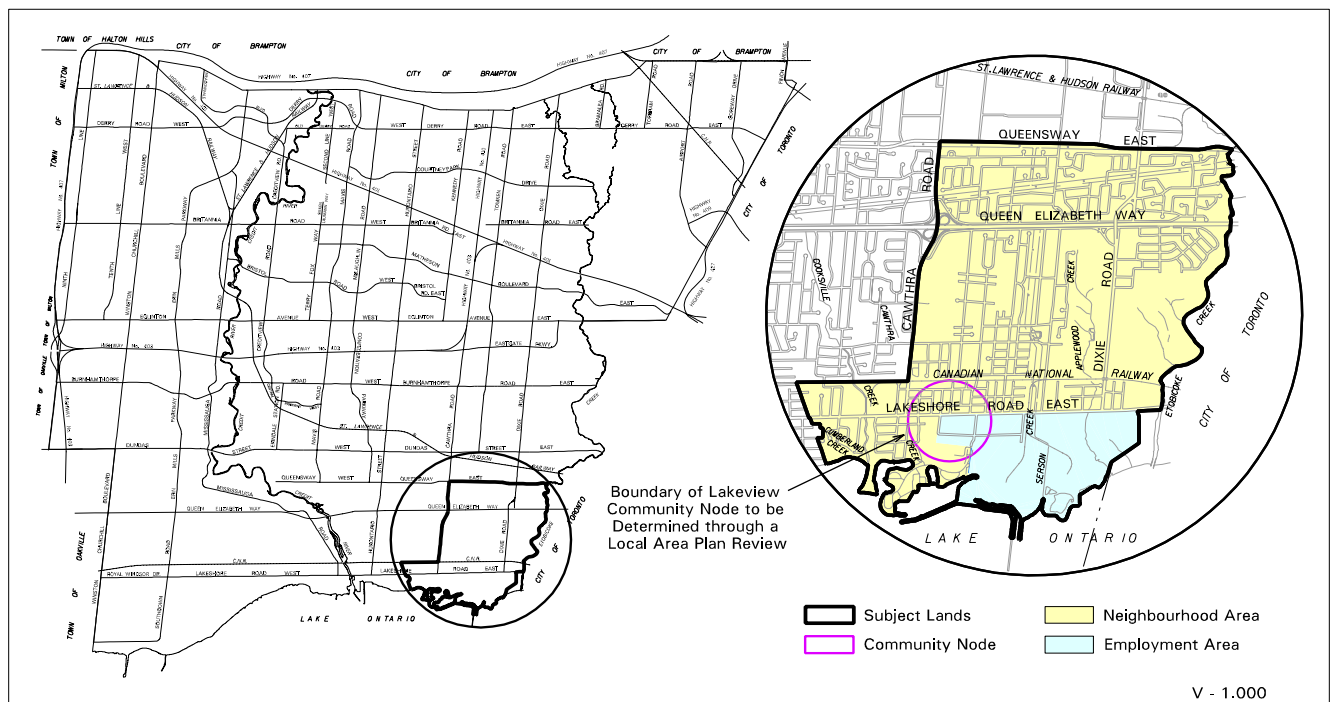


Figure 1: The Lakeview Local Area is located in the southeast corner of Mississauga and includes areas identified in the City Structure as Community Node, Neighbourhood and Employment Area.

## 2.0 Urban Design Policies

**2.1** The scale and character of all building and landscape designs will take into consideration the guidelines established in the Lakeshore Road Design Concept.

## 3.0 Land Use

### 3.1 Residential

#### 3.1.1 Residential Low Density I

The Residential Low Density I designation permits detached, semi-detached and duplex dwellings.

#### 3.1.2 Residential Low Density II

The Residential Low Density II designation permits detached, semi-detached, duplex, triplex and street townhouse dwellings.

#### 3.1.3 Residential Medium Density

The Residential Medium Density designation permits detached, semi-detached, duplex, townhouse dwellings and all forms of multiple dwellings.

#### 3.1.4 Residential High Density

The Residential High Density designation permits apartment dwellings.

### 3.2 Commercial

#### 3.2.1 Mixed Use

Buildings should have a minimum height of two storeys and a maximum height of three storeys.

### 3.3 Business Employment

3.3.1 Notwithstanding the Business Employment policies of this Plan, the following uses will not be permitted:

a. adult entertainment establishments;

- b. animal boarding establishments which may include outdoor facilities;
- c. body rub establishments;
- d. motor vehicle body repair facilities;
- e. transportation facilities;
- f. trucking terminals; and
- g. **waste processing stations** or **waste transfer stations** and composting facilities.

## 4.0 Special Site Policies

There are sites within Lakeview, as shown in Figure 2, that merit special attention and are subject to the following policies.

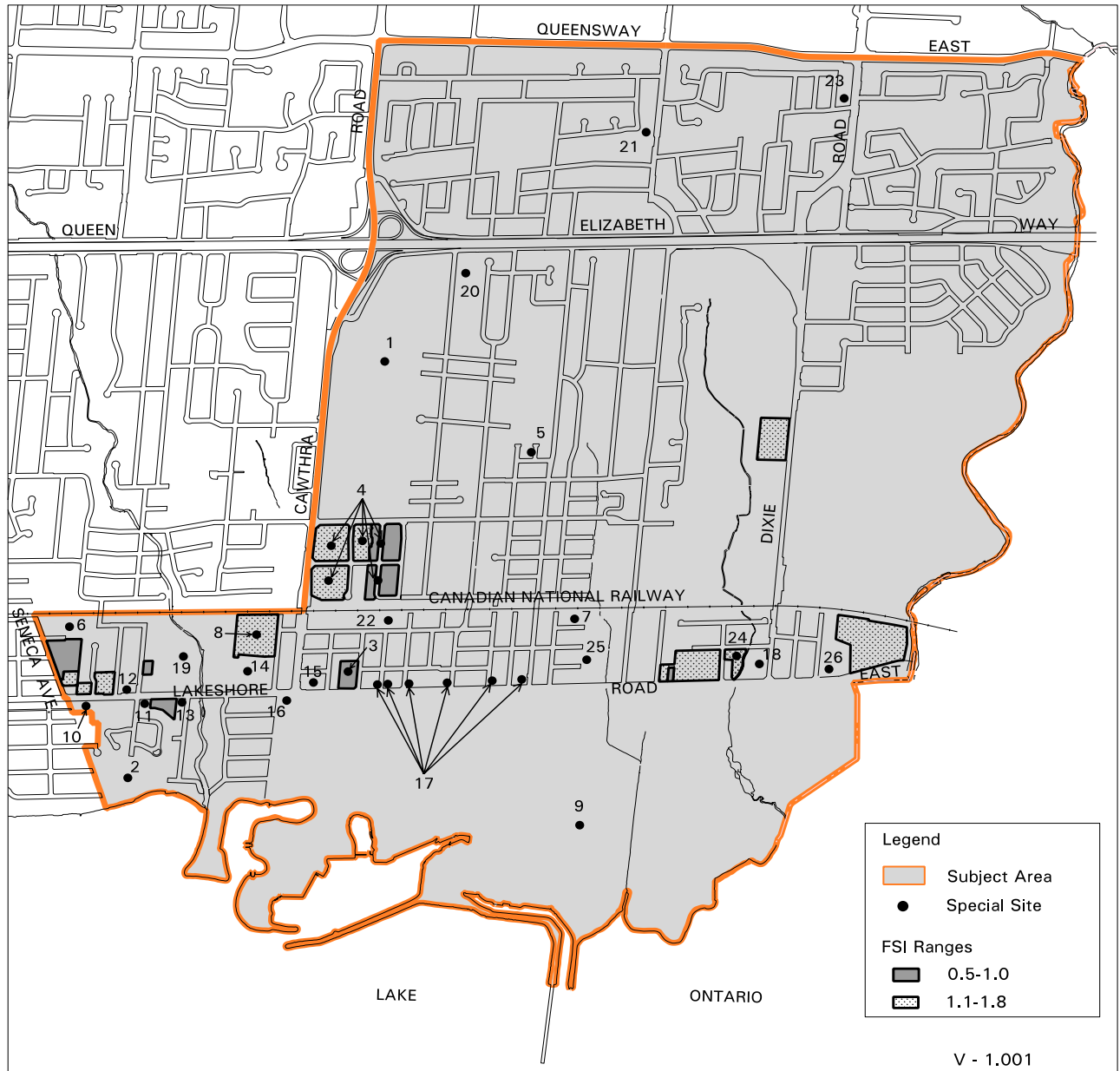
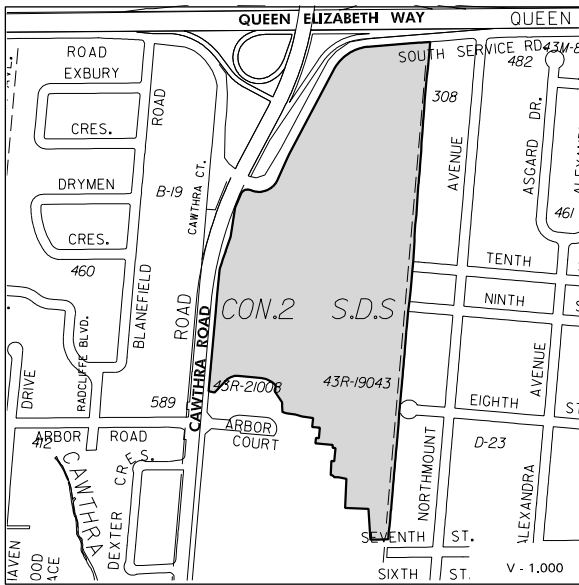


Figure 2: Location of Special Sites within the Lakeview Local Area.

## 4.1 Site 1



4.1.1 The lands identified as Special Site 1 are east of Cawthra Road and south of the South Service Road.

4.1.2 The lands are known as the Cawthra Woods and comprise both heritage and natural areas. Within the Cawthra Woods is located the Cawthra Estate which includes the Cawthra-Elliott Estate House and formal gardens on the north and south sides of the estate house, the walled garden, and sugar maple forest, all of which form a cultural heritage landscape which is designated under the *Ontario Heritage Act*. The site contains a Provincially significant wetland, an **Environmentally Significant Area (ESA)** as identified by Credit Valley Conservation, and a Regionally Significant Area of Natural and Scientific Interest (ANSI).

4.1.3 Notwithstanding the Public Open Space designation, the following additional policies will apply:

- a. the following uses will be permitted in the Cawthra-Elliott Estate House:
  - community infrastructure, including an academy for the performing arts;
  - **secondary offices**;

- a conference centre;
  - art gallery or studio; and
  - commercial school that may include a business school, driving school, dance school, music school, arts school, crafts school or a martial arts school, but shall not include a trade school or a private school.
- b. development of the Estate House will address the following:
    - the Environmental Policies of this Plan;
    - retention of the existing forest in a natural condition in accordance with an approved Cawthra Woods Management and Implementation Plan;
    - an approved Parks Master Plan;
    - preservation and rehabilitation of the Cawthra-Elliott cultural heritage landscape, including the existing house, outbuildings and formal gardens. Any minor development or minor site alterations will be limited in size and to a design compatible with the historically designated structure, will be designed and located in a manner that minimizes removal of forest cover and will be subject to completion and circulation of an Environmental Impact Study (EIS) and site plans to the Region of Peel and Credit Valley Conservation; and
    - a **Heritage Impact Statement** may be required.



## 4.2 Site 2



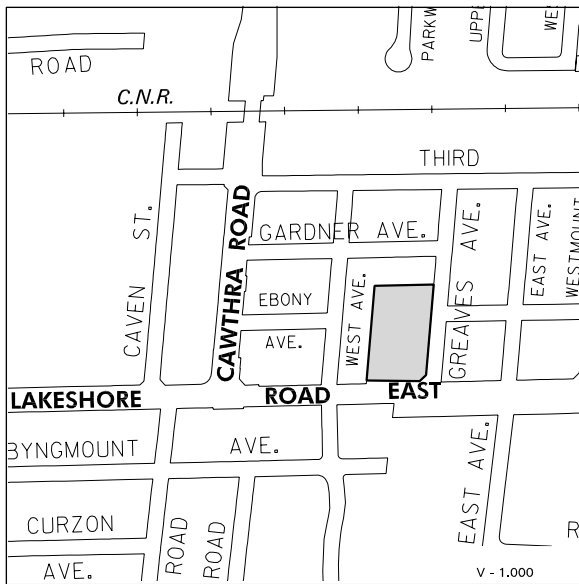
- maintenance of public access along the waterfront as well as the grounds of the estate;
- enhancement of recreation opportunities throughout the balance of Lakefront Promenade Park;
- recognition and respect of the existing character of the surrounding residential land uses; and
- the servicing requirements of the Region of Peel with respect to the sanitary sewer outlet and the use of backflow preventors in the buildings to prevent basement flooding.

4.2.1 The lands identified as Special Site 2 known as the Adamson Estate and Derry Property, are located along the shore of Lake Ontario, east of Cumberland Drive and south of Enola Avenue.

4.2.2 Notwithstanding the Public Open Space designation and the Natural Area classification of this Plan, the following additional policies will apply:

- a. in addition to the uses permitted in the Public Open Space designation, the following uses will also be permitted:
  - **secondary offices;**
  - a conference centre;
  - an academy for the performing arts; and
  - a specialty hospital;
- b. development of this site will be subject to site plan approval which will address, among other things, the following:
  - restoration and preservation of the historically designated dwelling and coach house;

### 4.3 Site 3

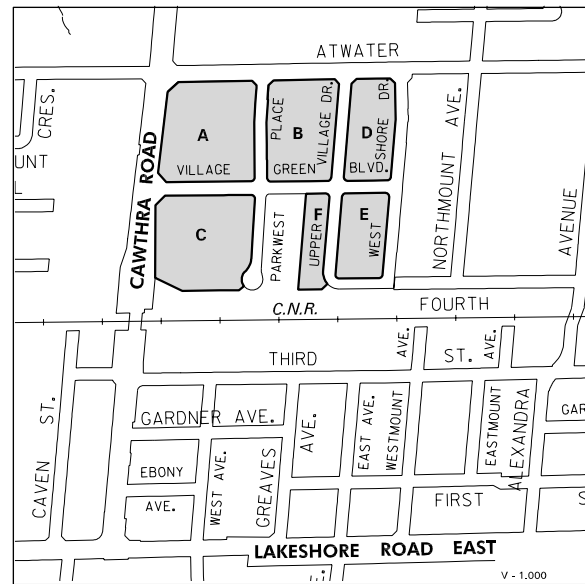


4.3.1 The lands identified as Special Site 3 are located at the northwest corner of Lakeshore Road East and Greaves Avenue.

4.3.2 Notwithstanding the Residential High Density designation of this Plan, the following policy will apply:

- a. apartment units are permitted with a maximum height of seven storeys and townhouses with a maximum height of two storeys. The overall density on the site will not exceed 138 units per **net residential hectare**.

### 4.4 Site 4



4.4.1 The lands identified as Special Site 4 are located in the southeast quadrant of Cawthra Road and Atwater Avenue.

4.4.2 The lands are designated Residential High Density. The maximum number of dwelling units on this property will not exceed 1 200 units.

4.4.3 The number of residential units within each Special Site has been identified on a site specific basis. Some residential units may be transferred between these areas without further amendment to this Plan provided the total number of residential units on Special Site 4 does not exceed 1 200 units and the transfer of residential units does not have an adverse effect on the neighbouring residential environment.

#### 4.4.4 Area A

Notwithstanding the provisions of the Residential High Density designation of this Plan, the following additional policy will apply to the lands identified as Area A:

- a. all types of multiple family dwellings including townhouses, street townhouses, other forms of horizontal multiple dwellings, apartments, or any combination of these uses with individual

frontages or in cluster arrangements will be permitted, provided the number of units without a transfer does not exceed 415, except that a maximum of 40 additional units may be transferred to this Area; and

- b. the maximum apartment building height will be 12 storeys.

#### 4.4.5 Area B

Notwithstanding the provisions of the Residential High Density designation of this Plan, the following additional policy will apply to the lands identified as Area B:

- a. all types of multiple family dwellings including townhouses, street townhouses, other forms of horizontal multiple dwellings, apartments, or any combination of these uses with individual frontages or in cluster arrangements will be permitted, provided the number of units without a transfer does not exceed 245, except that a maximum of 40 additional units may be transferred to this Area.

#### 4.4.6 Area C

Notwithstanding the provisions of the Residential High Density designation of this Plan, the following additional policies will apply to the lands identified as Area C:

- a. all types of multiple family dwellings including townhouses, street townhouses, other forms of horizontal multiple dwellings, apartments, or any combination of these uses with individual frontages or in cluster arrangements will be permitted, provided the number of units without a transfer does not exceed 370, except that a maximum of 40 additional units may be transferred to this Area; and
- b. public open space, flood control, conservation uses will also be permitted.

#### 4.4.7 Area D

Notwithstanding the provisions of the Residential High Density designation of this Plan, the following

additional policy will apply to the lands identified as Area D:

- a. all types of multiple family dwellings including townhouses, street townhouses, other forms of horizontal multiple dwellings, apartments, or any combination of these uses with individual frontages or in cluster arrangements will be permitted, provided the number of units without a transfer does not exceed 80, except that a maximum of 36 additional units may be transferred to this Area.

#### 4.4.8 Area E

Notwithstanding the provisions of the Residential High Density designation of this Plan, the following additional policy will apply to the lands identified as Area E:

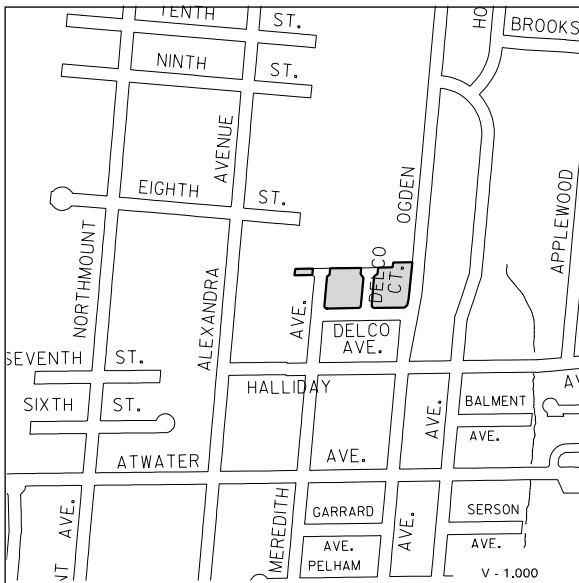
- a. all types of multiple family dwellings including townhouses, street townhouses, other forms of horizontal multiple dwellings, apartments, or any combination of these uses with individual frontages or in cluster arrangements will be permitted, provided the number of units without a transfer does not exceed 60, except that a maximum of 12 additional units may be transferred to this Area.

#### 4.4.9 Area F

Notwithstanding the provisions of the Residential High Density designation of this Plan, the following additional policy will apply to the lands identified as Area F:

- a. all types of multiple family dwellings including townhouses, street townhouses, other forms of horizontal multiple dwellings, apartments, or any combination of these uses with individual frontages or in cluster arrangements will be permitted, provided the number of units without a transfer does not exceed 30, except that a maximum of 10 additional units may be transferred to this Area.

## 4.5 Site 5

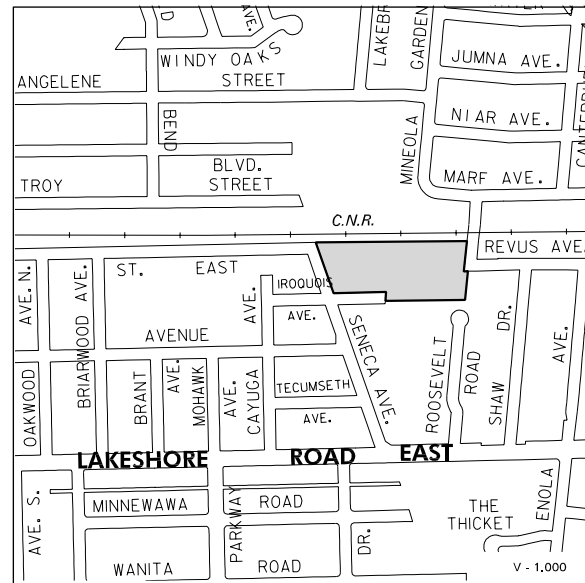


4.5.1 The lands identified as Special Site 5 are located on the north side of Delco Avenue, west of Ogden Avenue, on the east and west side of the north end of Meredith Avenue.

4.5.2 Notwithstanding the Residential Low Density II designation of this Plan, the following policy will apply:

- a. development on the lands will be restricted to 13 detached and 18 semi-detached dwellings.

## 4.6 Site 6



4.6.1 The lands identified as Special Site 6 are located west of Revus Avenue, south of the Canadian National Railway tracks.

4.6.2 Notwithstanding the provisions of the Business Employment designation, the following uses will not be permitted:

- a. outdoor storage;
- b. overnight accommodation and conference centres;
- c. broadcasting, communication, and utility rights-of-way;
- d. financial institutions;
- e. entertainment, recreation and sports facilities; and
- f. all types of restaurants.

## 4.7 Site 7



4.7.1 The lands identified as Special Site 7 are located along Casson Avenue, west of the utility corridor.

4.7.2 Notwithstanding the provisions of the Business Employment designation, the following uses will not be permitted:

- a. outdoor storage;
- b. overnight accommodation and conference centres;
- c. broadcasting, communication, and utility rights-of-way;
- d. financial institutions;
- e. entertainment, recreation and sports facilities; and
- f. all types of restaurants.

## 4.8 Site 8

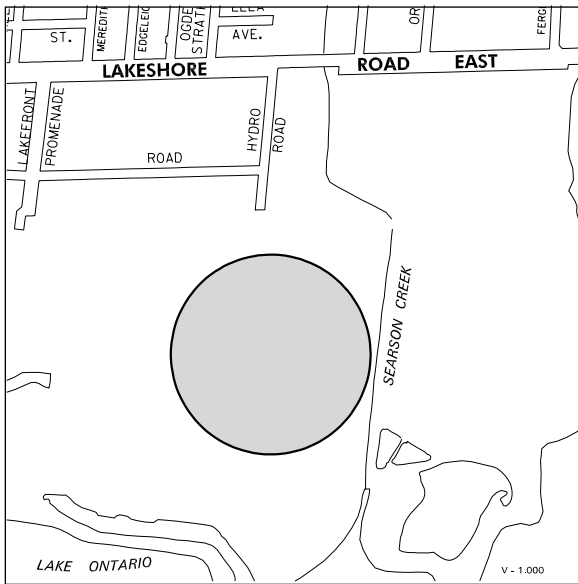


4.8.1 The lands identified as Special Site 8 are located on the west side of Caven Street and south of the Canadian National Railway tracks.

4.8.2 Notwithstanding the Residential High Density designation of this Plan, buildings will be permitted within a **floor space index** of 0.8-2.3.



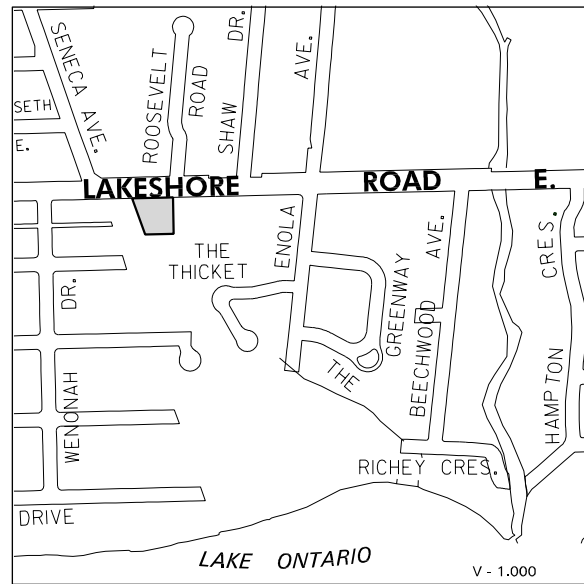
## 4.9 Site 9



4.9.1 The lands identified as Special Site 9 are located south of Lakeshore Road East along the waterfront between Searson Creek and Lakefront Promenade Park.

4.9.2 Notwithstanding the Utility designation, an electric power generating facility will be permitted.

## 4.10 Site 10

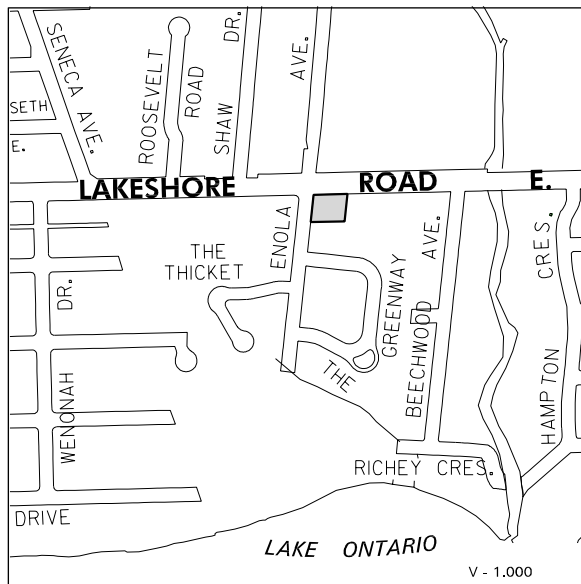


4.10.1 The lands identified as Special Site 10 are located on the south side of Lakeshore Road East, east of Wenonah Drive.

4.10.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. a convenience restaurant will be permitted; and
- b. notwithstanding the provisions of this Section, an existing drive-through facility at 362 Lakeshore Road East will be permitted.

## 4.11 Site 11

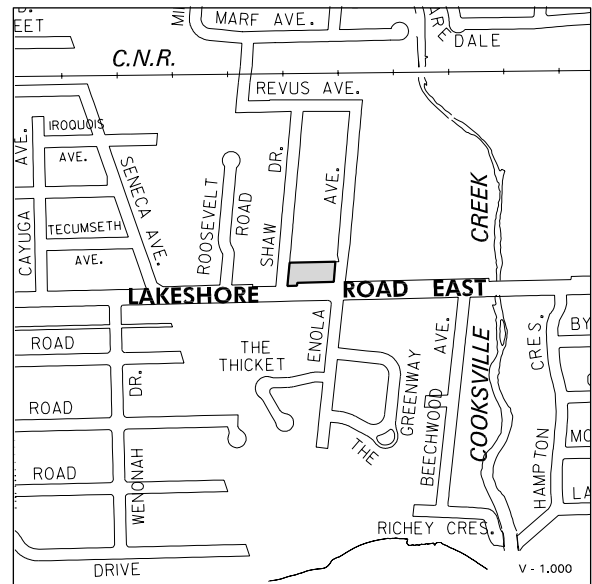


4.11.1 The lands identified as Special Site 11 are located at the southeast corner of Lakeshore Road East and Enola Avenue.

4.11.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. a motor vehicle service station will be permitted.

## 4.12 Site 12

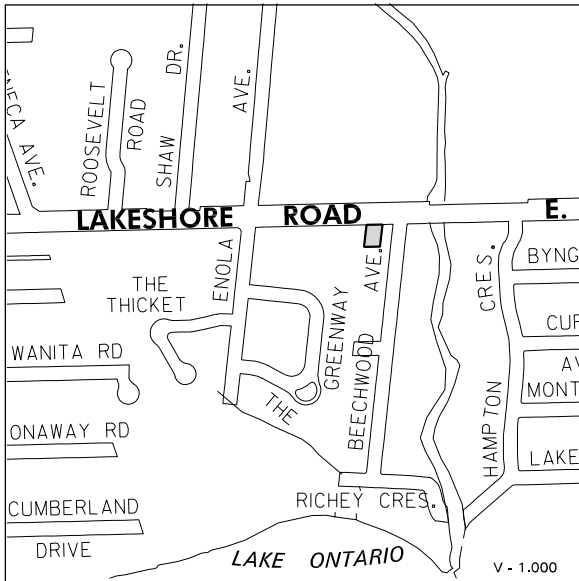


4.12.1 The lands identified as Special Site 12 are located on the north side of Lakeshore Road East between Shaw Drive and Enola Avenue.

4.12.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. a motor vehicle repair garage will be permitted.

#### 4.13 Site 13

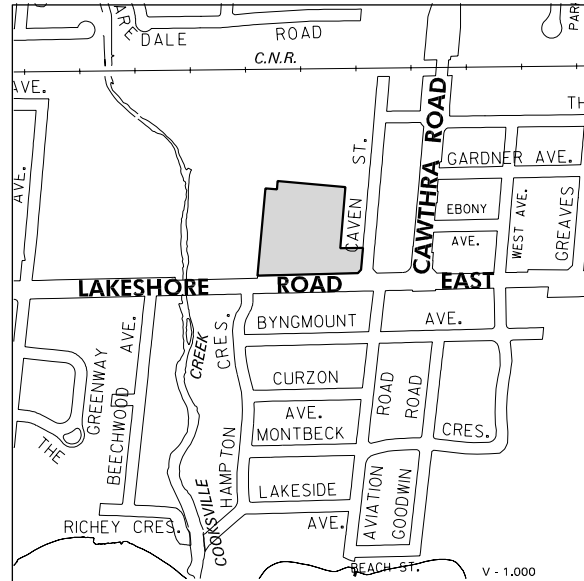


4.13.1 The lands identified as Special Site 13 are located on the south side of Lakeshore Road East, west of Beechwood Avenue.

4.13.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. a motor vehicle repair garage will be permitted.

#### 4.14 Site 14

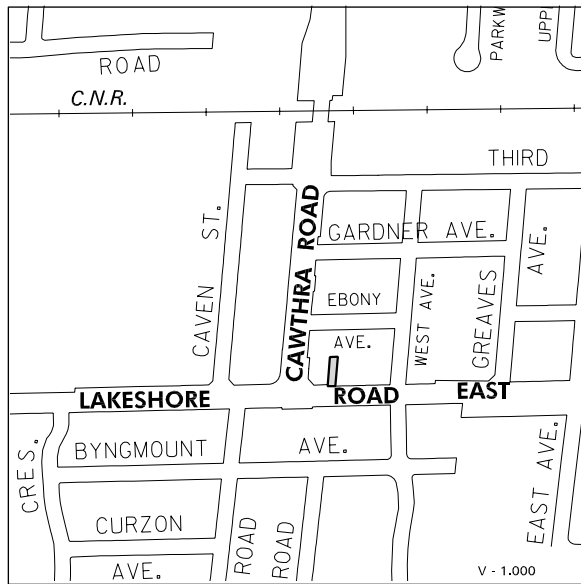


4.14.1 The lands identified as Special Site 14 are located on the north side of Lakeshore Road East, west of Caven Street.

4.14.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. a convenience restaurant will be permitted; and
- b. notwithstanding the provisions of this Section, an existing drive-through facility located at 585 Lakeshore Road East will be permitted.

## 4.15 Site 15



4.15.1 The lands identified as Special Site 15 are located on the north side of Lakeshore Road East, east of Cawthra Road.

4.15.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. a motor vehicle repair garage will be permitted.

## 4.16 Site 16

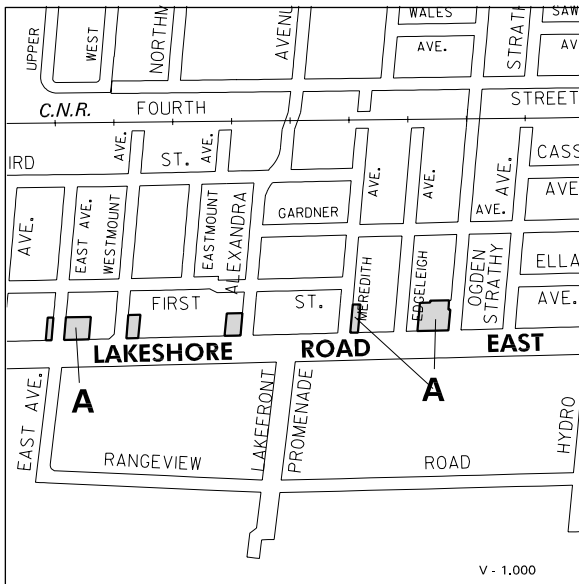


4.16.1 The lands identified as Special Site 16 are located at the southeast corner of Lakeshore Road East and Aviation Road.

4.16.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. motor vehicle commercial uses will be permitted.

## 4.17 Site 17

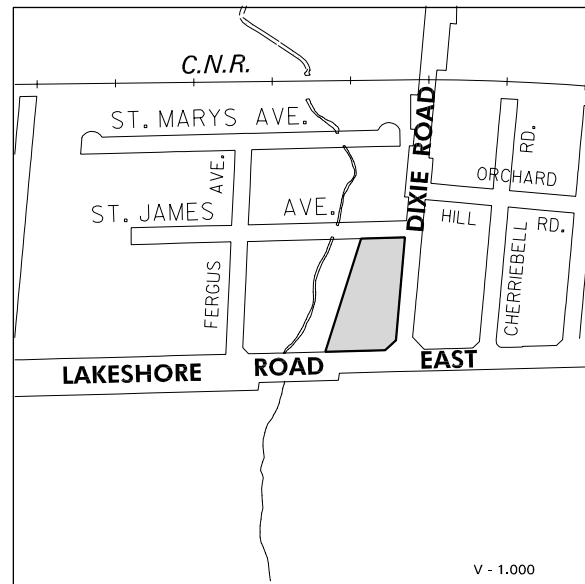


4.17.1 The lands identified as Special Site 17 include six sites located on the north side of Lakeshore Road East between Greaves Avenue and Ogden Avenue.

4.17.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. a motor vehicle repair garage will be permitted; and
- b. for the lands identified as Area A, motor vehicle sales and rental will also be permitted.

## 4.18 Site 18



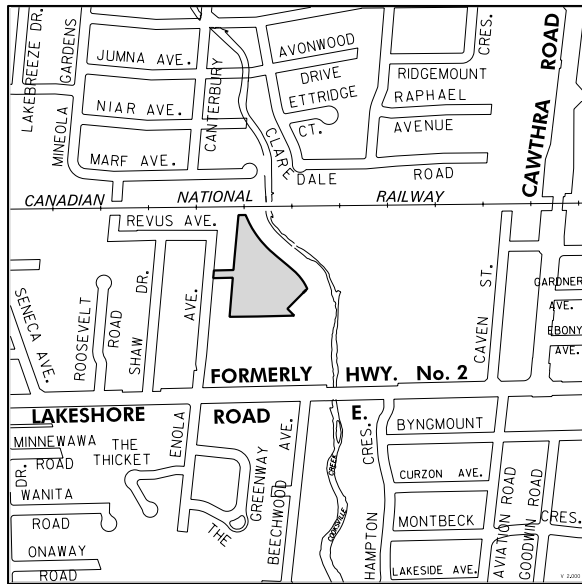
4.18.1 The lands identified as Special Site 18 are located at the northwest corner of Lakeshore Road East and Dixie Road.

4.18.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. motor vehicle sales and service will be permitted.



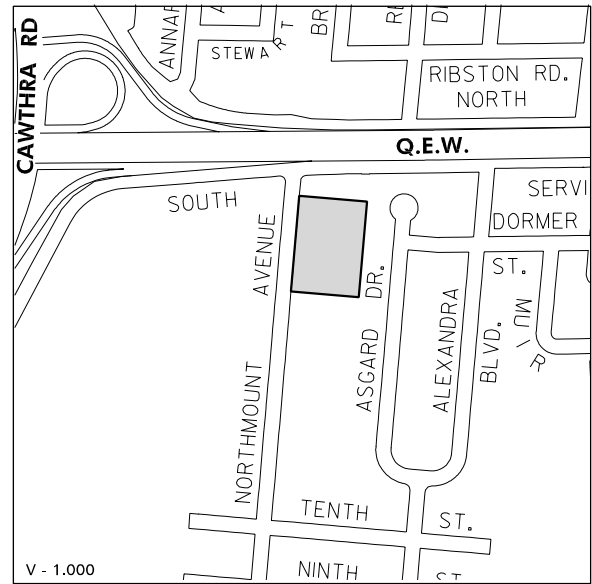
## 4.19 Site 19



4.19.1 The lands identified as Special Site 19 are located north of Lakeshore Road East, east of Enola Avenue.

4.19.2 Notwithstanding the policies of this Plan, townhouse and horizontal multiple dwellings will be permitted.

## 4.20 Site 20

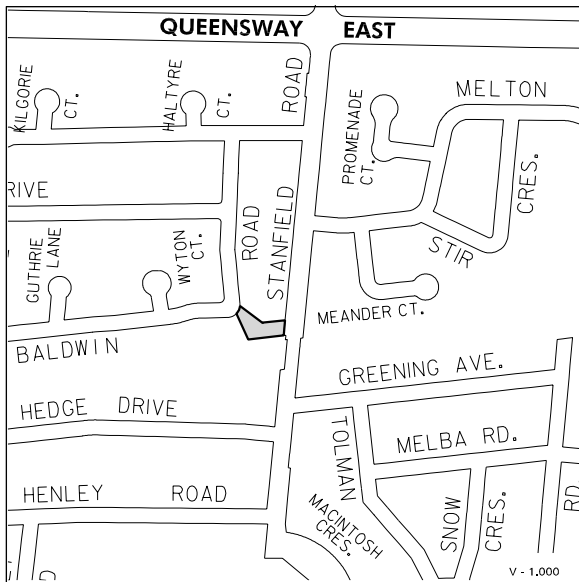


4.20.1 The lands identified as Special Site 20 are located on the east side of Northmount Avenue south of the South Service Road.

4.20.2 Notwithstanding the provisions of the Residential Low Density II designation as it applies to these lands, the following policies will apply:

- a. the overall net density on the site will not exceed 31 units per **net residential hectare**; and
- b. a maximum of 30 condominium townhouses with frontage on a private internal road are permitted.

## 4.21 Site 21

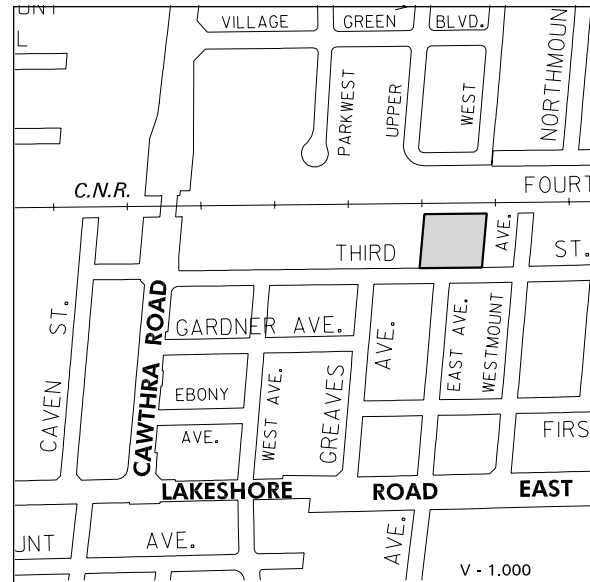


4.21.1 The lands identified as Special Site 21 are located on the west side of Stanfield Road, south of Queensway East.

4.21.2 Notwithstanding the provisions of the Residential Low Density I designation, the following additional policies will apply:

- hair care and aesthetics services will be permitted;
- secondary office** uses, excluding medical offices, will be permitted;
- the total gross floor area – non-residential of all buildings and structures on the lands shall not exceed 350 m<sup>2</sup>; and
- the total gross floor area – non-residential devoted to hair care and aesthetics services will not exceed 200 m<sup>2</sup> and will be located on the ground floor.

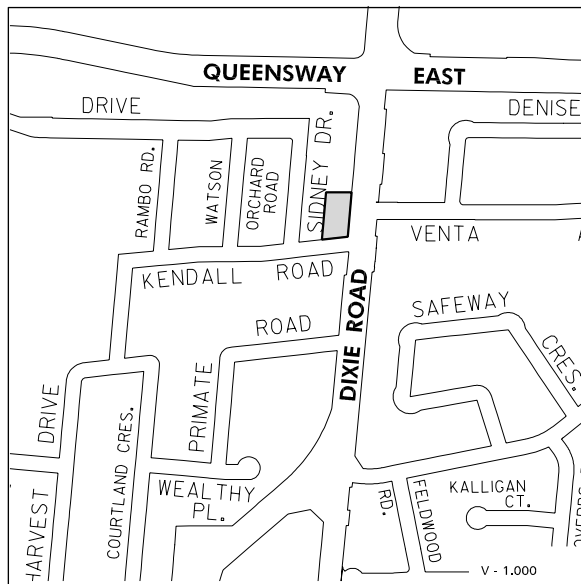
## 4.22 Site 22



4.22.1 The lands identified as Special Site 22 are located east of Cawthra Road, south of the Canadian National Railway tracks.

4.22.2 Notwithstanding the provisions of the Residential Low Density II designation, two three storey apartment buildings will be permitted.

## 4.23 Site 23



4.23.1 The lands identified as Special Site 23 are located at the northwest corner of Kendall Road and Dixie Road.

4.23.2 Notwithstanding the provisions of the Residential Low Density I designation, an office for a maximum of four medical practitioners will be permitted.

## 4.24 Site 24



4.24.1 The lands identified as Special Site 24 are located east of Fergus Avenue, north of Lakeshore Road East.

4.24.2 Notwithstanding the provisions of the Residential High Density designation, townhouse dwellings will be permitted.

## 4.25 Site 25

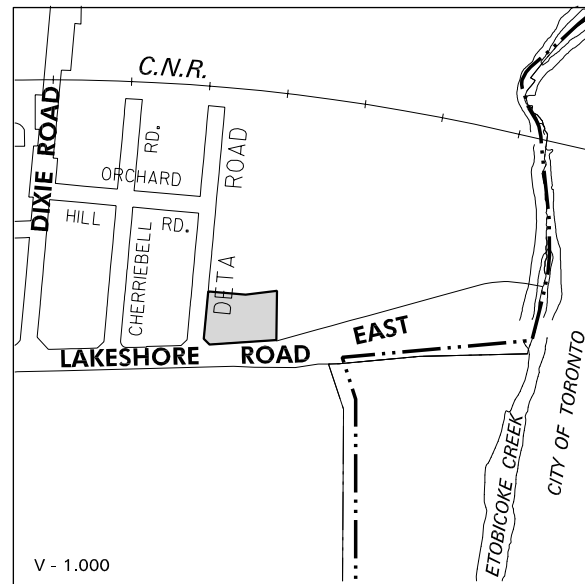


4.25.1 The lands identified as Special Site 25 are located on the north side of Lakeshore Road East, east of Strathy Avenue.

4.25.2 Notwithstanding the provisions of the Residential Low Density II and Mixed Use designations, the following additional policies will apply:

- a. townhouse dwellings will be permitted.

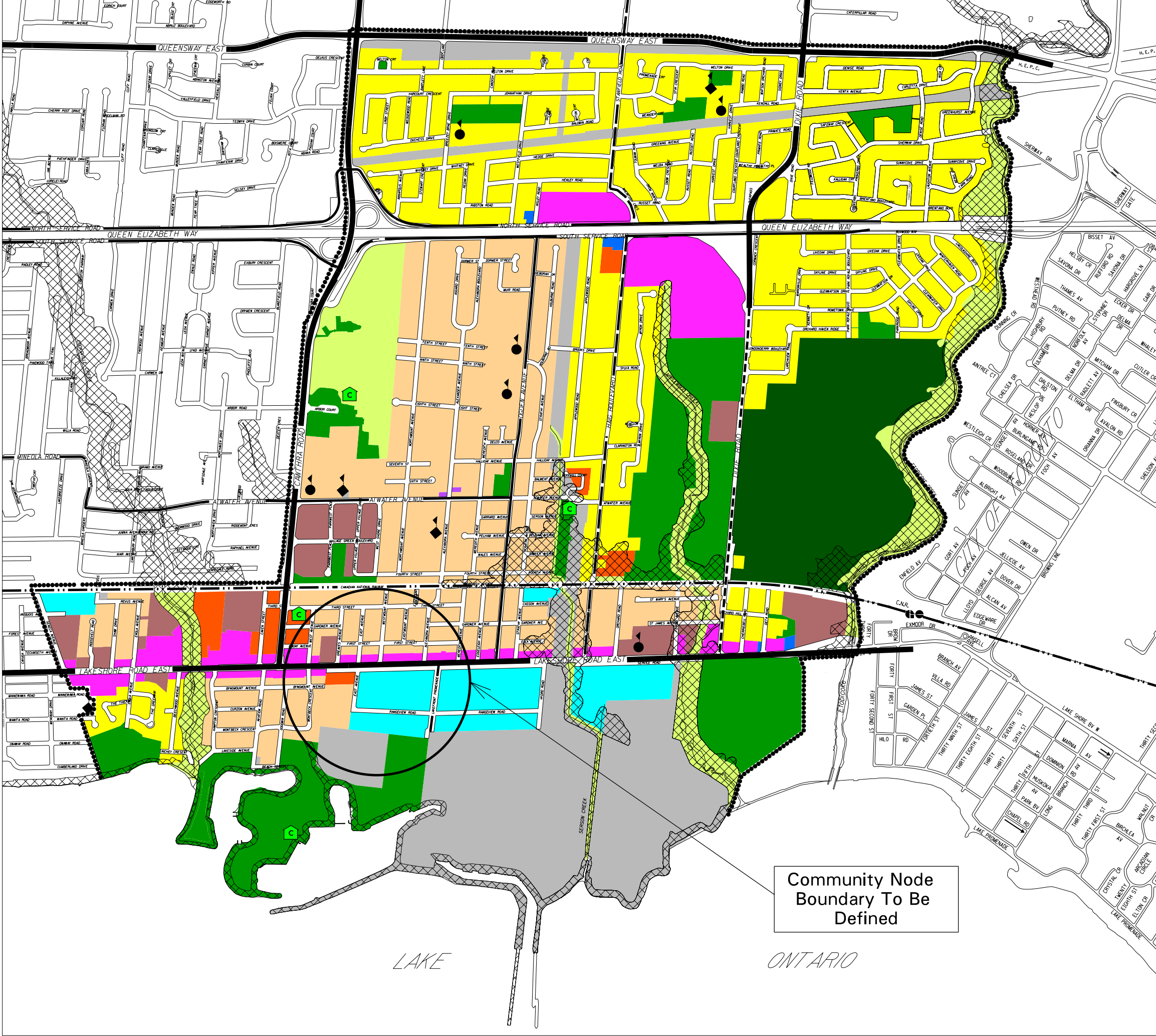
## 4.26 Site 26



4.26.1 The lands identified as Special Site 26 are located at the northeast corner of Lakeshore Road East and Delta Road.

4.26.2 Notwithstanding the Mixed Use designation, the following additional policies shall apply:

- a. the maximum number of horizontal multiple dwelling units permitted shall be 47; and
- b. the maximum **floor space index (FSI)** shall be 0.9.



LAND USE DESIGNATIONS

- Residential Low Density I
- Residential Low Density II
- Residential Medium Density
- Residential High Density
- Mixed Use
- Convenience Commercial
- Motor Vehicle Commercial
- Business Employment
- Public Open Space
- Private Open Space
- Greenbelt
- Utility

TRANSPORTATION LEGEND

- Provincial Highway and Interchange
- Arterial
- Major Collector
- Major Collector (Scenic Route)
- Minor Collector
- Local Road
- Existing Commuter Rail
- GO Transit Station

LAND USE LEGEND

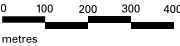
- Natural Hazards
- Public School
- Catholic School
- Community Facilities
- Local Area Plan Boundary

Notes:

- Any part of the road network shown outside the city boundaries is shown for information purposes only.
- Refer to Schedule 3, Natural System for the location of the Natural Areas System and Natural Hazards.
- The limits of the Natural Hazards shown on this map are for illustrative purposes only. The appropriate Conservation Authority should be consulted to determine their actual location.

Community Node  
Boundary To Be  
Defined

Lakeview Local Area Plan  
Land Use Map



City of Mississauga



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Tables and Maps

Port Credit Local Area Plan Land Use Map

# PORT CREDIT

## 1.0 How to Read the Port Credit Local Area Plan

Mississauga Official Plan is composed of a principal document and a series of local area plans, provided under separate cover. This is the Port Credit Local Area Plan and provides policies for lands located in south central Mississauga, as shown on Figure 1. It includes lands identified in the City Structure as Community Node and Neighbourhood.

There are some instances where the policies and schedules of the principal document do not address all circumstances particular to Port Credit. In these cases, the Port Credit Local Area Plan elaborates on, or provides exceptions to the policies or schedules of the principal document.

The Port Credit Local Area Plan must be read in conjunction with the Mississauga Official Plan principal document. Parts 1 to 4, the schedules and

the appendices of the principal document, are applicable to the Port Credit area, unless modified by this local area plan. Thus, for example, the policies of this local area plan must be read in conjunction with the environmental, multi-modal, urban form and land use policies of Parts 2 and 3 of the principal document. In the event of a conflict, the policies of this local area plan take precedence.

The Port Credit Local Area Plan was originally prepared as Mississauga Plan (2003) district policies and is readopted by Mississauga Official Plan. A planning study is currently being conducted for Port Credit that will result in a new local area plan.

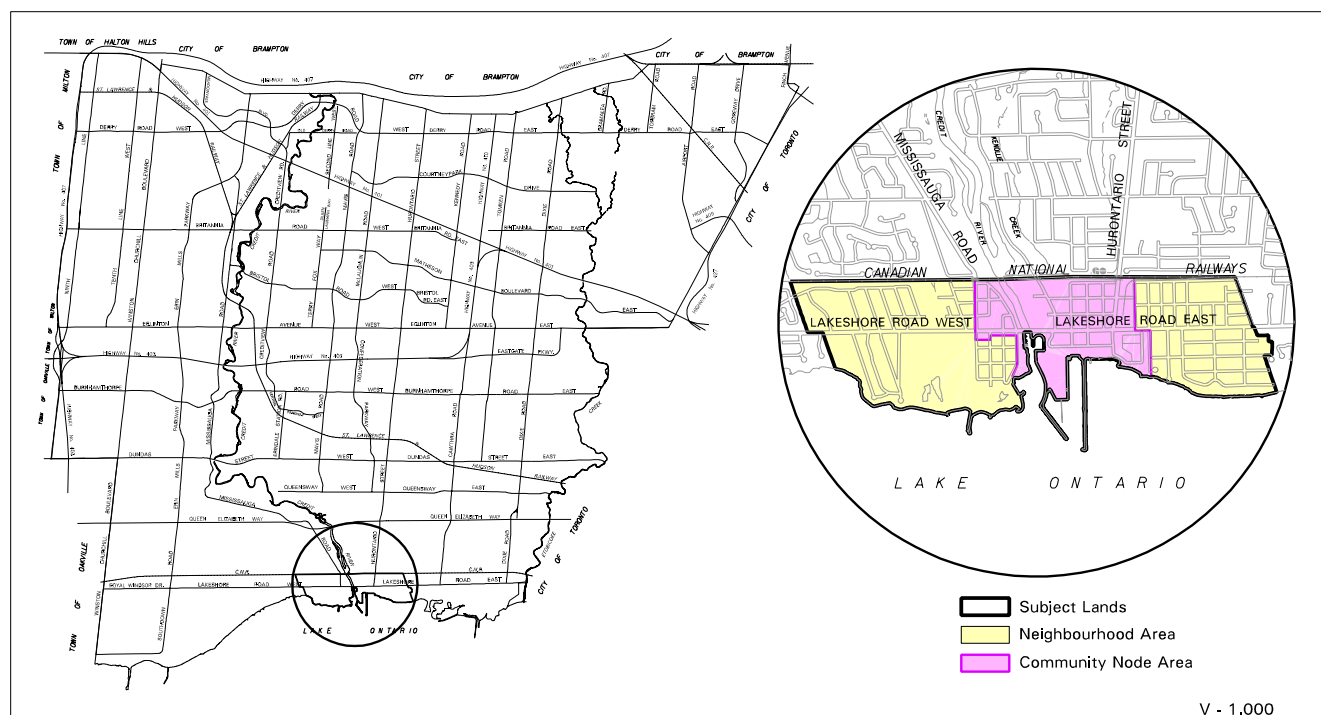


Figure 1: The Port Credit Local Area is located in south central Mississauga and includes areas identified in the City Structure as Community Node and Neighbourhood.

## 2.0 Urban Design Policies

### 2.1 Community Design

For the purpose of this Plan, Port Credit is considered to be composed of:

- four residential Precincts,
- two retail commercial Precincts,
- a Harbour Mixed Use Precinct; and
- a Precinct for the vacant former refinery site.

The locations of these Precincts are identified on Figure 2, Precincts - Port Credit Local Area Plan. Each Precinct includes lands of a variety of land use designations. The Community Design policies must be read in conjunction with the Land Use policies and all other Urban Design policies; together they are intended to encourage a development pattern and character which supports the Development Concept in this Plan.

#### 2.1.1 Community Identity and Focus

- New developments should maintain and enhance the identity of Port Credit as a diverse established community by integrating with the surrounding area and avoiding the establishment of enclaves.
- New developments or redevelopments should be designed to reflect and enhance the Port Credit Business Improvement Area (BIA) **streetscape**.
- The Port Credit Community Node, which includes the Central Residential area, Harbour Mixed Use area and the portion of the Mainstreet Commercial area which connects these two Precincts, will be the focus of activity for the area, combining residential uses, cultural activities, shopping, dining, commerce and recreation.
- An interconnected open space network including the Credit River and the Lake Ontario shoreline is a key feature in the identity of the Port Credit which should be recognized in any development. This will include enhancing visual and physical public access to and along the Lake Ontario waterfront, and enhancing/restoring terrestrial and aquatic habitat potential.

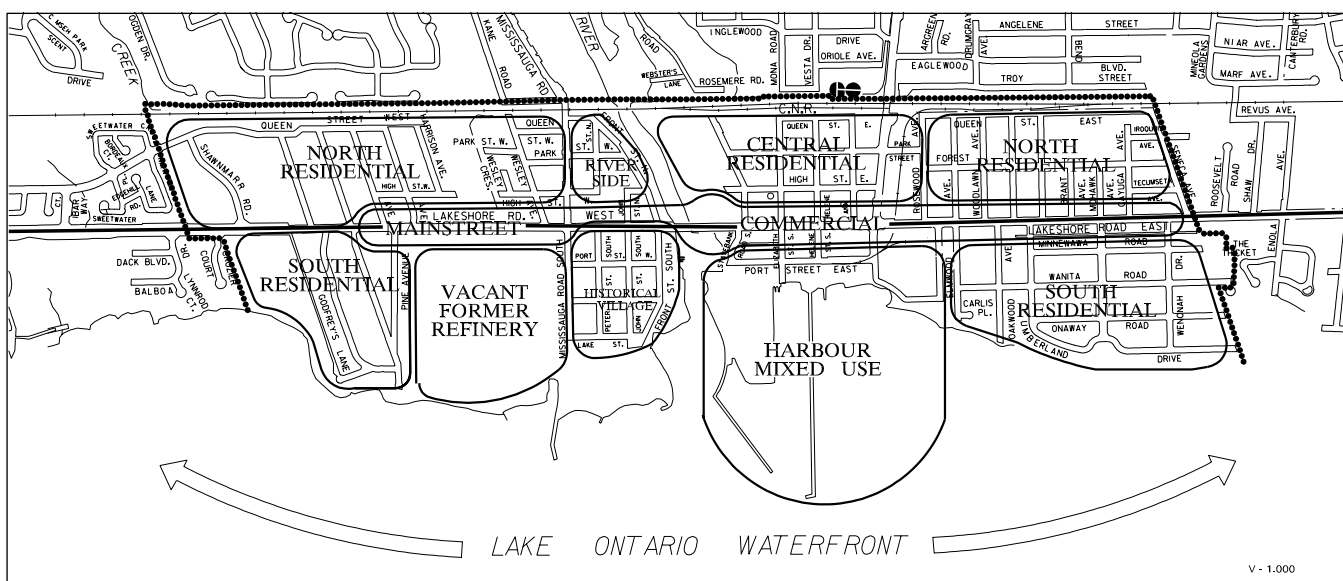


Figure 2: Precincts – Port Credit Local Area Plan

## 2.1.2 Precincts

### 2.1.2.1 North Residential

These stable residential areas should be maintained while allowing for infill which is compatible with and enhances the character of the area.

- a. The predominant characteristics of these areas should be preserved including: the one to two storey building heights; the combination of small building masses on small lots; the juxtaposition of industrial, employment, residential and commercial uses; the well landscaped **streetscapes**; and the regular street grid.
- b. Existing industrial uses along the railway tracks are encouraged to improve their transition to the adjacent residential areas. Where these properties are redeveloped, the development of an appropriate residential **streetscape** will be a priority.

### 2.1.2.2 South Residential

These stable residential areas should be maintained while allowing for infill which is compatible with and enhances the character of the area.

- a. The predominant characteristics of these areas should be preserved including: the one to two storey building heights; the combination of small building masses on small lots (i.e. the scale of the neighbourhood is of modest detached dwellings); the physical and visual access to Lake Ontario; the well landscaped **streetscapes**; and the irregular street grid.
- b. Any development between the lake and the continuous lakefront trail should provide ample side yards to ensure visual access to the lake between buildings.

### 2.1.2.3 Riverside

This is an evolving area which should create a transition from the 15 storey building heights of the Central Residential area to the two storey building

heights of the North Residential area while having regard for the Mississauga Road North **streetscape** and the spatial definition of the Credit River valley.

- a. New Residential High Density development should generally not exceed eight storeys to integrate with adjacent community, residential and parkland development.
- b. Any redevelopment along Mississauga Road North should support its character as a **scenic route** by: maintaining and enhancing the existing mature vegetation; maintaining existing building setbacks; continuing the fine-grained building massing; and, notwithstanding the building heights set in the subsection above of this section, building heights should not exceed four storeys for the half block on the east side of Mississauga Road North.
- c. Pedestrian open spaces should be planned to visually and physically extend Memorial Park into this neighbourhood along High Street West through to Front Street West, and along Park Street West.

### 2.1.2.4 Central Residential

This area has potential for infill and redevelopment, and will have the highest building heights in Port Credit; however, development proposals must avoid adverse impacts on the surrounding areas. Further, the existing character of the area should be maintained, particularly the mature trees and the well landscaped front yards.

- a. Subject to shadowing and overlook concerns, building heights should not exceed 15 storeys and should decrease toward the east, west and north boundaries of this area. Notwithstanding this 15 storey height limit, building heights should not exceed ten storeys along the east side of Stavebank Road North to a depth of half a block (or equivalent distance to the north) between Stavebank Road North and Elizabeth Street, between Anne Street and the west side of Hurontario Street and along the east side of Hurontario Street to a depth of half a block.



Buildings heights should not exceed four storeys on the west side of Stavebank Road.

- b. Development along Hurontario Street should be designed to create an appropriate gateway to Port Credit, maintaining and recreating the generous front yards, the mature vegetation, and the small scale articulation that was typical of the houses along Hurontario Street. Building massing should allow generous setbacks from the street to the main building mass, with smaller two storey elements projecting closer to the streetline. Side yard setbacks should be sufficient to allow generous planting between buildings to avoid the creation of a continuous built wall.
- c. On development sites which extend to Lakeshore Road, buildings or parts of buildings within the Mainstreet Commercial Precinct should conform to the policies for that Precinct.
- d. Buildings should be set back from the street to provide opportunities for landscaping to continue the existing street character of the area.

#### 2.1.2.5 Historical Village

This Precinct (also called the Historical Village of Port Credit) is a mixture of housing, retail commercial and community buildings of many types, representing different eras. While some of the housing stock is relatively new, the neighbourhood contains pockets of housing which are truly historic, representing various time frames and a pleasing sense of "time depth".

- a. The street pattern, its residential character, significant groupings of trees and other natural features, and the historic housing stock should be preserved to maintain the historical associations with the founding of Port Credit.
- b. Mississauga will encourage landscape screening along the west side of Mississauga Road South to buffer the adjacent Imperial Oil site.

- c. Any adaptive re-use should preserve the character of the buildings and properties.
- d. Other areas which are suitable for sensitive infill should display massing and scale sympathetic to the surroundings. Building heights should not exceed two storeys. Any additions, alterations or redevelopment should support the existing historic character, maintain the existing street grid, maintain and enhance the existing mature vegetation and maintain existing building setbacks.

#### 2.1.2.6 Mainstreet Commercial

The Mainstreet Commercial area includes Port Credit's traditional mainstreet and generally extends a half block north and south of Lakeshore Road, with a slightly greater depth at the Credit River. In many cases this coincides with the alignment of a public lane, where it does not, the limit of the area is defined by an extension of a line from the public lane.

- a. Along Lakeshore Road, mixed use developments with street related retail commercial uses and a rhythm of closely spaced storefronts lining the street are encouraged to foster an active pedestrian street and to minimize the apparent width of Lakeshore Road.
- b. Development should maintain existing view corridors to the lake such as along the Hurontario Street extension and create new view corridors, such as, along the new north/south public road and mid-block between Hurontario Street and the new north/south public road.
- c. Building heights should be a minimum of two storeys and a maximum of three storeys.
- d. In accordance with the Port Credit Parking Policies, where possible, parking for this area should be provided on-street or in small, distributed parking lots. It is critical that parking facilities be located and designed to be

compatible with the mainstreet character of the area by including measures such as landscape space, planters or other elements which reinforce the street wall and enhance public amenity.

#### **2.1.2.7 Harbour Mixed Use**

The redevelopment of the St. Lawrence Starch site and the City's initiatives for the Port Credit harbour have the potential to transform this area. Development in this area should be lower scale than the Central Residential area and should step down towards the lake and towards the east edge of the Character Area. Where the market potential permits, retail commercial uses should be extended from the Mainstreet Commercial Precinct towards the lake.

- a. The character of this area should be as an extension of the Mainstreet Commercial Precinct, but with the potential for higher buildings).
- b. Building heights should not exceed three storeys for the entire Harbour Mixed Use area with the exception of buildings fronting onto the north and south sides of Port Street East between the Hurontario Street extension and Helene Street South which may be six storeys high and buildings located on lands municipally known as 48-50 Port Street East which may be five storeys high. Buildings south of Port Street East should step down in tiers from a maximum of six storeys adjacent to Port Street East to a maximum of three storeys along the lands designated as Public Open Space.
- c. On development sites which extend to Lakeshore Road, buildings or parts of buildings within the Mainstreet Commercial Precinct should conform to the policies for that Precinct. Applicants may be required to submit visual analysis and view studies in support of the proposed building heights and massing to demonstrate that the Mainstreet Commercial character will be preserved.
- d. A Public Waterfront Square should be located at the foot of the extension of Hurontario Street. Efforts should be made to maximize public parking opportunities, nearby on-street and on publicly and privately owned lands.
- e. A new public road should connect the Hurontario Street extension to a new public road running north/south between Elmwood Avenue and Hurontario Street. This extension or waterfront road will separate new development from the Lake Ontario waterfront open space and the continuous Waterfront Trail to maximize public access to and along the Lake Ontario waterfront.
- f. Along the west side of the Hurontario Street extension and on the development parcel abutting the Public Waterfront Square, mixed use developments with street related commercial uses and a rhythm of closely spaced storefronts lining the street will be provided to foster an active pedestrian street, and to attract pedestrians to the lake. The parcel abutting the proposed Public Waterfront Square should be developed in a manner which encourages ground floor commercial facing the Public Waterfront Square in a built form offering the potential for weather protection, outdoor display areas and/or restaurants with exterior seating areas.
- g. On-street public parking must be provided to promote street activity and enliven public streets and spaces.
- h. Development should maintain existing view corridors to the lake such as the Hurontario Street extension and create new view corridors, such as, along the new north/south public road and mid-block between Hurontario Street and the new north/south public road.

#### **2.1.2.8 Vacant - Former Refinery**

This Plan does not propose land uses for this area because it is not known to what degree the site can be remediated. However, regardless of potential

future uses, this Precinct should ultimately be developed in a manner which is compatible with the surrounding lands, and which does not compete with the existing focal point at Port Credit harbour.

- a. Building heights should be limited to two to three storeys to be consistent with the adjacent South Residential and Historical Village Precincts.
- b. A public road should be provided in any future development by subdivision separating any new development from the Lake Ontario waterfront open space and the continuous Waterfront Trail to maximize public access to and along the Lake Ontario waterfront. The balance of the street pattern should be consistent with that of adjacent areas.
- c. If this site is used for outdoor storage (such as for boats) or parking, adequate setbacks will be provided from adjacent residential areas, the Lake Ontario Waterfront Trail and associated public parkland to allow appropriate buffer treatment to screen the uses.

## 2.2 Streetscape

The design of the street right-of-way and the design of the lands along the street affect the **streetscape** and should have regard for the following:

- a. on lands adjacent to Lakeshore Road, and in the area south of Lakeshore Road East between the Credit River and Tall Oaks Park, an urban character is appropriate; buildings should be located close to the street and aligned with it to enclose the street space, and gaps in the street wall should be ordered in a coherent fashion.

## 2.3 Buildings and Spaces

In applying the following policies, the effect of buildings and spaces on the surrounding environment should be considered equally with the functionality and aesthetic appeal of the site itself.

- a. Where developable lands separate Lake Ontario from the continuous Waterfront Trail, lot size and building envelopes will create generous side yards to allow glimpses of the lake from the Waterfront Trail.
- b. Notwithstanding the existing high buildings, the area around the intersection of Hurontario Street and Lakeshore Road East should be developed with low-rise buildings to maintain a comfortable street scale and to create an appropriate and compatible transition to the surrounding low-rise area.

## 3.0 Land Use

### 3.1 Residential

#### 3.1.1 Residential Low Density I

The Residential Low Density I designation permits detached dwellings.

Notwithstanding the provisions of the Residential Low Density I designation, a duplex dwelling will be permitted at 23-25 Peter Street South.

#### 3.1.2 Residential Low Density II

The Residential Low Density II designation permits detached, semi-detached, duplex, triplex and street townhouse dwellings.

#### 3.1.3 Residential Medium Density

The Residential Medium Density designation permits townhouse dwellings and all forms of horizontal multiple dwellings, as well as low-rise apartment dwellings. Heights of proposed buildings should not exceed three storeys, unless otherwise specified in the Port Credit Urban Design policies of this Plan.

### 3.1.4 Residential High Density

The Residential High Density designation permits apartment dwellings. Building height should not exceed eight storeys, unless otherwise specified in the Port Credit Urban Design policies of this Plan.

### 3.1.5 Business Employment

Notwithstanding the Land Use Policies of this Plan, only the following permitted uses will apply:

- a. Industrial uses within enclosed buildings including manufacturing, repairing, warehousing, wholesaling and offices up to a maximum **floor space index (FSI)** of 0.5;
- b. other accessory uses, either within industrial buildings or in separate buildings within a complex of associated industrial buildings, provided that they are clearly accessory and occupy a small area in terms of floor space or site coverage relative to the size of the industrial plant; and
- c. community infrastructure.

### 3.1.6 To Be Determined

Studies will be carried out for the area so designated, in consultation with the land owner, neighbouring residents, the Ward Councillor and other interested agencies, and the Plan will be appropriately amended. In determining the ultimate land use for these lands, particular attention will be given to the Natural Areas System, Waterfront Development and Special Site Areas policies of this Plan which apply to these lands.

## 4.0 Transportation

### 4.1 Road System

Lakeshore Road, including the Credit River Bridge, will not be built in excess of four lanes excluding turning lanes and bus bays, unless it can be demonstrated that additional lanes will not result in a major deterioration of the neighbouring residential or retail commercial environment. The implementation of such major roadway modifications will require an amendment to this Plan. The Lakeshore Road crossing of the Credit River is currently over capacity. This situation will be monitored in terms of the impacts of redevelopment, especially west of the Credit River.

Hurontario Street will not be built in excess of four lanes excluding turning lanes and bus bays, unless it can be demonstrated that additional lanes will not result in a major deterioration of the neighbouring residential or retail commercial environment. The implementation of such major roadway modifications will require an amendment to this Plan.

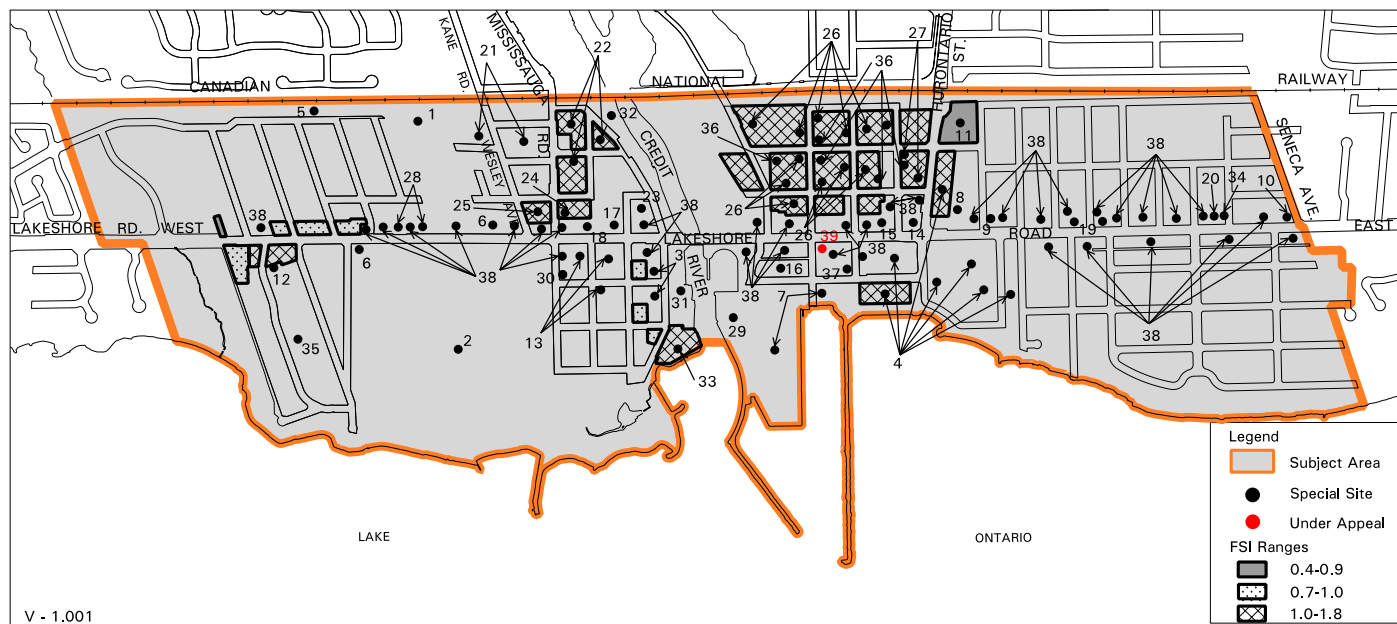
### 4.2 Parking

Larger redevelopment sites will be self-sufficient in the provision of parking, with the parking demands accommodated on-site. Most notably, this assumption would apply to the St. Lawrence Starch and Imperial Oil "South Property" lands, but would also apply to other relatively large sites.

The maintenance of pockets of parking and small lots throughout the area is preferred to the construction of large centrally located parking structures, in addressing the diverse users' needs and the long linear core area.

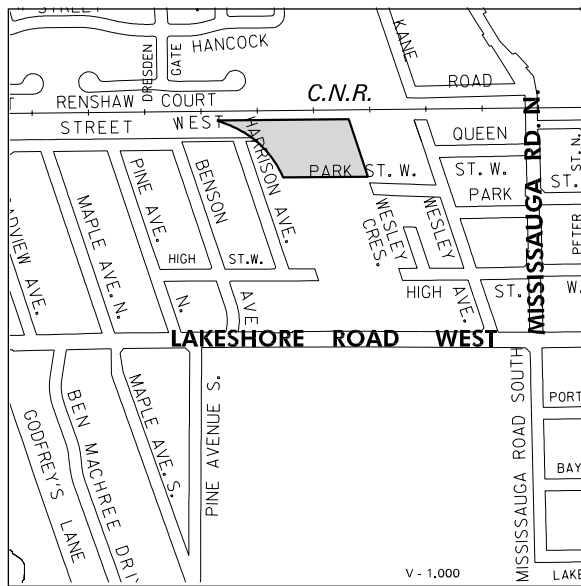
## 5.0 Special Site Policies

There are sites within Port Credit that merit special attention and are subject to the following policies.





## 5.1 Site 1

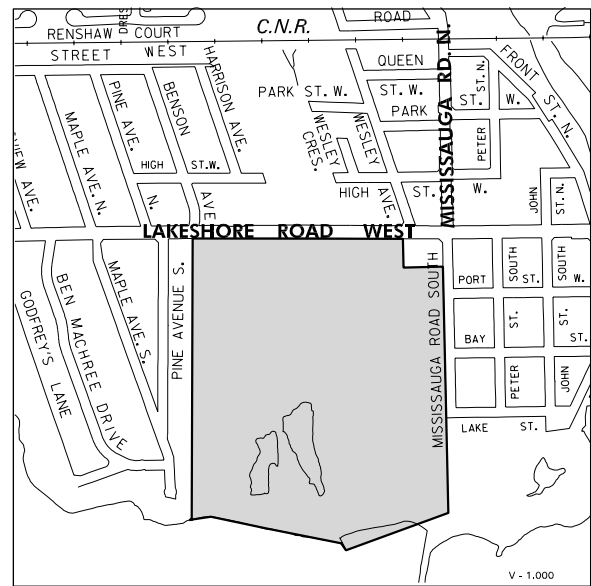


5.1.1 The lands identified as Special Site 1 are located south of the Canadian National Railway and east of Queen Street West.

5.1.2 Notwithstanding the provisions of the Business Employment designation, the following additional policy will apply:

- a. development (including servicing) will be permitted following issuance of the Section 17 Order under the *Environmental Protection Act*, and its registration on title;
- b. permitted industrial uses will be limited to low profile, light industrial uses within enclosed buildings such as storage warehouses and offices directly accessory to the industry; and
- c. vehicular access to the site will be from Lakeshore Road West only, with no access permitted from the surrounding residential streets.

## 5.2 Site 2



5.2.1 The lands identified as Special Site 2 are located south of Lakeshore Road West, west of Mississauga Road South and east of the existing residences on Pine Avenue South.

5.2.2 Notwithstanding the provisions of the To Be Determined, Public Open Space and Greenbelt designations, the uses permitted on this site will be subject to the following additional policies:

- a. development will recognize the scale and enhance the character of adjacent land uses, in particular the old townsite area immediately to the east;
- b. in the review of applications for development of these lands, regard will be had to the concepts and principles expressed in the Port Credit Harbour Transition Master Plan and the Mississauga Waterfront Plan;
- c. a landscaped buffer will be maintained between the site and the Pine Avenue South neighbourhood; and
- d. any proposal for redevelopment or use of the "Vacant - Former Refinery Property" will make adequate provision for implementation of proposals for the future redevelopment of J.C. Saddington Park.

### 5.3 Site 3



5.3.1 The lands identified as Special Site 3 are located on the west side of Front Street South and the south side of Port Street West, south of Lakeshore Road West.

5.3.2 Notwithstanding the Mixed Use designation on these lands, the following additional policies will apply:

- a. townhouses will be permitted only at 28 Front Street South;
- b. buildings, including amenities and signage, whether new or modified, will have a single detached residential appearance consistent with the form, design and scale of the low density residential land use in the Old Port Credit Village Heritage Conservation District, and the use will have a limited impact on the low density residential character of the area. New buildings or modified buildings will comply with the development standards of the adjacent low density residential area;
- c. buildings will not exceed two storeys in height;
- d. any garage, detached or attached, will be located in the side or rear yard behind the front

wall of the building. Any additional parking required will be accommodated to the rear of the lot;

- e. front yards will be mostly landscaped and oriented to Front Street South or Port Street West;
- f. appropriate design treatment should be provided to both façades at street corners;
- g. every effort will be made to have buildings of historic interest, as defined in the Old Port Credit Village Heritage Conservation District Plan, retained within any future redevelopment; and
- h. drive-through facilities will not be permitted.

## 5.4 Site 4



### 5.4.1 Area A

The lands identified as Area A are located on the south side of Lakeshore Road East between Helene Street South and Elmwood Avenue.

Notwithstanding any provisions in this Plan, the following additional policies shall apply to Area A.

#### 5.4.1.1 Development Concept

It is intended that development on Area A (the St. Lawrence Starch Company lands - South Property) will result in a low to mid-rise, urban, pedestrian oriented mixed-use precinct that will provide an attractive destination for local and City residents, with access to the lake and function as a lively mix of live, work, leisure and recreational activities, which will contribute to maximum public use and enjoyment of the waterfront. Built form will be in keeping with the established context of the old town of Port Credit, consisting of low to mid-rise buildings, mixed use, and a strong pedestrian orientation. New mixed-use commercial/residential development will be compatible with the existing street related village retail commercial uses and should occur primarily along Lakeshore Road East, the west side of Hurontario Street, west of the

proposed Community Square and on both sides of Port Street East.

#### 5.4.1.2 Development Objectives

##### a. Residential

Provide an opportunity for a development which offers a variety of dwelling types with built forms that are compatible with existing residential areas.

##### b. Design

Guide community design towards the creation of a visual identity for the St. Lawrence Starch Company lands and provide for a building massing and character that will be consistent with the Development Concept Policies of this Plan.

#### 5.4.1.3 Urban Design Policies

##### a. Community Design

For the purposes of Area A, the St. Lawrence Starch Company lands are composed of two Precincts, as identified on Figure 2, Precincts - Port Credit Local Area Plan. The Urban Design Policies of this Plan must be read in conjunction with the Land Use Policies of this Plan. Together, they are intended to encourage an urban form and character which supports the Development Concept in Area A.

##### Community Identity and Focus

An interconnected open space network including the Credit River, the Lake Ontario shoreline and portions of the tableland woodlot is the key feature in the identity of the area which should be recognized in any development. This will include enhancing visual and physical access to and along the waterfront, and enhancing/restoring terrestrial and aquatic habitat potential.

## Mainstreet Commercial Precinct

The Mainstreet Commercial Precinct includes Port Credit's traditional mainstreet and generally extends a half a block north and south of Lakeshore Road East.

- Building heights should generally not exceed two storeys. Buildings which front onto the south side of Lakeshore Road East between Hurontario Street and Elmwood Avenue are encouraged to be developed as mixed commercial/residential "live/work" units and shall not exceed a height of three storeys;
- Parking facilities should be located and designed to be compatible with the mainstreet character of the area by including measures such as landscape space, planters or other elements which reinforce the **streetscape** and enhance public amenity;
- One parking lot within the Mainstreet Commercial Precinct is to be located on the site of the former St. Lawrence Starch administration building at the southeast corner of Hurontario Street and Lakeshore Road East. The remaining areas for parking will be provided along Lakeshore Road East and/or on lands on the west side of Hurontario Street. Parking should only be provided on-street, underground or in small parking lots ideally located behind buildings;
- Future development will be compatible in form and scale with the former St. Lawrence Starch administration building which is a historically designated building under the *Ontario Heritage Act*. This building should be considered when designing future development in this Precinct;
- A new open space is to be created at the northeastern edge of Area A to integrate and preserve a portion of the existing woodlot into the new development; and
- A new Community Square will be situated along Lakeshore Road East. Street related activities such as restaurants and shops are to be encouraged in the grade levels of buildings on the west side of Hurontario Street, north of Port Street East, addressing the square. Surface parking lots or above grade structured parking are to be minimized adjacent to the Community Square.

## Harbour Mixed Use Precinct

Development in this area should be low in scale and provide a suitable transition in building mass towards the established low density scale residential neighbourhood to the east and to the higher scale buildings west of the Hurontario Street extension. Commercial uses may be extended south from the Mainstreet Commercial Precinct towards the lake, along the west side of Hurontario Street.

- The Harbour Mixed Use Precinct is an extension of the Mainstreet Commercial Precinct;
- Building heights shall not exceed three storeys in height for the entire Harbour Mixed Use Precinct with the exception of buildings fronting onto the north and south sides of Port Street East which may be six storeys high. Buildings south of Port Street East should step down gradually from a maximum of six storeys along the Port Street East frontage to a maximum of three storeys along the waterfront park;
- On development sites which extend to Lakeshore Road East, buildings or parts of buildings within the Mainstreet Commercial Precinct should conform to the policies for that Precinct;
- A Public Waterfront Square will be located at the foot of the extension of Hurontario Street. Public parking may be provided below the Public Waterfront Square or at

other locations on privately or publicly owned lands;

- Along the west side of Hurontario Street and on the development parcel abutting the Public Waterfront Square, mixed use developments with street related commercial uses and a rhythm of closely spaced storefronts lining the street will be provided to foster an active pedestrian street, and to attract pedestrians to the lake. The parcel abutting the Public Waterfront Square should be developed in a manner which encourages at grade commercial facing the Public Waterfront Square in a built form offering the potential for weather protection, outdoor display areas and/or restaurants with exterior seating areas;
- Development should maintain existing view corridors to the lake. The creation of new view corridors should be a major consideration when setting out new streets and the development pattern for the area;
- On-street public parking must be provided to promote street activity and enliven public streets and spaces;
- The development pattern should extend the street and block pattern that exists in the surrounding Port Credit neighbourhoods, consisting of an open grid street pattern with blocks generally rectangular in shape;
- To maximize public access to and along the waterfront, a public east-west waterfront road shall separate new development from the waterfront open space and the continuous Waterfront Trail;
- A waterfront open space, which includes the Public Waterfront Square, should extend the full width of Area A along the Lake Ontario shoreline. Development should face the open space. Reverse back lotting onto the open space is not permitted. The minimum width of the waterfront open space, west of the Public Waterfront

Square, south of Port Street East, will be 30 m; and

- Development should maintain the portions of the woodlot adjacent to Elmwood Avenue as a private open space block in conjunction with medium density residential development.

#### b. Buildings and Spaces

In applying the following policies, the effect of buildings and spaces on the surrounding environment should be considered equally with the functionality and aesthetic appeal of the site itself.

- Developable lands should not separate Lake Ontario from the continuous Waterfront Trail; and
- Residential areas should manifest a range of building types and forms to offer choices to people wishing to reside in this area.

#### 5.4.1.4 Land Use

##### a. Residential

Housing will be provided within one of the following residential density categories: Residential Medium Density, Residential High Density and Mixed Use.

##### Residential Medium Density

The Residential Medium Density designation permits townhouse development, either street related or in clusters. Building heights should not exceed three storeys unless otherwise specified in the Urban Design Policies of this Plan.

##### Residential High Density

The Residential High Density designation permits medium-rise apartment buildings. Medium-rise buildings will not exceed six storeys in height unless otherwise specified in the Urban Design Policies of this Plan. Ground related retail commercial uses are permitted.



b. Commercial

Mixed Use

Lands designated Mixed Use will provide a mix of pedestrian oriented street related retail commercial uses, offices, overnight accommodation, **community facilities** and open space. Residential uses will be permitted in combination with commercial uses. Drive-through facilities will not be permitted. The Mixed Use designation applies to lands between Lakeshore Road East and Port Street East, west of Hurontario Street and on the south side of Lakeshore Road East, between Hurontario Street and the new north/south public road, where the existing scale, form, and pedestrian orientation of development is encouraged to maintain and enhance the character of the former town centre.

- The lands bounded by Lakeshore Road East and Port Street East, west of Hurontario Street will be subject to an overall development total **floor space index (FSI)** of 2.0, of which residential uses will be limited to a maximum **floor space index (FSI)** of 1.5. Medium rise mixed use buildings should not exceed six storeys;
- The lands located on the south side of Lakeshore Road East between Hurontario Street and the new north/south public road permit street related commercial uses in conjunction with residential uses, and will be subject to an overall development total **floor space index (FSI)** of 1.5 of which commercial uses will be limited to a maximum **floor space index (FSI)** of 0.2; and
- The lands located on the southeast corner of Lakeshore Road East and Hurontario Street are occupied by the existing "Administration Building" and intended to be used for commercial uses.

5.4.1.5 Site Decommissioning

The lands in Area A have been identified as a **contaminated site**. Notwithstanding the lands use designations prescribed for the subject lands, the uses permitted on these lands will be subject to the following additional policy:

- a. City approval, for development on, or in proximity to, these lands will be subject to clean up and verification, in accordance with Provincial Government legislation and guidelines. Studies will be required of the proponent indicating levels of contamination, remediation plans, and clean up verification prior to development approval. Such studies will be prepared to Provincial Government specifications and address all concerns of the City.

5.4.1.6 Transportation

- a. Parking

Sites will be self-sufficient in the provision of parking, with the parking demands accommodated on-site or on neighbouring sites.

Parking requirements associated with individual developments will be provided at the rear of lots or underground. Parking lots abutting street frontages will not be permitted, except where no other alternative is available. Adequate design treatments must be established for the street frontage to maintain a continuous safe, urban **streetscape**. Opportunities for on-street parking should be maximized.

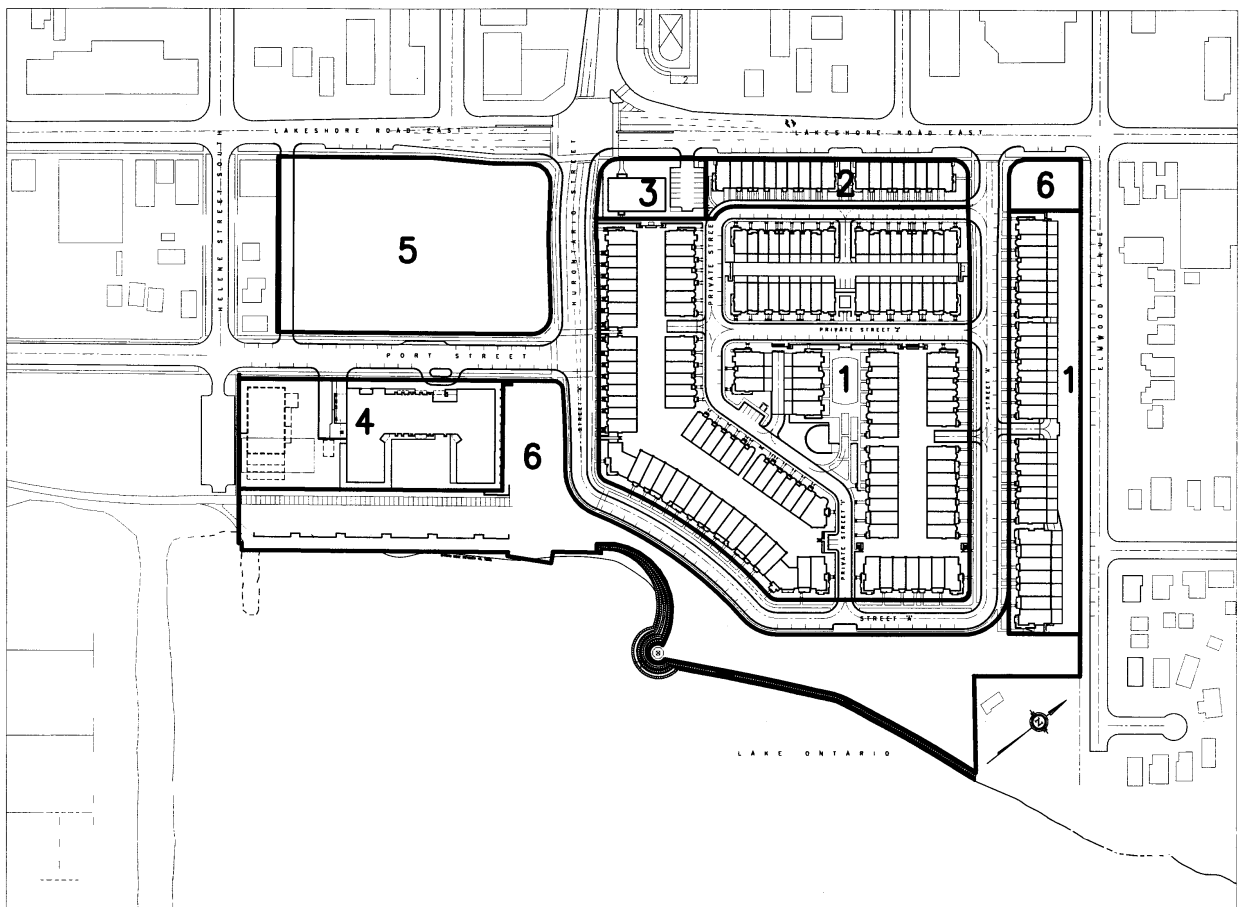


Figure 3: Concept Plan – Block Layout (South Property)

### Concept Plan - Block Layout (South Property)

PARCEL	LOT AREA (m <sup>2</sup> )	COMMERCIAL Maximum Gross Floor Area (m <sup>2</sup> )	RESIDENTIAL Maximum Gross Floor Area		TOTAL Maximum GFA (m <sup>2</sup> )	<b>FLOOR SPACE INDEX (FSI)</b> /DENSITY
			m <sup>2</sup>	Units		
MIXED USE						
2	3 350	720 (0.2 FSI)	4 300 (1.3 FSI)	18	5 020	1.5 FSI
3	1 600	1 560 (1.0 FSI)			1 560	1.0 FSI
5*	12 700	8 600 (0.7 FSI)	19 050 (1.5 FSI)	150	25 400	2.0 FSI
RESIDENTIAL MEDIUM DENSITY						
1	43 950		42 500	167	42 500	38 uph
RESIDENTIAL HIGH DENSITY						
4	8 116	460 (0.06 FSI)	20 090 (2.47 FSI)	95	20 290	2.5 FSI
TOTAL (BLDG. AREAS)	69 716	11 340	85 650	430	94 770	
OPEN SPACE						
6	17 884					
PUBLIC ROADS	13 200					
GREENBELT**	3 900					
TOTAL SITE AREA	104 700					

#### **FSI: Floor Space Index**

\* Maximum residential **FSI** = 1.5, maximum total **FSI** = 2.0

\*\* Greenbelt includes armourstone seawall, and water lots.

For information purposes only, the overall **Floor Space Index (FSI)** for all parcels (building areas) is 1.35.

### 5.4.2 Area B

The lands identified as Area B are located on the east side of Hurontario Street between Lakeshore Road East and Park Street East.

Notwithstanding any provisions in this Plan, the following additional policies will apply:

#### 5.4.2.1 Development Concept

Development on Area B will be of a higher density than the abutting Area A lands, south of Lakeshore Road East. The primarily mid-rise residential built form along Hurontario Street will be pedestrian oriented and will establish a transition between the area of high buildings to the west and the lower rise buildings to the east.

A significant building in both height and architecture will create a visual landmark near the northeast corner of Lakeshore Road East and Hurontario Street. A **tall building** stepped back from the intersection is appropriate due to this location's unique and prominent role as both the gateway into Port Credit from the north and the starting point of the city's most important north-south road, Hurontario Street. Its proximity to the Port Credit GO Transit Station and its location fronting onto a Major Transit corridor enhances its residential intensification potential. A **tall building** will also balance the massing of the adjacent tall apartment building located west of Hurontario Street.

Street related commercial uses within a low-rise building podium at the south limit of the site will integrate well with the existing built form, massing and retail commercial uses along Lakeshore Road East. A significant step back of the tower portion from the low-rise Lakeshore Road East building façade is important to maintain the traditional mainstreet pedestrian experience and **streetscape**.

#### 5.4.2.2 Urban Design Policies

##### a. Community Design

For the purposes of Area B, the subject lands are composed of two Precincts, as identified on Figure 2: Precincts - Port Credit Local Area Plan.

To encourage an urban form and character which supports the Development Concept for Area B, the Urban Design and Land Use Policies of this Plan will apply, except as follows:

##### Central Residential Precinct

The Central Residential Precinct will have mid-rise building heights and serve as a transition between the high buildings to the west and the lower rise buildings to the east.

- Building heights will not exceed six storeys, except for the portion between High Street East and Park Street East, which will not exceed seven storeys.
- A building podium of between two and six storeys will be developed close to Hurontario Street.
- Side yard setbacks will be sufficient to allow planting between buildings. As well, side yard setbacks should be provided opposite the alignment of High Street East, permitting access to the east.

##### Mixed Use Precinct

The Mixed Use Precinct includes Port Credit's traditional mainstreet and generally extends a half a block north and south of Lakeshore Road East.

- A building not exceeding 22 storeys and setback from the northeast corner of Lakeshore Road East and Hurontario Street is permitted. The tower portion of this building will be stepped back from a two storey building podium adjacent to Lakeshore Road East and a six storey building podium adjacent to Hurontario Street.
- The two storey Lakeshore Road East building podium will be located close to the street edge, achieving similar massing to that found along this traditional mainstreet.

- The six storey Hurontario Street building podium will achieve a well proportioned sense of street enclosure for pedestrians and others travelling along this street. The building podium will be located close to the street edge.

or underground. Parking lots abutting street frontages will not be permitted. Adequate design treatments must be established for the street frontage to maintain a continuous safe, urban **streetscape**.

#### 5.4.2.3 Land Use

##### a. Residential High Density

Buildings will not exceed six storeys, except between High Street East and Park Street East, which will not exceed seven storeys.

##### b. Mixed Use

A maximum of 1 150 m<sup>2</sup> of gross floor area is to be devoted to commercial uses.

##### c. Overall Development Permissions

Density is expressed in terms of **floor space index (FSI)**, which is defined as the ratio of the gross floor area of all buildings on a site to the net developable area of that site. For apartment blocks, the net developable area includes the lands for residential units, internal road and parking, landscaped areas, private open space and other associated amenities.

- Overall development is permitted up to a maximum **floor space index (FSI)** of 4.7 for Area B.
- A maximum of 214 apartment dwelling units are permitted within Area B.
- A maximum of 150 retirement dwelling units are permitted within Area B.

#### 5.4.2.4 Transportation

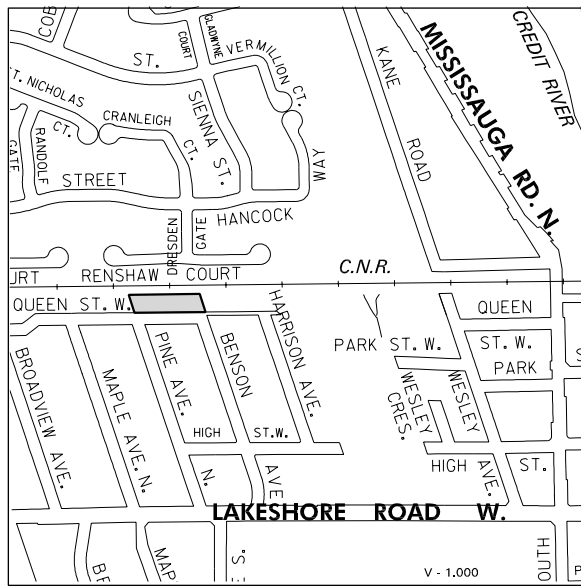
##### a. Parking

Sites will be self-sufficient in the provision of parking, with the parking demands accommodated on-site.

Parking requirements associated with individual developments will be provided at the rear of lots



## 5.5 Site 5

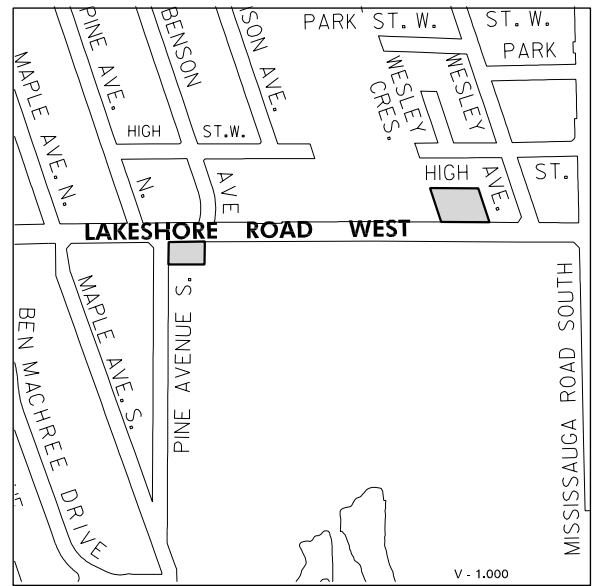


5.5.1 The lands identified as Special Site 5 are located on the north side of Queen Street West between Benson Avenue and Pine Avenue.

5.5.2 Notwithstanding the Business Employment designation on these lands, the following additional policy will apply:

- a. screened outdoor storage will be permitted.

## 5.6 Site 6

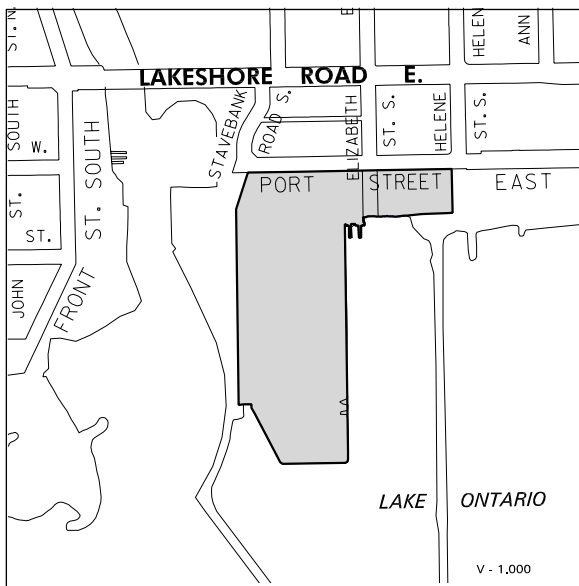


5.6.1 The lands identified as Special Site 6 are located on the north side of Lakeshore Road West, west of Wesley Avenue and on the south side of Lakeshore Road West, east of Pine Avenue South.

5.6.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. the existing motor vehicle sales and rental will be permitted; and
- b. drive-through facilities will not be permitted.

## 5.7 Site 7



5.7.1 The lands identified as Special Site 7 are located south of Port Street East and east of the Credit River.

5.7.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. boat repair, service and storage will be permitted; and
- b. drive-through facilities will not be permitted.

## 5.8 Site 8



5.8.1 The lands identified as Special Site 8 are located on the north side of Lakeshore Road East, east of Hurontario Street.

5.8.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. the existing gas bar and a motor vehicle wash will be permitted; and
- b. drive-through facilities will not be permitted.

## 5.9 Site 9

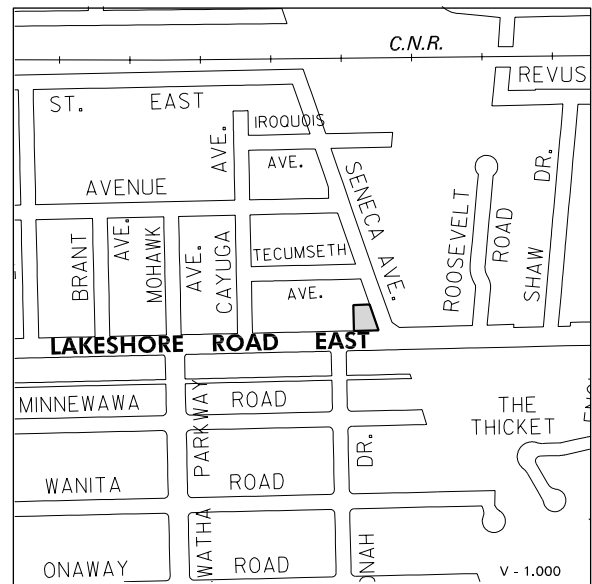


5.9.1 The lands identified as Special Site 9 are located at the northeast corner of Lakeshore Road East and Rosewood Avenue.

5.9.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. the existing motor vehicle repair garage will be permitted; and
- b. drive-through facilities will not be permitted.

## 5.10 Site 10

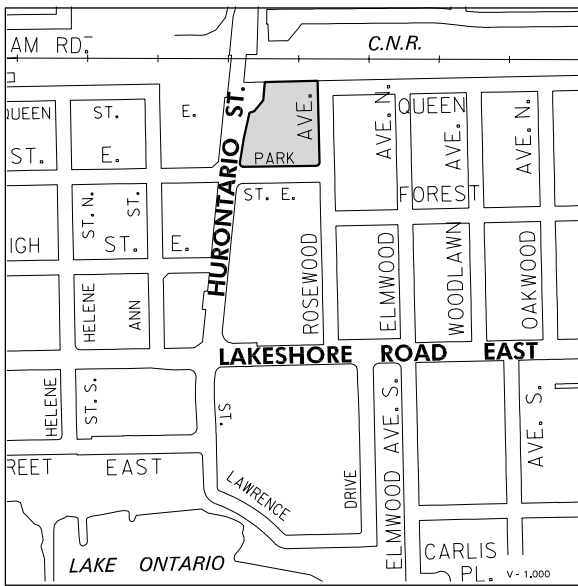


5.10.1 The lands identified as Special Site 10 are located at the northwest corner of Lakeshore Road East and Seneca Avenue.

5.10.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. the existing motor vehicle wash will be permitted; and
- b. drive-through facilities will not be permitted.

## 5.11 Site 11



5.11.1 The lands identified as Special Site 11 are located on the north side of Park Street East between Hurontario Street and Rosewood Avenue.

5.11.2 Notwithstanding the Residential Medium Density designation and the Urban Design Policies, Central Residential provisions, the following additional policies will apply:

- in addition to the uses permitted under the Residential Medium Density designation, townhouse development, either street related or in clusters will be permitted; and
- building heights should not exceed three storeys.

## 5.12 Site 12



5.12.1 The lands identified as Special Site 12 are located on the east side of Ben Machree Drive, south of Lakeshore Road West.

5.12.2 Notwithstanding the provisions of the Residential Low Density I designation, the following additional policy will apply:

- a residential building containing three apartment dwelling units may also be permitted, provided that no dwelling units are located below-grade.

The map shows a grid of streets in the Lakeshore West area of Mississauga. The main north-south street is Mississauga Rd. N. (labeled vertically). Other north-south streets include Queen St. N., St. W., St. N., Peter St., John St., St. S., South St., South W., Lake St., and Front St. S. East-west streets include Front St. N., Credit River, St. W., High St., and West. A large area is shaded in grey, representing the proposed development site. The map also shows the Credit River, Lake St., and the Credit River Bridge. The map is labeled 'LAKESHORE WEST' and 'MISSISSAUGA RD. N.'.

- a. special care is required to preserve the character of the buildings and properties of the community infrastructure along Lakeshore Road West and Port Street West. Particular care should be taken in the preservation of the exterior character of the structures, and related exterior open space areas. The existing cemetery at the corner of Lakeshore Road West and John Street South will be preserved; and
- b. drive-through facilities will not be permitted.

- a. the maximum height of the existing apartment building with street related commercial uses, will be 20 storeys; and
- b. drive-through facilities will not be permitted.



## 5.15 Site 15



5.15.1 The lands identified as Special Site 15 are located north of Lakeshore Road East, east of Helene Street North and west of Ann Street.

5.15.2 Notwithstanding the Urban Design Policies and the provisions of the Mixed Use designation, the following additional policies will apply:

- maximum height of the existing apartment building will be five storeys; and
- drive-through facilities will not be permitted.

## 5.16 Site 16



5.16.1 The lands identified as Special Site 16 are located on the north side of Port Street, west of Elizabeth Street South, east of Stavebank Road South.

### 5.16.2 Area A

Notwithstanding the Urban Design Policies, Harbour Mixed Use provisions, for the lands identified as Area A, the existing apartment hotel with a maximum building height of 14 storeys will be permitted. Drive-through facilities will not be permitted.

### 5.16.3 Area B

Notwithstanding the Urban Design Policies, Harbour Mixed Use provisions, for the lands identified as Area B, the existing apartment hotel with a maximum building height of seven storeys will be permitted. Drive-through facilities will not be permitted.



## 5.19 Site 19

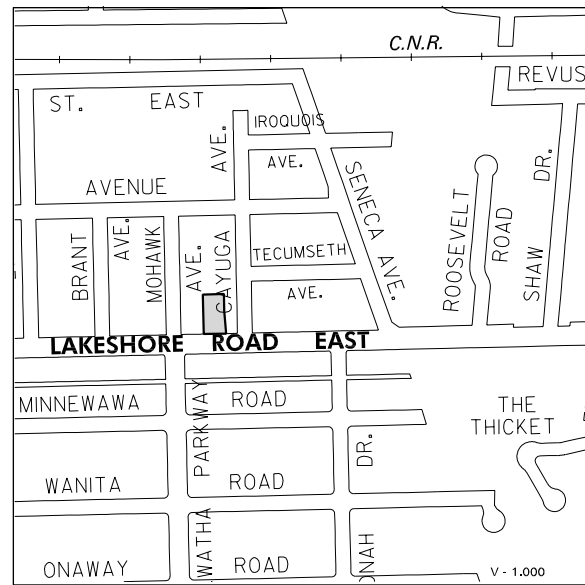


5.19.1 The lands identified as Special Site 19 are located on the north side of Lakeshore Road East, between Woodlawn Avenue and lands west of Briarwood Avenue.

5.19.2 Notwithstanding the Urban Design Policies and the provisions of the Mixed Use designation, the following additional policies will apply:

- the maximum height of the existing apartment buildings will be four storeys; and
- drive-through facilities will not be permitted.

## 5.20 Site 20

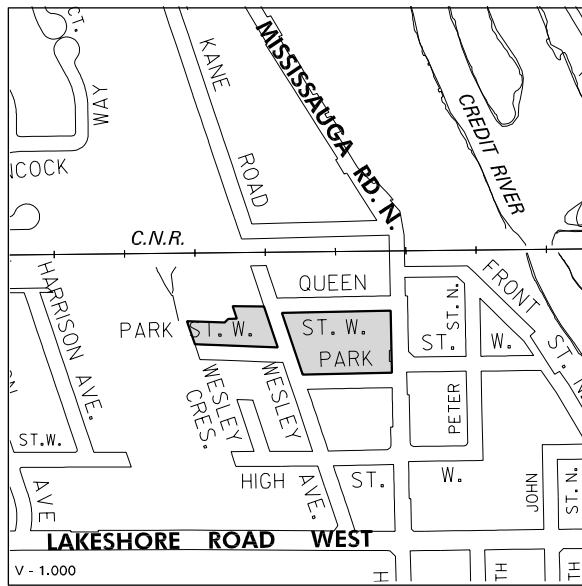


5.20.1 The lands identified as Special Site 20 are located on the north side of Lakeshore Road East, west of Cayuga Avenue.

5.20.2 Notwithstanding the Urban Design Policies and the provisions of the Mixed Use designation, the following additional policies will apply:

- the maximum height of the existing apartment building will be five storeys; and
- drive-through facilities will not be permitted.

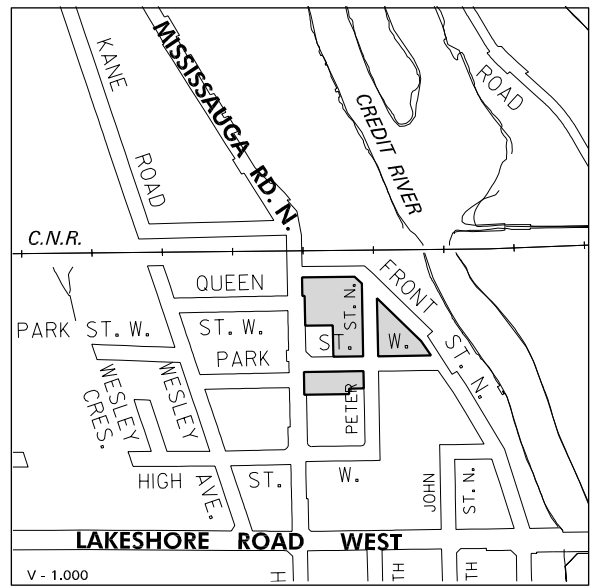
## 5.21 Site 21



5.21.1 The lands identified as Special Site 21 are located on the north side of Park Street West, south of Queen Street West and west of Mississauga Road North.

5.21.2 Notwithstanding the provisions of the Residential Medium Density designation, existing detached, existing duplex and existing triplex dwellings will be permitted.

## 5.22 Site 22



5.22.1 The lands identified as Special Site 22 are located north of Park Street West, east and west of Peter Street North and the lands south of Park Street West, between Mississauga Road North and Peter Street North.

5.22.2 Notwithstanding the provisions of the Residential High Density designation, existing detached, existing semi-detached, existing duplex, existing triplex and existing horizontal multiple dwellings will be permitted.

## 5.23 Site 23



5.23.1 The lands identified as Special Site 23 are located on the west side of Front Street North, north of Lakeshore Road West.

### 5.23.2 Area A

Notwithstanding the provisions of the Residential Medium Density designation, for the lands identified as Area A, an existing detached dwelling and offices will be permitted.

### 5.23.3 Area B

Notwithstanding the provisions of the Residential Medium Density designation, for the lands identified as Area B, the existing detached dwellings, existing duplex and existing apartment dwelling will be permitted.

## 5.24 Site 24



5.24.1 The lands identified as Special Site 24 are located on the east side of Mississauga Road North, south of High Street West.

5.24.2 Notwithstanding the provisions of the Residential High Density designation, existing horizontal multiple dwellings will be permitted.



QUEEN

ST. W.

PARK

WESLEY

HIGH AVE.

ST. W.

PETER

JOHN ST. N.

PORT ST.

W.

C.N.R.

FRONT ST. N.

CREDIT RIVER

MISSISSAUGA RD. N.

LAKESHORE ROAD WEST

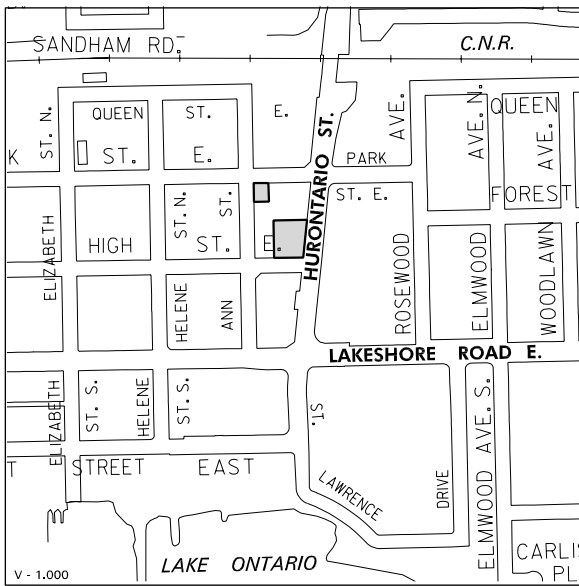
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5.25.2 Notwithstanding the provisions of the Residential High Density designation, existing detached, existing duplex and existing triplex dwellings will be permitted.

[illegible]

Area	Floor Space Index (FSI) Range	Maximum Building Height
A	1.0-2.5 <i>FSI</i>	13 storeys
B	1.0-2.5 <i>FSI</i>	12 storeys
C	1.0-2.5 <i>FSI</i>	13 storeys
D	1.0-2.5 <i>FSI</i>	11 storeys
E	1.0-2.5 <i>FSI</i>	8 storeys
F	1.0-4.0 <i>FSI</i>	28 storeys
G	1.0-2.5 <i>FSI</i>	10 storeys
H	1.0-2.4 <i>FSI</i>	16 storeys
I	1.0-2.8 <i>FSI</i>	14 storeys
J	1.0-2.5 <i>FSI</i>	10 storeys
K	1.0-2.5 <i>FSI</i>	10 storeys
L	1.0-2.5 <i>FSI</i>	12 storeys
M	1.0-2.5 <i>FSI</i>	13 storeys
N	1.0-2.8 <i>FSI</i>	14 storeys

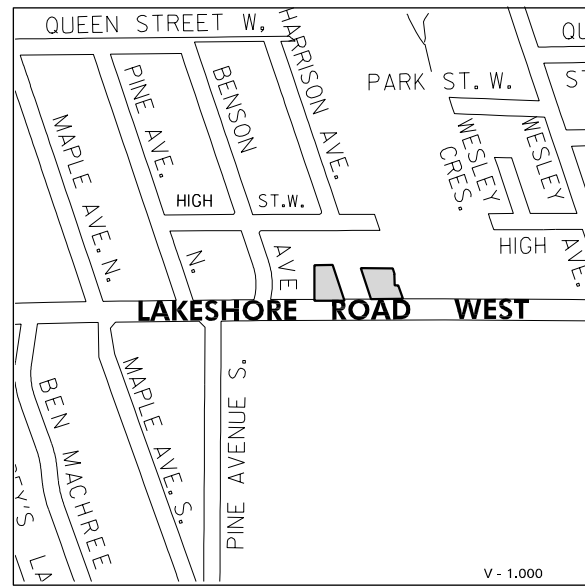
## 5.27 Site 27



5.27.1 The lands identified as Special Site 27 are located west of Hurontario Street, north of High Street East.

5.27.2 Notwithstanding the provisions of the Residential High Density designation, offices will be permitted in existing detached dwellings.

## 5.28 Site 28



5.28.1 The lands identified as Special Site 28 are located on the north side of Lakeshore Road West, east of Benson Avenue.

5.28.2 Notwithstanding the provisions of the Mixed Use designation the following additional policies will apply:

- a. the existing motor vehicle repair uses will be permitted; and
- b. drive-through facilities will not be permitted.

## 5.29 Site 29



5.29.1 The lands identified as Special Site 29 are located west of Stavebank Road South, south of Lakeshore Road East.

5.29.2 Notwithstanding the provisions of the Public Open Space and Greenbelt designations the existing restaurant and marina will be permitted.

## 5.30 Site 30



5.30.1 The lands identified as Special Site 30 are located east of Mississauga Road South, north of Port Street West.

5.30.2 Notwithstanding the provisions of the Residential Low Density I designation, the following additional policy will apply:

- a. offices, restaurants and personal service uses will be permitted.

### 5.31 Site 31



5.31.1 The lands identified as Special Site 31, Marina Park, are located at the southeast corner of Lakeshore Road West and Front Street South. Marina Park is to be developed as an integral component of the Port Credit Harbour and Historical Village in terms of complementary uses and design while recognizing its potential to establish a vibrant river and village edge.

5.31.2 Notwithstanding the provisions of the Public Open Space and Greenbelt designations on these lands, the following additional policies will apply:

- a. in addition to the uses permitted by the Public Open Space designation, the following uses will be permitted subject to, among other matters, the approval of Credit Valley Conservation:
  - community infrastructure;
  - **secondary offices;**
  - retail commercial uses, except for commercial uses with a drive-through facility;
  - conference centre; and
  - marina; and

- b. in addition to the uses permitted by the Greenbelt designation, a marina will be permitted;
- c. Marina Park will be developed as an integral part of both Port Credit Harbour and the Old Port Credit Village Heritage Conservation District, having regard for the following principles:
  - public access to the Credit River will be enhanced;
  - views of the Credit River from both the Port Street West and the Bay Street road allowances will be extended through the site;
  - building height will not exceed two storeys;
  - buildings will be articulated to reduce the perception of bulk;
  - buildings will be oriented to the Credit River, Front Street South and the street grid;
  - pedestrian links along the Credit River through the site and both north and south of it will be pursued, where feasible; and
  - historical interpretation of the site will be integrated into any future development; and
- d. prior to any development, the City will prepare a Master Plan to address the future use and layout of Marina Park. The Master Plan should be prepared in consultation with the public, and should address, among other matters, the future layout of the site, archaeological assessment, historical interpretation opportunities, and the feasibility of a river trail; and
- e. when the Master Plan has been prepared and approved by City Council, the "holding" designation in the Zoning By-law may be removed.

### 5.32 Site 32



5.32.1 The lands identified as Special Site 32 are located south of the Canadian National Railway tracks, east of Front Street North.

5.32.2 Notwithstanding the provisions of the Private Open Space and Greenbelt designations, the existing private club will also be permitted.

### 5.33 Site 33

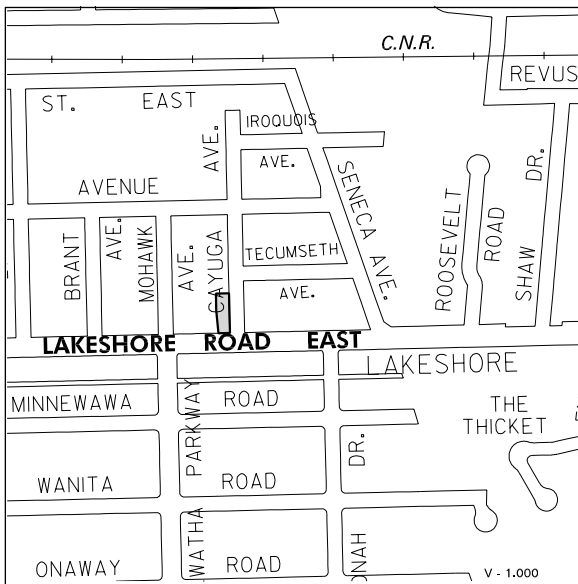


5.33.1 The lands identified as Special Site 33 are located east of Front Street South north of Lake Street.

5.33.2 Notwithstanding the Residential High Density and Greenbelt designations and the Urban Design Policies, Historical Village provisions, the maximum height of the existing apartment building will be 20 storeys.



### 5.34 Site 34



5.34.1 The lands identified as Special Site 34 are located at the northwest corner of Lakeshore Road East and Cayuga Avenue.

5.34.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. apartment dwellings with street related commercial uses with a maximum **floor space index (FSI)** of 1.5 and a maximum building height of six storeys will be permitted; and
- b. drive-through facilities will not be permitted.

### 5.35 Site 35

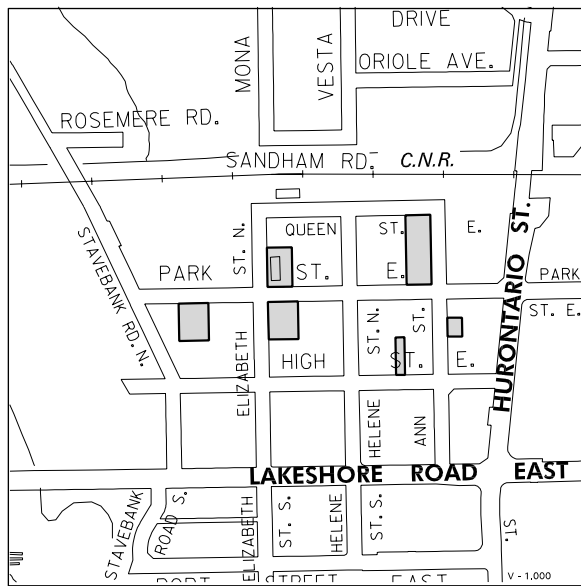


5.35.1 The lands identified as Special Site 35 are located on the east side of Ben Machree Drive, south of Lakeshore Road West.

5.35.2 Notwithstanding the provisions of the Residential Low Density I designation, the following additional policy will apply:

- a. the existing triplex will also be permitted.

### 5.36 Site 36



5.36.1 The lands identified as Special Site 36 are located north of Lakeshore Road East, south of the Canadian National Railway, east of Stavebank Road and west of Hurontario Street.

5.36.2 Notwithstanding the provisions of the Residential High Density designation, existing detached, existing duplex and existing triplex dwellings will also be permitted.

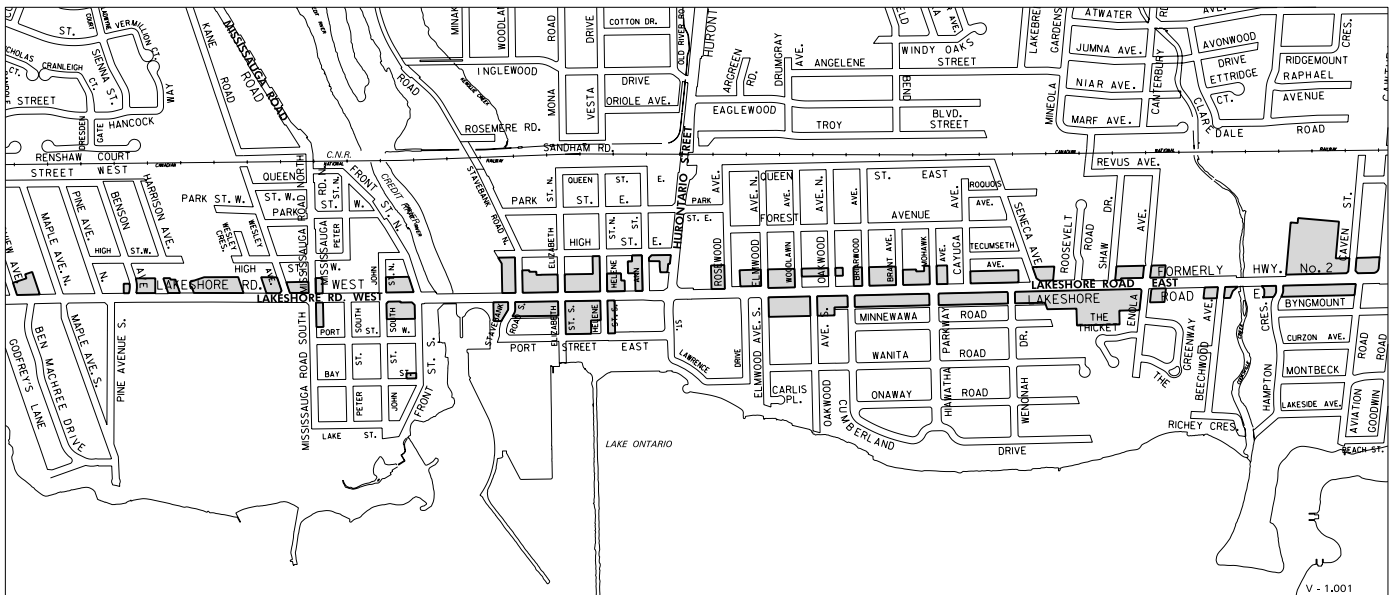
### 5.37 Site 37



5.37.1 The lands identified as Special Site 37 are located at the northwest corner of Port Street East and Helene Street South.

5.37.2 Notwithstanding the Urban Design Policies, Harbour Mixed Use provisions, the maximum height of the existing apartment building will be five storeys.

## 5.38 Site 38



5.38.1 The lands identified as Special Site 38 are located on the north and south sides of Lakeshore Road West and East between Mississauga Road and Hurontario Street and on the south side of High Street, west of Hurontario Street.

5.38.2 Notwithstanding the provisions of the Mixed Use designations, drive-through facilities will not be permitted.

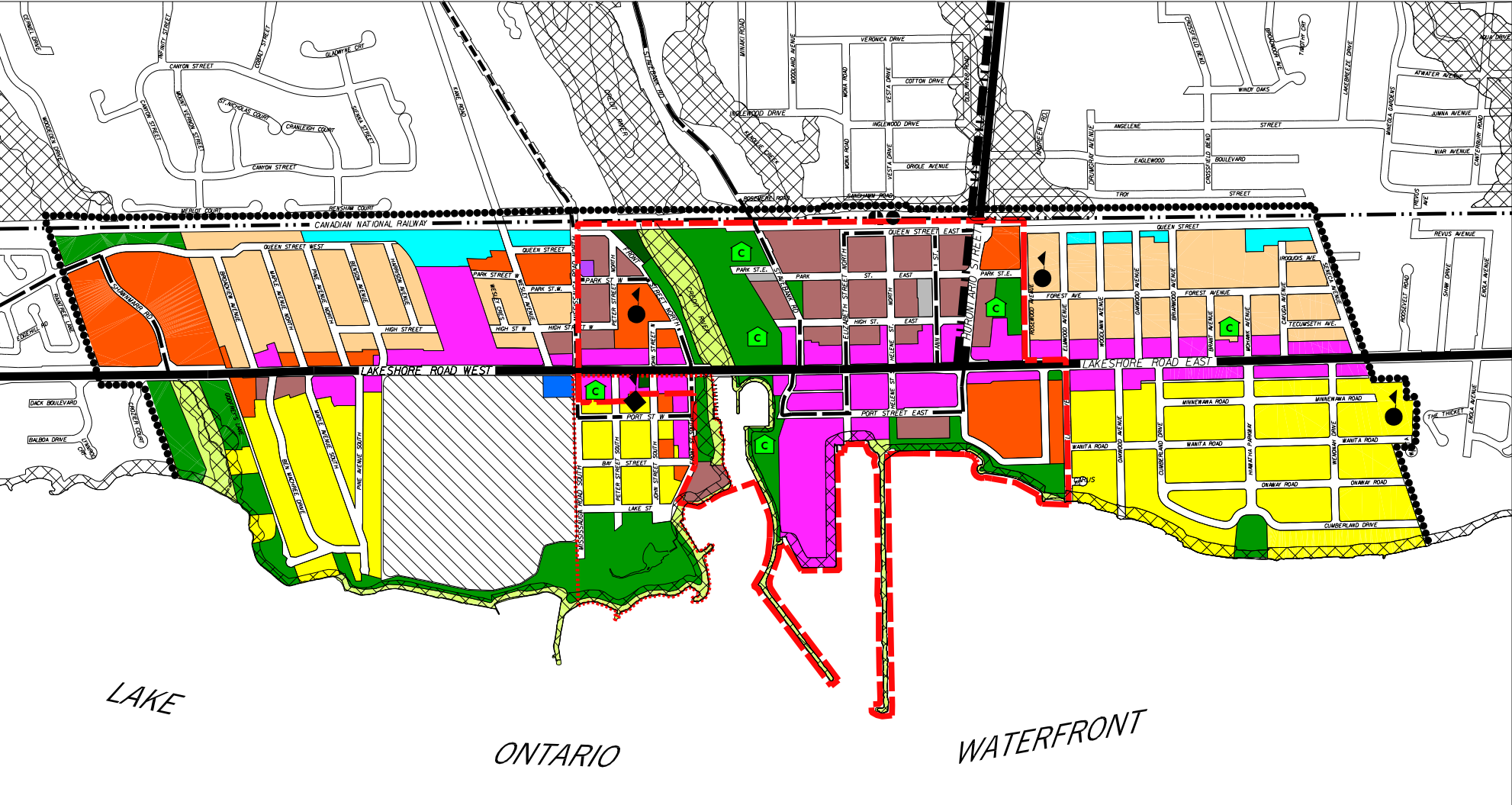
boundary of Area B shall be no greater than one storey.



5.39.1 The lands identified as Special Site 39 are located at the southeast corner of Lakeshore Road East and Elizabeth Street South.

5.39.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

- a. drive-through facilities will not be permitted;
- b. a maximum of 56 residential units are permitted;
- c. Area A
  - a maximum building height of four storeys is permitted;
  - building step backs from Lakeshore Road East shall be introduced to the façade
- d. Area B
  - a maximum building height of four storeys fronting Port Street East, stepping up to a maximum building height of ten storeys towards the north, is permitted.
  - notwithstanding the above height maximums, the building height adjacent to the easterly



LAND USE DESIGNATIONS

- Residential Low Density I
- Residential Low Density II
- Residential Medium Density
- Residential High Density
- Mixed Use
- Convenience Commercial
- Motor Vehicle Commercial
- Business Employment
- Public Open Space
- Private Open Space
- Greenbelt
- Utility
- To Be Determined

TRANSPORTATION LEGEND

- Arterial
- Major Collector
- Major Collector (Scenic Route)
- Minor Collector
- Local Road
- Existing Commuter Rail
- GO Transit Station
- Higher Order Transit Corridor

LAND USE LEGEND

- Community Node Boundary
- Heritage Conservation District
- Natural Hazards
- Public School
- Catholic School
- Community Facilities
- Local Area Plan Boundary

- Notes:
- Any part of the road network shown outside the city boundaries is shown for information purposes only.
  - Refer to Schedule 3, Natural System for the location of the Natural Areas System and Natural Hazards.
  - The limits of the Natural Hazards shown on this map are for illustrative purposes only. The appropriate Conservation Authority should be consulted to determine their actual location.

Port Credit Local Area Plan  
Land Use Map

0 100 200 300 400 metres

City of Mississauga



# Port Credit Local Area Plan

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## **Appendices**

- I Built Form Guide**
- II Lakeshore Road Transportation Review Study – Executive Summary**
- III Local Area Plan Credits**

# Port Credit

## 1.0 How to Read the Port Credit Local Area Plan

Mississauga Official Plan (the Plan) consists of a principal document and a series of local area plans, provided under separate cover. This is the Port Credit Local Area Plan (this Area Plan) and provides policies for lands in south central Mississauga, as shown on Figure 1. It includes lands identified in the City Structure as Community Node and Neighbourhood.

There are some instances where the policies and schedules of the principal document do not address all circumstances particular to Port Credit. In these cases, this Area Plan elaborates on, or provides exceptions to, the policies or schedules of the principal document.

This Area Plan must be read in conjunction with the principal document. Parts 1 to 4, the schedules and the appendices of the principal

document, are applicable to the Port Credit area, unless modified by this Area Plan. For example, the policies of this Area Plan must be read in conjunction with the direct growth, environmental, complete communities, multi-modal, urban form and land use policies of Parts 2 and 3 of the principal document. In the event of a conflict, the policies of this Area Plan take precedence.

For the purpose of this Area Plan, when Port Credit is referenced, it includes lands within both the Port Credit Community Node and the Port Credit Neighbourhoods.

Appendices attached to this Area Plan are provided for information purposes.

Included in the appendices is the Port Credit Built Form Guide to be used during the review of development applications. This Guide demonstrates how the urban form policies can be achieved.



Figure 1: Port Credit is located in south central Mississauga and includes areas identified in the City Structure as Community Node and Neighbourhood.

The Built Form Guide document is not considered part of this Area Plan, however, selected content has been incorporated and represents policy.

The Executive Summary from the Lakeshore Road Transportation Review Study has been included as Appendix II to provide additional information on how the Lakeshore Road Corridor within the Port Credit and Lakeview Character Areas can accommodate alternative modes of transportation.

## 2.0 Historical Context

Port Credit has a long history of habitation traced back to before the arrival of non-native settlers. Archaeological evidence suggests that native people were attracted to the Credit River Valley over a period of thousands of years, and by the 1700's the mouth of the Credit River had been settled by an Ojibwa group known as the Mississaugas. In the 1720s, French Fur traders are known to have exchanged goods with the Mississaugas, and as a result of allowing them to trade on credit, the river came to be known as the Credit River. The first organized planning occurred in 1834 when the village of Port Credit was surveyed and a town site laid out west of the Credit River. This area now forms part of the Old Port Credit Village Heritage Conservation District.

Over the ensuing years, Port Credit grew and evolved. Significant historical uses formed the Port Credit Village including a port (now predominately a marina and charter fishing centre), an oil refinery

(currently a vacant brownfield site), the St. Lawrence Starch Works (redeveloped into a mixed-use neighbourhood), and a local landfill (which was capped and developed for municipal facilities including a library). Much of the natural environment has been modified as a result of development, including the addition of fill in Lake Ontario to create both J.C. Saddington Park and the Port Credit Harbour Marina.

Port Credit was incorporated as a town in 1961 and amalgamated into the City of Mississauga in 1974.

## 3.0 Current Context

The Port Credit community is anchored by stable residential neighbourhoods linked by a commercial corridor. The area contains a range of residential, commercial, industrial, recreational and community uses within a diversity of built forms.

Residential development consists of a combination of dwelling types and forms comprising a high density area centrally located near the GO transit station, medium and high density development along Lakeshore Road (east and west) and around the harbour area, as well as low density areas characterized by tree-lined streets designed in grid patterns.

The area is served by commercial facilities primarily along Lakeshore Road (east and west), and within the Credit Landing Shopping Plaza. Lakeshore Road (east and west) generally has a mainstreet character



Figure 2: The Port Credit Node contains a range of building types, including: townhouses, mid-rise, and high rise buildings. The built form of the node generally transitions downwards toward the waterfront and stable residential neighbourhoods to the east and west. A limited number of high rise towers punctuate the skyline.

and is flanked by lay-by parking, sidewalks, and street furniture. The street is framed predominately by one to three storey buildings. Small scale industrial and commercial uses exist south of the Canadian National Railway tracks along Queen Street East and Queen Street West.

The open space system predominately consists of trails and parks along the Lake Ontario Waterfront and Credit River. These lands are culturally and recreationally significant and connect to the Mississauga's waterfront parks system.

The waterfront is one of the unique elements of the community, and is integral to the character of the area. Port Credit is a mixed use community, which is both physically and visually accessible to the waterfront. The community's location on the waterfront helps support local businesses and provides employment opportunities in the area.

Cultural and heritage resources include heritage buildings, the Old Port Credit Village Heritage Conservation District, and cultural landscapes that include: Port Credit Harbour, Port Credit Pier, the CN Bridge over the Credit River, Credit River Corridor and Mississauga Road Scenic Route.

Port Credit contains a number of community uses, including: schools, library, arena, swimming pool, and meeting spaces. Most of these uses are concentrated centrally within the area.

The Credit River is considered a warmwater fish habitat but is also a migratory route for coldwater species. The valley lands are a component of an important ecological corridor that extends north through the city. The shoreline provides unique ecological functions and habitat as well as an ecological corridor.

Most of the lands are developed with the exception of the vacant Imperial Oil (formerly Texaco) lands west of Mississauga Road. Given historic and current uses, there is potential for contamination issues on some areas within the site.

Port Credit is served by two arterial roads, Hurontario Street which runs north-south and

Lakeshore Road which runs east-west. In addition, Mississauga Road which runs north-south and parallel to the Credit River is designated as a Major Collector and a Scenic Route. There is a transit hub located on Queen Street East, west of Hurontario Street adjacent to the rail corridor. The hub is a transfer point for MiWay routes and is also the location of the Port Credit GO Transit station. Population, employment, and land area statistics are summarized in Figure 3.

Figure 3: PORT CREDIT AREA STATISTICS				
	Node	Neighbourhoods		Total Port Credit
		East	West	
Land Area <sup>1</sup>	78 ha	74 ha	125 ha	277 ha
Population <sup>2</sup>	6,860	2,530	3,130	12,520
Employment <sup>3</sup>	2,170	680	530	3,380
<p>1 Land area is a gross figure and includes everything within the defined boundary such as rivers, roads, and all other land uses.</p> <p>2 Population is adapted from Hemson Consultants, Growth Forecast, 2008. Population figures are based on the 2011 mid year forecast and include a 4.2% undercount from Statistics Canada.</p> <p>3 Employment figures include a work from home assumption of 31 jobs in 1,000 population, adapted from Hemson Consultants, Growth Forecast, 2008. Employment figures also include an adjustment factor for non-reporting businesses adapted from: City of Mississauga, Mississauga Employment Survey, 2010.</p> <p>Note: Numbers have been rounded</p>				

## 4.0 Collaboration

This Area Plan incorporates public input provided through extensive consultation, including stakeholder interviews, visioning sessions, place making workshops, youth outreach exercises and an open house.

The results of the visioning process are contained in the report "Lakeview and Port Credit District Policies Review and Public Engagement Process



Directions Report” (Directions Report), October 28, 2008. The Directions Report includes a range of policy recommendations that have informed the preparation of this Area Plan.

Collaboration included the formation of a Local Advisory Panel which provides a forum for the discussion of planning and related issues. Mississauga will ensure ongoing consultation with the public and other stakeholders on further initiatives and implementation pertaining to this Area Plan.

## 5.0 Vision

The Vision for Port Credit, is for an evolving urban waterfront village with a mixture of land uses, a variety of densities, compact pedestrian and cycling friendly, transit supportive urban forms, a significant public realm, public access to the waterfront and development that incorporates high quality built form.

Significant elements which give Port Credit its sense of place are to be preserved and enhanced, such as the main street village character along portions of Lakeshore Road (east and west), heritage buildings and landscapes, community facilities, stable residential neighbourhoods, open space, parks, and marina functions along the waterfront. The Vision reinforces the importance of retaining and enhancing the built elements that provide residents with a sense of local community and social activity.

The Vision is intended to manage change to ensure an appropriate balance is maintained between growth and preservation of what makes Port Credit a place where people want to live, learn, work and play.

The Vision and policies of this Area Plan advance the goals of the Strategic Plan, including:

- Move - directing growth to support transit;
- Belong - providing a range of housing option;

- Connect - promoting a village mainstreet;
- Prosper - encourage employment uses; and
- Green - promote conservation, restoration, and enhancement of the natural environment.

## 5.1 Guiding Principles

The Vision is based on six principles that provide local context and supplement the Guiding Principles of the principal document:

5.1.1 Protect and enhance the urban village character recognizing heritage resources, the mainstreet environment, compatibility in scale, design, mixture of uses and creating focal points and landmarks.

5.1.2 Support Port Credit as a distinct waterfront community with public access to the shoreline, protected views and vistas to Lake Ontario, the Credit River and active waterfront uses.

5.1.3 Enhance the public realm by promoting and protecting the pedestrian, cyclist and transit environment, creating well connected and balanced parks and open spaces and reinforcing high quality built form.

5.1.4 Support the preservation, restoration and enhancement of the natural environment.

5.1.5 Balance growth with existing character by directing intensification to the Community Node, along Lakeshore Road (east and west), brownfield sites and away from stable neighbourhoods. Intensification and development will respect the experience, identity and character of the surrounding context and Vision.

5.1.6 Promote a healthy and complete community by providing a range of opportunities to access transportation, housing, employment, the environment, recreational, educational, community and cultural infrastructure that can assist in meeting the day-to-day needs of residents.



Urban Village Character



Preservation, Restoration and Enhancement of Natural Environment



Distinct Waterfront Community



Balance Growth



Enhance Public Realm



Healthy and Complete Community

Figure 4: The Port Credit Vision is based on six principles

## 5.2 Community Concept

This Area Plan recognizes that various areas of the community perform different functions based on a community concept that incorporates the following elements:

- Green System;
- Community Node;
- Neighbourhoods; and
- Corridors.

These elements are further divided into precincts which recognize different character attributes of these areas and contain different policy directions.

The policies are intended to encourage a development pattern and character responsive to the Current Context, the Vision, Guiding Principles and Community Concept.

### 5.2.1 Green System

The Green System consists of an interconnected open space network including the Credit River and the Lake Ontario shoreline. These are key features as they contribute to the environmental, social and economic health of the community. In addition, the visual presence of the waterfront and river are beneficial to residents. Implications of new development on the Green System will be an important consideration in the review of any development application. The Green System overlaps with both the Community Node and Neighbourhood elements.

### 5.2.2 Community Node

The Community Node represents the focus for the surrounding neighbourhoods. It exhibits many of

the desirable characteristics intended for community nodes, including a mixture of uses, compact urban form, appropriate density, and in many ways has achieved its planned function. It has been identified primarily in recognition of the role it plays in the community and as a location for intensification.

As outlined in the principal document, a community node is similar to a major node but with lower heights and densities. This Area Plan respects the planned function and position within the City's hierarchy, while also reflecting the existing and planned character of Port Credit. As such, permitted building heights for new development in the Community Node will support the Vision as an urban waterfront village and respect the existing character. It is recognized that in the vicinity of the GO station and future Light Rail Transit station, additional height and density may be appropriate, however, the extent will be determined through further study.

Development has been primarily residential, however, the community node requires additional employment in order to ensure a balanced land use pattern. Attracting and accommodating employment uses is a priority but also a significant challenge. While Port Credit can benefit from some additional residential intensification, care needs to be taken to ensure that it is not done at the expense of protecting opportunities for employment uses and creating a balanced complete community.

The Community Node includes the GO station identified as a Major Transit Station Area and a Gateway Mobility Hub. In addition, the future Hurontario Light Rail Transit route proposes a station, located near the GO station. Ensuring safe and efficient movement of people between transit modes will be a key consideration in the review of development applications.



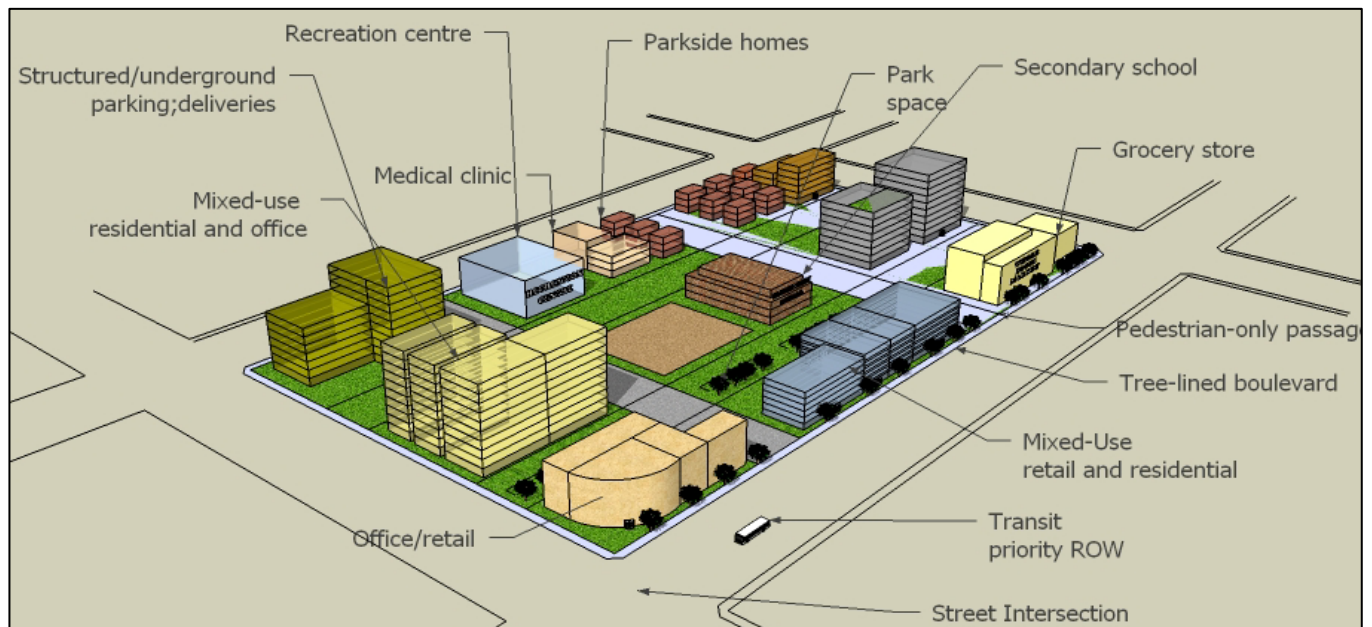


Figure 5 : Community Nodes are intended to provide a focus for a multitude of uses which address the fundamental services and facilities required for daily living - local shops and restaurants, community facilities, cultural activities, entertainment uses, schools, parks, open space as well as a diverse housing stock. Community Nodes should be compact, with pleasant walkable streets and a strong sense of place and community identity.

### 5.2.3 Neighbourhoods

Port Credit Neighbourhoods are on either side of the Community Node. These areas are substantially residential, but also include commercial uses along Lakeshore Road (east and west), and employment uses along the railway. Neighbourhoods are intended to recognize areas that are physically stable with a character to be protected.

Although stable, some change is anticipated. New development does not necessarily have to mirror existing development types and densities, however, it will respect the character of the area. The policies in this Area Plan and Built Form Guide provide direction for appropriate transitions in built form and scale of buildings.

The Neighbourhood to the west of the Community Node includes the Old Port Credit Village Heritage Conservation District where additional attention is required to ensure development appropriately reflects the character of the area. In addition, this Neighbourhood also includes a former refinery site which is vacant and represents a significant brownfield redevelopment opportunity. The Neighbourhood to the east of the Community Node primarily consists of stable residential areas, a

commercial mainstreet, and employment uses along the railway.

### 5.2.4 Corridors

The principal document identifies Lakeshore Road (east and west) as a Corridor and Hurontario Street as an Intensification Corridor. They play an important role connecting Port Credit to the surrounding communities. Lakeshore Road (east and west) also plays an important local role in connecting Port Credit neighbourhoods together and linking them with the Community Node. They may accommodate multi-modal transportation facilities, as the principal document identifies higher order transit along Hurontario Street and portions of Lakeshore Road East.

Within the corridors, the public realm is one of the important aspects where people can experience Port Credit on a day-to-day basis. A large portion of the corridor is planned to accommodate a mainstreet commercial environment, which represents an important part of the urban village of Port Credit.

Hurontario Street and Lakeshore Road (east and west) also have an important relationship to the

waterfront. Port Credit is one of the few areas where people travelling along these roads, can at certain locations, have the opportunity to visually see the waterfront. As such, views along these corridors will be preserved.

Lakeshore Road is the only east-west road that crosses the Credit River south of the QEW, serving both the local Port Credit community and regional travel. As such, movement within and through the Port Credit area is restricted by the limited road network, which is at or near capacity at peak travel times. Maintaining Lakeshore Road as a four lane roadway during peak travel times is, therefore, a transportation priority to meet current demand. Lakeshore Road is a constrained corridor that requires a context sensitive design approach. Trade-offs will be required to accommodate the envisioned multi-modal function of the corridor.

## 6.0 Direct Growth

Intensification is to be consistent with the planned function as reflected by the city structure and urban hierarchy.

The Node and Neighbourhoods are divided into precincts as identified on Schedule 1: Port Credit Character Areas and Precincts.

The amount of intensification will vary in accordance with the policies of this Area Plan. The specific manner in which new development will be accommodated (e.g. height and density) is further explained in subsequent sections of this Area Plan.

### 6.1 Community Node Character Area

The Community Node is an established focal point for the surrounding neighbourhoods and exhibits the density and many of the characteristics that community nodes should include, such as compact, mixed used development, pleasant walkable and

cycle friendly streets and a strong sense of place and community identity. In addition to serving many of the day-to-day needs of the community, this area also benefits from uses (e.g. marina, restaurants) and events which attract people from across the city.

The Node includes portions of the Hurontario Street Intensification Corridor and the Lakeshore Road Corridor, which are defined as lands fronting the street to a depth of half a block. The Community Node incorporates the GO Station which is identified as a Gateway Mobility Hub / Major Transit Station Area in the principal document. The limits of the Mobility Hub correspond to the Community Node, east of the Credit River. Subsequent sections of this Area Plan detail the manner in which intensification will be accommodated in these areas.

Relevant statistics pertaining to existing density in the Community Node are summarized in Table 1.

Table 1: Population and Employment Statistics Port Credit Community Node (October 2011)	
Residents	6,860
Jobs	2,170
Residents and Jobs combined per hectare	9,030
Existing Combined Residents and Jobs Density	115 /ha
Existing Port Credit Population to Employment Ratio	3.2:1
Note: Residents includes developments currently under construction but does not reflect any additional infill development that has not yet been approved. Density based on land area of 78.2 ha.	

With a gross density of 115 residents and jobs combined per hectare, Port Credit is within the targeted range for Community Nodes of between 100 and 200. As such, additional density is not required to meet the target, however, it is recognized that some infill and redevelopment will occur. This should focus on creating a more



complete community and in particular employment opportunities.

Increasing the gross density towards the upper limit of 200 residents and jobs combined per hectare is not sufficient planning justification on its own for approving amendments that permit additional height and density.

The current population to employment ratio of 3.2:1 does not meet the range for Community Nodes of 2:1 to 1:2. The target population to employment ratio for the Port Credit Community Node is 2:1. As such, additional employment is required to provide more opportunities to live and work in the community. The ratio is measured as an average across the entire node, and includes the former refinery site (located in the neighbourhood to the west).

6.1.1 The City will monitor the gross density and population to employment ratio in the Community Node and will assess its ability to accommodate further growth through the development approval process.

6.1.2 Increases in employment opportunities are to be accommodated on lands designated mixed use, which can accommodate a range of establishments including: retail, restaurants, and offices.

6.1.3 Development applications will be required to address, to the City's satisfaction, the appropriate range and amount of employment uses on the following sites:

- a. GO Station Parking Lot and vicinity (land at the four corners of Ann Street and Park Street East);
- b. Port Credit Harbour Marina (1 Port Street East); and,
- c. Former Refinery Site (south side of Lakeshore Road West located between Mississauga Road South and Pine Avenue South)

6.1.4 Mississauga will encourage redevelopment within the Mainstreet Precinct to accommodate employment uses on the second and third floors.

6.1.5 Strategies to encourage and support employment uses may be pursued including consideration of Community Improvement Plans and Bonus Zoning.

6.1.6 Intensification will address matters such as:

- a. contribution to a complete community;
- b. providing employment opportunities;
- c. sensitivity to existing and planned context and contribution to the village mainstreet character;
- d. respecting heritage; and
- e. protecting views and access to the waterfront.



Figure 6: Intensification within the Community Node should respect the experience, identity and character of the surrounding context. Providing views to Lake Ontario respects Port Credit's identity as a waterfront community.

## 6.2 Neighbourhood Character Areas (East and West)

The Neighbourhood Character Areas are on the east and west sides of the Community Node. These are stable areas, primarily residential in nature, and are not expected to experience significant change.

The Neighbourhood Character Areas include portions of the Lakeshore Road Corridor. The corridor generally represents a distance of half a block north and south of Lakeshore Road and corresponds to the limits of the Mainstreet Precinct.

6.2.1 Intensification will occur through modest infilling or redevelopment along Lakeshore Road Corridor, commercial plazas, or on the vacant former refinery site.

6.2.2 Intensification will be sensitive to the existing character of the residential areas and the planned context of Lakeshore Road Corridor.

## 7.0 VALUE THE ENVIRONMENT

The preservation, restoration and enhancement of the environment is a guiding principle. The Green System is an important element in the Community Concept. The Green System is located within a variety of land use designations in Port Credit.

Schedule 1A of the principal document identifies the Green System which consists of:

- Natural Areas System;
- Natural Hazard Lands, and
- Parks and Open Space.

Schedule 3 of the principal document identifies the Natural System including Natural Areas and Natural Hazards. Schedule 4 identifies Parks and Open Spaces.

Lands within the Port Credit Natural Area System perform an essential ecological function as they sustain biodiversity by providing habitat for plants and animals and they clean the air and water.



Figure 7: The Credit River and its banks are important components of the natural environment and public recreation system. Along with the Lake Ontario waterfront these areas are important local, city, regional and provincial assets. Policies implement a balanced approach that respects the environment and recognizes the role it plays in the community.

Natural Hazard Lands are associated with features such as the Tecumseh Creek, Credit River, Credit Valley and Lake Ontario Shoreline.

Waterfront parks are an important component of the Open Space System, and include Rhododendron Gardens, Ben Machree Park, Imperial Oil waterfront trail extension, J.C. Saddington Park, Marina Park, Memorial Park (East and West), St. Lawrence Park, Tall Oaks Park, and Hiawatha Park.

The mouth of the Credit River is an ecologically significant estuary where the Credit River meets Lake Ontario. In addition, the Credit River is a well known fishery, which contains a Provincially significant wetland complex just north of the Area Plan, and is identified in the Provincial Greenbelt Plan as an external connection.

## 7.1 Green System

7.1.1 The management of Rhododendron Gardens and Port Credit Memorial Park East will consider how the natural area components of these parks can be enhanced.

7.1.2 Within the waterfront parks system, the protection, preservation and restoration of existing natural systems will be prioritized and balanced to direct and guide the planning of existing and future waterfront activities.

7.1.3 Opportunities to enhance and restore the Credit River as a biologically productive and diverse ecosystem are encouraged.

## 7.2 Urban Forest

7.2.1 Mature trees are recognized as providing important environmental benefits and contributing to the character of Port Credit. Improvements to the urban forest are encouraged.

7.2.2 Opportunities to enhance the tree canopy and health of the urban forest in the public right-of-

ways and adjacent privately owned land will be considered in conjunction with new development.

7.2.3 Opportunities to improve the tree canopy and promote a healthy urban forest will be reviewed when considering improvements to the streetscape along public roads, with particular attention to the Community Node and Lakeshore Road Corridor.



Figure 8: Mature trees provide environmental benefits as well as contribute to the character of the area. Given sufficient landscaped area, some tree varieties in Port Credit have grown upwards of five storeys in height.

## 7.3 Living Green

7.3.1 Mississauga will encourage partnerships and stewardships in order to improve the health of the Green System.

7.3.2 Development will strive to minimize the impact on the environment and incorporate sustainable development practices in accordance with the City's Green Development Strategy and the Water Quality Control Strategy.

## 8.0 COMPLETE COMMUNITIES

Port Credit, and in particular the Community Node, contains many of the attributes associated with



complete communities, including among other things:

- a compact urban form that is walkable, provides convenient access to a mixture of land uses and an assortment of activities such as: shops, community facilities, parks, and public transit;
- a range of housing options with a mixture of housing forms and densities, along with a significant rental housing stock;
- cultural resources such as heritage buildings and landscapes associated with the Credit River and Lake Ontario, which help retain a connection to the past; and
- opportunities for people to engage in community life through meeting facilities, a high quality public realm and vibrant cultural activities.

The policies are intended to protect and enhance the above attributes to fully realize the complete community aspect of Port Credit.

## 8.1 Housing

8.1.1 The Community Node and Lakeshore Road Corridor are encouraged to develop with a range of housing choices in terms of type, tenure and price.

8.1.2 The provision of additional affordable housing, with a focus on rental housing units, is encouraged in the Community Node and Lakeshore Road Corridor.

8.1.3 Preservation of existing affordable housing will be a priority. Where development applications are proposing the removal of existing affordable housing, the replacement of these units will be encouraged on-site or within the community.

8.1.4 Mississauga will encourage investment in new rental housing and, in particular, affordable rental housing that meets the needs of young adults, older adults and families in the Community Node and along the Lakeshore Road Corridor.

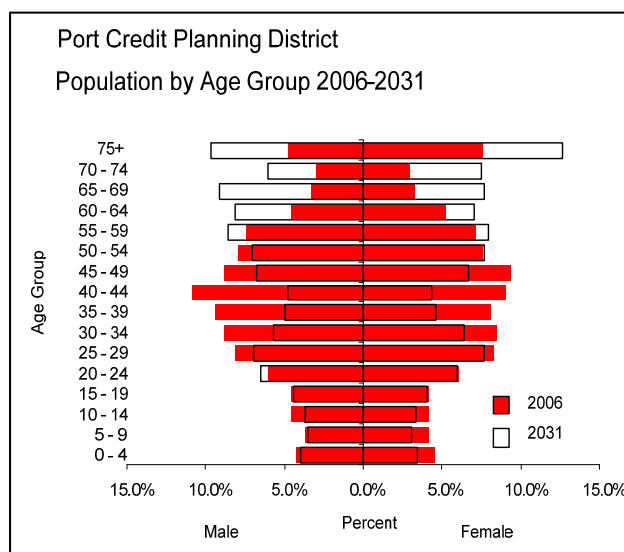


Figure 9: Similar to many established communities, Port Credit's population is forecast to age. In many cases, as people age they consider living in smaller accommodations and often prefer to live within the same general community.

## 8.2 Cultural Heritage

Port Credit contains a number of important cultural heritage resources and properties, including but not limited to:

- Old Port Credit Village Heritage Conservation District, these lands are subject to a Conservation District Plan, the goal of which is to conserve and enhance the historical character of old Port Credit village;
- Heritage Designations / Heritage Listings on the City's Heritage Register;
- Cultural Landscapes which include the Credit River Corridor, Port Credit Pier Scenic View, and Mississauga Road Scenic Route; and
- Registered archaeological sites.

8.2.1 Consideration will be given to including the Old Port Credit Village Heritage Conservation District into a Community Improvement Plan to promote and enhance the heritage character of the area.



Figure 10: Given the long history of settlement in Port Credit it is important to protect, recognize and enhance the area's heritage attributes.

### 8.3 Community and Cultural Infrastructure

Port Credit contains a concentration of facilities, buildings, properties and events which represent a cultural infrastructure cluster considered to be a cultural node. Continued investment in Port Credit's cultural infrastructure is encouraged.

8.3.1 The Community Node and Lakeshore Road Corridor are preferred locations for community infrastructure.

8.3.2 Cultural infrastructure will reinforce, where possible, the Vision and guiding principles of an urban waterfront village.

8.3.3 Creative enterprises that support the economy and create an engaging lively area year round are encouraged to locate in the Community Node.

8.3.4 Mississauga will encourage partnerships and collaboration with the local community to further develop the cultural aspects of the Community Node.

8.3.5 Consideration will be given to including the Community Node and Neighbourhood Mainstreet Precinct into a Community Improvement Plan to guide development of the cultural infrastructure.



Figure 11: The Mississauga Culture Master Plan notes that cultural facilities and creative enterprises support economic activity and play a key role in creating the engaging, lively and richly textured places where people want to live and visit. Creative enterprises can include a wide range of businesses, such as: artists, galleries, architects, graphic designers, and software developers.

### 8.4 Distinct Identity & Character

8.4.1 The character will reflect the vision of an urban waterfront village. City initiatives, including investments in lighting, public art, transportation features, streetscape improvements, parks planning, will contribute to the vision.

8.4.2 Lakeshore Road (east and west) is a key location for the promotion of Port Credit as a place for vibrant cultural activities and providing a "sense of place". The mainstreet environment, including the built form, public realm, street furniture, lighting, cycling amenities, trees, gathering places are to be protected and, where possible, enhanced.

8.4.3 A master streetscape plan for Lakeshore Road (east and west) may be undertaken that reinforces the Vision and encourages a consistent design theme.

8.4.4 Opportunities to further Port Credit's identity and character through improvements to the public realm will be considered through the planning and design of:



- Light Rail Transit along Hurontario Street;
- redevelopment of the GO Station parking lot; and,
- redevelopment along the Lake Ontario and Credit River shorelines, including the Port Credit marina.

## 8.5 Lake Ontario Waterfront

Port Credit is a waterfront community with a strong identity and orientation to the Lake Ontario and Credit River. Public access to the waterfront is an important part of the Port Credit identity.

8.5.1 Mississauga supports the continuation and improvement of water dependent activities and related employment uses such as marinas, facilities in support of recreational boating and sport fishing and uses that benefit from being near the shoreline, parks and the Waterfront Trail.

8.5.2 Uses in proximity to the waterfront will provide for public access, where appropriate. Through land acquisition, capital works and the review of proposals, Mississauga will endeavor to ensure this Vision is realized.

8.5.3 The Mississauga waterfront parks are a significant element of the Port Credit Character. Planning for the waterfront parks system will be guided by the Waterfront Parks Strategy, 2008.

## 9.0 MULTI-MODAL CITY

Integral to Port Credit is the transportation system which includes: transit, vehicular, active transportation (e.g. walking and cycling) and rail.

Tables 8-1 to 8-4 and Schedule 8 (Designated Right-of-Way Width) of the principal document identify the basic road characteristics. The long term multi-modal transportation system is shown on Schedule 5 (Long Term Road Network), Schedule 6 (Long Term Transit Network), and Schedule 7 (Long Term Cycling Network) of the principal document.

The Port Credit transportation system plays an important role in the overall livability and development of the area and in how Port Credit evolves as an urban waterfront village. Planned higher order transit will improve the area's transportation infrastructure; however, constraints such as the Credit River, the CN railway, and the existing road network, represent challenges to the overall functioning and capacity of the system.

On Lakeshore Road during the weekday morning and evening peak travel times, there are travel time delays and long queues experienced from approximately west of Mississauga Road to Hurontario Street. Vehicles travelling through this stretch experience "saturated flow", meaning that this stretch of roadway is approaching its motor vehicle capacity, with vehicle travel speeds being very low.



Figure 12: Water dependent activities are an important part of what gives Port Credit a distinct identity. Policies are intended to enhance the function and image of the area as a waterfront destination.

As Lakeshore Road is the only east-west road that crosses the Credit River south of the QEW serving both the local Port Credit community and regional travel, the City is concerned with exacerbating the problem with significant additional development in Port Credit.

The City is proposing to undertake a Lakeshore Road Transportation Master Plan, which will include Lakeshore Road (between the east and west City limits) and an examination of the transportation issues specific to the Port Credit area. As part of the future study, the City will review the higher order transit needs in the Port Credit area and opportunities to improve the transportation system for all modes.

Hurontario Street is identified as a higher order transit corridor, with Light Rail Transit (LRT) being the recommended transit technology. In addition, a future Higher Order Transit corridor has been identified along Lakeshore Road East, extending from Hurontario Street, to the City of Toronto boundary. A preferred transit solution (e.g. bus or rail) has not yet been identified for this corridor.

Depending on the density and transportation requirements of future development on significant land parcels or through land assembly, the extension of rapid transit to the west of Hurontario Street may be required.

## 9.1 Multi-Modal Network

9.1.1 The road and transportation network will consider the needs of all users, both those residing within the community and those that utilize the network from elsewhere in the city and surrounding areas.

9.1.2 Mississauga may acquire lands for a public transit right-of-way along Lakeshore Road East and Hurontario Street where the creation of a public transit right-of-way, separate from, adjacent to, or in addition to, a road right-of-way is deemed appropriate.

9.1.3 The proposed LRT stop in Port Credit represent potential place making opportunities and locations for public art. Development applications adjacent to LRT transit stops may be required to incorporate placemaking elements into their design.

9.1.4 Hurontario Street and Lakeshore Road (east and west), including the Credit River Bridge, will not be built in excess of four lanes, excluding turning lanes, bus bays, space for bicycles, space for higher order transit and parking, unless it can be demonstrated that additional lanes will not result in a major deterioration of the neighbouring residential or retail commercial environment or public realm. The implementation of any major roadway modifications will require an amendment to this Area Plan.

9.1.5 Lakeshore Road (east and west) will be planned to accommodate, to the extent possible, all modes of transportation, including, pedestrian facilities, cycling facilities and higher order transit facilities. Appendix II contains the executive summary from the Lakeshore Road Transportation Review Study, which provides a comprehensive and technical transportation review regarding how the Lakeshore Corridor can accommodate alternative modes of transportation.



Figure 13: Providing safe, sheltered and convenient access to various modes of transportation can help shift peoples lifestyles towards more sustainable transportation modes such as transit, cycling, and walking.

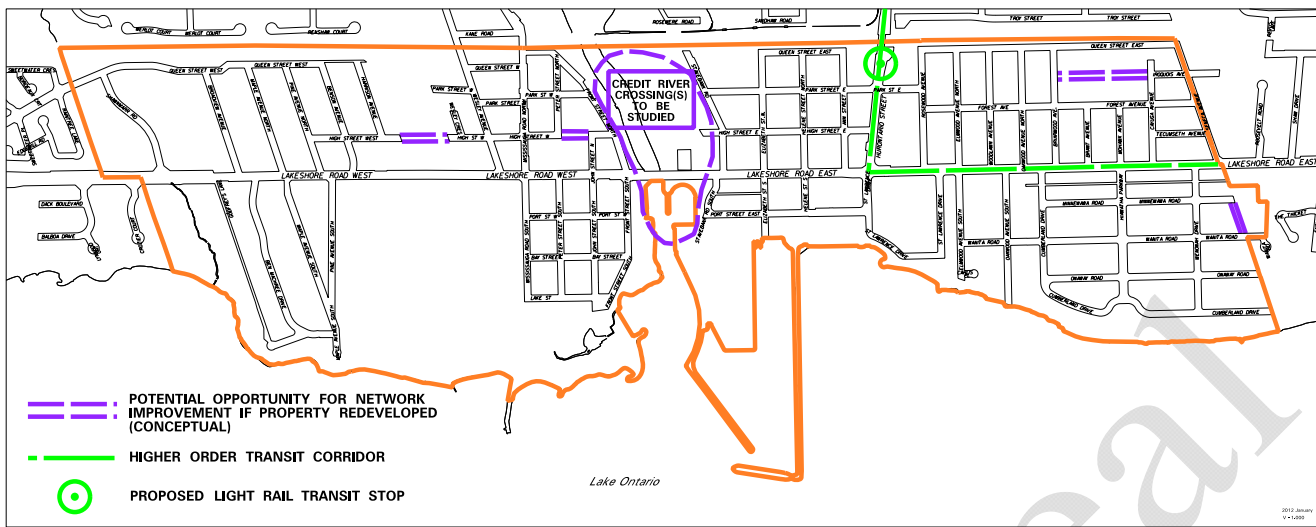


Figure 14: Potential Opportunities for Road Network Improvements and Higher Order Transit

9.1.6 Mississauga will work with the Province to develop a multi-modal plan for the Queen Elizabeth Way / Lakeshore Road Corridor that addresses regional and local transportation needs and property impacts.

9.1.7 During the review of development applications, consideration will be given to eliminating and/or consolidating vehicular turning movements to and from Lakeshore Road (east and west) and direct traffic towards signalized intersections, where appropriate. Vehicular access to redevelopment opportunities should be considered from existing north-south side streets or existing or proposed laneways parallel to Lakeshore Road (east and west).

9.1.8 During the review of development applications, consideration will be given to the manner in which the development provides additional public access to the shoreline.

9.1.9 Mississauga will work with the Province to ensure development of higher order transit services and related infrastructure including parking structures are integrated appropriately into Port Credit, having regard for the impact on all modes of transportation.

9.1.10 Cycling and pedestrian infrastructure will be provided in accordance with the Cycling Master Plan and the principal document.

9.1.11 Mississauga will identify and pursue infrastructure opportunities that enable additional cycling and pedestrian crossings of the Credit River and CN Railway tracks.

9.1.12 A Transportation Master Plan for Lakeshore Road (between the east and west City limits) and Port Credit may be undertaken that addresses improving current mobility for all modes of transportation, the implications of future growth on the network and considers placemaking initiatives that would promote the animation of the corridor. The Plan may assess improvements to the Port Credit road network, including additional pedestrian, cyclists and vehicle crossings of the Credit River as well as review the higher order transit needs in the Port Credit area.

9.1.13 Improvements to the road network and active transportation routes that provide connectivity and a fine grained network through Port Credit may be identified through a future Transportation Master Plan for the Lakeshore Road Corridor or through the development application process.

Improved connections will provide pedestrians, cyclists and vehicles a greater variety of routes and accessibility within the area. Potential opportunities

for network improvements include but are not limited to the following:

- High Street West between Harrison Avenue and Wesley Avenue;
- High Street West between Peter Street North to John Street North;
- Iroquois Avenue, from Cayuga Avenue to Briarwood Avenue;
- Extension of Minnewawa Road southerly to connect with Wanita Road; and
- Additional crossing(s) of the Credit River.

When reviewing the appropriateness of potential road connections, the City will consider the volume and type of traffic that would be accommodated on the road.

9.1.14 Development applications will be accompanied by transportation and traffic studies. Studies will address, amongst other matters, strategies for limiting impacts on the transportation network, where appropriate, including measures such as:

- reduced parking standards;
- transportation demand management;
- transit oriented design of the development;
- pedestrian/cycling connections; and
- access management plan.

9.1.15 Due to capacity constraints on the Port Credit transportation network, development applications requesting increases in density and height, over and above what is currently permitted in the Port Credit Local Area Plan will be discouraged unless it can be demonstrated, to the City's satisfaction, that the proposed development has included measures to limit the amount of additional vehicular demand.

## 9.2 Parking and Transportation Demand Management

9.2.1 Reduced parking requirements and maximum parking standards may be considered within:

- a. the Community Node, particularly in proximity to the GO Station and future LRT stops; and
- b. the Mainstreet Neighbourhood Precinct.

9.2.2 Public parking lots in the Community Node and Mainstreet Neighbourhood Precinct will be maintained and supplemented, where appropriate.

9.2.3 The City will encourage Transportation Demand Management measures, where appropriate, within the Community Node and as part of any significant redevelopment projects outside of the node.

## 9.3 Mobility Hub

The GO Transit station and vicinity is identified as a Gateway Mobility Hub in the Regional Transportation Plan prepared by Metrolinx and as a Major Transit Station in the principal document. The transit station area connects regional rapid transit along the GO line with local bus service. In the future, the area is planned to have connections to higher order transit lines serving Hurontario Street and Lakeshore Road East.

Port Credit exhibits many of the attributes of a successful mobility hub such as having a concentration of housing, shopping and recreational uses in the vicinity of a transit station where Regional and local transit services connect. The focus for future development is to support the planned character of the area, and the functioning of the mobility hub.

9.3.1 Investment in infrastructure, building and site design that supports the function of the mobility hub will be a priority. This includes, among other things:

- a. a vibrant, mixed use environment, providing additional employment opportunities and a greater range of commercial establishments;
- b. a built form that respects the planned character of the area;
- c. safe and efficient pedestrian and cycling connections to neighbourhoods that surround the mobility hub;
- d. safe and efficient movement of people transferring between transit modes, networks and routes;
- e. safe and efficient movement of people with high levels of pedestrian and cyclist priority;
- f. safe and efficient access for cyclists, and secure storage facilities;
- g. a well designed transit station for a high quality user experience;
- h. strategic parking management, including minimizing surface parking lots and designing parking structures to a high architectural and landscape standard;
- i. an attractive public realm;
- j. a minimized ecological footprint; and,
- k. effective partnerships and incentives for increased public and private investment.





Figure 15: The Regional Transportation Plan broadly defines the geographic limits of a mobility hub to be the area within 800 metres of a major transit station. The Port Credit Mobility Hub Study refined the limits by removing stable residential neighbourhoods as well as lands to the west of the Credit River, which is a barrier to travel. The limits of the Mobility Hub with Port Credit correspond to the Secondary Zone. The GO Station parking lot on the north side of the CN Railway is outside of the Port Credit Local Area Plan.





## 10.0 DESIRABLE URBAN FORM

The desirable urban form policies reflect the planned function and local context and are organized as follows:

### a) Community Node Character Area

This Character Area has been further subdivided into the following precincts:

- Central Residential;
- Mainstreet Node;
- Harbour Mixed-Use; and,
- Riverside.

### b) Neighbourhood Character Area

This Character Area has been further subdivided into the following precincts:

- Old Port Credit Village Heritage Conservation District;
- Vacant Former Refinery;
- North Residential Neighbourhood;
- South Residential Neighbourhood; and
- Mainstreet Neighbourhood.

The locations of these precincts are shown on Schedule 1.

Development will be guided by the Port Credit Built Form Guide, contained in Appendix I.

## 10.1 General Policies

10.1.1 Development will be in accordance with the minimum and maximum height limits as shown on Schedule 2A and 2B. The appropriate height within this range will be determined by the other policies of this Area Plan.

10.1.2 Heights in excess of the limits identified on Schedules 2A and 2B within the Community Node precincts and Mainstreet Neighbourhood precinct may be considered through a site specific Official Plan Amendment application, subject to demonstrating, among other matters, the following:

- a. The achievement of the overall intent, goals, objectives of this Plan;
- b. Appropriate site size and configuration;
- c. Appropriate built form that is compatible with the immediate context and planned character of the area;
- d. Appropriate transition to adjacent land uses and buildings, including built form design that will maximize sky views and minimize visual impact, overall massing, shadow and overlook;
- e. Particular design sensitivity in relation to adjacent heritage buildings; and
- f. Measures to limit the amount of additional vehicular and traffic impacts on the Port Credit transportation network.

10.1.3 For properties located in more than one precinct, development must conform to the policies appropriate for each portion of the precinct.

10.1.4 Above grade parking facilities will be designed to be compatible with the surrounding character through the use of architectural elements that fit with the scale and style and streetscape of the community. Secure storage facilities for bicycle parking will be provided on the ground floor of the structure. Consideration will also be given to including active pedestrian related uses on the ground floor of the structure to improve the animation of the street edge condition. Where possible, above grade parking structures will be designed in such a manner that they are integrated into the development.

## 10.2 Community Node Character Area

The Community Node Character Area will exhibit high standards of urban design that reinforce and enhance the identity of Port Credit as a vibrant and memorable urban place. Additional development is anticipated, however, the form and scale will vary within the node in accordance with the various precincts.

The policies are intended to reflect a number of objectives, including among other things:

- to ensure that the greatest height and density will be in close proximity to the GO station and future LRT transit stop at Hurontario Street and Park Street;
- to ensure building heights will reflect an appropriate transition towards the Credit River, Lake Ontario Shoreline, the mainstreet area, and surrounding neighbourhoods;
- to provide for a variety of building heights and massing that are well spaced to provide skyviews and an articulated skyline;
- to recognize the waterfront as an important attribute where public access will be provided and views protected;
- to provide for a village mainstreet environment that is characterized by low rise mixed use development with a high quality public realm along Lakeshore Road (east and west);
- to recognize key locations in the vicinity of the GO station and waterfront that require additional study prior to redevelopment; and,
- to ensure development will be sensitive to the existing context, heritage resources and planned character of the area.

### 10.2.1 Community Node Character Area General Policies

10.2.1.1 The overall development of the Node will be at a scale that reflects its role in the urban hierarchy.

10.2.1.2 Floor plate size for buildings over six storeys will decrease as building height increases, to address, among other matters:

- a. overall massing (reduce “wall effect”);
- b. visual impact of buildings;
- c. protect skyviews; and
- d. limit shadow impact.

10.2.1.3 Buildings over six storeys will maintain distance separations that, amongst other matters, address the following:

- a. existing distance separations between buildings;
- b. overcrowding of skyviews and skyline;
- c. protection of view corridors; and
- d. privacy and overlook of occupants.

10.2.1.4 New development will provide for landscape areas that, amongst other matters, address the following:

- a. landscaped character of existing properties and the planned function of the precinct;
- b. provide buffer between uses;
- c. incorporate stormwater best management practices;
- d. enhance the aesthetic quality of the area; and
- e. provide opportunities to enhance the tree canopy.

10.2.1.5 Streetscape will address, among other matters, the following:

- a. setbacks and side yards to reflect the planned function;



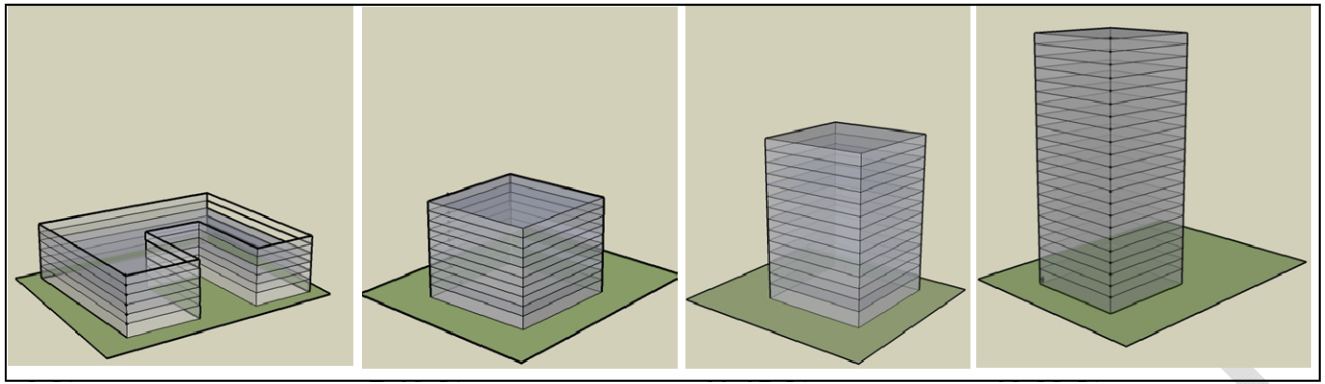


Figure 19: The floor plate of buildings will decrease as the height of the building increases.

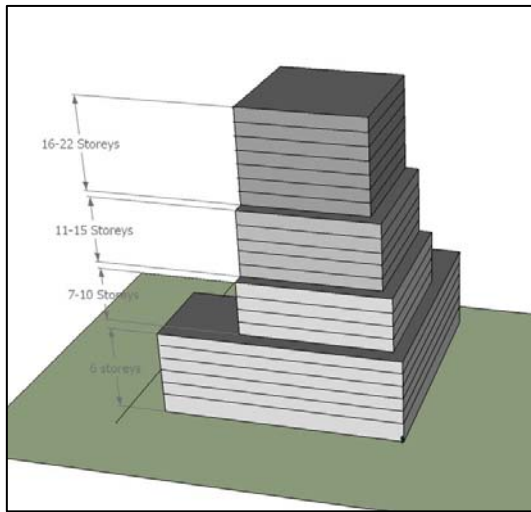


Figure 20: Combination of floor plates and heights may be permitted.

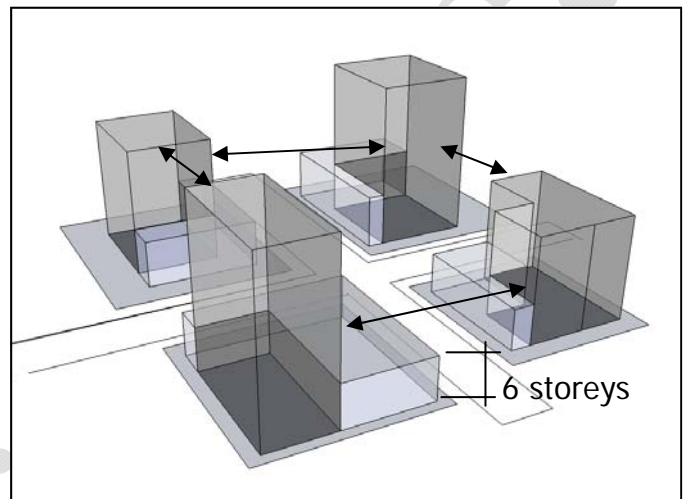


Figure 22: Development will reflect appropriate separation distances between buildings over six storeys.

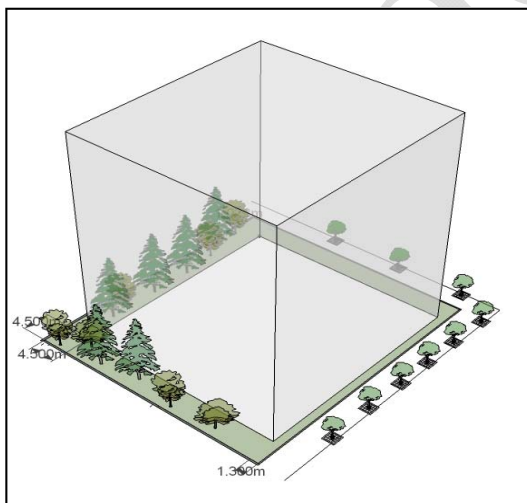


Figure 21: Appropriate landscaping, reflecting the use and context will be provided.



Figure 23: Different streets will have different streetscape requirements, for example, within the mainstreet precinct, development should provide a desirable streetedge condition that is ideal for the use of pedestrians.



- b. minimize vehicular access points; and
- c. creating an attractive public realm.

### **10.2.2 Central Residential Precinct**

This precinct contains a significant concentration of apartment buildings with potential for intensification, primarily in the immediate vicinity of the GO station and will have the highest building heights in Port Credit. The existing character of the area will generally be maintained, particularly the mature trees and the well landscaped front yards.

10.2.2.1 Building heights will generally decrease towards the east and west of the precinct, reflecting proximity of either the Credit River Valley or established residential neighbourhoods.

10.2.2.2 Building heights on lots adjacent to the Mainstreet Precinct will demonstrate an appropriate transition.

10.2.2.3 To achieve the maximum heights, as outlined on Schedule 2B, on the lands designated Mixed Use or Utility in the vicinity of the GO station, a detailed land use and urban design study will be required to verify appropriate heights, design, transition to adjacent lands and mix of uses.

### **10.2.3 Mainstreet Node Precinct**

This precinct includes part of Port Credit's traditional mainstreet which generally extends a half block north and south of Lakeshore Road (east and west). In many cases this coincides with the alignment of a public lane; where it does not, the limit of the area is defined by an extension of a line from the public lane.

10.2.3.1 This precinct will contain street related commercial uses with a rhythm of closely spaced storefronts lining the street in order to encourage and foster an active pedestrian street.

10.2.3.2 Single use residential buildings are not permitted.

10.2.3.3 The Mainstreet Node Precinct will meet both the day-to-day needs of local residents as well as those visiting the area.

10.2.3.4 For the portion of the Mainstreet Node Precinct on the south side of Lakeshore Road West, between Mississauga Road and the Credit River, the Old Port Credit Village Heritage Conservation District Plan also applies.

### **10.2.4 Harbour Mixed Use Precinct**

This precinct has potential for intensification and is intended to contain a mixture of uses and densities. Development will be at a lower overall scale than the Central Residential Precinct and will step down towards Lake Ontario.

Marina uses are recognized as important elements of the Harbour Mixed Use Precinct and Port Credit. It is recognized that the waterfront will be more urban in nature, with a wider range of uses and activities that help reinforce the elements of a waterfront setting.

The marina lands south of Port Street have redevelopment potential. Further study, however, is required to determine the appropriate built form of any redevelopment. The Special Site policies provide additional direction regarding issues that will need to be studied.

10.2.4.1 The scale of development will be supportive of an urban waterfront village theme.

10.2.4.2 Provision of public access and additional public lands along the waterfront will be a priority consideration in any redevelopment of properties on the waterfront.

10.2.4.3 Development will maintain existing view corridors to Lake Ontario.

10.2.4.4 For the portion of the Harbour Mixed Use Precinct, on the south side of Lakeshore Road West, between Front Street South and the Credit River, the Old Port Credit Village Heritage Conservation District Plan applies.

### 10.2.5 Riverside Precinct

This precinct is intended to provide a transition between the taller building heights in the Central Residential Precinct and the low rise building heights in the North Residential Neighbourhood.

10.2.5.1 New development will demonstrate an appropriate transition to the Credit River valley, and the Mainstreet Precinct.

10.2.5.2 Any redevelopment along Mississauga Road North will consider its character as a **scenic route**.

10.2.5.3 Pedestrian open spaces will be planned to visually and physically extend Port Credit Memorial Park West into this neighbourhood along High Street West through to Front Street West, and along Park Street West.

## 10.3 Neighbourhood Character Area

The Neighbourhood Character Area represents stable residential areas where the existing character is to be preserved and will not be the focus for intensification. Where development occurs, it will generally be through modest infilling or development within the Lakeshore Road (east and west) Mainstreet Neighbourhood Precinct, the existing commercial plaza or the vacant former refinery site.

The Neighbourhood Character Area is not uniform and contains unique issues that are addressed through various precincts (e.g. heritage conservation district, proximity to railway and existing employment uses, large vacant brownfield site).

Neighbourhood policies are intended to reflect a number of objectives, including among other things:

- to ensure development is sensitive to the existing low rise context and to reinforce the planned character of the area;
- to ensure Lakeshore Road (east and west) will undergo appropriate development and provide

for a public realm that reinforces its planned role as a location that helps connect the community and fosters an active pedestrian and cycling environment;

- to appropriately balance the constraints associated with both the Canadian National Railway line and adjacent residential uses;
- to recognize the former refinery site as an important location along the waterfront that requires special attention and further study; and
- to recognize the Old Port Credit Heritage Conservation District.

### 10.3.1 Neighbourhood Character Area General Policies

10.3.1.1 Streetscape along Lakeshore Road (east and west) will address among other matters the following:

- a. minimize vehicular access points;
- b. orient entrances and buildings towards the street; and
- c. create an attractive public realm.

### 10.3.2 Old Port Credit Village Heritage Conservation District Precinct

The Old Port Credit Village Heritage Conservation District Plan applies to the lands within this precinct.

The precinct contains a mixture of housing, retail commercial and community buildings of many types, representing different eras. While some of the housing stock is relatively new, the neighbourhood contains pockets of housing that date back to the nineteenth century, representing various time frames and a pleasing sense of "time depth".

The precinct is predominately low rise in character, however, the existing low rise apartment buildings are recognized as forming part of the precinct.

10.3.2.1 Any additions, alterations, adaptive reuse or redevelopment will address how the development:

- displays massing and scale sympathetic to surroundings;
- preserves the historic housing stock;
- supports the existing historical character;
- maintains the existing street grid pattern and building setbacks; and
- maintains and enhances significant groupings of trees and mature vegetation.

10.3.2.2 Mississauga will encourage landscape screening along the west side of Mississauga Road South to buffer the adjacent vacant former refinery site.

### 10.3.3 Vacant Former Refinery Precinct

This precinct consists of a property formerly used as a refinery that is predominately vacant. Lands along Lake Ontario accommodate public open space and the Waterfront Trail.

The precinct represents a significant property along Lake Ontario that has the opportunity to create vibrant areas of interest that can enhance the existing community and offer increased public access to the waterfront.

This precinct should ultimately be developed in a manner which is compatible with the surrounding lands, and which does not detract from the planned function of the Community Node.

This precinct has redevelopment potential, however, further study is required to determine appropriate development. The precinct policies and Special Site policies provide additional direction regarding issues that need to be addressed.

10.3.3.1 Building heights will provide appropriate transition to the adjacent South Residential and Old Port Credit Heritage Conservation District Precincts.

10.3.3.2 A public road will be provided in any future development that separates any new development from the Lake Ontario waterfront open space and the continuous Waterfront Trail to maximize public access to and along the Lake Ontario waterfront.

10.3.3.3 A landscaped buffer will be maintained between the precinct and the adjacent residential neighbourhood to the west.

### 10.3.4 North Residential Neighbourhoods (Shawnmarr/Indian Heights and Credit Grove) Precinct

This precinct includes the areas known as Shawnmarr/Indian Heights and Credit Grove located on the west and east sides of the Community Node, between the railway line and the neighbourhood mainstreet area along Lakeshore Road West and East. These predominately stable residential areas will be maintained while allowing for infill which is compatible with and enhances the character of the area.

10.3.4.1 The predominant characteristics of these areas will be preserved including:

- a. low rise building heights;
- b. the combination of small building masses on small lots;
- c. the well landscaped **streetscapes**; and
- d. the regular street grid.

10.3.4.2 New development is encouraged to reflect 1 to 2 storey residential building heights and should not exceed 3 storeys.

10.3.4.3 Properties fronting Lakeshore Road West will complement the adjacent Mainstreet Precinct by continuing the general 4 storey height limits. Single use residential buildings are permitted along this portion of Lakeshore Road West.

10.3.4.4 The existing commercial plaza located generally between Harrison Avenue and Wesley Crescent provides commercial floor space that serves the area but also represents a site for potential intensification with a maximum height limit of 4 storeys. The front portion of the commercial

site is considered to be part of the Mainstreet Precinct and the rear is part of the North Residential Neighbourhoods Precinct. Any future redevelopment of the property will address issues, including but not limited to:

- a. constraints given previous industrial uses on the property;
- b. potential road connections and improvements to the neighbourhood's fine grained road pattern;
- c. retention of commercial space; and
- d. appropriate transitions to adjacent residential uses.

10.3.4.5 The Canadian National Railway line defines the northern boundary of this area and restricts the types of use that can be redeveloped on adjacent properties. Development of these lands will:

- a. be encouraged to improve the transition to adjacent residential areas;
- b. have a maximum height generally equivalent to a 2 storey residential building, for warehousing, self-storage, wholesaling and manufacturing;
- c. ensure lighting, noise levels, loading and garbage areas do not negatively impact adjacent residential areas;
- d. provide appropriate landscaped buffers to adjacent residential areas;
- e. provide a **streetscape** that is compatible with adjacent neighbourhoods.

10.3.4.6 Any redevelopment along Mississauga Road North will consider its character as a **scenic route**.



Figure 24: Business Employment uses can be an appropriate buffer between residential neighbourhoods and the railway, provided they have an appropriate height and landscaping, and uses are kept predominately within buildings.

### 10.3.5 South Residential Neighbourhoods (Cranberry Cove, Hiawatha) Precinct

This precinct includes the areas known as Cranberry Cove and Hiawatha, located on the west and east sides of the Community Node, between Lakeshore Road West and East and the waterfront. These predominately stable residential areas will be maintained while allowing for infill which is compatible with and enhances the character of the area.

10.3.5.1 The predominant characteristics of these areas will be preserved including:

- a. low rise building heights;
- b. the combination of relatively small building masses on small lots;
- c. the physical and visual access to Lake Ontario from parks and the terminus of streets;
- d. the well landscaped **streetscapes**; and
- e. street grid pattern.





Figure 25: The established low density residential characteristics of Port Credit Neighbourhoods are to be preserved.

10.3.5.2 New development will have a maximum height generally equivalent to 2 storeys.

10.3.5.3 Properties fronting Lakeshore Road West will complement the adjacent mainstreet precinct by continuing the general 4 storey height limits. Single use residential buildings are permitted along this portion of Lakeshore Road West.

### 10.3.6 Mainstreet Neighbourhood Precinct

This Precinct is intended to contain a mixture of uses that help meet the day-to-day needs of residents. This precinct represents an extension of the Mainstreet Node Precinct and generally extends a half block north and south of Lakeshore Road West and East.

This precinct will contain street related commercial uses with closely spaced storefronts lining the street to encourage and foster an active pedestrian street.

10.3.6.1 Development will provide view corridors to the lake, where appropriate.

10.3.6.2 The assembly of adjacent low density residential land to enlarge properties fronting the Mainstreet Precinct is discouraged. However, should assembly occur, the primary purpose of these lands should be for buffering the adjacent residential uses and for amenity space and/or parking if required through the development.



Figure 26: Development along Lakeshore Road will provide for a mainstreet commercial environment and attractive public realm that reinforces its planned role as a location that helps connect the community and foster an attractive and safe public realm.

## 11.0 STRONG ECONOMY

Employment opportunities are an important component of Port Credit as they contribute to a healthy mixture of land uses, help provide a complete community, and are supportive of a village where people have the opportunity to live work and play in the same area.

Port Credit contains a significant number of jobs located outside of the Community Node. Employment is found primarily focused in the businesses along Lakeshore Road, along the railway corridor, and at school sites. In total, approximately one third of the Area Plan's employment is located outside of the node but within Port Credit.

This Area Plan provides for the continued operation and expansion of employment uses. Generally, the most appropriate employment uses are in the education, office, culture, and retail sectors. In particular, creative enterprises that are involved in the creation, production and distribution of goods and services that use creativity and intellectual capital as primary inputs are encouraged. These include but are not limited to: artists, galleries, architects, graphic designers, and software developers.

11.1 Opportunities for lake dependent or waterfront retail commercial activities, including



activities such as marinas, and facilities in support of recreational sport fishing, will be promoted along the portion of the waterfront located within the Community Node.

## **12.0 LAND USE DESIGNATIONS**

This section provides additional policy direction on the permitted land uses within this Area Plan. Schedule 10 (Land Use Designations) of the principal document, identifies the uses of land permitted and will be read in conjunction with the other schedules and policies in the Plan.

### **12.1 General**

12.1.1 Residential buildings legally constructed prior to the approval date of this Area Plan are permitted.

### **12.2 Residential Low Density I**

12.2.1 Notwithstanding the Residential Low Density I policies of the Plan, the following uses will not be permitted:

- a. semi-detached dwelling; and
- b. duplex dwelling.

### **12.3 Residential Low Density II**

12.3.1 Notwithstanding the Residential Low II Density policies of the Plan, the following additional use will be permitted:

- a. existing office uses;

### **12.4 Residential Medium Density**

12.4.1 Notwithstanding the Residential Medium Density policies of the Plan, the following additional use may be permitted:

- a. low rise apartment dwellings;

12.4.2 Notwithstanding the Residential Medium Density policies of the Plan, the following additional use will be permitted

- a. existing office uses;

### **12.5 Residential High Density**

12.5.1 Notwithstanding the Residential High Density policies of the Plan, the following additional use will be permitted:

- a. existing office uses.

### **12.6 Mixed Use**

12.6.1 Notwithstanding the Mixed Use policies of the Plan, the following uses will not be permitted:

- a. motor vehicle rental.

12.6.2 Notwithstanding the Mixed Use policies of the Plan, drive-through facilities are not permitted on sites identified on Schedule 3.

### **12.7 Public Open Space**

12.7.1 Notwithstanding the Public Open Space policies of the Plan, the following additional use may be permitted:

- a. commercial parking facility.

### **12.8 Business Employment**

12.8.1 Notwithstanding the Business Employment policies of the Plan, the following uses will not be permitted:

- a. adult entertainment establishments;
- b. animal boarding establishments, which may include outdoor facilities;
- c. body rub establishments;
- d. broadcasting, communication and utility rights-of-way;
- e. cardlock fuel dispensing facility;
- f. commercial parking facility;
- g. funeral establishment;
- h. motor vehicle rental;
- i. overnight accommodation;
- j. restaurant;
- k. transportation facility;
- l. truck terminal; and

m. waste processing or transfer stations and composting facilities.

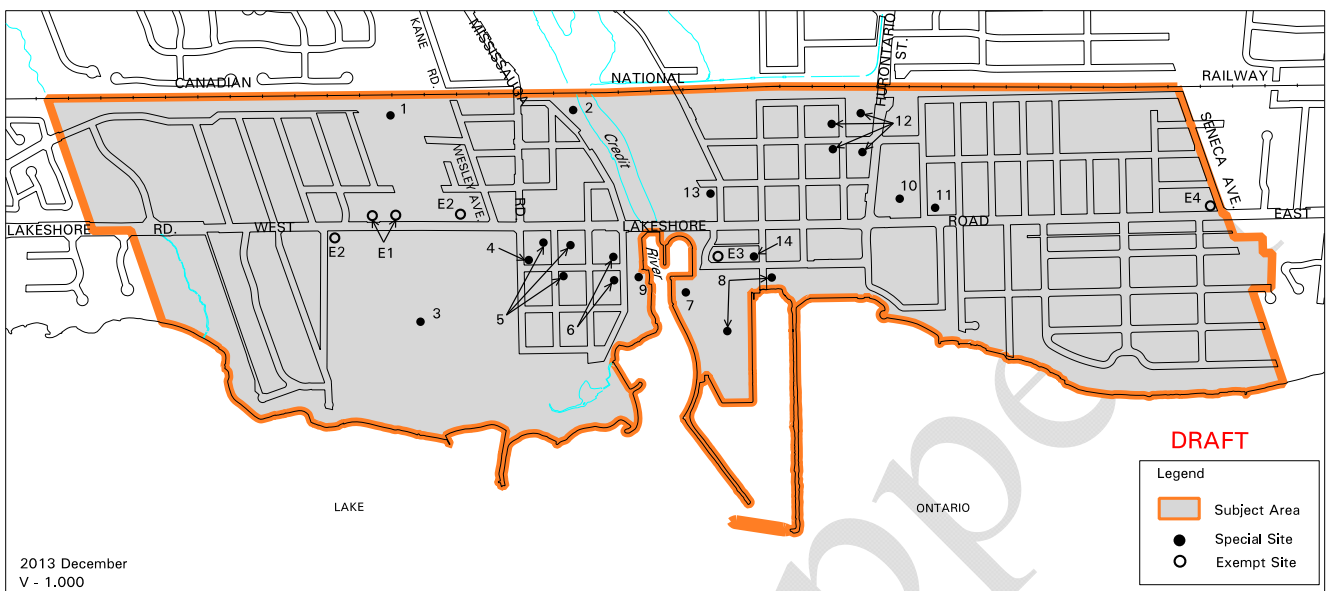
12.8.2 Notwithstanding the Business Employment policies of the Plan, the following policies apply:

a. permitted uses will operate entirely within enclosed buildings;

## **12.9 Special Waterfront**

A comprehensive master plan will be carried out to determine the use of the lands, following which this Area Plan will be appropriately amended.

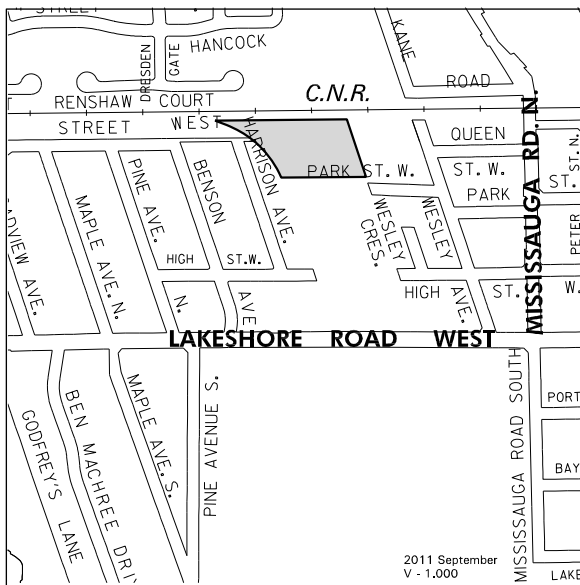
## 13.0 SPECIAL SITES & EXEMPT SITES



## 13.1 Special Sites

There are sites that merit special attention and are subject to the following policies.

### 13.1.1 Site 1



13.1.1.1 The lands identified as Special Site 1 are located south of the Canadian National Railway and east of Queen Street West.

13.1.1.2 Notwithstanding the provisions of the Business Employment designation and the Desirable Urban Form policies, the following additional policies will apply:

- a. development (including servicing) will be permitted following the filing of a Record of Site Condition (RSC) on the Ministry of Environment's Registry in accordance with Ont. Reg.153/04 as amended;
- b. vehicular access to the site will be provided from Lakeshore Road West through an existing easement. Access from surrounding residential streets will only be permitted subject to addressing the appropriateness of the volume

and type of traffic that would be accommodated on residential streets; and

- c. building heights for secondary offices uses will be a maximum of 4 storeys.

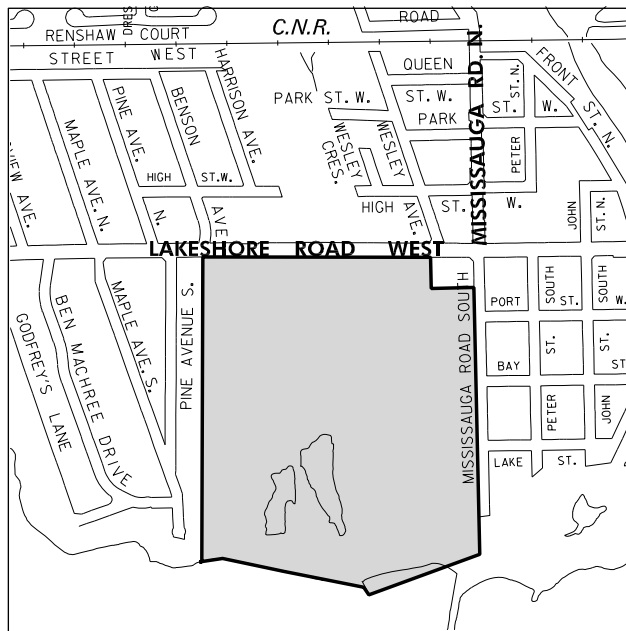
### 13.1.2 Site 2



13.1.2.1 The lands identified as Special Site 2 are located south of the Canadian National Railway tracks, east of Front Street North.

13.1.2.2 Notwithstanding the provisions of the Private Open Space and Greenbelt designations, the existing private club will be permitted.

### 13.1.3 Site 3



13.1.3.1 The lands identified as Special Site 3 are located on the south side of Lakeshore Road West, between Mississauga Road South and the rear yards of residential homes fronting onto Pine Avenue South.

13.1.3.2 Notwithstanding the provisions of the Special Waterfront, Public Open Space, Greenbelt designations, and the Desirable Urban Form policies, further study is required to determine the feasibility and appropriate type of redevelopment of these lands.

13.1.3.3 A comprehensive master plan will be prepared to the City's satisfaction that will address, among other matters, land use, built form and transportation. In addition, the master plan will:

- a. determine the type(s) and extent of contamination on the site, investigate remedial strategies and identify any constraints with respect to land uses proposed for the site;
- b. have regard for other City plans, policies and reports, such as the Lakeview and Port Credit District Policies Review and Public Engagement

Process – Directions Report and the Waterfront Parks Strategy 2008;

- c. include provision of significant public parklands along and access to the waterfront including the Waterfront Trail;
- d. provide views to Lake Ontario;
- e. continue the mainstreet mixed use function along Lakeshore Road West;
- f. examine unique opportunities to take advantage of the site's size and location on the waterfront; and,
- g. provide opportunities to accommodate employment uses.

13.1.3.4 Consultation on the comprehensive master plan will occur with the landowners, the local community, and other stakeholders.

13.1.3.5 Approval of an Official Plan Amendment implementing the master plan is required prior to development.



### 13.1.4 Site 4



13.1.4.1 The lands identified as Special Site 4 are located east of Mississauga Road South, north of Port Street West.

13.1.4.2 Notwithstanding the provisions of the Residential Low Density I designation, the following additional policy will apply:

- a. offices, restaurants and personal service uses may be permitted.

### 13.1.5 Site 5



13.1.5.1 The lands identified as Special Site 5 are located south of Lakeshore Road West and west of John Street South.

13.1.5.2 Notwithstanding the provisions of the Mixed Use and Residential Low Density I designations, the following additional policy will apply:

- a. special care is required to preserve the character of the buildings and properties of the community infrastructure along Lakeshore Road West and Port Street West. Particular care will be taken in the preservation of the exterior character of the structures, and related exterior open space areas. The existing cemetery at the corner of Lakeshore Road West and John Street South will be preserved.

### 13.1.6 Site 6



13.1.6.1 The lands identified as Special Site 6 are located on the west side of Front Street South and the south side of Port Street West, south of Lakeshore Road West.

13.1.6.2 Notwithstanding the provisions of the Mixed Use designation on these lands, the following additional policies will apply:

- buildings, including amenities and signage, whether new or modified, will have a single detached residential appearance consistent with the form, design and scale of the low density residential land use in the Old Port Credit Village Heritage Conservation District. In addition, the use will have a limited impact on the low density residential character of the area. New buildings or modified buildings will comply with the development standards of the adjacent low density residential area;
- any garage, detached or attached, will be located in the side or rear yard behind the front wall of the building. Any additional parking required will be accommodated to the rear of the lot;

- front yards will be mostly landscaped and oriented to Front Street South or Port Street West;
- appropriate design treatment will be provided to both facades at street corners;
- every effort will be made to have buildings of historic interest, as defined in the Old Port Credit Village Heritage Conservation District Plan, retained within any future redevelopment.

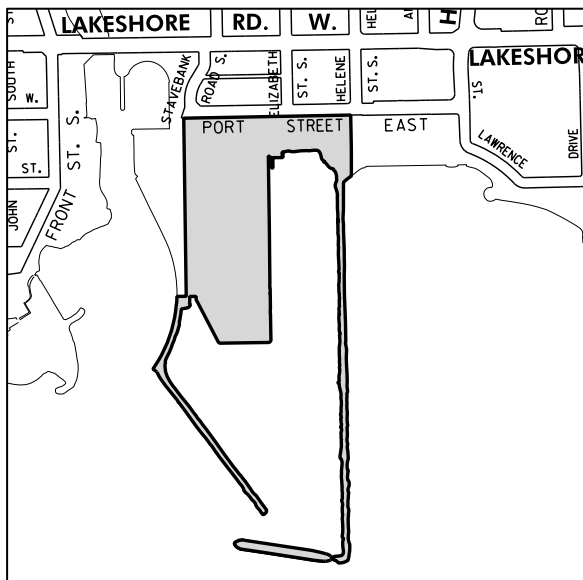
### 13.1.7 Site 7



13.1.7.1 The lands identified as Special Site 7 are located west of Stavebank Road South, south of Lakeshore Road East.

13.1.7.2 Notwithstanding the provisions of the Public Open Space and Greenbelt designations, the existing restaurant and marina will be permitted.

### 13.1.8 Site 8



13.1.8.1 The lands identified as Special Site 8 are located south of Port Street East and east of the Credit River.

13.1.8.2 Notwithstanding the provisions of the Mixed Use and Greenbelt designations and the Desirable Urban Form policies, the following additional policies will apply:

- a. marina, boat repair, service and storage will be permitted; and
- b. further study is required to determine the appropriate type of redevelopment on these lands.

13.1.8.3 A comprehensive master plan will be prepared to the City's satisfaction that will address, among other matters, land use, built form and transportation. In addition, the master plan will:

- a. determine contamination on the site;
- b. have regard for other City plans, policies and reports, such as the Lakeview and Port Credit District Policies Review and Public Engagement Process – Direction Report and the Waterfront Parks Strategy 2008;

- c. include provision of significant public parklands along and access to the waterfront including the extension of the Waterfront Trail;
- d. preserve and enhance views to Lake Ontario;
- e. examine unique opportunities to take advantage of the site's location and reflect the historic marine heritage of the area including preserving the marina function and the Ridgetown breakwater;
- f. consider the appropriate built form around the future terminus of the Light Rail Transit planned along Port Street East;
- g. provide opportunities to accommodate employment uses.

13.1.8.4 Consultation on the comprehensive master plan will occur with the landowners, local community, and other stakeholders.

### 13.1.9 Site 9



13.1.9.1 The lands identified as Special Site 9, Marina Park, are located at the southeast corner of Lakeshore Road West and Front Street South.

13.1.9.2 Marina Park is to be developed as an integral component of the Port Credit Harbour and Old Port Credit Conservation District in terms of complementary uses and design while recognizing its potential to establish a vibrant river and village edge.

13.1.9.3 Notwithstanding the provisions of the Public Open Space and Greenbelt designations, the following additional policies will apply:

a. in addition to the uses permitted by the Public Open Space designation, the following uses may be permitted subject to, among other matters, the approval of Credit Valley Conservation:

- **Secondary offices;**
- retail commercial uses, except for commercial uses with a drive-through facility;
- conference centre;
- marina;

b. in addition to the uses permitted by the Greenbelt designation, a marina will be permitted;

c. Marina Park will be developed as an integral part of both Port Credit Harbour and the Old Port Credit Village Heritage Conservation District, having regard for the following principles:

- public access to the Credit River will be enhanced;
- views of the Credit River from both the Port Street West and the Bay Street road allowances will be extended through the site;
- building height will not exceed 2 storeys;
- buildings will be articulated to reduce the perception of bulk;
- buildings will be oriented to the Credit River, Front Street South and the street grid;
- pedestrian links along the Credit River through the site and both north and south of it will be pursued, where feasible;
- historical interpretation of the site will be integrated into any future development;

d. prior to any development, the City will prepare a master plan to address the future use and layout of Marina Park. The master plan will be prepared in consultation with the public, and address, among other matters, the future layout of the site, archaeological assessment, historical interpretation opportunities, and the feasibility of a river trail; and

e. when the master plan has been prepared and approved by City Council, the "holding" designation in the Zoning By-law may be removed.

### 13.1.10 Site 10



13.1.10.1 The lands identified as Special Site 10 are located on the north side of Lakeshore Road East, east of Hurontario Street.

13.1.10.2 Notwithstanding the provisions of the Mixed Use designation and Desirable Urban Form policies, the following additional policy will apply:

- a. the existing gas bar and a motor vehicle wash will be permitted.

### 13.1.11 Site 11



13.1.11.1 The lands identified as Special Site 11 are located at the northeast corner of Lakeshore Road East and Rosewood Avenue.

13.1.11.2 Notwithstanding the provisions of the Mixed Use designation and Desirable Urban Form policies, the following additional policy will apply:

- a. the existing motor vehicle repair garage will be permitted.



### 13.1.12 Site 12



13.1.12.1 The lands identified as Special Site 12 are located west of Hurontario Street, south of the C.N. Railway, east of Helene Street, and north of High Street.

13.1.12.2 Notwithstanding the provisions of the Mixed Use and Utility designations and the Desirable Urban Form policies, further study is required to determine the appropriate type of redevelopment on these lands.

13.1.12.3 These lands are in an important location that can further the development of the Port Credit Mobility Hub. A comprehensive master plan will be prepared to the City's satisfaction that will address, among other matters, land use, built form, transportation and heritage resources. In addition, the master plan will:

- a) have regard for other City and Provincial plans, policies and reports such as those related to the future Light Rapid Transit on Hurontario and Mobility Hubs;
- b) determine appropriate access improvements and linkages for pedestrians, cyclists, and commuters traveling between the GO station and future LRT stop;

- c) provide amenities such as secure storage facilities for bicycles, car share drop-off areas, heated waiting areas, traveler information centres, cafes and restaurants, as well as services such as daycares, or grocery stores;
- d) address appropriate design of any parking structures; and
- e) provide of opportunities to accommodate employment uses.

13.1.12.4 Consultation on the comprehensive master plan will occur with the landowners, local community and other stakeholders.

### 13.1.13 Site 13



13.1.13.1 The lands identified as Special Site 13 are located west of Stavebank Road, south of High Street and north of Lakeshore Road East.

13.1.13.2 Notwithstanding the provisions of the Desirable Urban Form policies, building heights will be a maximum of 6 storeys and a minimum of 2 storeys, subject to:

- appropriate transition to buildings fronting Lakeshore Road East;
- appropriate transition to St. Andrews Church; and
- appropriate setbacks from Stavebank Road, above the third storey.

### 13.1.14 Site 14



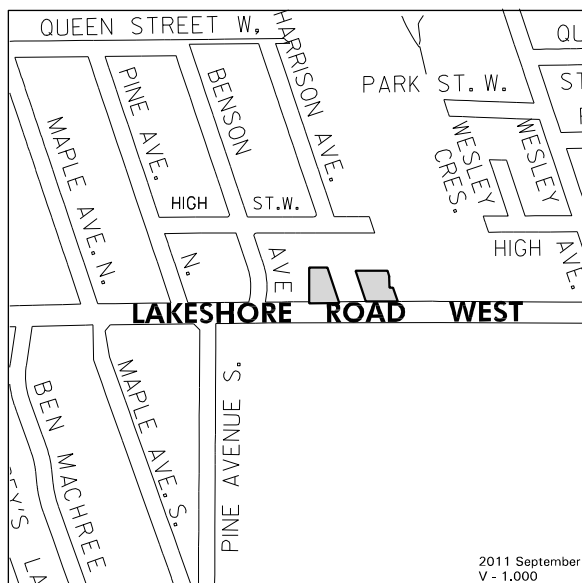
13.1.14.1 The lands identified as Special Site 14 are located on the north side of Port Street East and the west side of Elizabeth Street South.

13.1.14.2 Notwithstanding the provisions of the Desirable Urban Form policies, building heights will be a maximum of 7 storeys.

## 13.2 Exempt Sites

Exempt sites reflect unique circumstances that are not representative of the Vision, direction and planning policies of this Area Plan, but nonetheless, are recognized because they contain established land uses.

### 13.2.1 Exempt Site 1

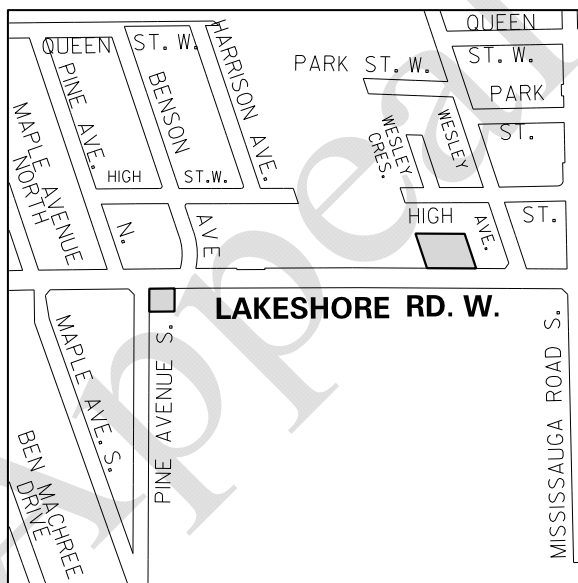


13.2.1.1 The lands identified as Exempt Site 1 are located on the north side of Lakeshore Road West, east of Benson Avenue.

13.2.1.2 Notwithstanding the provisions of the Mixed Use designation and Desirable Urban Form policies, the following additional policy will apply:

- a. the existing motor vehicle repair uses will be permitted.

### 13.2.2 Exempt Site 2



13.2.2.1 The lands identified as Exempt Site 2 are located on the north side of Lakeshore Road West, west of Wesley Avenue, and on the south side of Lakeshore Road West, east of Pine Avenue South.

13.2.2.2 Notwithstanding the provisions of the Mixed Use designation and Desirable Urban Form policies, the following additional policy will apply:

- a. the existing motor vehicle sales and rental will be permitted;

### 13.2.3 Exempt Site 3

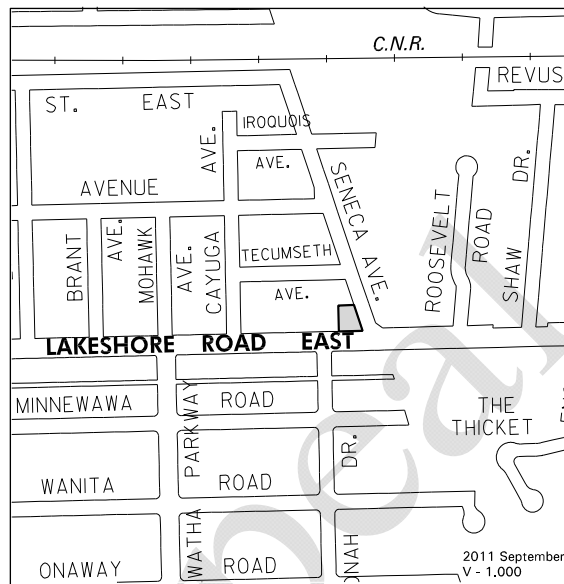


13.2.3.1 The lands identified as Exempt Special 3 are located on the north side of Port Street, east of Stavebank Road South.

13.2.3.2 Notwithstanding the Desirable Urban Form policies, the following additional policy will apply:

- a. the existing apartment hotel with a maximum building height of 14 storeys will be permitted.

### 13.2.4 Exempt Site 4



13.2.4.1 The lands identified as Exempt Site 4 are located at the northwest corner of Lakeshore Road East and Seneca Avenue.

13.2.4.2 Notwithstanding the provisions of the Mixed Use designation and Desirable Urban Form policies, the following additional policy will apply:

- a. the existing motor vehicle wash will be permitted.

## 14.0 IMPLEMENTATION

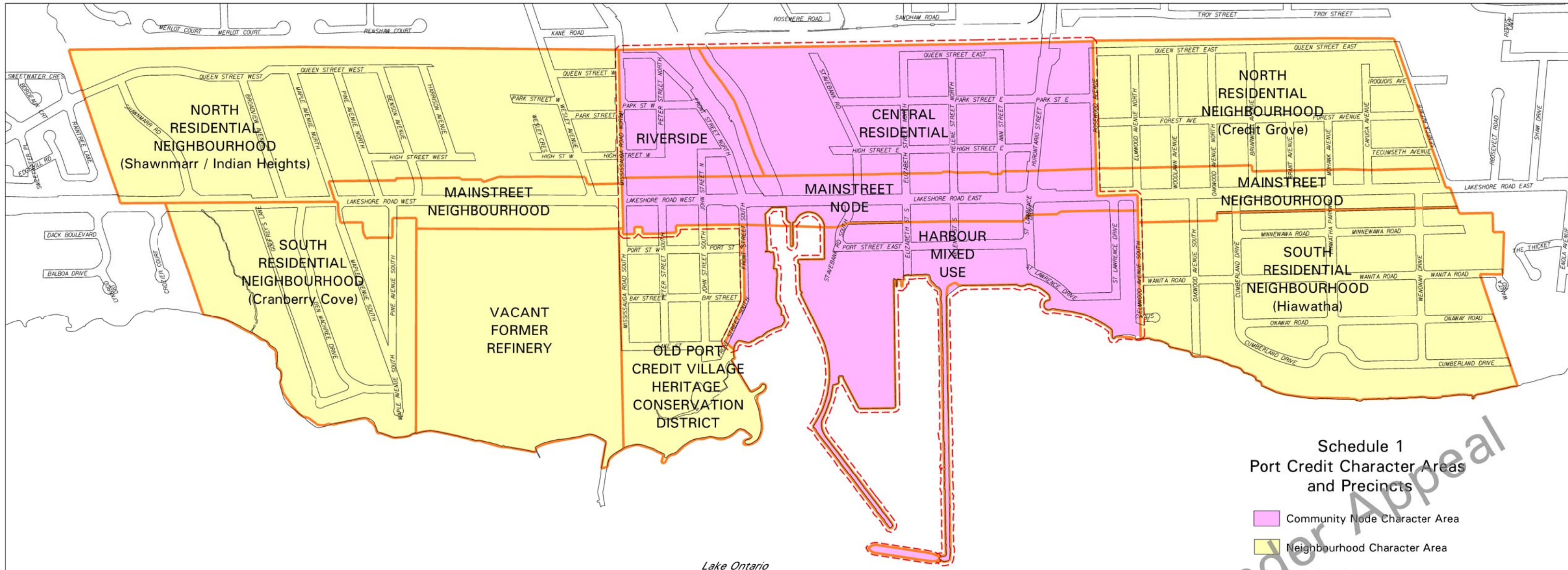
14.1 Mississauga will monitor development in Port Credit, including population density, and the population to employment ratio.

14.2 Mississauga, in conjunction with public consultation, may develop a list of facilities/matters that could be exchanged for granting bonuses in height and/or density.

14.3 Mississauga, in conjunction with public consultation, may prepare a Community Improvement Plan.

14.4 Prior to development, master plans for the Port Credit Marina property and the former refinery will be prepared to the City's satisfaction.





Schedule 1  
Port Credit Character Areas  
and Precincts

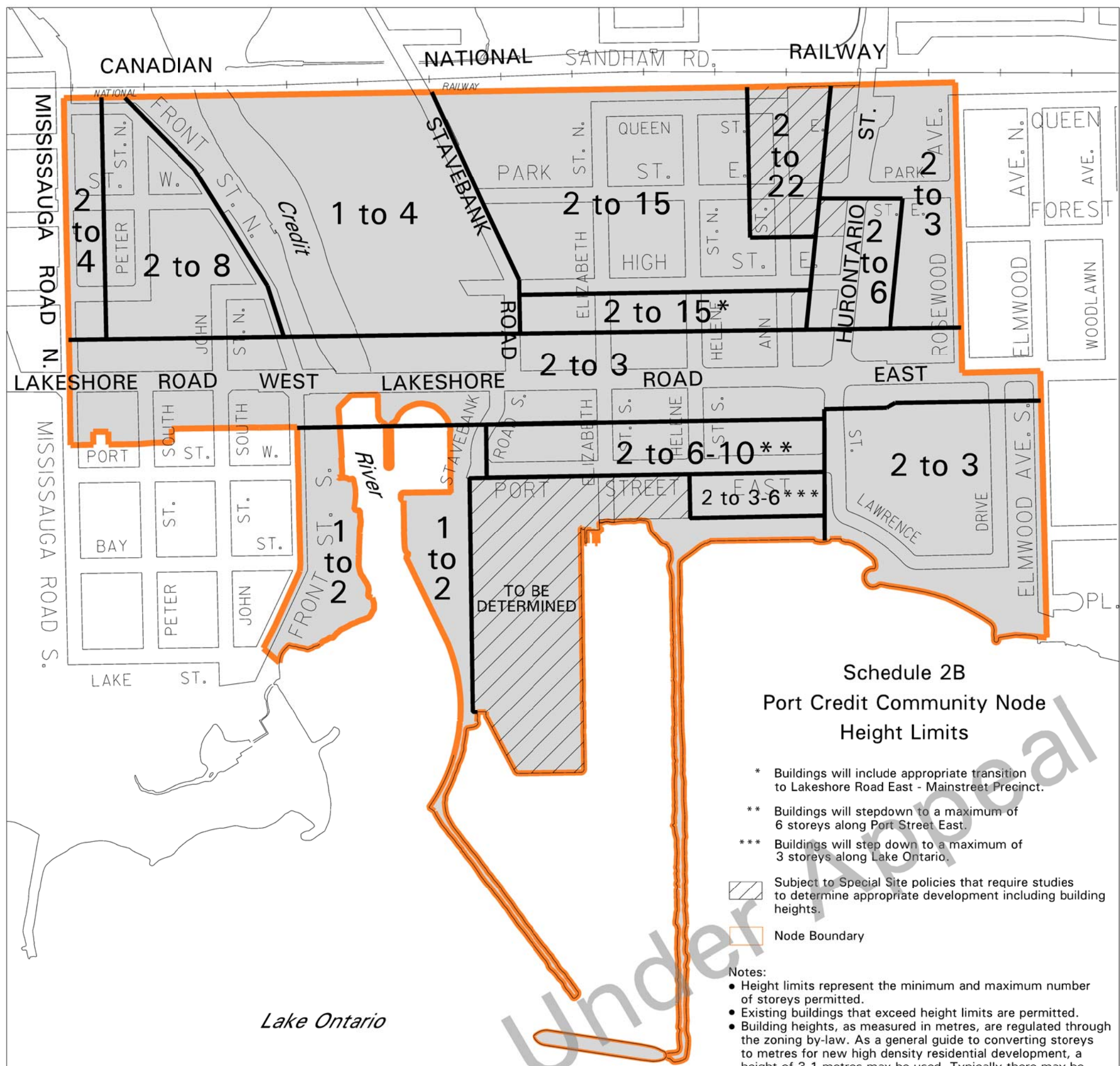
- Community Node Character Area
- Neighbourhood Character Area
- Node Boundary
- Precinct Boundary

Lake Ontario









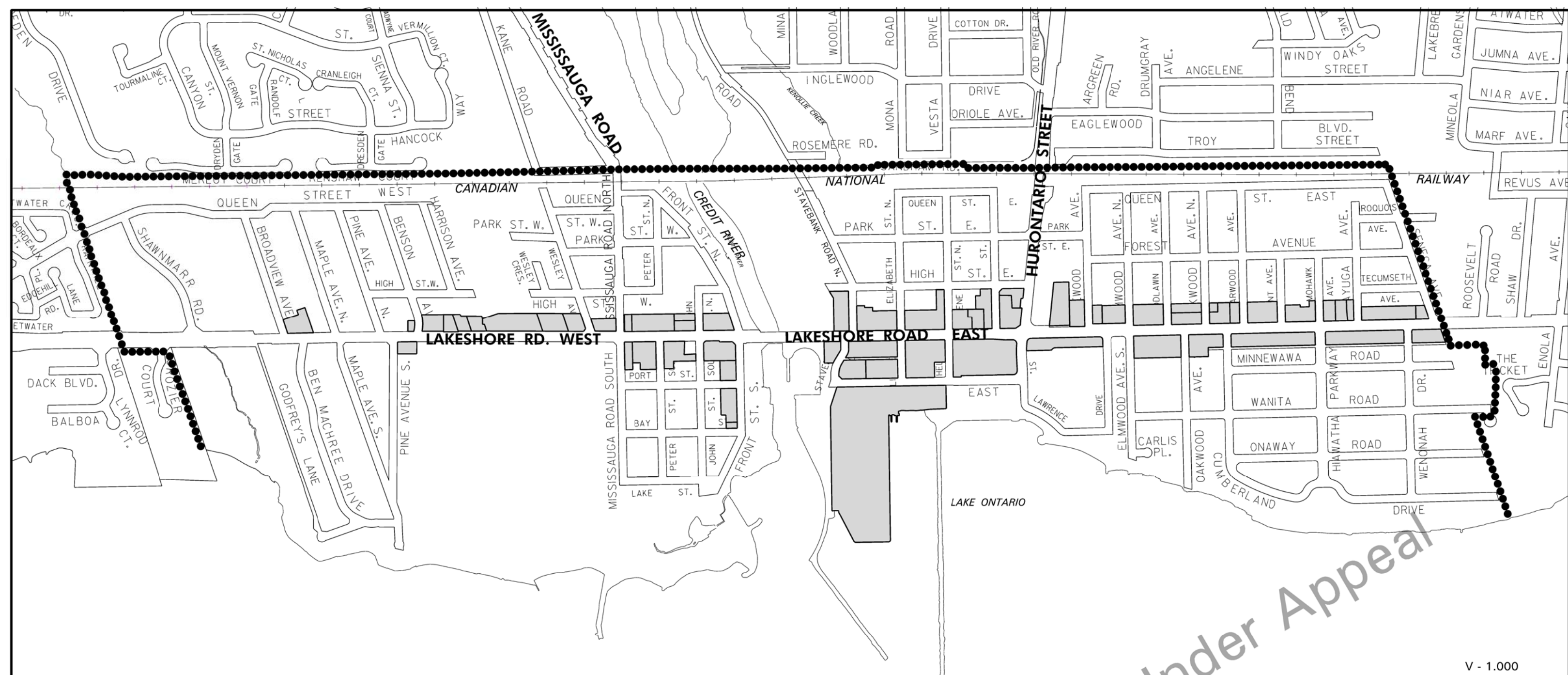


**Schedule 2B**  
**Port Credit Community Node**  
**Height Limits**

- \* Buildings will include appropriate transition to Lakeshore Road East - Mainstreet Precinct.
- \*\* Buildings will stepdown to a maximum of 6 storeys along Port Street East.
- \*\*\* Buildings will step down to a maximum of 3 storeys along Lake Ontario.
-  Subject to Special Site policies that require studies to determine appropriate development including building heights.
-  Node Boundary

**Notes:**

- Height limits represent the minimum and maximum number of storeys permitted.
- Existing buildings that exceed height limits are permitted.
- Building heights, as measured in metres, are regulated through the zoning by-law. As a general guide to converting storeys to metres for new high density residential development, a height of 3.1 metres may be used. Typically there may be a modest increase in height for lobby areas and/or commercial space.



V - 1.000

## SCHEDULE 3 – PORT CREDIT DRIVE-THROUGH PROHIBITIONS



LANDS DESIGNATED MIXED USE WHERE DRIVE-THROUGH FACILITIES  
ARE NOT PERMITTED, AS PER OMB DECISION ON DRIVE-THROUGH FACILITIES CASE NO: PL 111148, DECISION DATE- JULY 8, 2013



Produced by  
T&W, Geomatics

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# Appendix I

# PORT CREDIT

## Built Form Guide

October 2013



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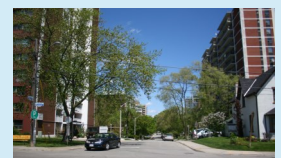
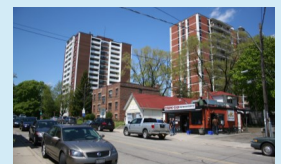


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# 1.0 Introduction

## 1.0 Introduction

### 1.1 How to Read the Built Form Guide

The Built Form Guide (the Guide) is to be read in conjunction with the policies in the Mississauga Official Plan (the Plan) and the Port Credit Local Area Plan (Area Plan). The Guide is to be used during the design and review of development applications.

This Guide demonstrates how the urban form policies in the Plan can be achieved. The Guide is not considered a part of the Area Plan; however, selected content from the Guide has been incorporated into the Area Plan and represents policy. Additionally, applicants must also refer to the principal document, Port Credit Local Area Plan, Zoning By-law, and Building Code and applicable design guidelines and reference notes to ensure that the applicable policies and requirements in these documents have been met. In addition, there may be other City initiatives and directions such as the Old Port Credit Heritage Conservation Plan, Green Development Strategy which need to be consulted.

### 1.2 Purpose

Building a desirable urban form is a key principle of the Mississauga Official Plan. The Guide is intended to assist in understanding and implementing the Desirable Urban Form policies in the Mississauga Official Plan and the Port Credit Local Area Plan. The Guide establishes and illustrates general requirements necessary to achieve a high quality urban form, site development and public realm.

The Guide is intended to ensure development is appropriate for Port Credit and reflects the unique characteristics of the area. Depending on the context or site, exceptions and variation from the Guide may be considered at the discretion of the City in order to provide some flexibility.



Figure A1 — Port Credit Node and Neighbourhood Images

## 1.3 Port Credit Local Area Plan

The Area Plan includes lands identified in the City structure as Community Node and Neighbourhood.

Both the Community Node and Neighbourhood Character Areas are divided into precincts which recognize different character attributes of these areas and contain different policy directions and are organized as follows:

### Community Node Character Area

The Character Area has been further subdivided into the following precincts:

- Central Residential Area;
- Mainstreet Node;
- Harbour Mixed-Use; and
- Riverside.

### Neighbourhood Character Area

The Character Areas have been further subdivided into the following precincts:

- Old Port Credit Village Heritage Conservation District;
- Vacant Former Refinery;
- North Residential Neighbourhood;
- South Residential Neighbourhood; and
- Mainstreet Neighbourhood.

The Guide follows a similar organization, with different sections for the Community Node Character Area and Neighbourhood Character Area. Where appropriate specific direction is given for individual precincts.

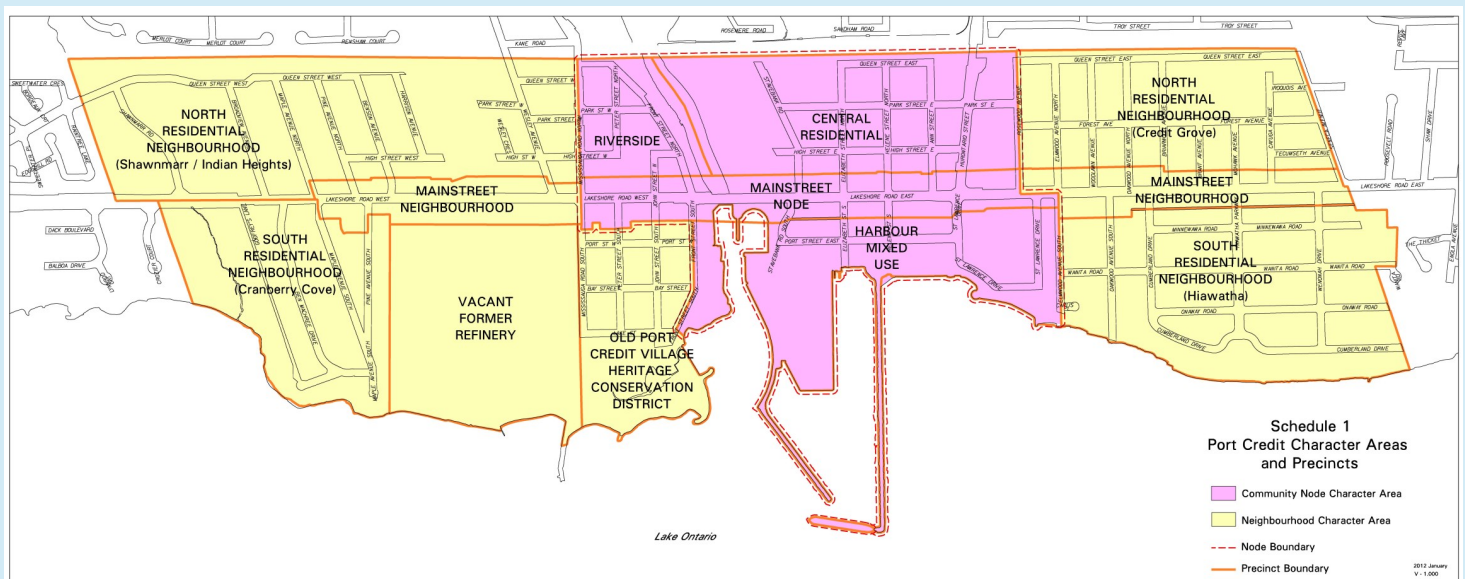


Figure A2 — Port Credit Precinct Map



# 2.0 Port Credit Community Node

## 2.0 Port Credit Community Node

The Community Node is expected to exhibit a high quality of urban design that should reinforce and enhance the identity of Port Credit as a vibrant and memorable urban place. Additional development is anticipated, however, the form and scale of the development will vary within the Node in accordance with the various precincts. The overall development of the Node shall be at a scale that reflects its role in the urban hierarchy.

The community Node is comprised of the following precincts:

- Riverside Precinct;
- Central Residential Precincts;
- Mainstreet Precinct; and,
- Harbour Mixed Use Precinct

The following provides additional context and information on each of the Community Node Precincts. Subsequent sections provided further direction on specific built form issues.

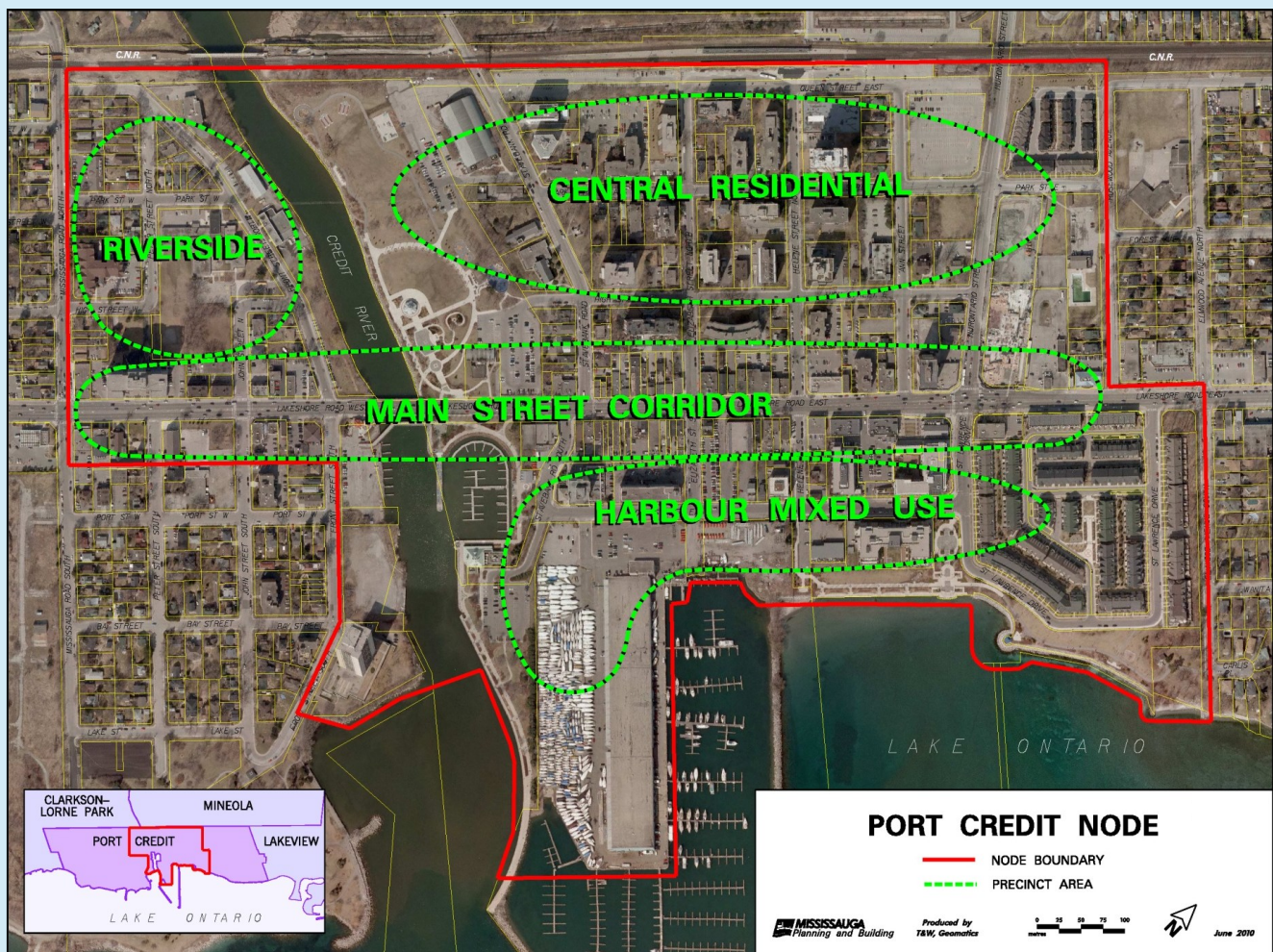


Figure B1 — Port Credit Node Precincts



## 2.0 Port Credit Community Node



**Figure B2** — Central Residential Precinct Image off of Port Street



**Figure B3** — Central Residential Precinct heritage building



**Figure B4** — Riverside Precinct Image



**Figure B5** — Central Residential Precinct Image



**Figure B6** — Main Street Corridor Precinct (Commercial Area) Image



**Figure B7** — Harbour Mixed Use Precinct Image



# 2.0 Port Credit Community Node

## 2.1 Contextual Building Heights

The Port Credit Community Node consists of a mixture of older high rise developments, newer mid-rise developments, older multi-unit walk-ups, as well as detached dwellings.

The majority of the tall buildings are concentrated within the Central Residential Precinct.

There are 33 buildings in the Node between 5 storeys and 27 storeys. The predominant character is buildings ranging from 5 to 16 storeys with taller buildings in key

locations.

There are 6 buildings in the Port Credit Node which are taller than 15 storeys:

- 16 storeys—Stavebank Road north and High Street;
- 18 storeys—West Gateway into the Port Credit Node;
- 20 storeys—West mouth of the Credit River;
- 27 storeys—at the GO Station entrance; and,
- 20 storeys and 22 storeys—located at the foot of Hurontario Street and Lakeshore Road East.

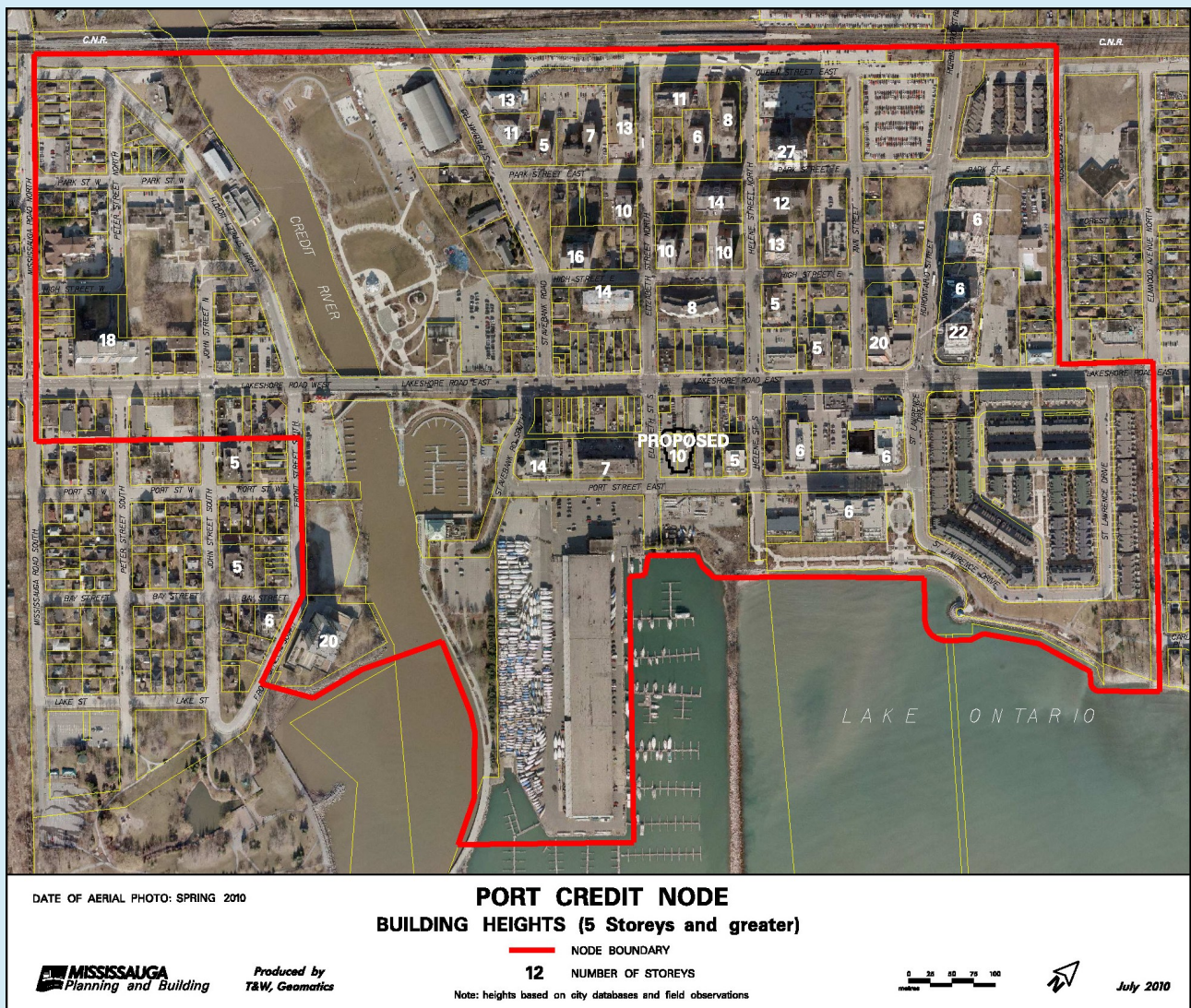


Figure B8 — Existing building heights in Port Credit



## 2.0 Port Credit Community Node

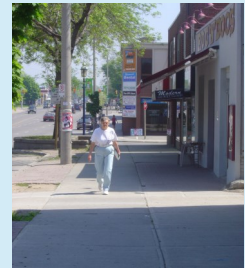
It should be noted that of these buildings, only the 22 storey structure located at the northeast corner of Hurontario Street and Lakeshore Road East was recently constructed whereas the remaining are more than 30 years old.

Approximately 30% of the properties between Hurontario Street and Stavebank Road are less than 5 storeys.

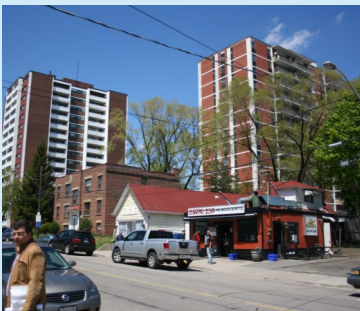
The existing setback from the street for developments within the Central Residential area are characterized by ample landscaped area and mature trees, which helps mitigate the perception of height.



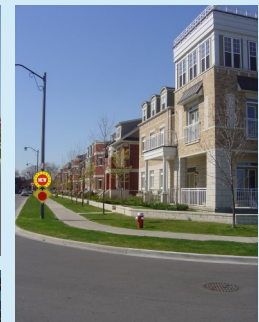
**Figure B9 — Riverside Precinct**



**Figure B10 — Main Street Corridor Precinct  
(Commercial Area)**



**Figure B11 — Central Residential Precinct**



**Figure B12 — Harbour Mixed Use**

# 2.0 Port Credit Community Node

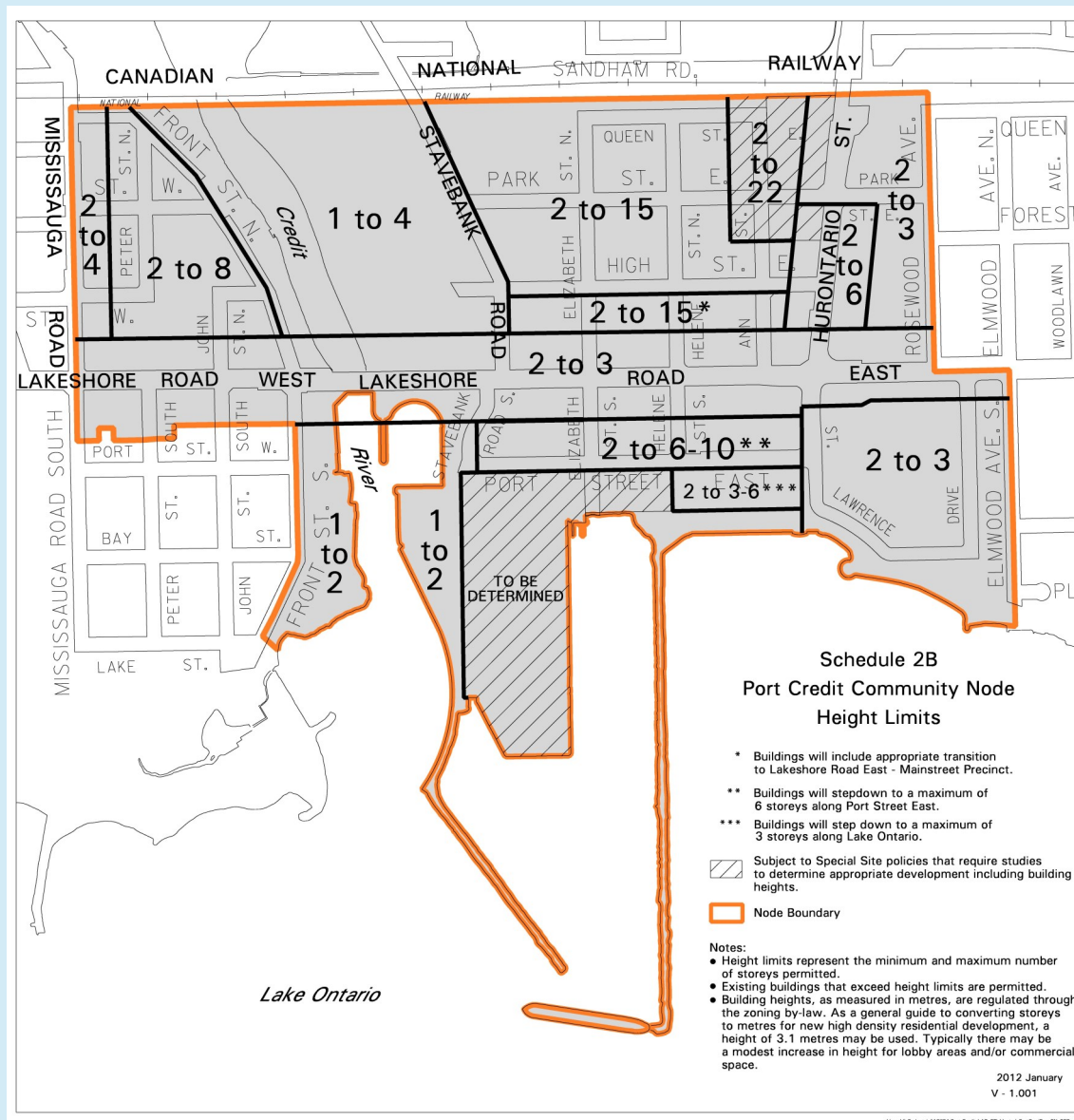
## 2.2 Planned Building Heights

Proposals for new buildings must take into account the complexities of the broader context within which they are planned, including the existing urban rhythms, local architectural language, the fine grain urban detail and the historic setting.

New buildings should make reference to their surroundings through footprint, setback, street and building alignment. Aligning tall buildings in key locations can create a strong reference point, which enriches urban legibility and aids in navigation.

The greatest heights in the Node are generally located in the Central Residential Precinct closest to the GO Transit Station. Building heights should generally slope down from the railway tracks to Lakeshore Road East. The highest buildings should be in the vicinity of the GO station and future LRT Station at Park Street and Hurontario Street and then transition downward to Lakeshore Road East and to Lake Ontario and the Credit River.

In general, buildings with the greatest heights should be limited to sites that can be used for way-finding and/or



**Figure B13 — Maxi-  
mum Building  
Heights in the Port  
Credit Node Precinct**



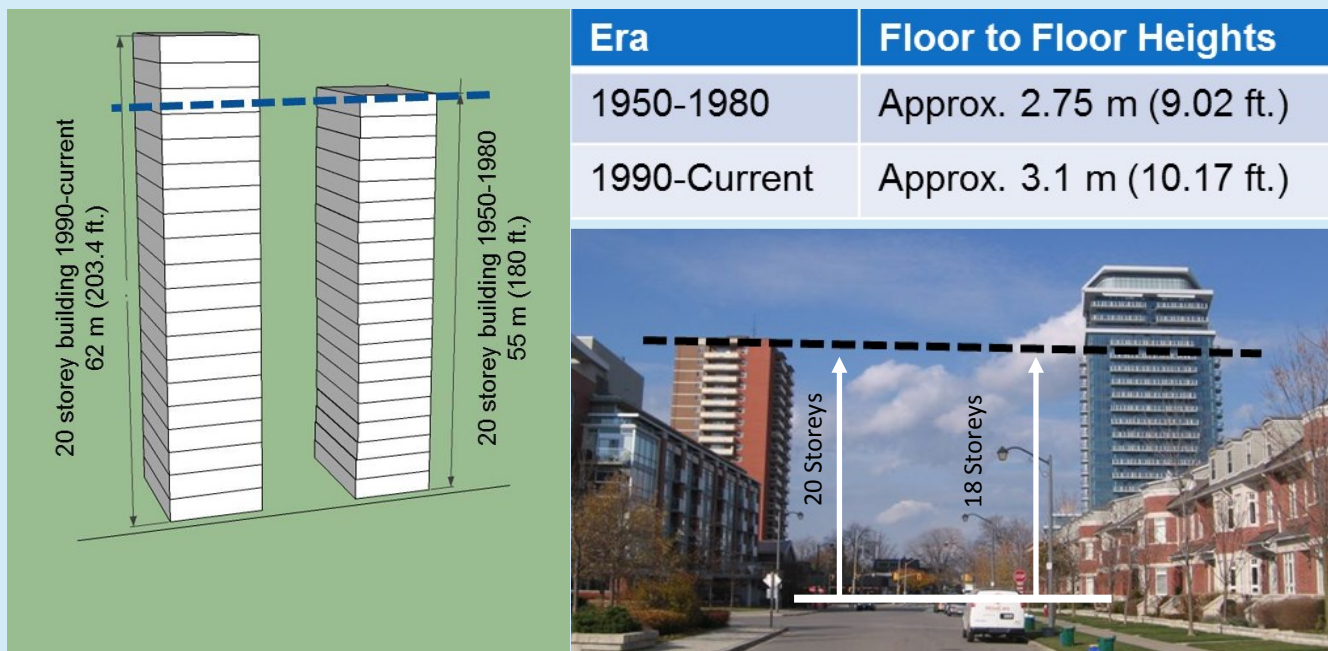
## 2.0 Port Credit Community Node

landmark locations. The maximum height in the Port Credit Community Node shall be 22 storeys which reflects existing building heights in the node and recognizes that 'Community Node' development should not be as high as "Major Nodes". As Community Nodes are intensification areas, all new buildings shall have a height of 2 storeys.

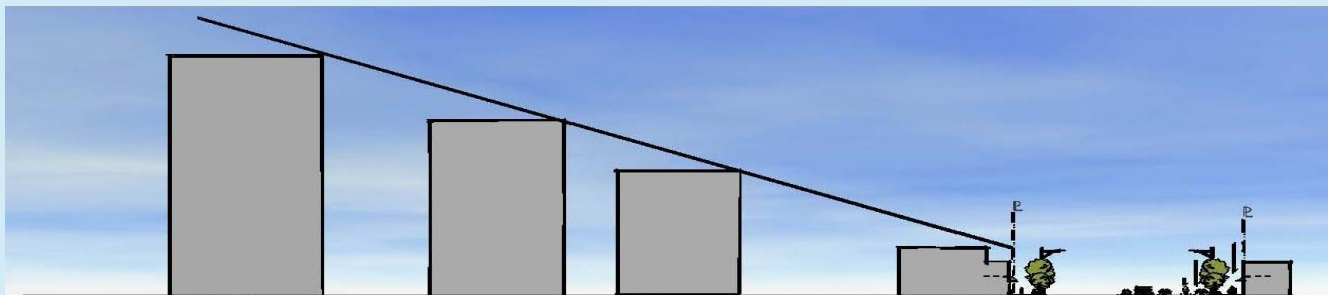
The majority of the towers in the Port Credit Node were constructed between the post war era and 1980. The dimension of the floor to ceiling heights during this era are

significantly lower than those of today (see Figure B14). Consequently, a 20 storey building today is significantly taller than a 20 storey building constructed between the 1950's and 1980's.

A 22 storey building was approved in 2007 for the northeast corner of Lakeshore Road East and Hurontario Street. This building height was determined to be appropriate given its location at a key intersection and gateway into Port Credit which will create a new visual



**Figure B14** — Differences in Building Heights. Buildings with the same number of floors are approximately 13% taller now.



**Figure B15** — Building Transition from the Railway Tracks to Lakeshore Road East and West. Buildings should also transition from Lakeshore Road East and West to Lake Ontario with the exception of key landmark locations (e.g. Intersection of Hurontario Street and Lakeshore Road East)

## 2.0 Port Credit Community Node

landmark that balances the massing of the 20 storey building just west of Hurontario Street.

In a limited number of situations, existing buildings heights exceed the maximum limits. However, the intent of the Guide and related Area Plan policies is to generally reinforce the prevailing character, as opposed to increasing the overall height of buildings in the area.

The building heights shown on Figure B13 illustrate the minimum and maximum building heights that will be considered for properties if a series of design, land use and technical objectives can be demonstrated by the applicant. Lower heights may be appropriate for specific properties. Maximum height has been applied to areas that particularly require sensitive building step-backs to achieve appropriate height transitions.

The relationship of a building size to the site area and configuration should be considered in order to avoid a

building overwhelming its site. In such cases lower heights should be used. New towers should be compatible and characteristic with the existing and proposed neighbouring structures and in terms of their contribution to the skyline.



Figure B16 — Mainstreet Precinct, Mississauga Road and Lakeshore Road West. New construction.



## 2.0 Port Credit Community Node



**Figure B17** — Building heights in the Central Residential Precinct



**Figure B18** — Port Credit Marina, Snug Harbour



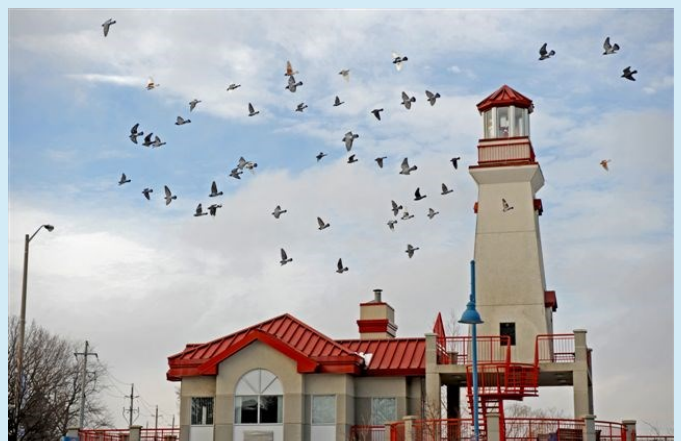
**Figure B19** — Mainstreet Precinct on Lakeshore Road West



**Figure B20** — Market Square Development, Lakeshore Road East



**Figure B21** — Port Credit Node waterfront view



**Figure B22** — Port Credit light house



# 2.0 Port Credit Community Node

## 2.3 Community Node Precincts

### 2.3.1 Riverside Precinct

This Precinct consists primarily of a residential neighbourhood. The area consists of one 18 storey apartment building, a mix of 4 storey walk up apartments, detached and semi detached dwellings, a 4 storey retirement home, a school and some low rise office and commercial uses.

This precinct should create a transition from the building heights of the Central Residential precincts to the low rise building heights in the residential neighbourhoods to the west. The heights in this area should also transition down

to the Credit River, the mainstreet precinct and to the stable neighbourhoods to the west of Mississauga Road.



Figure B23 — Image of the Riverside Precinct



Figure B24 — Image of the Riverside Precinct



Figure B25 — Image of the Riverside Precinct



Figure B26 — Image of the Riverside Precinct

## 2.0 Port Credit Community Node

### 2.3.2 Central Residential Precinct

The Central Residential Precinct has the greatest number of apartment buildings ranging from 5 to 27 storeys. Currently, three buildings are higher than 16 storeys (20, 22 and 27) and are located at the foot of Lakeshore Road East and Hurontario Street East, and across from the GO Station.

This area will have the greatest building heights in Port Credit with heights transitioning downward towards the Credit River Valley, the mainstreet precinct and stable neighbourhoods to the east as illustrated in figure B13.

The vicinity between the Go Station and the future LRT stop (on Hurontario Street) has the potential to accommodate the

greatest heights in the area and may have a more urban built form in order to provide a more conducive environment for pedestrians walking between the LRT stop and the GO Station. The specific heights, built form and land uses in this area are subject to further study.

The existing character of the area will generally be maintained including mature trees and well landscaped front yards.

Buildings adjacent to the mainstreet precinct will be required to demonstrate appropriate transition with respect to issues such as sky views, visual impact, and shadow impact.



Figure B27 — Image of the Central Residential Precinct



Figure B28 — Image of the Central Residential Precinct



Figure B29 — Image of the Central Residential Precinct



Figure B30 — Image of the Central Residential Precinct



## 2.0 Port Credit Community Node

### 2.3.3 Mainstreet Precinct

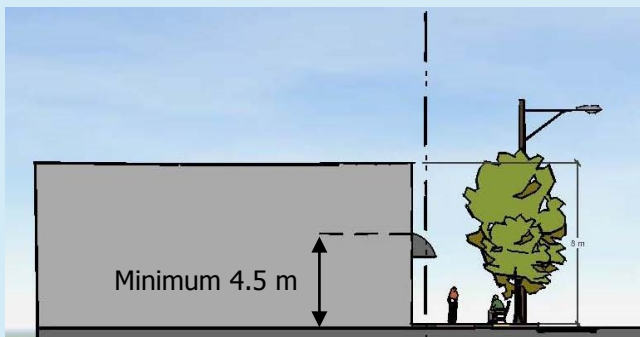
This Precinct includes part of Port Credit's traditional mainstreet, which generally extends a half block north and south of Lakeshore Road East and West. New developments along Lakeshore Road East and West shall be a minimum height of 2 storeys or a height of 7.5 m for any new construction along the Mainstreet Corridor Precinct. A maximum height of 3 storeys or 12 m is permitted as outlined in Figure B31 and B32. The second and third storey must be usable space.

The mixed use component of new development should be continuous for the first half block of Lakeshore Road East and West.

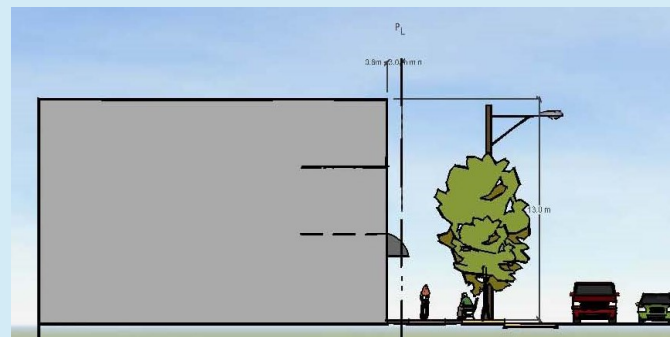
The minimum first floor height of a building along the Lakeshore Road East and West frontage shall be 4.5 m as indicated in Figure B31.

To ensure buildings and structures relate to human scale and reinforce the scale of the community:

- Built form should be closely related to, and integrated with, the street line, and with minimal building setbacks, to provide spatial enclosure and street-related activity;
- New buildings should be compatible in bulk, massing and scale of the built form to provide an integrated streetscape.
- Retail uses will be required along Lakeshore Road with direct access to the public sidewalk;
- No parking lots or areas should be provided between the building and the street line on principal street frontages, with the exception of on-street parking;



**Figure B31** — Minimum building height of 2 storeys will be permitted



**Figure B32** — Maximum building height of 3 storeys or 12 m



**Figure B33** — Conceptual 3 storey mainstreet building



**Figure B34** — Image of the existing mainstreet building

## 2.0 Port Credit Community Node

### 2.3.4 Harbour Mixed Use Precinct

- e. Blank walls must be avoided facing principal frontages and intersections;
- f. Service loading and garbage storage should be accessed from the rear or side lanes;
- g. Front building facades should be parallel with the street and provide periodic indentations for visual relief and features such as urban squares;
- h. Signage should be integrated with the scale and character of built form;
- i. Continuity of built form should exist from one property to the next with minimal gaps between buildings;
- j. Buildings adjacent to the Old Port Credit Village Heritage Conservation District should be designed so that they are sensitive to and compatible with the adjacent built form; and
- k. For Lands on the South side of Lakeshore Road, between Mississauga Road and the Credit River, *The Old Port Credit Village Heritage Conservation District Plan* applies.

This area will contain a mixture of uses and densities. Development should be lower in scale than the Central Residential Precinct and step down towards the lake. A minimum height of 2 storeys is required and a maximum height of 10 storeys stepping down to 6 on Port Street and from 6 to 3 storeys towards Lake Ontario will be permitted as outlined in Figure B13.

Mixed uses along Port Street East, Elizabeth Street South and Helene Street South are required. The main floor of all new development should be a minimum of 4.5 m in height to ensure commercial uses can be accommodated on the ground floor.

The Port Credit Harbour Marina Lands are subject to further study to determine appropriate heights, built form and land uses.



(top left) **Figure B35** — Existing development south of Port Street East.



(top right) **Figure B36** — Existing townhouse development south of Lakeshore Road East



**Figure B37** — Existing development south of Port Street East stepping down from 6 to 3 storeys to the Lake.



**Figure B38** — Existing development on Port Street East and Stavebank Road South

## 2.0 Port Credit Community Node

### 2.4 Built Form

As infill occurs it is important to ensure that the size of the building, the separation distance, the orientation of the building and the shape are considered.

#### 2.4.1 Building Floor Plates and Building Orientation

The visual impact of increased building heights and scale can be offset by limitations on floor plate sizes for taller buildings.

Constraints on floor plate size for the upper levels of buildings help to avoid bulky taller buildings and contribute to a more graceful skyline.

Floor plate size limits are not applied to the lower level of tall buildings in order to allow for greater design flexibility and to encourage continuous street wall conditions at the pedestrian level.

The size, articulation and orientation of a floor plate in a tall building is instrumental in the perception of the overall massing and visual impact of a building. The size and articulation of the floor plate of buildings over 6 storeys is key to maintaining shadow impact, loss of sky views and a potential of a wall effect from certain angles.

The use of smaller floor plates is required as they result in smaller shadows that tend to move quickly, improve sky views and permit better views between buildings.

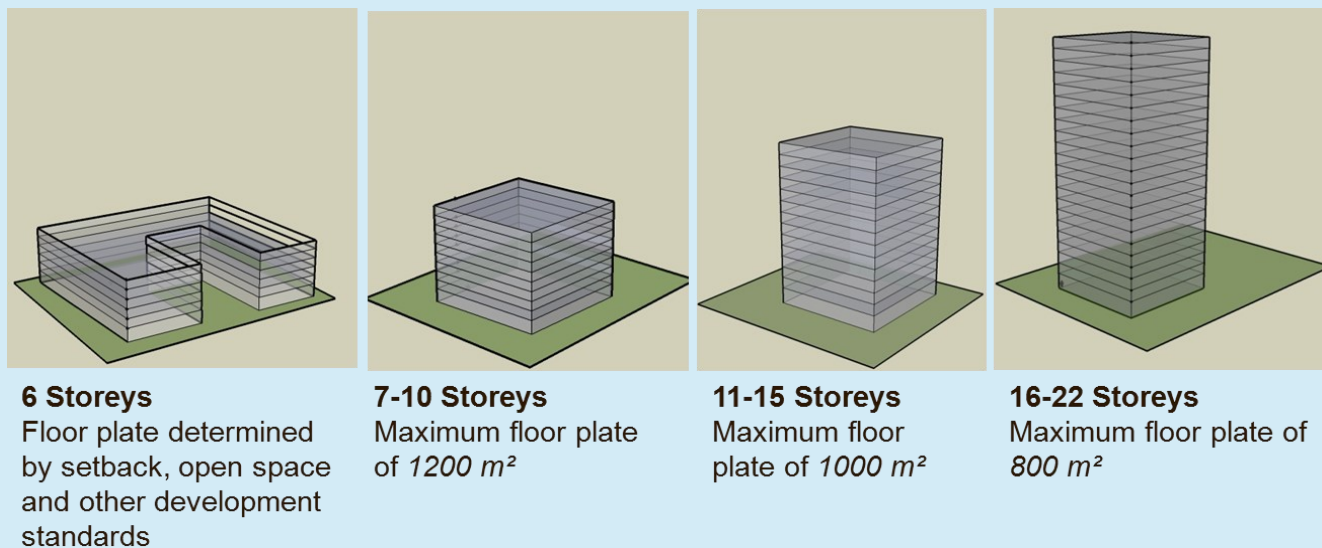


Figure B39 — Floor plate sizes for varying heights



Figure B40 — Go Transit Lands parking lot looking south. Floor plate of the Northshore building is 964 m<sup>2</sup> (as defined under GFA)



## 2.0 Port Credit Community Node

In addition, smaller floor plates promote sustainability by providing opportunity for increased daylight catchments and more efficient climate control within the building.

Buildings over 6 storeys should be designed so they are as square as possible to ensure minimal shadow impact and to ensure they do not create the visual impact of larger bulky floor plates. The maximum length of any building over 6 storeys should be 35 m including balconies for buildings under 15 storeys and 30 m for buildings over 16 storeys.

Currently the Residential Floor plates in the area range from approximately 550 m<sup>2</sup> to 1 350 m<sup>2</sup>.

The maximum floor plates of buildings over 6 storeys, inclusive of balconies, shall be:

### 7-10 Storeys:

- Maximum floor plate of 1 200 m<sup>2</sup>

### 11-15 Storeys:

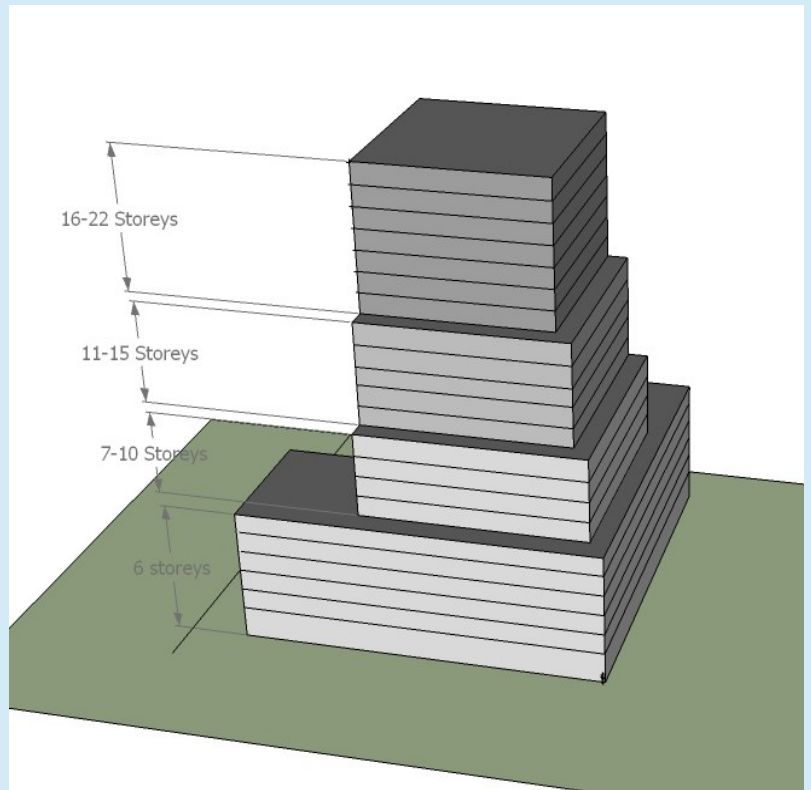
- Maximum floor plate of 1 000 m<sup>2</sup>

### 16-22 Storeys:

- Maximum floor plate of 800 m<sup>2</sup>



**Figure B41** — Existing building in the Central Residential Precinct. Floor plate of 683 m<sup>2</sup>



**Figure B42** — Combination of building floor plates and heights may be permitted subject to other development criteria being met

## 2.0 Port Credit Community Node

### 2.4.2 Building Separation Distances

The spacing between the faces of tall buildings enhances privacy, opens up views between buildings and permits access to sunlight and views of the sky.

Building separations in the Node are on average approximately 38 m for any building over 5 storeys, however many of the taller buildings are over 40 m apart.

A minimum of 35 to 40 m from any portion of a building that is over 6 storeys to another building that is over 6 storeys is required. Taller buildings require greater separation distances and therefore will be required to meet the 40 m separation distance. These separation distances will ensure that new tall buildings maintain sky views, and develop as an elegant skyline. The current eclectic mix of low and high rise buildings, a diverse housing stock and mature landscaping should be maintained.

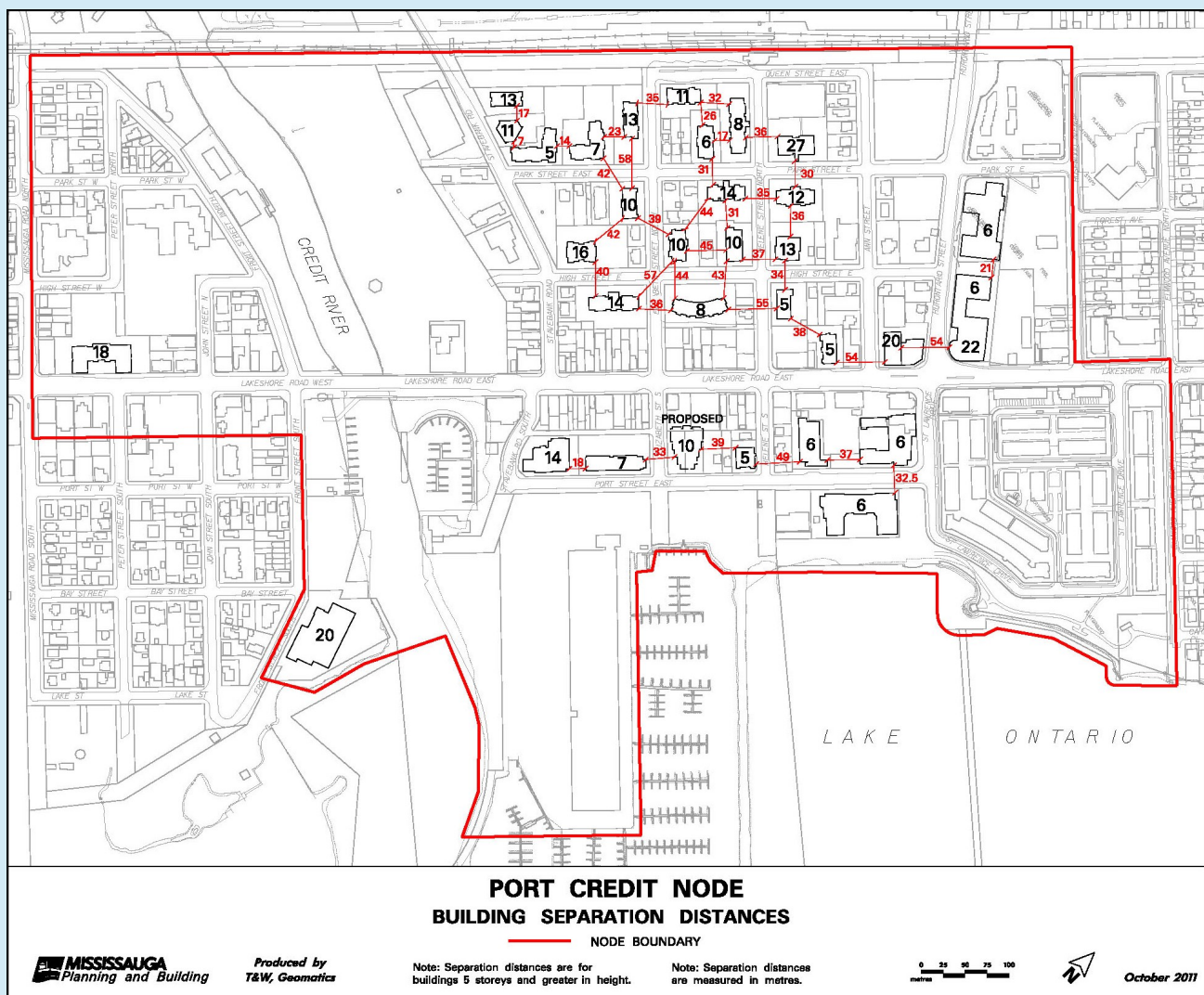


Figure B43 — Existing building separations



## 2.0 Port Credit Community Node



Figure B46 — Large existing separation distances

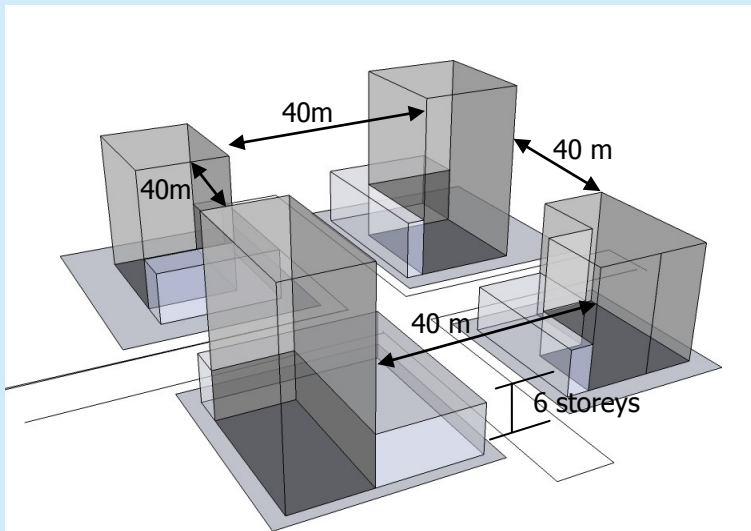


Figure B44 — Block to block separation distance



Figure B47 — Building separation distances on High Street East



Figure B48 — Building separation distances on Hurontario Street

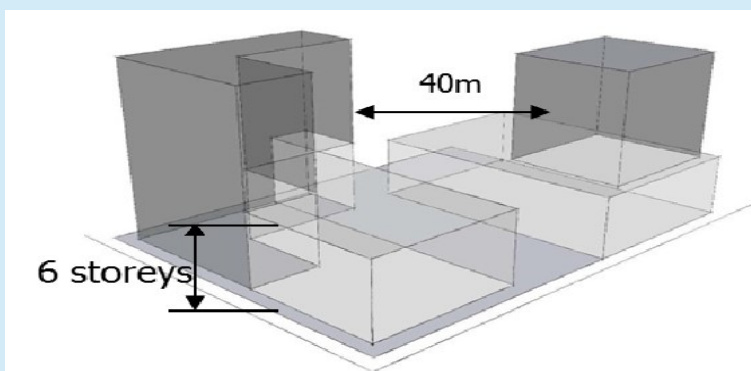


Figure B45 — Within a block separation distance



Figure B49 — Building separation distances on High Street East

## 2.0 Port Credit Community Node

### 2.4.3 Skyline

A skyline is the overall or partial view of a city's tall buildings and structures consisting of skyscrapers in front of the sky in the background. It can also be described as the artificial horizon that a city's overall structure creates. Skylines identify the city or place from a distance. Taller buildings are typically located where there is a desire for visibility and way finding.

The skyline is the finger print of Port Credit. Key sites should be highlighted in the skyline to ensure way finding. The architecture of the top of buildings should be unique and should highlight the skyline and the location of the building.

It is important to maintain the existing views to Lake Ontario and within Port Credit and ensure that future buildings maintain sky views.

The placement and orientation of new buildings should also be oriented to maximize sky views along the length of Lakeshore Road East and the West Corridor so as not to create a wall effect.

New developments will be required to demonstrate how their building fits into the context through photographic imagery.



**Figure B50** — View from Lakeshore Road West looking East



**Figure B51** — View from Lakeshore Road East looking West



**Figure B52** — View from Lake Ontario looking North at Port Credit



## 2.0 Port Credit Community

### 2.4.4 Site Size

The relationship of a site size to the size of the building and configuration should be considered in order to avoid a building overwhelming its site. In such cases, lower densities should be used. The form of buildings as a group should take precedence over the form of single buildings by virtue of considering the overall composition of the group.

Tall buildings must be set back a minimum of 10 m from side and rear property lines or the centre line of an abutting lane, measured from an external wall or exterior face of balconies to ensure maximum opportunity for fenestration and to ensure appropriate separation distances can be accomplished.

Sites that are too small to permit a tower with the required setbacks on all sides are not appropriate for tall buildings. Small sites are generally considered to be 45 m by 45 m for mid block and 40 m by 45 m for corner lots.

A building on a small site may only be constructed to 6 storeys above which a 45 degree angular plane may be used for additional levels set back from the street and the side and rear property lines.



**Figure B53** — *Maintaining sky views are essential in Port Credit*



**Figure B54** — *Smaller sites should only construct smaller buildings*

## 2.0 Port Credit Community Node

### 2.4.5 Microclimate

Tall buildings over 10.7 m can adversely effect the environmental quality of surrounding areas through the diversion of high speed winds and through the overshadowing of the adjacent public realm and residential buildings, including public/private amenity spaces.

The impact of both of these elements can be mitigated through good design and sensitive siting. The impact of shadows at different times of the day and throughout the year will need to be assessed. The use of architectural devices such as screens, terraces, awnings and also façade setbacks can be adopted to minimize the effects of high speed wind at the base of buildings and of shadow effects.

Individual proposals should seek to create well oriented and lively spaces that contribute positively to the wider public realm.

#### 2.4.5.1 Shadow Impacts

Shadow studies will be requested in support of development applications to demonstrate that the height and/or location of a proposed building will not generate excessive shadows over adjacent lands or the public realm.

Shadow studies will be required for buildings greater than 10.7 m in height which may cause a new shadow impact on adjacent residential properties, properties identified as listed or designated on the Heritage register, and public parkland, open space and the public realm. Particular attention will be focused on Lakeshore Road East and West and Hurontario Street where a comfortable pedestrian environment is strongly encouraged.

Development applications are to adhere to the City's Standards for Shadow Studies.

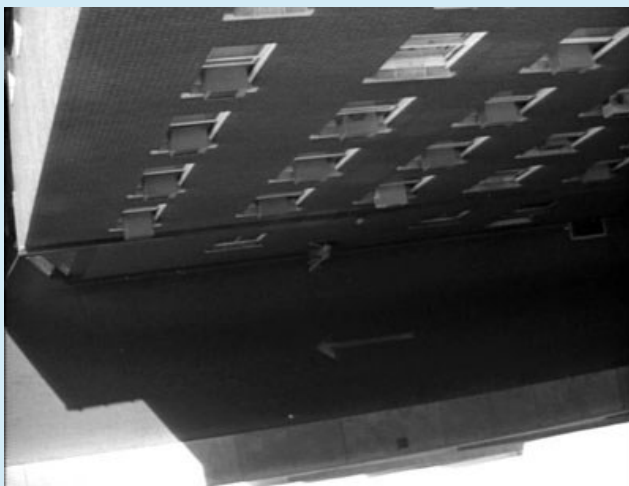


Figure B55 — Shadow on the public realm

The following dates and times will be required for shadow studies:

- March/September 21st from 9:12 a.m. to 6:12 p.m. on an hourly basis
- June 21st—8:12 a.m. to 8:12 p.m. on an hourly basis
- December 21st 9:12 a.m. to 4:12 p.m. on an hourly basis

## 2.0 Port Credit Community Node

### 2.4.5.2 Wind Comfort

The objective of the study is to ensure appropriate comfort and safety levels are maintained in the pedestrian realm, streetscapes, public spaces and areas immediately adjacent to and/or surrounding the proposed development.

The study may be required on development applications higher than 3 storeys or 16 m. The criteria to be used for the analysis should be signed and sealed by a certified engineer. The *Terms of Reference for Pedestrian Wind Comfort Studies* should be used.

The evaluation of the existing wind conditions in the immediate and surrounding area, prior to the proposed development will be required along with a comparison of the wind conditions based on the proposed development.

The evaluation will include conditions based on seasonal variations (i.e. summer, spring, fall and winter). The intended use of the area will be considered to determine the appropriate wind conditions that will be permitted.

Areas will be considered appropriate for their intended use if microclimate/wind conditions are satisfied 80%.

Categories will be classified by their intended use and appropriate impact:

- i) **Sitting - 0 to 10 km/h:** reading and seating areas, outdoor cafés.
- ii) **Standing - 0 to 14 km/h:** passive areas, building entrances, short term seating.
- iii) **Walking - 0 to 19 km/h:** areas with pedestrian movement, sidewalks, street frontages.
- iv) **Uncomfortable - greater than 19 km/h:** areas with little pedestrian activity.

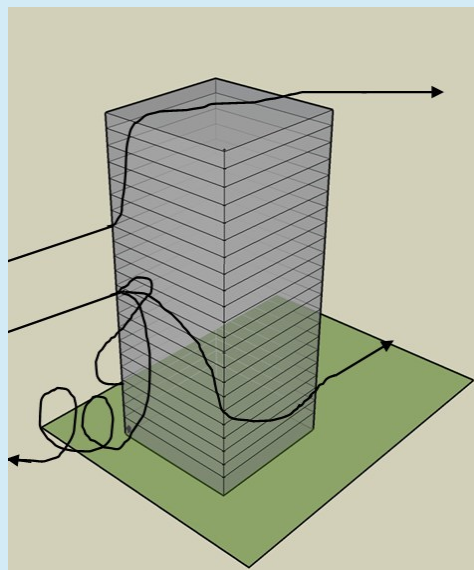


Figure B56 — Potential wind conditions on a tall building without a podium

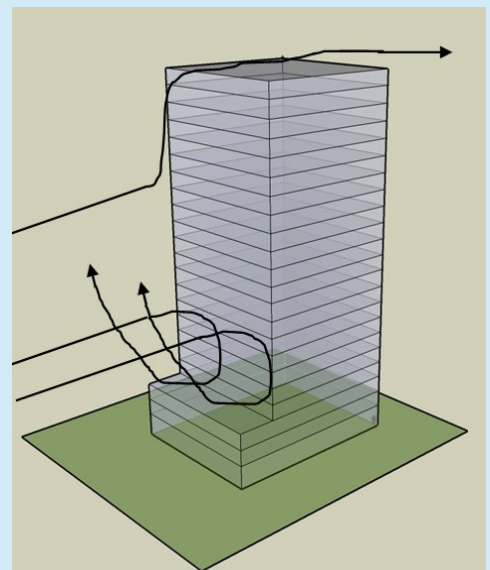


Figure B57 — Potential Wind conditions on a tall building with a podium



## 2.0 Port Credit Community Node

### 2.4.6 At Grade Commercial Requirements

Ground floor retail uses concentrated at important areas within the Node are vital to the function of Port Credit. Retail primarily exists along Lakeshore Road East and West with some smaller scale retail extending on side streets off of Lakeshore Road East and West. Retail along Hurontario Street is also an important continuation of the mixed use area.

Commercial uses will be required along Lakeshore Road East/ West; along Hurontario Street; in proximity to the GO Transit Station where it is an essential component of transit oriented development; along Port Street; and along the routes that connect them as indicated in Figure B58 as areas outlined in blue.

Areas outlined in red are streets where retail is encouraged but not required and are considered transitional streets.

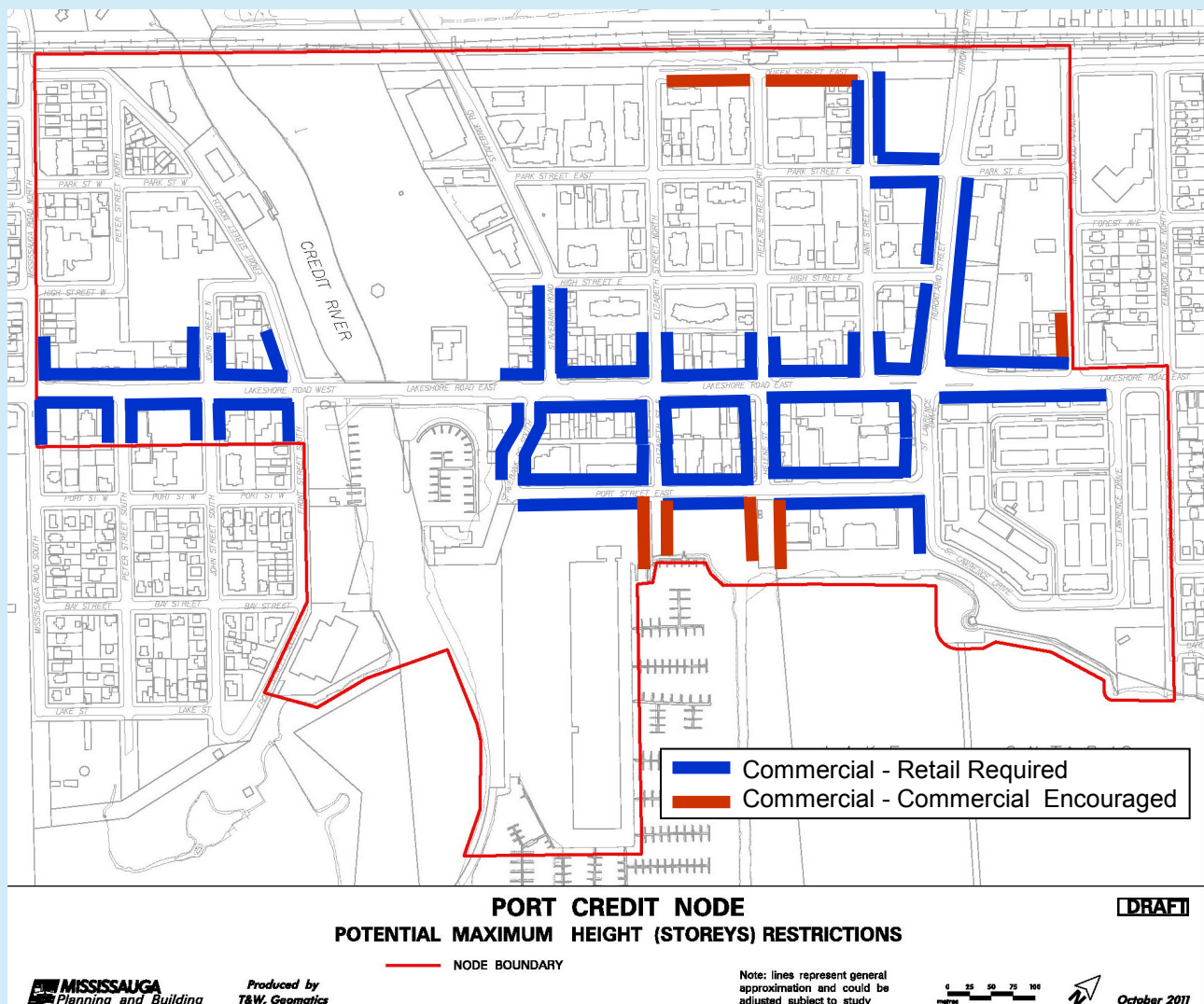


Figure B58 — At grade uses in the Port Credit Node

## 2.0 Port Credit Community Node

### Retail Guidelines:

- a. A main front usable door shall face Lakeshore Road East/ West;
- b. Generally retail areas require a minimum of 4.5 m (15 ft.) of clear height from grade and a minimum of 15 m (50 ft.) of frontage;
- c. A minimum of 75% glazing is required for retail storefronts along the street wall;
- d. A minimum 6 m (20 ft.) store front extension around the corner from a primary street where retail is required;
- e. Retail tenant signs shall be designed of high quality material, colour and scale in keeping with the design of the building;
- f. Fascia signs shall be limited to the first floor level;
- g. Tenant signage shall be of a consistent design if there is more than one tenant;
- h. Ground signs are prohibited;
- i. Store front window signage is permitted up to 25% of the glass surface area and shall not block clear views of exits or entrances and shall maintain visibility into the interior of the premises at all times; and,
- j. Tables and other active uses adjacent to storefront windows are encouraged where permitted.



Figure B59 – Image examples of Mainstreet conditions

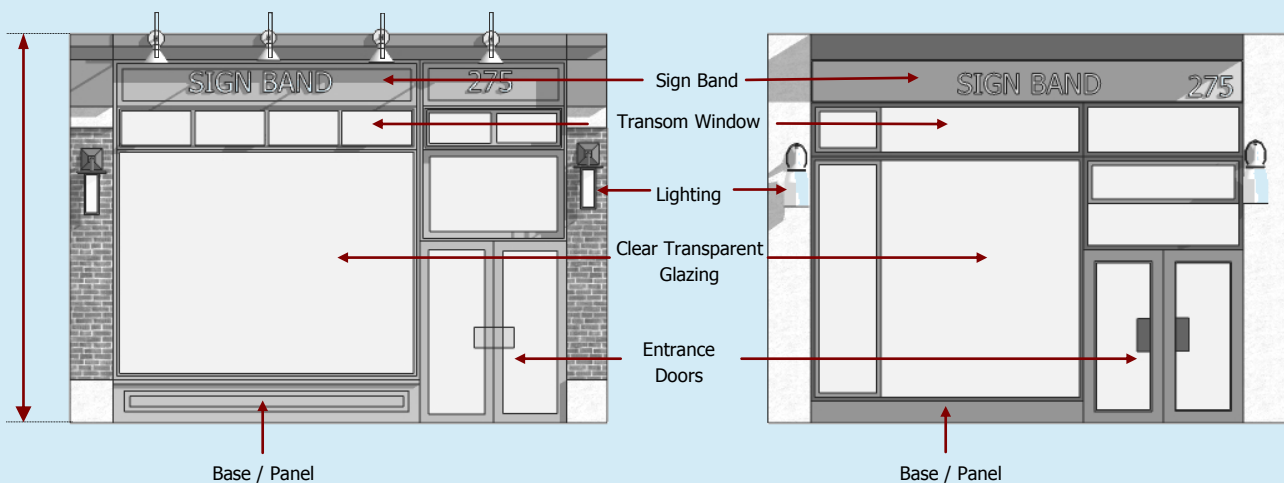


Figure B60 – Illustration of Mainstreet retail

# 2.0 Port Credit Community Node

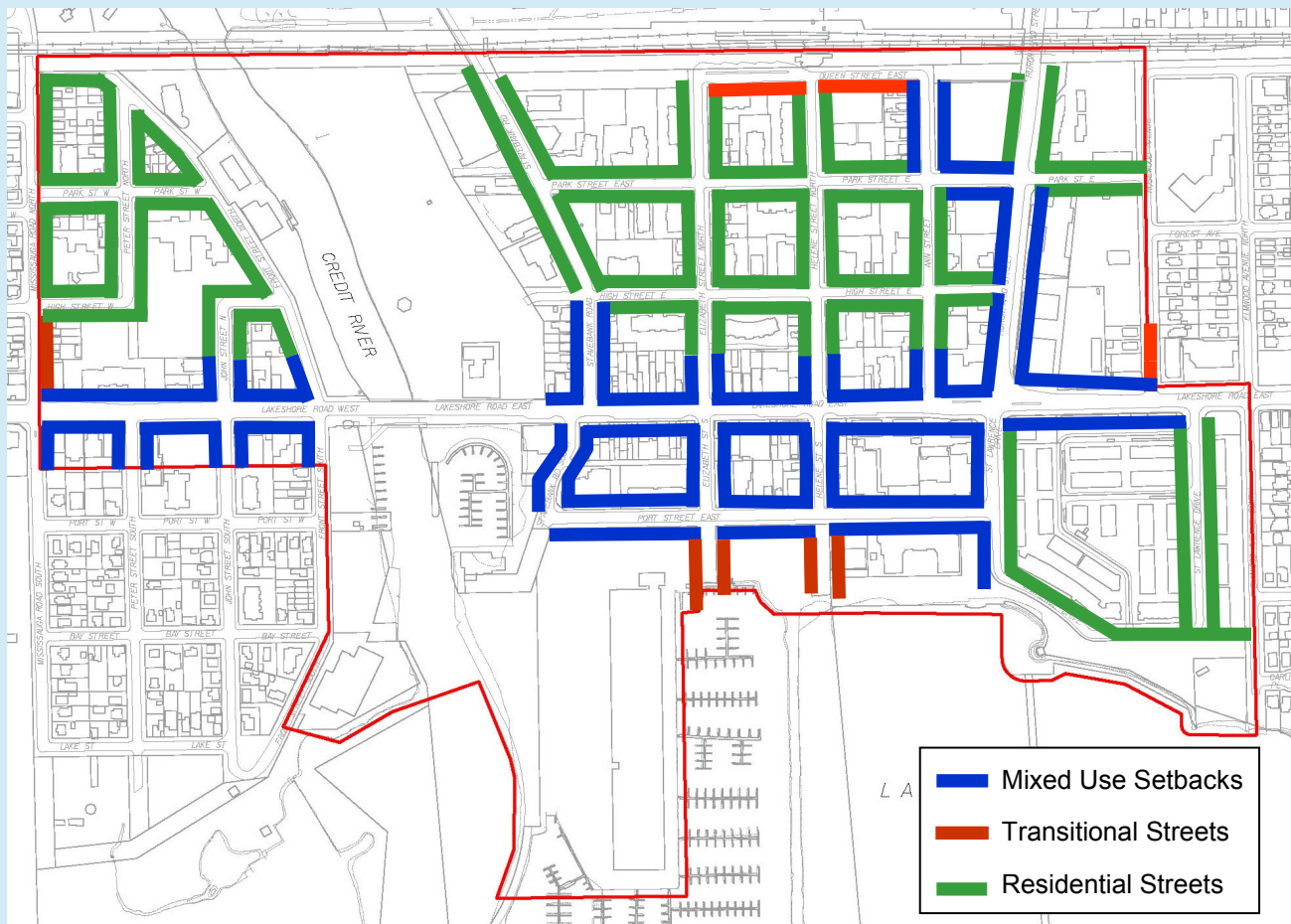
## 2.4.7 Building Setback

### Mixed Use Setbacks

Setbacks on streets where retail is required should generally be 0.0 m to 3.0 m from the property line. The exact location of new buildings will be determined once the dimension of the public realm has been achieved. New development must ensure that a minimum of 5.6 m public realm from the sidewalk/street curb to the face of the building can be accommodated to ensure appropriate streetscape treatment can be achieved.

### Transitional Streets

On transitional streets that are shown as red on Figure B61, commercial uses will be encouraged however not required. Buildings should be designed so that they may be converted to commercial uses when market conditions allow.



**PORT CREDIT NODE**  
**Building Setbacks**

MISSISSAUGA  
Planning and Building

Produced by  
T&W, Geomatics

— NODE BOUNDARY

Note: lines represent general  
approximation and could be  
adjusted subject to study

0 25 50 75 100  
metres



October 2011

Figure B61 – Building Setback Requirements



## 2.0 Port Credit Community Node

### *Residential Street Setbacks*

On residential streets (shown on Figure B65 as green) the setback to a building shall be a minimum of 4.5 m and a maximum of 7.0 m depending on the character of the adjacent developments and the configuration of the proposed building. The setback should ensure that there is ample appropriate landscape treatment to fit in with the existing character of the community.

Sidewalk curb to the face of the building



**Figure B64** — Sidewalk curb to face of the building for new developments will be 5.6 m



**Figure B62** — Mainstreet setback of 0.6 m to 3.0 m. Setback varies depending on the size of the public realm



**Figure B65** — Residential setbacks should be well landscaped. Setbacks should be a minimum of 4.5 m and a maximum of 7.0 m



**Figure B63** — Residential setbacks should be well landscaped. Setbacks should be a minimum of 4.5 m and a maximum of 7.0 m



**Figure B66** — Transitional areas such as Helene Street South

## 2.0 Port Credit Community Node

### 2.4.8 Building Frontages

#### *Mixed Use Streets*

Port Credit consists of closely spaced mainstreet buildings with no interruptions in the sidewalk. This ensures a safe pedestrian environment.

Buildings fronting onto streets that are required to have retail should be closely spaced with no driveway access points. Front doors shall face Lakeshore Road. A minimum

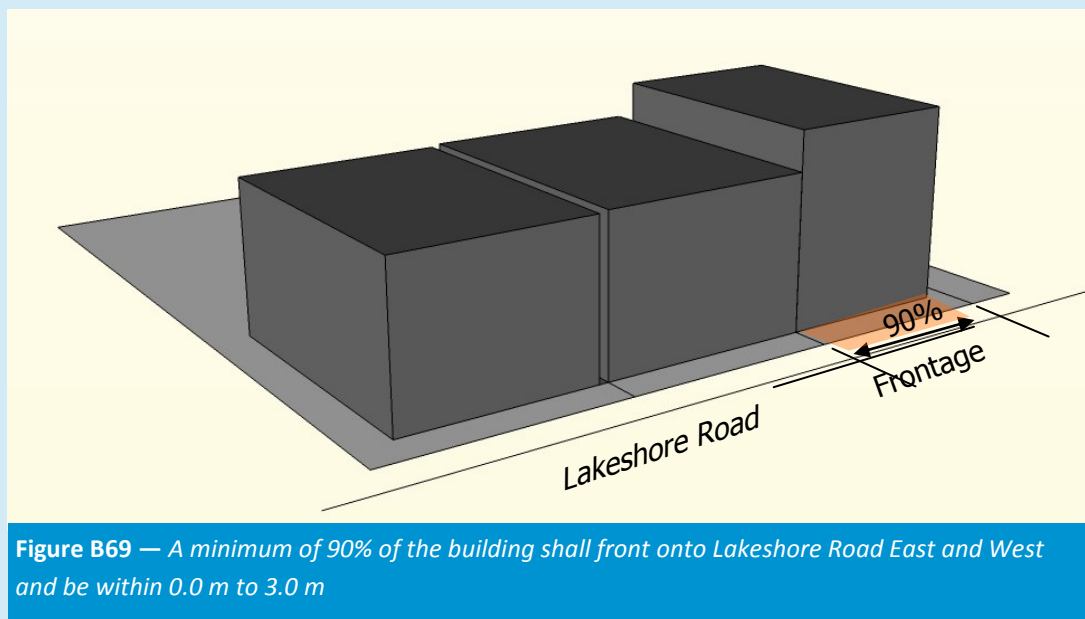
of 90% of the building face shall front onto Lakeshore Road and be within 0.6 m to 3.0 m of the front property line.



**Figure B67** — Mainstreet building frontages



**Figure B68** — 90% of the building frontage is 0.6 m to 3.0 m



**Figure B69** — A minimum of 90% of the building shall front onto Lakeshore Road East and West and be within 0.0 m to 3.0 m



## 2.0 Port Credit Community Node

### Transitional Streets

Buildings fronting onto transitional streets should be closely spaced with minimal driveway access points. Front doors shall face the street. A minimum of 70% of the building face shall front onto the street and be within 0.0 m—3.0 m of the front property line.

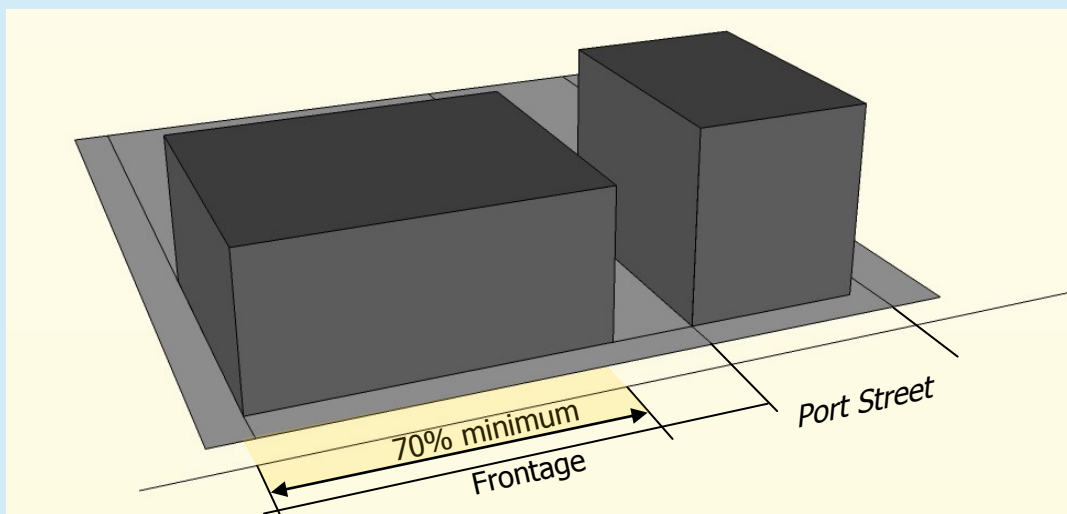
If residential units are proposed at grade, the building should be set back 4.5 m to 7.5 m.



**Figure B70** — 70% of the frontages along Port Street East shall be 0.6 m to 3.0 m if no residential uses are proposed



**Figure B71** — 70% of the building frontage should be 4.5 m to 7.5 m if residential units are proposed



**Figure B72** — A minimum of 70% of the building shall front onto Port Street East and be within 0.6 m to 3.0 m

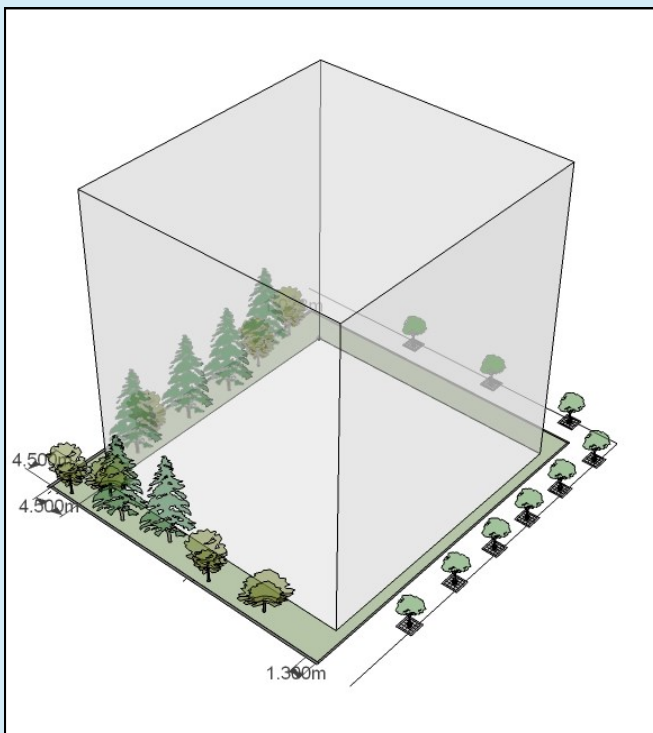
## 2.0 Port Credit Community Node

### 2.4.9 Landscape Area

The Port Credit Node precincts, particularly the Central Residential Precinct and the Riverside Precinct are characterized by well landscaped front yards and mature trees. The landscape area is defined as any outdoor area on a lot, located at grade, including the landscaped buffer area. It is suitable for the growth and maintenance of grass, flowers, shrubs, trees and other landscape features, and may include walkways, berms, retaining walls and outdoor amenity areas.

However, it shall not include driveways, aisles, ramps or internal roads, parking areas whether surfaced or not, curbs, any open space beneath or within any building, structure or part thereof, or any exterior garbage storage or handling area.

A minimum of 30 % landscape area is required for all sites within the Central Residential Precinct, the Riverside Precinct and the Harbour Mixed Use Precinct. This is to ensure that all lots can achieve a minimum buffer to adjacent uses; that the existing context which contains well landscaped front yards, particularly on high density sites, can be achieved; to ensure sustainable measures can be attained so fenestration can be achieved; and that overdevelopment of sites does not occur. It will also help protect views to Lake Ontario through future development of sites along the waterfront.



**Figure B73** — 30% landscape area for a small lot on a 40 m x 45 m lot size produces the minimum landscape buffer requirement of 4.5 m on the rear and side lot line. It does not maintain the required front and exterior side yard back from the front property line.



**Figure B74** — Image of the Riverside Precinct



**Figure B75** — Image of the Central Residential Precinct

## 2.0 Port Credit Community Node

At a minimum, the landscape area should achieve a row of coniferous trees with shrubs and/or perennial plantings to provide a natural visual buffer between the two uses.

In the Mainstreet Precinct, where development is intended to create a compact commercial mainstreet, no minimum landscape area will be required, however, a minimum 4.5 m landscape buffer is required when a mixed use zone abuts a residential zone.

A landscape buffer is defined as a continuous, open, unobstructed width of land substantially parallel to and adjoining a lot line that is intended for the growth and maintenance of plant material including trees, shrubs and other landscape features such as retaining walls.



**Figure B76** — Image of the Mainstreet Corridor Precinct



**Figure B77** — Image of the Harbour Mixed Use Precinct



**Figure B78** — Image of the Central Residential Precinct



**Figure B79** — Image of the Harbour Mixed Use Precinct



## 2.0 Port Credit Community Node

### 2.4.10 Pedestrian Realm/ Streetscape

The public realm is an integral part of any site development. The relationship between the buildings, site layout and elements within the public realm has a great impact on the urban form and the experience of those who live, work and play in Port Credit.

New developments should enhance public streets and the open space system by creating a desirable street edge condition that is ideal for the use of pedestrians.

Different streets will have different streetscape requirements depending on their uses. Sidewalk width and content should relate to its function. For example, streets that include public transit should have wider sidewalks and room for street furniture related to the bus stops.

Parking garage ventilation should not be located at grade along any street frontages and should be integrated into the façade of the building.

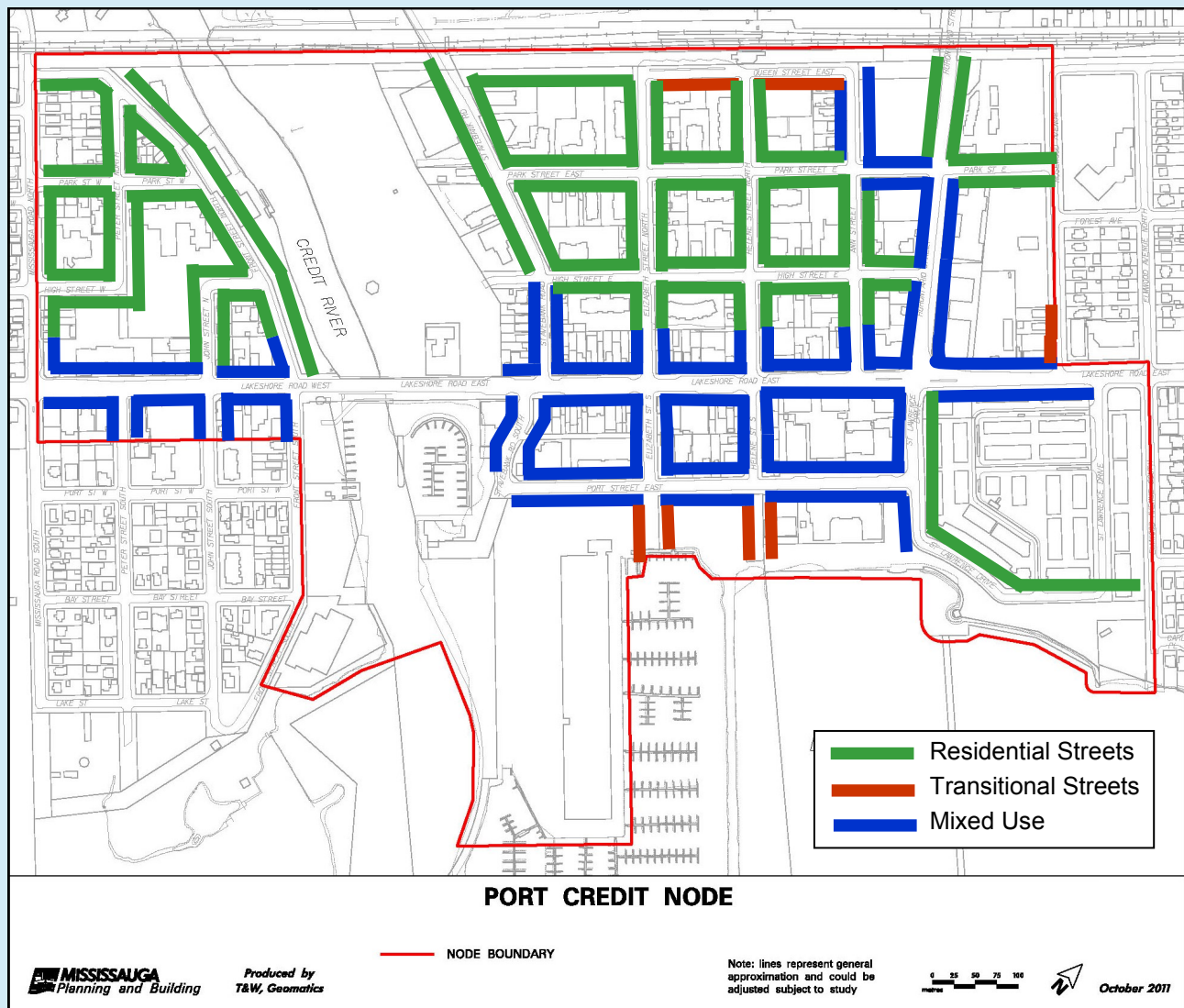


Figure B80 — Image of the pedestrian realm/streetscape

## 2.0 Port Credit Community Node

### Mixed Use

The vitality of the Node depends on the health of the mixed use and its local business. Lakeshore Road East and West require streetscape improvements which will help to revitalize the most important streets in Port Credit.

Additional requirements for building setbacks may be required to achieve the ideal pedestrian experience within the streetscape corridor.

A Master Streetscape Study is needed for Lakeshore Road East

and West, Hurontario Street, Port Street and the streets connecting these locations.

Utilities such as overhead wires and underground cables are important uses that occur within the road right of way. Careful consideration will be given when planning streetscape improvements such as tree planting, to ensure compatibility with utilities. The design of utility infrastructure should reflect best practices in design, while ensuring sufficient technical resilience to provide for increasing number and quality of service demanded by the public.



Figure B81 — Conceptual mainstreet streetscape

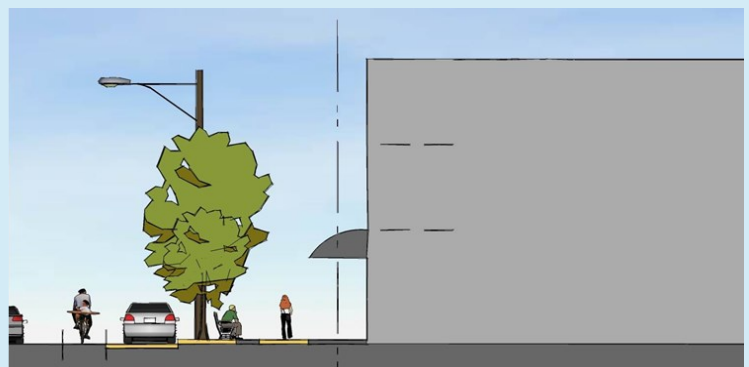


Figure B82 — Conceptual mainstreet streetscape



Figure B83 — Lakeshore Road West older streetscape treatment



Figure B84 — Lakeshore Road East recent development and streetscape treatment



## 2.0 Port Credit Community Node

### *Residential Streets*

Residential streets are those that have residential uses at grade. Residential streets are characterized by generous setbacks, upgraded tree planting and landscape treatment.

Residential streets should continue to maintain these characteristics. The pedestrian realm will include a sidewalk on the street edge where appropriate with tree planting and grassed areas on the interior of the sidewalk.

New development will ensure that existing trees are preserved, maintained and enhanced.

Buildings will generally be set back 4.5 m to 7.0 m to ensure an adequate landscape treatment can be accommodated at grade.



**Figure B85** — Hurontario Street  
South East side



**Figure B86** — Riverside Precinct



**Figure B87** — Riverside Precinct



**Figure B88** — High Street looking West

## 2.0 Port Credit Community Node

### *Transitional Streets*

Transitional streets are those that may one day be used as commercial, retail or employment streets as Port Credit evolves into a more compact village.

These areas should be developed with flexibility. Buildings should be located within the setback of 0.0 m to 3.0 m for commercial retail and 4.5 m to 7.0 m for at grade residential uses. The pedestrian realm and streetscape should be continuous and provide a wider pedestrian realm to ensure flexibility. Street trees and coordinated furniture including post and ring bike parking are a requirement.



Figure B89 — Port Street East



Figure B90 — Ann Street



Figure B91 — Port Street East



Figure B92 — Hurontario Street



## 2.0 Port Credit Community Node

### 2.4.11 Parking, Loading and Service Areas

The design of parking, servicing and loading areas are a key component in the development of sites. These areas serve a functional need, but should be designed in a manner that screens less desirable aspects and provides high quality treatment of exposed areas while addressing safe and efficient movement of pedestrians, cyclists and vehicles.

Service, loading and garbage storage areas should be integrated into the buildings, located interior to the building or alternatively at the rear of the building and screened from the public realm and adjacent residential uses. Parking should be located underground, internal to the building or to the rear of buildings.

Above grade parking facilities will be designed to be compatible with the surrounding character through the use of architectural elements that fit with the scale, style and streetscape of the community. Consideration should be given to active pedestrian related uses on the ground floor of the structure in order to improve the animation of street edge conditions. Above grade parking structures should be designed in such a manner that they are integrated into the development. Vehicles should not be visible to the public realm.



Figure B93 — Laneway south of Lakeshore Road East



Figure B94 — Laneway south of Lakeshore Road East



Figure B95 — Laneway north of Lakeshore Road East

## 2.0 Port Credit Community Node

### 2.4.12 Roof Top Mechanical Penthouse Units

All rooftop units should be internal to the buildings and hidden from public view.

All mechanical penthouses should be designed and clad with materials to compliment the building façade.

The portion of the roof not utilized as mechanical penthouse should be developed as green roofs and/or useable outdoor amenity space.



Figure B96 — Green Roofs on buildings



Figure B97 — Example of roof top mechanical that has been integrated and designed into the building



Figure B98 —  
Green Roof Example



## 2.0 Port Credit Community Node

### 2.4.13 Architectural Expression and Materiality

New buildings should reference their physical, cultural and historic surroundings through their architectural language and high quality materials. Materials should show sensitivity to their surroundings and should aim to be of the highest quality, responding directly to the existing urban fabric, whether by utilizing similar or sympathetic materials or by positive contrast.

Materials should be chosen with regard to their performance in sustainable terms. The use of local or recycled materials, and/or materials from renewable resources is encouraged.

High quality materials will be required in all new developments. The material most widely found in Port Credit is red tone brick. New developments are encouraged to incorporate this material into their development.

For the mainstreet, vision glass should be used for all store

fronts. No spandrel glass will be permitted along Lakeshore Road.

Materials that are not dominant in Port Credit are discouraged as the dominant feature of any new development. These materials include architectural concrete block, stucco, spandrel glass and EIFS panels. These materials will not be permitted below the 4th storey. Concrete block is not permitted to be exposed.

Building scale should be broken down through the use of stepping, projections, canopies, trellises, changes in scale, fenestration patterns, materials and finishes.

The private space that extends from the building face to the public right-of-way must be designed in a such a way that it seamlessly blends with the design of the public realm.

Future sidewalk/boulevard treatment including site elements such as seating and lighting, should match that of the public right-of-way in order to blur the line between public and private realm.



Figure B99 — Preferred materials to be exposed: brown/red brick

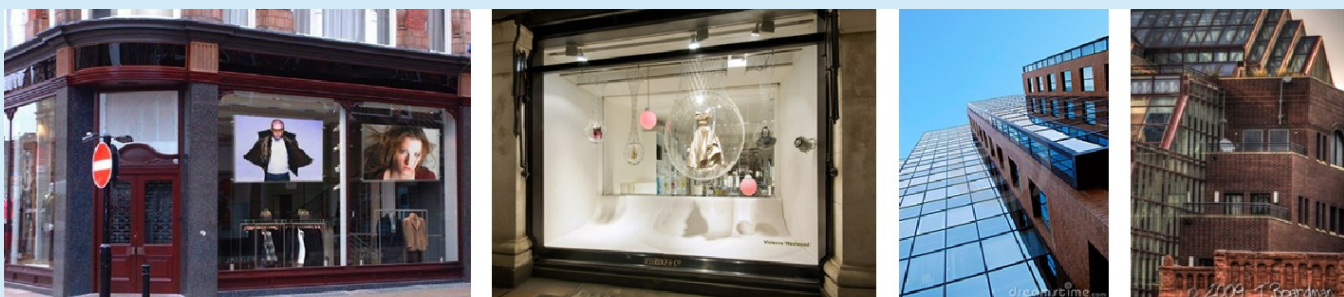


Figure B100 — Vision Glass/Brick Combination



## 2.0 Port Credit Community Node

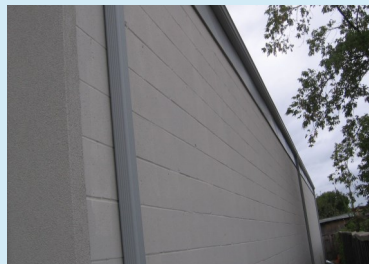


**Figure 101** — Balconies should be designed so that they are part of the building rather than added onto the building as an afterthought.

Glass balconies should have 50% tinting to ensure items on the balcony are not fully exposed



**Figure B102** — Prohibited to be exposed: Concrete Block



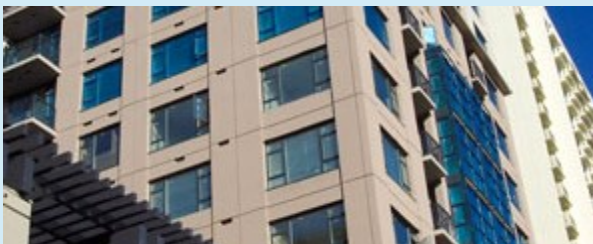
**Figure B103** — Prohibited to be exposed: Painted Concrete Block



**Figure B104** — Discouraged materials: Architectural Concrete Block



**Figure B105** — Discouraged materials: Stucco (synthetic or natural)



**Figure B106** — EIFS Panels for more than 20% of the building material and should not be placed within the first 2 storeys



**Figure B107** — Glass balconies should be 50% tinted



## 2.0 Port Credit Community Node

### 2.5 Scenic Routes, Key Sites and Views

Currently, the Port Credit community has physical and visual access to the Lake Ontario and the Credit River. As development occurs, views to the water should be preserved and enhanced as they are major contributors to the community's character and pride.

Public views of important natural or man-made features along streets and scenic routes need to be protected as they add value to the built form and contribute to neighbourhood identity.

When new development occurs, it must maintain, and in some cases, enhance these views and vistas to prominent features.

Sites with prominence, high visibility and access should be considered as a priority for civic buildings and community infrastructure.

Special care will be taken with development along Scenic Routes to preserve and complement the scenic historic character of the street. Scenic Routes and views identified in Figure B108 should be preserved and enhanced.

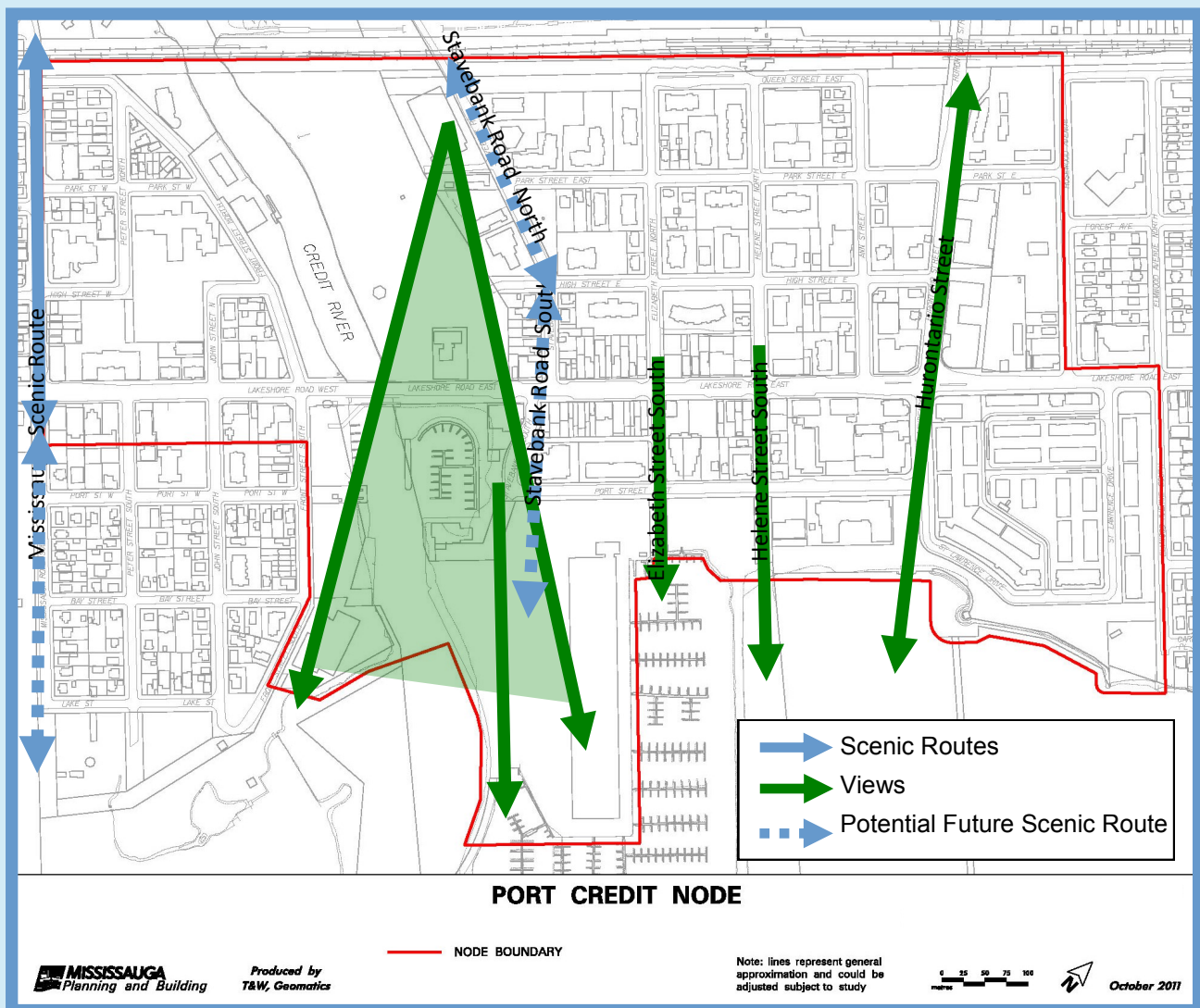


Figure B108 — Scenic routes and views



## 2.0 Port Credit Community Node

### Mississauga Road Scenic Route



Lands abutting the Mississauga Road right-of-way between the St. Lawrence and Hudson Railway and Lakeshore Road West (frontage, flanked and rear yards) are designated a scenic route and will be subject to special policies as outlined the Mississauga Official Plan.

### Potential Future Scenic Routes



Stavebank Road North and South from the Railway tracks to the Port Credit Pier and Mississauga Road South have been identified as potential future Scenic routes that will be required to be studied further through separate review.

### Views



Port Credit has many natural views to Lake Ontario. These views shall be enhanced and maintained. There are a number of views that still need to be developed. These include the extension of Elizabeth Street South and Helene Street South. Streetscapes along these streets should enhance the views to Lake Ontario. Architectural interest should be heightened on these corridors.

In addition to the views to Lake Ontario Port Credit is visually connected to Downtown Mississauga. Views to prominent Architecture “The Marilyn” in the Absolute project should be maintained and enhanced along Hurontario Street.



Figure B109 — Mississauga Road Scenic Route



Figure B110 — Hurontario Street looking North, “Marilyn Building” in the background



Figure B111 — View from Lake Ontario

# 2.0 Port Credit Community Node

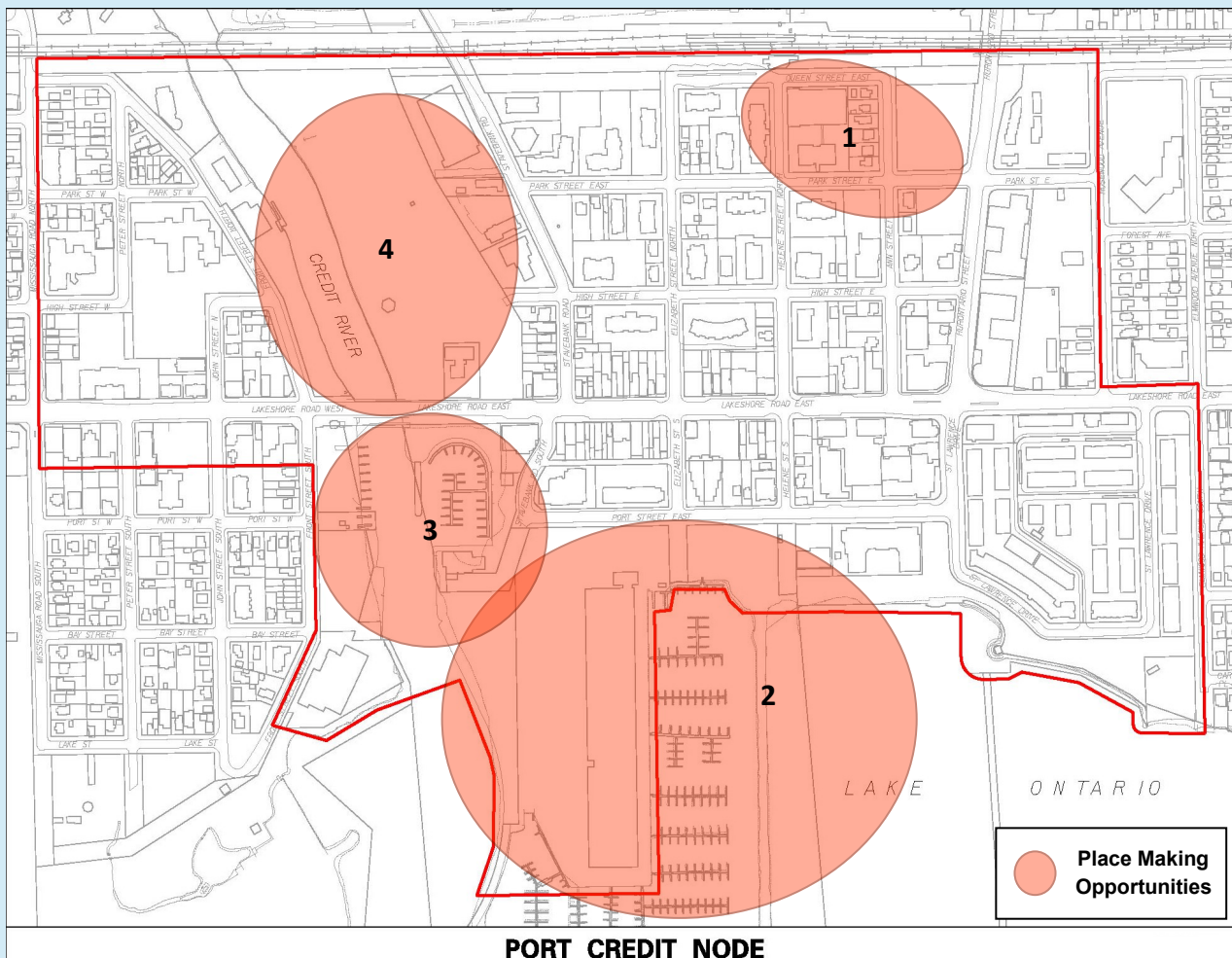
## 2.6 Place Making Opportunities

Place-making is the process that fosters the creation of vital public destinations: the kind of places where people feel a strong stake in their communities and a commitment to making things better.

Place-making capitalizes on a local community's assets, inspiration, and potential, ultimately creating good public spaces that promote people's social interaction, health, happiness, and well being.

Although the entire Node, as a centre for surrounding neighbourhoods, should be recognized as offering place-making opportunities, the following represent areas which have the opportunity to make a substantial contribution to Port Credit:

1. Transportation hub: the vicinity of the GO station, parking lot, and future LRT;
2. Waterfront marina: the vicinity of the Port Credit Harbour Marina, future LRT stop on Port Street East, and terminuses of Stavebank Road South, Elizabeth Street South, and Helene Street South;



MISSISSAUGA  
Planning and Building

Produced by  
T&W, Geomatics

— NODE BOUNDARY

Note: lines represent general  
approximation and could be  
adjusted subject to study

0 25 50 75 100  
metres



October 2011

Figure B112 — Place-making opportunities



## 2.0 Port Credit Community Node

3. Mouth of the Credit River: the vicinity of Snug Harbour, J.J. Plaus Park, Marina Park, Heritage Building (former Canada Post Office), Region of Peel Lighthouse; and
4. Credit River Valley: vicinity including Port Credit library, Port Credit Arena, Memorial Park, Canoe and Rowing Clubs, Legion Hall, Stavebank Road properties which back onto the valley.

When reviewing development applications, consideration should be given to capitalizing on any opportunities that may foster place-making and would contribute to the urban form of Port Credit.

The inclusion of Public Art should be considered in these locations.



**Figure B113 — (1) Go Station parking area**



**Figure B114 — (1) Go Station parking area**



**Figure B115 — (2) Port Credit Harbour Marina**



**Figure B116 — (2) -St Lawrence Square**



**Figure B117 — (2) -St Lawrence Park Lookout**



**Figure B118 — (2) St Lawrence Park**



**Figure B119 — (3) -J.J. Plaus Park**



**Figure B120 — (4) Port Credit Memorial Park**



**Figure B121 — (4) Credit River**



## 2.0 Port Credit Community Node

### 2.7 Continuous Waterfront Access

The Lake Ontario shoreline is an integral component of the green system and is a key Provincial linkage due to the unique ecological functions and habitats it provides. In addition, it has an important role in leisure activity and tourism.

As a condition of development approval, provisions of significant public parklands and access to the waterfront including the extension of the Waterfront Trail, will be required.

Continuous waterfront access along the shoreline is a priority for the residents of Port Credit and the City as a whole. In these situations an appropriate balance between use and public access must be found.

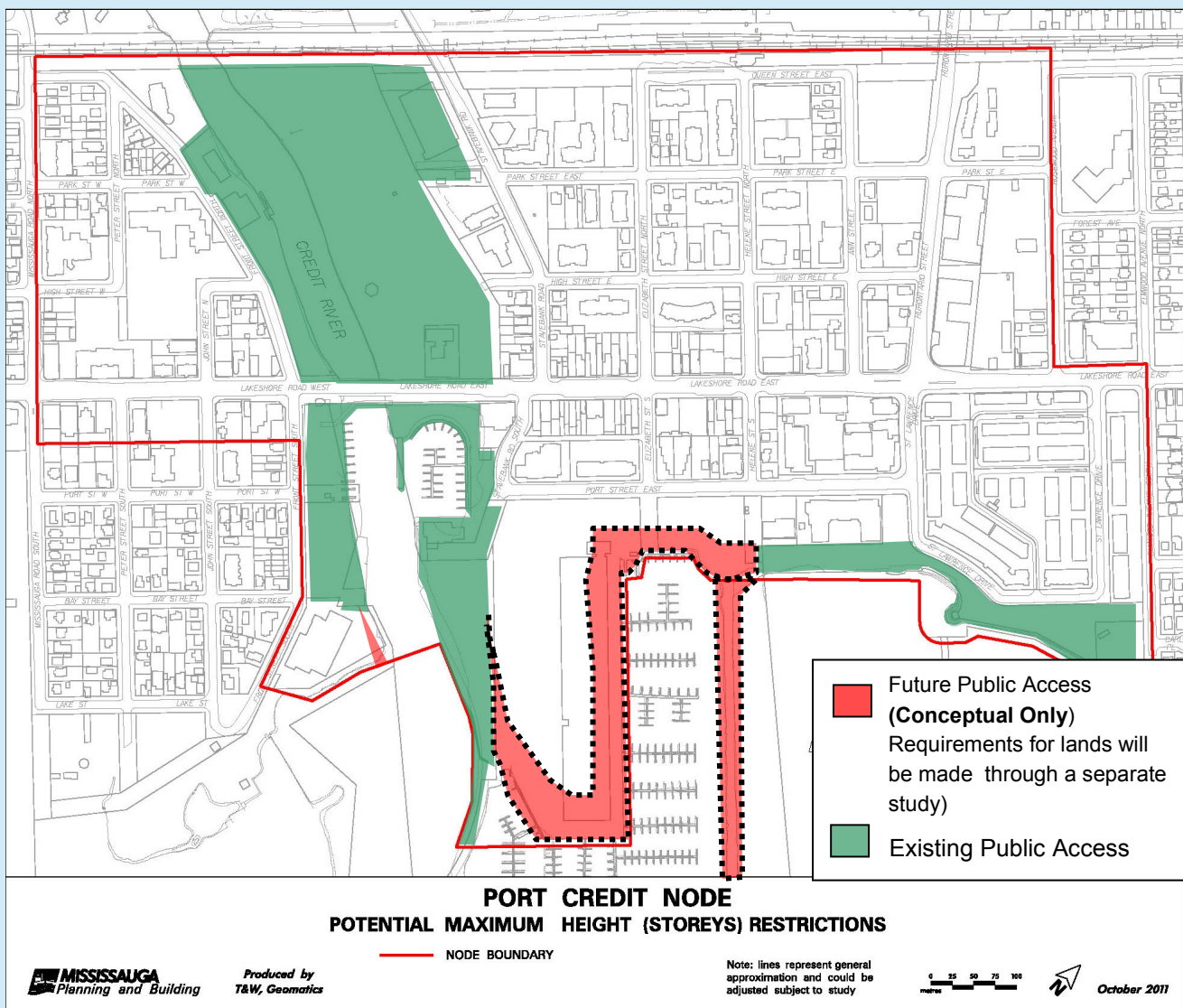


Figure B122 — Continuous Waterfront Access, Existing and proposed

## 2.0 Port Credit Community Node



Figure B123 — Port Credit Harbour Marina



Figure B124 — St Lawrence Park



Figure B125 — Credit River



Figure B126 — J.J. Plaus Park



Figure B127 — Credit River



Figure B128 — Port Credit Harbour Marina



Figure B129 — Port Credit Harbour Marina



Figure B130 — Boat launch, Marina Park



Figure B131 — St Lawrence Park



Figure B132 — St Lawrence Park



# 2.0 Port Credit Community Node

## 2.8 Cultural Heritage Resources

Heritage is an important characteristic of the Port Credit Node. Within the Port Credit Node there are over 45 properties that are on the City's Heritage Register. The Register contains two types of properties:

- Designated (recognized by the City through by-law as being of Heritage significance); and
- Listed (identified but not fully researched as to heritage significance).

In addition, the Port Credit Node also contains Cultural Landscapes which can be defined as a setting which has enhanced a community's vibrancy, aesthetic quality, distinctiveness and sense of history or sense of place. Cultural landscapes within the Port Credit Node include:

- Port Credit Harbour;
- Port Credit Pier;
- the CN Bridge over the Credit River;
- Credit River Corridor; and
- Mississauga Road Scenic Route.

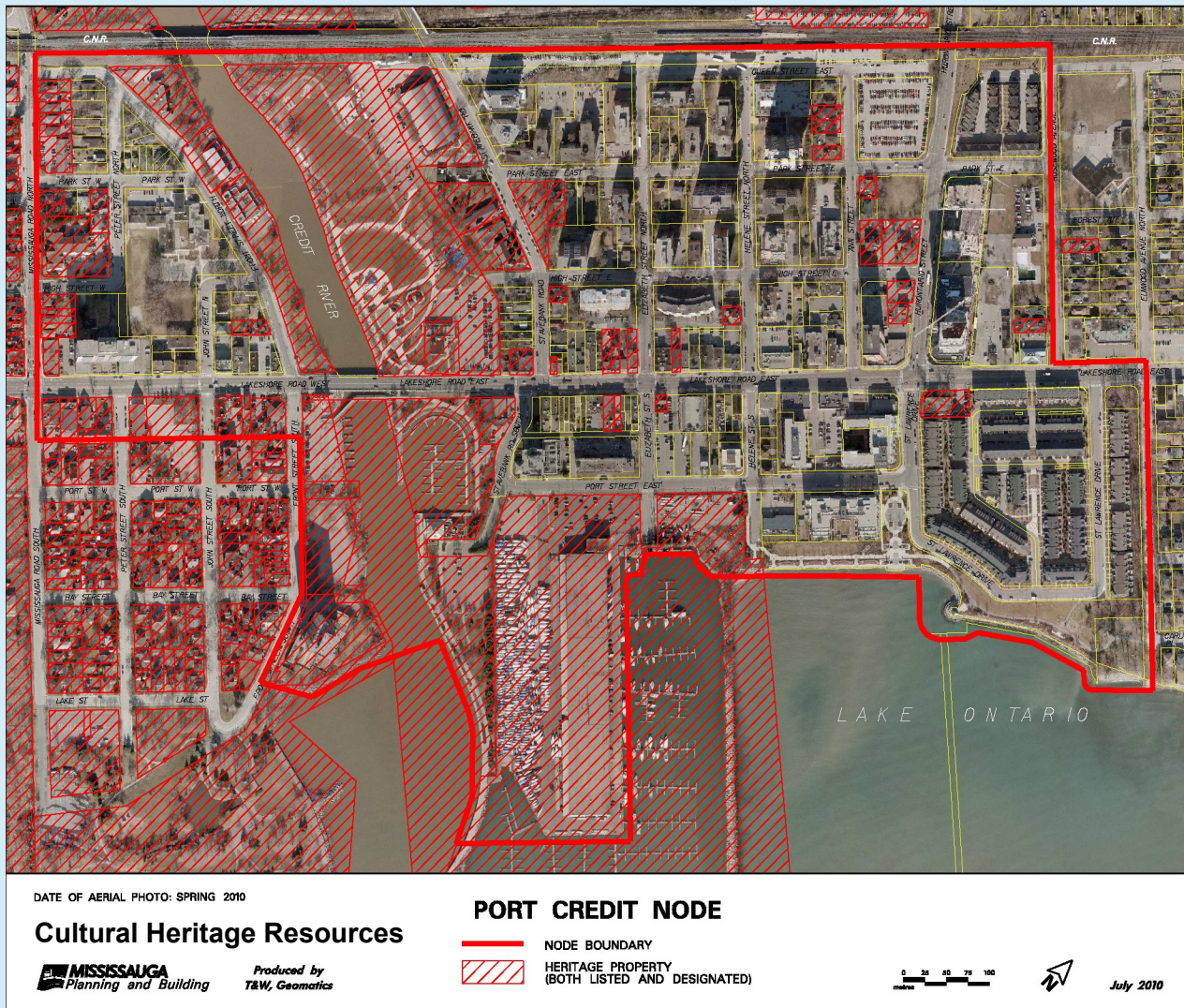


Figure B133 — Cultural Heritage Resources



## 2.0 Port Credit Community Node

The Port Credit Node has 10 properties that are designated. Designated properties are to be retained.

The retention and enhancement of heritage listed properties is strongly encouraged. Properties listed on the Heritage Register will be preserved in their existing location. Any development will incorporate these structures in the design of the proposal. Any changes to these structures or developments adjacent to these structures will require a Heritage Impact Statement and may have additional requirements. Additional requirements may include, but are not limited to, a review and approval from the Heritage Advisory Committee and Council.

Tall buildings will not visually impede the setting of listed/designated heritage buildings. Where heritage buildings are low-scale, the podium of the tall building will respect and reflect the unique urban character, visual relationships, topography and materials of the surrounding historic building. The tall buildings will preserve and enhance the character and appearance of the setting of the adjacent listed/designated building(s).

Where heritage buildings are low-scale, the podium or ground façade of tall buildings will respect and reflect the unique urban grain and scale, visual relationship, topography and materials of the surrounding historic buildings.

For lands on the south side of Lakeshore Road West, between Mississauga Road and the Credit River, the *Old Port Credit Village Heritage Conservation District Plan* applies.



Figure B134 — 90 High Street East, Designated



Figure B135 — 84 High Street East, CG Hamilton House, Designated



Figure B136 — 12 Peter Street South, Designated



Figure B137 — 141 Lakeshore Road West, Designated



# 3.0 Port Credit Neighbourhoods

## 3.0 Port Credit Neighbourhoods

Port Credit neighbourhoods are located on either side of the Port Credit Community Node. Lands identified as Neighbourhood Precincts are considered to be stable areas where the existing character is to be preserved and will not be the focus for intensification.

Although stable, some change is anticipated. New development does not necessarily have to mirror existing development types and densities, however it will respect the character of the area. New development does not necessarily have to mirror existing development types and densities, however, it will respect the character of the area.

The Port Credit Neighbourhood Character Area is comprised of five precincts:

- North Residential (which include the Shawnmarr/Indian Heights and Credit Grove neighbourhoods);
- South Residential (which includes the Cranberry Cove and Hiawatha neighbourhoods);
- Mainstreet Neighbourhood ;
- Historic Village of Port Credit; and
- Vacant Former Refinery.

The following sections provide additional context and information on each of the Neighbourhood Precincts. Subsequent sections provided further direction on specific built form issues.

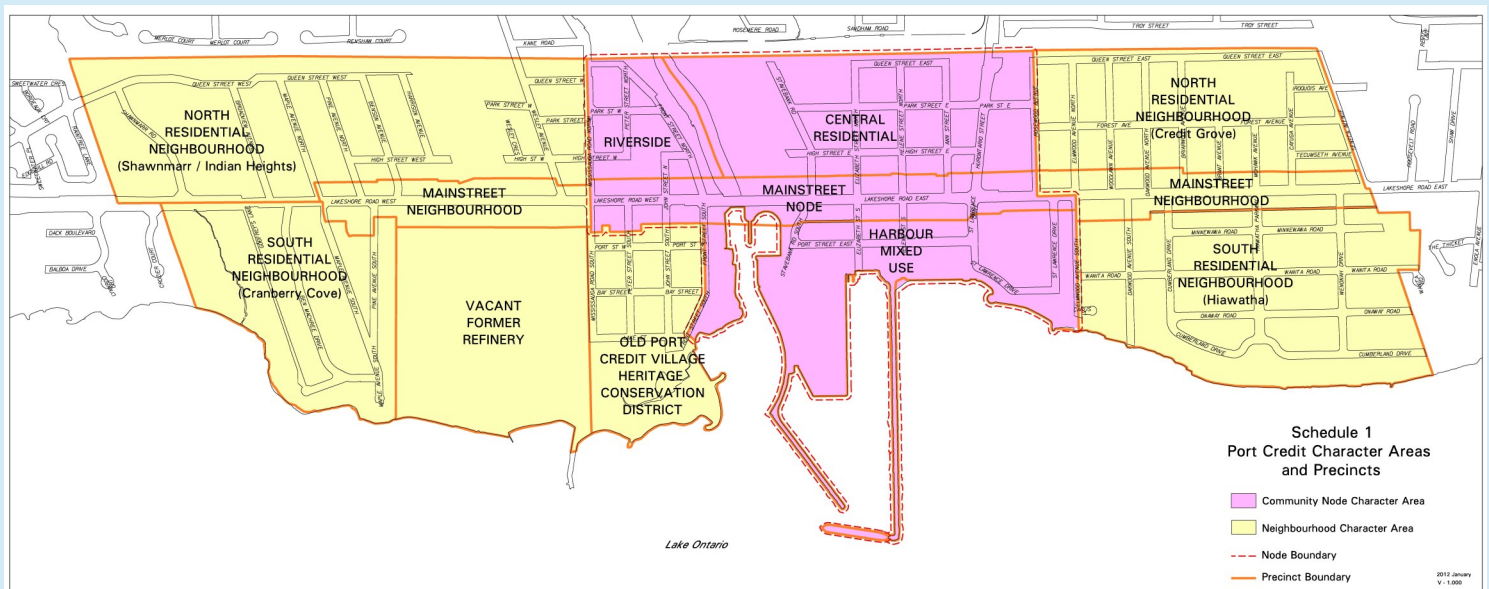


Figure C1 — Port Credit Neighbourhood Precinct Areas



## 3.0 Port Credit Neighbourhoods



Figure C2 — Port Credit Neighbourhood Areas



Figure C3 — Port Credit Neighbourhood Areas



## 3.0 Port Credit Neighbourhoods

### 3.1 Contextual Building Heights

Port Credit neighbourhood areas are characterized by predominately low rise buildings of 3 storeys or less. There are a limited number of older 4 to 6 storey apartment buildings located along Lakeshore Road or in the Heritage Precinct.

There is also an 8 storey apartment building representing the tallest structure in the Neighbourhood Precincts, located at Lakeshore Road West and Maple Avenue South.

While Neighbourhoods are to be stable, some change is anticipated. Where development occurs it will generally be through modest infilling or development within the mainstreet precinct, the existing commercial plaza or vacant former refinery site.

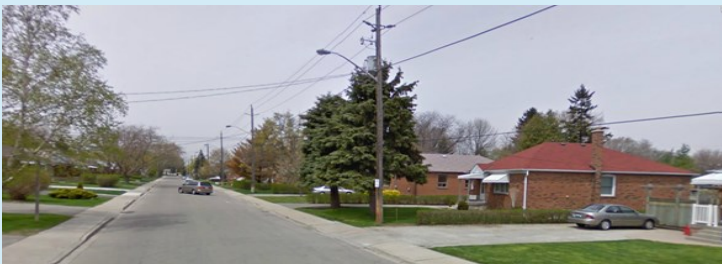
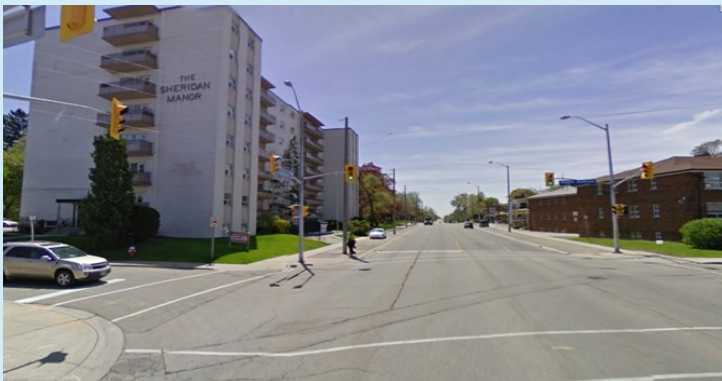


Figure C5 — Port Credit Neighbourhood Precinct Areas

# 3.0 Port Credit Neighbourhoods

## 3.2 Planned Building Heights

Building heights reflect the existing context and planned function for the Neighbourhood Character Area and are summarized in Figure C6. In some cases, building heights are reduced in the zoning by-law thereby requiring a proposal to go through a process to achieve the maximum height permitted.

Additional information is provided in the following sections on the individual precincts and subsequent discussions on specific built form issues.

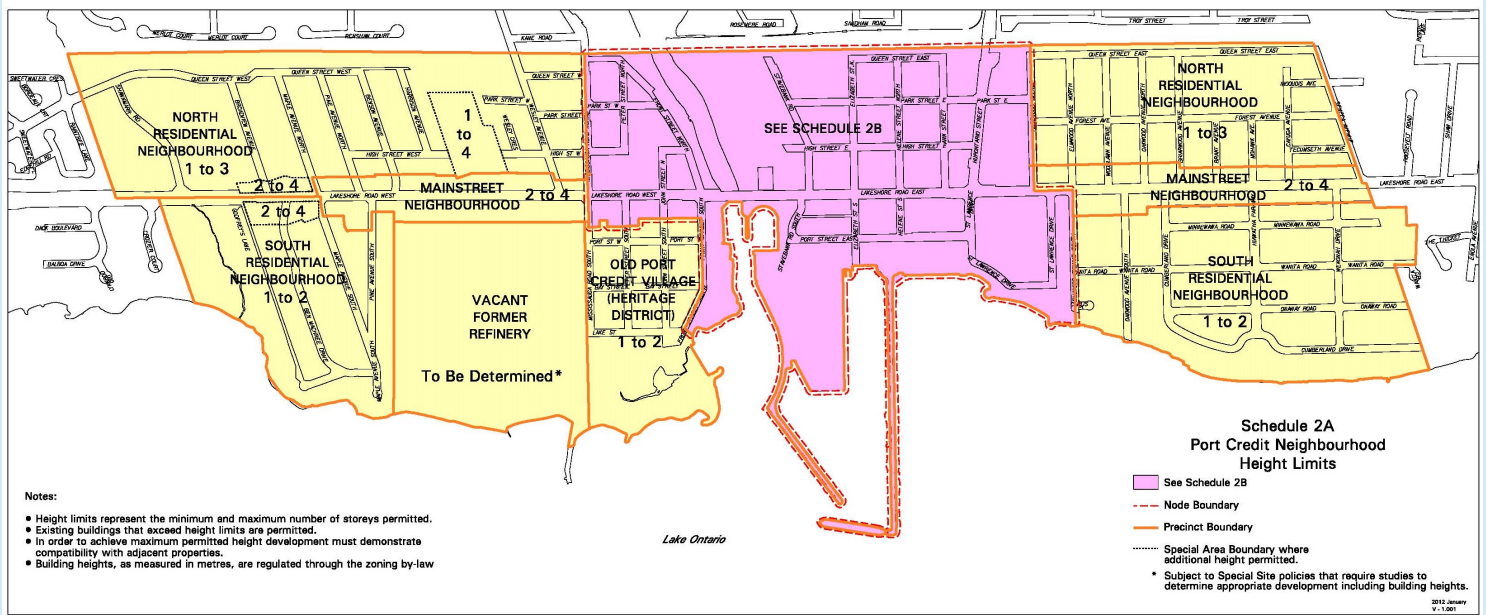


Figure C6 — Port Credit Neighbourhood Precinct Areas and their minimum and maximum height limits



## 3.0 Port Credit Neighbourhoods

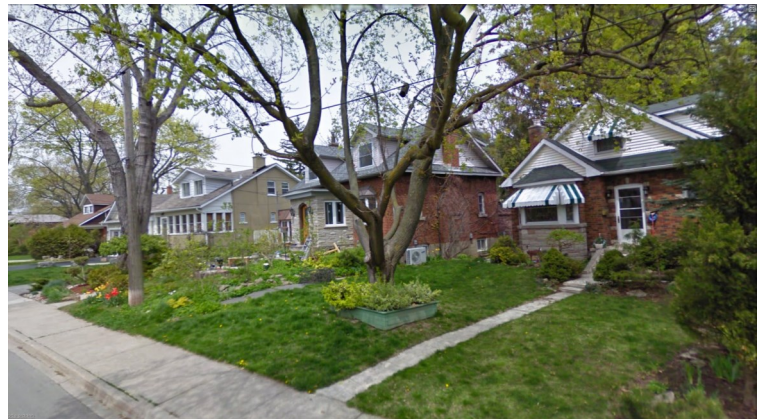
### 3.3 Neighbourhood Precinct Areas

#### 3.3.1 North Residential Precinct

This area consists of the Shawnmarr/Indian Heights and Credit Grove neighbourhoods. These stable residential areas will be maintained while allowing for infill which is compatible with and enhances the character of the area.

- a. The predominant characteristics of these areas will be preserved including: low rise building heights; the combination of small building masses on small lots; the well-landscaped streetscapes; and the regular street grid;
- b. New development is encouraged to reflect 1 to 2 storey building heights and should not exceed 3 storeys;
- c. Properties fronting onto Lakeshore Road West will complement the adjacent Mainstreet Precinct by continuing the 4 storey height limit; and
- d. Existing industrial uses along the railway tracks are encouraged to improve their transition to the adjacent residential areas. The maximum permitted height for industrial buildings (e.g. warehousing, self storage, etc.) will be the equivalent to a 2 storey residential building. New development will include appropriate buffers which

ensure lighting, noise levels, loading and garbage areas do not negatively impact adjacent residential areas. Development will provide a streetscape that is compatible with adjacent neighbourhood residential uses.



**Figure C7** — Examples of dwellings within the Port Credit North Residential Neighbourhoods



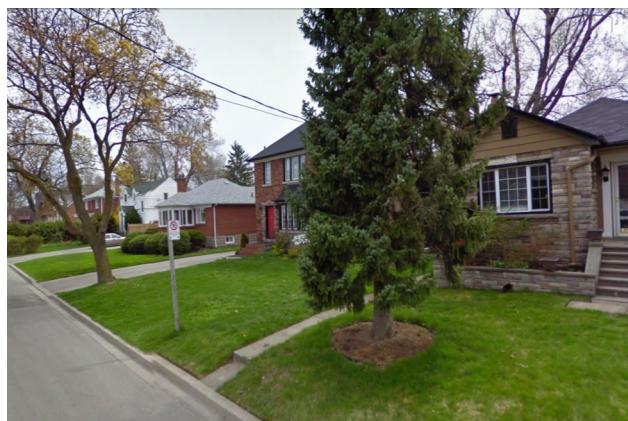
**Figure C8** — Examples of dwellings within the Port Credit North Residential Neighbourhoods

## 3.0 Port Credit Neighbourhoods

### 3.3.2 South Residential Precinct

This area consists of the Cranberry Cove and Hiawatha neighbourhoods. These stable residential areas will be maintained while allowing for infill which is compatible with and enhances the character of the area.

- a. The predominant characteristics of these areas will be preserved including: the low rise building heights; combination of small building masses on small lots; physical and visual access to Lake Ontario from parks and the terminus of streets; the well landscaped streetscape and street grid pattern;
- c. New development will have a maximum height generally equivalent to 2 storeys; and
- d. Properties fronting onto Lakeshore Road West will complement the adjacent mainstreet precinct by continuing the 4 storey height limit.



**Figure C9** — Examples of dwellings within the Port Credit South Residential Neighbourhoods



**Figure C10** — Examples of dwellings within the Port Credit South Neighbourhoods

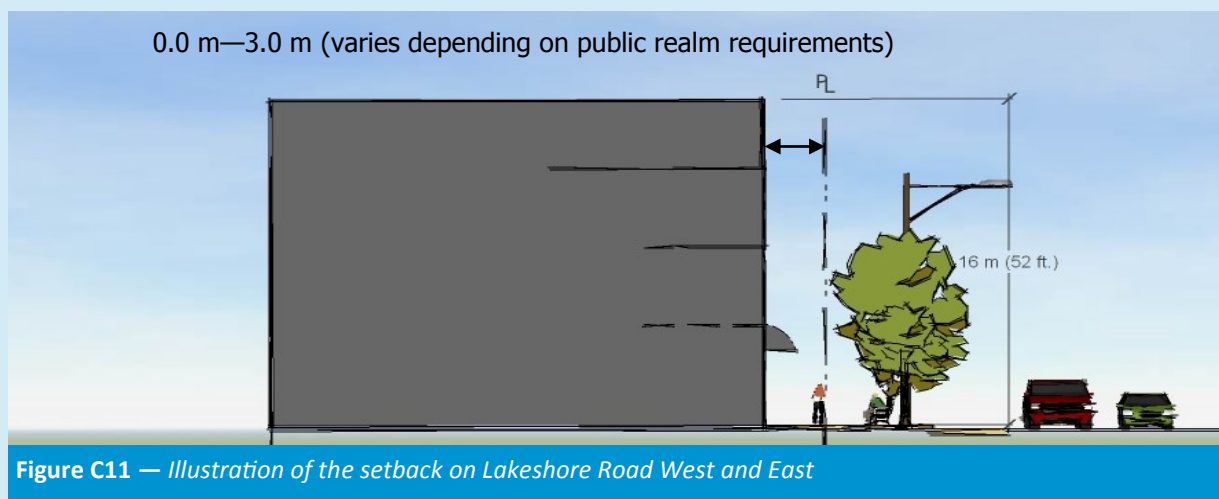


## 3.0 Port Credit Neighbourhoods

### 3.3.3 Neighbourhood Mainstreet Precinct

The lands fronting onto Lakeshore Road East and West outside of the Port Credit Community Node are known as the Neighbourhood Mainstreet Precincts. These lands will be developed as a mixed use area.

- a. Heights along the Lakeshore Road Corridor will be a minimum of 2 storeys and a maximum of 4 storeys;
- b. Along Lakeshore Road East and West, mixed-use developments with street-related retail commercial uses and a rhythm of closely spaced storefronts lining the street will be required to foster an active pedestrian realm and to minimize the apparent width of Lakeshore Road East and West;
- c. Developments along Lakeshore Road East and West will be close to the street and have a setback of 0.6 m to 3.0 m. The appropriate setback will be determined through a detailed analysis of the public realm and streetscape requirements. Additional setbacks may be required to ensure an appropriate pedestrian realm;
- d. Main entrances of buildings will be located along the Lakeshore Road East and West frontage;
- e. Parking will be provided on street or in small, distributed parking lots or at the rear of the building through laneways. It is critical that parking facilities be located and designed to be compatible with the mainstreet character of the area by including measures such as landscape space, planters or other elements which activate the street wall and enhance public amenity;
- f. Parking, loading and service areas will be located behind the buildings fronting Lakeshore Road East and West and will be screened from view from residential properties to the north and south;
- g. New access points along Lakeshore Road East and West will be strongly discouraged. Where alternatives are not feasible, access will be consolidated between properties and preferably located where a traffic signal opportunity is feasible;
- h. Where development is proposed adjacent to residential neighbourhoods, attention should be given to ensuring appropriate transition;
- i. The assembly of adjacent low density residential land to enlarge properties fronting the Mainstreet Precinct is discouraged. However, should assembly occur, the primary purpose of these lands should be for buffering the adjacent residential uses and for amenity space and/or parking if required through the development;

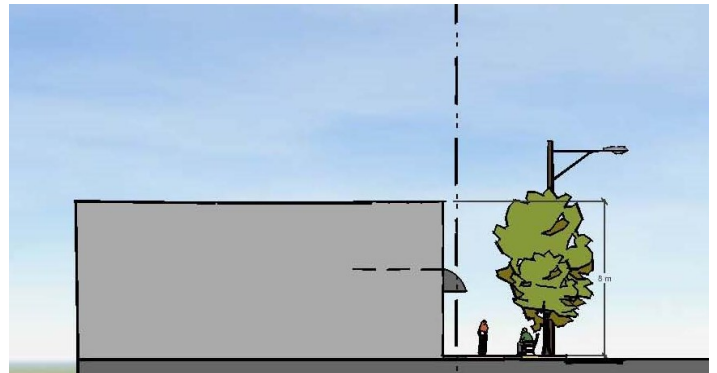


## 3.0 Port Credit Neighbourhoods

- j. Developments should maintain existing view corridors to the lake, such as along Oakwood Avenue South, and create new view corridors, such as, along the north/south public roads and
- k. Future development will recognize the character of adjacent land uses.



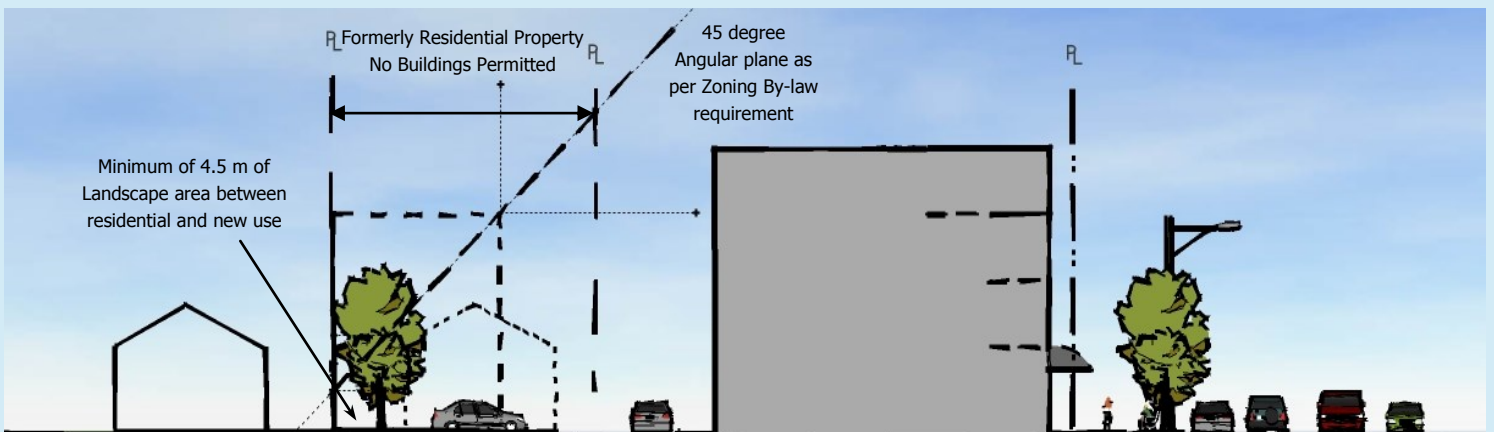
**Figure C12** — Example of potential building heights on Lakeshore Road West and East



**Figure C13** — Minimum building heights on Lakeshore Road West and East



**Figure C14** — Maximum building heights on Lakeshore Road West and East



**Figure C15** — Illustration of how the assembly of residential lands will be treated



## 3.0 Port Credit Neighbourhoods

### 3.3.4 Historic Village of Port Credit Precinct

This Precinct (also called the Historic Village of Port Credit) is a mixture of housing, retail commercial and community buildings of many types, representing different eras. While some of the housing stock is relatively new, the neighbourhood contains pockets of housing that dates back to the nineteenth century, creating a pleasing sense of "time depth". The Old Port Credit Village Heritage Conservation District Plan applies to lands within this precinct.

- a. Any additions, alterations, adaptive re-use or redevelopment will address how the development:
- Displays massing and scale sympathetic to surroundings;
  - Preserves the historic housing stock;
  - Supports the existing historic character;
  - Maintains the existing street grid pattern and building setbacks; and
  - Maintains and enhances significant groupings of trees and mature vegetation.



**Figure C17** — Example of structures in the Historic Village of Port Credit



**Figure C16** — Example of structures in the Historic Village of Port Credit



**Figure C18** — Example of structures in the Historic Village of Port Credit

## 3.0 Port Credit Neighbourhoods

### 3.3.5 Vacant Former Refinery Precinct

This precinct has redevelopment potential, however, further study is required to determine the appropriate development. This precinct should ultimately be developed in a manner which is compatible with the surrounding lands and which does not detract from the planned function of the Community Node.

- Building heights will provide appropriate transition to adjacent South Residential and Old Port Credit Heritage Conservation District Precincts;
- Public roads will be provided in any future development that separates any new development from the Lake Ontario Waterfront Open Space and the continuous waterfront trail to maximize public access to and along the Lake Ontario Waterfront; and
- A landscape buffer will be provided between the precinct and the adjacent residential neighbourhood to the west.
- The Vacant Former Refinery, along with associated lands located in the Neighbourhood Mainstreet Precinct are subject to further study to determine appropriate heights, built form and land uses.



**Figure C19 —**  
*Imperial Oil Lands,  
waterfront path*



**Figure C20 —**  
*Imperial Oil Lands,  
waterfront path, old  
pier*



**Figure C21 —**  
*Imperial Oil Lands,  
View Looking north  
from the public  
pathway*



**Figure C22 —**  
*Imperial Oil  
Lands*



# 3.0 Port Credit Neighbourhoods

## 3.4 Views

Views to Lake Ontario are vital and should be enhanced where possible.

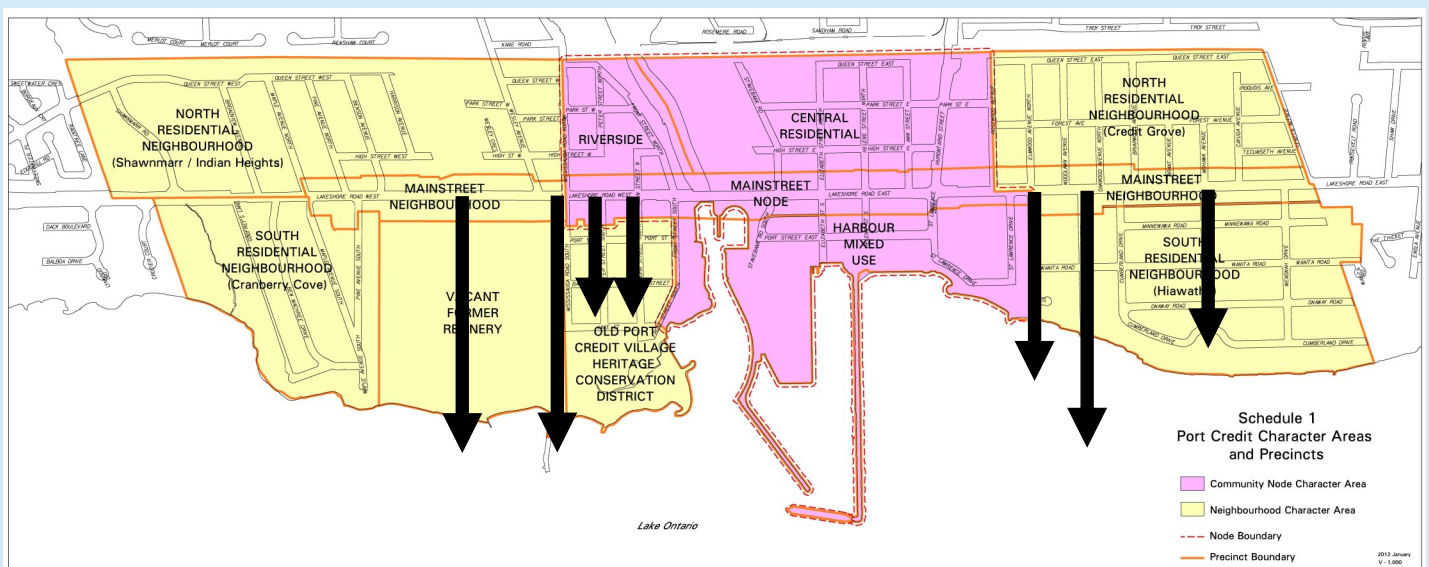
New development should ensure views to Lake Ontario within the Port Credit Neighbourhoods are maintained.



**Figure C42** — Hiawatha Parkway and Onaway Road, view to Hiawatha Park



**Figure C43** — Elmwood Avenue South



**Figure C44** — Views to Lake Ontario

## 3.0 Port Credit Neighbourhoods

### 3.5 Cultural Heritage Resources

There are a number of heritage resources in the Port Credit Neighbourhoods. Designated properties are to be retained. The retention and enhancement of heritage listed properties is strongly encouraged.

New development proposed in proximity to these buildings should be developed in a manner that is sensitive and complimentary to the character of the structure.

New developments should provide the appropriate transition to properties listed on the heritage register and in the Historic Village of Port Credit Precinct.



Figure C45 — 44 Wesley Avenue



Figure C46 — 34 Peter Street



Figure C47 — 5 Oakwood Avenue North



Figure C48 — 305 Lakeshore Road West  
Designated

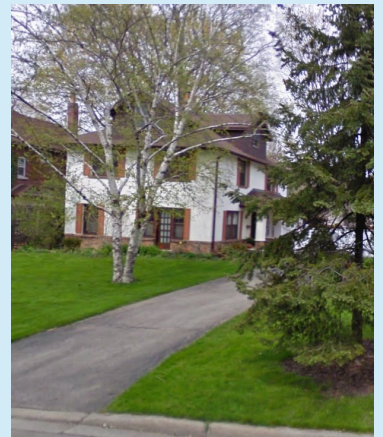


Figure C49 — 11 Oakwood  
Avenue North



## 3.0 Port Credit Neighbourhoods

### 3.6 Parking, Servicing and Loading

The design of parking, servicing and loading areas is a key component in the development of sites. These areas serve a functional need, but should be designed in a manner that screens less desirable aspects and provides high quality treatment of exposed areas while addressing safe and efficient movement of pedestrians, cyclists and vehicles.

Parking should be located underground, internal to the building or to the rear of the buildings.

Above grade parking structures should be designed in such a manner that vehicles are not visible to the public and have appropriate directional signage to the structure.

Service, loading and garbage storage areas should be integrated into the building or located at the rear of the building and screened from the public realm and adjacent residential uses.



Figure C50 — Briarwood Avenue north of Lakeshore Road East

## 3.0 Port Credit Neighbourhoods

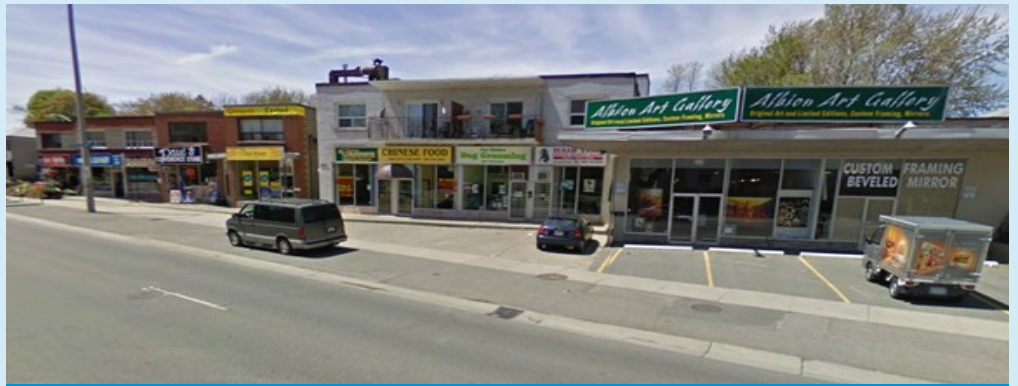
### 3.7 Access Points

The consolidation and relocation of access points along Lakeshore Road East and West will be encouraged to promote pedestrian safety and a cycle friendly environment to provide the opportunity for a continuous streetscape.

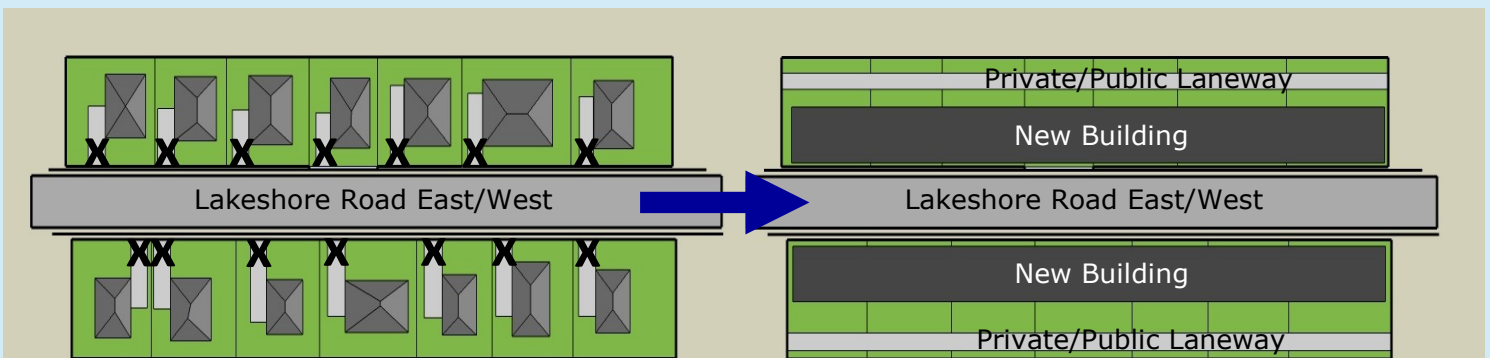
Mutual access between abutting properties will be essential in achieving a pedestrian oriented environment. Existing access will be relocated whenever possible to side streets and the rear of the property. The addition of access points will not be permitted.



**Figure C51** — *Pedestrian Realm with no access points*



**Figure C52** — *Existing Access Points*



**Figure C53** — *Conceptual consolidation of access points along Lakeshore Road West and East*



## 3.0 Port Credit Neighbourhoods

### 3.8 Pedestrian Realm/Streetscape

The Neighbourhood Precincts have an established streetscape particularly in the residential areas. These consist of mature tree lined streets and some sidewalks on the street edge. A significant number of the residential streets, however, do not have sidewalks. This will be maintained.

In the Neighbourhood Mainstreet Precinct, a Master Streetscape Plan will be required to determine the appropriate streetscape and to ensure any planned streetscape work takes into consideration established and planned utilities. Coordinated street furniture, including post and ring bike parking, should be established through this process.

In the interim, applicants for development applications along mainstreets will be required to submit a streetscape plan which clearly demonstrates how they can achieve the optimal boulevard width of 5.6 m and how the proposed development will fit in with the current streetscape pattern.



Figure C54 — Residential street in Port Credit



Figure C55 — Neighbourhood Mainstreet Precinct



Figure C56 — Existing pedestrian realm in the Neighbourhood Mainstreet

# 3.0 Port Credit Neighbourhoods

## 3.9 Microclimate

Shadow and wind comfort studies will be required as per the City of Mississauga Design Guidelines and Reference Notes.

### 3.9.1 Shadow Impact

Shadow studies will be requested in support of development applications to demonstrate that the height and/or location of a proposed building will not generate excessive shadows over adjacent lands.

Shadow studies will be required for buildings greater than 10.7 m in height which may cause new shadow impacts on adjacent residential properties, public parkland, open space and the public realm. Particular attention will be focused on the mainstreet corridors (Lakeshore Road East or West and Hurontario Street) where a pedestrian environment is strongly encouraged and fostered.

### 3.9.2 Wind Comfort

Wind studies may be requested for developments over 3 stories or 16 m in height to ensure appropriate comfort and safety levels are maintained in the pedestrian realm, streetscapes, public spaces, private public amenity areas and areas immediately adjacent to and surrounding the proposed development. The study may be required on development applications higher than 3 storeys.

Evaluation of the existing wind conditions in the immediate and surrounding area, prior to the proposed development will be required along with a comparison of the wind conditions .

The criteria to be used for the analysis should be signed and sealed by a certified engineer. The *Terms of Reference for Pedestrian Wind Comfort Studies* should be used.



**Figure C57** — Shadow impact on the Neighbourhood Mainstreet Precinct



**Figure C58** — Wind impact on the pedestrian environment



## 3.0 Port Credit Neighbourhoods

### 3.10 Materiality

As in the Community Node, new buildings should reference their surroundings through their architectural language and high quality materials. In addition, there may be other City initiatives and directions (e.g. Relevant Urban Design Guidelines, Port Credit Village Heritage Conservation District Plan, among some) which need to be consulted.

High quality building materials will be required in all new developments in Port Credit. The predominant exterior

building material is a reddish tone brick and stone. New buildings are required to maintain this theme along the neighbourhood mainstreet and in the residential neighbourhoods.

For the mainstreet, vision glass should be used for all store fronts. Spandrel glass will not be permitted along Lakeshore Road East and West.

Concrete block is not permitted to be exposed. Painted concrete block is not permitted.



Figure C60 — Example of brick and stone material



Figure C61 — Example of brick and stone material





## 3.0 Port Credit Neighbourhoods



Figure C62 — Example of brick and stone material



Figure C63 — Example of brick and stone material / Example of roof feature



Figure C64 — Example of brick and stone material / Example of Canopy

# 4.0 Port Credit

## 4.0 Environmental Sustainability

New developments must be sustainable in all aspects, taking into account social and economic impact, based on whole life costs and benefits. Advances in construction technology combined with a growing body of architectural knowledge mean that sustainable practices should be easily achievable.

On July 7, 2010, City Council adopted the Green Development Strategy that focuses on achieving sustainability and environmental responsibility in new development in Mississauga. The City strongly encourages applicants to incorporate green sustainable elements into proposed buildings, site works, construction methods and long-term maintenance programs. Further, the City also encourages that applicants pursue LEED-NC credits required to achieve Silver certification.

For More information, Visit Canada Green Building Council for the LEED – NC program, CVC/TRCA website for the Low Impact Development Stormwater Management Planning



**Figure D1** — *Example of vertical parking grate screened by landscape*

and Design Guide, City of Mississauga for the Green Development Strategy and Stage One Development Standards and Mississauga Water Quality Control Strategy.



**Figure D2** — *Example of enhanced dry grass swale*



**Figure D3** — *Typical rain water barrel*



**Figure D4** — *Extensive' Green Roof, Mountain Coop, Toronto*

# 4.0 Port Credit

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# 5.0 Appendix A

## 5.1 Photo Credits

Page	Description	Credit
Cover	Small Upper: 10 Restaurant, Port Credit	Mississauga Image Library
	Small Lower: Detached dwellings in Port Credit	Mississauga Image Library
	Snug Harbour	Sharon Mittmann, City of Mississauga
Page 2	In order from top to bottom:	
	Port Credit Lighthouse	John Carvalho
	Ten Restaurant, Port Credit	Mississauga Image Library
	Second Cup, Port Credit	Sharon Mittmann, City of Mississauga
	Lakeshore Road West	Sharon Mittmann, City of Mississauga
	1 Port Street	Sharon Mittmann, City of Mississauga
	Clark Hall, Port Credit	Sharon Mittmann, City of Mississauga
	St. Lawrence Development	Sharon Mittmann, City of Mississauga
	Port Credit local Road	Sharon Mittmann, City of Mississauga
	Port Street East	Sharon Mittmann, City of Mississauga
Page 3	In order from top to bottom	
	Live Work Units on Lakeshore Road West	Mississauga Image Library
	Port Street	Sharon Mittmann, City of Mississauga
	Mainstreet Development on Lakeshore Rd. W.	Sharon Mittmann, City of Mississauga
	Central Residential Area	Sharon Mittmann, City of Mississauga
	High Street East	Sharon Mittmann, City of Mississauga
	Central Residential Area	Sharon Mittmann, City of Mississauga
	St. Lawrence Park	Sharon Mittmann, City of Mississauga
	1 Port Street	Sharon Mittmann, City of Mississauga
	Lakeshore Road East	Sharon Mittmann, City of Mississauga
Page 4	From top left	
	Lakeshore Road W/E bridge over Credit River	Sharon Mittmann, City of Mississauga
	Central Residential Area	Sharon Mittmann, City of Mississauga
	Detached dwelling in Port Credit	Sharon Mittmann, City of Mississauga
	Ten Restaurant Port Credit	Sharon Mittmann, City of Mississauga
	Port Credit Go Sign	Sharon Mittmann, City of Mississauga
	Triplex, Port Credit	Sharon Mittmann, City of Mississauga
Page 5	Port Credit Precinct Map	Geomatics, City of Mississauga
Page 6	Port Credit Precinct Map	Geomatics, City of Mississauga
Page 7	Central Residential Precinct Image off of Port Street	Sharon Mittmann, City of Mississauga
	Central Residential Precinct Image Heritage Building	Sharon Mittmann, City of Mississauga
	Riverside Precinct Image	Sharon Mittmann, City of Mississauga
	Central Residential Precinct Image	Mississauga Image Library
	Main Street Corridor Precinct (Commercial Area) Image	Sharon Mittmann, City of Mississauga
	Harbour Mixed Use Precinct Image	Sharon Mittmann, City of Mississauga
Page 8	Existing building heights in Port Credit	Geomatics, City of Mississauga
Page 9	Riverside Precinct	Sharon Mittmann, City of Mississauga
	Main Street Corridor Precinct (Commercial Area)	Sharon Mittmann, City of Mississauga
	Central Residential Precinct	Sharon Mittmann, City of Mississauga
	Harbour Mixed Use	Sharon Mittmann, City of Mississauga
Page 10	Maximum Building Heights in the Port Credit Node Precinct	Geomatics, City of Mississauga
Page 11	Differences in Building Heights. Buildings with the same number of floors are approximately 13% taller now.	Sharon Mittmann, City of Mississauga

## 5.0 Appendix A

	Building Transition from the Railway Tracks to Lakeshore Road East and West. Buildings should also transition from Lakeshore Road East and West to Lake Ontario with the exception of key landmark locations (e.g. Intersection of Hurontario Street and Lakeshore Road East)	Sharon Mittmann, City of Mississauga
Page 12	Mainstreet Precinct, Mississauga Road and Lakeshore Road West. New construction.	Sharon Mittmann, City of Mississauga
Page 13	Building heights in the Central Residential Precinct	Sharon Mittmann, City of Mississauga
	Port Credit Marina, Snug Harbour	Mississauga Image Library
	Mainstreet Precinct on Lakeshore Road West	Dan Magee, City of Mississauga
	Market Square Development, Lakeshore Road East	Mississauga Image Library
	Port Credit Node waterfront view	David Broderick, City of Mississauga
	Port Credit Light house	John Carvalho
Page 14	Image of the Riverside Precinct	Sharon Mittmann, City of Mississauga
	Image of the Riverside Precinct	Dan Magee, City of Mississauga
	Image of the Riverside Precinct	Dan Magee, City of Mississauga
	Image of the Riverside Precinct	Sharon Mittmann, City of Mississauga
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## Notes

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City of Mississauga

# LAKESHORE ROAD



## Transportation Review Study

FINAL REPORT

# EXECUTIVE SUMMARY

## **Study Purpose**

The overall goal of this study was to provide a comprehensive and technical transportation review to identify how the Lakeshore Road corridor can accommodate alternative modes of transportation and provide input and background into the preparation of Official Plan policies for Lakeview and Port Credit and address stakeholder comments provided in the “*Directions Report*”.

As the City looks to the future, long term plans for corridors such as Lakeshore Road will include providing the necessary facilities for all modes of transportation. As such, pedestrian facilities, cycling facilities and higher order transit facilities all need to be accommodated for a revitalized and more active Lakeshore Road.

The project conceptually reviewed the feasibility of future higher order transit within the existing Lakeshore Road right-of-way from Hurontario Street to the Long Branch TTC / Mississauga Transit terminal (the study area stops at east municipal boundary, just west of the Long Branch terminal at Etobicoke Creek).

## **History**

Lakeshore Road was formally part of the Provincial King’s Highway network as Highway 2 continuous through the GTA. It has also supported the ongoing development of Port Credit, Lakeview and Clarkson urban villages as well as industrial and residential areas along the corridor. Construction of the QEW subsequently relieved most of the through traffic from Lakeshore Road.

## **Current Role**

The roadway has an important role to play in supporting the variety of tourist, commercial, industrial and residential land uses along the corridor. There are no alternative parallel continuous east-west routes except for the access controlled QEW some 2 kms to the north.

Currently Lakeshore Road provides for autos, bus transit and pedestrians, but not explicitly for bicycles. An evaluation of alternative modes and how these modes can be accommodated within the changing Lakeshore Road ROW has been undertaken. The following conclusion and results were developed in the course of this evaluation and functional design exercise.

Currently, around 75% of the traffic crossing the Credit River in the AM peak originates from the southwest part of Mississauga. Approximately 50% of the trips crossing the Credit River on Lakeshore Road are destined to a work / school locations in eastern Mississauga. On occasions when the QEW is blocked, traffic overflows onto Lakeshore Road which exacerbates the already congested traffic entering Port Credit. Traffic flow improves east of Hurontario Street through Port Credit and Lakeview.

## **Traffic Trends**

All day traffic volumes have not risen in the last 10 years. Forecasts show that for the most part, peak direction volumes will not increase in the next 20 years, except for a small increase west of Port Credit as a result of increased GO Train ridership out of the Port Credit GO Station. Traffic volumes in the off-peak direction are forecast to increase resulting in more balanced flows along Lakeshore Road.

## **Future Transit**

On November 28, 2008, Metrolinx released their Draft Regional Transportation Plan entitled *“The Big Move: Transforming Transportation in the Greater Toronto and Hamilton area”*. This document sets out the actions to build new transportation infrastructure and improve transit service in the Greater Toronto and Hamilton area. One such initiative is the Waterfront West Rapid Transit project which will be a new east-west rapid transit connection from Union Station to the Port Credit GO Station. Although not currently financed to be programmed, this initiative is included in their 15-Year Plan. The technology to be utilized and its ultimate alignment were not determined however, it was assumed to be accommodated within the road right-of-way for Lakeshore Road.

## **LRT**

Higher order transit cannot be achieved west of Hurontario Street without compromising other modes and / or streetscaping. The long term plan includes LRT in mixed traffic (similar to Queen / King Streetcar service in Toronto) in the restricted parts of the corridor between Hurontario Street and Greaves Avenue. East of Greaves Avenue, LRT on its own right of way is indicated in the long term functional plans

## **Right-of-way Challenges**

Between Broadview Avenue and Seneca Avenue, a right of way (ROW) width of only 26.2 m is available, with no likelihood of increasing that width. An additional constraint exists between Cawthra Road and Greaves Avenue; however there will be opportunities to achieve the ultimate 30 m ROW in the future. Therefore, in the near term even with a potential narrowing of vehicular lane widths, it is not feasible to implement on-street bicycle lanes in this stretch while maintaining 4 lanes of traffic, on-street parking and the current boulevard / sidewalk widths (which are considered the minimum desirable already). Between Seneca Avenue and Cawthra Road and again between Greaves Avenue and the east municipal boundary, greater ROW widths are available (or could be available) and therefore do not provide the same challenge that the Port Credit and Cawthra Road to Greaves Avenue sections pose.

A series of cross-section alternatives were developed and assessed for use in the short term (based on bus transit in mixed traffic) and long term (with LRT east of Hurontario Street). Many of the more desirable options were not feasible due to inadequate ROW availability. One of the main ROW challenges was to accommodate cyclists more safely than the current situation without adversely impacting space requirements for vehicles, pedestrians and streetscaping.



### **Lane Widths**

In order to better accommodate bicycles and in some scenarios transit, lane widths on Lakeshore Road were reduced in the short and long term functional designs. Typically 3.35 m lanes were used (3.0 m for left turn lanes) except for the locations where sharrows are proposed in wider 4.0 m curb lanes.

### **Centre Two-Way-Left-Turn-Lane (CTWLTL)**

The existing CTWLTL in place between Seneca Avenue and the east municipal boundary provides a measurable and significant safety advantage compared with a scenario of having no left turn storage. In the future, should it be possible to reduce the frequency of access points along this stretch, it may be possible to implement a raised median with channelized left turns at the remaining access points to provide a further safety advantage over the CTWLTL. This would also have the advantage of providing better definition (delineation) for pedestrians.

In the long term plan, the CTWLTL would have to be removed in the section east of Greaves Avenue as it would be replaced by an LRT in an exclusive ROW along the centre of the road. In the long term plan west of Greaves Avenue, left turn pockets could potentially be made between the tracks, or more likely the left turns would take place on the tracks (delaying transit). More analysis would be needed to determine the appropriate solution at specific locations.

### **Bicycle Plan**

The Mississauga Cycling Master Plan was recently completed which has highlighted Lakeshore Road as a future on-street bicycle facility. Input from this study will help determine how cycling will be accommodated.

This study has determined that a wide curb lane with sharrows is the near term preferred application through the areas with a narrow right of way – from Broadview Avenue to Greaves Avenue.

The exception being a small section between Seneca Avenue and Cawthra Road that is able to accommodate on-street bicycle lanes; however, this will require a 2 metre widening of the road pavement which appears to be feasible given the 31 metre ROW available. Again east of Greaves Avenue to the municipal boundary, on-street bicycle lanes are recommended which would require a road widening on both sides.

The long term functional plan shows on-street bicycle lanes throughout the corridor, this is made possible by an assumption that on-street parking (on one side of the road) through Port Credit may ultimately be removed and may be replaced with off-street parking lots / garages.

Although not explicitly assessed in this study, bicycle lanes are also suggested for the section of Lakeshore Road between Clarkson and Port Credit (Johnson's Lane to Shawnmarr Road) as a result of higher vehicular speeds and the availability of a wider ROW.

### **Credit River Bridge**

The Credit River Bridge will need widening to accommodate a relocation of bicycles from the street to the current sidewalks. A number of structural alternatives to achieve this will need to be investigated further in a Class Environmental Assessment (EA) study.

### **On-Street Parking**

Current on-street paid parking in Port Credit provides an important support function to the commercial core and tourist area of Port Credit. Over the longer term, there may come a time when changes in modal usage may reduce the need for on-street parking. There is also a parking management plan underway for the Port Credit area that includes among other things an examination of the feasibility of replacing a portion of the on-street parking supply with off-street / side street parking.

In the meantime, as long as 4 traffic lanes are required and on-street parking is present, it is extremely difficult to provide either on-street bicycle lanes or higher order transit through Port Credit.

A strategic parking plan for the Lakeshore Road corridor through Lakeview is currently underway which will set the parking framework for this area.

Although the near term recommended plan for the corridor includes retention of the existing parking spaces, in order to provide more opportunity for other modes, at least one side of the on-street parking may need to be replaced by adequate and convenient off-street parking. This provides a longer term challenge and is reflected in the long term plan which includes bicycle lanes with parking on the south side only. This results in the removal of 88 on-street parking spaces on the north side.

### **Recommended Plans**

Two design plans were developed for the corridor. One is considered to be a long term plan which includes the LRT and also removal of on-street parking from one-side of the road, while a near term option was developed which did not consider LRT but maintained the on-street parking.

### **Near Term Plan**

The basic cross-section alternative selected for the near term option through Port Credit and another narrow ROW section east of Cawthra Road is shown below in **Exhibit E1**.

The cross-section includes the addition of sharrows in a 4 m curb lane and involves a slight modification of the existing cross-section (mainly a reduction in vehicular lane and parking widths) to accommodate a wide curb lane for sharrows to better accommodate cyclists. For areas with a wider ROW, between Greaves Avenue and Cawthra Road and again east of Greaves Avenue to the municipal boundary, on-street bicycle lanes are included instead of sharrows. Minor curb adjustments are required to implement the cycling facilities.

## CONCEPT FOR: BUSES IN MIXED TRAFFIC + SHARROW LANE + ON-STREET PARKING

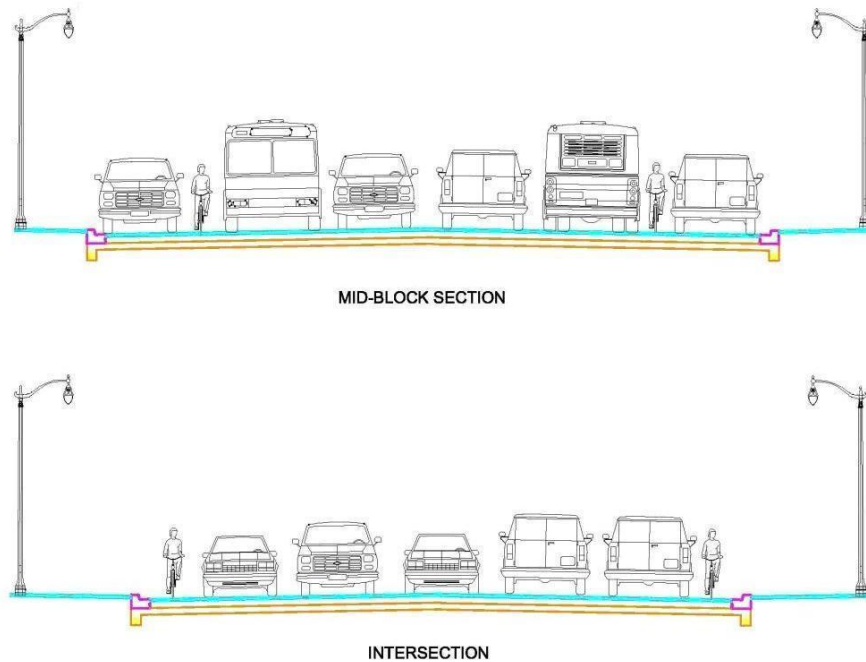


EXHIBIT E1: NEAR TERM CONCEPT THROUGH RESTRICTED ROW

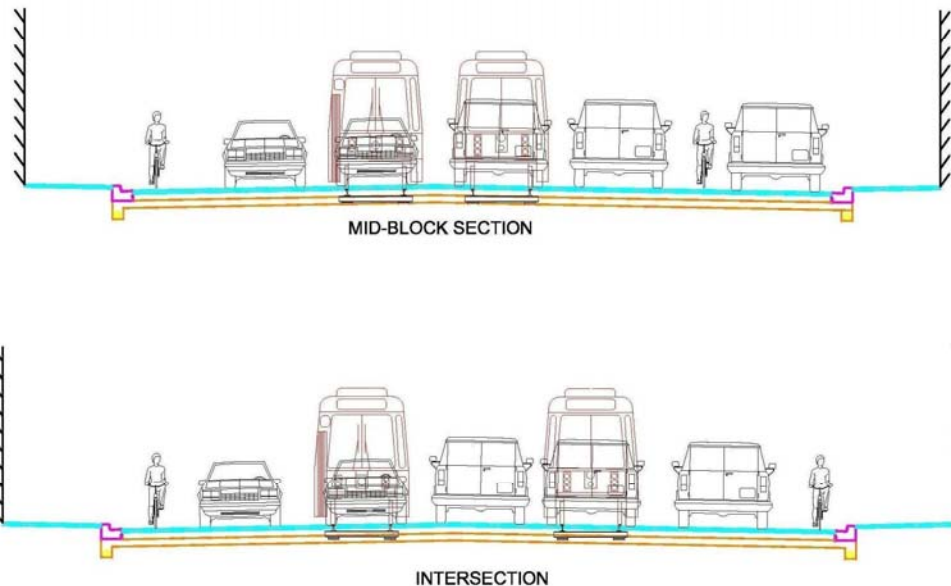
**Note: Subject to further review including Community consultation and preliminary design.**

The functional design for this near term option is shown in the upper half of Plates 1 through 20 at the back of this report.

### Long Term Option

Between Hurontario Street and Greaves Avenue, due to the constricted ROW sections, the basic cross-section alternative selected for the long term option is LRT in mixed traffic (e.g. similar to Queen St / King St. in Toronto). East of Greaves Avenue to the east municipal boundary, LRT on its own ROW is feasible. A short section between Seneca Avenue and Cawthra Road could also have accommodated LRT, but the section is too short to accommodate a transition to and from LRT / mixed traffic. Bicycle lanes are shown throughout the corridor. This is made possible through the 26.2 m ROW sections by removing parking on the north side (choice of the north side was arbitrary). The recommended typical cross-section for the long term between Hurontario Street and Greaves Avenue is shown below in **Exhibit E2**.

## CONCEPT FOR: LRT IN MIXED TRAFFIC + ON-STREET BICYCLE LANES + ONE SIDE ON STREET PARKING



### EXHIBIT E2: LONG TERM CONCEPT THROUGH RESTRICTED ROW (EAST OF HURONTARIO STREET)

**Note: Subject to further review including Community consultation and preliminary design.**

The functional design for the long term option is shown in the lower half of Plates 1 through 20 at the back of this report.

#### Implementation

##### Near Term

To implement near term changes, including curb adjustments required for cycling facilities, it is anticipated this would occur in phases as part of the Transportation and Works Department road rehabilitation / reconstruction program. These minor curb adjustments are required to accommodate the on-road bicycle facilities (sharrows / bike lanes) based on the assumption that traffic lanes will be maintained at a minimum width of 3.25 m to 3.35 m in width.

Notwithstanding the above, there may be an opportunity to implement cycling on Lakeshore Road without curb adjustments. This would be accomplished by reducing the inside through lane to a width of 3.05 m to 3.10 m and widening the curb lane to 4.0 m with the addition of bicycle sharrows markings.



This appears to be feasible based on existing road plans but would need to be confirmed in the field. The reduced width would be subject to safety considerations, including but not limited to speed and access / intersection proximity.

On the assumption that sufficient pavement width is available, these changes would be implemented through the road resurfacing program or independently when appropriate through the removal and re-application of the pavement markings.

### **Long Term**

The long term plan proposes an LRT system operating in mixed traffic through Port Credit (west of Greaves Avenue) and in a dedicated ROW east of Greaves Avenue. In this light, an adjustment to the ROW from the existing 35 m to 44.5 m (44.5 m is currently available through this section of Lakeshore Road, with the exception of property fronting the OPG and Metropolitan Toronto and Region Conservation Authority lands) will be required. There is no time frame at this point for the study required for the consideration of an LRT system. This information is important with regard to the implementation of future curb adjustments relative to the ROW needs for on-road bicycle lanes in the absence of an LRT system.

The long term plan also assumes implementation of on-road bicycle lanes throughout the corridor which will impact on-street parking through Port Credit (Broadview Avenue to Seneca Avenue). The study concludes that parking would need to be eliminated from one side of the road and relocated to side streets and / or new surface parking lots. On-street parking will be an issue even if an LRT system is not implemented, since the LRT is proposed to operate in mixed traffic through Port Credit. It is the addition of the on-road bicycle lanes that will precipitate the need for changes. Additional parking information is anticipated in 2011 upon completion of a parking management plan currently underway in Port Credit.

The study speaks to the ultimate removal of the centre turn lane from Seneca Avenue to the east City boundary and its replacement with a dedicated LRT ROW. This will require a detailed review of the feasibility of access consolidation throughout this section of Lakeshore Road.

## APPENDIX III: LOCAL AREA PLAN CREDITS

Figure No.	Description	Source
Figure 1	Port Credit locational context map and city structure elements	City of Mississauga Transportation and Works Department, Business Services Division, Geomatics
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# SOUTHDOWN

## 1.0 How to Read the Southdown Local Area Plan

Mississauga Official Plan is composed of a principal document and a series of local area plans, provided under separate cover. This is the Southdown Local Area Plan and provides policies for lands located in southwest Mississauga, as shown on Figure 1. It includes lands identified in the City Structure as Employment Area.

There are some instances where the policies and schedules of the principal document do not address all circumstances particular to Southdown. In these cases, the Southdown Local Area Plan elaborates on, or provides exceptions to the policies or schedules of the principal document.

The Southdown Local Area Plan must be read in conjunction with the Mississauga Official Plan principal document. Parts 1 to 4, the schedules and

the appendices of the principal document, are applicable to the Southdown area, unless modified by this local area plan. Thus, for example, the policies of this local area plan must be read in conjunction with the environmental, multi-modal, urban form and land use policies of Parts 2 and 3 of the principal document. In the event of a conflict, the policies of this local area plan take precedence.

The Southdown Local Area Plan incorporates the policies of the Southdown District Policies of Mississauga Plan (2003), modified to conform with Mississauga Official Plan (2010).

## 2.0 Purpose

The purpose of the local area plan is to provide clear direction for development and to address the unique circumstances within the area and adjacent communities.

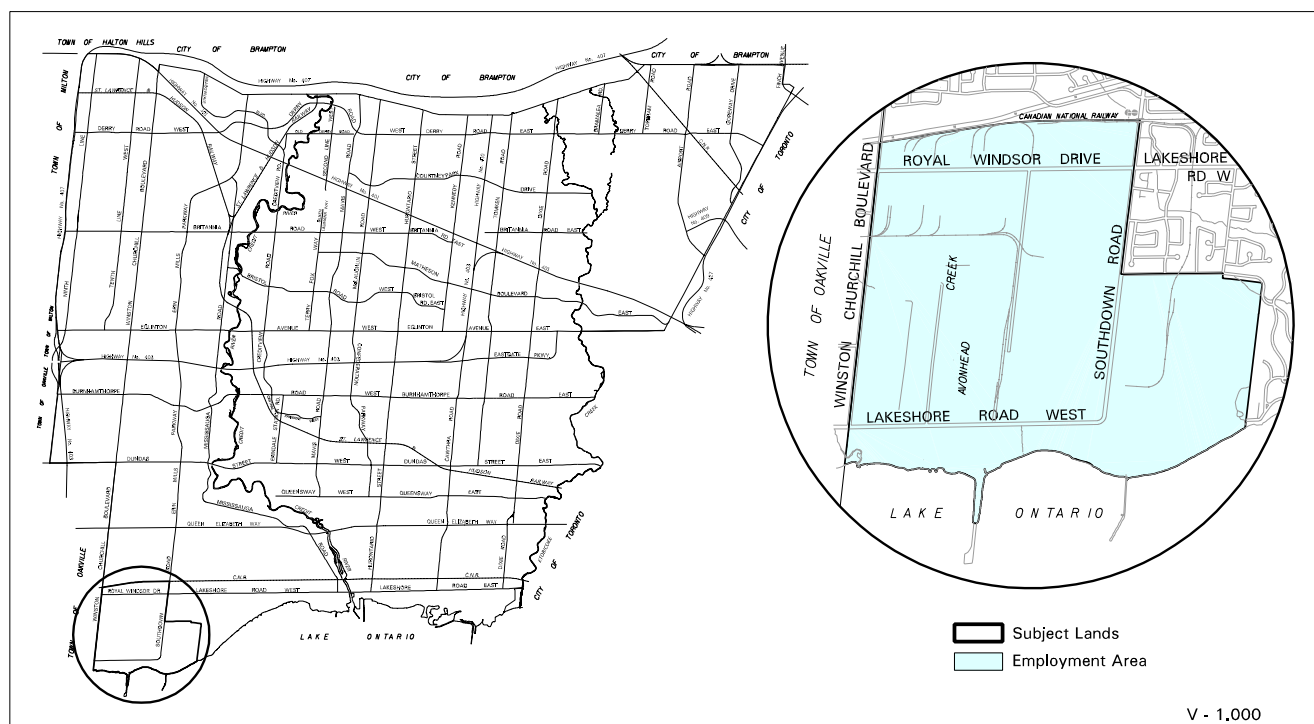


Figure 1: The Southdown Character Area is located in the southwest corner of Mississauga and is identified in the City Structure as an Employment Area.

Southdown provides a vital and necessary economic role for the City of Mississauga. It has been a long-standing area for industrial operations offering both employment and needed services. The policies contained in this document advance a continuation of this role with the intent to also improve the appearance and relationship of the area with the surrounding residential communities in a sustainable and environmentally sensitive manner.

In addition, the Local Area Plan recognizes that Lake Ontario represents one of the great natural features in Mississauga and needs to be elevated in profile and promoted as an important place for public gathering.

### 3.0 Historical Summary

Following the York (Toronto) purchase of land in 1805 from the Mississaugans, rural development officially commenced in the region. Over the course of the next 50 years, farming communities were established across the newly formed Toronto Township including the area in the southwest corner of the township, known today as Southdown. By the mid-19th Century, industrial landscapes, such as a major shipping dock and the completion of the Great Western Railway Line began to alter the direction of the quiet rural economies of Southdown and Clarkson.

Today, Southdown maintains a strong industrial character including long-standing industries such as the Petro Canada refinery which opened in the early 1940s as the British American Oil Company. Key to the urban design policies is recognizing the role of industrial heritage, inclusive of its natural resources, in forming the future physical character of Southdown over the next generation of urban development. Appendix A to the Southdown Local Area Plan provides a detailed historical overview.



Figure 2: The St. Lawrence Cement Company commenced operations in 1957 with the intent of becoming one of the most technically innovative industries of its kind in the world.



Figure 3: In 1995, the Mississauga leg of the Waterfront Trail System, extending from the Niagara Region to Gananoque, was opened.

### 4.0 Planning Context

The Official (Primary) Plan, approved in 1981, and the Clarkson-Lorne Park Secondary Plan approved in 1986, established the current configuration of Southdown.

City Plan, approved in 1997, established Southdown as an Employment District which contains industrial and business employment uses. Some of the existing industrial operations are major facilities with extensive amounts of outdoor storage (e.g. Petro Canada, St. Lawrence Cement).

The area is well served by a network of spur lines from the CNR mainline and accessibility to the Queen Elizabeth Way.

Southdown is a well established, stable Employment Area and is primarily designated for industrial uses. Of the approximately 670 hectares of land in Southdown, 380 hectares are designated Industrial, 180 hectares are designated Business Employment and 20 hectares are designated General Commercial.

According to the 2008 Vacant Lands report, there are 177 hectares of vacant land, representing 24% of all land in the Character Area. Over half of the approximately 20 vacant parcels are less than two hectares. However, there are six larger parcels between eight to 20 hectares and three parcels that are between 20 to 40 hectares. Of the 290 business sites, there are 25 vacancies, consisting of vacant units, buildings or floors.

The 2007 Employment Profile indicates there are 6 200 employees in Southdown, employed in approximately 270 businesses. Southdown's business profile is dominated by small businesses. Forty-one percent of all businesses employ fewer than five people. Firms with fewer than five employees represent only 4% of total employment, with almost 50% of employment found in businesses that employ between 20 to 299 people, a mid-range firm size.

There are many types of business activities in Southdown. However, approximately 80% of the businesses are within the manufacturing and wholesaling trade sector. Other business sectors represented include construction, transportation and warehousing, and **waste** management and remediation services.

The role of Southdown, to date, has been to provide a variety of employment opportunities. However, it is unique in that it abuts residential areas as well as the Lake Ontario Shoreline, resulting in both opportunities and constraints due to its location.

The impact of industrial operations on air quality has long been of concern to the surrounding residents.

Trucking and vehicular operations have contributed to noise and traffic impacts, as well as to the taxed air shed. Extensive outdoor processing and storage activity has detracted from achieving quality urban design within the area. A power generating facility, approved in 2001, is proposed on the east side of Winston Churchill Boulevard, south of Royal Windsor Drive.

The City's Waterfront Parks Strategy has identified Lakeside Park and Park 389 as priority parks, where detailed concept plans will be developed to outline potential uses and features.

Southdown also contains a number of contaminated or potentially **contaminated sites** and closed or current **waste disposal sites**.



Figure 4: A defining aspect of Southdown's character is its industrial heritage (shown above: the St. Lawrence Cement Company).



Figure 5: The waterfront trail along Lakeshore Road West offers public access and use of Lake Ontario.



## 5.0 Natural Environment

Southdown, consisting of approximately five and a half square kilometres, contains an abundance of environmental features. The area is defined by the **sub-watersheds** of Avonhead Creek, Clearview Creek, Joshua's Creek, Lakeside Creek, Sheridan Creek and Lake Ontario and is subject to the Southdown District Master Drainage Plan.

Joshua Creek has been identified as an **Environmentally Sensitive Area** by the Region of Halton and is considered largely undisturbed and provides warmwater fish habitat.

Portions of Avonhead Creek, Lakeside Creek and Clearview Creek have been channelized within the area. Clearview Creek supports a warmwater fish habitat and is connected to several upland woodlands. It also serves as a wildlife corridor and linkage to other features within and beyond Southdown, including Lake Ontario, Joshua Creek and the Town of Oakville.

Lake Ontario retains much of its original shoreline with the exception of a prominent pier element protruding out from the shoreline built to move cement products from ships harbouring along side it. The shoreline is characterized by a natural beach west of the Petro Canada lands, which includes some of the last remaining cobble beaches along Lake Ontario. Shoreline erosion protection works are located east of the Petro Canada lands. The lands along Lake Ontario are partially within the Regulatory Shoreline as defined by the 100 year floodplain, including wave uprush, the 100 year erosion limit and the dynamic beach hazards. A variety of fish habitat is provided in the near shore areas of Lake Ontario.

The shoreline at the Mississauga/Oakville municipal boundary is also characterized by a plantation woodland, which is considered the last remaining woodland of any size along the Mississauga shoreline. The woodland is also associated with the valley corridor of Joshua Creek.

Few original woodlots remain from pre-land clearing days, however, new ones have grown in areas where industrial activity and uses have not been established. This is evident along Winston Churchill Boulevard, the waterfront area south of Lakeshore Road West (also known as Highway 2), and the interior. The most significant of the woodland areas is the red maple-red oak forest located south of existing Orr Road.

These policies designate the three **watercourses** and the length of the Lake Ontario shoreline as Greenbelt.



Figure 6: Clearview Creek as seen from Lakeshore Road West.

## 6.0 Built Environment



Figure 7: Aerial photograph of Southdown Character Area.

Existing development consists of low-rise buildings, multi-unit developments, office uses and industrial uses that require outdoor processing or storage, including two large land extensive users, Petro Canada and St. Lawrence Cement. Retail commercial uses are located at the northwest and southwest corners of Royal Windsor Drive and Southdown Road, including Clarkson Crossing Commercial Centre. Residential development borders the area on the north and east sides, along with industrial/commercial uses and vacant lands to the west, in the Town of Oakville. The Clarkson GO station is located at the CNR tracks and Southdown Road, supporting both the residential and employment communities.

The buildings offer minimal street enclosure and parking is generally located in the front, with some landscaping. In some areas, the streets do not have sidewalks, pedestrian amenity and sufficient **streetscape** treatment to support and encourage walking and cycling.

Both Southdown Road and Winston Churchill Boulevard provide connections to Lake Ontario. This natural connection is reinforced by the existing nurseries, agricultural uses, open fields, trees and other landscape features.



Figure 8: Northwest corner of Royal Windsor Drive and Southdown Road.



Figure 9: Existing low-rise office building.

There are attractive views of the lake from Lakeshore Road West. Walkways and bike paths connect the two waterfront parks, providing public access to the lake and a framework for an active waterfront system connected to the rest of Mississauga. The St. Lawrence Cement Company conveyor bridge spans Lakeshore Road West and is a significant component of the area's landscape.

## 7.0 Vision

The vision for Southdown is to continue to provide a place of employment (inclusive of current land uses) and to evolve in the long term, to embrace other employment uses such as a mix of manufacturing, research and development and offices. The vision contemplates a denser grid of streets and blocks, an improved public realm and land uses that recognize the distinct location, its proximity to neighbouring residential areas, and as an important gateway into the City of Mississauga.

These Policies are also intended to address the interface between the employment and residential



Figure 10: Southdown will continue to be a place of employment, evolving in the longer term to include a range of other compatible land uses.

communities, as well as the visual and functional impact of development by:

- promoting a distinctive urban form and **streetscape**;
- limiting development which may require extensive outdoor storage and/or processing; and
- limiting transportation and other related uses.

To do so, these policies encourage new development as well as redevelopment to accommodate industrial activities consisting of a mix of manufacturing, research and development and office uses primarily within enclosed buildings. To achieve a high quality business image, specific design policies apply along arterial roads with Royal Windsor Drive and Lakeshore Road West functioning as gateways into Mississauga.

The vision also acknowledges the importance of the Lake Ontario waterfront. It advocates a high quality environment with improved urban design consisting of pedestrian and transit supportive development which is environmentally sensitive and sustainable.

The waterfront parks, Lakeside Park and Park 389 (not yet named), are identified as priority parks for which a concept plan will be developed to outline potential linkages, cultural heritage resources, features and uses. As such, specific urban design policies apply to Winston Churchill Boulevard and

Southdown Road to provide a connection to the waterfront parks as well as Lake Ontario.

Existing industrial operations, which may have extensive outdoor processing or storage areas, will be permitted to continue, however, the long term vision is to develop certain of the vacant lands in accordance with this vision.

The proposed Orr Road extension from Southdown Road to Winston Churchill Boulevard and the proposed Avonhead Road extension from Royal Windsor Drive to Lakeshore Road West will improve accessibility within the area. Such future improvements are expected to stimulate the development of vacant lands.

All development should ensure that there is minimal impact on adjacent residential lands.

## 8.0 Urban Design Strategy

This section provides detailed guidance with respect to how the Vision may be achieved through new development and redevelopment of existing sites.

These principles establish priorities for community identity and guide site organization, built form and landscaping qualities, with an emphasis on development that is transit and pedestrian supportive. Emphasis is also placed on the importance of connectivity to the waterfront with overall enhancement of the pedestrian environment, encouraging accessibility, direct linkages and area vibrancy. The following statements assist in guiding the Vision:



Figure 11: New development should support a pedestrian and transit friendly environment along key **Corridors** and intersections in the Southdown Character Area, as illustrated above.



- a. Encourage public accessibility and reinforce the character, vitality and recreational nature of the Lake Ontario waterfront as an important amenity for Southdown, adjacent residential areas, and the city and the region as a whole. Further, enhance connectivity for pedestrians and bicycles, where possible, and reinforce view corridors connecting to the waterfront;
- b. Create a permeable network of streets and blocks, among the introduction of new streets;
- c. Provide a pedestrian and cycling environment that allows safe and comfortable movement through the area while having regard for industrial operations;
- d. Encourage and facilitate transit trips among employment buildings, neighbouring residential areas, the Lake Ontario waterfront and other area amenities;
- e. Create community identity and character by promoting distinctive built form, a high quality public realm and landscaping that define the image of the community. This would include provision for significant gateways on Royal Windsor Drive and Lakeshore Road West through built form and/or landscape components. Enhance Southdown Road and Winston Churchill Boulevard as both visual and physical connections to Lake Ontario and the waterfront parks by means of **streetscape**, scenic views, focal points, view corridors and improve pedestrian and cycling access;
- f. Development should have regard for heritage resources and features, including those with



Figure 12: Buildings which are carefully articulated and massed to promote distinctive character and community identity are encouraged, as illustrated above.



Figure 13: Major traffic corridors should be complemented, where possible, by rows of street trees, well designed sidewalks and coordinated **streetscape** treatment help to create a pedestrian friendly environment.

industrial significance, to form the evolving basis of a distinct community character and identity;

- g. Mature trees and vegetation should be recognized for providing important environmental benefits and contributing to aspects of roadside character along Royal Windsor Drive, Lakeshore Road West and the presentation of the Lake Ontario waterfront;
- h. Development will consider adjacent residential communities regarding scale and built form, and provide appropriate landscape buffers and interfaces between uses; and
- i. Development should consider sustainable building practices (i.e. green roofs and development performance standards), environmentally enhanced construction standards, energy efficiency and site



Figure 14: Green roofs ameliorate the urban heat island effect and support natural habitat.



development strategies to reduce stormwater run off, hard surface pavement and the heat island effect.

## 9.0 Urban Design Policies

Based on the foregoing Urban Design Strategy, the following urban design policies implement the vision for Southdown and focus on the arterial corridors.

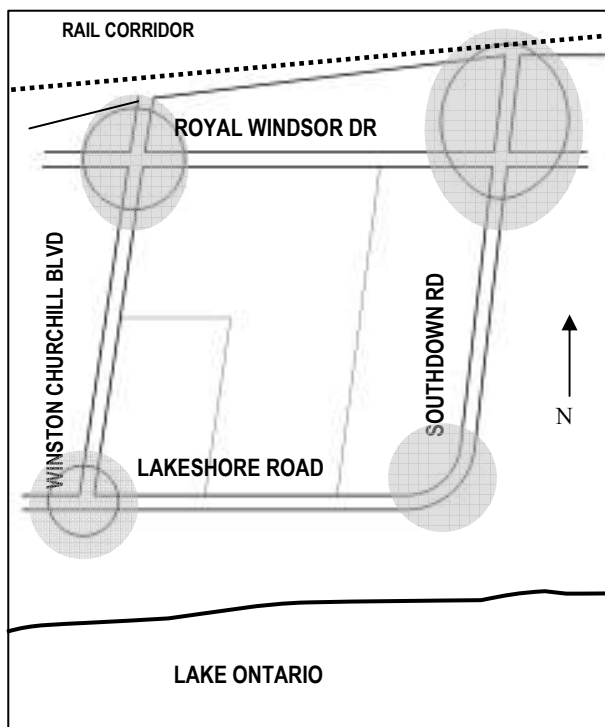


Figure 15: Development at major intersections should have substantial built form, with landscape treatments and landmark quality architecture.

### 9.1 Community Pattern

Streets, Blocks and Circulation:

- A streets and blocks network should be created to accommodate future industrial and/or business employment development, taking into consideration the needs of truck transport and related activities;
- Continuous sidewalks and open spaces should be provided on new public streets that link into the greater arterial road network and the

Waterfront Trail located along the edge of Lake Ontario;

- New streets should support a pedestrian and transit friendly environment connected to the Waterfront Trail, where possible;
- At entry locations, encourage development to form gateways that are substantial in built form with landmark architectural character and quality and special landscape treatment (see key plan: Gateway Locations);
- A **streetscape** master plan should be undertaken to coordinate treatments for the public realm;

Cultural Heritage Resources:

- Development will be compatible with cultural heritage properties by respecting their massing, scale, built form qualities and setbacks, and by providing ample buffer areas with appropriate urban design, architectural and landscape treatments necessary to preserve important aspects of a property's setting and sense of place;
- Develop environmentally-sensitive means to connect into the system of **watercourses**, such as Avonhead and Clearview Creeks, to encourage public awareness regarding



Figure 16: The Robertson Farm (located north of Lakeshore Road West and east of Winston Churchill Boulevard) is an example of agricultural heritage in Southdown and is listed on the City's Heritage Register as a heritage designated property.

sensitivity of these mature natural settings and ecosystems. Provide continuous pedestrian walkway systems along these **watercourses**, where appropriate;

- h. Opportunities to express forms of interpretation of local cultural heritage resources should be explored as development occurs on or adjacent to properties of cultural heritage value; and

#### Sustainable Design:

- i. Build upon the unique qualities of the existing streets where distinctive green elements and sustainable features such as swales and wetlands are already present. Enhance and reinforce these attributes and integrate them into proposed new development sites.

## 9.2 Site Organization

- a. Buildings should be oriented to have a strong relationship to the street with most of the required parking located at the rear or the sides of the building(s);
- b. A generous amount of landscape treatment should be promoted between the building's face and the edge of the right-of-way. The allocation of parking within this setback area should be limited to visitor's parking, and parcelled into small areas with well landscaped islands and pedestrian walkways connecting to principal building entrances;
- c. Encourage the use of shared access points between adjacent properties to minimize conflicts with public sidewalks while promoting continuous and attractive **streetscapes**. Consider shared truck service routes and common points of site entry that lead to rear service areas, and which minimize conflicts with pedestrian movement;
- d. Main building entrances should be oriented to the street, and clearly articulated and linked to pedestrian walkway systems, in particular, the public sidewalk. The design of building



Figure 17: Buildings should have a strong relationship to the street with most parking located at the rear or sides of the buildings.



Figure 18: Development located at the intersection of two roads require upgraded elevations with office components located to reinforce corner conditions and overall presentation to the street.



Figure 19: Development along Royal Windsor Drive should have regard for existing pedestrian contexts, and contribute positively to the **streetscape** through animated façades that engage the street and support transit, as illustrated above.

entrances should reinforce building identification and articulation of the exterior form. Large manufacturing, warehousing and industrial facilities are encouraged to orient associated office components to address the public street;

- e. Buildings should provide for pedestrian comfort through the incorporation of such elements as canopies, arcades and colonnades at ground level;
- f. Prestigious buildings should be located on corner sites with the highest order of buildings located at the major intersection corners;
- g. Buildings on corner sites should incorporate special architectural treatment along both street frontages with offices and entrance elements located directly at corners;

#### Royal Windsor Drive:

- h. New buildings along Royal Windsor Drive should support a transit and pedestrian environment to create a strong and continuous street edge that has regard for adjacent community patterns. New development should contain uses and spaces that encourage pedestrian activity and animate the street. Parking areas should be allocated to rear, sides or below-grade to achieve **streetscape** objectives; and

#### Parking and Loading Areas:

- i. Parking lots should have defined pedestrian circulation systems leading conveniently to main and employee entries.

### 9.3 Building Mass and Articulation

- a. Buildings brought forward to the street should be a minimum of two storeys in height and contain administrative/office and retail functions associated with manufacturing, warehousing and industrial uses;
- b. Façades visible to the street should have upgraded elevations and active frontages with sufficient window fenestration and door

entrances to animate the street edge. Buildings should also be designed with prominent roof forms, elaborated parapets, upper level window bands, architectural projections, canopies or



Figure 20: Buildings in Southdown should engage street frontages with upgraded elevations and materials to encourage visual interest and direct connections to the public sidewalk.



Figure 21: Buildings brought forward to the street should be a minimum of two storeys in height and contain administrative office or retail functions associated with manufacturing, warehousing and industrial uses, as illustrated above.

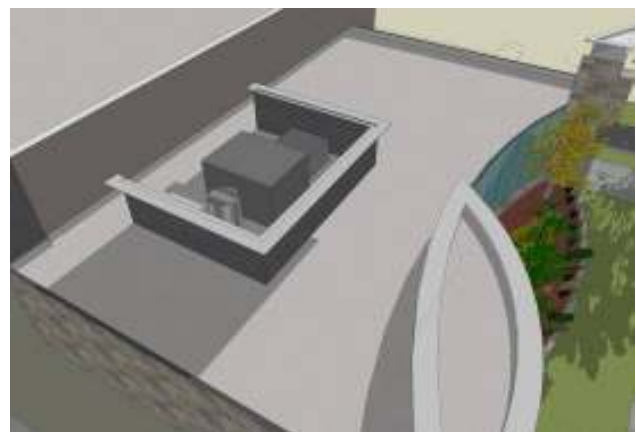


Figure 22: Provide architectural screening around roof-mounted mechanical equipment to minimize the visual impact from the street.



shading devices, enhanced exit doors and changes in materials;

- c. Notwithstanding the placement of the main entrances and major office components, equal attention to detail and quality of materials should be given to arterial and local street elevations to establish a consistent and quality image; and
- d. Buildings, landscape elements and plantings should screen views of facility operations and storage areas from the street(s). As an alternative, in situations where such activity and operations are visual features of the area, consideration should be given to developing a visual foreground that softens and enhances those features.

## 9.4 Pedestrian Environment

Public Sidewalks, Bicycle Facility, and Private Walkways:

- a. Continuous sidewalks should be provided along all streets. Comfortable and barrier free walkways should be provided from buildings to public sidewalks and transit stops. In situations where there are no public sidewalks within the public street allowance, provide walkway connections to the public roadway. Walkways should be uninterrupted by parking or drive aisles and consist of paving materials other than asphalt;
- b. Along arterials roads, consideration will be given to providing sidewalks on both sides of the street;
- c. Transit stops and stations should also include places for seating and weather protective areas;
- d. Consideration should be given to providing a second continuous pedestrian system of walkways, lighting and other amenities located completely within development that connects to adjacent properties, particularly in instances where multiple units are proposed along the street frontage;



Figure 23: A transit stop conveniently located to an adjacent office building provides weather protection and seating.



Figure 24: Generous pedestrian sidewalks and a double row of trees provide rhythm, structure and comfort for pedestrians, and helps screen adjacent industrial operations.



Figure 25: Boulevards in parking areas should incorporate soft landscape materials and shade trees with design elements and systems that mitigate storm water run-off.



#### Landscaping:

- e. Planting should visually enhance individual sites, screen parking and loading areas, while enabling views of buildings and creating a consistent landscape treatment along streets. It should be appropriate to the **streetscape** while improving the appearance of parking areas and internal roads. Landscaping should contribute to the visual continuity of the street edge;
- f. Shade tree planting and landscaped areas should be provided on the site to break up the parking areas, and in areas where pavement is not required;
- g. Encourage environmental revitalization and restoration of sites through planting and other landscape treatments;
- h. Appropriately scaled and landscaped outdoor amenity areas should be provided in an area suitable for users within the development, and should include an area that is protected from the elements;
- i. Opportunities for strategic **streetscape** and landscape improvements along Lakeshore Road West with significant preservation of trees and vegetation, and for enhancement and additional greening along this route should be explored as development occurs;

#### Pedestrian Amenity:

- j. Development should provide for the storage of bicycles on-site to encourage the use of alternative modes of transportation to, and within the area. These areas need to be protected from the natural elements, in addition to providing clear and safe access to the bicycle path network;
- k. The inclusion of public art is encouraged in areas of social gathering, as part of built form



Figure 26: Opportunities for environmental restoration and landscaping of sites should be considered in new development, as illustrated above.



Figure 27: On-site bicycle storage provides choices for alternative modes of transportation.



Figure 28: Pedestrian scale lighting provides illumination for public amenity areas, sidewalks and pathways, as illustrated above.

expression, or adjacent to the public right-of-way. Such installations should be encouraged to contribute to the development of a distinctive character while drawing upon the unique qualities of the Southdown Character Area in theme;

#### Signage & Lighting:

- l. A continuous system of pedestrian lighting should be investigated to illuminate sidewalks and multipurpose trails to ensure safe access to and along the waterfront. Particular attention should be given to the development of a pedestrian lighting system along major streets, inclusive of major traffic corridors, to be attractive, distinct and separate from roadway lighting;
- m. Lighting should be provided on pedestrian paths and outdoor amenity areas on private lands. Landscape lights should be placed to avoid spill-over on adjacent properties;
- n. The design and location of way finding and building signage should be coordinated throughout the site, and integrated with its landscape treatment and building façade(s) through appropriate scale, colour, materials and consistency; and
- o. The design and location of entry features and waterfront park signage should be coordinated and integrated into the **streetscape** design.

## 10.0 Land Use

### 10.1 Business Employment

- a. Existing industrial operations, including existing outdoor storage areas, will be permitted to continue.
- b. Outdoor storage of raw materials such as, but not limited to, salt and sand, will be within enclosed containers, a structure with a minimum of three sides and a roof, or otherwise

covered to mitigate the effects of dust or particulate matter that may be detrimental to other businesses or neighbouring areas. Expansions to the outdoor storage areas will be permitted in accordance with the Business Employment Policies.

- c. Notwithstanding the Business Employment policies of this Plan, the following uses will not be permitted:

- trucking terminals;
- outdoor storage of motor vehicles exceeding 3 000 kg in weight; and
- **waste processing station** or **waste transfer stations** and composting facilities.

### 10.2 Industrial

- a. Existing industrial operations, including existing outdoor processing and outdoor storage will be permitted to continue.
- b. Notwithstanding the Industrial policies of this Plan, the following uses will not be permitted:
  - trucking terminals;
  - **waste processing station** or **waste transfer stations** and composting facilities; and
  - expansions to or new outdoor processing; and
- c. Outdoor storage of materials, except extracted resources, such as, but not limited to, salt and sand, will be within enclosed containers, a structure with a minimum of three sides and a roof, or otherwise covered to mitigate the effects of dust or particulate matter that may be detrimental to other businesses or neighbouring areas.

## 10.3 Prohibited Uses

Notwithstanding the Business Employment and Industrial policies of this Plan, for the lands fronting on Royal Windsor Drive, Lakeshore Road West, Winston Churchill Boulevard and Southdown Road, the following uses will not be permitted:

- adult entertainment establishments;
- body rub establishments;
- cardlock fuel dispensing facilities;
- motor vehicle storage, commercial motor vehicle storage, truck tractors and/or tractor trailer storage;
- motor vehicle body repair facility;
- contractor service shop;
- contractors yard; and
- vehicle pound facility.

## 11.0 Transportation

### 11.1 Road System

- a. The intersection of the proposed Orr Road extension at Southdown Road will be designed in such a manner as to restrict through traffic from Southdown to the residential community to the east.
- b. The location of the proposed Orr Road extension from Southdown Road to Hazelhurst Road will be determined during the processing of development applications.
- c. The width of the proposed Orr Road extension from Southdown Road to Winston Churchill Boulevard will be a minimum of 24 m.

## 12.0 Special Site Policies

There are sites within Southdown that merit special attention and are subject to the following policies.

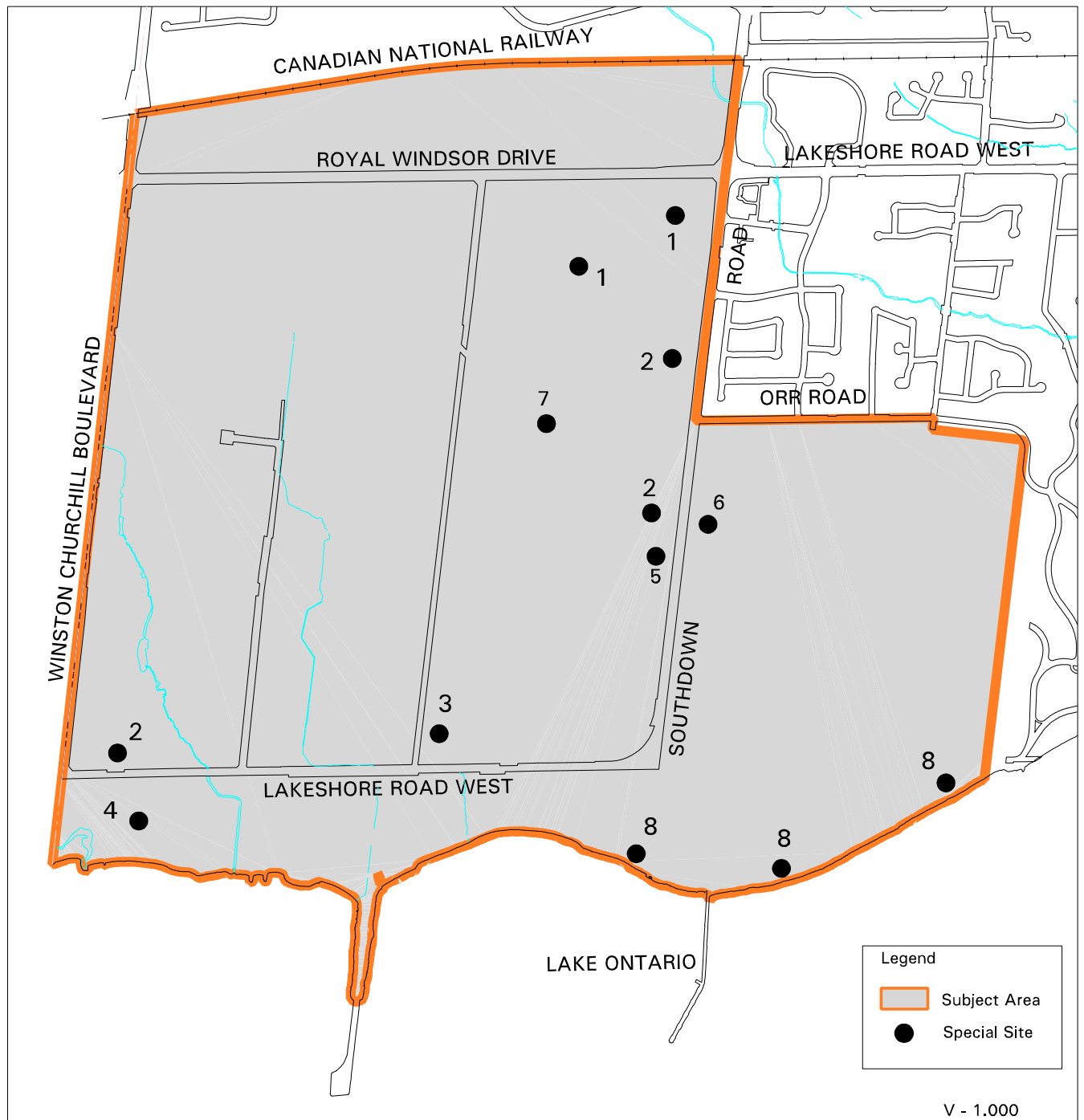
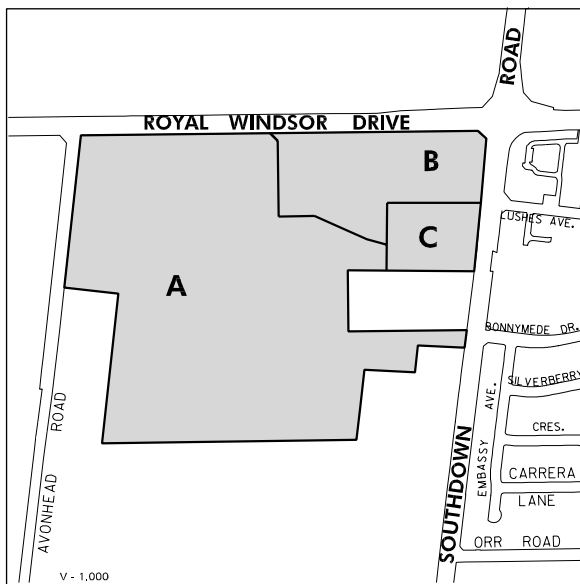


Figure 29: Location of Special Sites within the Southdown Local Area.



## 12.1 Site 1



### 12.1.1 Area A

The lands identified as Area A are located on the south side of Royal Windsor Drive and west of Areas B and C.

Notwithstanding the Business Employment designation, the uses permitted on the lands will be subject to the following additional policies:

- a. access will be available to Southdown Road opposite Bonnymede Drive and to Royal Windsor Drive opposite the City Works yard access; and further additional points to Royal Windsor Drive, Southdown Road and Avonhead Road may be considered by the City at the time of site plan approval when the ultimate development plan has been determined; and
- b. prior to site plan approval for any part of Area A, a concept plan may be required to indicate how adjacent lands could be developed with respect to ensuring that all lands have adequate access and parking, and that traffic and pedestrian circulation is adequately provided.

### 12.1.2 Area B

The lands identified as Area B are located at the immediate southwest corner of Royal Windsor Drive and Southdown Road.

Notwithstanding the Mixed Use designation, the following additional policies will apply:

- a. development at the intersection of Royal Windsor Drive and Southdown Road is encouraged to have generous open spaces with landscaping to enhance the **streetscape**; and
- b. access will be available to Royal Windsor Drive opposite the City Works yard access. Additional access points to Royal Windsor Drive may be considered by the City at the time of site plan approval.

### 12.1.3 Area C

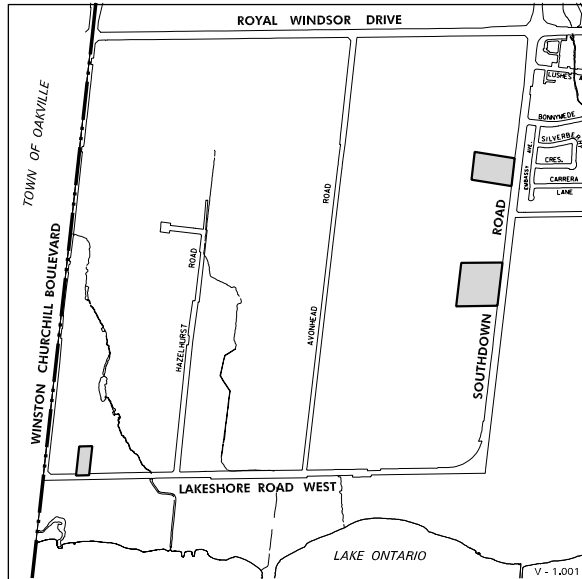
The lands identified as Area C are located on the west side of Southdown Road, immediately to the south of Area B.

Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. permitted uses will only include retail warehouses, which generally consist of home furnishings, home accessories and an automotive centre;
- b. in addition to the above, community infrastructure and all types of restaurants, except for convenience restaurants, will be permitted;
- c. business employment uses;
- d. uses not permitted within this designation include department stores, supermarkets and drug stores, and other commercial uses which, either individually or collectively, could create an impact on commercial centres; and
- e. outdoor storage and display areas should not be visible from major roads, parks, greenbelt or residential lands. Appropriate setbacks,

screening and buffering will be required to lessen the visual impact of the facility. Display areas are to be an integral part of the overall site design and evaluated based on their visual impact on the **streetscape**.

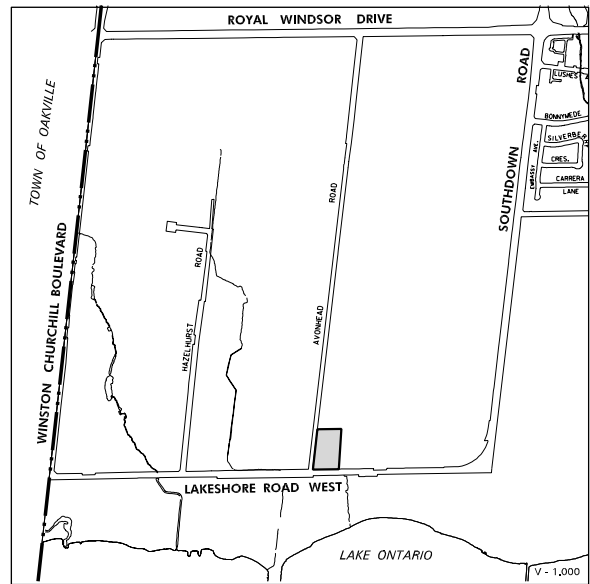
## 12.2 Site 2



12.2.1 The lands identified as Special Site 2 consist of three areas, two are located on the west side of Southdown Road, south of Royal Windsor Drive and the other is located on the north side of Lakeshore Road West, east of Winston Churchill Boulevard.

12.2.2 Notwithstanding the Business Employment designation, the lands may also be used for a garden centre.

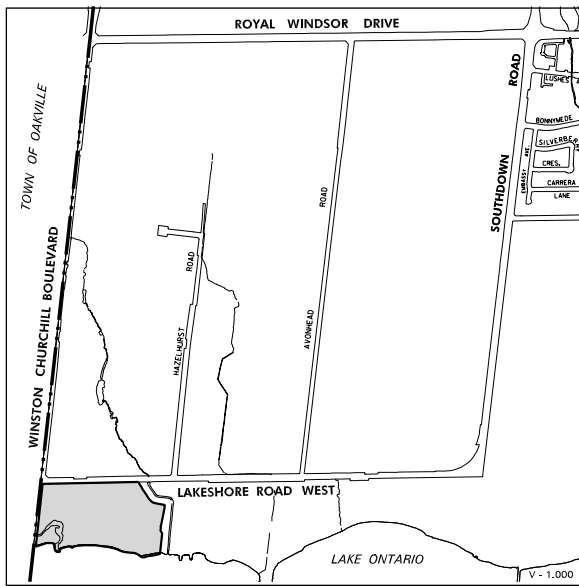
## 12.3 Site 3



12.3.1 The lands identified as Special Site 3 are located on the north side of Lakeshore Road West, west of Southdown Road.

12.3.2 Notwithstanding the provisions of the Utility designation, the lands may also be developed for a Community Recycling Centre with outdoor storage and an accessory retail store for reusable goods.

## 12.4 Site 4

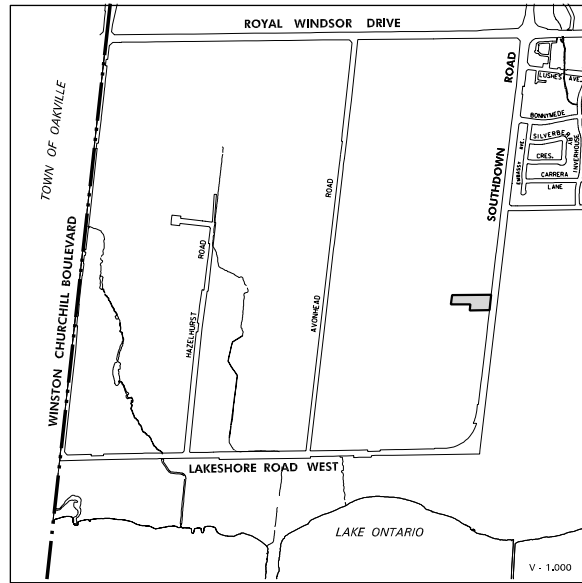


12.4.1 The lands identified as Special Site 4 are located on the south side of Lakeshore Road West, east of Winston Churchill Boulevard.

12.4.2 Notwithstanding the provisions of the Public Open Space designation on these lands, the following additional policies will apply:

- a. in addition to the uses permitted in the Public Open Space designation, the following uses will also be permitted within the existing structure:
  - **secondary offices;**
  - commercial uses that are accessory to the park;
  - conference centre;
  - banquet hall;
  - conservatory/greenhouse complex;
  - commercial school; and
  - restaurant; and
- b. any proposed structure will be limited in size and to a design compatible with the existing structures and will be designed and located in a manner that minimizes the removal of forest cover.

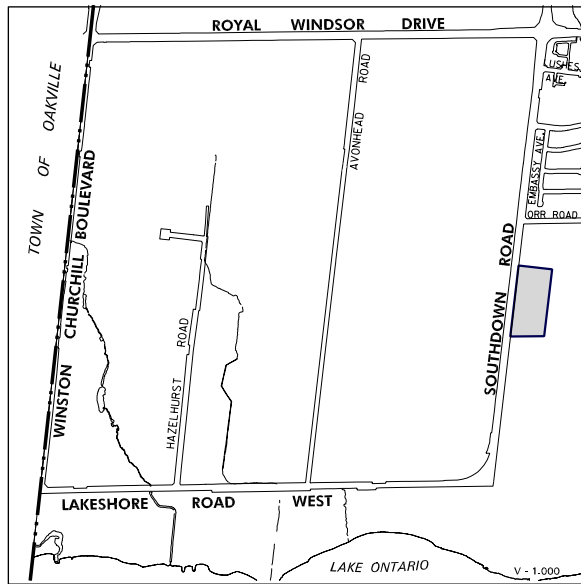
## 12.5 Site 5



12.5.1 The lands identified as Special Site 5 are located on the west side of Southdown Road at 566 Southdown Road.

12.5.2 Notwithstanding the provisions of the Business Employment designation, the lands may also be used for a gas processing operation including outdoor processing activities and outdoor storage and may include the temporary parking of commercial motor vehicles.

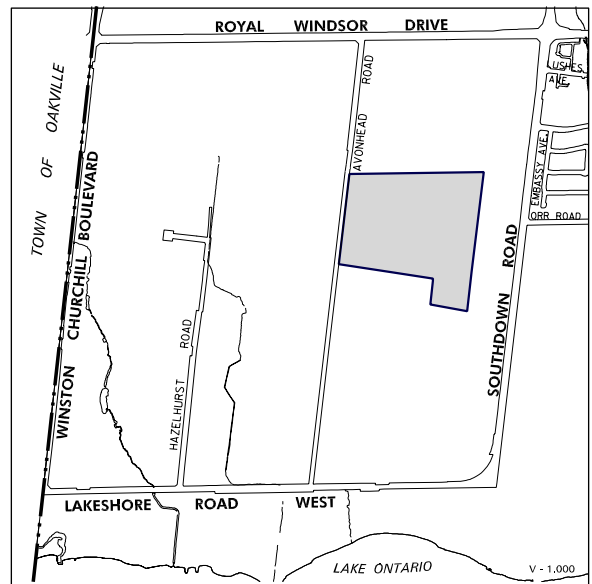
## 12.6 Site 6



12.6.1 The lands identified as Special Site 6 are located on the east side of Southdown Road, south of Orr Road.

12.6.2 Notwithstanding the provisions of Section 10.3 Prohibited Uses, the lands may also be used for a cardlock fuel dispensing facility.

## 12.7 Site 7

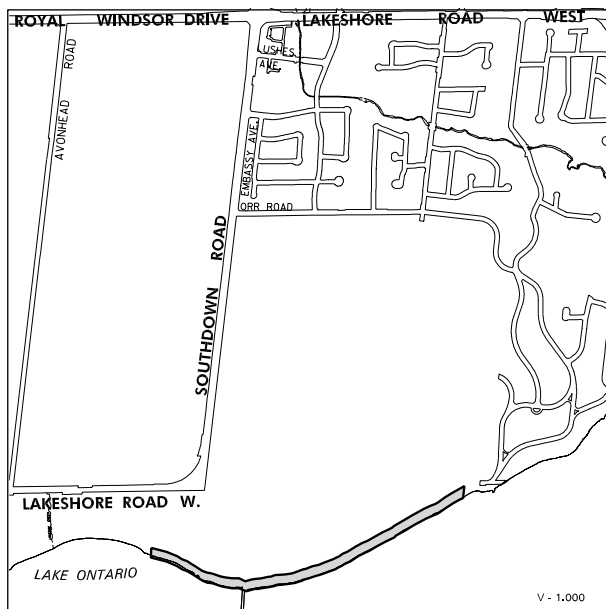


12.7.1 The lands identified as Special Site 7 are located on the west side of Southdown Road, south of Royal Windsor Drive.

12.7.2 Notwithstanding the provisions of the Industrial designation, only the existing land farming operation will be permitted as an accessory use to the existing lubricants centre located at 385 Southdown Road.



## 12.8 Site 8



12.8.1 The lands identified as Special Site 8 are located along the Lake Ontario shoreline, from Lakeside Park to the eastern district boundary.

12.8.2 Notwithstanding the provisions of the Greenbelt designation, the following additional policies apply:

- a. existing structures and uses are permitted;
- b. expansion to existing structures or new development are subject to the Natural Hazard policies of this Plan; and
- c. detailed studies to allow expansions or new development will be subject to satisfying the City and the appropriate Conservation Authority.

## Appendix A: Evolution of Arterial Roads

The following concepts demonstrate how change could be accommodated along principal arterial roads which define Southdown over a 25 year time frame. Each illustration takes into consideration a number of staged improvements geared to supporting transit, pedestrians, alternative modes of transportation such as cycling, a coordinated public realm, and a **streetscape** treatment with an articulated built form to define street edges. Also shown are improved amenities and accessibility to the Lake Ontario waterfront. These changes result in improved image and presentation of Southdown.

## 1. Southdown Road



### Commentary:

#### Existing Condition

The view shown in View 1 is looking south to Lake Ontario, from Orr Road, on the east side of Southdown Road. This portion exists as a two lane road (with a left turning lane in the middle), curb lined, sodded boulevards, and a conventional 1.5 m sidewalk on the east side only. A right turn lane services Orr Road with unsheltered bus stops located on both sides of Southdown Road. Power wires are overhead, but all other utilities are buried underground, or located at the rear of properties.

#### Ten-to-Fifteen Year Condition

The early phase of redevelopment within the Southdown Road right-of-way should consist of modest boulevard upgrades including (View 2):

- soft landscape improvements, a bicycle facility within the right-of-way and additional transit stops to encourage transportation alternatives;
- private development brought closer to the street edge with a generous landscape setback; and
- new development with front doors and windows oriented to the streets, and visitor's parking minimized along frontages with appropriate landscape screening.



#### Twenty-Five Year Vision

The long term vision should include significant changes in both the public and private realms (View 3):

- overhead utility lines relocated to below-grade (and the utility poles should be removed), with new street lighting;
- designated bicycle facilities extended in each direction of the roadway, and transit shelters strategically positioned at high volume bus stops;
- boulevards planted with street trees in a soft landscape permeable surface;



Figure 30: Over time, Southdown Road will intensify with a built form that frames the street and provides a quality public realm and pedestrian environment.

- generous sidewalks to accommodate pedestrians and phased in, on-street parking for convenience, and to help mitigate travel speeds for an improved pedestrian environment;
- on private lands, two to three storey buildings with harmonized setbacks creating consistent **streetscape**;
- buildings engaging the public realm with active, high quality façades through front entries and windows to support a vibrant public realm; and
- buildings addressing the street with an appropriate setback, a public realm coordinated with high quality **streetscape**, street furniture, pedestrian amenities, way finding signage, street trees and pedestrian scale lighting to augment the area's character.



## 2. Lakeshore Road West



Figure 31: Over time, Lakeshore Road West will intensify with a built form that frames the street and provides a quality public realm and pedestrian environment.

### Commentary:

#### Existing Condition

The view shown in View 4 is looking west toward Oakville on Lakeshore Road West:

- the existing two lane road (left turn lane in middle), has been planted with continuous rows of trees on both sides of street;
- on the south side, west from the access drive into Lakeside Park, continuous engineered curb (sodded boulevard) three foot wide splash pad;
- on north side, soft gravel shoulder, shallow ditch and indigenous grass and ornamental front yards, and right turning lanes servicing several intersecting streets; and
- no pedestrian sidewalk in the ROW, but a bike and foot path runs on south side parallel to Lakeshore Road West behind the continuous rows of trees.

#### Ten-Fifteen Year Condition

The early phase of redevelopment within the Lakeshore Road West ROW should consist of the following (View 5):

- significant boulevard upgrades including soft landscape improvements, bicycle facilities and lay-by parking within the roadway and more sheltered bus stops to facilitate transit use and travel to lakefront;
- relocated overhead wiring to below-grade, new streetlighting combining pedestrian scaled lighting, coordinated street furniture and enhanced landscape treatment to transform the area into an attractive destination;
- significant private development is not anticipated along the corridor, but, where possible, place it closer to street edge in order to provide definition, enclosure and a **streetscape** supporting street activity and attractive public spaces; and

- highest quality architecture with active façades and front entrances and windows oriented to street, placing visitor parking at sides or rear of buildings.

### Twenty-Five Year Vision

In the long term, Lakeshore Road West should change considerably (View 6):

- greater intensification of **streetscape** amenities should reinforce the pedestrian function;
- continuous rows of boulevard trees planted in a soft landscape permeable surface, with sidewalks constructed on both sides of the street to accommodate increased pedestrian traffic;
- further beautify the street with landscape features such as a landscaped median to help slow traffic speeds and make pedestrians primary street users;
- new development to consist of two to three storey buildings along north side to frame, enclose and reinforce a consistent pedestrian scaled **streetscape**;
- increased front doors, principal window fenestration and high quality elevation design; and
- buildings minimally setback, with landscape that integrates with **streetscape** character, and consider at grade retail to support future pedestrian activity.

### 3. Royal Windsor Drive



Figure 32: Over time, Royal Windsor Drive will intensify with a built form that frames the street and provides a quality public realm and pedestrian environment.

#### Commentary:

##### Existing Condition

The view shown in View 7 is looking west to Oakville, from Avonhead Road, on the south side of Royal Windsor Drive:

- Five lane road (left turn lane in middle), curb lined on both sides, grass boulevards, narrow 1.5 m wide sidewalk on north side and 0.75 m wide concrete splash pad on south side;
- grass swales, overhead power lines, varying industrial building types dominating street character; and
- right turn lane located at intersecting streets, with unsheltered bus stops on both sides.

##### Ten-Fifteen Year Condition

If Royal Windsor Drive is to evolve into a pedestrian 'main street' character within an industrial park theme, the following should proceed (View 8):

- upgraded public realm with wide pedestrian sidewalks and landscape improvements such as landscaped medians, transit shelters, benches, etc.;
- bicycle facility within roadway encouraging transit alternatives, overhead wiring replaced by attractive street lighting;
- phase in parallel service road across development frontages to form continuous network, with some parking in front of buildings, in an attractive manner;
- new two to three storey buildings reinforcing future service road street edge on both sides, to occur incrementally transforming street into more pedestrian one, with more walking and cycling; and
- significantly high quality of architecture with front doors and principal windows oriented to streets.

## Twenty-Five Year Vision

Royal Windsor Drive should continue to transform, with significant changes occurring within the public and private realms (View 9):

- attractive **streetscapes** that promote pedestrian environments common to 'typical' main streets;
- additional landscape to augment public boulevards with continuous street trees, horticultural displays in planter boxes, decorative fencing and array of street furnishings supporting pedestrian usage;
- provide wide sidewalks and plant street trees in continuous raised beds;
- consider a central landscaped median to beautify and visually narrow the street, slowing down traffic and creating a pedestrian oriented street; and
- continue new two to three storey buildings at edge of service road to providing street definition, and active façades with principal front entrances, transparent window fenestration and high quality elevation design.



#### 4. Winston Churchill Boulevard



Figure 33: Over time, Winston Churchill Boulevard will intensify with a built form that frames the street and provides a quality public realm and pedestrian environment.

#### Commentary:

##### Existing Condition

The view shown in View 10 is looking north along Winston Churchill Boulevard towards Royal Windsor Drive. Winston Churchill Boulevard holds a unique place within Southdown. It defines the border between the Town of Oakville and the City of Mississauga at its western edge. The road has a distinctive rural profile, measuring two lanes in width, with soft shoulders and drainage ditches on both sides containing tall grasses and other natural plant material. Overhead power lines exist on the east side of the road. A range of building types such as 19th Century houses and barns, to industrial and residential buildings (Oakville side) also form a part of the street character. While the street has limited capacity to accommodate bicycles, it does not encourage pedestrian uses.

##### Ten-Fifteen Year Condition

A number of subtle interventions should be introduced in this phase (View 11):

- important elements include enhanced bicycle lanes and pedestrian sidewalks designed to encourage recreational access to the Lake Ontario waterfront. These improvements should be modest in nature with a view to enhancing the street character; and
- landscape enhancements should reinforce the rural quality of the area, particularly at the roadway edges, utilizing native plant materials and plantings that augment the pastoral quality of Winston Churchill Boulevard.

##### Twenty-five Year Vision

Winston Churchill Boulevard should continue to evolve in the long term along the Mississauga interface, with some modest development that integrates with its pastoral landscape character (View 12):

- a different urban design is recommended that encourages subtle variation in setbacks for

buildings in order to complement the pastoral street character and provide visual interest;

- parking should be located on the side or at the rear of buildings to establish a thematic frontage condition along the street through generous landscaped buffers along individual property frontages;
- landscaping should incorporate sustainable native species, with designs that reinforce the street profile, complement the architecture of buildings, and the rural quality of Winston Churchill Boulevard; and
- all overhead wiring and poles should be relocated underground, and replaced with appropriate street lighting.

## Appendix B: Historical Overview of Southdown



Figure 34: “The Anchorage”, built circa 1840 by Captain John Skynner, is an example of Ontario Regency Architecture. It was relocated in 1978 to the Bradley Museum grounds.



Figure 35: Southdown Road (formerly 5<sup>th</sup> Line) looking north of Royal Windsor Drive circa 1850.

In 1805, the Government of York (Toronto) bought 34 000 hectares of land called the Mississauga Tract, and proclaimed it “open for settlement”. Mississauga purportedly means “river of the north with many mouths”.

The area identified as Southdown Character Area was first settled between 1807 and 1815 through

government allotments of large parcels of land, typically for farming purposes to the original settlers.

Due to early land grants, additional allotments, and the continuous acquisition of lands by early settlers, land assemblies evolved to include large areas held by prominent landowners. In addition to the farming community, a major shipping dock was located in proximity to the lands known as Lakeside Park, which was used for shipping grain to England.

The northern boundary of the area was defined around 1853 by the Great Western Railway which formed a portion of the new province’s first major railway corridor. As the railway was constructed, the quiet rural economy that existed in the Southdown and Clarkson areas was expanded as railway workers moved into the area.

The transition from rural farming to an industrial area began in the early 1900s when George Gooderham and his son introduced the idea of mass production to this area, including mass production for agricultural uses. The industrial nature of the Gooderham’s operation helped define the future of Southdown as one of industrial prominence. Also of note, in the early 1900s, the greenhouses that have characterized the west side of Southdown Road, including Herridge’s Farm Fresh Market, Sheridan Nurseries and Clairgreen Greenhouses, were established.



Figure 36: The Robertson House, located on the first grant allotment (Gable Allotment) was built circa 1851. The structure, barn and farmstead layout exists today at northeast corner of Winston Churchill Boulevard and Lakeshore Road West.





Figure 37: The British American Oil Company commenced building its facilities in 1944 at the edge of Lake Ontario.

The transition to an industrial area was furthered by the establishment of the British American Oil Company and its waterfront refinery in the early 1940s. Eventually, through amalgamations and take overs, the British American Oil Company became Petro-Canada in 1985.

The St. Lawrence Cement Company, another notable industrial establishment in this area, was introduced in the 1950s and is known for its conveyor belt that arcs Lakeshore Road West. The conveyor belt is a prominent landmark and is one representation of Southdown's industrial heritage.

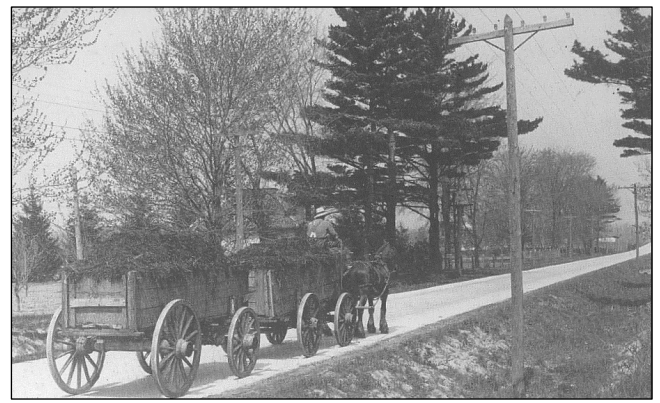


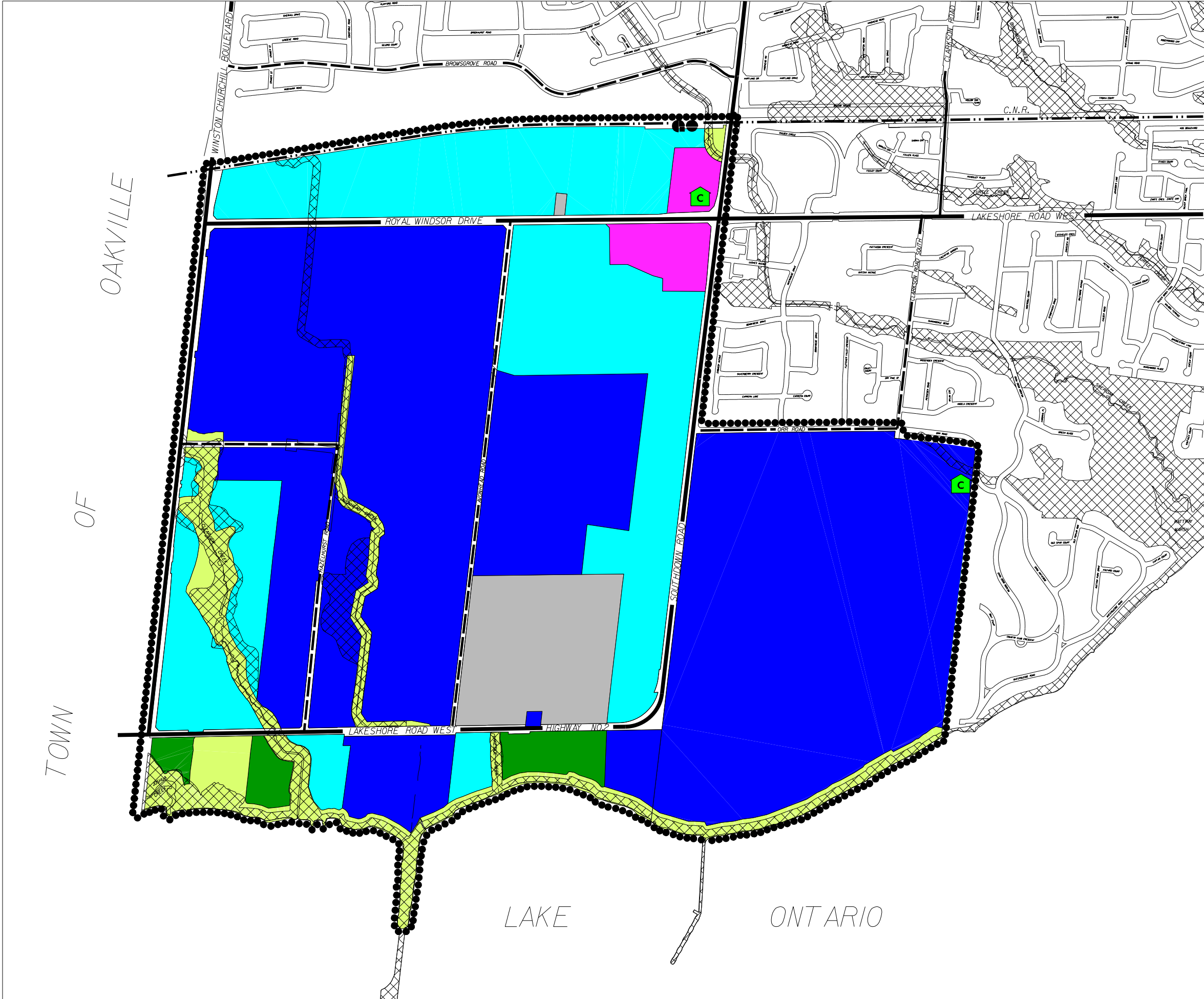
Figure 39: Lakeshore Road was one of the first highways constructed in 1914-22, of concrete in the country. Public protests resulted in Lakeshore Road being re-routed south along Southdown Road (formerly 5<sup>th</sup> Line) rather than slicing diagonally through south Clarkson.

Since the early 1800s, the area now known as Southdown has transformed from a quiet rural farming community to an employment and industrial area. It is envisioned that this area will intensify and continue to provide employment in the form of office, manufacturing and industrial uses while recognizing and respecting the important heritage and natural features of this area in accordance with the Southdown Local Area Plan.



Figure 38: Historical maps showing early settlement patterns and property ownership in Southdown.





LAND USE DESIGNATIONS

- Mixed Use
- Business Employment
- Industrial
- Public Open Space
- Greenbelt
- Utility

TRANSPORATION LEGEND

- Provincial Highway and Interchange
- Arterial
- Major Collector
- Minor Collector
- Local Road
- Existing Commuter Rail
- GO Transit Station

LAND USE LEGEND

- Natural Hazards
- Community Facilities
- Local Area Plan Boundary

- Notes:
- Any part of the road network shown outside the city boundaries is shown for information purposes only.
  - Refer to Schedule 3, Natural System for the location of the Natural Areas System and Natural Hazards.
  - The limits of the Natural Hazards shown on this map are for illustrative purposes only. The appropriate Conservation Authority should be consulted to determine their actual location.

Southdown Local Area Plan  
Land Use Map



City of Mississauga