

City of Mississauga  
**Corporate Report**



<p>Date: December 15, 2017</p> <p>To: Chair and Members of Planning and Development Committee</p>	<p>Originator's file: CD.06 HOR</p>
<p>From: Edward R. Sajecki, Commissioner of Planning and Building</p>	<p>Meeting date: 2018/01/15</p>

## Subject

### RECOMMENDATION REPORT (ALL WARDS)

**Proposed Zoning By-law Amendments and Urban Design Guidelines for Back to Back and Stacked Townhouses**

**File: CD.06 HOR**

## Recommendation

That the Report dated December 15, 2017, from the Commissioner of Planning and Building titled "Recommendation Report (All Wards) Proposed Zoning By-law Amendments and Urban Design Guidelines for Back to Back and Stacked Townhouses", be adopted in accordance with the following:

1. That notwithstanding that subsequent to the public meeting, changes to the Zoning By-law Amendments and Urban Design Guidelines have been proposed, Council considers that the changes do not require further notice and, therefore, pursuant to the provisions of subsection 34(17) of the *Planning Act*, any further notice regarding the proposed amendments is hereby waived.
2. That the proposed amendments to Zoning By-law 0225-2007 be approved in accordance with Appendix 3 of this report.
3. That the proposed Urban Design Guidelines for Back to Back and Stacked Townhouses be approved in accordance with Appendix 4 of this report.

## Report Highlights

- A public meeting was held on September 25, 2017 to hear comments regarding the draft Zoning By-law regulations and Urban Design Guidelines for Back to Back and Stacked

### Townhouses

- The proposed Zoning By-law Amendments include renaming the existing **RM9 (Horizontal Multiple Dwellings with more than 6 Dwelling Units)** zone and introducing four new Back to Back and Stacked Townhouse zones. The proposed amendments will better represent the different types of Back to Back and Stacked Townhouses and their unique attributes through modified regulations and definitions
- Through the circulation of the proposed Zoning By-law Amendment and Urban Design Guidelines to various agencies and departments, along with the public consultation process, several comments were received, reviewed and proposed modifications recommended, where appropriate

## Background

A public meeting was held by the Planning and Development Committee on September 25, 2017, at which time an Information Report (Appendix 1) was received for information. Recommendation PDC-0048-2017 was then adopted by Council on October 11, 2017.

1. That the report dated September 1, 2017, from the Commissioner of Planning and Building regarding the proposed Zoning By-law Amendments and Urban Design Guidelines for Back to Back and Stacked Townhouses under file CD.06.HOR (All Wards), be received for information.
2. That one oral submission to the Planning and Development Committee made on September 25, 2017, be received.

## Comments

### STAKEHOLDER COMMENTS

Comments received through the various stakeholder engagement sessions or written submissions are included in the table contained in Appendix 2. A response and corresponding action, where appropriate, has been provided for each comment.

### MODIFICATIONS TO DRAFT ZONING BY-LAW REGULATIONS AND URBAN DESIGN GUIDELINES

Since the public meeting, the following additional changes have been made to the proposed Zoning By-law Amendments and Urban Design Guidelines:

- Basement units are no longer prohibited. An additional regulation has been added to ensure the design of below grade amenity areas allows for increased light penetration into units. The newly proposed regulation prohibits any first storey projections from exceeding 50% of the depth of a below grade patio

- The minimum setback of a rooftop amenity space from all exterior edges of a building has been reduced to 1.0 m (3.3 ft.) from the previously proposed 1.2 m (3.9 ft.). This change is to allow for an adequately sized rooftop amenity space balanced with the minimum setback requirements of structures for rooftop access. Additionally, greater clarity has been added to acknowledge that the 1.0 m (3.3 ft.) setback requirement only applies where the rooftop amenity space overlooks adjacent properties, not where it overlooks internal to the site
- Clarification is added to the Urban Design Guidelines to reflect that a common amenity area is only required for developments with more than 20 units and that the City is flexible in terms of the type of amenity area provided
- The calculation of building height now excludes a structure used for rooftop access, as long as the structure has a maximum height of 3.0 m (9.8 ft.), maximum floor area of 20.0 m<sup>2</sup> (215.3 ft<sup>2</sup>), and is set back a minimum of 3.0 m (9.8 ft.) from the exterior edge of the building
- Clarification is added to the Urban Design Guidelines to indicate that the 45 degree angular plane is measured from all lot lines
- Additional graphics are included in the Urban Design Guidelines to better describe first storey, below grade unit, through-unit and double-wide unit
- The definition of **Amenity Area** is simplified in the Zoning By-law and regulations are added to reflect the City's existing Outdoor Amenity Areas Design Reference Note. These regulations include a minimum 3.0 m (9.8 ft.) setback from an amenity area to a building, structure or any lot line. These changes are intended to clarify that a mews does not count towards the minimum required amenity area
- The words "where appropriate" and "where feasible" have been added to various sections of the Urban Design Guidelines

## Financial Impact

Not applicable.

## Conclusion

The City has seen a significant increase in the number of development applications proposing Back to Back and Stacked Townhouses. A number of common challenges have emerged among many of these development applications. In light of this trend, new Zoning By-law regulations and Urban Design Guidelines are required to establish a clear design expectation for this increasingly popular built form.

A significant amount of stakeholder engagement has occurred throughout the study process, including several meetings with the development industry, City departments and external

agencies, and the public. Based on the feedback received through this engagement, modifications have been made to both the Zoning By-law Amendments and Urban Design Guidelines. Overall the Zoning By-law regulations and Urban Design Guidelines address the numerous challenges associated with this built form and achieve the specific goal of setting a design and planning expectation for developments which include Back to Back and Stacked Townhouses.

## Attachments

- Appendix 1: Information Report
- Appendix 2: Stakeholder Comments on Zoning By-law Regulations and Urban Design Guidelines for Back to Back and Stacked Townhouses
- Appendix 3: Zoning By-law Regulations and Definitions, December 2017
- Appendix 4: Urban Design Guidelines for Back to Back and Stacked Townhouses, December 2017



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Edward R. Sajecki, Commissioner of Planning and Building

Prepared by: Ashlee Rivet, Development Planner



City of Mississauga  
**Corporate Report**



<p>Date: September 1, 2017</p> <p>To: Chair and Members of Planning and Development Committee</p>	<p>Originator's file: CD.06 HOR</p>
<p>From: Edward R. Sajecki, Commissioner of Planning and Building</p>	<p>Meeting date: 2017/09/25</p>

## Subject

### **PUBLIC MEETING INFORMATION REPORT (ALL WARDS)**

**Proposed Zoning By-law Amendments and Urban Design Guidelines for Back to Back and Stacked Townhouses**

**File: CD.06 HOR**

## Recommendation

That the report dated September 1, 2017, from the Commissioner of Planning and Building regarding the proposed Zoning By-law Amendments and Urban Design Guidelines for Back to Back and Stacked Townhouses under File CD.06 HOR (All Wards), be received for information.

## Report Highlights

- This report has been prepared for a public meeting to hear from the community
- Draft Zoning By-law regulations and Urban Design Guidelines for Back to Back and Stacked Townhouses were made available on the City's website on March 3, 2017
- Planning staff have held stakeholder engagement sessions with the development industry, the public, City Departments and external agencies, to get their input on the proposed regulations and guidelines for Back to Back and Stacked Townhouses
- Feedback received to date includes, but is not limited to, the flexibility of the guidelines, block length, below grade units, outdoor amenity area requirements, angular planes, building separation distances and setbacks, and utilities
- Based on the feedback received, modifications to the draft Zoning By-law regulations and Urban Design Guidelines are proposed
- Prior to the next report, staff will compile all feedback received and make additional amendments to the draft documents, where appropriate

## Background

On September 19, 2016, the Planning and Development Committee (PDC) directed Planning staff to prepare Urban Design Guidelines and to review the current zoning terminology and zone regulations for Back to Back and Stacked Townhouses (formerly Horizontal Multiple Dwellings) ([https://www7.mississauga.ca/documents/committees/pdc/2016/09\\_19\\_16\\_-\\_PDC\\_Agenda.pdf](https://www7.mississauga.ca/documents/committees/pdc/2016/09_19_16_-_PDC_Agenda.pdf)).

On February 27, 2017, the Planning and Development Committee (PDC) received a report titled "Back to Back and Stacked Townhouses (formerly Horizontal Multiple Dwellings) – Proposed Zoning By-law Amendments and Urban Design Guidelines (All Wards)" ([https://www7.mississauga.ca/documents/committees/pdc/2017/2017\\_02\\_17\\_-\\_REVISED\\_PDC\\_Agenda.pdf](https://www7.mississauga.ca/documents/committees/pdc/2017/2017_02_17_-_REVISED_PDC_Agenda.pdf)). PDC passed Recommendation PDC-0005-2017 which was adopted by Council as follows:

1. That the report dated February 3, 2017, from the Commissioner of Planning and Building titled "Back to Back and Stacked Townhouses (formerly Horizontal Multiple Dwellings) – Proposed Zoning By-law Amendments and Urban Design Guidelines (All Wards)", be received for information.
2. That staff report back to Planning and Development Committee at a future statutory public meeting with the results of the consultation on the proposed Zoning By-law amendments and Urban Design Guidelines for Back to Back and Stacked Townhouses.

On March 3, 2017, the proposed Zoning By-law regulations and Urban Design Guidelines for Back to Back and Stacked Townhouses were made available on the City's website.

The purpose of this report is to:

1. Outline the stakeholder engagement sessions that have occurred
2. Summarize the feedback received to date on the proposed Zoning By-law regulations and Urban Design Guidelines
3. Provide the latest drafts of the Zoning By-law regulations and Urban Design Guidelines, which include some modifications based on feedback received to date
4. Seek comments from the community

## Comments

Since receiving direction from PDC on September 19, 2016 to prepare Urban Design Guidelines and review the current Zoning By-law regulations for Back to Back and Stacked Townhouses, Planning staff have held the following stakeholder engagement sessions:

- November 29, 2016 Presentation and discussion at the Building Industry Liaison Team (BILT) meeting

- March 29, 2017 Open House attended by developers, development industry professionals (planners and architects), and members of the public
- May 16, 2017 Presentation and discussion at the Building Industry and Land Development Association (BILD) Peel Chapter meeting
- June 20, 2017 Draft Urban Design Guidelines and Zoning By-law regulations considered by the Mississauga Urban Design Advisory Panel (MUDAP)

Planning staff have also consulted with various City departments and external agencies, including:

- March 30, 2017 Comment letter from Bell Canada
- June 28, 2017 Discussion with Enbridge Gas
- July 20, 2017 Discussion with the City's Chief Building Official and Acting Assistant Chief of Fire Prevention and Life Safety
- July 25, 2017 Discussion with Alectra Utilities

In addition to the above sessions, staff visited a number of existing Back to Back and Stacked Townhouse developments in other municipalities, including Toronto (Etobicoke and North York), Milton, and Markham (Cornell), to gain a better understanding of the complexities of this form of housing. Staff also met developers and their architects individually to discuss their successes and challenges with this built form.

### **FEEDBACK RECEIVED**

Comments received by various stakeholders on the draft Zoning By-law regulations and Urban Design Guidelines through our engagement sessions are summarized below and are grouped by issue. Some comments have been addressed through modifications to the proposed documents. All comments received, including those raised at the public meeting will be addressed in the Recommendation Report, which will come at a later date.

- The guidelines should allow for greater flexibility in their application
- Greater consideration should be given to how the guidelines will apply to smaller sites
- The proposed maximum block length of 41 m (134.5 ft.) is too restrictive and should be evaluated on a case by case basis
- The guidelines pertaining to partially below grade units are confusing. Greater clarity is required
- The requirement for common outdoor amenity area on all new multi-unit residential developments is excessive and impacts affordability and the ability for the developer to maximize unit yield
- The use of angular planes is not appropriate for this type of low-rise built form and more appropriately applied to taller buildings

- The proposed separation distances between buildings and setbacks are excessive and should relate to building heights
- Utility companies are generally happy to work with the City to appropriately locate their infrastructure and agree with the guidelines' direction to consider the location of these services in the early stages of site design

### **URBAN DESIGN REVIEW PANEL**

The Urban Design Review Panel reviewed the draft Urban Design Guidelines and Zoning By-law regulations on June 20, 2017. Comments from the panel include the following:

- The Panel acknowledged the clarity and comprehensiveness of the guidelines, but suggests that the documents allow for flexibility, innovation and uniqueness depending on the site context
- Proposed minimum lot frontage, separation distances between blocks and interior side yard setbacks should be reviewed in greater detail
- The Panel agreed with the proposed minimum requirements for common outdoor and private outdoor amenity areas
- Consideration should be given to how "storey" is defined as it is key to assessing this built form and manipulation of site grades. Many buildings appear to be 5 storeys with below grade units and roof top amenity areas
- The guidelines should ensure a variation in built form, material and colour to avoid repetitiveness and monotony

### **MODIFICATIONS TO PROPOSED DRAFT GUIDELINES AND ZONING REGULATIONS**

Although staff continue to review and refine the draft Zoning By-law regulations and Urban Design Guidelines based on the input received thus far, the following modifications have been made to the updated document in Appendices 1 and 2:

- The minimum lot frontage regulation in the Zoning By-law has been reduced to 38.0 m (124.7 ft.) from 42.0 m (137.8 ft.)
- The maximum 41 m (134.5 ft.) block length has been removed from the draft Zoning By-law regulations. The parameter remains in the draft Guidelines only
- Guidelines and regulations pertaining to below grade units and basement units have been clarified. Basement units will no longer be prohibited. Additional regulations will be added to the Zoning By-law to ensure below grade units are designed to allow for adequate light and air into units and private outdoor spaces
- The definition of Context Grade has been modified to recognize the permissions for basement units with private outdoor space

- The Guidelines recommend a limit of 3 to 7 risers to a unit entrance, whereas 3 to 5 risers were previously recommended. This change reflects Ontario Building Code (OBC) restrictions on the maximum height of a porch
- Minimum interior side yard regulations have been reduced where the side lot line abuts a zone permitting detached and/or semi-detached dwellings and where the front wall of a proposed building faces the interior side lot line. The minimum rear yard regulations have similarly been reduced
- The minimum front wall to side wall separation distance has been reduced
- The Zoning By-law regulation requiring an additional 1.0 m (3.2 ft.) setback where below grade units are proposed has been removed. The minimum front wall to front wall separation distance now ranges from 12.0 m (39.4 ft.) to 15.0 m (49.2 ft.) depending on building height
- The minimum width of a sidewalk has been adjusted. A 2.0 m (6.6 ft.) sidewalk is proposed only where the sidewalk is traversed by a driveway. Where the sidewalk is not traversed by a driveway, a 1.8 m (5.9 ft.) wide sidewalk is proposed. The minimum width of a walkway internal to the site has been reduced to 1.5 m (4.9 ft.)
- The Guidelines recommend a sidewalk on only one side of a condominium road (except for large developments), whereas a sidewalk on both sides of a condominium road was previously recommended
- Reference to Fire Route By-law 1036-81
- Consideration of OBC requirements
- Consideration of Enbridge Gas and Alectra Utilities requirements

Planning staff continue to review comments and feedback received by stakeholders. Additional modifications may be made to these documents. A final version of the draft Zoning By-law and Urban Design Guidelines will be presented in the Recommendation Report at a later date.

## Financial Impact

Not applicable.

## Conclusion

The Planning and Building Department will consider all comments and feedback received and after the public meeting will make changes, as appropriate, to the draft Zoning By-law regulations and Urban Design Guidelines. A Recommendation Report will be brought to a future PDC meeting for consideration.

## Attachments

Appendix 1: Draft Urban Design Guidelines for Back to Back and Stacked Townhouses, September 2017

Appendix 2: Proposed Zoning By-law Regulations and New and Amended Definitions,  
September 2017

A handwritten signature in black ink, appearing to read "E. R. Sajecki", enclosed in a thin black rectangular border.

Edward R. Sajecki, Commissioner of Planning and Building

Prepared by: Ashlee Rivet, Development Planner



# Urban Design Guidelines



DRAFT September 2017

## Back to Back and Stacked Townhouses

# Table of Contents

<b>1</b>	<b>Introduction</b>	<b>1</b>
1.1	Purpose	1
1.2	Urban Design Objectives	1
1.3	Building Types	2
<b>2</b>	<b>Checklist of Principles</b>	<b>3</b>
2.1	Zoning By-law	3
2.2	Building Height	3
2.3	Building Setbacks	3
2.4	Separation between Buildings	4
2.5	Block Length	4
2.6	Natural Features	4
2.7	Grading and Retaining Walls	4
2.8	Below Grade Units	6
2.9	Building Elevations	6
2.10	Exposed Parking Structures	7
2.11	Landscaped Soft Areas	8
2.12	Common Outdoor Amenity Area	8
2.13	Private Outdoor Space	9
2.14	Pedestrian Connectivity	9
2.15	Waste Collection and Storage	10
2.16	Surface Parking	10
2.17	Utilities and Services	11
2.18	Property Management and Maintenance	11
2.19	Other considerations	11
<b>3</b>	<b>Design Standard Diagrams</b>	<b>12</b>
3.1	RM9 Stacked Townhouses Design Standards	12
3.2	RM10 Back to Back Townhouses on Condominium Road Design Standards	13
3.3	RM11 Back to Back Townhouses on CEC-Road Design Standards	14



# Introduction

The City of Mississauga is at the end of its greenfield development phase. New growth is being accommodated through infill and development on vacant and underutilized sites. Development patterns are becoming more compact, using land and resources more efficiently, while maximizing existing infrastructure and community facilities, and promoting alternative modes of transportation. Traditional forms of housing are becoming less common, as land values rise and market demands shift. Back to Back Townhouses (BBT) and Stacked Townhouses (ST) are becoming increasingly popular throughout the GTA for several reasons:

- Achieve increased densities in a low-rise form of housing
- A sensitive way to transition between low-density and high-density built forms
- Contribute to a diversity of housing choices to meet different needs and preferences
- Less expensive construction methods and reduced maintenance fees allow for a more affordable form of housing
- Viewed as being grade related, with a front door directly to the outside

## 1.1 Purpose

The purpose of these guidelines is to ensure that new developments that include BBTs and STs are designed to be compatible with and sensitive to the established context and to minimize undue impacts on adjacent properties. The guidelines are intended to establish a design expectation for landowners, the development industry and the public, to ensure high quality of development that meets the City of Mississauga's minimum development standards. These guidelines shall be read in conjunction with Mississauga Official Plan, the City Zoning By-law, and other City guidelines and standards.

## 1.2 Urban Design Objectives

The following objectives provide the framework for the design guidelines:

- Ensure compatibility with the existing and planned context
- Design to meet the needs of people of all ages, abilities and incomes
- Balance functional design and aesthetics with long-term sustainability
- Protect and enhance natural features
- Connect streets and provide pedestrian linkages
- Provide high quality private and common amenity areas

## 4.6 - 14

### 1.3 Building Types

BBTs and STs are typically

- 3 to 4 storeys tall
- Comprised of units that are stacked vertically and/or horizontally with access from grade
- Front onto a public street, condominium road, pedestrian mews or open space
- Include surface and/or underground parking

These are illustrated in Figure 1.1 and Figure 1.2

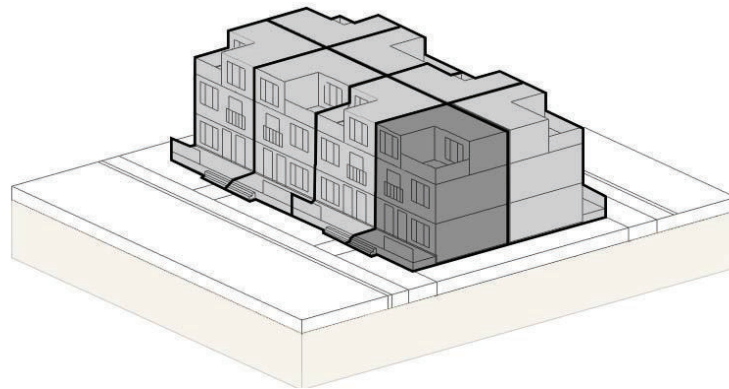


Figure 1.1: Example of Back to Back Townhouse

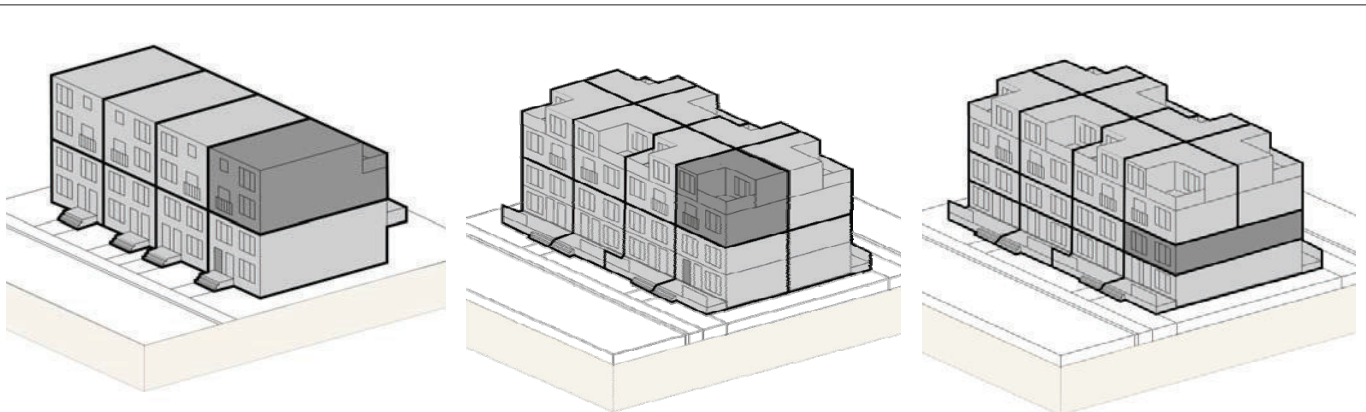


Figure 1.2: Examples of Stacked Townhouse



# Checklist of Principles

The following principles are to be considered when designing a development that includes BBTs and/or STs. These principles are intended to ensure that new developments are compatible with and respect the existing and/or planned context through appropriate setbacks, tree preservation and landscape buffers. Consideration shall be given to site design, building massing, orientation, height and grading relative to the street to ensure new developments are compatible with and sensitive to the surrounding context.

This checklist is to be used as a guide for developers, design professionals, property owners and the public to ensure they have considered key issues associated with this residential built form.

Review and check each principle when complete -----

## 2.1 Zoning By-law .....

- Refer to the Zoning By-law regulations that apply to the proposed built form. Generally BBT's and ST's are zoned RM9, RM10, RM11 and RM12 or in combination with other zones

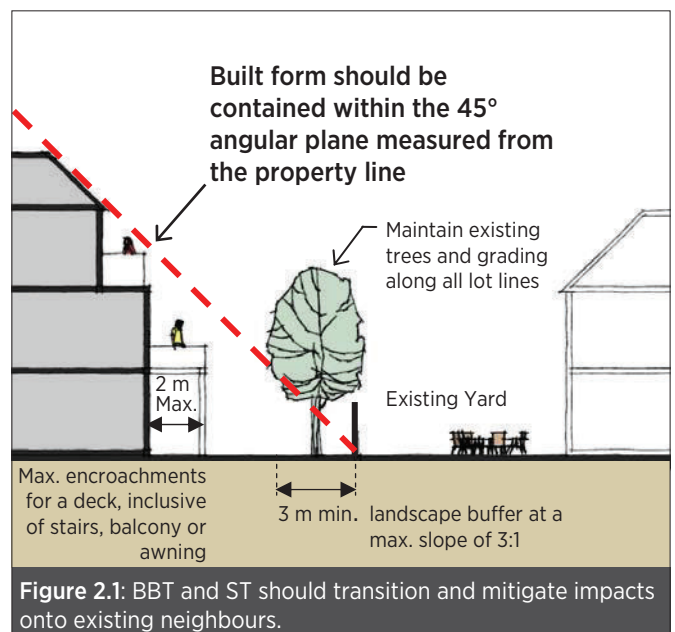
## 2.2 Building Height .....

- New developments will be required to demonstrate an appropriate transition in building heights

- Buildings heights shall be contained within a 45° angular plane, measured from the property line (See Figure 2.1)
- Maximum building heights of 3 storeys for BBTs and 4 storeys for STs

## 2.3 Building Setbacks .....

- When existing adjacent front yard setbacks vary, new buildings should align with the average setback between the two adjacent properties or the minimum zoning requirement, whichever is greater



**2.4 Separation between Buildings** .....

- Separation distance between buildings should be the minimum setbacks as outlined in the Zoning By-law
- In the case of a front wall to front wall condition, the separation distance should be the greater of the 45° angular plane or the minimum setbacks as outlined in the Zoning By-law (See Figure 2.2)
- Where a basement unit forms part of a 3 storey development the minimum separation distance will be 15 m

**2.5 Block Length** .....

- Excessively long blocks should be avoided
- The maximum length of a block should generally not exceed the greater of 41 m or 8 linear modules to promote pedestrian connections, allow for landscaping and provide a break in the massing (See Figure 2.3)

**2.6 Natural Features** .....

- New developments should preserve and enhance natural heritage features; including, trees, woodlands, valleys and wetlands
- Appropriate setbacks and buffers should be provided to existing and proposed natural features to ensure their health and continued growth

**2.7 Grading and Retaining Walls** .....

- Manipulation of site grades should be avoided
- Match existing grades and provide a minimum 3 m wide landscape buffer around the property
- The landscape buffer should be unencumbered by below grade parking structures, easements, retaining walls, utilities, severe grade changes and hard surface areas

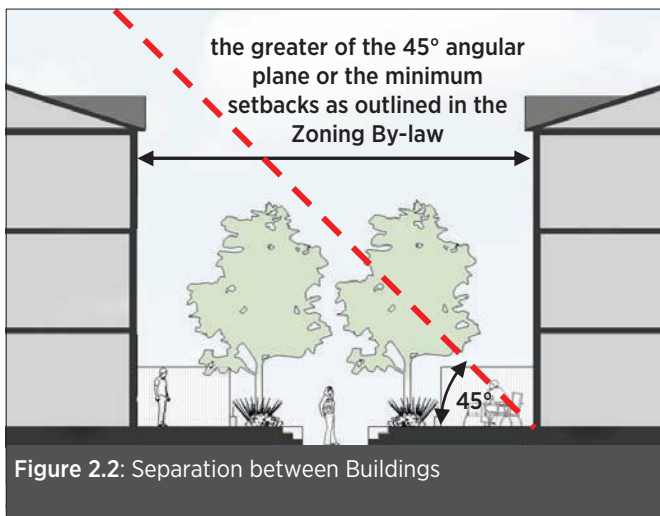


Figure 2.2: Separation between Buildings

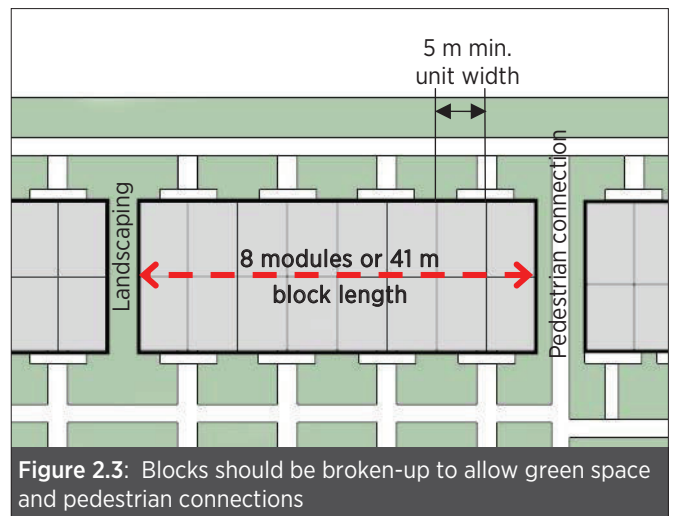


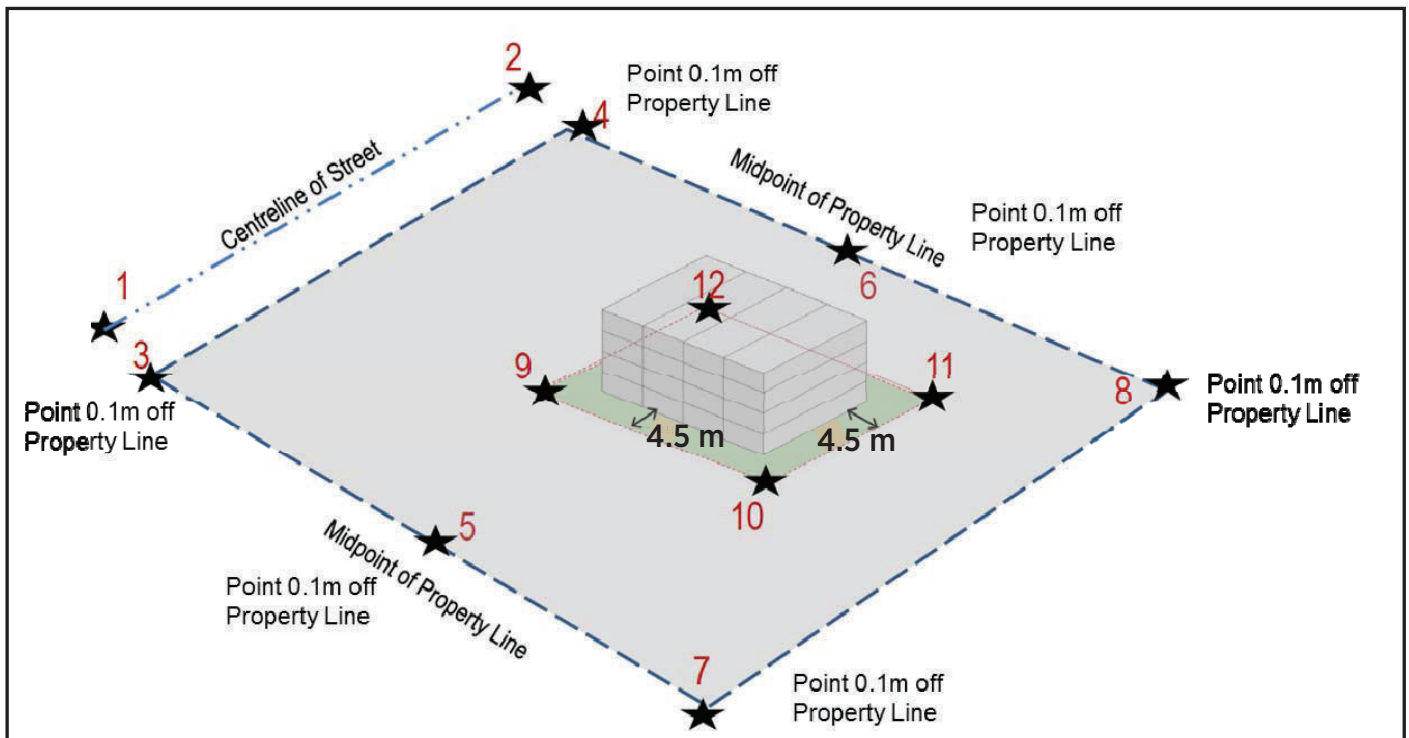
Figure 2.3: Blocks should be broken-up to allow green space and pedestrian connections

4.6 - 17

## Checklist of Principles



- Each individual building will establish a grade elevation based on 'Context Grade'. Context Grade means the average of 12 points, 8 of which are taken around the perimeter of the site and 4 of which are taken around each individual building (See Figure 2.4)
- The use of retaining walls should be avoided. Where retaining walls are required, their height should be limited to a maximum of 0.6 m to eliminate the need for railings and to reduce long-term maintenance costs (See Figure 2.6)
- The first storey means a storey of a building that has its floor closest to the context grade and its ceiling more than 1.8 m above the context grade (See Figure 2.5)



**Figure 2.4:** Context Grade: The average of 12 Points. 8 of which are around the perimeter of the site and 4 points located 4.5 m around each building

**2.8 Below Grade Units** .....

- Below grade units should be avoided
- Manipulation of site grades requiring retaining walls to accommodate below grade units is discouraged
- If a below grade unit is proposed, it must be a through-unit that has windows on both the front and rear of the building (See Figure 2.7) or a double wide (i.e. 10 m wide) back to back unit
- Below grade units require a minimum of 6 m<sup>2</sup> of private outdoor space located at the unit's floor level with unobstructed views and access to daylight (See Figure 2.7)

- All building projections, including balconies and porches located over private outdoor spaces or windows of below grade units should not obstruct access to daylight. See the Zoning By-law for the overhang regulations (See Figure 2.7)

**2.9 Building Elevations** .....

- New development should be compatible with the existing context in terms of height, scale, massing and materials
- Where appropriate, incorporate sloped roofs and half-storeys with dormer windows on upper levels to reduce perceived heights, scale and massing
- Ensure new developments have a variety of facade articulation, building materials and colours for visual interest

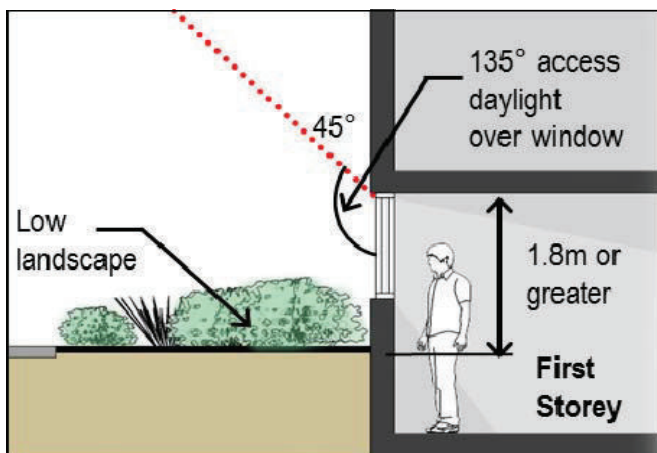


Figure 2.5: Definition of First Storey

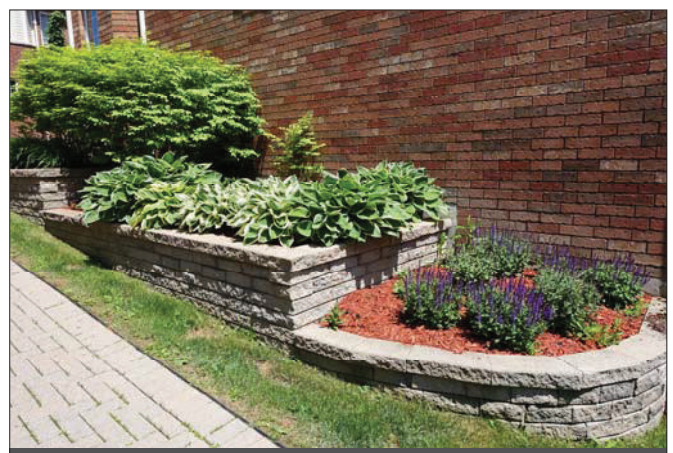


Figure 2.6: Landscape retaining walls should not be higher than 0.6 m





# Checklist of Principles

4.6 - 19

- Blank facades on the visible end unit elevation are unacceptable. End units that are visible should have entrances, windows and architectural interest to animate the elevation
- Buildings should be designed with high quality and durable materials to avoid long term maintenance costs. Stone and brick is preferred. Stucco and wood are discouraged
- Stepback roof top mechanical rooms 3 m from the exterior edges of the building to reduce their visual impact
- The mechanical floor area located on a unit roof top should not be greater than 20 m<sup>2</sup>, inclusive of stair

## 2.10 Exposed Parking Structures

- Exposed parking structures should be avoided. Where portions of the underground parking structure are exposed, they should match the building materials
- Consolidate the entrances to underground parking structures within the same development to minimize the number of overhead doors
- Maintain the minimum soil volume over the parking structure to support the growth of the vegetation. Minimum soil volume varies based on the type of vegetation

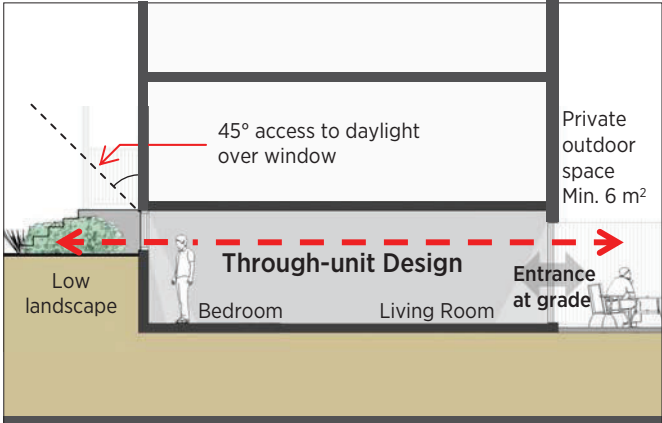


Figure 2.7: Below Grade Units

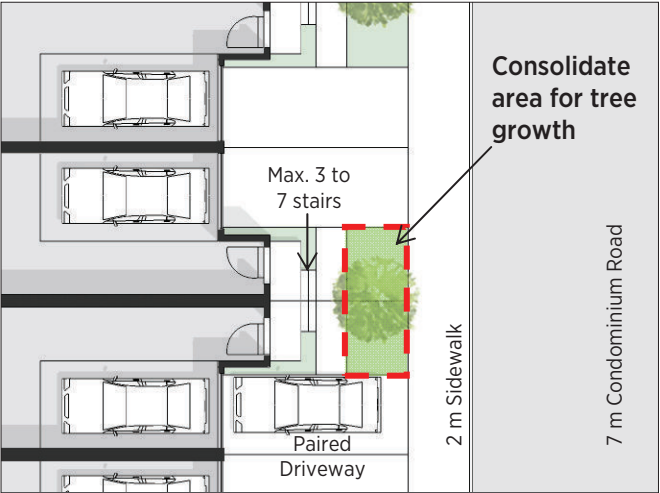


Figure 2.8: Combine landscaped soft areas for tree growth

**2.11 Landscaped Soft Areas** .....

- Landscaped soft areas are required adjacent to paved areas and around the perimeter of the site. To provide relief between buildings landscaped soft areas should be distributed throughout the development
- Landscaped soft areas should be provided between entrances to individual units and sidewalks, walkways, public streets and condominium roads
- Pair individual landscaped soft areas to increase soil volume for tree growth particularly where there is a driveway (See Figure 2.8)
- Limit the number of stairs to a unit entrance to 3 to 7 risers to maximize landscaped soft area, mitigate safety issues in the winter and reduce maintenance costs

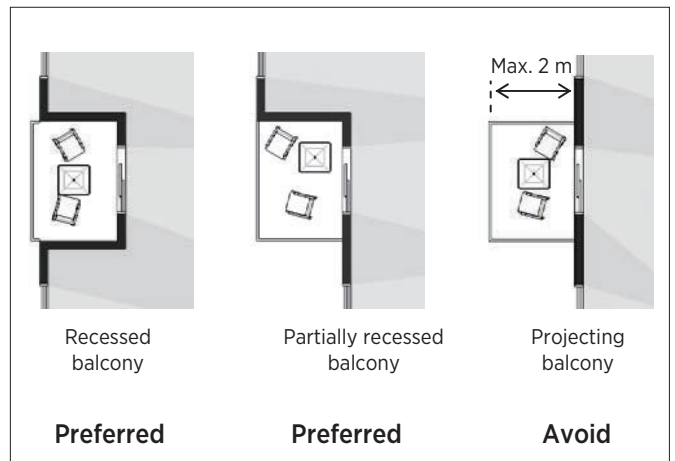
- All stairs should be poured-in-place concrete. Precast stairs are not permitted

**2.12 Common Outdoor Amenity Area** .....

- A common outdoor amenity area is required for all new multi-unit residential developments
- The total space required is the greater of 5.6 m<sup>2</sup> per dwelling unit or 10% of the site area
- Common outdoor amenity areas should be centrally located, highly visible and accessible by all residents (See Figure 2.9)
- A minimum of 50% of the required common outdoor amenity area shall be provided in one contiguous area
- A mews will not be considered a common outdoor amenity area

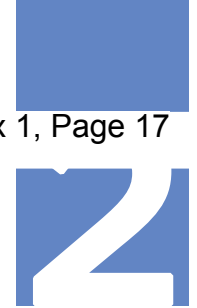


**Figure 2.9:** Common Outdoor Amenity Areas should be centrally located, accessible and highly visible.



**Figure 2.10:** Balconies as Private Outdoor Space





- Refer to the Outdoor Amenity Area Design Reference Note for additional detail  
[http://www7.mississauga.ca/documents/pb/main/2015/Amenity\\_Space\\_Reference.pdf](http://www7.mississauga.ca/documents/pb/main/2015/Amenity_Space_Reference.pdf)

**2.13 Private Outdoor Space** .....

- Each unit requires a private outdoor space with a minimum contiguous area of 6 m<sup>2</sup>
- The private outdoor space may be located at grade, on a balcony, deck, porch or on a roof top
- Recessed or partially recessed balconies are preferred. Projecting balcony shall be avoided (See Figure 2.10). If a projecting balcony is proposed, it may project a maximum of 2 m beyond any building façade and should be designed with solid or opaque materials or tinted glass

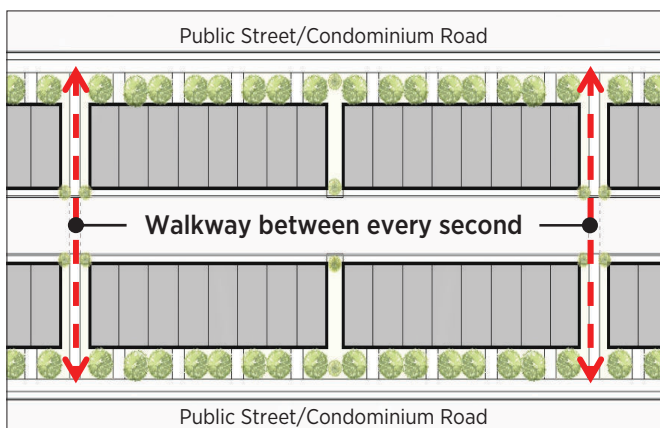
- Mechanical equipment, including air conditioning units and the storage of personal items are discouraged in private outdoor spaces

**2.14 Pedestrian Connectivity** .....

- Provide a walkway between every second block to allow connectivity (See Figure 2.11)
- Sidewalks will be located on one side of a road. Sidewalks on both sides of the street maybe required for large developments
- The following sidewalk widths will be required:
  - Sidewalks abutting a road minimum 1.8 m
  - Sidewalks abutting a road, where traversed by a driveway minimum 2 m
  - Walkways in all other areas minimum 1.5 m
- There should be at least one barrier-free path of travel that meets AODA (Accessibility for Ontarians with Disability Act) standards throughout the site

**2.15 Waste Collection and Storage** .....

- Waste storage rooms, drop-off locations (i.e. garbage chutes) and waste collection points (temporary pick-up) should be considered early in the site design stage to ensure appropriate placement and functionality



**Figure 2.11:** Pedestrian connections should be located after every second block

4.6 - 22

- The waste storage rooms and the waste collection point should be located internal to the site and should not be visible from a public street or impact residential units or adjacent properties (See Figure 2.12)
- Above grade waste storage rooms/enclosures should be well screened and appropriately setback from existing uses and proposed dwelling units to minimize undesirable noise, odour and visual impacts
- The waste collection facility should consider the space requirements for the waste, recycling and green bins, along with bulky items
- Waste drop-off areas should be easily accessible by the residents via a sidewalk or walkway and distributed throughout the site
- Waste collection points (pick-up areas) should not encumber parking stalls or access to other elements of the development (e.g. fire route, entry to the underground parking garage, mailboxes, etc.)
- Waste collection points should be made of durable concrete and be at the same level as the road
- Refer to the Region of Peel's Waste Collection Design Standards Manual for more information  
<https://www.peelregion.ca/pw/standards/design/waste-collection-design-manual-2016.pdf>

**2.16 Surface Parking** .....

- Surface parking should be centrally located within the site and accessed by a sidewalk or walkway



**Figure 2.12:** Waste storage room and waste collection areas should be constructed of durable materials.



**Figure 2.13:** Community mailboxes covered and in a central location

## Checklist of Principles

- Parking lots should be setback a minimum of 3 m from a lot line and not located between the front face of a building and the street
- A minimum 3 m setback should be provided between the side wall of a building and a surface parking space

### 2.17 Utilities and Services .....

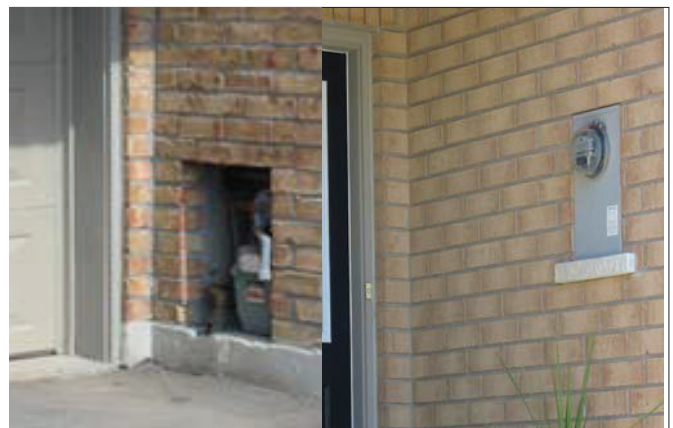
- The location of above and below grade utilities and services should be considered early in the site design stage to ensure they meet utility requirements and that any visual impacts from the public street are mitigated
- Through the development process provide the locations of above and below grade utilities, easements, etc. to ensure sufficient unencumbered space is provided for public and private trees, and landscaped soft areas
- Transformer vaults are typically located on a streetline and generally on a serviceable pad (i.e. minimum 3 m x 3 m pad for smaller developments). Contact Alectra Utilities for further requirements
- Community mailboxes should be centrally located and accessed by a sidewalk or walkway (See Figure 2.13)
- Conceal or recess hydro and gas meters into the building's exterior walls (See Figure 2.14)

### 2.18 Property Management and Maintenance

- Long term maintenance and property management should be considered early in the development process to avoid costly maintenance issues
- Use durable and high quality building and site materials. Stucco is discouraged on the first 2 storeys of a building

### 2.19 Other Considerations .....

- Review Mississauga's Fire Route By-law 1036-81 early in the site design stage for the fire route design, building access requirements, etc.
- Review the Ontario Building Code to ensure that site and building designs comply with the relevant requirements



**Figure 2.14:** Place Hydro and Gas Meters and other utilities in concealed or recessed locations.

3.1 RM9 Stacked Townhouses Design Standards

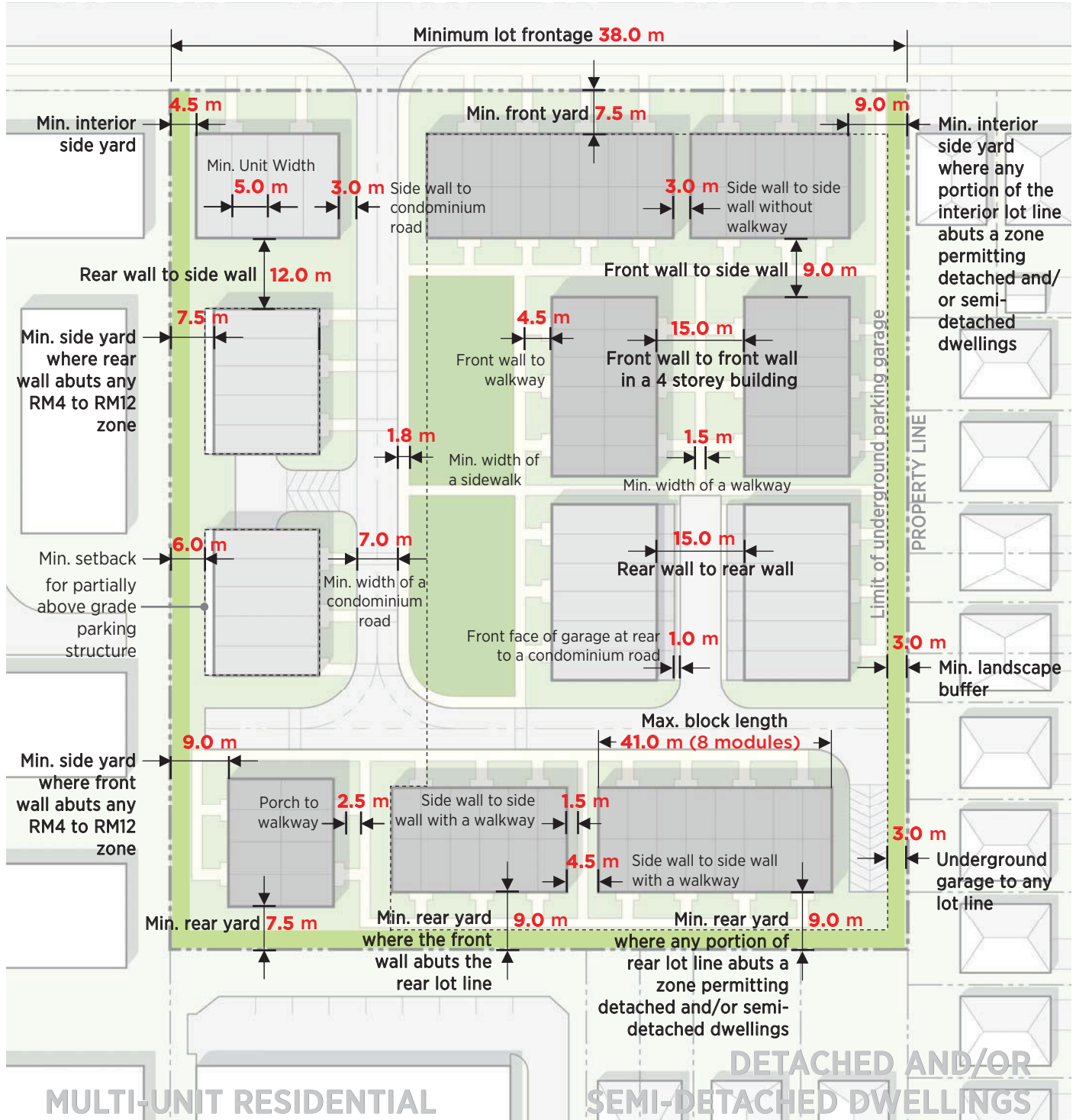


Figure 2.13: Standard Dimensions for Stacked Townhouses (RM9). For Additional Standards refer to the Zoning By-Law. The above drawing is for illustration purpose only and not to scale.





Design Standard Diagrams

3.2 RM10 Back to Back Townhouses on Condominium Road Design Standards

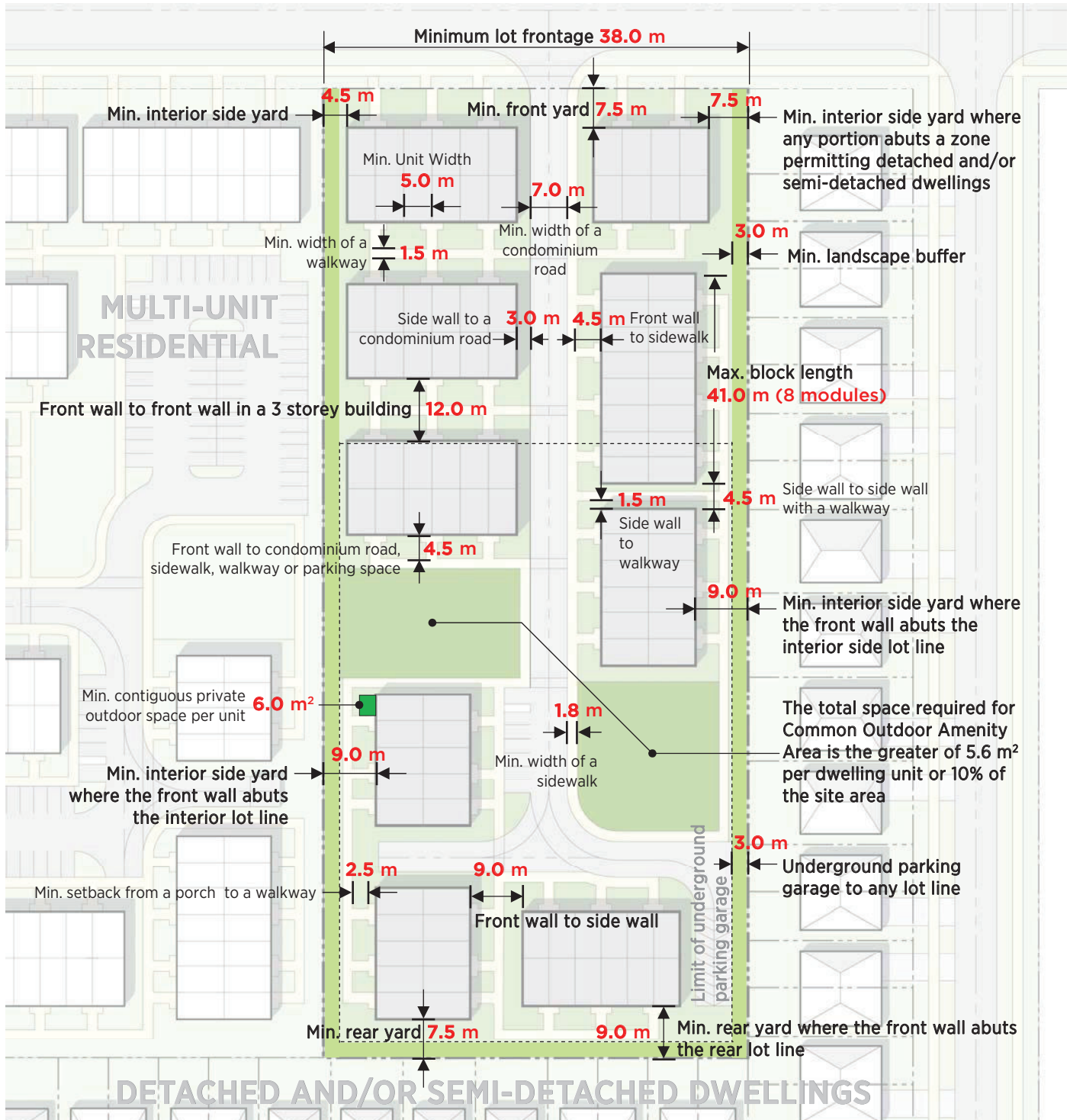


Figure 2.14: Standard Dimensions for Back to Back Townhouses (RM10). For Additional Standards refer to the Zoning By-Law. The above drawing is for illustration purpose only and not to scale.

3.3 RM11 Back to Back Townhouses on a CEC-Road Design Standards

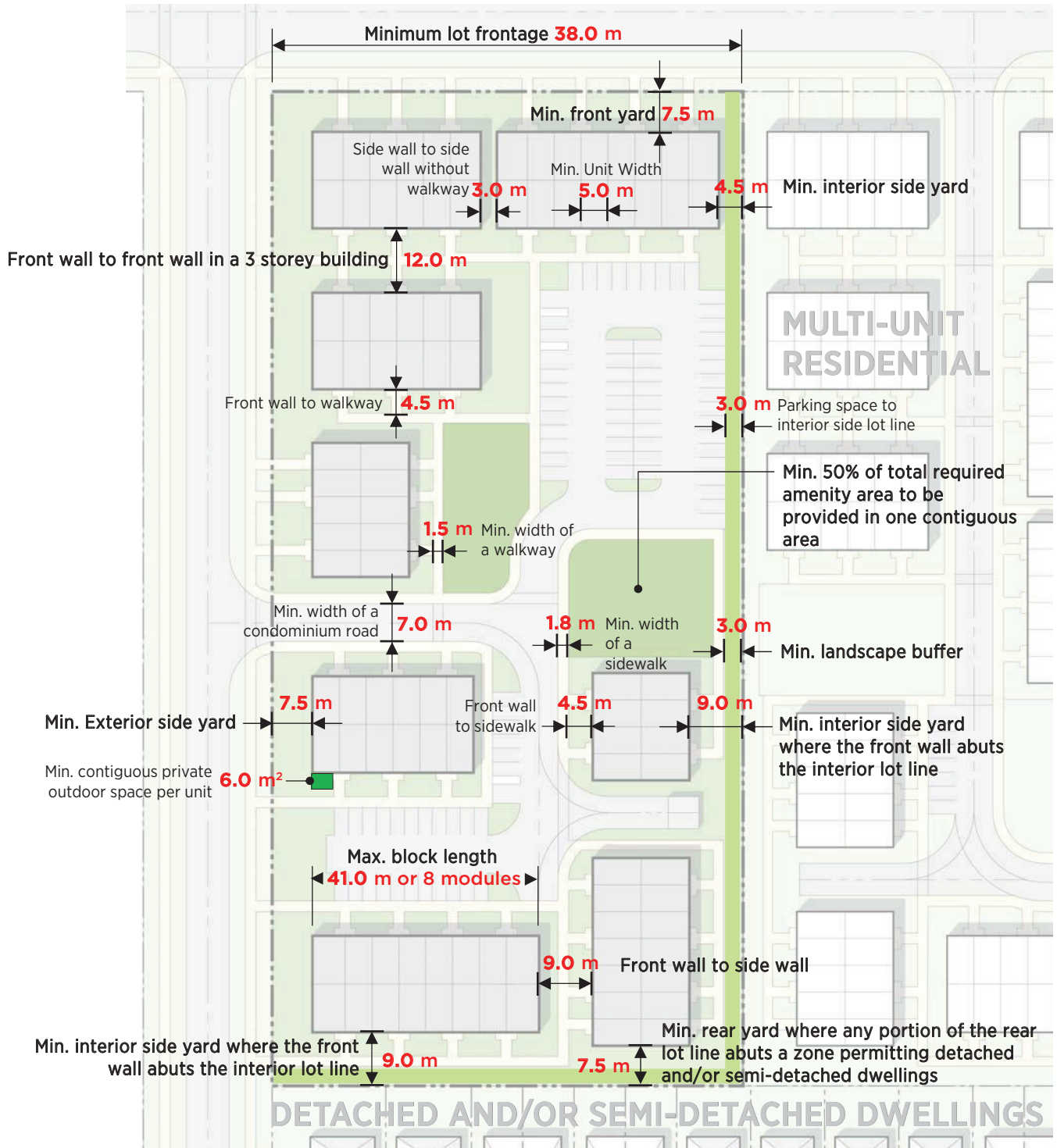


Figure 2.14: Standard Dimensions for Back to Back Townhouses (RM11). For Additional Standards refer to the Zoning By-Law. The above drawing is for illustration purpose only and not to scale.

Design Standard Diagrams







Proposed RM9, RM10 and RM11 Zone Regulations, September 2017

Column	A	B	C	D
Line 1.0	ZONES	RM9	RM10	RM11
	PERMITTED USES			
2.0	RESIDENTIAL			
2.1	Stacked Townhouse	✓		
2.2	Back to Back Townhouse on a Condominium Road		✓	
2.3	Back to Back Townhouse on a CEC-Road			✓
	ZONE REGULATIONS			
3.0	MINIMUM LOT FRONTAGE	<del>42</del> 38.0 m	<del>42</del> 38.0 m	<del>42</del> 38.0 m
4.0	MINIMUM DWELLING UNIT WIDTH	5.0 m	5.0 m	5.0 m
<del>6.0</del>	<del>MAXIMUM LENGTH OF A STACKED TOWNHOUSE AND A BACK-TO-BACK TOWNHOUSE</del>	<del>41.0 m<sup>(4)</sup></del>	<del>41.0 m<sup>(4)</sup></del>	<del>41.0 m<sup>(4)</sup></del>
<del>6.5.0</del>	MAXIMUM DWELLING HEIGHT			
<del>6.5.1</del>	Flat roof	13.0 m and 4 storeys	11.0 m and 3 storeys	11.0 m and 3 storeys
<del>6.5.2</del>	Sloped roof	17.0 m <sup>(2)</sup> and 4 storeys	15.0 m <sup>(2)</sup> and 3 storeys	15.0 m <sup>(2)</sup> and 3 storeys
<del>7.0</del>	MINIMUM FRONT YARD	7.5 m <sup>(3)</sup>	7.5 m <sup>(3)</sup>	7.5 m <sup>(3)</sup>
<del>8.0</del>	MINIMUM EXTERIOR SIDE YARD	7.5 m <sup>(3)</sup>	7.5 m <sup>(3)</sup>	7.5 m <sup>(3)</sup>
<del>9.0</del>	MINIMUM INTERIOR SIDE YARD	4.5 m <sup>(3)</sup>	4.5 m <sup>(3)</sup>	4.5 m <sup>(3)</sup>
<del>9.1</del>	Where any portion of the interior side lot line abuts a zone permitting <b>detached</b> and/or <b>semi-detached dwellings</b>	<del>4.0-9.0</del> 0 m <sup>(3)</sup>	7.5 m <sup>(3)</sup>	7.5 m <sup>(3)</sup>
<del>9.2</del>	Where the interior side lot line abuts a RM4, RM5, RM6, RM7, RM8, RM9, RM10, RM11, or RM12 zone and the rear wall of the building abuts the interior side lot line	7.5 m <sup>(3)/(4)</sup>	n/a	n/a

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Proposed RM9, RM10 and RM11 Zone Regulations, September 2017

Column	A		B	C	D
Line 1.0	ZONES		RM9	RM10	RM11
<del>409.3</del>	Where the front wall of a <b>building</b> abuts the <b>interior side lot line</b>		<del>409.0 m</del> <sup>(3)</sup>	<del>409.0 m</del> <sup>(3)</sup>	<del>409.0 m</del> <sup>(3)</sup>
<del>409.0</del>	<b>MINIMUM REAR YARD</b>		7.5 m <sup>(3)</sup>	7.5 m <sup>(3)</sup>	7.5 m <sup>(3)</sup>
<del>409.1</del>	Where any portion of the <b>rear lot line</b> abuts a zone permitting <b>detached</b> and/or <b>semi-detached dwellings</b>		<del>409.0 m</del> <sup>(3)</sup>	7.5 m <sup>(3)</sup>	7.5 m <sup>(3)</sup>
<del>409.2</del>	Where a front wall of a <b>building</b> abuts the <b>rear lot line</b>		<del>409.0 m</del> <sup>(3)</sup>	<del>409.0 m</del> <sup>(3)</sup>	<del>409.0 m</del> <sup>(3)</sup>
<del>410.0</del>	<b>MAXIMUM ENCROACHMENT OF A DECK INCLUSIVE OF STAIRS, BALCONY OR AWNING, ATTACHED TO A REAR OR FRONT WALL, INTO A REQUIRED YARD</b>		2.0 m	2.0 m	2.0 m
<del>411.0</del>	<b>MINIMUM INTERNAL SETBACKS</b>				
<del>411.1</del>	From a <b>front garage face</b> to a <b>condominium road</b> or <b>sidewalk</b>		6.0 m	6.0 m	6.0 m
<del>411.2</del>	From a <b>front garage face</b> to a <b>condominium road</b> or <b>sidewalk</b> , where the <b>garage</b> and <b>driveway</b> are accessed at the rear of the <b>dwelling unit</b>		1.0 m	n/a	n/a
<del>411.3</del>	From a front wall of a <b>building</b> to a <b>condominium road, sidewalk, walkway</b> or <b>parking space</b>		4.5 m	4.5 m	4.5 m
<del>411.4</del>	From a <b>porch</b> , exclusive of stairs, <u>located at and accessible from the first storey or below the first storey</u> to a <b>condominium road, sidewalk, walkway</b> or <b>parking space</b>		2.5 m	2.5 m	2.5 m
<del>411.5</del>	From a rear wall of a <b>building</b> containing a <b>dwelling unit</b> to a side wall of another <b>building</b> containing a <b>dwelling unit</b> on the same lot		12.0 m	n/a	n/a
<del>411.6</del>	From a rear wall of a <b>building</b> containing a <b>dwelling unit</b> to a rear wall of another <b>building</b> containing a <b>dwelling unit</b> on the same lot		15.0 m	n/a	n/a

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**Proposed RM9, RM10 and RM11 Zone Regulations, September 2017**

Column		A		B	C	D
Line 1.0	ZONES	RM9	RM10	RM11		
<del>4211.7</del>	From a side wall of a <b>building</b> containing a <b>dwelling unit</b> to a side wall of another <b>building</b> containing a <b>dwelling unit</b> on the same lot	3.0 m	3.0 m	3.0 m		
<del>4211.8</del>	From a side wall of any <b>building</b> to a <b>walkway</b>	1.5 m	1.5 m	1.5 m		
<del>4211.9</del>	From a side wall of a <b>building</b> to a <b>condominium road, sidewalk, or parking space</b>	3.0 m	3.0 m	3.0 m		
<del>4211.10</del>	From a front wall of a <b>building</b> containing a <b>dwelling unit</b> to a front wall of another <b>building</b> containing a <b>dwelling unit</b> on the same lot, where the <b>building</b> is less than or equal to three storeys	12.0 m <sup>(6)</sup>	12.0 m	12.0 m		
<del>11.11</del>	<del>From a front wall of a <b>building</b> containing a <b>dwelling unit</b> to a front wall of another <b>building</b> containing a <b>dwelling unit</b> on the same lot, where the <b>building</b> is less than or equal to three storeys and contains a <b>dwelling unit</b> in the <b>basement</b></del>	<del>15.0 m <sup>(6)</sup></del>	<del>n/a</del>	<del>n/a</del>		<del>n/a</del>
<del>4211.124</del>	From a front wall of a <b>building</b> containing a <b>dwelling unit</b> to a front wall of another <b>building</b> containing a <b>dwelling unit</b> on the same lot, where the <b>building</b> is four storeys	15.0 m <sup>(6)</sup>	n/a	n/a		n/a
<del>4211.132</del>	From a front wall of a <b>building</b> containing a <b>dwelling unit</b> to a side wall of another <b>building</b> containing a <b>dwelling unit</b> on the same lot	4.0 m	4.0 m	4.0 m		4.0 m
<del>42.13</del>	<del>Increase in required setback identified in Lines 12.10, 12.11 and 12.12 where a front wall of a <b>building</b> contains stairs, stairwells or retaining walls to facilitate below-grade access to a <b>dwelling unit</b></del>					
<del>4312.0</del>	ATTACHED GARAGE, PARKING AND DRIVEWAY					
<del>4312.1</del>	Attached garage	Permitted <sup>(6)</sup>	Permitted <sup>(6)</sup>	Permitted <sup>(6)</sup>		Permitted <sup>(6)</sup>
<del>4312.2</del>	Minimum parking spaces	✓ <sup>(7)(8)</sup>	✓ <sup>(7)(8)</sup>	✓ <sup>(7)(8)</sup>		✓ <sup>(7)(8)</sup>
<del>4312.3</del>	Minimum visitor parking spaces	✓ <sup>(7)</sup>	✓ <sup>(7)</sup>	✓ <sup>(7)</sup>		✓ <sup>(7)</sup>

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Proposed RM9, RM10 and RM11 Zone Regulations, September 2017

Column	A	B	C	D
Line 1.0	ZONES	RM9	RM10	RM11
<del>4</del> 12.4	Maximum driveway width	2.6 m <sup>(8)</sup>	2.6 m <sup>(8)</sup>	2.6 m <sup>(8)</sup>
<del>4</del> 13.0	<b>PARKING AREAS AND PARKING STRUCTURE SETBACKS</b>			
<del>4</del> 13.1	Minimum setback between a parking space and an interior side lot line and/or rear lot line	3.0 m	3.0 m	3.0 m
<del>4</del> 13.2	Minimum setback of a parking structure constructed above or partially above finished grade to any lot line	6.0 m	6.0 m	6.0 m
<del>4</del> 13.3	Minimum setback of a parking structure constructed completely below finished grade to any lot line	3.0 m	3.0 m	3.0 m
<del>4</del> 14.0	<b>INTERNAL ROADS AND SIDEWALKS</b>			
<del>4</del> 14.1	Minimum width of a condominium road	7.0 m	7.0 m	7.0 m
<del>4</del> 14.2	Condominium roads are permitted to be shared with abutting lands zoned to permit stacked townhouse, back to back townhouse, townhouse or apartment dwelling, or any combination of dwellings thereof	✓	✓	✓
<del>4</del> 5-315.0	<b><u>MINIMUM WIDTH OF SIDEWALKS AND WALKWAYS</u></b>			
<del>15.1</del>	<u>Minimum width of a sidewalk traversed by a driveway</u>	<del>2.0 m</del>	<del>2.0 m</del>	<del>2.0 m</del>
<del>15.2</del>	<u>Minimum width of a sidewalk not traversed by a driveway</u>	<del>1.8 m</del>	<del>1.8 m</del>	<del>1.8 m</del>
<del>15.34</del>	Minimum width of a walkway	<del>1.58 m</del>	<del>1.58 m</del>	<del>1.58 m</del>
<del>16.0</del>	<b>MINIMUM AMENITY AREA AND LANDSCAPED AREA</b>			
<del>16.1</del>	Minimum landscaped area	40% of lot area	40% of lot area	40% of lot area
<del>16.2</del>	Minimum landscaped soft area per parcel of tied land	n/a	n/a	7.5 m <sup>2</sup>
<del>16.3</del>	Minimum landscape buffer abutting any side and rear lot line	3.0 m	3.0 m	3.0 m

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**Proposed RM9, RM10 and RM11 Zone Regulations, September 2017**

Column		A	B	C	D
Line 1.0	ZONES		RM9	RM10	RM11
16.4	Minimum amenity area		The greater of 5.6 m <sup>2</sup> per dwelling unit or 10% of the lot area <sup>(9)</sup>	The greater of 5.6 m <sup>2</sup> per dwelling unit or 10% of the lot area <sup>(9)</sup>	The greater of 5.6 m <sup>2</sup> per dwelling unit or 10% of the lot area <sup>(9)</sup>
16.5	Minimum percentage of total required amenity area to be provided in one contiguous area		50%	50%	50%
16.6	Minimum contiguous private outdoor space per unit		6.0 m <sup>2</sup>	6.0 m <sup>2</sup>	6.0 m <sup>2</sup>
16.7	Minimum setback of a rooftop amenity space from all exterior edges of a building		1.2 m	1.2 m	1.2 m
17.0	<b>ACCESSORY BUILDINGS AND STRUCTURES</b>				

**NOTES:**

- (1) Exclusive of wing walls, utility service walls, and garbage rooms.
- (2) Measured to the highest ridge of a sloped roof.
- (3) See also Subsections 4.1.7 and 4.1.8 of this By-law.
- (4) Only applies to lands zoned RM7 if used for Duplex and/or Triplex.
- (5) Where there are buildings with different heights on one lot, the average of the required setbacks shall be used.
- (6) See also Subsection 4.1.12 of this By-law.
- (7) See also Part 3 of this By-law.
- (8) See also Subsection 4.1.9 of this By-law.
- (9) Excludes private amenity space.
- (10) See also subsection 4.1.2 of this By-law.

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**Proposed RM12 Zone Regulations**

Column	A	B
Line 1.0	ZONES	RM12
<b>PERMITTED USES</b>		
2.0	RESIDENTIAL	
2.1	Back to Back Townhouse on a Street	✓
<b>ZONE REGULATIONS</b>		
3.0	MINIMUM LOT FRONTAGE	
3.1	Interior Lot	6.0 m
3.3	Corner Lot	10.5 m
4.0	MAXIMUM NUMBER OF DWELLING UNITS IN A BACK TO BACK TOWNHOUSE BLOCK	12
5.0	MAXIMUM DWELLING HEIGHT	
5.1	Flat roof	11.0 m and 3 storeys
5.2	Sloped roof	15.0 m <sup>(1)</sup> and 3 storeys
6.0	MINIMUM FRONT YARD	4.5 m <sup>(2)(3)</sup>
6.1	Front garage face	6.0 m
7.0	MINIMUM EXTERIOR SIDE YARD	4.5 m <sup>(2)(3)</sup>
7.1	Front garage face	6.0 m
8.0	MINIMUM INTERIOR SIDE YARD	
8.1	Attached Side	0.0 m
8.2	Unattached Side	1.5 m <sup>(2)</sup>
8.3	Where any portion of the interior lot line abuts a zone permitting detached and/or semi-detached dwellings	7.5 m <sup>(2)</sup>
9.0	MINIMUM REAR YARD	0.0 m
10.0	MAXIMUM ENCROACHMENT OF A BALCONY ATTACHED TO A FRONT WALL	3.0 m
11.0	MINIMUM SETBACK FROM A PORCH, EXCLUSIVE OF STAIRS	2.0 m
12.0	ATTACHED GARAGE, PARKING AND DRIVEWAY	
12.1	Attached garage	Permitted <sup>(4)</sup>
12.2	Minimum parking spaces	✓ <sup>(5) (6)</sup>
12.3	Maximum driveway width	2.6 m <sup>(6)</sup>

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**Proposed RM12 Zone Regulations**

<b>13.0</b>	<b>MINIMUM LANDSCAPED AREA</b>	
13.1	Minimum landscaped area	6.5 m <sup>2</sup>
13.2	Minimum percentage of required front yard landscaped area to be landscaped soft area	75%
<b>14.0</b>	<b>AMENITY SPACE</b>	
14.1	Minimum contiguous private outdoor amenity space	6.0 m <sup>2</sup> (7)
14.2	Maximum encroachment of a balcony attached to a front wall	2.5 m

**NOTES:**

- (1) Measured to the highest ridge of a sloped roof.
- (2) See also Subsections 4.1.7 and 4.1.8 of this By-law.
- (3) Air conditioning equipment is permitted in the required front yard, provided it is located on a balcony.
- (4) See also Subsection 4.1.12 of this By-law.
- (5) See also Part 3 of this By-law.
- (6) See also Subsection 4.1.9 of this By-law.
- (7) Exclusive of landscaped area at-grade.

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**Proposed New and Amended Definitions**

<b>CEC - Road</b>	means a private right-of-way for vehicular travel over common elements that are maintained by a common element condominium corporation.
<b>CEC - Parcel of Tied Land</b>	means an area of land associated with a common element condominium.
<b>Context Grade</b>	<p>Means, with reference to a townhouse, back to back townhouse or stacked townhouse, the average of 12 grade points, eight of which are taken around the perimeter of the site and four of which are based on the location of the proposed building(s):</p> <ul style="list-style-type: none"> <li>-2 points at the centreline of the street extending from the side property lines</li> <li>-2 points located 10 cm outside the subject site from where the side property lines meet the front property line</li> <li>-2 points located 10 cm outside the subject site at the midpoint of the side property lines</li> <li>-2 points located 10 cm outside the subject site, measured out from the side property lines, from where the side and rear property lines meet</li> <li>-4 points taken <u>34.5</u> m from the corners of the proposed buildings</li> </ul>
<b>Driveway</b>	means an internal roadway that is not a street, private road, CEC - road, condominium road or lane, which provides vehicular access from a street, private road, CEC - road, condominium road or lane to parking or loading spaces.
<b>Back to Back Townhouse</b>	means a building that has four or more dwelling units divided vertically, including a common rear wall, each with an independent entrance and has a yard abutting at least one exterior wall of each dwelling unit.
<b>Stacked Townhouse</b>	means a building that has four or more dwelling units divided horizontally and/or vertically, each with an entrance that is independent or through a shared landing and/or external stairwell. Units may also be divided vertically by a common rear wall.
<b>Townhouse</b>	means a building that has three or more attached dwelling units divided vertically above grade by a party wall at least 5.0 m in

<p><b>DRAFT</b> – For discussion purposes, subject to change</p>
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	length and at least 2.0 m in height, and has a yard abutting at least two (2) exterior walls of each dwelling unit.
<b>Condominium Road</b>	means a private right-of-way over private property for vehicular travel which provides access to buildings and/or dwelling units on the same property, is not maintained by a public body, and includes CEC-Road.
<b>Sidewalk</b>	means an area for pedestrian travel that is abutting a street, condominium road or private road.
<b>Walkway</b>	means an area for pedestrian travel that provides access within or to a property that is not abutting a street, condominium road or private road.

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STAKEHOLDER COMMENTS ON ZONING BY-LAW REGULATIONS AND URBAN DESIGN GUIDELINES FOR BACK TO BACK AND STACKED TOWNHOUSES

Item	Respondent / Site of Interest (if applicable)	Date	Issue/Summary of Comment	Staff Comment
1	Daniel Teperman, Haven Developments	March 29, 2017	<p>Will basement units be totally prohibited?</p> <p>Are there incentives for developers to increase proposed heights and densities of developments?</p>	<p>The draft Urban Design Guidelines (UDGs) and proposed Zoning By-law (ZBL) regulations have been revised to remove the previously proposed prohibition on basement units. Additional regulations have been added to ensure basement units are appropriately designed with adequate access to light and air.</p> <p>No, the current study does not propose incentives for developers to increase proposed heights and densities.</p> <p><b>ACTION:</b> The ZBL regulations and UDGs have been updated to no longer prohibit basement units. Additional regulations and guidelines have been added regarding the design of basement units.</p>
2	Mark Bozzo, Queenscorp Group	March 29, 2017	<p>The requirement for a 1.2 m (3.9 ft.) setback from a rooftop amenity space to all exterior edges of a building is concerning. The requirement should be reduced to 0.5 m (1.6 ft.).</p>	<p>Based on the requirement for a minimum of 6 m<sup>2</sup> (64.6 ft<sup>2</sup>) of private outdoor space per unit and the minimum 3 m (9.8 ft.) setback of rooftop mechanical rooms from the building edge, the City agrees that 1.2 m (3.9 ft.) may be excessive in some cases.</p> <p><b>ACTION:</b> The ZBL regulations and UDGs have been revised to reduce the requirement to 1.0 m (3.3 ft.) for rooftop amenity space to exterior edges. The UDGs also clarify that the requirement is only applicable where rooftop amenity spaces overlook abutting properties, not internal to the development.</p>

STAKEHOLDER COMMENTS ON ZONING BY-LAW REGULATIONS AND URBAN DESIGN GUIDELINES FOR BACK TO BACK AND STACKED TOWNHOUSES

3	Bell Canada	March 30, 2017	<p>Bell understands the City’s objective to mitigate the visual impacts of utilities and services from public streets and is committed to working closely with municipalities to achieve this objective in a manner that does not compromise the provision and maintenance of utility infrastructure.</p> <p>Bell is generally supportive of the guidelines and proposes the following revisions:</p> <p>Bullet #1: “2.17 The location of above and below grade utilities and services should be considered in the early stages of site design to ensure they meet utility requirements (<u>ease of maintenance, access</u>) and that any visual impacts from public streets are mitigated.”</p> <p>Bullet #2: “2.17 <u>Where it is feasible to do so</u>, locate above and below grade utilities, easements, etc. to ensure sufficient unencumbered space is provided for public and private trees, and landscaped soft areas.”</p>	<p>The City will continue to work with Bell Canada and other utility providers.</p> <p><b>ACTION:</b> The UDGs have been updated to reflect the proposed wording.</p> <p><b>ACTION:</b> None. The applicant should plan appropriately to ensure adequate space is provided for landscaping/vegetation and utilities.</p>
4			<p>Bell Canada has developed an Urban Design Manual (UDM) which speaks to the location and configuration of utility infrastructure to balance ease of access with design.</p>	<p><b>ACTION:</b> The UDGs revised to include reference to Bell Canada’s UDM.</p>
5	Building Industry and Land Development	April 3, 2017	<p>The broad application of the UDGs to all back to back and stacked townhouses (BBTs and STs) is a concern for BILD</p>	<p>It is not the City’s intention to hinder creativity. As is the case with all of the City’s existing UDGs and standards, as well as</p>

	Association (BILD) – Peel Chapter		members as it may hinder a project’s ability to identify creative solutions to contextual situations.	Zoning By-law regulations, the City needs to establish minimum design expectations and zoning regulations for BBTs and STs.  The proposed ZBL regulations include four zones (a modified RM9 zone and three new zones). The introduction of these additional zones is intended to recognize the different types of BBTs and STs. This is contrary to the existing RM9 zone which is broadly applied to various types of BBTs and STs.  <b>ACTION:</b> None
6			BILD members suggest using words like “encourage” and “promote”, rather than “provide”, “ensure”, “require” and/or “should”, which are restrictive in nature to allow for flexibility in the application of the guidelines.	Through this process, UDGs and ZBL regulations are proposed. The UDGs are guidelines and are intended to be more flexible based on context and other factors. The ZBL establishes regulations which shall be complied with; otherwise applicants have the option to apply for a minor variance through the Committee of Adjustment or submit a Zoning By-law Amendment.  Refer to response to comment #16.  Comments from other stakeholders indicate that the language in the UDGs is too vague.  <b>ACTION:</b> None
7			It is suggested that a “How to Use this Document” section be included in the guidelines to provide clarity for the reader and establish a consistency in how the guidelines are interpreted and implemented.	The UDGs are organized into a checklist format. Applicants are encouraged check off each section of the UDGs as they prepare a concept for BBTs and STs to ensure they have considered all principles and directives.  <b>ACTION:</b> Modified wording included in the

				<p>UDGs advising readers to “review and check each principle when complete”.</p>
<p>8</p>			<p>How do the proposed guidelines take into consideration Regional Official Plan Amendment (ROPA) No. 27 and the Region’s Healthy Development Framework and Assessment Tool?</p>	<p>ROPA 27 encourages communities to be age-friendly, walkable, provide access to transit services and contain a mix of housing options. It also promotes the use of universal accessibility design to enhance the mobility and independence of all residents. The City similarly encourages all of these elements as part of a complete community and good planning and design. The key objectives for the UDGs refer to some of these elements, including:</p> <ul style="list-style-type: none"> <li>• Design to meet the needs of people of all ages, abilities and incomes</li> <li>• Connect streets and provide pedestrian linkages</li> </ul> <p>Additional principles are included in the guidelines themselves, including:</p> <ul style="list-style-type: none"> <li>• Avoiding excessively long development blocks to promote pedestrian connections</li> <li>• Limiting the number of risers into a unit to minimize physical barriers for residents</li> <li>• Establishing minimum common and private amenity areas for residents</li> <li>• Requiring at least one barrier-free path of travel through the development that meets AODA standards</li> </ul> <p>The Region’s Healthy Development Framework and Assessment were not part of the scope of this project. The City is currently</p>



				<p>assessing ROPA 27 and determining how best to incorporate it's requirements into the City's policies and development approval process. This work is being undertaken outside of the UDGs and ZBL for BBTs and STs.</p> <p><b>ACTION:</b> None</p>
9			<p>Why are BBTs and STs subject to a 45 degree angular plane, 2 m (6.6 ft.) maximum encroachment of a deck and a maximum slope of 3:1 for landscape buffers?</p>	<p>A 45 degree angular plane is used to ensure that shadow impacts are minimized and that sufficient light and air are able penetrate into the mews and amenity areas. They are also used to ensure that impacts on adjacent established uses are limited.</p> <p>A 2 m (6.6 ft.) maximum encroachment for a deck is not intended to limit the depth of the deck, rather it is staff's preference that decks be partially recessed to avoid excessive projections beyond the building face.</p> <p>A maximum slope of 3:1 is provided for appropriate conditions for the proper growth of vegetation and is intended to limit the height of retaining walls.</p> <p><b>ACTION:</b> None</p>
10			<p>BBTs and STs are typically 3 to 4 storeys in height and are considered a low-rise development and do not create an imposition on the public realm, especially as it relates to shadow impacts. These standards limit the efficiency of a development site and reduces affordability of each unit.</p>	<p>The impacts from BBTs and STs relates a great deal to how they have been designed. Some may have greater impacts than others. Under Mississauga Official Plan, BBTs and STs are a medium density built form and therefore their impacts need to be appropriately mitigated.</p> <p><b>ACTION:</b> None</p>

11			<p>Provide reasoning behind proposed maximum block length of 41 m (134.5 ft.) or 8 linear modules.</p>	<p>The proposed maximum block length is intended to address excessively long blocks without visual breaks/relief and to ensure adequate mid-block pedestrian connections are provided.</p> <p><b>ACTION:</b> Since the initial draft documents were released, staff have removed maximum block lengths from the draft ZBL regulations. The principle remains in the UDGs.</p>
12			<p>Members find the guidelines regarding below grade units to be confusing. Partially below-grade units are very common in STs and members are concerned that the City will be inclined to refuse partially below-grade units.</p>	<p>Refer to response to comment #1.</p> <p><b>ACTION:</b> Additional graphics added to the UDGs to describe what is the 1<sup>st</sup> storey vs. basement level and below grade units.</p>
13			<p>The proposed restriction of the number of stairs to a unit entrance of 3 to 5 is a concern as there are cases where the existing site grades would find a split staircase more suitable. The City should incorporate this additional design concept into the final set of guidelines.</p>	<p>Although it is recognized that there may be cases where existing site grades dictate the need for additional risers, this principle was included to discourage the manipulation of site grades which has become common with this built form to achieve maximum height restrictions under the OBC. Additionally, based on our discussions with the City's Chief Building Official, the maximum height of a porch according to the OBC is 1.5 m (4.9 ft.).</p> <p>The UDGs do not restrict the use of split stair cases.</p> <p><b>ACTION:</b> Since the initial draft documents were released, staff have amended the Guidelines to suggest a limit on the number of stairs to a unit entrance to 3 to 7 risers.</p>

<p>14</p>			<p>Members do not agree with the requirement for a common outdoor amenity area on all new multi-unit residential developments, especially for smaller sites or sites adjacent to a functional park. Common amenity areas affect condo fees and affordability of units. Additionally, the <i>Planning Act</i> already allows municipalities to require parkland dedication, therefore the additional outdoor amenity area requirement would result in developers inability to maximize the efficiency of the site and provide fewer units.</p>	<p>As indicated in the City's Outdoor Amenity Area Design Reference Note, only developments with more than 20 residential units are required to provide outdoor amenity areas.</p> <p>The outdoor amenity area requirement of the greater of 5.6 m<sup>2</sup> (60.3 ft<sup>2</sup>) per unit or 10% of the lot area is an existing ZBL regulation in the RM9 zone and therefore staff are only recommending that this requirement be carried forward into the new zone regulations.</p> <p>Staff have been flexible in accepting various types of amenity areas (i.e. tot lots, communal gathering space, passive recreational space, indoor common rooms, etc.) within a development.</p> <p>The matter of parkland dedication is separate from this exercise.</p> <p><b>ACTION:</b> The UDGs have been updated to reflect the requirements of the City's Outdoor Amenity Area Design Reference Note and indicate that a common outdoor amenity area is required for new developments with greater than 20 units.</p>
<p>15</p>	<p>Q4A (on behalf of Mattamy Homes)</p>	<p>April 12, 2017</p>	<p>Why do the guidelines refer to four storeys? Four storeys is a Part 3 building under the OBC and requires sprinklers and fire house standpipes. This built form is 3.5 storeys, with the lower level half sunken.</p>	<p>The UDG and ZBL regulations are prepared based on the definitions contained in Zoning By-law 0225-2007. A storey is defined differently under the City's Zoning By-law than it is in the OBC.</p>

16			<p>The checklist of principles in the guidelines states that “the following principles are to be considered when designing...”. This language is too vague.</p>	<p>Refer to response to comment #6.</p> <p>Comments from other stakeholders indicate that the language in the UDG is too restrictive.</p> <p><b>ACTION:</b> None</p>
17			<p>Angular planes are usually reserved for taller structures impinging on the enjoyment of sunlight of lower structures. At 3.5 storeys heights, angular planes seem like overkill.</p>	<p>Refer to response to comment #9.</p> <p>Angular planes are also used for 1 and 2 storey buildings. At Council’s direction, we require shadow studies for all buildings 10.7 m (35.1 ft.) or taller.</p> <p><b>ACTION:</b> None</p>
18			<p>The proposed 15 m (49.2 ft.) separation between front walls of buildings is an exaggeration. A 15 m (49.2 ft.) setback is a throwback to a less dense building form, more landscaping common area and more condo fees to maintain these areas. Separation is greatly related to building height and the width and scale of spaces. The taller the building, the wider the separation, but in no case wider than 11 m (36.1 ft.) or 12 m (39.4 ft.) maximum.</p>	<p>A 15 m front wall to front wall separation may be excessive for a 3 storey building. However, 15 m (49.2 ft.) is appropriate for a 4 storey built form to ensure adequate sunlight can get into the mews.</p> <p><b>ACTION:</b> The UDG and ZBL regulations have been updated to reduce the minimum front wall to front wall separation for a 3 storey building to 12 m (39.4 ft.).</p>
19			<p>It is arbitrary to limit block length at 41 m (134.5 ft.). Super long blocks are undesirable, but they should be evaluated on a case by case basis in consideration of the whole project.</p>	<p>Refer to response to comment #11.</p>
20			<p>There are affordability issues when limiting a minimum width of a townhouse [5 m (16.4 ft.) width proposed].</p>	<p>The livability and functionality of a unit should not be compromised to achieve affordability. The proposed 5 m (16.4 ft.) minimum unit width is consistent with the</p>

				<p>minimum townhouse unit width, which is also appropriate for this built form.</p> <p><b>ACTION:</b> None</p>
21			<p>While yes, retaining walls should not be very high, a maximum 0.6 m (2.0 ft.) height seems devoid of context. Grading will dictate much of this and a wall taller than 0.6 m (2.0 ft.), appropriately designed should be acceptable.</p>	<p>It is recognized that there are instances where retaining walls, sometimes taller than 0.6 m (2.0 ft.), may be required based on existing site grading. This directive is included to address grade manipulation, which is becoming increasingly common with this built form. Staff are concerned that numerous retaining walls, at significant heights, are both undesirable from an aesthetics perspective and can become a maintenance liability for the condominium corporation in the future. As such, we discourage retaining walls wherever possible.</p> <p><b>ACTION:</b> None</p>
22			<p>Provide clarification on what a “below grade” unit is.</p>	<p>Refer to response to comment #12.</p>
23			<p>The guidelines should not prescribe roof forms.</p>	<p>It was not the City’s intent to prescribe roof form rather mitigate excessive roof height.</p> <p><b>ACTION:</b> Modifications to the UDG have been made to indicate “where appropriate”.</p>
24			<p>The 3 m (9.8 ft.) setback at the roof for mechanical enclosures is impractical, especially in end units.</p>	<p>It was not the City’s intent to impact end units. The requirement will be evaluated on a case by case basis, based on adjacent land uses.</p> <p><b>ACTION:</b> None</p>
25			<p>No stucco and no wood policy is arbitrary. These building materials are valid and if detailed carefully are totally</p>	<p>It is the City’s intent to ensure solid and durable materials are used, especially within the first 2 storeys, to avoid costly</p>



			acceptable.	maintenance issues. <b>ACTION:</b> None
26			The limit of 3 to 5 risers per unit entrance is fairly prescriptive and could become impractical in sloping sites.	Refer to response to comment #13.
27			Can the 2 m (6.6 ft.) maximum balcony projection be increased to 2.5 m (8.2 ft.) if the balcony is recessed?	Balconies can be as deep as desired, as long as only 2 m (6.6 ft.) is projecting beyond the building face. <b>ACTION:</b> None
28			Is a 2 m (6.6 ft.) sidewalk required on both sides of the condo road?	The draft UDGs have been amended to eliminate the requirement for sidewalks on both sides of a condominium road. With this said however, there may be instances (e.g. in large developments) where sidewalks on both sides of the road are warranted. This will be determined on a site by site basis.  Additionally, minimum sidewalk and walkway widths have been amended. A 2 m (6.6 ft.) wide sidewalk is only required when it is traversed by a driveway, in all other cases, a 1.8 m (5.9 ft.) sidewalk is required. The width of internal walkways have also been reduced to 1.5 m (4.9 ft.).  <b>ACTION:</b> The UDGs have been amended to no longer require sidewalks on both sides of a condominium road.
29	Port Credit West Village Partners Inc. (WVP) / 70 Mississauga Road South	April 13, 2017 and September 25, 2017	The group assumes and desires that a series of site-specific guidelines that address and respond to the site-specific constraints of the site will be developed through their development application process.	Development applications that are currently in process and have been deemed complete will be evaluated against the policies and regulations in effect at the time of submitting the applications. With this said however, the UDGs for BBTs and STs are based on good

**STAKEHOLDER COMMENTS ON ZONING BY-LAW REGULATIONS AND URBAN DESIGN GUIDELINES FOR BACK TO BACK AND STACKED TOWNHOUSES**

				<p>planning and urban design principles and should therefore be considered in all developments proposing this built form, regardless of when the applications were submitted.</p> <p><b>ACTION:</b> None</p>
30			<p>The group agrees with the guidelines' overall intent of providing for compatibility with and sensitivity to the established context, minimizing undue impacts on adjacent properties, and providing for a high quality of development.</p>	<p>Noted.</p> <p><b>ACTION:</b> None</p>
31			<p>The Design Guidelines should function to facilitate design excellence and provide flexibility for creative solutions. It is important that these guidelines recognize site-specific issues and offer an adaptable approach.</p>	<p>Noted. Refer to response to comment #6.</p>
32	Glen Broll, Glen Schnarr & Associates Inc.	May 16, 2017	<p>Modifications to the City's Fire Route By-law are required. The current By-law requirements impede affordability.</p>	<p>Amendments to the Fire Route By-law are outside the scope of this project. Any comments with respect to the Fire Route By-law should be directed to Fire and Emergency Services.</p> <p>From a planning and urban design perspective, life safety should not be compromised for affordability. BBT and ST developments can be difficult to navigate under normal circumstances, let alone in an emergency situation. Emergency response times should not be compromised for any reasons.</p> <p><b>ACTION:</b> None</p>

33			<p>Mews should be included in the amenity area calculation.</p>	<p>The City disagrees that mews should be included in the amenity area calculation. Mews are intended to provide access to individual units and not intended to be a primary amenity space on-site. However, should an enhanced mews be proposed substantially larger than the minimum By-law requirements, then the mews may be included in the amenity area calculation.</p> <p><b>ACTION:</b> To provide greater clarity regarding the exclusion of mews in amenity areas, the definition of “Amenity Area” in the ZBL has been amended and additional regulations have been added to delineate what spaces count towards the amenity area calculation. The UDGs also indicate that a mews is not an amenity area.</p>
34	BILD	July 12, 2017	<p><i>Coordination of Utility Locations</i>                  A large component to developing a site is the coordination of utility locations. As such, we kindly suggest that City staff engage local utility providers in the consultation process of the draft Guidelines to understand and align utility-related requirements. Specific areas of concern for the industry include, but are not limited to:</p> <ul style="list-style-type: none"> <li>• The common demand from a number of utility and communication providers for a “blanket” easement over the whole grounds.</li> </ul>	<p>The City has engaged all utility companies during the preparation of the UDG and ZBL regulations for BBTs and STs. We have also met with Alectra Utilities and Enbridge Gas. We will continue to work with the utility companies to determine their requirements early in the development process.</p> <p>Utility companies have indicated a desire for developers to engage them earlier in the process to allow for the greatest flexibility in locating utilities.</p> <p>Noted. This requirement should be discussed with the utility companies directly.</p>

			<ul style="list-style-type: none"> <li>• The requirement for a central mail kiosk should be subject to the latest Canada Post Multiple Units Box design and standards.</li> <li>• It is our position that parapet walls on the perimeter of flat roofs should not count on the height of buildings and be limited to the 0.6 m (2.0 ft.) in height restrictions.</li> <li>• The UDG requirements for combined landscape soft areas for tree growth does not take into consideration the required utility corridor for hydro, lighting, telecom, and civil engineering. These requirements are noted below for your reference:             <ul style="list-style-type: none"> <li>○ If the site proposes BBTs with surface mounted parking (i.e. driveway/garage) then a 3 m (9.8 ft.) wide utility corridor will be required.</li> <li>○ If the site proposes STs sitting on top of a parking garage structure then you will have a central located electrical room where the hydro/telecom services will egress from.</li> <li>○ If the site proposes STs siting slab on grade with surface mounted parking, then the hydro meters will</li> </ul> </li> </ul>	<p>Agreed. Applicants are encouraged to consult with Canada Post directly.</p> <p>Based on the current definition of Height in the Zoning By-law, parapets are not included in the measurement of height. No change to this definition is proposed through the ZBL amendments.</p> <p>Required utility corridors are to be located outside of landscaped areas. It is the developers responsibility to ensure that utility requirements are satisfied.</p> <p>Noted.</p> <p>Noted.</p> <p>Noted.</p>
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			<p>be located on the end walls of the block, with the gas meters on the opposite side. The hydro meters would be located within an electrical closet with 24/7 access for hydro. Typically the electrical closet is 6-8' wide x 1' high x 2' deep with double doors. Please refer to the attached PDF of the multiple metering guide for more information - this application meets ESA standards.</p> <ul style="list-style-type: none"> <li>The placing of hydro and gas metres and other utilities in concealed or recessed locations only works for detached units or row housing. It would not be compatible with stacked townhouses. There may be scenarios where it may be possible with back to back townhouses with surface mounted parking, but only if an architect has come up with a concept to conceal the meter locations at the front of the units. There should be flexibility in the Guidelines to consider these instances.</li> </ul>	<p>Noted.</p> <p><b>ACTION:</b> The UDGs have been updated to state “where feasible” and “less visible location” to recognize that it may not always be possible to conceal or recess utility meters.</p>
35			<p><i>Waste Collection Services</i> We are generally in agreement with the City’s intent to ensure that waste collection areas should not be visible from a public street. However, this requirement may not be achievable at all times and there should be an opportunity for good judgement and compromise between City staff and the developer.</p>	<p>In cases where it may not be possible to locate waste collection areas interior to a development, the applicant should work with staff to develop an approach to screening the area from the public street.</p>



			<p>Members request City staff to clarify, with the Region of Peel, standards and practices around waste collection services, including those provided by private companies. It is our understanding that the Region discourages the use of private services and, as such, we ask City staff to review the Guidelines with its upper-tier municipality.</p>	<p>The Region of Peel does not allow private waste collection for residential developments. This is a result of existing condominium corporations with private waste collection deciding to change to Regional collection and there being challenges because the developments were not designed in accordance with Regional Waste Collection Design Standards Manual.</p> <p>All developments are required to comply with the Regional Waste Collection Design Standards Manual. Any questions about the requirements of the Design Manual should be directed to Region of Peel staff.</p> <p><b>ACTION:</b> None</p>
36			<p><i>Rooftop Amenity Spaces and Set Backs</i> Our members do not agree with the requirements for a 45 degree angular plane. While it suggests that this angular plane should be maintained to reduce overlook and allow for sunlight into units, we do not think that this guideline is appropriate for this type of low-rise built form.</p> <p>There are alternative ways to address overlook concerns. Additionally, appropriate facing distances can provide for better sunlight to units. City staff should also acknowledge that sometimes new development units back onto a park or open space and not a residential area. In these instances, we believe it would be unnecessary to apply the proposed</p>	<p>Refer to response to comment #9.</p>

			angular plane.  Members think the Guidelines should not limit BBTs to 3 storeys if height is defined in metres by the zoning by-law.	The product that has been presented to the City is 3 storeys plus a rooftop terrace. Staff are flexible in building height depending on the context and adjacent land uses.  <b>ACTION:</b> None
37			<i>Block Lengths</i> We request that the guidelines do not include a dimension limitation such as the 41 m (134.5 ft.) length, and instead only suggest the number of linear models.	Refer to response to comment #11.
38			<i>Grading and Retaining Walls</i> The 3 m (9.8 ft.) landscape buffer around the property is excessive and not necessary in all site conditions.  Unencumbered buffers by a below-grade structure is also difficult to achieve, and we are unclear as to the design rationale behind this requirement.  The requirement for minimum soil volumes on top of all underground structures is not necessary.	The compact nature of BBTs and STs is such that there is limited space for on-site tree planting and landscaping. In order to ensure that an adequate landscape buffer is provided between new and existing development, a minimum 3 m (9.8 ft.) wide landscape buffer is proposed.  This 3 m (9.8 ft.) wide landscape buffer shall not be encumbered by below-grade structures or utilities to ensure that there is an adequate soil volume for trees and vegetation to grow on the existing property and to minimize impacts on adjacent properties.  We disagree. Minimum soil volumes are necessary for tree growth.  <b>ACTION:</b> None

<p>39</p>			<p><i>Building Elevations</i>                  The guidelines recommend sloped roofs and half-storeys on upper levels but we believe it should really state that the intent is to reduce perceived height and scale. There are a number of ways to achieve this without resorting to sloped roofs and limiting design.</p> <p>Smaller units may not be able to accommodate the 3 m (9.8 ft.) setback of rooftop mechanical rooms to exterior edges of buildings. As an alternative, setbacks could be reduced without causing negative visual impact through quality architectural design of mechanical rooms.</p> <p>The guidelines state that buildings should be designed with high quality and durable materials, specifically discouraging the use of stucco and wood. Members feel this guideline should instead focus on good design and appropriate materials to express the architectural design, with durability as a consideration.</p>	<p>Refer to response to comment #23.</p> <p>Refer to response to comment #24.</p> <p>Refer to response to comment #25.</p>
<p>40</p>			<p><i>Below Grade Units</i>                  Below grade units offer an affordable housing option that can be designed to allow for sufficient sunlight if the appropriate separation distances are provided.</p>	<p>Below grade units may provide opportunities for more affordable units within a development. However, we do not believe that livability and functionality of units should be compromised to achieve affordability, especially when it comes to availability of light and air to below grade units. Refer to response to comment #1.</p>

			<p>The requirement for through units is too restrictive and we request that this not be part of the Guidelines.</p> <p>Additional, specific concerns related to below grade units:</p> <ul style="list-style-type: none"> <li>• If there is a desire to restrict below grade units, it would be appropriate to not allow their use along public streets.</li> <li>• Below grade units require a minimum 6 m<sup>2</sup> (64.6 ft<sup>2</sup>) private outdoor space located at grade, but this may not actually be the homebuyers preference. Some may prefer additional interior unit space instead and the flexibility to have more square footage should be allowed in the UDGs.</li> <li>• It is unclear if unobstructed views and access to daylight means unobstructed by other buildings. Please also clarify if this would allow for privacy screening.</li> <li>• Site grading conditions may dictate if</li> </ul>	<p><b>ACTION:</b> None</p> <p>The intention of requiring through units is to ensure that units have access to light and air.</p> <p><b>ACTION:</b> Recognizing that in some cases it may be challenging to implement through units, the UDGs have been updated to also allow for double wide units.</p> <p>Basement units are no longer prohibited in the updated documents.</p> <p>The applicant has the opportunity to increase unit size while still providing amenity space.</p> <p>Anything (buildings, retaining walls, landscaping, screening, etc.) that impedes views or access to daylight shall not be permitted.</p> <p>Noted.</p>
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			<p>a unit is below-grade unit or not and if they need to face a certain direction. For example, if there was a BBTs built on a downslope, it would provide the opportunity for the rear unit to be below-grade while the front unit would be at-grade. This all depends on the topography and grading conditions of an area on which a townhouse is built.</p>	
41			<p><i>Private Amenity Space and Common Outdoor Amenity Areas</i>                  The common outdoor amenity area requirement of the greater of 5.6 m<sup>2</sup> (60.3 ft<sup>2</sup>) per unit or 10% of the site area is excessive, especially in combination with parkland dedication requirements.</p> <p>The common outdoor amenity area requirement should be noted in the Zoning By-law and not in the proposed guidelines.</p> <p>A central location for a common outdoor amenity area may not always lead to the best design or practical option.</p> <p>The guideline discouraging the use of balconies for storage areas (bikes, strollers, etc.) is too restrictive. This issue should be resolved through condo documents or encouraging better site and building design that incorporates more storage and places to lock up bikes and strollers.</p>	<p>Refer to response to comment #14.</p> <p>The requirement is included in both the ZBL and UDGs.</p> <p>Agreed, however generally speaking, the preferred location for common amenity area is central to a development to provide equal access to all residents.</p> <p>The City encourages site and building designs that include areas for storage of large bulky items and are also supportive of clauses to this effect being included in condo documentation. However, we are of the opinion that the issue is also worth noting in the UDGs.</p>

42			<p><i>Additional General Comments:</i></p> <ul style="list-style-type: none"> <li>We are unclear as to the rationale behind prohibiting precast stairs. The guidelines should instead focus on the quality of design, appropriate materials, durability, and maintenance.</li> <li>The guideline to provide a walkway at every second block is too rigid and may not always be desirable. More flexibility is required to consider the design and context of walkways on an individual basis.</li> <li>The guidelines stating that “<i>surface parking should be centrally located</i>” may not always be practical or provide for the best design.</li> </ul>	<p><b>ACTION:</b> None</p> <p>Based on our experience, poured in place stairs are more durable and require less maintenance in the long term.</p> <p><b>ACTION:</b> None</p> <p>The directive is included in the UDGs, not in the ZBL. Staff are flexible in terms of applicability on a site-by-site basis.</p> <p><b>ACTION:</b> None</p> <p>Agreed, however it is important that surface parking not be visible from the public street and is located interior to the development. Also, by centrally locating parking it will be equal distance to all units, thus being more convenient for visitors rather than parking on adjacent municipal rights-of-way.</p>
43	Stephane Angers	June 26, 2017	The study currently being undertaken by the City of Mississauga on BBTs and STs represents very reasonable guidelines for development of such high density projects. We are hoping that the City will work with the developers towards meeting these UDGs.	<p>Noted.</p> <p><b>ACTION:</b> None</p>
44	Weston Consulting on behalf of NYX Capital / Tannery Street and Kirwin Avenue	September 22, 2017	<p><i>Interior Side Yard Setbacks</i></p> <p>The City should consider reductions to the default interior side yard setbacks of 4.5 m (14.8 ft.) to 7.5 m (24.6 ft.) where the abutting land is zoned for open</p>	<p>The proposed ZBL regulations already require a minimum interior side yard of 4.5 m (14.8 ft.) abutting land zoned for open space or parks. Increases in interior side yard only</p>



			space, parks or other appropriate zones.	<p>apply if:</p> <ul style="list-style-type: none"> <li>i) The interior side yard abuts a zone permitted a detached and/or semi-detached dwelling.</li> <li>ii) The interior side yard abuts a medium density zone <u>and</u> the rear wall of the proposed building abuts the interior side yard.</li> <li>iii) The front wall of the proposed building abuts the interior side yard.</li> </ul> <p><b>ACTION:</b> None</p>
45			<p><i>Front Yard Setback</i> The City should consider reducing the 7.5 m (24.6 ft.) front yard setback requirement or acknowledge in the guidelines that relief from this requirement is supported under certain circumstances. Many townhouse projects are on higher order streets in areas identified for intensification and a lesser front yard setback is appropriate and desirable from a design perspective.</p>	<p>The majority of BBTs and STs developments are being proposed in existing mature low-rise neighbourhoods and therefore the 7.5 m (24.6 ft.) front yard is intended to reflect and maintain the character of this existing context.</p> <p><b>ACTION:</b> None</p>
46			<p><i>Rear Yard Setback</i> A reduction in the proposed rear yard setback of 7.5 m (24.6 ft.) should be contemplated in instances where rear yards abut open space zones as the overall impact of the reduced rear yard is minimal for existing or future surrounding uses, while still providing a usable rear yard.</p>	<p>The City is willing to consider this on a site-by-site basis, through the appropriate development application.</p> <p><b>ACTION:</b> None</p>
47			<p><i>3 m (9.8 ft.) Landscape Buffer</i> A 3 m (9.8 ft.) wide landscape strip and the prohibition of below grade parking</p>	<p>Refer to response to comment #38.</p>

STAKEHOLDER COMMENTS ON ZONING BY-LAW REGULATIONS AND URBAN DESIGN GUIDELINES FOR BACK TO BACK AND STACKED TOWNHOUSES

			within 3 m of the property limit are difficult to achieve. A reduced landscape buffer both at grade and below grade should be considered.	
48			<i>Common Outdoor Amenity</i> Recognition should be provided for the context of individual projects where proximity to public parks and other amenity area is readily accessible.	Refer to response to comment #14.
49			<i>Angular Plane</i> Building height controlled through a 45 degree angular plane measured from the property lines is not appropriate for all situations and for all property lines. The Guidelines should provide greater clarity on the intent of this guidelines and under what circumstances it does or does not apply.	Refer to response to comment #9.  The 45 degree angular plane is measured from all lot lines.  <b>ACTION:</b> The UDGs have been updated to indicate that the 45 degree angular plane is measured from all lot lines.
50	Glen Schnarr & Associates Inc.	September 25, 2017	The cumulative impact of the proposed UDGs and ZBL Amendment is proving challenging and overly restrictive whereas they should guide design and development only.  The proposed documents are too restrictive for design creativity which can hamper innovation, improved building and site design, and affordability.  The rigid nature of the UDGs and ZBL Amendment doesn't reflect the reality of the uniqueness of each site and its	Refer to responses to previous comments.  It is not the City's intent to hamper innovation and creativity, rather achieve a balance between providing direction and flexibility. However, given the scope and magnitude of challenges that have been encountered with BBTs and STs it is clear that a design expectation needs to be established. BBTs and STs are a complicated built form with many moving parts. The long-term sustainability of these developments is greatly impacted by good initial design and planning.  It is not possible to prepare ZBL regulations and UDGs to reflect the uniqueness of all sites and their context.

			<p>context (i.e. surrounding land uses, grading conditions).</p> <p>The collective amount of prescribed dimensions/requirements in the proposed guidelines and regulations result in design delays as an owner requires more technical inputs from a larger consulting team.</p>	<p>Owners and applicants are strongly encouraged to consider the requirements of the ZBL regulations and UDGs at the initial project development stages. Infill developments, especially those with BBTs and STs, are by nature complex and require technical input from many disciplines to ensure their success.</p> <p><b>ACTION:</b> None</p>
51			<p>The proposed UDGs and ZBL Amendment results in process impacts and needs to recognize development already underway.</p> <p>Additional Zoning restrictions result in over-regulation and don't account for conceptual designs earlier in the process and final designs which form part of future site development plan and building permit applications. This will likely lead to further minor variance applications during final City approvals or post-construction which may mislead the public as to why the minor variances are required.</p> <p>From a process perspective, it is clear that current development applications already are/or could get caught in a more stringent review and re-designing to address new City staff concerns resulting in unnecessary delays. The pipeline</p>	<p>Refer to response to comment #29.</p> <p>Very few sites in the City are pre-zoned to permit BBTs and STs and therefore require at a minimum a rezoning to permit the proposed built form. Through the rezoning process, the onus is on the applicant to identify and justify Zoning By-law deficiencies with the proposed development, since Zoning staff do not review rezoning applications. Future site plan and building permit applications will be evaluated against the Zoning By-law enacted through the rezoning. If a minor variance is required, then the deficiency was not captured by the applicant.</p> <p><b>ACTION:</b> None</p>

STAKEHOLDER COMMENTS ON ZONING BY-LAW REGULATIONS AND URBAN DESIGN GUIDELINES FOR BACK TO BACK AND STACKED TOWNHOUSES

			<p>timing for projects both currently before the City, but also underway with the design/technical teams means that most of the fundamental design elements are already pre-determined based on known City requirements. These designs might be many months in the making and are based on extensive design and technical inputs. The City needs to consider these timelines for new development and grandfather existing development applications at the City and upcoming development applications which did not have the benefit of incorporating new City requirements in to the fundamental design elements.</p>	
52	<p>Weston Consulting on behalf of Sierra Building Group / 4005 Hickory Drive and 650 Atwater Avenue</p>	<p>September 25, 2017</p>	<p><i>Angular Plane Principles</i> Additional information required to identify which property lines the 45 degree angular plane is measured from (side, rear or front) and how the guideline applies to infill development areas where the standard cannot be achieved due to site specific restrictions or “as built” conditions.</p>	<p>Refer to responses to comments # 9 and 49.</p>
53			<p><i>Building Setback Principles</i> The guidelines state “when existing adjacent front yard setbacks vary, new buildings should align with the average setback between the two adjacent properties or the minimum zoning requirements, whichever is greater.”</p> <p>This principle does not have regard for the planned future context of abutting lands. Some flexibility should be allowed.</p>	<p>Acknowledged.</p> <p><b>ACTION:</b> The UDGs have been updated to consider the existing and planned context, where applicable.</p>

54			<p><i>Grading and Retaining Walls Principles</i> It may be difficult to achieve the principle that landscape buffers should be unencumbered by below grade parking structures, easements, retaining walls, utilities, severe grade changes and hard surface area. These matters should be dealt with on a site-by-site basis or a reduced setback should be considered, and may be appropriate in some cases.</p>	<p>Noted. It is the applicant's responsibility to ensure all site requirements are satisfied and landscape areas are unencumbered.</p> <p><b>ACTION:</b> None</p>
55			<p><i>Common Outdoor Amenity Area Principles</i> The requirement for common outdoor amenity area in all new multi-unit residential developments may be difficult to achieve in certain small infill developments with a limited number of units. Consideration should be given to developments with access or direct proximity to park land and open space areas adjacent to the development.</p>	<p>Refer to response to comment #14.</p>
56			<p><i>Implementation</i> We recommend the introduction of transition clauses within the Guidelines and amending By-law. This would provide clarity in relation to the applicable guidelines and policies for applications that have already been submitted under the existing policy and zoning framework.</p>	<p>Refer to response to comment #29.</p>
57			<p><i>Interior Side Yard</i> Minimum interior side yard setbacks should not be required for developments abutting commercial, open space or park zones, as these uses do not require the same transition and sensitivity as</p>	<p>The City disagrees. Minimum interior side yards are not only intended to provide a buffer/transition to adjacent land uses but also to the proposed units themselves. The UDGs and ZBL regulations already allow for</p>

STAKEHOLDER COMMENTS ON ZONING BY-LAW REGULATIONS AND URBAN DESIGN GUIDELINES  
FOR BACK TO BACK AND STACKED TOWNHOUSES

			residential zones.	a reduced interior side yard of 4.5 m (14.8 ft.) adjacent to commercial, open space and park zones. <b>ACTION:</b> None
58			<i>Rear Yard Setback</i> Opportunities for reduced rear yard setbacks should be contemplated in instances where rear yards abut open space zones, as the overall impact of a reduced rear yard may be minimum in some cases, while still providing a usable rear yard.	Refer to response to comment #46.
59			<i>Parking Structure Setbacks</i> The proposed 3.0 m (9.8 ft.) minimum setback of a below grade parking structure to a lot line is overly restrictive. This matter should be dealt with through detailed design once utilities, servicing, shoring, and the identification of preservation of significant vegetation has been considered.	Refer to response to comment #38.
60	Jim Levac, Glen Schnarr & Associates Inc. on behalf of Dunpar Developments / 80 Thomas Street	September 25, 2017	The proposed UDGs and ZBL regulations are a challenge for the proposed development at 80 Thomas Street.  The front and rear yard regulations are outdated. In particular, the front yard requirement of 7.5 m (24.6 ft.) is excessive and will result in front yards turning into rear yards with the storage of play equipment, etc.	Noted.  Refer to response to comment #45. The UDGs and ZBL regulations establish minimum private outdoor space for each unit and common amenity areas for the development to avoid the need for residents to locate play equipment in their front yards. Additionally, these units are clearly sold with no expectation of having a rear yard amenity area, therefore this is irrelevant to this



			<p>The proposed regulation requiring a 1.5 m (4.9 ft.) setback from the side wall of a building to a walkway is excessive [equals 4.5 m (14.8 ft.) required separation from a side wall to a side wall when a walkway is proposed]. A 3 m (9.8 ft.) side wall to side wall separation has always been required.</p>	<p>situation.</p> <p>The 1.5 m (4.9 ft.) setback from a side wall to a walkway is an existing RM9 zone regulation. No change is proposed to this regulation in the new ZBL.</p> <p>With the requirement for a minimum 2.0 m (6.6 ft.) wide sidewalk, the existing RM9 zone regulations require a total side wall to side wall separation of 5.5 m (18.0 ft.) when a walkway is proposed, whereas the new regulations require only a 4.5 m (14.8 ft.) side wall to side wall separation. Meaning the proposed ZBL regulations are actually a reduction from what is currently required.</p> <p><b>ACTION:</b> None</p>
61	Your Home Developments	October 5, 2017	<p>The UDGs and ZBL Amendments outline very workable standards for many of the regulations and guidelines proposed. Many of the changes proposed can improve livability of a stacked townhouse development. Sensitivity to the number of risers for exterior stairs and deletion of below grade entry doors make a lot of sense.</p>	<p>Noted.</p> <p><b>ACTION:</b> None</p>
62			<p>The proposed standards for setbacks to roads and interior side yards threaten the viability of BBTs and STs units by reducing the density that should be able to be achieved with this type of housing.</p> <p>The proposed 7.5 m (24.6 ft.) front yard would only encourage the use of this space as a rear yard. This setback</p>	<p>Noted.</p> <p>Refer to response to comment #60.</p>

**STAKEHOLDER COMMENTS ON ZONING BY-LAW REGULATIONS AND URBAN DESIGN GUIDELINES FOR BACK TO BACK AND STACKED TOWNHOUSES**

			<p>defeats the concept of definable street edges that encourages its use as an interactive outdoor space.</p> <p>The 9.0 m (29.5 ft.) required setback from a front wall to an interior lot line is excessive. This condition should be treated no differently than a rear yard with a 7.5 m (24.6 ft.) setback requirement.</p>	<p>The minimum rear yard where a front wall abuts the rear lot line is 9.0 m (29.5 ft.). The 9.0 m (29.5 ft.) interior side yard and rear yard requirements are to ensure that sufficient space is available for the minimum landscape buffer, walkway, unit setback and porch.</p> <p><b>ACTION:</b> None</p>
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Note: The majority of the verbal comments made at the Open House held on March 29, 2017 and the BILD Peel Chapter Meeting on May 16, 2017 are captured in subsequent written correspondence from various stakeholders and therefore have not been added to the above table. Those comments from the Open House and BILD Peel Chapter Meeting not reflected in subsequent correspondence have been added to the above table.

## Proposed RM9, RM10 and RM11 Zone Regulations, December 2017

Column	A	B	C	D
Line 1.0	ZONES	RM9	RM10	RM11
<b>PERMITTED USES</b>				
2.0	RESIDENTIAL			
2.1	<b>Stacked Townhouse</b>	✓		
2.2	<b>Back to Back Townhouse on a Condominium Road</b>		✓	
2.3	<b>Back to Back Townhouse on a CEC-Road</b>			✓
<b>ZONE REGULATIONS</b>				
3.0	MINIMUM LOT FRONTAGE	38.0 m	38.0 m	38.0 m
4.0	MINIMUM DWELLING UNIT WIDTH	5.0 m	5.0 m	5.0 m
5.0	MAXIMUM DWELLING HEIGHT <sup>(10)</sup>			
5.1	<b>Flat roof</b>	13.0 m and 4 storeys	11.0 m and 3 storeys	11.0 m and 3 storeys
5.2	<b>Sloped roof</b>	17.0 m <sup>(1)</sup> and 4 storeys	15.0 m <sup>(1)</sup> and 3 storeys	15.0 m <sup>(1)</sup> and 3 storeys
6.0	MINIMUM FRONT YARD	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>
7.0	MINIMUM EXTERIOR SIDE YARD	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>
8.0	MINIMUM INTERIOR SIDE YARD	4.5 m <sup>(2)</sup>	4.5 m <sup>(2)</sup>	4.5 m <sup>(2)</sup>
8.1	Where any portion of the <b>interior side lot line</b> abuts a zone permitting <b>detached</b> and/or <b>semi-detached dwellings</b>	9.0 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>
8.2	Where the <b>interior side lot line</b> abuts a RM4, RM5, RM6, RM7, RM8, RM9, RM10, RM11, or RM12 zone and the rear wall of the <b>building</b> abuts the <b>interior side lot line</b>	7.5 m <sup>(2)(3)</sup>	n/a	n/a

Column	A	B	C	D
Line 1.0	ZONES	RM9	RM10	RM11
8.3	Where the front wall of a <b>building</b> abuts the <b>interior side lot line</b>	9.0 m <sup>(2)</sup>	9.0 m <sup>(2)</sup>	9.0 m <sup>(2)</sup>
<b>9.0</b>	<b>MINIMUM REAR YARD</b>	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>
9.1	Where any portion of the <b>rear lot line</b> abuts a zone permitting <b>detached</b> and/or <b>semi-detached dwellings</b>	9.0 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>
9.2	Where a front wall of a <b>building</b> abuts the <b>rear lot line</b>	9.0 m <sup>(2)</sup>	9.0 m <sup>(2)</sup>	9.0 m <sup>(2)</sup>
<b>10.0</b>	<b>ENCROACHMENTS AND PROJECTIONS</b>			
<b>10.1</b>	Maximum encroachment of a <b>deck</b> inclusive of stairs, <b>balcony</b> or awning, attached to a rear or front wall, into a required <b>yard</b>	2.0 m	2.0 m	2.0 m
<b>10.2</b>	Maximum projection, located at the first <b>storey</b> , from any wall of a <b>building</b> , in relation to a below grade patio that provides access to a basement unit	50% of patio depth	50% of patio depth	50% of patio depth
<b>11.0</b>	<b>MINIMUM INTERNAL SETBACKS</b>			
11.1	From a <b>front garage face</b> to a <b>condominium road</b> or <b>sidewalk</b>	6.0 m	6.0 m	6.0 m
11.2	From a <b>front garage face</b> to a <b>condominium road</b> or <b>sidewalk</b> , where the <b>garage</b> and <b>driveway</b> are accessed at the rear of the <b>dwelling unit</b>	1.0 m	n/a	n/a
11.3	From a front wall of a <b>building</b> to a <b>condominium road, sidewalk, walkway</b> or <b>parking space</b>	4.5 m	4.5 m	4.5 m
11.4	From a <b>porch</b> , exclusive of stairs, located at and accessible from the <b>first storey</b> or below the <b>first storey</b> to a <b>condominium road, sidewalk, walkway</b> or <b>parking space</b>	2.5 m	2.5 m	2.5 m
11.5	From a rear wall of a <b>building</b> to a side wall of another <b>building</b> on the same lot	12.0 m	n/a	n/a
11.6	From a rear wall of a <b>building</b> to a rear wall of another <b>building</b> on the same lot	15.0 m	n/a	n/a

Column	A	B	C	D
Line 1.0	ZONES	RM9	RM10	RM11
11.7	From a side wall of a <b>building</b> to a side wall of another <b>building</b> on the same lot	3.0 m	3.0 m	3.0 m
11.8	From a side wall of any <b>building</b> to a <b>walkway</b>	1.5 m	1.5 m	1.5 m
11.9	From a side wall of a <b>building</b> to a <b>condominium road, sidewalk, or parking space</b>	3.0 m	3.0 m	3.0 m
11.10	From a front wall of a <b>building</b> to a front wall of another <b>building</b> on the same lot, where the <b>building</b> is less than or equal to three storeys	12.0 m <sup>(4)</sup>	12.0 m	12.0 m
11.11	From a front wall of a <b>building</b> to a front wall of another <b>building</b> on the same lot, where the <b>building</b> is less than or equal to three storeys and contains a <b>dwelling unit</b> in the <b>basement</b>	15.0 m <sup>(4)</sup>	n/a	n/a
11.12	From a front wall of a <b>building</b> to a front wall of another <b>building</b> on the same lot, where the <b>building</b> is four storeys	15.0 m <sup>(4)</sup>	n/a	n/a
11.13	From a front wall of a <b>building</b> to a side wall of another <b>building</b> on the same lot	9.0 m <sup>(4)</sup>	9.0 m	9.0 m
<b>12.0</b>	<b>ATTACHED GARAGE, PARKING AND DRIVEWAY</b>			
12.1	Attached <b>garage</b>	Permitted <sup>(5)</sup>	Permitted <sup>(5)</sup>	Permitted <sup>(5)</sup>
12.2	Minimum <b>parking spaces</b>	✓ <sup>(6)</sup> <sup>(7)</sup>	✓ <sup>(6)</sup> <sup>(7)</sup>	✓ <sup>(6)</sup> <sup>(7)</sup>
12.3	Minimum visitor <b>parking spaces</b>	✓ <sup>(6)</sup>	✓ <sup>(6)</sup>	✓ <sup>(6)</sup>
12.4	Maximum <b>driveway</b> width	2.6 m <sup>(7)</sup>	2.6 m <sup>(7)</sup>	2.6 m <sup>(7)</sup>
<b>13.0</b>	<b>PARKING AREAS AND PARKING STRUCTURE SETBACKS</b>			
13.1	Minimum setback between a <b>parking space</b> and an <b>interior side lot line</b> and/or <b>rear lot line</b>	3.0 m	3.0 m	3.0 m
13.2	Minimum setback of a <b>parking structure</b> constructed above or partially above finished grade to any <b>lot line</b>	6.0 m	6.0 m	6.0 m

Column	A	B	C	D
Line 1.0	ZONES	RM9	RM10	RM11
13.3	Minimum setback of a <b>parking structure</b> constructed completely below finished grade to any <b>lot line</b>	3.0 m	3.0 m	3.0 m
<b>14.0</b>	<b>INTERNAL ROADS, SIDEWALKS AND WALKWAYS</b>			
14.1	Minimum width of a <b>condominium road</b>	7.0 m	7.0 m	7.0 m
14.2	<b>Condominium roads</b> are permitted to be shared with abutting lands zoned to permit <b>stacked townhouse, back to back townhouse, townhouse</b> or <b>apartment dwelling</b> , or any combination of dwellings thereof	✓	✓	✓
15.3	Minimum width of a sidewalk traversed by a driveway	2.0 m	2.0 m	2.0 m
15.4	Minimum width of a sidewalk not traversed by a driveway	1.8 m	1.8 m	1.8 m
15.5	Minimum width of a <b>walkway</b>	1.5 m	1.5 m	1.5 m
<b>15.0</b>	<b>MINIMUM AMENITY AREA AND LANDSCAPED AREA</b>			
15.1	Minimum <b>landscaped area</b>	40% of <b>lot area</b>	40% of <b>lot area</b>	40% of <b>lot area</b>
15.2	Minimum required <b>landscaped soft area</b>	50% of <b>landscaped area</b>	50% of <b>landscaped area</b>	3.0 m <sup>2</sup> <sup>(8)</sup>
15.3	Minimum <b>landscape buffer</b> abutting any side and <b>rear lot line</b>	3.0 m	3.0 m	3.0 m
15.4	Minimum <b>amenity area</b>	The greater of 5.6 m <sup>2</sup> per <b>dwelling unit</b> or 10% of the <b>lot area</b> <sup>(8)</sup>	The greater of 5.6 m <sup>2</sup> per <b>dwelling unit</b> or 10% of the <b>lot area</b> <sup>(8)</sup>	The greater of 5.6 m <sup>2</sup> per <b>dwelling unit</b> or 10% of the <b>lot area</b> <sup>(8)</sup>
15.5	Minimum percentage of total required <b>amenity area</b> to be provided in one contiguous area	50%	50%	50%
15.6	Minimum contiguous private outdoor space per unit	6.0 m <sup>2</sup>	6.0 m <sup>2</sup>	6.0 m <sup>2</sup>



Column	A	B	C	D
Line 1.0	ZONES	RM9	RM10	RM11
15.7	Minimum setback of a rooftop amenity space from all exterior edges of a <b>building</b>	1.0 m	1.0 m	1.0 m
15.8	Minimum setback from an <b>amenity area</b> to a <b>building, structure</b> or any <b>lot line</b>	3.0 m	3.0 m	3.0 m
15.9	A setback from an <b>amenity area</b> shall be unencumbered except for a perpendicular <b>walkway</b> and <b>soft landscape</b> material	✓	✓	✓
<b>16.0</b>	<b>ACCESSORY BUILDINGS AND STRUCTURES</b>	✓ <sup>(9)</sup>	✓ <sup>(9)</sup>	✓ <sup>(9)</sup>

- NOTES:**
- (1) Measured to the highest ridge of a **sloped roof**.
  - (2) See also Subsections 4.1.7 and 4.1.8 of this By-law.
  - (3) Only applies to the RM7 zone if lands are used for a **Duplex** or **Triplex**.
  - (4) Where there are **buildings** with different heights on one lot, the average of the required setbacks shall be used.
  - (5) See also Subsection 4.1.12 of this By-law.
  - (6) See also Part 3 of this By-law.
  - (7) See also Subsection 4.1.9 of this By-law.
  - (8) Excludes private amenity space.
  - (9) See Subsection 4.1.2 of this By-law.
  - (10) The calculation of **height** shall be exclusive of structures for rooftop access, provided that the structure has a maximum **height** of 3.0 m; a maximum **floor area** of 20.0 m<sup>2</sup>; and it is set back a minimum of 3.0 m from the exterior edge of the building.

Proposed RM12 Zone Regulations, December 2017

Column	A	B
<b>Line</b> 1.0	<b>ZONES</b>	<b>RM12</b>
<b>PERMITTED USES</b>		
2.0	RESIDENTIAL	
2.1	Back to Back Townhouse on a Street	✓
<b>ZONE REGULATIONS</b>		
3.0	MINIMUM LOT FRONTAGE	
3.1	Interior Lot	6.0 m
3.3	Corner Lot	10.5 m
4.0	MAXIMUM NUMBER OF DWELLING UNITS IN A BACK TO BACK TOWNHOUSE BLOCK	12
5.0	MAXIMUM DWELLING HEIGHT	
5.1	Flat roof	11.0 m and 3 storeys
5.2	Sloped roof	15.0 m <sup>(1)</sup> and 3 storeys
6.0	MINIMUM FRONT YARD	4.5 m <sup>(2)(3)</sup>
6.1	Front garage face	6.0 m
7.0	MINIMUM EXTERIOR SIDE YARD	4.5 m <sup>(2)(3)</sup>
7.1	Front garage face	6.0 m
8.0	MINIMUM INTERIOR SIDE YARD	
8.1	Attached Side	0.0 m
8.2	Unattached Side	1.5 m <sup>(2)</sup>
8.3	Where any portion of the interior lot line abuts a zone permitting detached and/or semi-detached dwellings	7.5 m <sup>(2)</sup>
9.0	MINIMUM REAR YARD	0.0 m
10.0	MAXIMUM ENCROACHMENT OF A BALCONY ATTACHED TO A FRONT WALL	2.5 m
11.0	MINIMUM SETBACK FROM A PORCH, EXCLUSIVE OF STAIRS	2.0 m
12.0	ATTACHED GARAGE, PARKING AND DRIVEWAY	
12.1	Attached garage	Permitted <sup>(4)</sup>
12.2	Minimum parking spaces	✓ <sup>(5) (6)</sup>
12.3	Maximum driveway width	2.6 m <sup>(6)</sup>

<b>13.0</b>	<b>MINIMUM LANDSCAPED AREA</b>	
13.1	Minimum landscaped area	6.5 m <sup>2</sup>
13.2	Minimum percentage of required front yard landscaped area to be landscaped soft area	75%
<b>14.0</b>	<b>MINIMUM CONTIGUOUS PRIVATE OUTDOOR AMENITY SPACE</b>	6.0 m <sup>2</sup>

**NOTES:**

- (1) Measured to the highest ridge of a **sloped roof**.
- (2) See also Subsections 4.1.7 and 4.1.8 of this By-law.
- (3) Air conditioning equipment is permitted in the required **front yard**, provided it is located on a **balcony**.
- (4) See also Subsection 4.1.12 of this By-law.
- (5) See also Part 3 of this By-law.
- (6) See also Subsection 4.1.9 of this By-law.
- (7) Exclusive of **landscaped area** at-grade.

**Proposed New and Amended Definitions, December 2017**

**Amenity Area**

means an indoor space or outdoor recreational area provided for the communal use of the residents of a development.

**CEC - Road**

means a private right-of-way for vehicular travel over common elements that are maintained by a common element condominium corporation.

**CEC - Parcel of Tied Land**

means an area of land associated with a common element condominium.

**Context Grade**

Means, with reference to a townhouse, back to back townhouse or stacked townhouse, the average of 12 grade points, eight of which are taken around the perimeter of the site and four of which are based on the location of the proposed building(s):

- 2 points at the centreline of the street extending from the side property lines

- 2 points located 10 cm outside the subject site from where the side property lines meet the front property line

- 2 points located 10 cm outside the subject site at the midpoint of the side property lines

- 2 points located 10 cm outside the subject site, measured out from the side property lines, from where the side and rear property lines meet

- 4 points located at the corners of the proposed building, taken a 4.5 m distance from the proposed location of the exterior walls.

**Driveway**

means an internal roadway that is not a street, private road, CEC - road, condominium road or lane, which provides vehicular access from a street, private road, CEC - road, condominium road or lane to parking or loading spaces.

**Back to Back Townhouse**

means a building that has four or more dwelling units divided vertically, including a common rear wall, each with an independent entrance and has a yard abutting at least one exterior wall of each dwelling unit.

**Stacked Townhouse**

means a building that has four or more dwelling units divided horizontally and/or vertically, each with an entrance that is

independent or through a shared landing and/or external stairwell. Units may also be divided vertically by a common rear wall.

**Townhouse**

means a building that has three or more attached dwelling units divided vertically above grade by a party wall at least 5.0 m in length and at least 2.0 m in height, and has a yard abutting at least two (2) exterior walls of each dwelling unit.

**Condominium Road**

means a private right-of-way over private property for vehicular travel which provides access to buildings and/or dwelling units on the same property, is not maintained by a public body, and includes CEC-Road.

**Sidewalk**

means an area for pedestrian travel that is abutting a street, condominium road or private road.

**Walkway**

means an area for pedestrian travel that provides access within or to a property that is not abutting a street, condominium road or private road.



# Urban Design Guidelines



December 2017

# Back to Back and Stacked Townhouses

# Table of Contents

<b>1</b>	<b>Introduction</b>	<b>1</b>
1.1	Purpose	1
1.2	Urban Design Objectives	1
1.3	Building Types	2
<b>2</b>	<b>Checklist of Principles</b>	<b>3</b>
2.1	Zoning By-law	3
2.2	Building Height	3
2.3	Building Setbacks	4
2.4	Separation Between Buildings	4
2.5	Block Length	4
2.6	Natural Features	4
2.7	Grading and Retaining Walls	5
2.8	Below Grade Units	6
2.9	Building Elevations	7
2.10	Exposed Parking Structures	8
2.11	Landscaped Soft Areas	8
2.12	Common Outdoor Amenity Area	9
2.13	Private Outdoor Space	9
2.14	Pedestrian Connectivity	10
2.15	Waste Collection and Storage	10
2.16	Surface Parking	11
2.17	Utilities and Services	11
2.18	Property Management and Maintenance	12
2.19	Other Considerations	12
<b>3</b>	<b>Design Standard Diagrams</b>	<b>13</b>
3.1	RM9 Stacked Townhouses Design Standards	13
3.2	RM10 Back to Back Townhouses on Condominium Road Design Standards	14
3.3	RM11 Back to Back Townhouses on CEC - Road Design Standards	15



# Introduction

The City of Mississauga is at the end of its greenfield development phase. New growth is being accommodated through infill and development on vacant and underutilized sites. Development patterns are becoming more compact, using land and resources more efficiently, while maximizing existing infrastructure and community facilities, and promoting alternative modes of transportation. Traditional forms of housing are becoming less common, as land values rise and market demands shift. Back to Back Townhouses (BBT) and Stacked Townhouses (ST) are becoming increasingly popular throughout the GTA for several reasons:

- achieve increased densities in a low-rise form of housing
- a sensitive way to transition between low-density and high-density built forms
- contribute to a diversity of housing choices to meet different needs and preferences
- less expensive construction methods and reduced maintenance fees allow for a more affordable form of housing
- viewed as being grade related, with a front door directly to the outside

## 1.1 Purpose

The purpose of these guidelines is to ensure new developments that include BBTs and STs are designed to be compatible with, and sensitive to, the established context, and to minimize impacts on adjacent properties. The guidelines are intended to establish a design expectation for landowners, the development industry and the public, to ensure high quality development that meet the City of Mississauga's minimum development standards. These guidelines shall be read in conjunction with: the Official Plan, Zoning By-law, and other City guidelines and standards.

## 1.2 Urban Design Objectives

The following objectives provide the framework for the design guidelines:

- ensure compatibility with the existing and planned context
- design to meet the needs of people of all ages, abilities and incomes
- balance functional design and aesthetics with long-term sustainability
- protect and enhance natural features
- connect streets and provide pedestrian linkages
- provide high quality private and common amenity areas

### 1.3 Building Types

BBTs and STs are typically:

- three to four storeys in height
- comprised of units that are stacked vertically and/or horizontally with access from grade
- front onto a public street, condominium road, pedestrian mews or open space
- include surface and/or underground parking

These are illustrated in Figure 1 and Figure 2

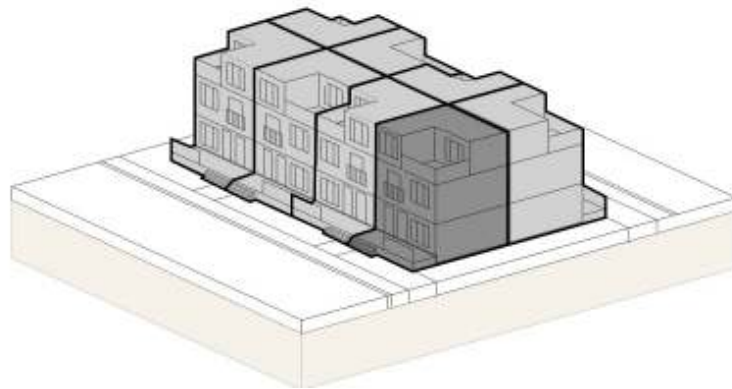


Figure 1: Example of a Back to Back Townhouse

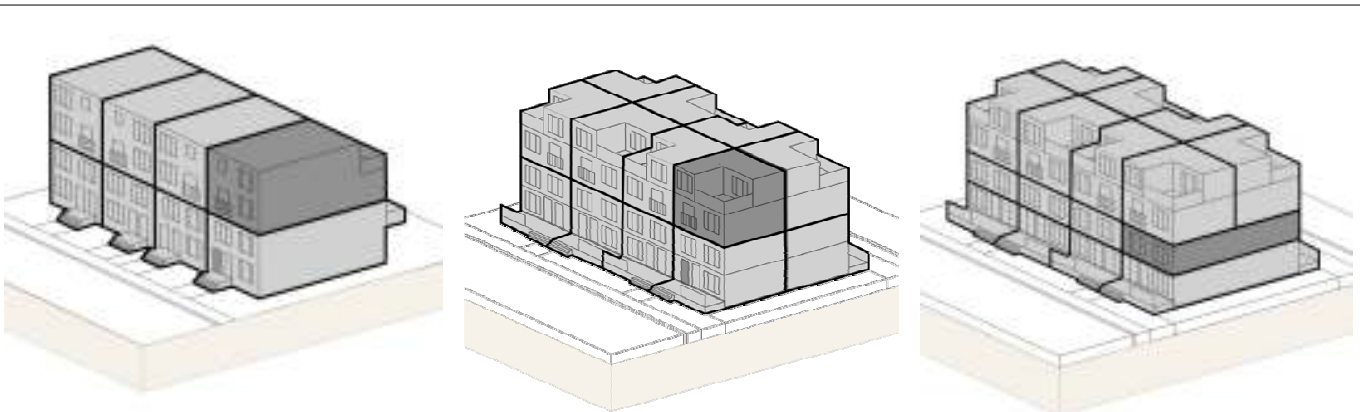


Figure 2: Examples of a Stacked Townhouse

# Checklist of Principles

The following principles are to be considered when designing a development that includes BBTs and/or STs. These principles are intended to ensure that new developments are compatible with and respect the existing and/or planned context through appropriate setbacks, tree preservation and landscaped buffers. Consideration shall be given to site design, building massing, orientation, height and grading relative to the street, to ensure new developments are compatible with, and sensitive to the surrounding context.

This checklist is to be used as a guide for developers, design professionals, property owners and the public to ensure they have considered key issues associated with this residential built form.

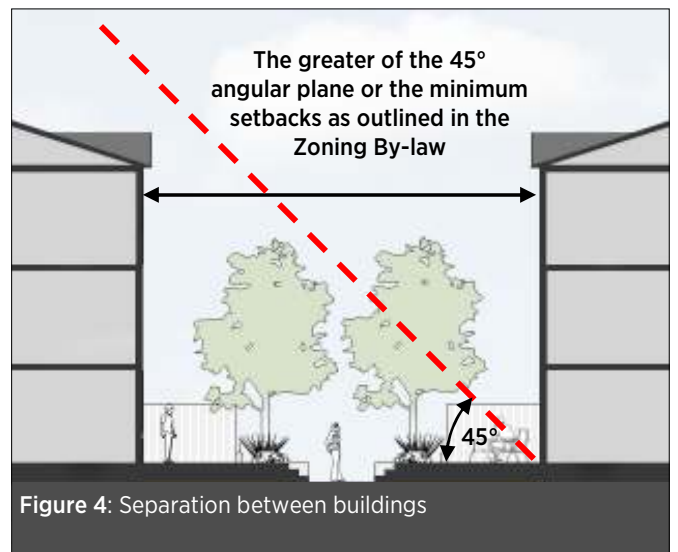
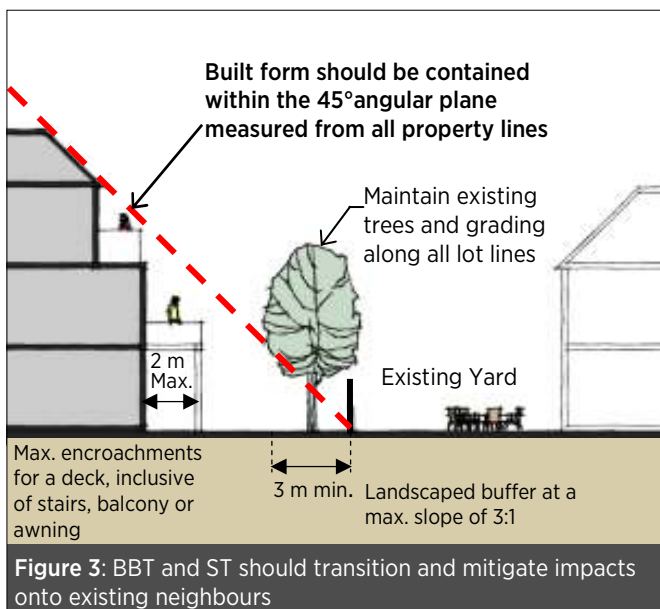
Review and check **each** principle when complete .....

## 2.1 Zoning By-law .....

- Refer to the Zoning By-law regulations that apply to the proposed built form. Generally BBTs and STs are zoned RM9, RM10, RM11 and RM12 or in combination with other zones

## 2.2 Building Height .....

- New developments will be required to demonstrate an appropriate transition in building heights
- Buildings heights shall be contained within a 45° angular plane, measured from all property lines (See Figure 3)
- Maximum building heights of three storeys for BBTs and four storeys for STs



**2.3 Building Setbacks** .....

- When existing adjacent front yard setbacks vary, new buildings should align with the average setback between the two adjacent properties or the minimum zoning requirement, whichever is greater
- Where applicable, the planned context should be considered in determining the front yard setback

**2.4 Separation between Buildings** .....

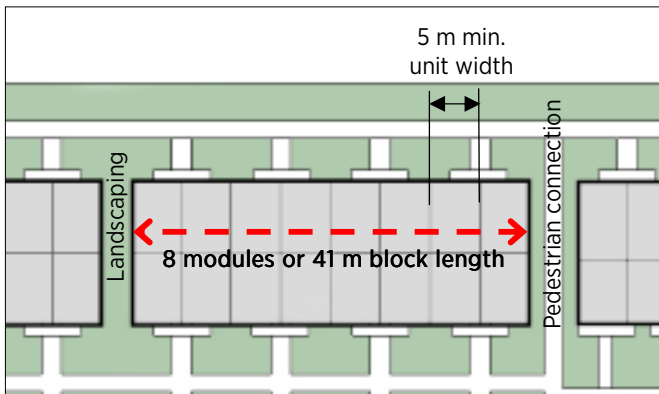
- Separation distance between buildings should be the minimum setbacks as outlined in the Zoning By-law
- In the case of a front wall to front wall condition, the separation distance should be the greater of the 45° angular plane or the minimum setbacks as outlined in the Zoning By-law (See Figure 4)
- Where a basement unit forms part of a three storey development the minimum separation distance will be 15 m

**2.5 Block Length** .....

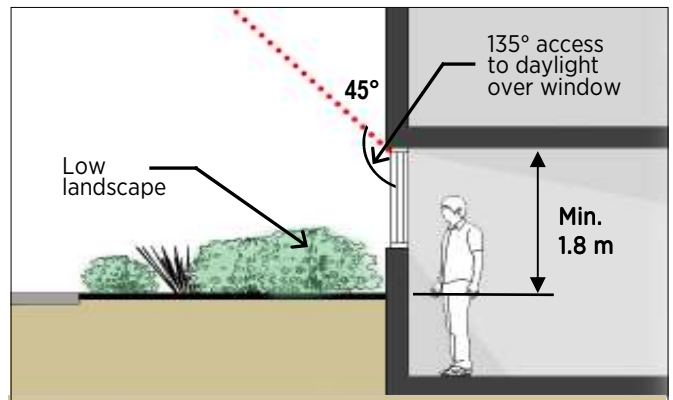
- Excessively long blocks should be avoided
- The maximum length of a block should generally not exceed the greater of 41 m or eight linear modules to promote pedestrian connections, allow for landscaping and provide a break in the massing (See Figure 5)

**2.6 Natural Features** .....

- New developments should preserve and enhance natural heritage features; including, trees, woodlands, valleys and wetlands
- Appropriate setbacks and buffers should be provided to existing and proposed natural features to ensure their health and continued growth



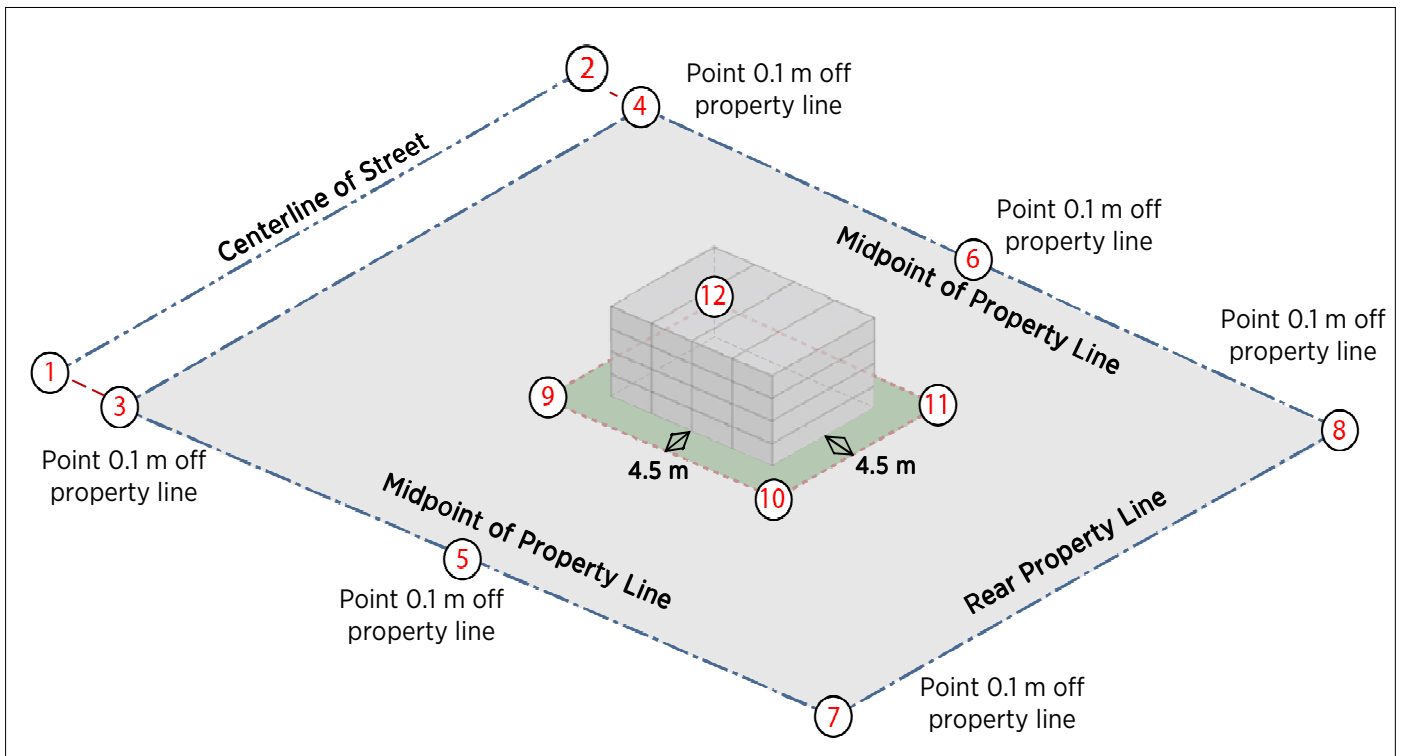
**Figure 5:** Blocks should be broken-up to allow green space and pedestrian connections



**Figure 6:** Definition of first storey

**2.7 Grading and Retaining Walls** ..... □

- Manipulation of site grades should be avoided
- Match existing grades and provide a minimum 3 m wide landscaped buffer around the property
- The landscaped buffer should be unencumbered by below grade parking structures, easements, retaining walls, utilities, severe grade changes and hard surface areas
- The first storey means a storey of a building that has its floor closest to the context grade and its ceiling more than 1.8 m above the context grade (See Figure 6)
- Each individual building will establish a grade elevation based on 'Context Grade'. Context Grade means the average of 12 points, eight of which are taken around the perimeter of the site and four of which are taken around each individual building (See Figure 7)



**Figure 7:** Context Grade: The average of 12 points, eight of which are around the perimeter of the site and four points located 4.5 m around each building

- The use of retaining walls should be avoided. Where retaining walls are required, their height should be limited to a maximum of 0.6 m to eliminate the need for railings and to reduce long-term maintenance costs (See Figure 8)

**2.8 Below Grade Units** □

- Below grade units should be avoided
- Manipulation of site grades requiring retaining walls to accommodate below grade units is discouraged
- If a below grade unit is proposed, it must be a through-unit that has windows on both the front and rear of the building (See Figure 9), or be a double wide back to back unit (min. 10 m wide) (See Figure 10)

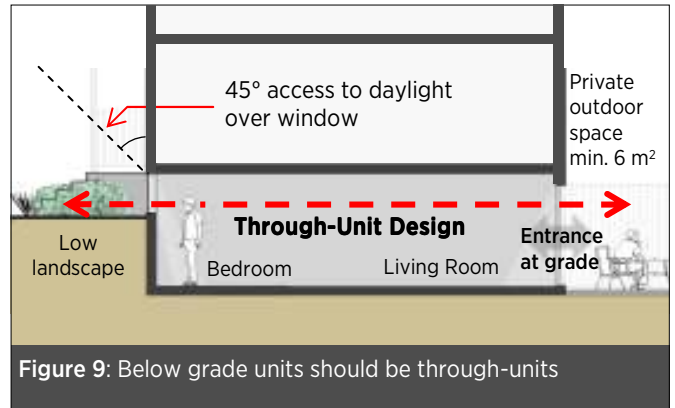


Figure 9: Below grade units should be through-units

- Below grade units require a minimum of 6 m<sup>2</sup> of private outdoor space located at the unit's floor level with unobstructed views and access to daylight (See Figure 2.7 and 2.9)
- All building projections including balconies and porches located over private outdoor spaces or windows of below grade units should not obstruct access to daylight. See the Zoning By-law for projection regulations (See Figure 9 and 11)

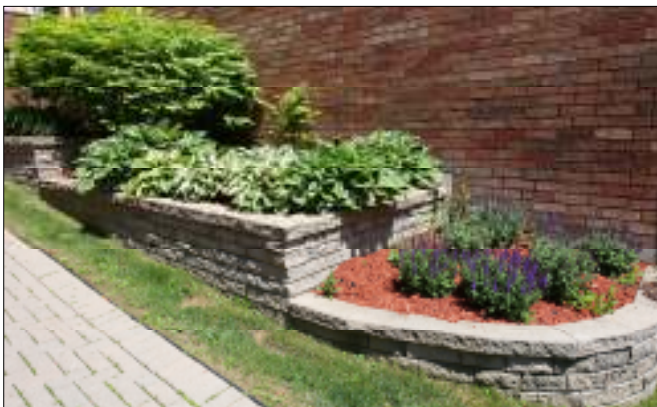


Figure 8: Landscape retaining walls should not be higher than 0.6 m



Figure 10: Below grade units should be double-wide back to back (min. 10 m wide) to allow light and air

**2.9 Building Elevations** ..... □

- New development should be compatible with the existing context in terms of height, scale, massing and materials
- Where appropriate, incorporate sloped roofs and half storeys with dormer windows on upper levels to reduce perceived height, scale and massing
- Ensure new developments have a variety of facade articulation, building materials and colours for visual interest
- Blank facades on the visible end unit elevation are unacceptable. End units that are visible should have entrances, windows and architectural interest to animate the elevation

- Buildings should be designed with high quality and durable materials to avoid long-term maintenance costs. Stone and brick is preferred. Stucco and wood are discouraged
- Stepback the structure for rooftop access (i.e. rooftop mechanical room) a minimum of 3 m from the exterior edges of the building to reduce visual impact (See Figure 12)
- The structure for rooftop access should not be greater than 20 m<sup>2</sup>, inclusive of stairs
- Rooftop outdoor amenity areas (common or private) should be setback a minimum of 1 m from the building's exterior edge to mitigate overlook concerns. This will not be required for internal units

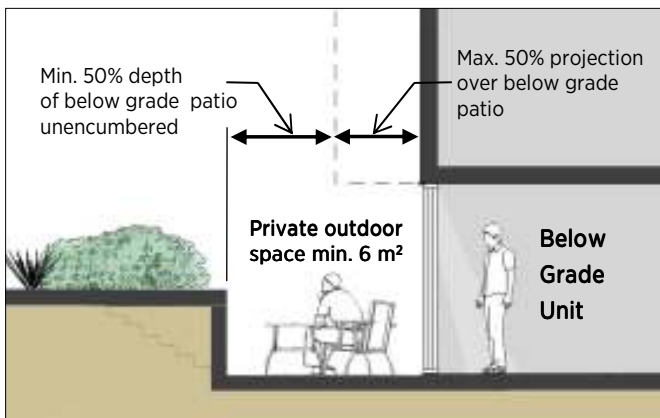


Figure 11: Permitted projections over below grade private outdoor patios

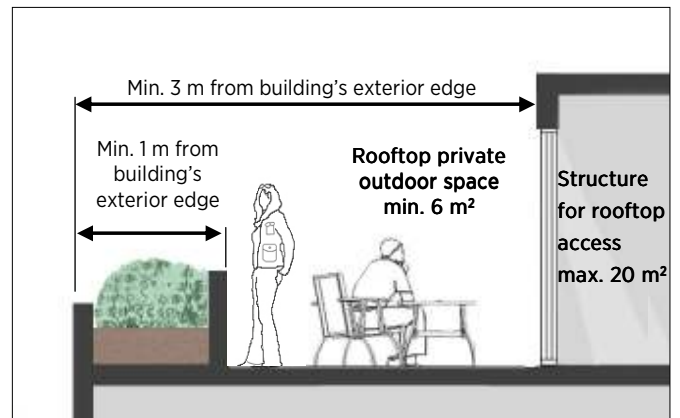


Figure 12: Setbacks from the building's exterior edge

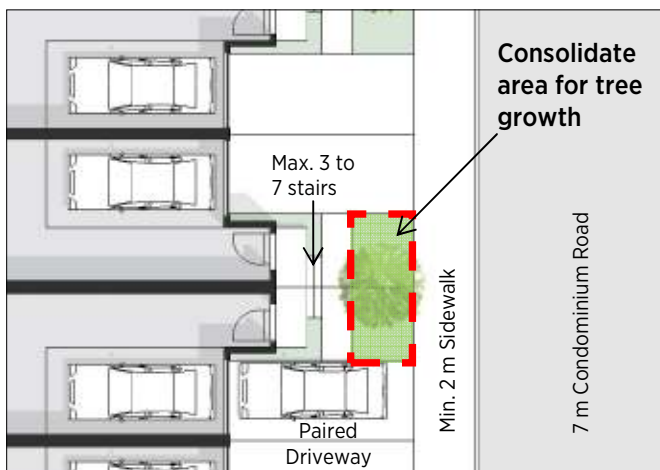


**2.10 Exposed Parking Structures** ..... □

- Exposed parking structures should be avoided. Where portions of the underground parking structure are exposed, they should match the building materials
- Consolidate the entrances to underground parking structures within the same development to minimize the number of overhead doors
- Maintain the minimum soil volume over the parking structure to support the growth of the vegetation. Minimum soil volume varies based on the type of vegetation
- Stairs exiting underground parking should be fully enclosed in glass to increase visibility and address issues of safety, security and weather protection

**2.11 Landscaped Soft Areas** ..... □

- Landscaped soft areas are required adjacent to paved areas and around the perimeter of the site. To provide relief between buildings, landscaped soft areas should be distributed throughout the development
- Landscaped soft areas should be provided between entrances to individual units and sidewalks, walkways, public streets and condominium roads
- Pair individual landscaped soft areas to increase soil volume for tree growth particularly where there is a driveway (See Figure 13)
- Limit the number of stairs to a unit entrance from three to seven risers to maximize landscaped soft area, mitigate safety issues in the winter and reduce maintenance costs
- All stairs should be poured in place concrete. Precast stairs are not permitted



**Figure 13:** Combine landscaped soft areas for tree growth



**Figure 14:** Common outdoor amenity areas should be centrally located, accessible and highly visible





**2.12 Common Outdoor Amenity Area** .....

- A common outdoor amenity area is required for all new multi-unit residential developments with more than 20 units.
- The total space required is the greater of 5.6 m<sup>2</sup> per dwelling unit or 10% of the site area
- Common outdoor amenity areas should be centrally located, highly visible and accessible by all residents (See Figure 14)
- A minimum of 50% of the required common outdoor amenity area shall be provided in one contiguous area
- A mews will not be considered a common outdoor amenity area

- Refer to the Outdoor Amenity Area Design Reference Note for additional details  
[http://www7.mississauga.ca/documents/pb/main/2015/Amenity\\_Space\\_Reference.pdf](http://www7.mississauga.ca/documents/pb/main/2015/Amenity_Space_Reference.pdf)

**2.13 Private Outdoor Space** .....

- Each unit requires a private outdoor space with a minimum contiguous area of 6 m<sup>2</sup>
- The private outdoor space may be located at-grade, on a balcony, deck, porch or on a rooftop
- Recessed or partially recessed balconies are preferred. Projecting balconies shall be avoided (See Figure 15). If a projecting balcony is proposed, it may project a maximum of 2 m beyond any building façade and should be designed with solid or opaque materials or tinted glass

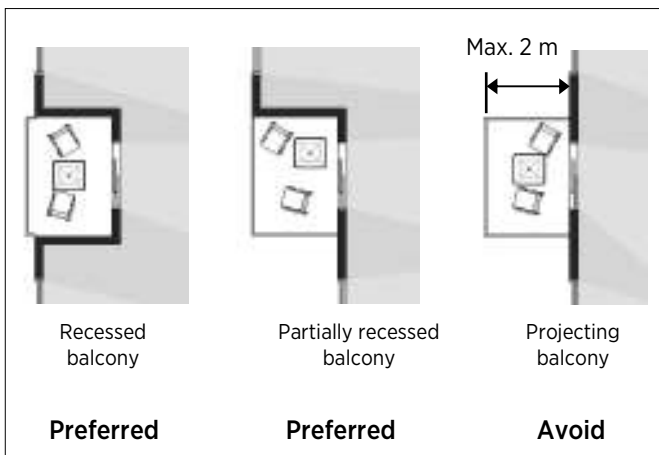


Figure 15: Balconies as private outdoor space

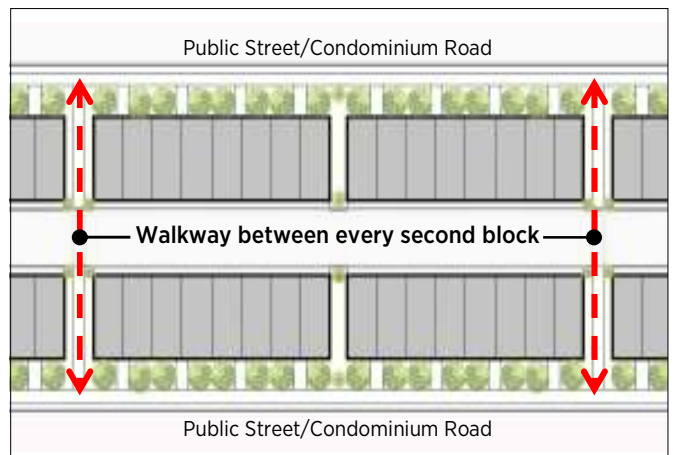


Figure 16: Pedestrian connections should be located between every second block

- Mechanical equipment, including air conditioning units and the storage of personal items are discouraged in private outdoor spaces

**2.14 Pedestrian Connectivity** .....

- Provide a walkway between every second block to allow connectivity (See Figure 16)
- Sidewalks will be located on one side of a condominium road. Sidewalks on both sides of the condominium road maybe required for large developments
- The following sidewalk widths will be required:
  - sidewalks abutting a road, minimum 1.8 m
  - sidewalks abutting a road, where traversed by a driveway, minimum 2 m
  - walkways in all other areas, minimum 1.5 m

- There should be at least one barrier-free path of travel that meets AODA (*Accessibility for Ontarians with Disabilities Act*) standards throughout the site
- Where accessible parking is located below grade (i.e. underground parking) it should be accessed via an elevator and forms part of a barrier-free path of travel

**2.15 Waste Collection and Storage** .....

- Waste storage rooms, drop-off locations (i.e. garbage chutes) and waste collection points (temporary pick-up areas) should be considered early in the site design stage to ensure appropriate placement and functionality
- The waste storage rooms and the waste collection points (pick-up areas) should be located internal to the site and should not be visible from a public street or impact residential units or adjacent properties (See Figure 17)
- Above grade waste storage rooms/enclosures should be well screened and appropriately setback from existing uses and proposed dwelling units to minimize undesirable noise, odour and visual impacts
- The waste collection facility should consider the space requirements for waste, recycling and green bins, along with bulky items (min. 10m<sup>2</sup>)



**Figure 17:** Waste storage room and waste collection areas should be constructed of durable materials



- Waste drop-off areas should not be greater than 100 m from a dwelling unit and be easily accessible via a sidewalk or walkway
- Waste collection points (pick-up areas) should not encumber parking stalls or access to other elements of the development (i.e. fire route, entry to the underground parking garage, mailboxes, etc.)
- Waste collection points should be made of durable concrete and be at the same level as the road
- Refer to the Region of Peel’s Waste Collection Design Standards Manual for more information <https://www.peelregion.ca/pw/standards/design/waste-collection-design-manual-2016.pdf>

**2.16 Surface Parking** .....

- Surface parking should be centrally located within the site and accessed by a sidewalk or walkway
- Parking lots should be setback a minimum of 3 m from a lot line and not located between the front face of a building and the street
- A minimum 3 m setback should be provided between the side wall of a building and a surface parking space

**2.17 Utilities and Services** .....

- The location of above and below grade utilities and services should be considered early in the site design stage to ensure they meet utility requirements (i.e. ease of maintenance, access) and ensure any visual impacts from the public street are mitigated
- Through the development process, provide the locations of above and below grade utilities, easements, etc., to ensure sufficient unencumbered space is provided for public and private trees, and landscaped soft areas
- Transformer vaults are typically located on a streetline and generally on a serviceable pad (i.e. minimum 3 m x 3 m pad for smaller developments). Contact Alectra Utilities for further requirements



**Figure 18:** Community mailboxes covered and in a central location

- Community mailboxes should be centrally located and accessed by a sidewalk or walkway (See Figure 18)
- Conceal or recess hydro and gas meters into the building's exterior walls or in a less visible location (See Figure 19)

**2.18 Property Management and Maintenance** .....

- Long-term maintenance and property management should be considered early in the development process to avoid costly maintenance issues
- Use durable and high quality building and site materials. Stucco is discouraged on the first two storeys of a building

**2.19 Other Considerations** .....

- Review Mississauga's Fire Route By-law 1036-81 early in the site design stage for the fire route design, building access requirements, etc.
- Review the Ontario Building Code to ensure that site and building designs comply with the relevant requirements
- Review the Bell Urban Design Manual for utility standard requirements



**Figure 19:** Place hydro and gas meters and other utilities in concealed or recessed locations



3.1 RM9 Stacked Townhouses Design Standards

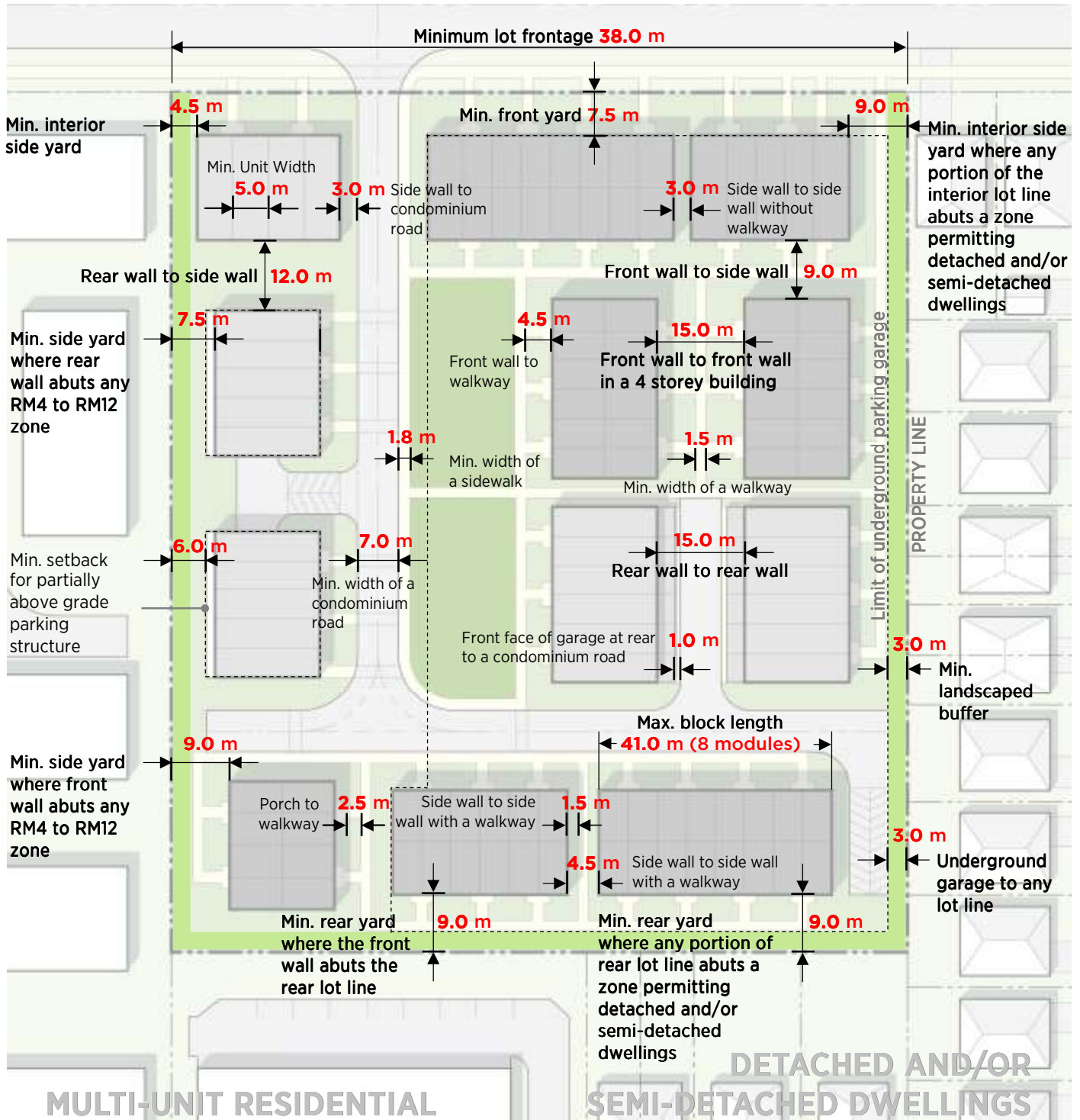


Figure 20: Standard dimensions for Stacked Townhouses (RM9). For additional standards refer to the Zoning Bylaw. The above drawing is for illustration purposes only and is not to scale.



3.2 RM10 Back to Back Townhouses on Condominium Road Design Standards

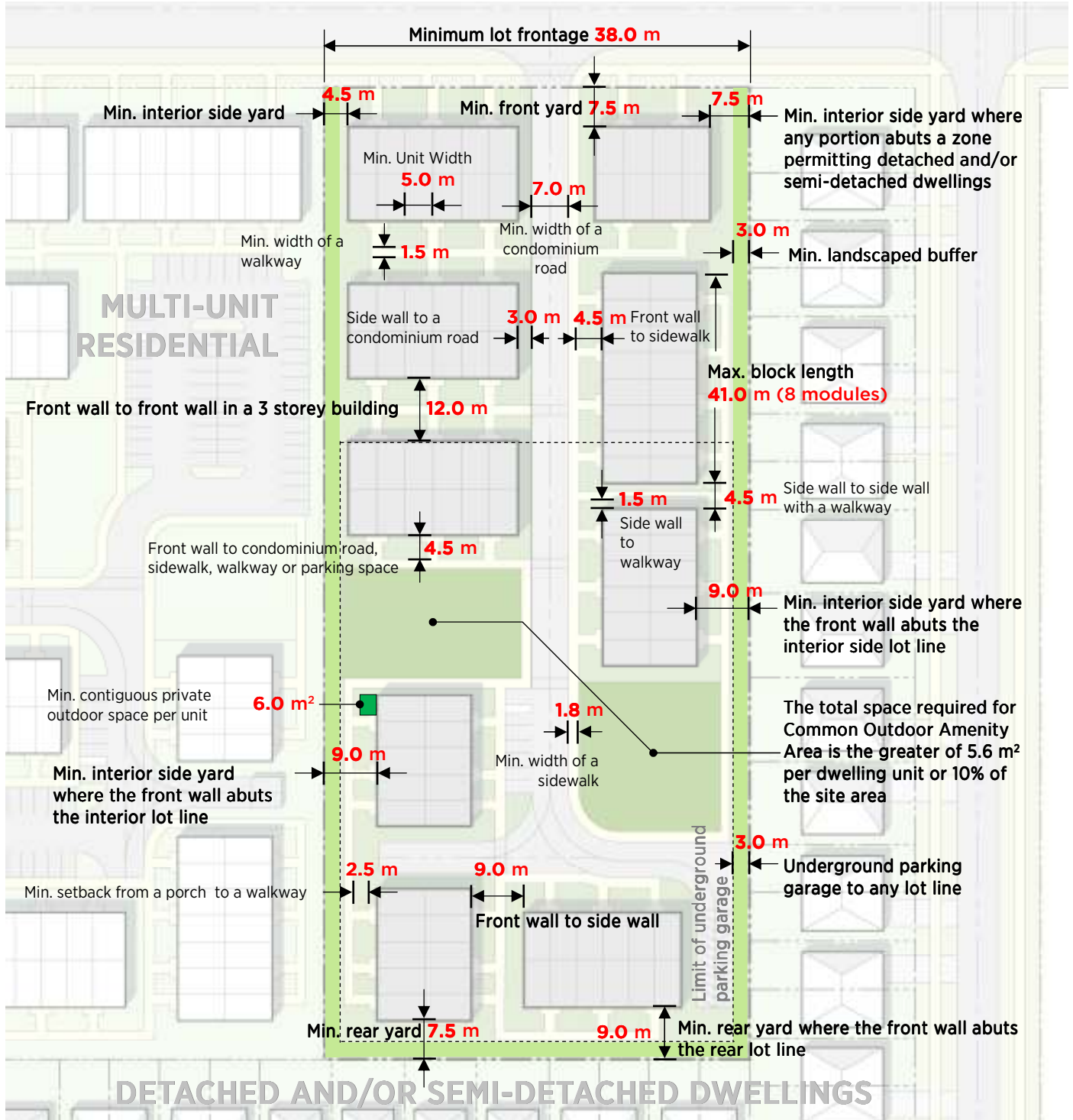


Figure 21: Standard dimensions for Back to Back Townhouses (RM10). For additional standards refer to the Zoning By-law. The above drawing is for illustration purposes only and is not to scale.



Design Standard Diagrams

3.3 RM11 Back to Back Townhouses on a CEC - Road Design Standards

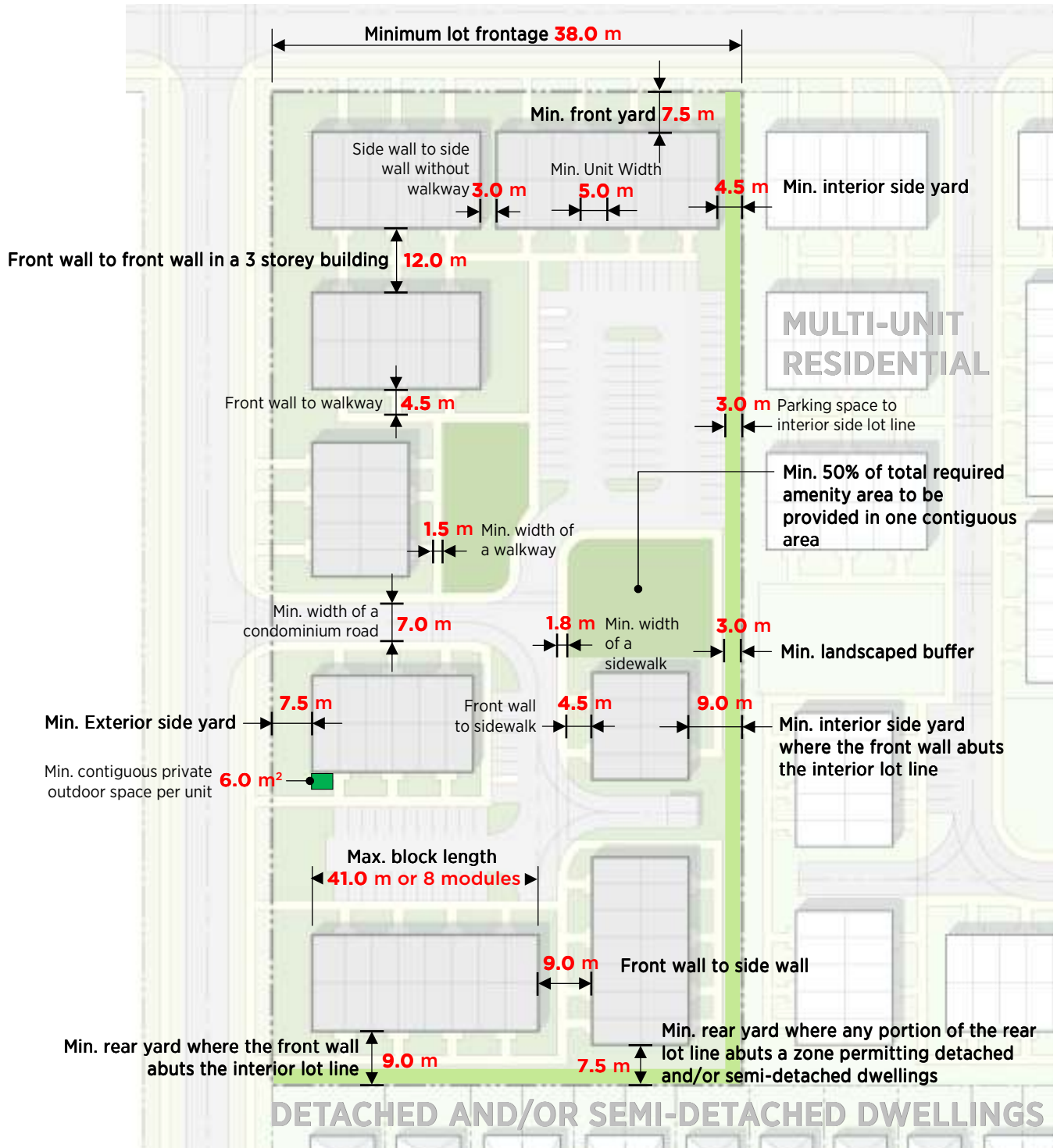


Figure 22: Standard dimensions for Back to Back Townhouses (RM11). For additional standards refer to the Zoning By-law. The above drawing is for illustration purposes only and is not to scale.

