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DATE: October 12, 2012

TO: Chair and Members of Planning and Development Committee
Meeting Date: November 5, 2012

FROM: Edward R. Sajecki
Commissioner of Planning and Building

SUBJECT: **Interim Control By-law for the Downtown Core
Implementing Documents
Draft Official Plan Amendment, Zoning Framework and Design
Standards**

PUBLIC MEETING **WARDS 4 and 7**

RECOMMENDATION: That the report titled "*Interim Control By-law for the Downtown Core, Implementing Documents*", dated October 12, 2012 from the Commissioner of Planning and Building, be received for information.

**REPORT
HIGHLIGHTS:**

- A planning study was undertaken to develop official plan policies, zoning framework and urban design standards. An overview of these documents is provided.
- The draft implementing documents are based on the vision set out in the City's Strategic Plan and the Downtown21 Master Plan to create a vibrant downtown, with a mixture of land uses, supported by multi-modal transportation choices and development that incorporates high quality built form.

- A statutory public meeting is a requirement under the *Planning Act* and represents the next step in the process of implementing the planning framework required to address the concerns related to the passing of the Interim Control By-law for the Downtown Core.

BACKGROUND:

On March 9, 2011, City Council passed an Interim Control By-law (0046-2012), because the existing planning regime could have resulted in development that would be detrimental to the long term viability of the vision for the Downtown Core. A one year extension (By-law 0036-2012) was passed on March 7, 2012. The Interim Control By-law was initially appealed by a number of land owners, however, all but one appeal has been settled by means of minor amendments to the by-law allowing minor alterations to existing buildings.

On June 26, 2012, a public open house was held to discuss and release the draft implementing documents. The open house was held in conjunction with the public information centre for the Hurontario-Main Street Light Rail Transit Project. Comments received regarding the proposed policies, regulations and standards will be addressed in a subsequent report on comments.

COMMENTS:

City Council initiated the ICB because the existing Downtown Core Local Area Plan and Zoning By-law permit certain land uses that are incompatible with the City's vision for the downtown. The vision for the downtown was developed through the Downtown 21 Master Plan which describes an intense, mixed use, transit oriented and pedestrian friendly downtown. The lack of alignment between current policies and zoning and the Downtown21 Master plan was a concern that Council determined required a planning study to come up with recommendations to ensure that the guiding principles of the Master Plan would be protected through appropriate planning tools. As a result, a draft Local Area Plan for the Downtown Core has been developed. The draft Plan provides a more extensive local planning framework for the downtown, and contains policies to address unique circumstances of the area. It should be noted that the draft Plan must be read in conjunction with the Mississauga Official Plan (2011) and

follows the same general organization in order to reinforce the importance of interpreting the two documents in conjunction with each other. Given the imminent approval of Mississauga Official Plan (2011), the final Local Area Plan is expected to be reformatted from its current format to ensure alignment with the new base document.

The draft Plan (see Appendix 1) incorporates and builds upon the policies in the existing Local Area Plan included as part of Mississauga Official Plan (2011). Many existing policies and land use designations have been carried forward, however, a number of modifications have been made to align with the vision set out in the Downtown21 Master Plan. The proposed policies of the draft Plan are intended to ensure that:

- Strategic land uses are preserved;
- The road network is updated;
- “A” and “B” street frontage classifications are introduced and implemented;
- New districts and transit station character areas are added;
- The urban design policies are updated and expanded to reflect a pedestrian oriented downtown;
- Retail activation is implemented.

The zoning framework document has been updated since the initial release of the documents to provide more detail and a description for the changes proposed to the Zoning By-law. The proposed changes to the Zoning By-law are intended to implement the draft Plan. In addition to revising existing zoning provisions, a new Downtown Core Office zone is being proposed to implement the new Office Designation.

New Built Form Standards for the downtown are also being introduced and provide detailed urban design direction and guidance for developments that are going through the planning development application process. The standards will be used to assess, promote and fulfill the intent of the local area plan, the zoning by-law and the Downtown21 Master plan.

The public meeting is the statutory public meeting to fulfill the

requirements of the *Planning Act* which will provide the Planning and Building Department the opportunity to obtain comments from the public on the proposed changes to the land use policies, associated zoning regulations and built form standards, all of which are intended to implement the vision set out in the Downtown21 Master Plan.

STRATEGIC PLAN: The draft Downtown Core Local Area Plan, the zoning framework and Built Form Standards support the City's Strategic Plan and its Strategic Pillar for Change to "create a vibrant downtown" that functions as a strong economic centre, while acting as the civic heart and soul of the City.

FINANCIAL IMPACT: Not Applicable

CONCLUSION: Following the statutory public meeting, a report on comments will be prepared for consideration by Planning and Development Committee, which will address comments received from the public and from the circulation of the Draft Plan.

ATTACHMENTS: APPENDIX 1: Downtown Core Local Area Plan
APPENDIX 2: Proposed Amendments to Zoning By-law 0225-2007
for the Downtown Core Local Area Plan
APPENDIX 3 : Built Form Standards for the Downtown Core

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DRAFT

October 12, 2012

Downtown Core

1.0 How to Read the Downtown Core Local Area Plan

Mississauga Official Plan is composed of a principal document and a series of local area plans, provided under separate cover. This is the Downtown Core Local Area Plan and provides policies for lands located in central Mississauga, as shown on Figure 1. It includes lands identified in the City Structure as the Downtown Core.

There are some instances where the policies and schedules of the principal document do not address all circumstances particular to the Downtown Core. In these cases, the Downtown Core Local Area Plan elaborates on, or provides exceptions to the policies or schedules of the principal document.

The Downtown Core Local Area Plan must be read in conjunction with the Mississauga Official Plan principal document. Parts 1 to 4, the schedules and the appendices of the principal document, are applicable to the Downtown Core area, unless modified by this local area plan. Thus, for example, the policies of this local area plan must be read in conjunction with the environmental, multi-modal, urban form and land use policies of Parts 2 and 3 of the principal document. In the event of a conflict, the policies of this local area plan take precedence.

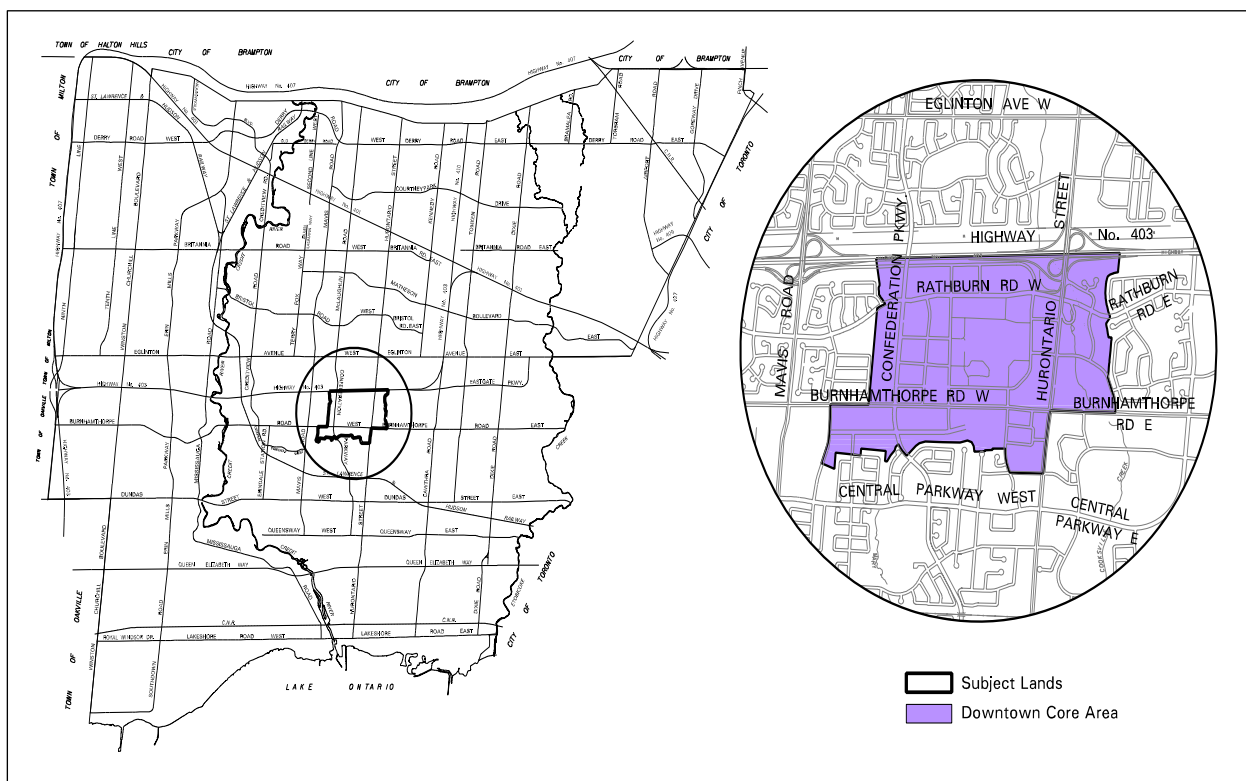


Figure 1: The Downtown Core Local Area is located in central Mississauga and is identified in the City Structure as Downtown Core.

1.1 Context

Downtown Mississauga is evolving into an exciting urban place. The Downtown Core consists of high density residential developments, office buildings, mixed-use developments, parks, cultural facilities, civic uses and recreational and entertainment uses developed around the periphery of the Square One Shopping Centre.

Over the last decade, flexible zoning provisions combined with favourable market conditions have resulted in extensive private investment in high density residential and mixed-use condominium development such as the internationally acclaimed Marilyn buildings among others. During this time, there has been some expansion of the Square One Shopping Centre but limited new office development.

Recent public investments, including the new Sheridan College campus, regional transit infrastructure (Bus Rapid Transit, Hurontario Light Rail Transit (LRT)) and significant city-wide public realm investments and amenities such as Celebration Square continue to enhance the Downtown's role as a regional destination in the Greater Toronto Area. The Downtown Core is located within a major designated growth area and identified as an Anchor Hub in the Regional Transportation Plan. The Downtown Core continues to be a significant focus for investment in the Greater Toronto Area.

1.2 Vision

A key goal of the City's Strategic Plan is to Create a Vibrant Downtown that will be the civic and cultural soul of the city as well as a strong economic centre.

The basis for the Downtown Core Local Area Plan is the Downtown21 Master Plan, which was received by Council in April 2010. The Downtown21 Master Plan sets the direction for transforming the Downtown Core from a suburban model to an urban mixed use centre.

The goal is to create a high quality, walkable, human scaled environment that continues to attract lasting public and private investment in the Downtown and supports existing and planned infrastructure, particularly higher-order transit.

Additional investment is being made to transit in the Downtown, including the Light Rapid Transit system along Hurontario Street and within the road network of the Core. The Bus Rapid Transit system will also serve the Downtown Core connecting it with areas to both the east and west. The current focus for local and inter-regional public transit is in the area of Rathbun Road West and Station Gate Road consisting of the City Centre Transit Terminal and GO platforms. The future BRT station will also be constructed in this area. With the on going commitment to public transit infrastructure combined with transit supportive developments this area will develop as a key mobility hub within the Greater Toronto Area.

The existing road pattern is made up of super blocks. An expanded road network is proposed that will create an urban pattern of development blocks that are walkable in scale and well connected. The new road network will result in urban blocks, providing routing options for vehicular, servicing and goods movement, pedestrian and cycling movement within the Core. The scale of the streets is to be narrower with special attention paid to the public realm and elements within the boulevard to ensure a comfortable, enjoyable and safe pedestrian environment.

Downtown Mississauga has already begun its transformation from a suburban auto-oriented centre, to one that is truly urban, rich in character, memorable and geared to the pedestrian experience.

The Downtown which forms a part of the Urban Growth Centre is expected to be the focus for population and employment growth in the City to 2031. The Core is expected to contain the highest densities, a diverse range of building heights and the greatest mix of uses.

This Local Area Plan aims to address the following key challenges for the Downtown Core:

- a. Strengthen the Core's role as the primary location for major office, highest concentration of retail commercial, mixed use, civic uses and cultural development;
- b. Accommodate forecasted growth without impacting the natural environment and quality of life;
- c. Attract new office development to balance population and employment;
- d. Create a finer-grained, well-connected road network that supports active transportation modes: walking, cycling, transit, servicing and goods movement, and the car;
- e. Develop an integrated urban place that achieves design excellence in buildings, the public realm and distinctive character in mixed use districts; and
- f. Provide a new development framework and policy regime founded on greater predictability and certainty of outcomes, to better direct growth and support existing and planned services.

The vision for the Downtown Core is to be a mixed use urban centre, with a balance of residential and employment uses, complemented by retail, civic and cultural uses. Additional office development is critical to achieve a strong employment base and move the Downtown in a forward direction.

This Plan will better direct development, achieve a stronger economic base, strengthen the transportation systems, create linkages and access, improve urban design and enhance the pedestrian experience resulting in a more vibrant and active mixed use Downtown Core.

2.0 Introduction

The focus of these policies is to reinforce and enhance the image of the Downtown Core as a destination and an urban mixed use centre. These policies are also intended to promote a high quality of urban design by integrating the public realm with private development. Its position as an employment, retail commercial and civic centre will be balanced by residential development, education, entertainment, and facilities for business visitors and tourists. These policies will serve to facilitate the dynamic growth and change of the Downtown Core as it matures.

2.1 Development Objectives

2.1.1 A high quality of urban design in the built form that is unique, mixed use, pedestrian friendly and transit supportive, and which contributes to the unique identity and prominence of the Downtown Core.

2.1.2 The location of streets, the mix of uses, a high standard of urban and *streetscape* design, and the development and enhancement of a comprehensible public/private system of pathways and open space, including public art will:

- a. integrate retail commercial, office, civic and other facilities;
- b. facilitate pleasant and safe pedestrian and cyclist movement throughout the Downtown Core;
- c. provide opportunities for passive outdoor recreation within the Downtown Core;
- d. provide access to and from the Cooksville Creek greenbelt;
- e. reinforce and expand the role of the Downtown Core as the focus of city wide cultural, community and civic activities.

2.1.3 Street related development will pay particular attention to pedestrian access and amenities.

2.1.4 Development will result in a unique Downtown Core character, enhanced through the creation of view corridors and gateway features, and visibility of the Civic Centre and public gathering places.

2.1.5 The Downtown Core road system will be completed to achieve an active transportation system.

2.1.6 The placement and design of parking facilities will achieve the highest standard of urban design and architecture in support the pedestrian environment and public realm.

2.1.7 A mix of uses will be found within buildings and along individual street frontages.

2.1.8 A balance of residential and employment uses will be developed.

2.1.9 Foster the continued development of a variety of entertainment activities that will contribute to activity both day and night.

2.1.10 Development and activities will enhance the Downtown as a destination.

2.1.11 Development and activities that will expand tourism for the region.

2.1.12 The prominence of the Downtown Core as the City's primary mixed use centre will be maintained.

2.1.13 Vehicular and transit access to and from the Downtown to the Toronto Lester B. Pearson International Airport and interregional transit facilities, such as the Cooksville GO Transit station will be developed.

3.0 Urban Design

3.1 General

The intent of the urban design policies is to define principles for the management of the physical form and character of the Downtown Core. In this respect, urban design policy is intended to:

- achieve a distinctive image for the Downtown Core which is urban in character;

- achieve the highest standard of urban design in both the public and private realm;
- provide a sense of entry into the Downtown Core area by encouraging gateway treatments such as, prominent buildings located close to the street, distinctive landscape and streetscape treatment, entry signage, distinctive precincts and skyline elements;
- achieve built form that has a high level of physical continuity, cohesion and linkage between buildings, from block to block, and from street to street;
- reinforce and extend the public realm - streets, public open spaces, and boulevards - as a significant public resource of the highest design standard, which encourages a sense of place, civic identity and physical continuity in the built environment;
- achieve urban scale land blocks, streets and built form appropriate to a transit supportive and pedestrian oriented core;
- achieve elements of linkage to the abutting communities while ensuring compatible integration and context sensitivity;
- achieve a balance of vehicular traffic and transit and active transportation needs to achieve pedestrian comfort, convenience, safety and accessibility;
- prohibit the construction of new surface parking lots in the Downtown Core;
- prohibit blank building walls abutting the street; and
- recognize utility corridors required for hard service components.

3.1.1 Urban form in the Downtown Core will develop incrementally. The submission of a concept plan will be required for all phased or multi-building development applications and must demonstrate how the urban design policies will be implemented through the development review process.

3.1.2 The urban design policies and Downtown Core Built Form Standards included in Appendix 1 of this Plan will provide interpretation and direction on the urban design policies, which will also be used to evaluate development proposals.

3.2 Urban Design Components

The urban form of the Downtown Core is created by the public realm, and the private realm. The seamless integration of these components is critical to the character and development of the Downtown Core based on the following:

3.2.1 Design of the Public Realm

The public realm includes streets, boulevards, and open space that is accessible to the public. These elements can include, but are not limited to, pedestrian ways, bikeways, bridges, plazas nodes, squares, transportation hubs, gateways, parks, natural features, view corridors, landmarks and the interface with buildings.

Streets

Streets are the connections between spaces and places, as well as being spaces themselves. They are defined by their physical dimension and character as well as the size, scale, and character of the buildings that line them. Streets will be defined by their form and function. A hierarchy of streets will be established to distinguish their purpose and ensure they contribute to places within the urban structure of the Downtown Core.

3.2.1.1 Development will provide physical definition of the streets and ensure a seamless relationship between the public and private realm. Existing and new roads in the Downtown Core will be identified for the purposes of:

- a. establishing categories of street frontages;
- b. identifying urban design policies that articulate the intent of the street frontage categories; and
- c. specifying Built Form Standards that provide direction and detailed development guidance in relation to the urban design frontage

categories, included in the Downtown Core Local Area Plan as Appendix 1.

3.2.1.2 Mississauga will ensure a fine grain pattern of urban streets in the Downtown Core to:

- a. create high quality street designs that support a comfortable, pedestrian scale, cycling and transit supportive environment.

3.2.1.3 In addition to providing routes for multi-modal travel, streets are a significant public resource within the Downtown Core. The design of streetscape should:

- a. ensure a high level of urban design in all components;
- b. enclose and contain the streets with well designed buildings and groups of buildings to create a predictable pattern of development, and define the public realm and open space system;
- c. ensure streets achieve a sense of enclosure by minimizing their visual width and scale with the use of streetwalls and boulevard treatments;
- d. ensure pedestrian and cycling amenity, comfort, convenience and safety;
- e. ensure design and functional elements are incorporated which reduce traffic speed;
- f. accommodate on-street parking in public rights-of-way, where feasible;
- g. incorporate coordinated street furniture and lighting systems; and
- h. provide coordinated traffic signage, which is designed in scale and character with the Downtown Core *streetscape* elements.

Boulevards

3.2.1.4 In addition to providing corridors for utilities, boulevards provide the framework for public amenities on the Downtown Core streets and integrate diverse areas of character. The design of boulevards should:

- a. incorporate coordinated design themes, materials and landscaping throughout the Downtown Core;
- b. be designed as active public spaces integrating and linking development;
- c. should facilitate pedestrian and cycling linkage between the curb and abutting private lands;
- d. incorporate enclosure and definition of the street space;
- e. maximize the opportunity to incorporate places to meet, sit and socialize; and
- f. achieve pedestrian comfort, weather protection and safety, particularly at transit stops.

Open Spaces

3.2.1.5 Parks and other open spaces provide urban amenities for the Downtown Core. These features should:

- a. encourage a variety of open spaces, squares, and central green areas;
- b. promote a sense of identity and place;
- c. provide pedestrian connections between the public sidewalk, significant buildings and features;
- d. frame a view, a significant building or terminate a vista, where appropriate; and
- e. be flanked by public roads to ensure they are not considered private spaces.

Transit

3.2.1.6 The design of built form and landscape elements will:

- a. integrate transit stations and bus stops with development;
- b. incorporate weather protection in waiting areas;
- c. provide walkways from transit stops to principal building entrance(s); and
- d. ensure visibility of transit stops from the public street and abutting development.

3.2.2 Design of the Private Realm and Frontage Requirements

Buildings are the most pronounced element of the urban fabric and create the sense of place. Buildings shape and articulate streets and open space by forming edges and streetwalls to establish definition and enclosure. Collectively they create the pedestrian environment and frame the public realm.

'A' & 'B' STREETS

3.2.2.1 All streets are designated 'A' and/or 'B' streets, which differentiate the various streets by their function, character, built form treatments and design.

- a. 'A' Streets have a critical role and function in the Downtown Core. They are intended to be the most important for securing animation and character, a comfortable pedestrian environment, street activity and vibrancy.
- b. 'B' streets allow for site servicing and access and are also intended to support street activity, achieve character and a pedestrian friendly environment.
- c. New development will follow specified 'A' and/or 'B' street standards for each street in the Downtown Core. The character of built-form varies according to the 'A' or 'B' street designation. This framework is shown in the A & B Street Schedule of this Plan.

Provisions for Development on 'A' & 'B' Streets

3.2.2.2 Where streets have been identified as having an 'A' or 'B' street frontage, development will address the following provisions:

- a. locate build-to line requirements on development blocks;
- b. coordinate build-to lines with adjacent properties in order to create consistent edges and street walls along frontages;
- c. locate buildings parallel to the street at the build-to line to contain the street and provide enclosure;
- d. ensure a variation in set backs along the building frontages to articulate façade emphasis at the build-to line to allow for visual interest, outdoor patios, recessed entries and landscaped areas;
- e. buildings should incorporate active ground floor uses to animate the public realm and will be designed as an extension of the pedestrian environment;
- f. ground floor uses will incorporate transparent windows;
- g. development will locate functioning principal building entrances so that they are clearly identifiable and prominent with direct access to the public sidewalk, pedestrian connections and transit facilities; and
- h. retail and commercial entrances, and lobby entrances serving buildings will be flush with the sidewalk elevation.

'A' Streets

3.2.2.3 'A' streets require the greatest attention to urban design and will have a cohesive built form and streetscape character to support the public realm.

3.2.2.4 'A' streets will have continuous building frontages along the length of development blocks to provide a continuity of built form from one property to the next.

3.2.2.5 Functioning principal entrances to buildings will be provided on 'A' streets.

3.2.2.6 Curb cuts, driveways and laneways are prohibited on 'A' streets, except where a site or block does not have access from a 'B' street, or if there is a requirement for emergency vehicle access.

'B' Streets

3.2.2.7 'B' streets provide development blocks with vehicular access to off street parking, deliveries, garbage pick-up, service and loading. 'B' streets will also have an important pedestrian character designed to a high standard of built form and streetscape to support the public realm.

3.2.2.8 'B' streets will have continuous building frontages along the length of development blocks to provide a continuity of built form from one property to the next with minimal gaps between buildings and provision for loading and servicing facilities.

3.2.2.9 Functioning principal building entrances will be located on 'B' streets, except where a development block may have 'A' and 'B' streets, the most prominent building entrances will be located on 'A' streets.

Parking, Servicing, and Loading Design in the Streetscape

3.2.2.10 The design and location of parking facilities will have:

- a. below grade or above grade structured parking;
- b. walkways, traffic islands and pedestrian refuges as integral components of parking facilities;
- c. identified access to parking facilities from the public street;
- d. integration of parking on abutting sites; and
- e. safe vehicular circulation and orientation within the parking areas.

3.2.2.11 Above grade structured parking will be designed so that:

- a. structured parking and vehicular access will be located to minimize impacts on the property and on surrounding properties and will improve the safety and attractiveness of adjacent streets, parks and open spaces;
- b. parking structures will not directly front onto 'A' streets, but will be screened by liner buildings incorporating a mix of uses between the parking structure and street space;
- c. on the ground floor, parking structures will have active uses such as commercial or retail with an appropriate scale and architectural expression to support activity on the streets, parks and/or open spaces;
- d. for a given development block, parking structures will only directly front onto one 'B' street, where more than one 'B' street frontage exists;
- e. all parking structures and their facades above the ground floor will be designed to the highest level of architectural treatment and façade animation to mask the parking and screen views of the interior;
- f. entrances, lobbies and passageways that provide a convenient means of access to parking facilities from the street will:
 - be located on streets where the least amount of retail is required, or where no retail frontage requirement is specified in accordance with the Retail Activation Schedule included in this Plan; and
 - be fully enclosed, appropriately signed and integrated into the façade design without appearing as dominant elements on the street frontages.
- g. at grade exhaust vents serving structured parking facilities will not be permitted to front onto 'A' streets, but may be located on 'B' streets provided that they are architecturally

treated and vertically integrated into a building's exterior wall and cladding system.

3.2.2.12 Service, loading and garbage storage facilities will be located internally within buildings.

Vehicular Access and Facilities in the Streetscape

3.2.2.13 The design of facilities for vehicular access to properties will:

- a. provide clarity, safety and internal overlook opportunities to properties;
- b. have limited driveway access points to the public street and crossings of the sidewalk; and
- c. have recognition of the needs of pedestrians and cyclists.

Retail and Commercial Frontage Design

3.2.2.14 Development incorporating at grade retail and commercial uses at the frontages, will be designed so that:

- a. lobbies and principal entrances located at grade serving residential uses above the ground storey shall be limited in size in order to allow for maximum activation of the street;
- b. where areas of retail focus have been identified in accordance with the Retail Activation Schedule of this Plan, lobbies and principal entrances serving residential uses above the ground storey will be located on streets where the least amount of retail is required, or where no retail frontage requirement is specified;
- c. storefronts are expressed with a narrow width and frequent rhythm that supports the pedestrian experience at the street level;
- d. uses will be directly related to public sidewalk and street level, and will incorporate operating entrances, doorways and storefront windows oriented to the street, along with signage and lighting to provide animation, interest and variety in the streetscape;
- e. such uses incorporate architectural cantilevers, fixed canopies, awnings or similar features in

order to achieve pedestrian scale, comfort and weather sheltered pedestrian routes. These should complement the architectural character of the building and be made of highly durable materials; and

- f. where appropriate, patios and outdoor amenity space should be provided adjacent to retail and commercial entrances to promote activation and street vibrancy.

3.2.2.15 Development incorporating at grade retail and commercial uses will also incorporate the highest standard of storefront design consisting of:

- a. high quality materials;
- b. clear transparent windows and doors that allow for views into and out of storefronts; dark tinted, reflective or opaque glazing or 'lifestyle' panels placed on storefront windows are not permitted;
- c. elements such as mullions, glazing bars and transoms to help frame, divide and define storefront window sections and apertures where appropriate; and
- d. coordinated signage and lighting that integrates with the storefront design, and which complements or acts as an extension of the buildings architectural character.

Residential Use Frontages

3.2.2.16 Development incorporating residential uses will be designed to:

- a. provide functioning, individual entrances to residential units and to animate the frontage with windows on the ground floor that look out onto the street;
- b. create a high quality interface and transition between the public sidewalk and front door to ensure adequate separation, definition and privacy;
- c. provide high quality landscape architecture treatments within frontages, transition zones and setbacks; and

- d. consider designs that allow for the potential conversion of ground floor residential uses to commercial and retail uses over time.

Special Frontage Provisions

3.2.2.17 Buildings that surround, have proximity or have street frontage facing parks or open space will be designed according to 'A' street requirements and have the highest level of architectural expression, articulation and use of materials.

3.2.2.18 Proposed development will ensure that important view along Burnamthorpe Road and Hurontario Street, and views towards the Civic Centre precinct and the Living Arts Centre are considered.

a. Pedestrians in the Streetscape

3.2.2.19 The design of built form and landscape elements will:

- a. integrate spaces between the building wall and the streetline with the public boulevard for design continuity;
- b. identify internal building spaces and exterior courtyards accessible to the public from the public street; and
- c. incorporate ease of pedestrian movement and minimal grade changes through the site integrating with the public streetscape.

3.2.3 Built Form

3.2.3.1 To achieve a variety of character and diversity in built form, a wide range of building forms will be encouraged across the Downtown Core to include the following:

- a. Low-rise buildings (3 - 5 storeys)
- b. Mid-rise buildings (6 - 12 storeys)
- c. Tall buildings (above 12 storeys)

3.2.3.2 Low rise buildings will be designed to provide a minimum of 3 storeys both at the street frontage and across the entire building area.

3.2.3.3 Mid-rise buildings will be designed to:

- a. incorporate special massing, stepbacks, pedestrian perception lines, and articulation at the streetwall and upper storeys to contain the street or public space, and support a pedestrian scale; and
- b. mitigate the impacts of wind, shade and shadow and demonstrate how proposals adhere to the City of Mississauga Standards for Shadows Studies.

3.2.3.4 Tall buildings characterize a good part of the Downtown Core landscape and have a role in their contribution to the emerging downtown urban context.

3.2.3.5 Development of tall buildings will consider location, placement, relationship to the street, height ranges, built form qualities and architectural treatment in order to enhance the pedestrian environment, and minimize shadowing of the public realm. They have a role to play in terms of protecting important views, impacts on cultural resources, accommodating a range of uses and supporting transit. They must also ensure fundamental quality of life aspects such as access to natural light, sky views and privacy for those that live, work and visit the Downtown Core.

3.2.3.6 Tall buildings will be required to demonstrate how proposals adhere to the City of Mississauga Standards for Shadows Studies.

3.2.3.7 Tall buildings in the Downtown Core will be designed and massed in the form of a podium, tower and top.

3.2.3.8 Podiums will be located and massed to fit harmoniously into their existing and/or planned context so as to minimize shadowing on adjacent streets, parks, open spaces and adjacent properties. They are intended to support a pedestrian scale, contain the street and shape the public realm. Podiums will integrate with adjacent buildings and address the impacts of parking, loading and servicing.

- a. The height of the podium will generally be equal to the width of the right of way, but not less than 3 storeys in height;

- b. Podiums will incorporate a street wall placed at the build-to-line, having a minimum of 3 storeys;
- c. Podiums and their street walls will be articulated and massed at the upper storeys using step-backs to:
 - mitigate the perception of height and create a comfortable scale for pedestrians;
 - maximize sunlight on the public realm; and
 - mitigate the effects of wind on the pedestrian environment.

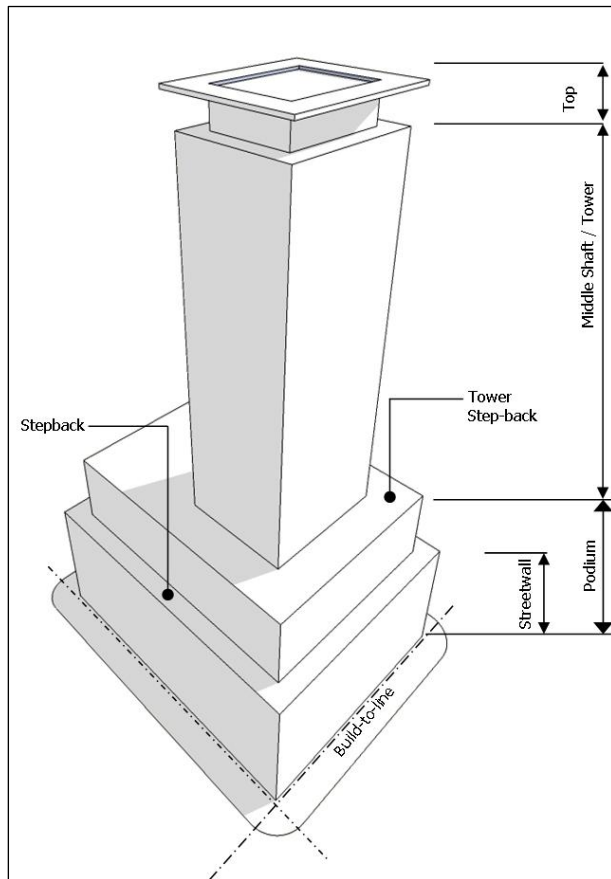


Figure 2: Massing of Tall Buildings

3.2.3.9 The middle shaft/tower will be located and massed to fit harmoniously into its existing and/or planned context so as to limit impacts on adjacent streets, parks, open spaces and adjacent properties by:

- a. maintaining generous spatial separation between towers to maximize access to sky views, natural daylighting, adequate privacy, minimize wind conditions and minimize shade on the streets, parks and open spaces.
- b. floor plate size and shape will be designed with appropriate dimensions for the site, locate and orient the tower shaft on the site and in relationship to the Podium and adjacent buildings.
- c. articulating the floor plates to break down the mass of the building and to create street interest and enhance skyline character.

3.2.3.10 Design the uppermost floors of the tops of tall buildings to achieve a distinctive skyline profile.

3.2.3.11 Design the tops of tall buildings to integrate the mechanical penthouse function into the complete building design.

3.2.3.12 Use materials, finishes and patterns that are consistent with the overall building design and architectural expression.

Built Form in the Streetscape

3.2.3.13 The design, location and character of Built Form will:

- a. have regard for scale and enclosure of the public street;
- b. mitigate the perceived mass of large buildings and long frontages by:
 - ensuring buildings and their streetwalls are well proportioned to address human scale;
 - incorporating variation in the overall design of the streetwall;
 - using special massing to articulate the built form;
 - providing a change in materials, textures, patterns, colours and details to create a sense of smaller scale buildings;
- c. ensure that streetwalls for mixed use buildings articulate a base, middle and top to define the ground floor and upper storeys;
- d. incorporate a frequent vertical rhythm and articulation across the streetwall in order to provide interest, a sense of human scale, orientation, comfort and accessibility;
- e. design corner lot buildings with special massing and architectural treatments on both streets to give prominence along the frontages and visually distinguish these sites;
- f. express principal building entrances with a height, proportion and level of articulation that is compatible and in scale with the ground

floor storey height, so that entrances are visible and serve as focal points for orientation and access from the street;

- g. have buildings that deploy high-quality, enduring and durable materials such as stone, clay brick, wood and glass; and
- h. ensure materials such as stucco, vinyl, embossed face brick, exterior insulation finish systems (EIFS), and architectural concrete block are not used in the Downtown Core.

Signage

3.2.3.14 Free-standing pylon and stand-alone signs are prohibited in the Downtown Core.

3.2.3.15 The design, location, size and character of building signage will:

- a. be visually integrated with development as an extension of the building's architectural expression;
- b. ensure compatibility of scale and character of the signage with the building and the context of the site; and
- c. provide clear identification of the site address.

3.2.4 Landscape Design and Public Art in the Streetscape

3.2.4.1 The design, location and character of landscape features will have:

- a. have connection and linkage with the public street;
- b. have high quality, safe and accessible amenity and open space on their sites;
- c. formal landscape elements respective of the urban setting;
- d. continuity with the design elements of the public boulevard;
- e. public art as an integral component of the street providing visual interest and a sense of place; and
- f. architectural and landscape features reinforcing common objectives.

3.3 Districts

The City will prepare detailed plans and policies for the various Districts identified on Schedule "C", each with its own identity, scale and character, through future amendments to this Plan. The following provides a general outline of the intended character for each District within the Downtown Core. The Districts are:

- Main Street District
- Civic District
- Confederation District
- Rathburn District
- Cleary Park District
- Hurontario District
- Square One District
- Sussex District

3.3.1 Main Street District

The Main Street District is envisioned as the heart of the Downtown Core, with a broad mix of active street level retail, restaurants, cafes, patios and entertainment facilities with a mix of uses above. The District will be a vibrant, walkable place that is active at all times of the day. The Main Street District is intended to bring together the commercial activity of the north, the Civic Centre district to the west, with the residential developments to the south. The District straddles Burnhamthorpe Road West, extending southward to Kariya Drive and northward to Square One Mall. The Civic District and Duke of York are the western boundary with Kariya Drive and the Office District being the eastern boundary.

The vision for this District is to create an active lively pedestrian-oriented urban place in the heart of the emerging “downtown” that would serve as a model, catalyst and attractor for ongoing investment in the larger area. Key objectives include the following:

- a. to create at its heart a “main street” that connects from Square One Mall across Burnhamthorpe Road West to the larger residential communities to the south of Burnhamthorpe Road West;
- b. to incorporate a broad mix of uses and users that would contribute to make this a vibrant part of the city fabric;
- c. to create a critical mass of at least 5,000 residents, 5,000 employees, with neighbourhood-oriented retail/restaurant uses; and
- d. to realize this vision in a relatively short time horizon.

The Main Street Transit Station Area

3.3.1.1. The Main Street Station serves as a destination node for the Main Street and is shown in the Long Term Transit Network Schedule on this Plan. This station area will be a vibrant, walkable, mixed use place.

3.3.2 Civic District

The Civic Centre District is a five block public campus and public open space that is home to the City’s premiere public institutions including the Civic Centre, Central Library, Living Arts Centre and Sheridan College Campus. This district is envisioned as a singular civic space comprised of smaller parks and plazas and institutional uses, linked together by the pedestrian street design of Duke of York Boulevard, Living Arts Drive and the flush street design of City Centre Drive, Princess Royal Drive, Prince of Wales Drive and Square One Drive.

Civic Centre Station Area

The Civic Centre Station Area serves as a destination node for the concentration of civic uses along Living Arts Drive including Celebration Square, Civic Centre, Central Library, YMCA, Living Arts Centre and Sheridan College. Shown in the Long Term Transit Network Schedule of this Plan, the station area will provide connection to the public open space network and serve a significant residential population in the Confederation Precinct. Placemaking and wayfinding objectives will guide the development in this station area.

3.3.3 Confederation Parkway District

The Confederation Parkway District serves as the Western gateway to the Downtown Core. This is a rapidly growing urban residential neighbourhood that will be home to over 20,000 people, centred along Confederation Parkway. The district will contain a series of new park spaces that will connect Zonta Meadows Park to the Civic District. The Confederation Parkway district will serve as the northwestern gateway into the Downtown Core and will include a mix of uses which encourage a sense of place and pedestrian movement.

3.3.4 Rathburn District

The Rathburn District is currently a pattern of suburban retail uses and surface parking lots. This district will intensify over the long-term with an emphasis on office and employment uses that can

take advantage of access to future higher order transit. Visibility and access to Highway 403 and transit make this a prestigious employment district.

Rathburn Transit Station Area

3.3.4.1 The Rathburn Station Area is served by the LRT and Bus Rapid Transit and, is adjacent to the City Centre Transit Terminal. It connects with the future development north of Square One Mall along Rathburn Road. The station area is characterized as an office district, with some entertainment uses.

3.3.5 Cleary Park District

This future neighbourhood extends west to Grand Park Drive and forms the south western gateway to the Downtown Core. The extension of John Cleary Park north to Burnhamthorpe Road will serve the future residential community and completes the Mary Fix Greenway connection, which will link this neighbourhood to the balance of the Downtown Core.

3.3.6 Hurontario District

The Hurontario District includes Downtown's highest concentration of existing office uses, providing the foundation for new office development in the short-term. The district has the potential to increase the amount of office space through new development and intensification of existing sites.

Robert Speck Station Area

3.3.6.1 This station serves the concentration of office at Robert Speck Parkway and Hurontario Street and future office development along City Centre Drive.

3.3.7 Square One District

The Square One Precinct is home to the Square One Mall, one of the largest and most successful shopping centres in Canada. It is expected that the shopping centre will continue to draw people from across the city and the larger region. It is also expected that it will evolve and adapt over time

and intensify lands currently utilized for surface parking. Office uses combined with ground floor retail and possibly residential on the upper floors are encouraged, particularly along City Centre Drive, Duke of York, Robert Speck Parkway and Square One Drive. In the process of intensifying, the connectivity of the mall to the surrounding area and the street network will be strengthened and a more urban built form will emerge.

Additions to Square One Mall will:

- be expressed in an outward fashion and designed to support a pedestrian friendly environment along the frontages;
- be designed in accordance with 'A' street frontage requirements when additions surround, have proximity or have frontage facing public or private open space;
- have the highest level of architectural expression, articulation and use of materials at the frontage;
- have active ground floor uses to animate the adjoining walkway area, street or public or private open space; and
- have ground floor uses that incorporate transparent windows and entrances that have direct access to the adjoining walkway, street, or public or private open space.

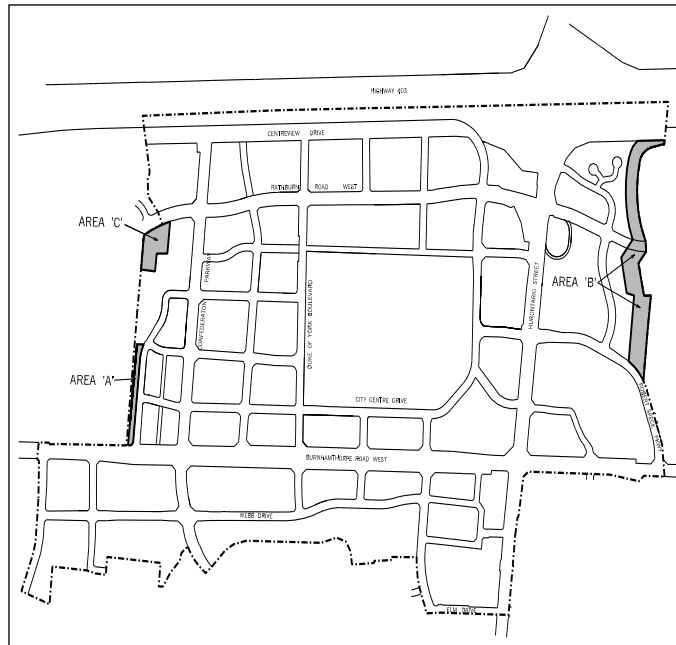
3.3.8 Sussex District

This is currently the densest, mixed use district in the Downtown Core and includes significant office and residential development. Some infill opportunities exist for additional office, ancillary uses, and residential development.

Matthews Gate Station Area

3.3.8.1 This station area at Hurontario Street and Matthews Gate serves the concentration of office and residential lands in the Sussex and Mississauga Valley Precincts. The station area will be designed to facilitate pedestrian connections among the Sussex Centre, Kariya Park, and the school board lands while also serving the needs of a dense residential area population.

3.3.9 Transition Areas



Map 1: Transition Areas

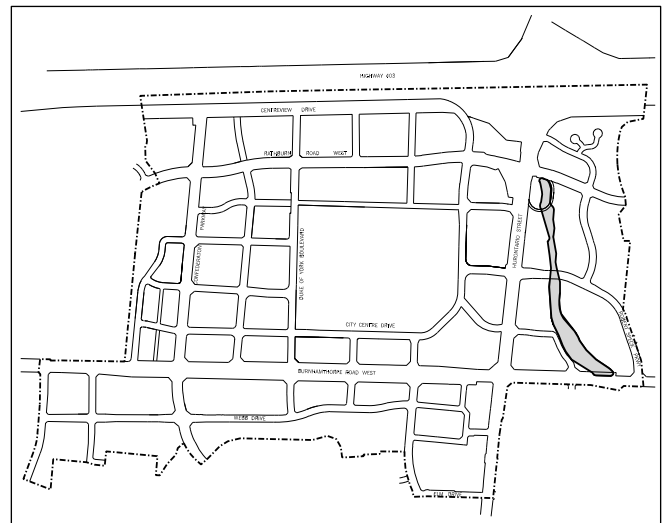
3.3.9.1 The Transition Areas will promote positive integration of building scale and intensity of development between the Downtown Core and abutting communities.

3.3.9.2 The design of the built environment should encourage:

- mid-rise built form adjacent to the Downtown Core boundaries and north of Burnhamthorpe Road West, with height transitions to a maximum of five storeys, on lands shown as Area 'A' on Map 1;
- low-rise built form adjacent to Downtown Core boundaries, with height transitions from a maximum of two storeys at the Downtown Core boundaries to a maximum of eight storeys at the inner edge of the Transition Areas for lands abutting the easterly limit of the Downtown Core, shown as Area 'B' on Map 1 and lands immediately south of Rathburn Road West, shown as Area 'C' on Map 1;
- orientation of buildings enabling a sensitive integration with adjacent low density development;

- internalized service areas and parking to minimize impact on adjacent residential;
- pedestrian and bicycle links to adjacent communities; and
- appropriate setbacks and landscaping adjacent to the existing walkways.

3.3.10 Cooksville Creek Corridor



Map 2: Cooksville Creek Corridor

3.3.10.1 The Cooksville Creek Corridor is recognized as a public amenity and resource. To protect, enhance and restore this natural feature, the design of the built environment will ensure:

- appropriate setback of buildings from the corridor (landscaping only);
- improved visibility, access and linkage to the corridor from development parcels and public streets;
- views/informal surveillance to the corridor;
- integration of public/private open space and landscape areas;
- compatible positive relationship between the built form and the corridor;
- parking structures are designed and located to complement the corridor.

4.0 Land Use

4.1 General Land Use

4.1.1 Uses Permitted in all Designations

4.1.1.1 Notwithstanding the Uses Permitted in all Designation, the following additional uses will also be permitted:

- a. *cogeneration* facility will be permitted as an accessory use.
- b. commercial parking facility;

4.1.2 Open Space

4.1.2.1 Public open space within the Downtown Core should be flanked by public roads to ensure they are open to all and not considered private spaces.

4.1.2.2 Public open spaces will be connected and continuous to form an 'Emerald Necklace' throughout the Downtown Core and connect the downtown to existing adjacent parkland such as Zonta Meadows Park, John Cleary Park, Civic View Walk and Kariya Park, and as greenbelt lands such as Bishopstoke Walk along Cooksville Creek. The size and configuration of all proposed parks will be subject to further review through a Local Area Plan update or through the review of individual development applications.

4.1.2.3 Stormwater retention and stormwater quality ponds are generally not appropriate uses for public parkland, however, in some instances overland flow pond stormwater facilities may be accommodated in public parkland.

4.1.2.4 Private Open Space

Private open space may be incorporated into new developments along 'A' or 'B' Street frontages in part or whole, provided that:

- the design of private open space will integrate seamlessly with the adjoining street-network,

pedestrian environment and overall open space system;

- the design of the private open space contributes positively to the identity of the Downtown Core;
- the private open space interfaces with existing and/or proposed development in a legible and cohesive manner;
- the design of the private open space is executed to the highest urban design and landscape architecture standards;
- private open space is accessible, safe and is being provided for the use and enjoyment of the general public on a year-round, seasonal and 24 hour basis.

4.1.3 Residential

4.1.3.1 Residential consists of the following designation:

- a. Residential High Density.

4.1.3.2 Notwithstanding the Residential policies of this Plan Designations, the Residential High Density designation will also permit the following uses:

- a. horizontal multiple dwellings accessory to an apartment dwelling;
- b. long-term care dwelling; and
- c. retirement dwelling.

4.1.3.3 Special needs housing will be in a built form consistent with the dwelling forms permitted by the residential designation.

4.1.3.4 A horizontal multiple dwelling will be permitted with an apartment dwelling.

4.1.3.5 In addition to the uses permitted in the High Density Residential designation, a commercial facility will be permitted provided that:

- a. it forms an integral part of the ground floor of the building; and
- b. is oriented to pedestrian use.

4.1.4 Mixed Use

4.1.4.1 Notwithstanding the Mixed Use policies of this Plan, the following additional uses will also be permitted:

- a. hospital;
- b. major office;

The following uses are not permitted:

- a. convenience restaurant with drive-thru facility.

4.1.5 Retail Activation

4.1.5.1 Existing and new streets in the Downtown Core have been identified for the purpose of:

- a. establishing retail objectives within mixed use buildings;
- b. identifying districts and areas of focus for which retail has been determined to be a strategic priority in the Downtown Core;
- c. setting out requirements for the amount of at-grade retail to be achieved on street frontages in accordance with the Retail Activation Schedule included in this Plan; and
- d. identifying permitted retail uses as outlined below.

4.1.5.2 Notwithstanding the Mixed Use designation, only the following uses will be permitted on the ground floor:

- a. financial institution;
- b. personal service establishment;
- c. restaurant; and
- d. retail store.

4.1.6 Office

4.1.6.1 Notwithstanding the Office policies of this Plan, the following additional uses will also be permitted:

- a. hospital;
- b. post-secondary educational facility;
- c. residential dwellings associated with an institutional use.

4.1.6.2 Notwithstanding the Office policies of this Plan, the total Gross Floor Area of accessory uses will not be limited.

5.0 Transportation

5.1 Road System

5.1.1 All roads shown on the Downtown Core Land Use Map and the Road Network Schedule will be public. The design, access requirements and public/private responsibilities will be determined through the development application process or through a Local Area Plan update.

5.1.2 Daylight triangles will be required. Daylight roundings may be considered where appropriate.

5.1.3 Minor adjustments to the basic rights-of-way for minor collector roads and local roads may be made without an amendment to this Plan subject to the City being satisfied that the role and function of such roads are maintained.

5.1.4 Additional right-of-way will be required for the proposed roundabouts identified on Schedule - Downtown Core Road Network and Classification.

5.1.5 As traffic volumes increase, dedicated transit lanes may be necessary.

5.1.6 The proposed Highway 403 North Collector (Northern Distribution Road), located north of the Downtown Core, is intended to provide access to and from the Downtown Core road network.

5.1.7 Centre View Drive is intended to provide access to eastbound Highway 403 via a link east of City Centre Drive.

5.1.8 A future functional feasibility study will be undertaken to review the north collector, Centre View Drive link to Highway 403 and other road or ramp proposals contained in the Downtown21 Master Plan which may impact Highway 403.

5.1.9 Cycling facilities will be incorporated per the Cycling Master Plan. The City may in the future identify secondary cycling routes to be integrated with the design of the public realm.

5.2 Transit

5.2.1 The Downtown Core will be served by a *higher order transit* system that provides service within the Downtown with connections to other parts of the City, neighbouring municipalities and inter-regionally.

5.2.2 An *Anchor Hub* is designated at the interchange of the Bus Rapid Transit Corridor and the Hurontario Light Rail Corridor. The exact location will be determined through a future study.

5.2.3 As part of the proposed Mississauga Bus Rapid Transit (BRT) facility, the existing transit terminal may be modified to serve future development growth and to connect with the BRT facility. In the long term, an additional BRT station is also proposed near the intersection of Rathburn Road West and City Centre Drive.

5.2.4 Transit facilities will be designed to serve the City-wide transit network; internal transit services; the BRT facility, inter-regional transit systems; and the pedestrian system linking developments, Square One and cultural facilities.

5.2.5 *Higher order transit* stations will be situated within the Downtown Core:

5.2.6 The precise location of *higher-order transit* stations within the areas illustrated above will be determined through the Hurontario-Main LRT Project.

5.2.7 Within transit station areas, the City will require that development applications address Transit Oriented Development (TOD) objectives.

These include moderate to high density development, comprised of a mix of uses and designed with an emphasis on the pedestrian experience and opportunities to maximize transit ridership.

5.2.8 Development applications will be supported by studies that include a review of the land use and urban design components of the station area as a whole, including surrounding privately held lands, the configuration and design of intersections and other pedestrian design elements within the public realm.

5.2.9 Ground level retail development will be required within Transit Station Areas to activate the area and street.

5.2.10 Parking will be managed carefully within Transit Station Areas. The City will consider reducing parking requirements within Transit Station Areas.

5.2.11 A distinct character for each Transit Station Area will develop, guided by the Precinct character descriptions of the Local Area Plan. The City may also develop Transit Station Area Plans as a basis for guiding and assessing development applications in Transit Station Areas.

6.0 Stormwater Management

6.1 Any development within the Cooksville Creek watershed will require the implementation of on site stormwater management techniques to control the 100 year post-development stormwater discharge to 2 year pre-development levels

6.2 Development applications will follow the Green Development Standards as they relate to storm water management measures. All practices will be designed and implemented to the satisfaction of the City and CVC Authority.

7.0 Implementation

7.1 Mississauga may apply a holding provision to lands within Intensification Areas to ensure that the policies of this Plan are implemented. The removal of the holding provision will be conditional on the applicant satisfying the requirements of the policies of this Plan.

7.2 The policies of this Plan should not be construed to require the gratuitous dedication of land for new public roads, including realignments of roads or impose an obligation upon a landowner to construct or pay for the construction of new roads, where not otherwise permitted by the *Planning Act*.

8.0 Special Site Policies

There are sites within the Downtown Core that merit special attention and are subject to the following policies:

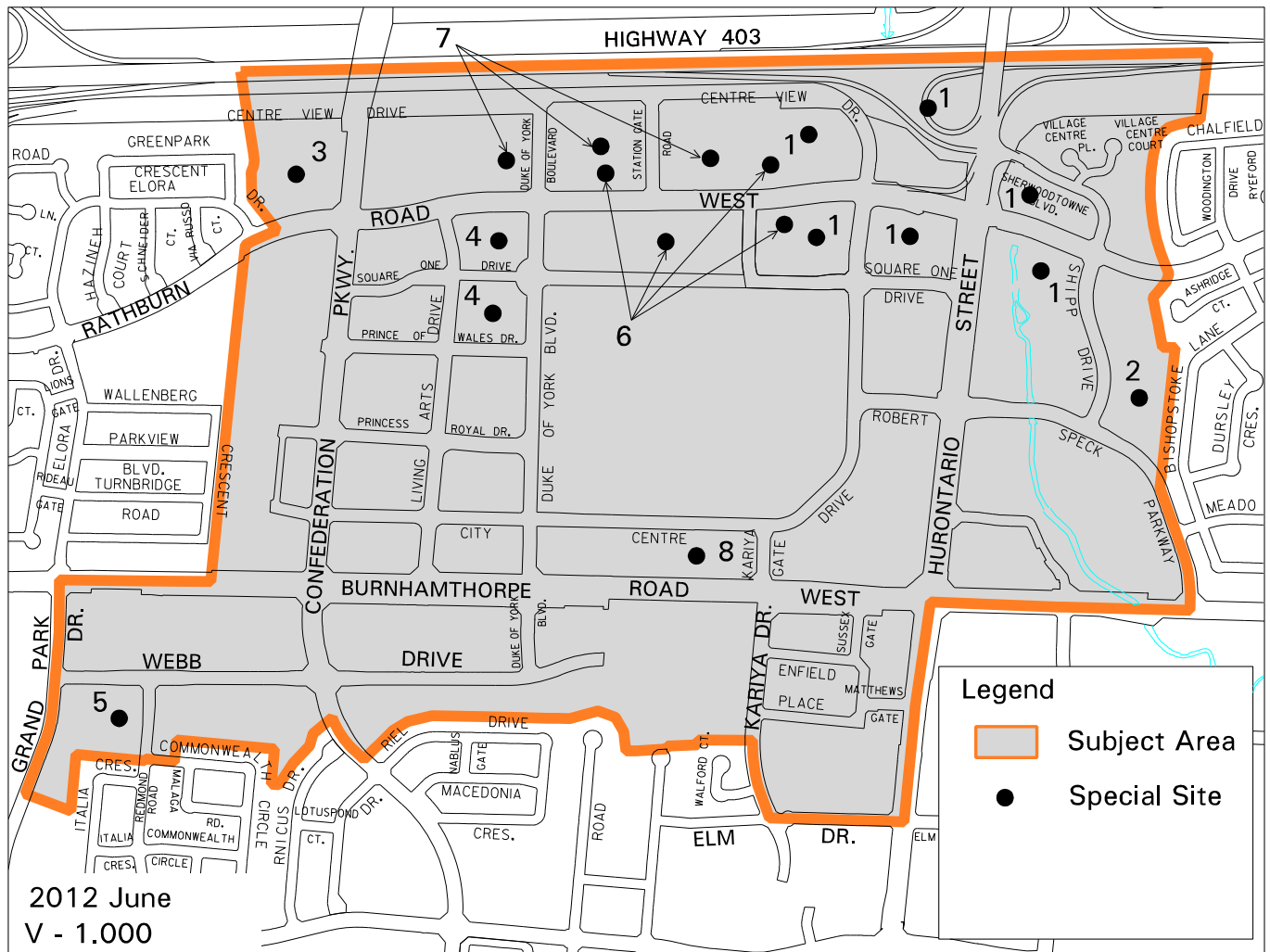
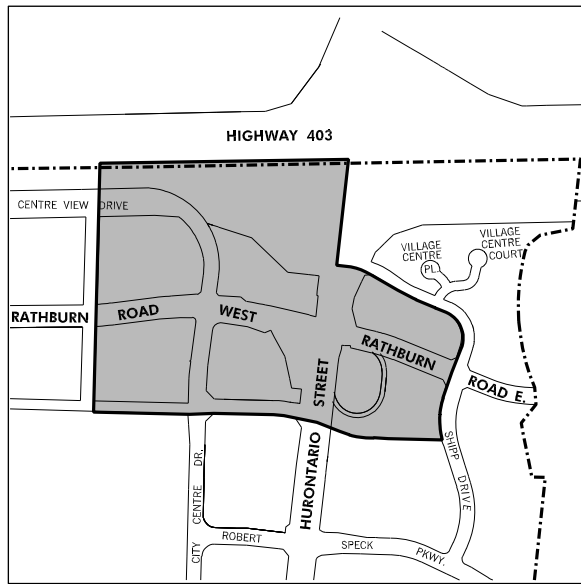


Figure 3: Location of Special Sites within the Downtown Core Local Area Plan.

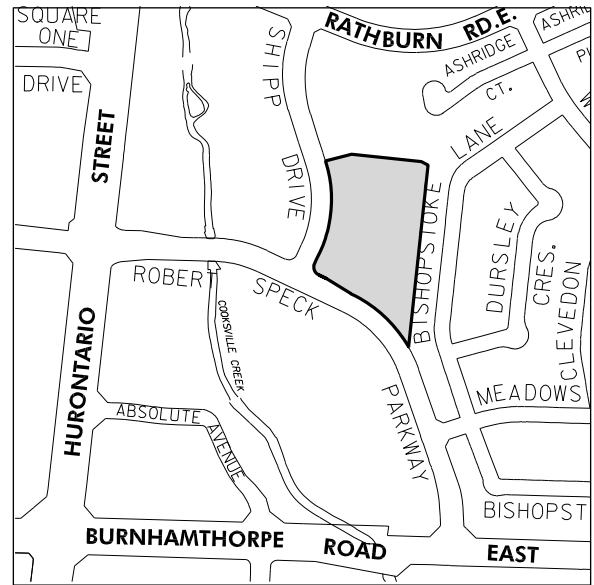
8.1 Site 1



8.1.1 The lands identified as Special Site 1 are located in the four quadrants adjacent to the Hurontario Street and Rathburn Road intersection.

8.1.2 Prior to the finalization of the road network for this site area, the appropriate transportation studies will be carried out in conjunction with the Provincial Government.

8.2 Site 2

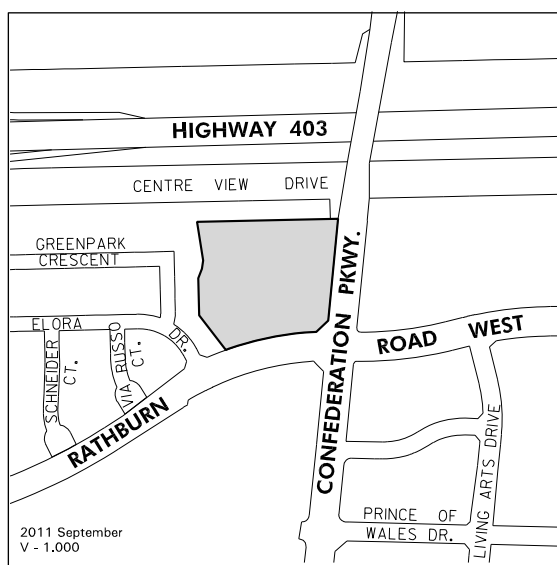


8.2.1 The lands identified as Special Site 2 are located at the northeast quadrant of Robert Speck Parkway and Shipp Drive.

8.2.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. townhouses will be permitted on the entire site.

8.3 Site 3



8.3.1 The lands identified as Special Site 3 are located north of Rathburn Road, west of the Confederation Parkway extension.

8.3.2 Notwithstanding the provisions of the Mixed Use designation, townhouse dwellings will be permitted.

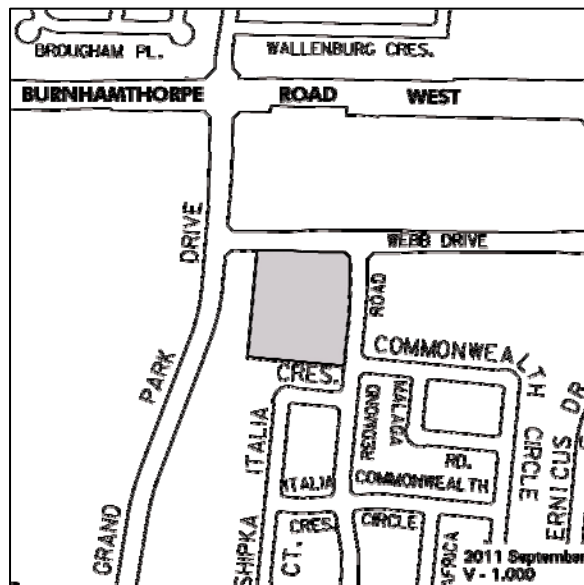
8.4 Site 4



8.4.1 The lands identified as Special Site 4 are bounded by Rathburn Road West to the north, Duke of York Boulevard to the east, Prince of Wales Boulevard to the south and Living Arts Drive to the west.

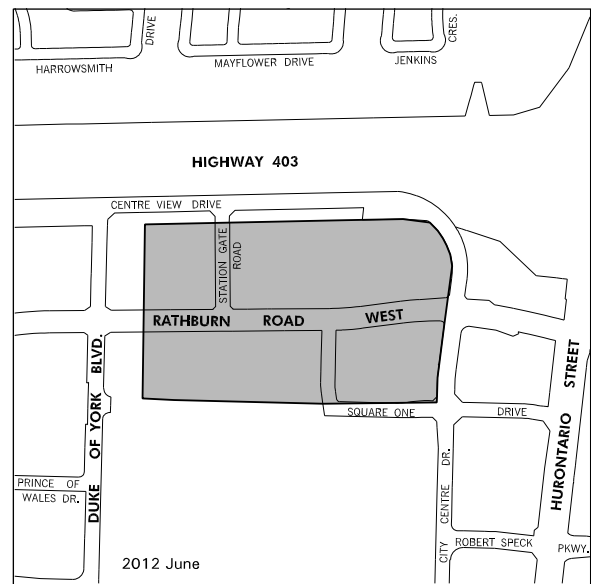
8.4.2 Notwithstanding the Open Space and Mixed Use designations shown on the Land Use Plan, the lands designated Open Space and Mixed Use may be increased or decreased in size, reconfigured or relocated within Special Site 4 without an amendment to this Plan and these lands may also be used for parking.

8.5 Site 5



8.5.1 Notwithstanding the Residential High Density designation, an apartment dwelling with a maximum height of 27 storeys will be permitted.

8.6 Site 6



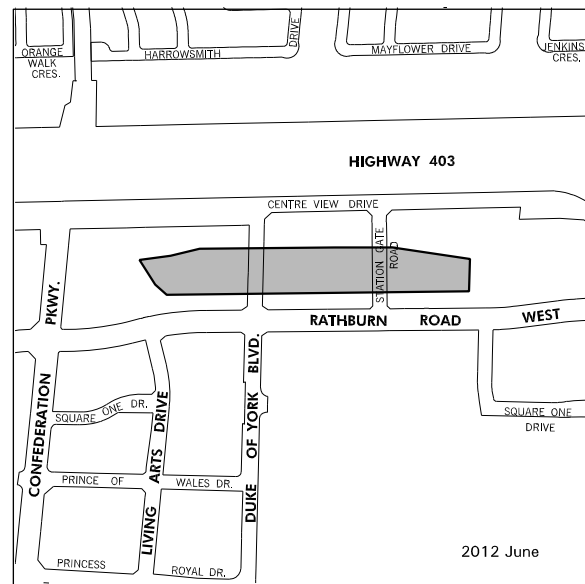
8.6.1 The lands identified as Special Site 6 are located on the north side of Rathburn Road West, between Hurontario Street and the future extension of Duke of York Boulevard.

8.6.2 Notwithstanding the Office and Open Space designation, the area will accommodate one or more areas of public parkland and will form part of an extensive system of linked parks and open spaces. The location, number and size of the park block(s) shall be determined in conjunction with the development of lands in this area. In order to achieve parkland of acceptable size, configuration and location, the City will encourage consolidation of parkland of multiple sites within the Area, either through dedication or acquisition, having consideration for matters including the proximity to residential uses and to the transit hub.

Prior to approval of any draft plan of subdivision, severance or any other development application within this area, property owners shall demonstrate to the satisfaction of the City how the City's parkland objectives within this area will be satisfied, through the provision of development and parkland concept plans. Notwithstanding the provision of public recreational facilities provided through the registration of Plan M-1010, prior to final approval of any severance, subdivision or any

other development application within this area, property owners shall dedicate sufficient parkland or enter into appropriate agreements with the City and other owners of lands to secure the provision of adequate parkland from to other development parcels with the area.

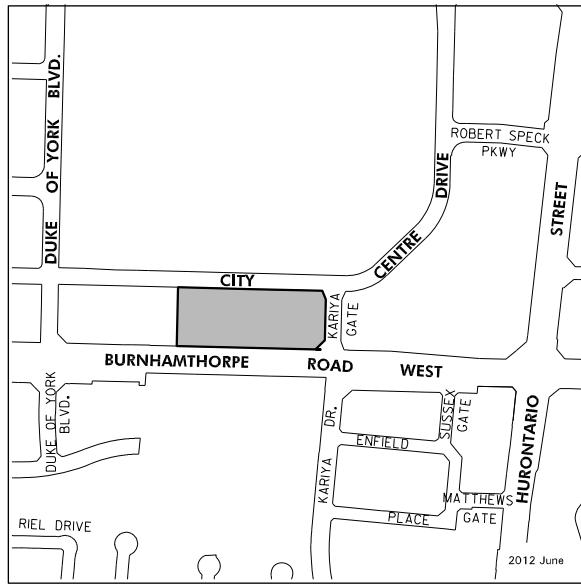
8.7 Site 7



8.7.1 The lands identified as Special Site 7 are located along the new east-west street north of Rathburn Road West, from west of City Centre Drive to Living Arts Drive.

8.7.2 This new street and future developments will serve as an important linkage in the parks and open space system. Future development along this corridor shall be designed to provide a high quality pedestrian experience along boulevards and through private and public open spaces such as plazas and parks.

8.8 Site 8

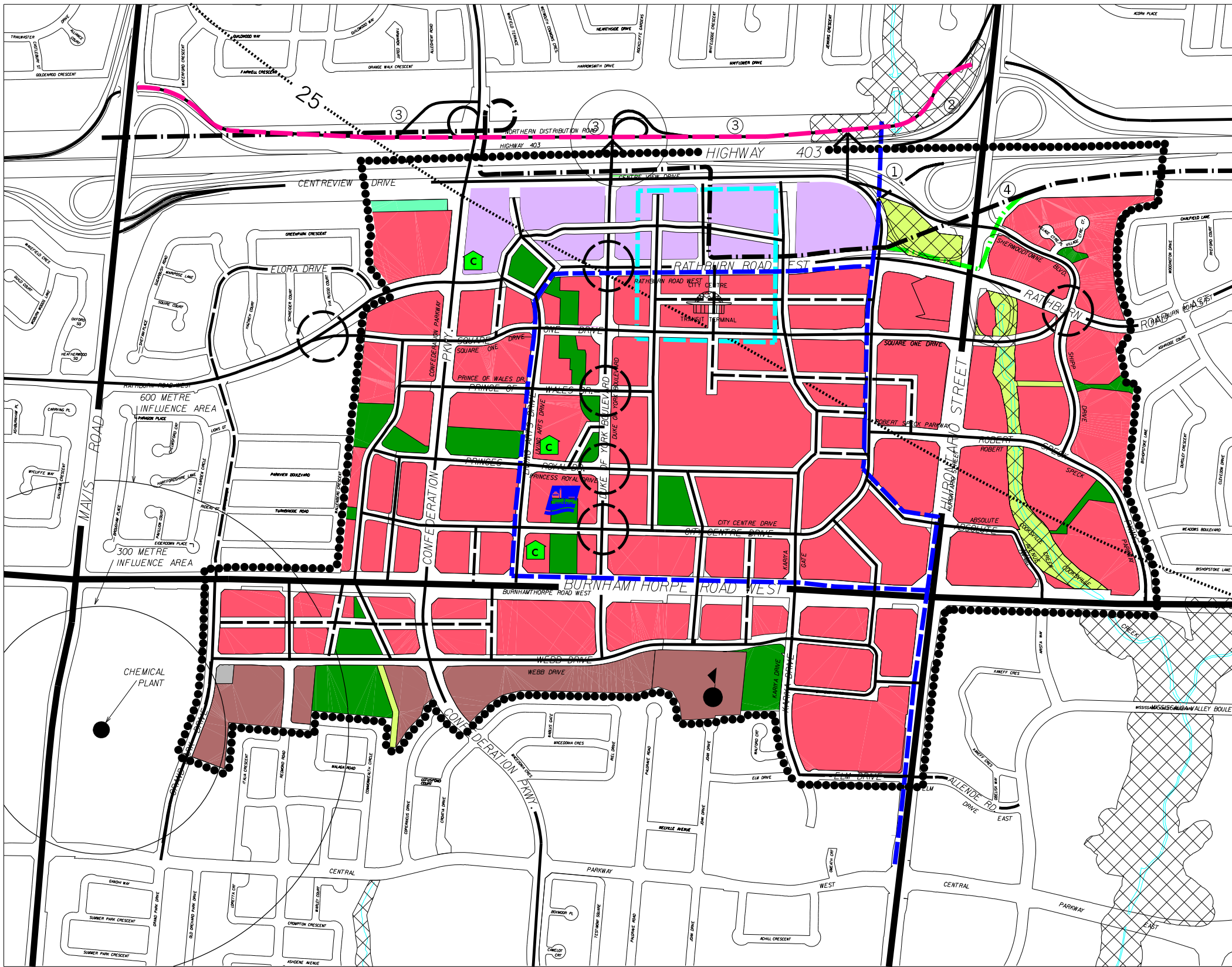


registration of Plan M-1010, prior to final approval of any severance, subdivision or any other development application within this area, property owners may be required to dedicate parkland or enter into appropriate agreements with the City and other owners of lands to secure the provision of adequate open space from other development parcels within the area.

8.8.1 The lands identified as Special Site __ are located north of Burnhamthorpe Road West, south of City Centre Drive, west of Kariya Gate and east of the proposed Main Street.

8.8.2 Notwithstanding the Mixed Use designation of these lands, future development will accommodate parkland and will form part of an extensive system of linked parks and open spaces. The provision of parkland may be in the form of boulevards, plazas and park spaces which shall be made accessible to the public. The location and size shall be determined in conjunction with the development of the lands in this area. In order to achieve open space of acceptable size, configuration and location, the City will consider how the proposed open space enhances connectivity between Kariya Park and the remainder of the Main Street District north of Burnhamthorpe Road West. Prior to the approval of any draft plan of subdivision, severance or any other development application within this area, property owners shall demonstrate to the satisfaction of the City, how open space objectives within this area will be satisfied through the submission of development and open space concept plans.

8.8.3 Notwithstanding the provision of park and public recreational facilities provided through the



LAND USE DESIGNATIONS

- Mixed Use
- Office
- Public Open Space
- Greenbelt
- Parkway Belt West
- Residential High Density
- Utility

LAND USE LEGEND

- 1996 NEP / 2000 NEF Composite Noise Contours
- Natural Hazards

TRANSPORTATION LEGEND

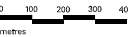
- Provincial Highway and Interchange
- Arterial
- Minor Collector
- Local Road
- Bus Rapid Transit Corridor
- Interim Bus Rapid Transit Corridor
- Light Rail Transit Corridor
- Proposed Grade Separation
- Proposed Roundabout
- Anchor Hub
- Civic Centre (City Hall)
- Public School
- City Centre Transit Terminal
- Community Facilities
- Local Area Plan Boundary

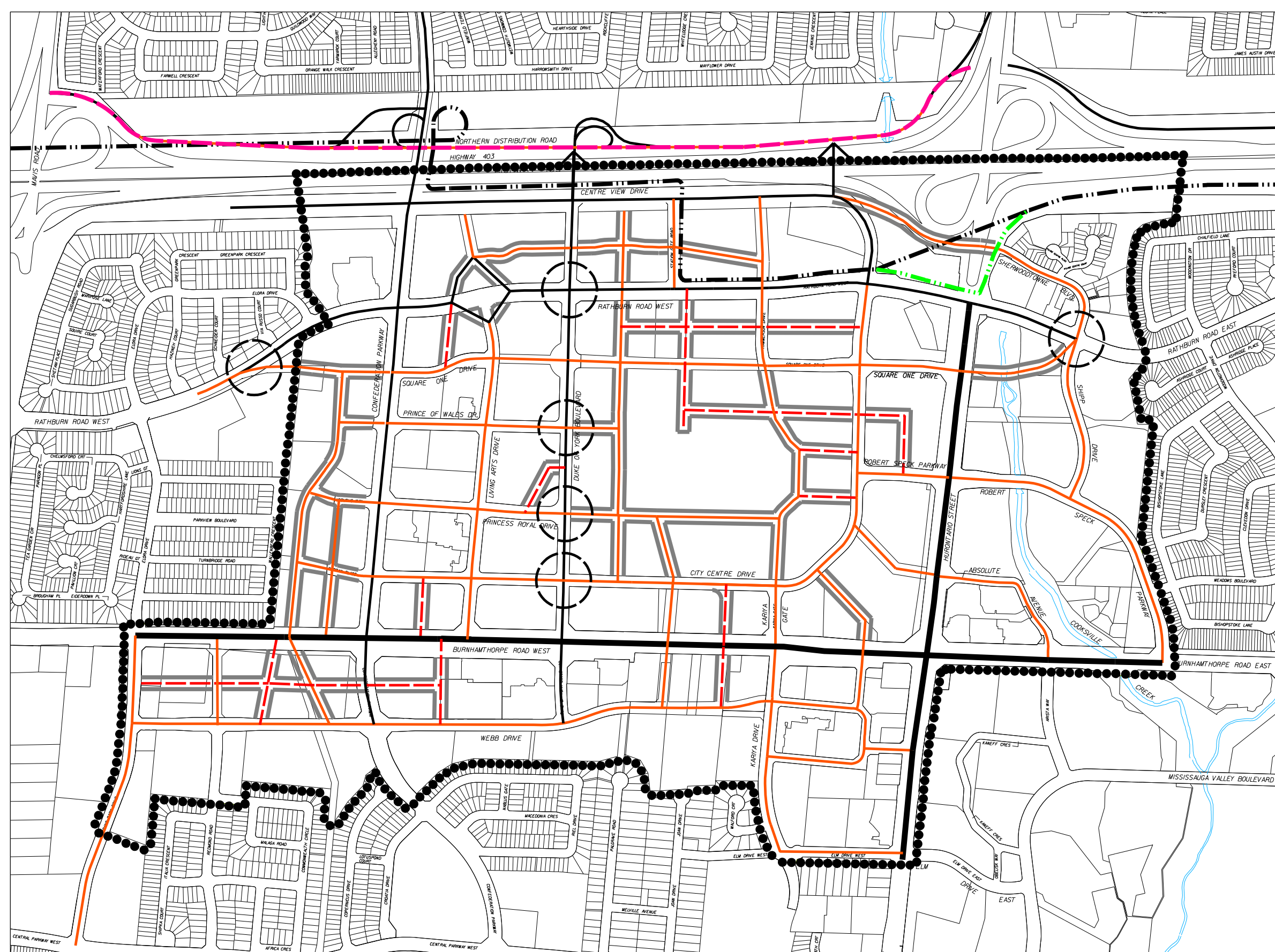
- Option 1, Conceptual Centre View Drive connection to EB Hwy. 403 concept.
- Conceptual N-W ramp terminal relocation.
- Future Road - For additional information see Schedule 5: Long Term Road Network and Table 8-2: Road Classification - Major Collectors
- The Interim Bus Rapid Transit Corridor depicts the interim alignment of the Bus Rapid Transit as per 2010 Environmental Assessment Approvals.

Notes:

- Any part of the road network shown outside the city boundaries is shown for information purposes only.
- All road proposals within the Highway 403 Corridor are conceptual and require review and approval of the Provincial Government.
- Refer to Schedule 3, Natural System for the location of the Natural Areas System and Natural Hazards.
- The limits of the Natural Hazards shown on this map are for illustrative purposes only. The appropriate Conservation Authority should be consulted to determine their actual location.
- The 1996 Noise Exposure Projection (NEP)/2000 Noise Exposure Forecast (NEF) Composite Noise Contours are shown for information purposes only and are not part of this Local Area Plan. These contours will change from time to time as new information becomes available. For accurate reference the composite NEP/NEF map produced by Transport Canada at a scale of 1:50 000 should be consulted.

Downtown Core Local Area Plan
Land Use Map





Legend

- Northern Distribution Road
- Arterial
- Major Collector
- Minor Collector
- Local Road
- Proposed Roundabout
- · - Bus Rapid Transit Corridor
- · - Interim Bus Rapid Transit Corridor
- ↑ Proposed New Crossings of Highway 403
- New Roads

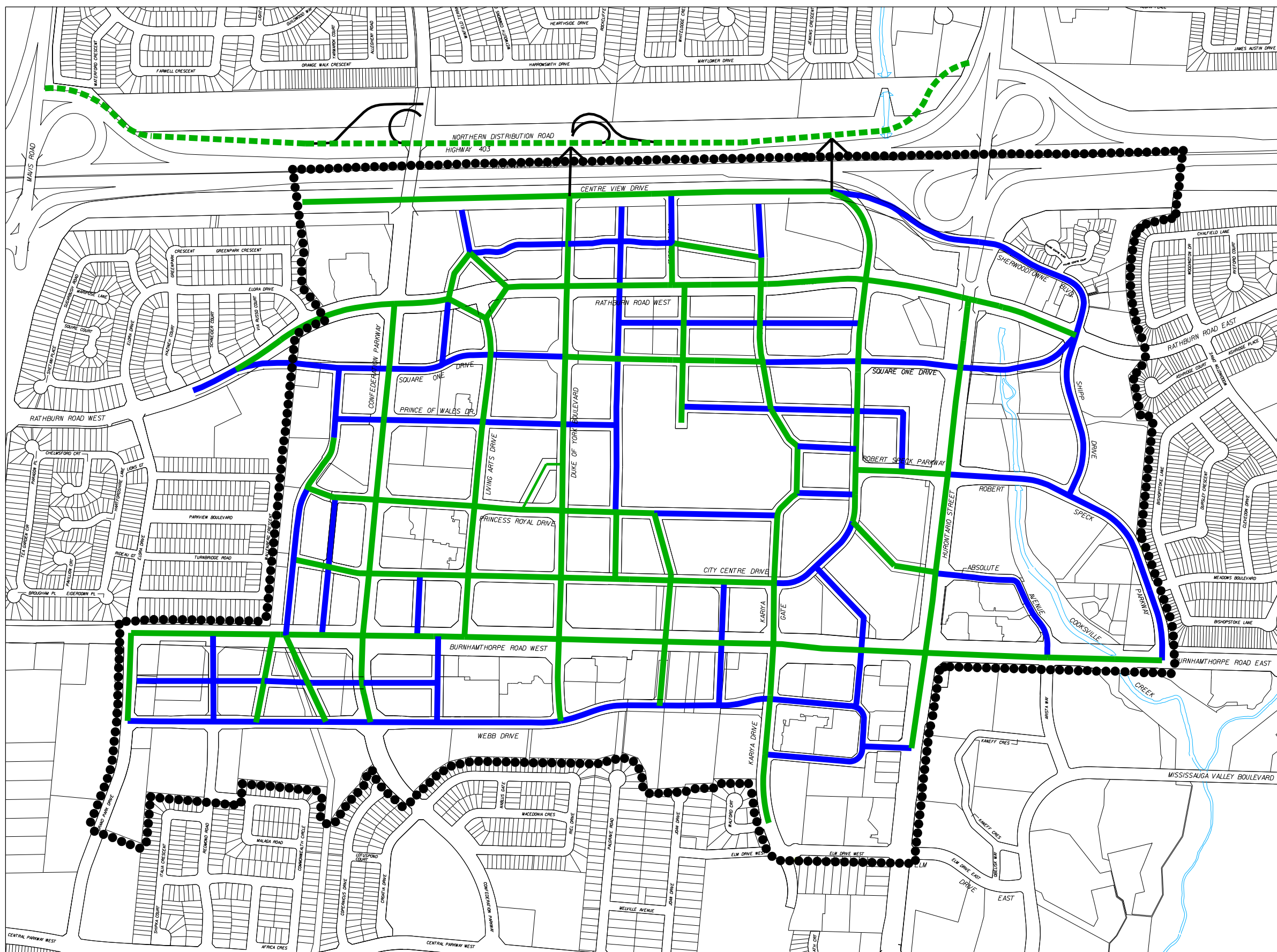
Note:

New Transportation Elements will be Subject to a Future Transportation Study.

The Interim Bus Rapid Transit Corridor depicts the interim alignment of the Bus Rapid Transit as per 2010 Environmental Assessment Approvals.

Schedule Downtown Core Road Network and Classification





Legend

- Northern Distribution Road
- A - Frontage
- B - Frontage
- ↑ Proposed New Crossings of Highway 403

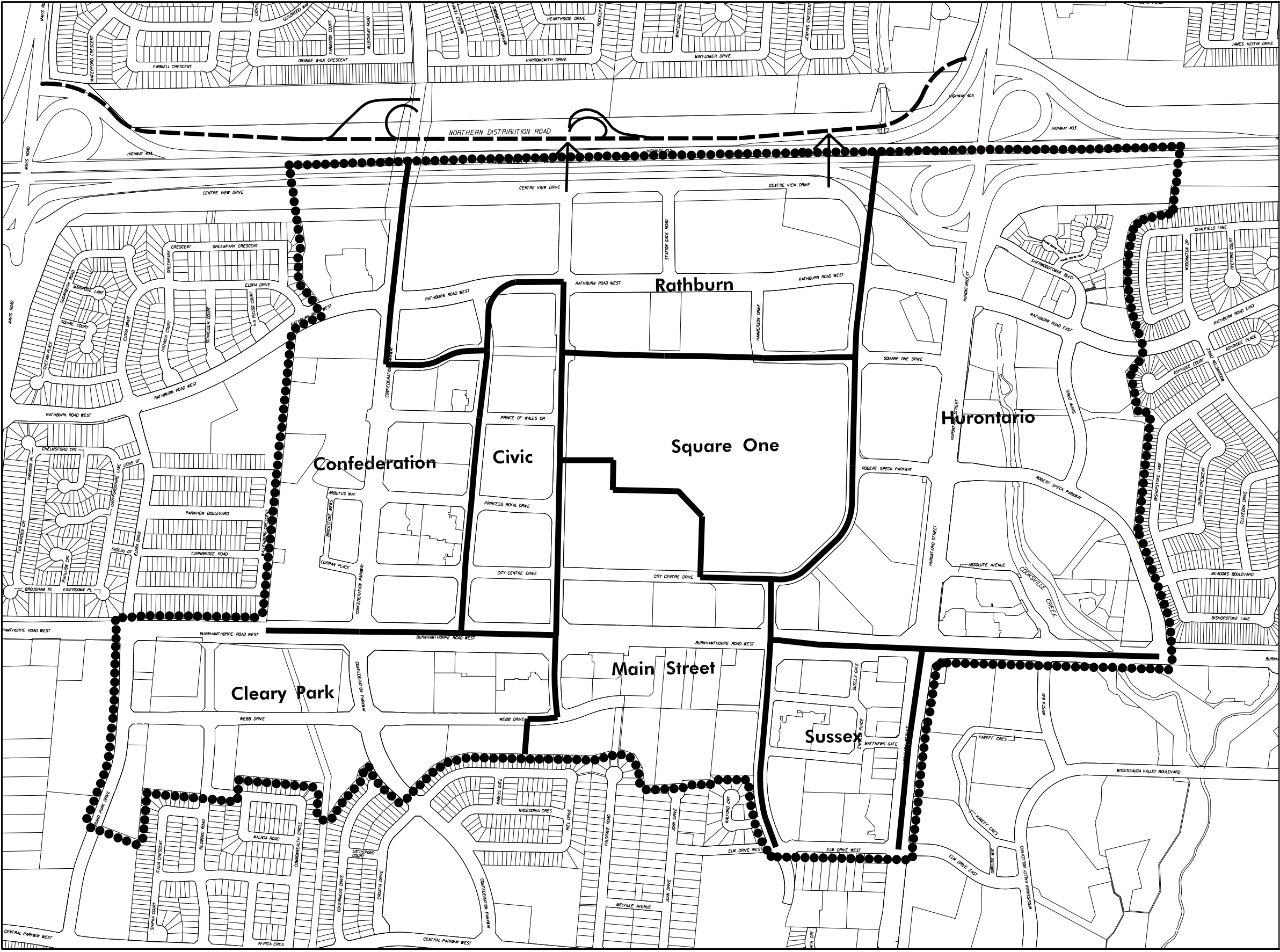
Draft

Schedule
Downtown Core
A & B Street Frontage



City of Mississauga

2012 June



- Legend
- Northern Distribution Road
 - ↑ Proposed New Crossings of Highway 403

Draft

Downtown Core Districts



Legend

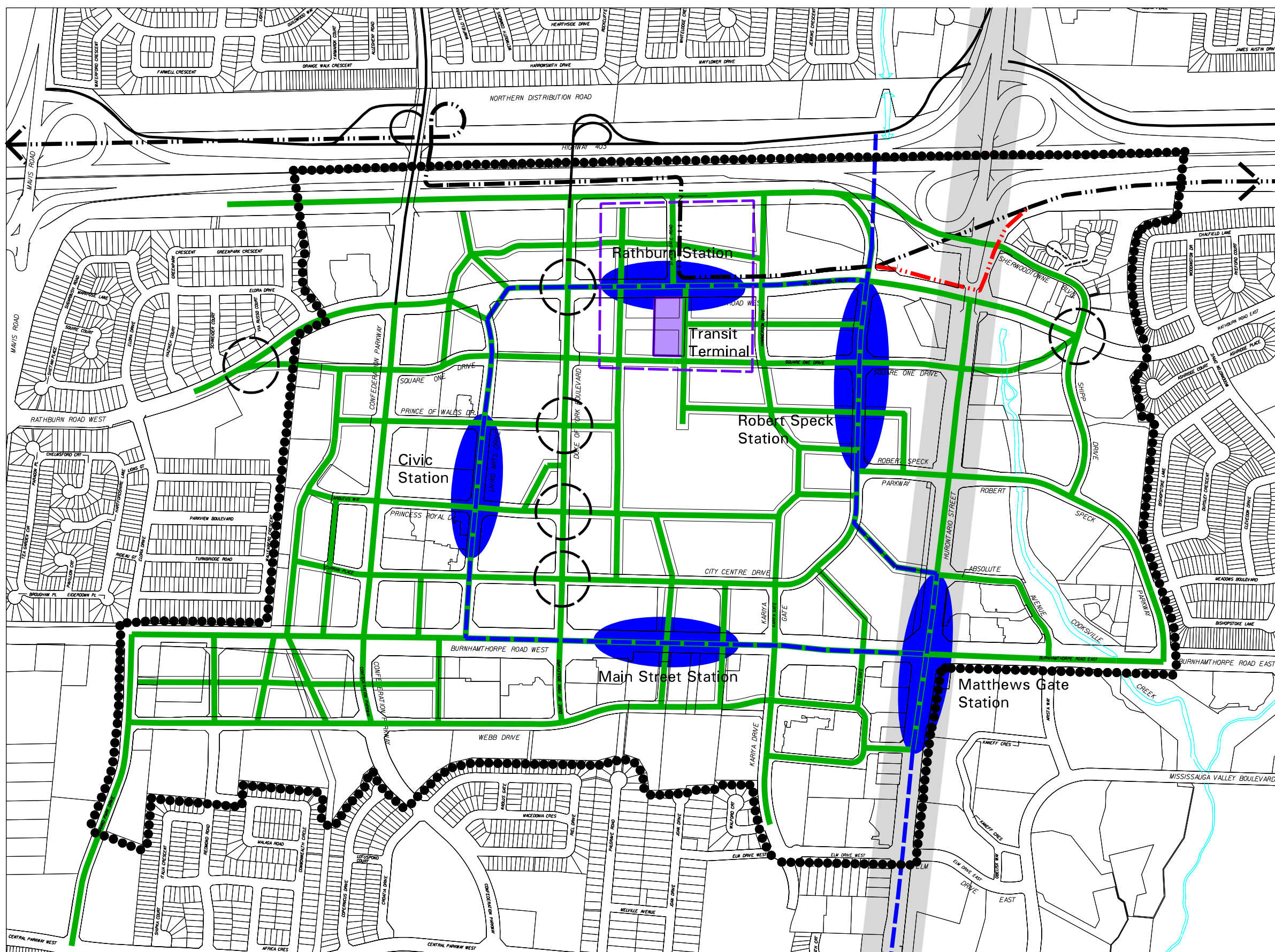
Percentage of ground floor along the street frontage required to contain retail uses:

- 90%
- 50%

Draft

Schedule
Retail Activation





Legend

- Bus Rapid Transit Corridor
- Interim Bus Rapid Transit Corridor
- Light Rail Transit
- Light Rail Transit Station Area
- Transit Terminal
- Hurontario Street - Intensification Corridor (OP)
- Anchor Hub
- Road Network

Note:

Light Rail Transit Alignment and Station Locations are subject to the Hurontario-Main MRT Environmental Assessment Study.

The Interim Bus Rapid Transit Corridor depicts the interim alignment of the Bus Rapid Transit as per 2010 Environmental Assessment Approvals.

Schedule Downtown Core Long Term Transit Network



Proposed Amendments to Zoning By-law 0225-2007 Downtown Core Local Area Plan

October 12, 2012

ITEM #	MOP/LAP SECTION	MISSISSAUGA OFFICIAL PLAN (MOP) - POLICIES/ LOCAL AREA PLAN (LAP) - POLICIES	PROPOSED AMENDMENTS TO ZONING BY-LAW	COMMENTS/EXPLANATION
2.0 INTRODUCTION				
1.	MOP 12.2 Downtown Core	MOP states: Official Plan policies for lands within the Downtown Core are contained in the Downtown Core Local Area Plan (LAP)	<u>Revise Part 7:</u> Part 7: Downtown Core City Centre Zones	The name of the Part 7 zones has been revised from "City Centre" Zones to "Downtown Core" Zones in conformity with Mississauga Official Plan (MOP) and the Downtown Core Local Area Plan (LAP).
2.	MOP 12.1.1.2 Downtown - General	MOP states: "A minimum building height of three storeys is required on lands within the Downtown."	<u>Add to Table 7.2.1:</u> Line 4B.0 - HEIGHT 4B.1 - Minimum height - 3 storeys (CC1, CC2, CC3, CC4 and CCO zones) <u>Delete "CC1 to CC4" from Subsection 2.1.27 and Delete Note (1) from Table 7.2.1:</u> "All buildings containing a residential dwelling unit that are zoned C4, CC1 to CC4 , and are located within the hatched area identified on Schedule 2.1.27 of this Subsection, shall have a minimum height of three (3) storeys.	Minimum height of 3 storeys has been introduced for all development in the Downtown Core in conformity with Mississauga Official Plan (MOP).

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2. (cont'd)	MOP 12.1.1.2 Downtown - General (continued)		<p><u>Delete Sentence 7.1.5.1.2</u> (minimum 3 storey height) from the CC4 zone regulations for apartment, long-term care and retirement dwellings.</p> <p><u>Delete Article 7.1.5.4</u> (building height measurement regulations) from the CC4 zone regulations.</p>	A separate explanation of "height" in the CC4 zone is not required since the Section 1.2 definition of "Height" applies to the Downtown Core Zones with exceptions for roof top equipment in Subsections 2.1.13 and 4.1.15.
3.0 URBAN DESIGN				
3.	3.1 Urban Design - General	LAP states: "prohibit the construction of new surface parking lots in the Downtown Core;"	Delete "Parking Lot" from Table 7.2.1 - Line 2.15	"Parking Lot" has been deleted as a permitted use in CC1 to CC4 zones in conformity with the Local Area Plan.
4.	3.1 Urban Design - General 3.2.2.2	<p>LAP states: "prohibit blank building walls abutting the street;"</p> <p>LAP states: "f. ground floor uses will incorporate transparent windows;"</p>	<p><u>Add to Table 7.2.1:</u> Line 4A.1 and 4A.8 requiring glazing on the first storey on an 'A' Street (75%) and 'B' Street (50%)</p> <p><u>Add to Section 1.2 - Definitions:</u> "Glazing" means clear or transparent glass.</p>	<p>Regulations requiring glazing have been added for the first storey on 'A' and 'B' Streets.</p> <p>Definition of "glazing" has been added.</p>

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3.2.2 - Design of the Private Realm and Frontage Requirements				
5.	3.2.2.1 'A' and 'B' Streets	LAP states: "All streets are designated 'A' and/or 'B' streets...."	<p><u>Add to Table 7.2.1:</u> Line 4A.0 - 'A' and 'B' STREET FRONTAGES</p> <p><u>Add to Schedules CC1 to CC3:</u> 'A' Street Frontage 'B' Street Frontage</p> <p><u>Revise Note (4) in Table 7.2.1,</u> which exempts additions to existing buildings from the Build-to area regulations in Lines 4.1 and 4.2, to <u>add:</u> ".... for 100 City Centre Drive"</p>	<p>'A' and 'B' Street Frontages have been added to the Zoning By-law, with regulations, and illustrated on the Schedules for the Downtown Core Zones to implement the policies of the Downtown Core Local Area Plan.</p> <p>The exemption from the Build-to area regulations for additions to existing buildings that is currently in the Zoning By-law is retained for Square One - 100 City Centre Drive.</p>
6.	3.2.2.2 Provisions for Development on 'A' & 'B' Streets	LAP states: "c. locate buildings parallel to the street at the build-to line to contain the street and provide enclosure;"	<p><u>Retain the Build-to Areas</u> on Schedules CC1 to CC2(3) and add to Schedule CCO</p> <p><u>Delete Line 4.3 in Table 7.2.1</u> that permits 30% of the length of the streetwall to be set back beyond the Build-to area</p>	<p>Build-to Areas are retained as is on the Schedules in the Zoning By-law.</p> <p>The regulation currently in the Zoning By-law that allows 30% of the length of the streetwall to be set back beyond the Build-to area is deleted to ensure that development is within the Build-to areas shown on the Schedules.</p>

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7.	3.2.2.5 'A' Streets	LAP states: "Functioning principal entrances to buildings will be provided on 'A' Streets."	<u>Add to Table 7.2.1:</u> Line 4A.2 - The main front entrance to each first storey unit that has an exterior wall abutting an 'A' Street shall be provided from that 'A' Street. Line 4A.3 - The main front entrance to units above or below the first storey, within a building, structure or part thereof that has frontage along an 'A' Street, shall be provided from that 'A' Street.	Regulations have been added restricting the location of the main front entrance for units on the first storey, and for units above or below the first storey on 'A' and 'B' Streets.
	3.2.2.9 'B' Streets	"Functioning principal building entrances will be located on 'B' Streets"	Line 4A.9 -The main front entrance to each first storey unit that has an exterior wall abutting a 'B' Street shall be provided from that 'B' Street Line 4A.10 - The main front entrance to units above or below the first storey, within a building, structure or part thereof that has frontage along a 'B' Street, shall be provided from that 'B' Street.	
		except where a development block may have 'A' and 'B' streets, the most prominent building entrances will be located on 'A' streets."	Line 4A.4 - The main front entrance to each first storey unit that has exterior walls abutting both 'A' and 'B' Streets shall be provided from the 'A' Street.	

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7. (cont'd)	'A' and 'B' Streets		Line 4A.5 - The main front entrance to units above or below the first storey, within a building, structure or part thereof that has frontage along both 'A' and 'B' Streets, shall be provided from the 'A' Street.	
8.	3.2.2.7 'B' Streets	LAP states: " 'B' streets provide development blocks with vehicular access to off street parking,"	Add to Table 7.2.1: Line 4A.7 - Vehicular access to a building, structure or part thereof which abuts both 'A' and 'B' Streets shall be provided from the 'B' Street	A regulation has been added to require vehicular access from the 'B' Street where a building abuts both 'A' and 'B' Streets.
9.	3.2.2.11 Parking, Servicing and Loading Design in the Streetscape	LAP states: "b. parking structures will not directly front onto 'A' streets..."	<u>Add to Table 7.2.1:</u> Line 4A.6 - A parking structure shall not front on a street with an 'A' Street Frontage.	A regulation has been added to prohibit parking structures from fronting onto 'A' Streets.
10.	Parking Standards		<u>Add to Table 3.1.2.2:</u> 30.2 Night Club (in CC1, CC2 and CCO zones) - 9.0 spaces per 100 m ² GFA - non-residential	A reduced parking standard for "Night Clubs" in CC1, CC2 and CCO zones has been added.
11.	Parking Standards		<u>Add CCO to the Parking Standard in Table 3.1.2.2:</u> 33.3 - Personal Service Establishment (in CC2 to CC4 and CCO zones) - 4.3 spaces per 100 m ² GFA - non-residential	The parking standard for "Personal Service Establishments" in CC2 to CC4 zones has been applied to the CCO (Office) zone.

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12.	Parking Standards		<u>Add CCO to the Parking Standard in Table 3.1.2.2:</u> 41.3 - Retail Store (in CC2 to CC4 and CCO zones) - 4.3 spaces per 100 m ² GFA - non-residential	The "Retail Store" parking standard in CC2 to CC4 Zones has been applied to the CCO (Office) Zone.
13.	Parking Standards		<u>Add to Table 3.1.2.2:</u> 42.5 - Restaurant (less than or equal to 220 m ² GFA - restaurant in CC2 to CC4 and CCO zones)- 4.3 spaces per 100 m ² GFA - restaurant 42.6 - Restaurant (greater than 220 m ² GFA - restaurant in CC2 to CC4 and CCO zones)- 9.0 spaces per 100 m ² GFA - restaurant	A reduced parking standard for Restaurants less than or equal to 220 m ² has been added for CC2 to CC4 and CCO Zones. A reduced parking standard for Restaurants greater than 220 m ² in CC2 to CC4 and CCO Zones has also been added.
3.2.3 - Built Form				
14.	3.2.3.7 Built Form	LAP states: "Tall buildings in the Downtown Core will be designed and massed in the form of a podium, tower and top.	<u>Add to Table 7.2.1:</u> Line 4C.0 - PODIUMS IN RESIDENTIAL BUILDINGS Line 4C.1 - Apartment, long-term care and retirement dwellings greater than 12 storeys shall contain a podium.	A regulation has been added requiring a podium for residential dwellings over 12 storeys.

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15.	3.2.3.8 Built Form	LAP states: "Podiums will be located and massed to fit harmoniously into their existing and/or planned context"	<p><u>Delete from s. 1.2</u> - existing definition of "Podium"</p> <p><u>Add to s. 1.2:</u> "Podium" means the base of a building, structure or part thereof located at or above established grade that projects from the tower portion of the building.</p> <p><u>Add to s. 1.2:</u> Height of a Podium means the vertical distance between the established grade and the highest point of the roof surface of the podium.</p> <p><u>Delete:</u> 7.1.5.3 – Podiums</p> <p><u>Retain in s. 1.2</u> - existing definition of "Tower Floor Plate"</p>	<p>Since the existing definition of "Podium" in the Zoning By-law refers only to residential uses, a new definition of "Podium" has been added.</p> <p>A definition of "Height of a Podium" has been added.</p> <p>A separate explanation of "Podium" in the CC4 Zone is not required since the Section 1.2 revised definition of "Podium" in the Zoning By-law applies to the Downtown Core Zones.</p> <p>The existing definition of "Tower Floor Plate" has been retained.</p>

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16.	3.2.3.8 Built Form	LAP states: "a. The height of the podium will generally be equal to the width of the right of way but not less than 3 storeys in height; b. Podiums will incorporate a street wall placed at the build-to-line, having a minimum of 3 storeys;"	<u>Add to Table 7.2.1:</u> Line 4C.2 - The minimum height of a podium measured at the streetwall shall be 3 storeys.	A regulation has been added requiring a minimum height of 3 storeys for a podium.
17.	3.2.3.9 Built Form	LAP states: "b. designing the floor plate size and shape with appropriate dimensions for the site, ..."	<u>Add to Table 7.2.1:</u> Line 3.2 - Maximum tower floor plate for an apartment dwelling shall be 750 m ²	A maximum tower floor plate size has been introduced into the Zoning By-law.
18.	3.3.9 Transition Areas	LAP identifies specific policies for Transition Areas 'A', 'B' and 'C', including heights.	The existing By-law contains the following Exception Zones: Area 'A' - H-CC4-5 Area 'B' - CC3-1, H-CC3(2), H-CC3(3) Area 'C' - H-CC3(1)	The lands in the Transition Areas identified in the Local Area Plan are in existing Exception Zones and/or on Schedules in the Zoning By-law which contain regulations restricting the heights. The existing Exception zones have been retained.
19.	3.3.10 Cooksville Creek Corridor	LAP identifies specific policies for the Cooksville Creek Corridor	The "G1" Zone does not permit development.	The existing Zoning By-law zones the Cooksville Creek Corridor as "G1" (Greenbelt). The "G1" zone has been retained for the Cooksville Creek Corridor.

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4.0 LAND USE				
20.	4.1.1.1 Uses Permitted in all Designations	LAP states: "a. cogeneration facility will be permitted as an accessory use"	<p><u>Add Item (1) to Sentence 7.1.4.1.1 in Uses Accessory to a Permitted Use in the CC1 Zone</u> (1) Cogeneration Facility</p> <p><u>Add Item (10) to Sentence 7.1.4.2.1 in Uses Accessory to a Permitted Use in CC2, CC3 and CC4 Zones</u> (10) Cogeneration Facility</p> <p><u>Include Item (10) in Sentence 7.1.4.3.1 in Uses Accessory to a Permitted Use in the CCO Zone</u> (10) Cogeneration Facility</p>	<p>A cogeneration facility has been added as an accessory use in CC1 to CC4 zones in conformity with the Downtown Core Local Area Plan.</p> <p>A cogeneration facility has been included in the CCO (Office) zone.</p>
21.	MOP 11.2.2 Utility	MOP states: "...lands designated Utility will also permit the following uses: a. parking; and b. accessory uses.	Retain the "U" Zone as is	The only site in the Downtown Core designated "Utility" is the Mississauga Hydro property at Webb Drive and Grand Park Drive. The site is zoned "U" which does not permit parking or accessory uses. The existing "U" zone has been retained on this site.

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22.	MOP 11.2.4.2 Open Space	MOP permits in Public Open Space: agriculture demonstration site, cemetery, conservation, golf course, nursery gardening, recreational facilities, stormwater retention and stormwater quality pond and accessory uses	Retain the CCOS zoning provisions. Add CCOS zone to Z-Maps 22 and 29 and Schedules CC1, CC2(1) and CCO in conformity with the Public Open Space designation on the Downtown Core Local Area Plan Land Use Map.	The CCOS zone permits: Active Recreational Use, Passive Recreational Use and Parking Structure - Below Grade Only. The CCOS zone has been retained as is. All the lands designated Public Open Space in the Downtown Core Local Area Plan have been zoned CCOS.
23.	4.1.2 Open Space 8.6 Special Site Policies - Site 6	LAP - Site 6 states: "8.6.2 - Notwithstanding the Office designation, the area will accommodate one or more areas of public parkland The location, number and size of the park block(s) shall be determined in conjunction with the development of lands in this area."	No identification of CCOS zoning is required in the Site 6 area at this time. Lands will be rezoned to CCOS for the proposed park blocks at such time as the lands in the area are developed.	Lands will be rezoned to CCOS for the proposed park blocks in the Special Site 6 area identified in the Downtown Core Local Area Plan (s. 8.6), with the location to be determined when the lands are developed.
24.	4.1.3.2 Residential	LAP states: " the Residential High Density designation will also permit the following uses: a. horizontal multiple dwellings accessory to an apartment dwelling; b. long-term care dwelling; and c. retirement dwelling."	RA1 to RA5 zones permit apartment dwellings, long-term care dwellings and retirement dwellings. Horizontal multiple dwellings are not permitted in the RA1 to RA5 Base Zones	The Apartment Dwelling Zones in the Residential High Density designation in the Downtown Core are Exceptions: RA4-5, RA4-6, RA5-10, RA5-15, RA5-17 permit apartment, long-term care and retirement dwellings; RA1-28 permits only a Place of Religious Assembly and Day Care; "D" permits the existing use (vacant). These Exception zones have been retained.

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27.	4.1.4 Mixed Use	Lands formerly designated within the City Centre District in Mississauga Plan are designated "Mixed Use" in the Downtown Core Local Area Plan (LAP).	<p><u>Revise Table 7.2.1:</u> CC1 - City Centre - Retail Downtown Core - Core Commercial CC2 - City Centre - Downtown Core - Mixed Use CC3 - City Centre - Downtown Core - Mixed Use Transition Area CC4 - City Centre - Downtown Core - Mixed Use CCO - City Centre - Downtown Core - Office CCOS - City Centre - Downtown Core - Open Space</p> <p><u>Revise Line 40.3 in Table 3.1.2.2:</u> CC1 - Retail Downtown Core - Core Commercial - 4.57 spaces per 100 m² GFA - non-residential</p>	The names of the CC1 to CC4, CCOS and CCO (Office) zones have been changed to "Downtown Core", to be consistent with the Downtown Core Local Area Plan.
28.	4.1.4 Mixed Use	Square One lands and lands south of Rathburn Road West redesignated from "Downtown Core Commercial" (MOP) to "Mixed Use" in LAP	Lands are zoned H-CC1 and CC1	The CC1 zone was established mainly for Square One Mall and lands to the north. The list of uses permitted includes all the C1 to C4 uses. The CC1 and H-CC1 zones have been retained.
29.	4.1.4.1 Mixed Use	Comparison of Permitted Uses in LAP (s. 4.1.4.1) with MOP (s. 11.2.6) and MOP (s. 12.1.3)	No additional changes to Permitted Uses in Table 7.2.1	The list of Permitted Uses in the Downtown Core Zones (CC1 to CC4) is in conformity with the "Mixed Use" designation in the Downtown Core Local Area Plan.

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30.	4.1.4 Mixed Use		<p><u>Add</u> Article 7.1.4.1: Uses Accessory to a Permitted Use in the CC1 Zone</p> <p>7.1.4.1.1 The following uses are permitted: (1) Cogeneration facility (2) Night club</p> <p>7.1.4.1.2 An outdoor patio shall be permitted accessory to a restaurant, take-out restaurant or retail store where the primary function is the sale of food</p>	A separate section has been added to the General Provisions for the Downtown Core Zones to permit cogeneration facilities, night clubs and outdoor patios accessory to a permitted use in the CC1 zone.
31.	4.1.4 Mixed Use MOP 11.2.6.1 Mixed Use	MOP states: "..... lands designated Mixed Use will also permit the following: j. residential"	Table 7.2.1 permits: "2.8 - All uses permitted in C1 to C4 Base Zones", including "Dwelling unit located above the first storey of a commercial building"	A "Dwelling unit located above the first storey of a commercial building" is a permitted use in C4. Since the CC1 zone permits C1 to C4 Base Zone uses, this use is also permitted in the CC1 zone.
32.	4.1.4 Mixed Use		<p><u>Add to Sentence 7.1.4.2.1:</u> "The following uses are permitted accessory to an office building, medical office building, apartment dwelling, ... <u>hospital, university/college, staff/student residence, parking structure</u></p>	The list of uses in CC2, CC3 and CC4 that permit accessory uses has been expanded to also allow accessory uses to a hospital, university/college, staff/student residence and parking structure.

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33.	4.1.4 Mixed Use		<u>Amend the definition of "Staff/Student Residence"</u> in Section 1.2 to add "on behalf of" as follows: "..... means accommodation owned or operated by <i>or on behalf of</i> a university, college or hospital consisting of dwelling units or rooms or both."	The definition of "Staff/Student Residence" has been expanded to add "on behalf of" a university, college or hospital.
34.	4.1.4 Mixed Use		<u>Add to Sentences 7.1.4.2.1 and 7.1.4.3.1: Uses Accessory to a Permitted Use in CC2, CC3, CC4 and CCO Zones:</u> "Outdoor patio accessory to a restaurant, take-out restaurant or retail store where the primary function is the sale of food"	In CC2, CC3, CC4 and CCO zones, the permission for an outdoor patio accessory to a restaurant and take-out restaurant has been retained and an "outdoor patio accessory to a retail store where the primary function is the sale of food" has been added.
35.	4.1.4 Mixed Use		<u>Add s. 7.1.4.1.1(2) and 7.1.4.2.2</u> to permit a night club accessory to a non-residential use in CC1 and CC2 zones in compliance with Table 2.1.2.2.1 <u>Add to s. 7.1.4.3.1</u> (11) Night Club in compliance with Table 2.1.2.2.1 (in CCO zone) <u>Add to Table 2.1.2.2.1 - Night Club,</u> provisions for Night Clubs in CC1, CC2 and CCO zones and revise Schedule 2.1.2.2(2) to show where the use is permitted	A Night Club has been added as an accessory use in CC1, CC2 and CCO zones. Provisions for a night club in the Downtown Core Zones have been added to Table 2.1.2.2.1 including: the specific area where night clubs are permitted (the area bounded by Hurontario Street, Burnhamthorpe Road West, Duke of York Blvd. and Centreview Drive), a maximum gross floor area of 300 m ² and the requirement that a night club be located only in a non-residential building.

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36.	MOP 11.2.6.1 Mixed Use LAP 4.1.4 Mixed Use	MOP permits in Mixed Use: "j. residential" LAP (s. 4.1.4.1) permits Residential High Density (Apartments) in Residential. s. 4.1.4.2 also permits: a. horizontal multiple dwellings accessory to an apartment dwelling; b. long-term care dwelling; and c. retirement dwelling."	<u>Table 7.2.1</u> permits the following in CC1 to CC4: Apartment Dwelling Long-Term Care Dwelling Retirement Dwelling <u>Article 7.1.3A.1</u> states: Horizontal Multiple Dwellings shall only be permitted accessory to an apartment dwelling in CC1, CC2, CC3 and CC4 zones	The residential uses permitted in the Mixed Use designation are the same as the residential uses permitted in the Residential High Density designation in the Downtown Core, namely, apartment, long-term care and retirement dwellings as well as horizontal multiple dwellings accessory to an apartment dwelling.
37.	4.1.4 Mixed Use	LAP (s. 4.1.4.3) permits townhouses in Transition Areas 'B' and 'C' (identified on Map 1 in Subsection 3.3.9). LAP (s. 8.3 - Site 3) permits townhouse dwellings. LAP (s. 8.3 - Site 2) permits townhouses.	<u>Table 7.2.1</u> and Subsection 7.1.3 permit Townhouse and Street Townhouse Dwellings within Areas 'A' and 'B' on Schedules CC3(1) to CC3(3). Exception CC2-2 permits Townhouse Dwellings in an area north of Rathburn Road West, west of Confederation Pkwy. Exception CC3-1 permits Townhouse and Street Townhouse Dwellings north of Robert Speck Pkwy., east of Shipp Drive	Townhouse and street townhouse dwellings are permitted only in specific locations identified in the Downtown Core Local Area Plan and the Zoning By-law.

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38.	MOP 11.2.6.1 Mixed Use	MOP permits: "c. entertainment, recreation and sports facility"	Article 2.1.9.6 permits a "Community Centre" in CC1 to CC4 and CCOS. CC1 zone (which permits C1 to C4 uses) permits a freestanding "Recreational Establishment" and "Entertainment Establishment". These uses are permitted in CC2 to CC4 and CCO as an accessory use.	The Zoning By-law permits Community Centres in all Downtown Core Zones. Entertainment and Recreation Establishments are permitted in the CC1 zone, and only as an accessory use in the CC2 to CC4 and CCO zones.
39.	MOP 11.2.6.5 Mixed Use	MOP states: "Residential uses will be discouraged on the ground floor."	<u>Add Line 3.3 to Table 7.2.1:</u> Notwithstanding the uses permitted in Lines 2.3, 2.4 and 2.5 of this Table, apartment, retirement and long-term care dwelling units shall not be permitted on the first storey of a building. Shared entrance and exit facilities through a common vestibule shall be permitted for the dwelling units. (CC1)	The CC1 (Downtown Core - Core Commercial) zone permits High Density Residential uses; however, a provision has been added to not allow residential dwelling units on the first storey of a building.
40.	4.1.4 Mixed Use	LAP does not carry forward the Mixed Use Special Site 4 that permits a motor vehicle service station at the southwest corner of Burnhamthorpe Road West and Hurontario Street.	<u>Retain the H-CC2-3 zone and revise Sentence 7.2.3.3.1 to state:</u> Additional Permitted Use 7.2.3.3.1 (1) Motor Vehicle Service Station legally existing on the date of passing of this By-law	The H-CC2-3 zone has been revised to recognize only the existing motor vehicle service station on the site and to continue to permit CC2 uses upon removal of the "H" (Holding) provision.

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41.	4.1.5.1 Retail Activation	LAP states: "Existing and new streets in the Downtown Core have been identified for the purpose of: a. establishing retail objectives within mixed use buildings;"	<u>Add Subsection 7.1.4A:</u> "7.1.4A Retail Activation Frontages"	A new Subsection has been added to the Downtown Core General Provisions entitled "Retail Activation Frontages".
42.	4.1.5.2 Retail Activation	LAP states: "... only the following uses will be permitted on the ground floor: a. financial institution; b. person service establishment; c. restaurant; and d. retail store"	<u>Add Sentence:</u> The first storey of a building shall contain Retail Activation uses in units abutting the street. Retail Activation uses shall only include the following: (1) Financial Institution (2) Personal Service Establishment (3) Restaurant (4) Take-out Restaurant (5) Retail Store	The range of uses permitted on the first storey in buildings with Retail Activation Frontages is limited for units facing the street.
43.	4.1.5.2 Retail Activation		<u>Add Sentence:</u> In CC2 and CCO zones, the following uses shall also be permitted on the second storey: (1) Repair Establishment (2) Recreational Establishment (3) Entertainment Establishment (4) Motor Vehicle Rental Facility (5) Cogeneration Facility (6) Night Club as an accessory use in a non-residential building	Additional uses are permitted on the second storey of buildings with Retail Activation Frontages.

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44.	4.1.5.1 Retail Activation	LAP states: "Existing and new streets in the Downtown Core have been identified for the purpose of: c. setting out requirements in accordance with the Retail Activation Schedule included in this Plan;"	<u>Add Sentence:</u> to identify a 90% Retail Activation Frontage and a 50% Retail Activation Frontage <u>Add to the Schedules:</u> 90% Retail Activation Frontage 50% Retail Activation Frontage	Provisions for 90% and 50% Retail Activation Frontages have been added. Retail Activation Frontages are illustrated on the Schedules for the Downtown Core Zones.
45.	4.1.5 Retail Activation 3.2.2.14 Retail and Commercial Frontage Design	LAP states: "Development incorporating at grade retail and commercial uses at the frontages, will be designed so that: c. storefronts are expressed with a narrow width ..."	<u>Add Sentences:</u> Maximum width of a unit - 10.0 m Maximum depth of a unit - 22.0 m Minimum height of a unit used for a Retail Activation use shall be 4.5 m	Provisions to limit the size of the retail and commercial units abutting/within the linear building frontage have been added. A minimum height of 4.5 m has also been added for Retail Activation uses.
46.	4.1.5 Retail Activation 3.2.2.14 Retail and Commercial Frontage Design	LAP states: "Development incorporating at grade retail and commercial uses at the frontages, will be designed so that: b. lobbies and principal entrances serving residential uses above the ground storey will be located on streets where the least amount of retail is required"	<u>Add Sentence:</u> access to uses permitted above and below the first storey of a residential building shall not be located on a street with a 90% Retail Activation Frontage;	A provision has been added to restrict the location of access to residential uses above the first storey.

ITEM #	MOP/LAP SECTION	MISSISSAUGA OFFICIAL PLAN (MOP) - POLICIES/ LOCAL AREA PLAN (LAP) - POLICIES	PROPOSED AMENDMENTS TO ZONING BY-LAW	COMMENTS/EXPLANATION
47.	4.1.5 Retail Activation 3.2.2.14 Retail and Commercial Frontage Design	LAP states: "Development incorporating at grade retail and commercial uses at the frontages, will be designed so that:	<u>Add Sentence:</u> where a residential building only has frontage on a street with a 90% Retail Activation Frontage, access to uses permitted above and below the first storey shall be permitted provided that the width of the access shall not be greater than 5.0 m;	A provision has been added to limit the size of the access to residential uses above the first storey where the only access is on a 90% Retail Activation Frontage.
48.	4.1.5 Retail Activation		<u>Add to Table 7.2.1:</u> <u>Holding Provision</u> (2) transfer to the City of a right-of-way to be used for a street on 'A' and 'B' Street Frontages identified on the Schedules of this Part of the By-law, where a street currently does not exist.	Since the Retail Activation policies depend on the street network identified on the Schedules, a condition of removing the "H" (Holding) Provision for lands fronting onto these streets has been added requiring the dedication of the street to the City.
49.	4.1.5 Retail Activation		<u>Add to Table 7.2.1, NOTES:</u> (7) See Subsection 7.1.4A of this By-law.	A note has been added to Table 7.2.1 to cross-reference the Retail Activation Frontages provisions.
50.	3.2.2.14 Retail and Commercial Frontage Design	LAP states: "Development incorporating at grade retail and commercial uses at the frontages, will be designed so that:" (to meet specific criteria)	<u>Revise Sentence 7.1.4.2.4 (Accessory Uses in CC2, CC3 and CC4) and delete former Article 7.1.4.4 :</u> A maximum of 20% of the total GFA non-residential may be used for Uses identified in Sentence 7.1.4.2.1 and 7.1.4.2.2 and shall not be permitted above the first storey.	The 20% limit on gross floor area for accessory uses in CC2 to CC4 Zones in the current Zoning By-law has been deleted; however the provision that the accessory uses must be located on the first storey has been retained.

ITEM #	MOP/LAP SECTION	MISSISSAUGA OFFICIAL PLAN (MOP) - POLICIES/ LOCAL AREA PLAN (LAP) - POLICIES	PROPOSED AMENDMENTS TO ZONING BY-LAW	COMMENTS/EXPLANATION
51.	4.1.6 Office	Lands north of Rathburn Road West from Centre View Drive to Confederation Pkwy. redesignated from "Downtown Core Commercial" (MOP) to "Office"	<p>Introduce "CCO - City Centre Office" Zone.</p> <p>Rezone lands from H-CC1 and H-CC2(1) to H-CCO on Z-Map 29.</p> <p>Add CCO Base Zone to Table 7.2.1.</p> <p>Add CCO to Lines 4.1 and 4.2 (Build-to Areas) in Table 7.2.1.</p> <p>Add CCO to the Introductory Sentence in the Holding Provisions in Table 7.2.1.</p>	A CCO (Office) zone has been introduced in conformity with the "Office" designation in the Downtown Core Local Area Plan. The lands north of Rathburn Road West between City Centre Drive and Confederation Pkwy. have been zoned "H-CCO". The CCO zone has been added to the Downtown Core Base Zone Table.
52.	4.1.6 Office	Comparison of Permitted Uses in LAP (s. 4.1.6.1) with MOP (s. 11.2.7) and MOP (s. 12.1.4)	<p>Permitted Uses in CCO in Table 7.2.1:</p> <ul style="list-style-type: none"> - Office - Medical Office - Hospital - University/College - Active Recreational Use - Passive Recreational Use 	The list of Permitted Uses in the CCO (Downtown Core - Office) zone is in conformity with the "Office" designation in the Downtown Core Local Area Plan.
53.	MOP 11.2.7 Office	MOP permits: "c. accessory uses"	<p><u>Add</u></p> <p>Article 7.1.4.3: Uses Accessory to a Permitted Use in the CCO Zone</p>	The General Provisions for the Downtown Core Zones have been amended to add accessory uses and provisions for the CCO Zone. The list of accessory uses and provisions is based on the provisions for CC2 to CC4 Zones, modified to be in conformity with the "Office" designation policies in the Downtown Core Local Area Plan.

ITEM #	MOP/LAP SECTION	MISSISSAUGA OFFICIAL PLAN (MOP) - POLICIES/ LOCAL AREA PLAN (LAP) - POLICIES	PROPOSED AMENDMENTS TO ZONING BY-LAW	COMMENTS/EXPLANATION
54.	MOP 11.2.1 Uses Permitted in all Designations (Office)	MOP permits: (a) community infrastructure (including community uses) (b) community gardening	Subsection 2.1.9 permits community uses in more than one (1) zone category. <u>Add the CCO zone</u> to the following uses: 2.1.9.4 - Day Care 2.1.9.5 - Essential Emergency Service 2.1.9.6 - Community Centre, Community Athletic Field, Public Walkway and/or Library 2.1.9.7 - Temporary Tent and/or Stage 2.1.9.8 - Community Garden 2.1.9.9 - Parking/Security Attendant Booth 2.1.9.10 - Transit Terminal and/or Transit Corridor	The community uses permitted in the CCO (Office) zone are in conformity with the Mississauga Official Plan policies, with limitations to maintain the emphasis on office and employment uses in the Rathburn District policies (s. 3.3.4) of the Downtown Core Local Area Plan.
55.	4.1.6 Office	Lands along Centre View Drive with "No Designation" - used for GO Transit parking. Designated to "Office" in LAP.	Rezone lands from "No Zone" to H-CCO south of Centre View Drive on Z-Map 29	The lands south of Centre View Drive have been rezoned from "No Zone" to H-CCO (Office) in conformity with the Downtown Core Local Area Plan.
56.	4.1.6.2 Office	LAP states: "Notwithstanding the Office policies of this Plan, the total Gross Floor Area of accessory uses will not be limited."	<u>Add Sentence 7.1.4.3.3:</u> Uses identified in Sentence 7.1.4.3.1 shall not be permitted above the first storey.	In the CCO (Office) zone, the gross floor area for accessory uses is not limited; however, a provision has been added to not permit accessory uses above the first storey.

ITEM #	MOP/LAP SECTION	MISSISSAUGA OFFICIAL PLAN (MOP) - POLICIES/ LOCAL AREA PLAN (LAP) - POLICIES	PROPOSED AMENDMENTS TO ZONING BY-LAW	COMMENTS/EXPLANATION
57.	Zone Boundaries		<u>Renumber Article 7.1.5.5:</u> to Subsection 7.1.7 - Zone Boundaries 7.1.7.1 - Where a zone boundary follows a proposed or existing street, the centreline of the street is the zone boundary.	The General Provision regarding Zone Boundaries in CC4 has been expanded to apply to CC1 to CC4, CCO and CCOS.
58.	Format for Zoning By-law		The greyed out text, identified in Items 1 to 57 inclusive of this By-law, is for information purposes only and does not form part of the amendments contained in this By-law.	Clarifies the format used to identify the amendments in the Zoning By-law for the Downtown Core.
Zoning Maps				
59.	Map 22	LAP designation: "Mixed Use"	Retain the "H-CC2-3" zone. (southwest corner of Burnhamthorpe Road West and Hurontario Street)	The H-CC2-3 zone has been retained and the wording has been modified to permit only the existing service station, in addition to the CC2 uses permitted on the site.
60.	Map 22	LAP designation: "Public Open Space"	Rezone the lands from "H-CC2(1) to "H-CCOS". (south of Burnhamthorpe Road West, west of Confederation Pkwy.)	The lands in the middle of the block north of Webb Drive have been rezoned to H-CCOS, in conformity with the Downtown Core Local Area Plan.
61.	Map 29	LAP designation: "Public Open Space"	Rezone the lands from "H-CC1" to "H-CCOS" (north of City Centre Drive, east of Duke of York Blvd.)	A block of land east of Duke of York Blvd. has been rezoned to H-CCOS in conformity with the Downtown Core Local Area Plan.

ITEM #	MOP/LAP SECTION	MISSISSAUGA OFFICIAL PLAN (MOP) - POLICIES/ LOCAL AREA PLAN (LAP) - POLICIES	PROPOSED AMENDMENTS TO ZONING BY-LAW	COMMENTS/EXPLANATION
62.	Map 29	LAP designation: "Public Open Space"	Rezone the lands from "CC2(1)" to "H-CCOS" (north of Prince of Wales Drive, south of Rathburn Road West, between Duke of York Blvd. and Living Arts Drive)	The lands in the middle of the blocks between Duke of York Blvd. and Living Arts Drive have been rezoned to H-CCOS in conformity with the Downtown Core Local Area Plan.
63.	Map 29	LAP designation: "Public Open Space"	Rezone the lands from "H-CC2(1)" to "H-CCOS" (block at the intersection of Rathburn Road West and Living Arts Drive)	The block of land to be created at the intersection of Rathburn Road West and Living Arts Drive has been zoned H-CCOS in conformity with the Downtown Core Local Area Plan.
64.	Map 29	LAP designation: "Office"	Rezone the lands from "H-CC1" and "H-CC2(1)" to "H-CCO" (north of Rathburn Road West from Centre View Drive to Confederation Pkwy.)	Lands north of Rathburn Road West have been designated "Office" in the Downtown Core Local Area Plan. These lands have been zoned H-CCO in conformity with the Local Area Plan.
65	Map 29	LAP designation: "Office"	Rezone the lands from "No Zone" to "H-CCO" (south of Centre View Drive from the easterly limit of Centre View Drive to Confederation Pkwy.)	The lands along the south side of Centre View Drive are being designated "Office" in the Downtown Core Local Area Plan. They are currently shown as part of the road allowance and are not designated. These lands are being zoned H-CCO in conformity with the Local Area Plan.

ITEM #	MOP/LAP SECTION	MISSISSAUGA OFFICIAL PLAN (MOP) - POLICIES/ LOCAL AREA PLAN (LAP) - POLICIES	PROPOSED AMENDMENTS TO ZONING BY-LAW	COMMENTS/EXPLANATION
66.	Map 29	LAP designation: "Greenbelt - Natural Hazards"	Rezone the lands from "No Zone" to "G1" (southwest of Hwy. 403 and Hurontario Street)	The lands north of Rathburn Road West between Hurontario Street and Centre View Drive are being designated "Greenbelt" in the Downtown Core Local Area Plan. They are currently shown as part of the Hwy. 403 road allowance with no land use designation. These lands are being zoned G1 in conformity with the Local Area Plan.

October 12, 2012

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DRAFT

OCTOBER 12, 2012



DOWNTOWN CORE BUILT FORM STANDARDS

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INTRODUCTION

1.0 Purpose

The purpose of the Downtown Core Built Form Standards is to provide urban design direction and guidance for proposed development at the planning application stage in order to assess, promote and fulfill the intent of the City's official plan policies, Downtown21 vision and zoning by-law - all of which shape and have influence on the urban structure, built form qualities and overall character of the Downtown Core.

Selected content from this document, has been incorporated into the Local Area Plan as Official Plan policy. Applicants should also refer to the City's Official Plan principal document, Downtown Core Local Area Plan, Zoning By-law, and Building Code to ensure that the applicable policies and requirements in these documents have been met. Furthermore, other City initiatives and special projects should be consulted to determine applicability, such as the Strategic Plan, Downtown21 Master Plan, Green Development Strategy, Hurontario Light Rail Transit Study, Accessibility Design Handbook and the City of Mississauga Standards for Shadow Studies, etc.

2.0 Objectives

'Build a Desirable Urban Form' (Section 9) is a fundamental component of the Mississauga Official Plan. This section along with the policies contained within Section 12 'Downtown' and 'Downtown Local Area Plan' set out the urban design policies which supports the vision for a vibrant Downtown Core. These built form standards act to articulate the policies in addition to the following objectives:

- Support the City of Mississauga's Strategic Plan and its Strategic Pillars for Change - a paramount goal is to *Create a Vibrant Downtown* that functions as a strong economic centre, while acting as the civic heart and soul of the city;
- Promote development that supports and implements the six guiding principles of the Downtown21 Master Plan; specifically, #6 - 'Create a Development Framework with Predictability' which recommends creating a policy framework that directs downtown development in a coordinated, comprehensive fashion through new urban design standards;
- Facilitate the fair and consistent application of design objectives;
- Accommodate a mix of uses, through appropriate built form, including retail and commercial uses, offices, residential, cultural, entertainment and institutional uses — the whole of which is intended to put people in close proximity to a broad range of urban amenities and experiences;
- Achieve a high-quality built form and strengthen the continuity of buildings that contribute to the emerging Downtown Core urban context;
- Ensure that development is environmentally friendly, resilient, safe and universally accessible; and
- Foster compact, pedestrian and transit-oriented development that achieves vibrant street level activity and a public realm of the highest standard.



*The 20th Century was about getting around...
The 21st Century will be about staying in a
place worth staying in ...*

~ JAMES HOWARD KUNSTLER

INTRODUCTION

3.0 Expectation of the Standards

The Built Form Standards act to provide further direction on the Urban Design Policies set out in the Official Plan, Downtown21 Master Plan and other city initiatives to support land use decisions and strategies for the Downtown Core.

The Standards establish detailed requirements to achieve a high-quality built form that interfaces with the public realm in a seamless fashion, with the objective of creating a pedestrian environment in the Downtown Core that is memorable and rich in character.

Designers, landowners and developers are expected to address and achieve the Standards. The Standards have

been developed to communicate the design expectations, in advance of an application being filed, related to the quality and outcome of development in the Downtown Core.

It should be noted that these may be amended, modified or updated on an as-needed basis to provide clarity on the intent of the Downtown21 Master Plan, the Downtown Core Local Area Plan, provisions of the Zoning By-law and other studies or initiatives that have relevance to the Downtown Core.



3.1 How to Read the Standards

The rationale of the Downtown Core Built Form Standards is best understood by reviewing all sections, text and diagrams, including the policies cited within the City's Official Plan (Downtown Core Local Area Plan). The Built Form Standards reflect an integrated approach to the build-out of the Downtown Core, in which buildings are keyed to the streets through street frontage standards that guide and provide direction on the general disposition of buildings through a form-based approach.

Moreover, the Standards are also contingent on an understanding that downtowns are complex urban places that require an overarching organizing structure with a view to creating a coherent, legible, high quality public realm and memorable sense of place.

The Standards generally cover the following:

- Design for Street Frontage
- Access, Loading and Servicing
- Design for Frontage around Open Space
- Integration of Parking Facilities
- Design for Retail & Commercial Uses
- Minimum Building Heights
- General Built Form Standards
- Tall Buildings
- Transition to Adjacent Development



FRONTAGE STANDARDS

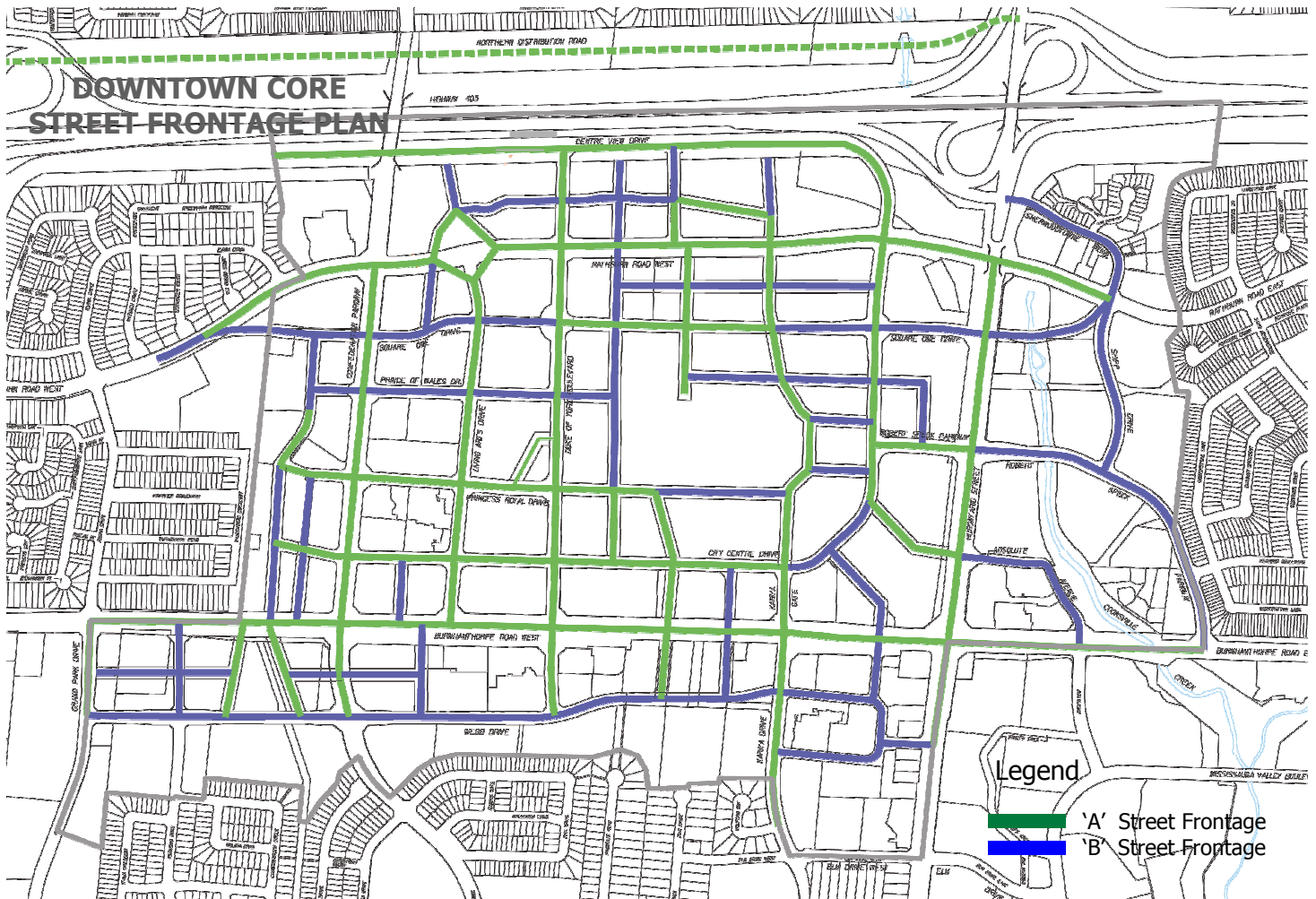


Figure 1: The Downtown Core Street Frontage Plan designates the frontage type for all existing and new streets in the Downtown Core. There are two categories - 'A' & 'B' Frontages. Refer to Downtown Core Local Area Plan policies and schedule.

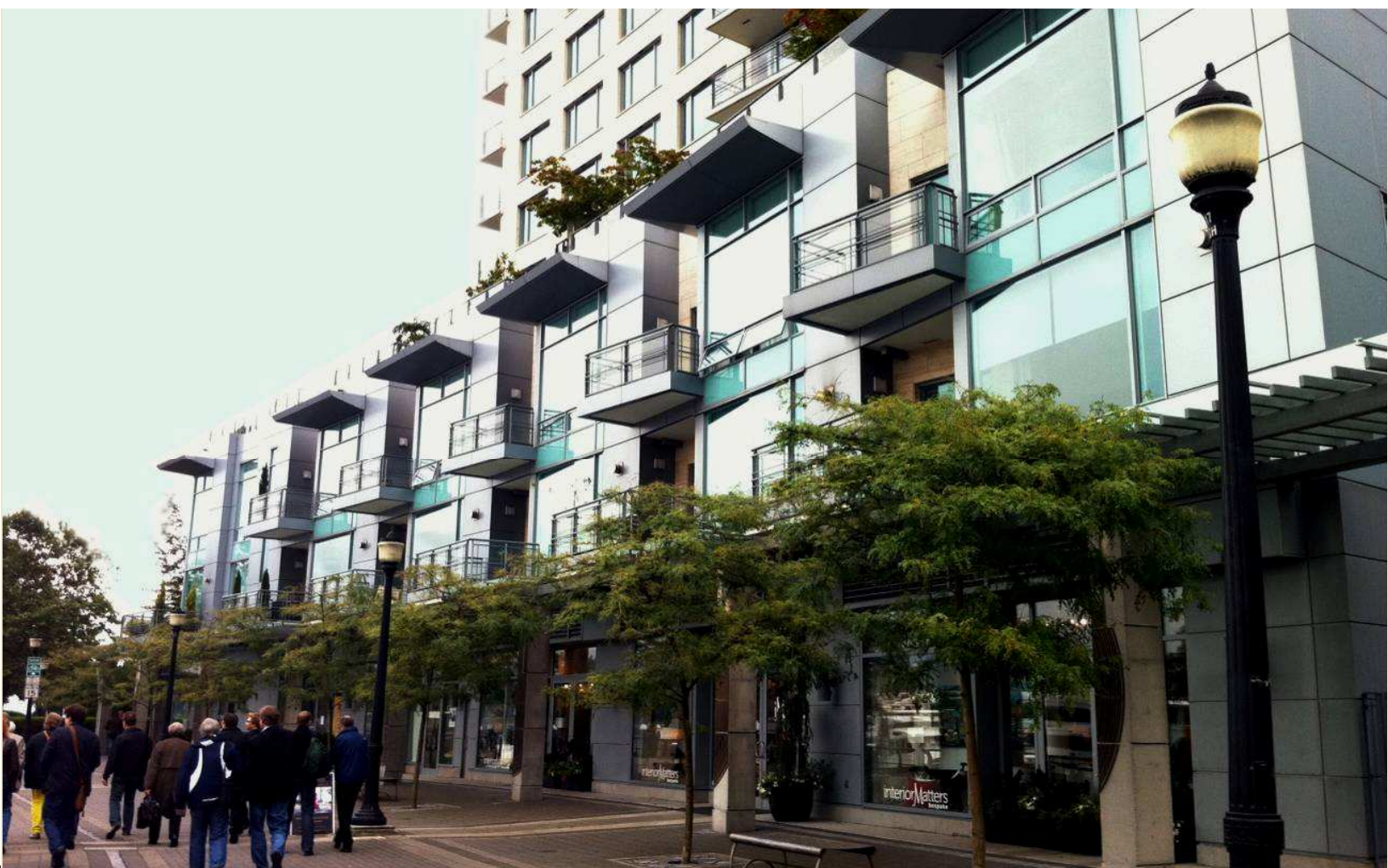
4.0 Design for 'A' & 'B' Street Frontages

The City of Mississauga's Downtown Core vision (Downtown21 Master Plan) proposes new public streets to augment the existing public and private street pattern, thereby creating a comprehensive and interconnected urban grid. This framework is shown in the Downtown Core Street Frontage Plan above (see Figure 1).

Categories of frontages differentiate the various streets in the Downtown Core according to their function, character and design. As such, all streets are categorized as 'A' and/or 'B' frontages. 'A' Street Frontages will require the highest attention to urban design, having a cohesive built form to achieve character and a vibrant pedestrian environment.

Similarly, 'B' Street Frontages are designed to ensure a quality pedestrian environment and high standard of built form, but provide defined locations for necessary access, delivery, service, loading and parking facilities serving development blocks.

In general, new development will follow specified standards for each street frontage type which prescribes how buildings (through their site design, streetwalls, built form treatments, etc.) individually contain and provide visual enclosure of the street in order to collectively frame and animate the public realm.



For each type of street frontage ('A' and/or 'B') the standards outline specific design requirements for new buildings such as:

- **Building placement along frontages**
- **Articulation and set backs of the streetwall**
- **Location of building entries**
- **At grade conditions and treatments**
- **Vehicular access, loading and servicing**
- **Interface with Parks and Open Space**
- **Parking facilities, etc.**

The following sections set out the standards to be achieved for buildings along the frontage types in accordance with the Downtown Core Local Area Plan policies and the Downtown Core Street Frontage Plan (Figure 1):

FRONTAGE STANDARDS (cont'd)

5.0 General Standards for Buildings on 'A' & 'B' Street Frontages

'A' and 'B' Streets provide the overarching framework for the design of building frontages, informing how development creates the most attractive and vibrant streets, or those which can accommodate loading, servicing and vehicular access throughout the Downtown Core. Notwithstanding, there are common standards that apply to both 'A' and 'B' frontages that are fundamentally critical to guiding site organizational aspects and setting the Downtown Core context:

The following standards will apply:

- G1** Locate build-to-line requirements on development blocks to inform the orientation and placement of buildings and their streetwalls;
- G2** Coordinate build-to lines with adjacent properties in order to create consistent edges and street walls along frontages;
- G3** Locate buildings parallel to the street at the build to line to contain the street and provide enclosure;
- G4** Ensure a variation in set backs along the building frontages to articulate façade emphasis at the build-to-line, in order to allow for visual interest, accommodate outdoor patios, recessed entries and landscaped areas (*refer to Section 5.1 Specific Standards for 'A' Streets and Section 5.2 Specific Standards for 'B' Streets*);
- G5** Buildings should incorporate active uses at grade, such as commercial and retail, to animate the public realm and pedestrian environment (*Also, refer to "Retail Activation Schedule, Downtown Core Local Area Plan and Section 7.0 for Design of Retail and Commercial Street Frontages*);



Figure 2: Buildings are located parallel to street to provide enclosure and definition of the street space.

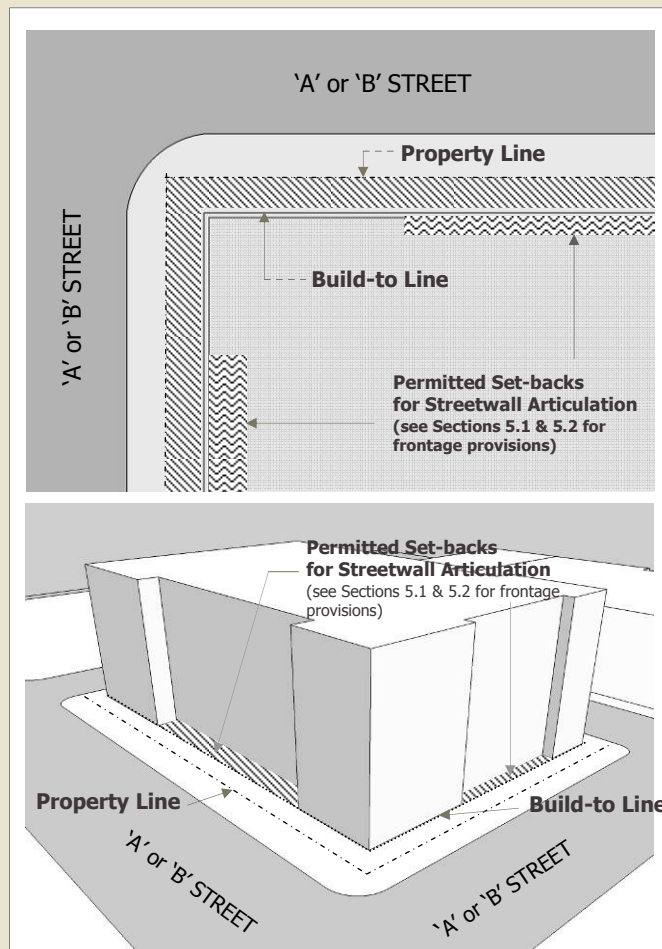


Figure 3: Development is located parallel to the street and placed at established build-to lines. Depending on the frontage type, the streetwall can stepback to accommodate façade articulation, in addition to patios and entrances.

- G6** Ground floor uses will incorporate transparent windows;
- G7** Locate principle entrances flush with the public sidewalk;
- G8** Ensure site designs relate to and interface with existing, proposed and future transit stops and facilities;
- G9** Locate principal building entrances so that they are clearly identifiable and prominent with direct access to the public sidewalk, pedestrian connections and transit facilities;

G10 Balconies may not protrude into the public realm, but may extend as far as the build-to line;

G11 Balconies will be designed with high quality materials, including upgraded balustrades and railings; and

G12 Below grade parking structures may not protrude into the public realm, but may extend as far as the property line provided that a clearance of 1.5 m is provided between the top of the parking structure slab and sidewalk.

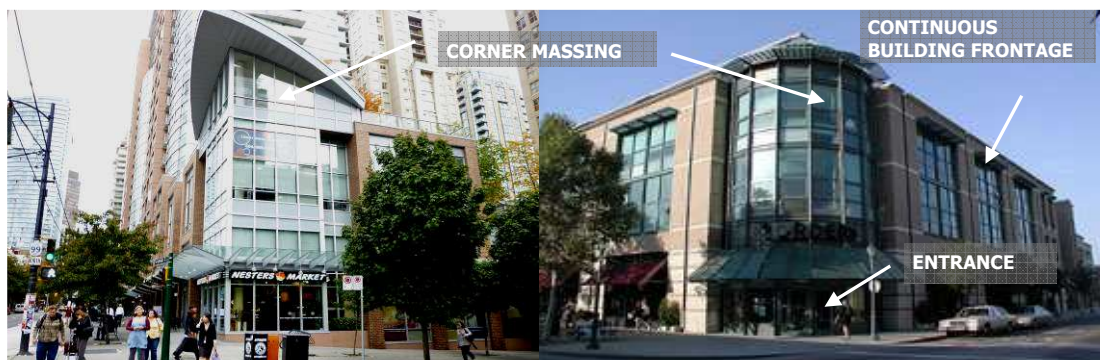


Figure 4: The street emerges as an urban place when buildings collectively deploy streetwalls, treatments to reinforce corners and at grade conditions that animate the edges.



Figure 5: Clear points of entry and flush entrances to commercial and retail uses create an extension of the pedestrian environment from the public sidewalk.

FRONTAGE STANDARDS (cont'd)

5.1 Specific Standards for Buildings on 'A' Street Frontages

'A' Frontages are the most important for securing animation and character, street activity and vibrancy. They are intended to be the most pedestrian focussed, ensuring comfort to achieve a quality public realm that is attractive and cohesive, thereby contributing to a lively and robust Downtown Core character.

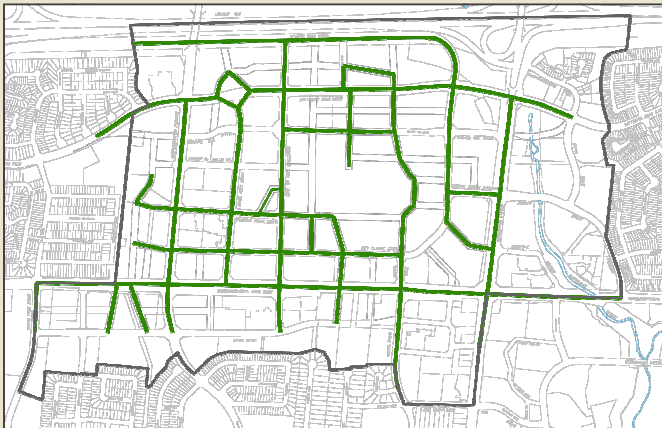


Figure 6: — = 'A' Street Frontages in the Downtown Core.

The following standards will apply:

- A1** Provide continuous building frontages on 'A' streets for the length of development blocks to provide continuity of built form from one property to the next;
- A2** A minimum of 95% of a property's frontage is required to be occupied by the streetwall at the build-to line (*Refer to Section 9.0 for Minimum Building Height*);
- A3** Curb cuts, driveways and access are prohibited on 'A'-Street frontages, except where a site or block does not have access from a 'B' Street or other means of access, or if there is a requirement for emergency vehicle access (*Refer to '5.2 Specific Standards for Buildings on 'B' Street Frontages*);

- A4** To allow for articulation of the streetwall, including provision for outdoor patios, recessed entries and landscaped areas, a maximum of 25% (i.e. 25% of 95%) of the building frontage will be allowed to step back to a maximum of 4.5 m. from the build-to line;
- A5** Functioning principal entrances to buildings will be provided on 'A' Streets. (*Also, see Section 7.0 Design for Retail & Commercial Frontages*);
- A6** Ground floor elevations along 'A' frontages will have a minimum of 75% transparent vision glazing with views into the building; and
- A7** Where residential uses are permitted at the ground level, special provisions will apply for the design of unit entrances and setbacks (*see Section 11.9 Design for at grade Residential Uses*).



Figure 7: Continuous streetwalls on 'A' frontages are required along the length of a property with provision for articulation and step-back of the street-wall.

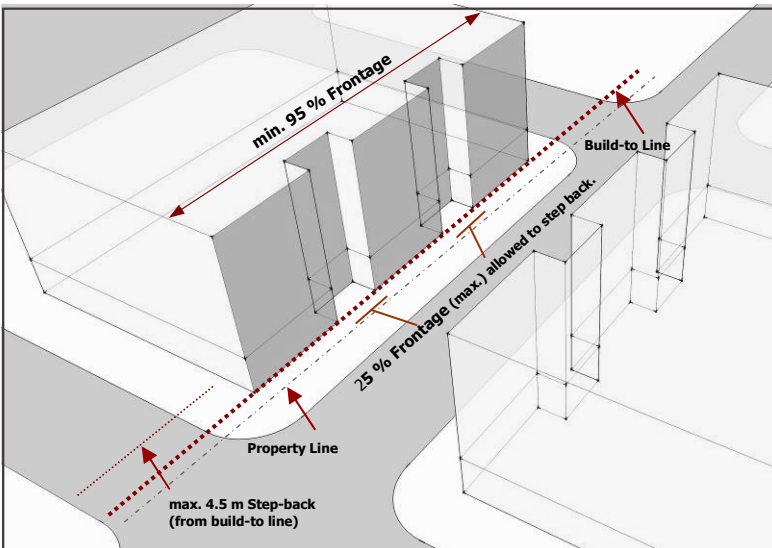


Figure 8: Illustration of a building streetwall positioned on 'A' Frontages



Figure 9: Continuous streetwalls on 'A' frontages are required along the length of a property with provision for articulation and step-back of the streetwall.



Figure 10: Buildings follow the streetline and incorporate entrances at regular intervals with access to at grade uses.



Figure 11: Articulation of the facade along the build-to line provides visual interest at the street level.



Figure 12: Step backs at the street wall provide visual interest and reinforce a strong pedestrian scale.



Figure 13: Principal entrances are clearly identifiable and articulated at the street frontage.

5.2 Specific Standards for Buildings on 'B' Street Frontages

'B' Streets generally connect 'A' Streets to each other. In contrast to 'A' Streets, they provide development blocks with access for deliveries, garbage pick-up, service and loading, including vehicular access to structured and off-street parking within development sites. It should be noted that 'B' Streets are also intended to support a pedestrian environment, integrating a high standard of urban design to support street activity.

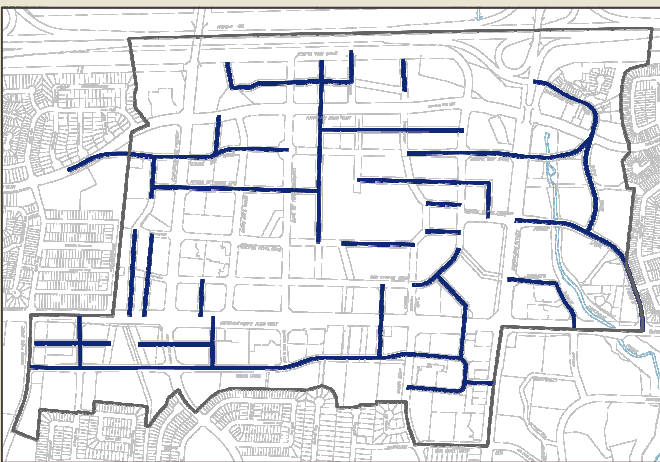


Figure 14: — = 'B' Street Frontages in the Downtown Core.

The following standards will apply:

- B1** 'B' Streets will have continuous building frontages along the length of development blocks to provide a continuity of built form from one property to the next with minimal gaps between buildings;
- B2** A minimum of 75% of a property's frontage is required to be occupied by the streetwall at the build-to line;
- B3** To allow for articulation of the streetwall, including provision for outdoor patios, recessed entries and landscaped areas, a maximum of 15% (i.e. 15% of 75%) of the building frontage will be allowed to step back to a maximum of 3.0 m. from the build-to line;

- B4** Locate and organize vehicular parking and access, service areas and utilities such that they minimize adverse impacts on the property, on surrounding properties, and improve the safety and attractiveness of adjacent streets, parks and open spaces (*Refer to Section 5.3 Design for Service and Loading*);
- B5** Locate functioning principle building entrances to buildings on 'B' frontages except where a development block has 'A' and 'B' frontages, the prominent building entrance will be located on 'A' frontages (*also see Section 5.1 Specific Standards for 'A' Frontages and 7.0 Design of Retail & Commercial Frontages*); and
- B6** Ground floor elevations along 'B' frontages will have a minimum of 50% transparent vision glazing with views into the building.



Figure 15: Access to parking and services is discretely provided within these developments near the street edge, acknowledging the importance of the pedestrian realm.

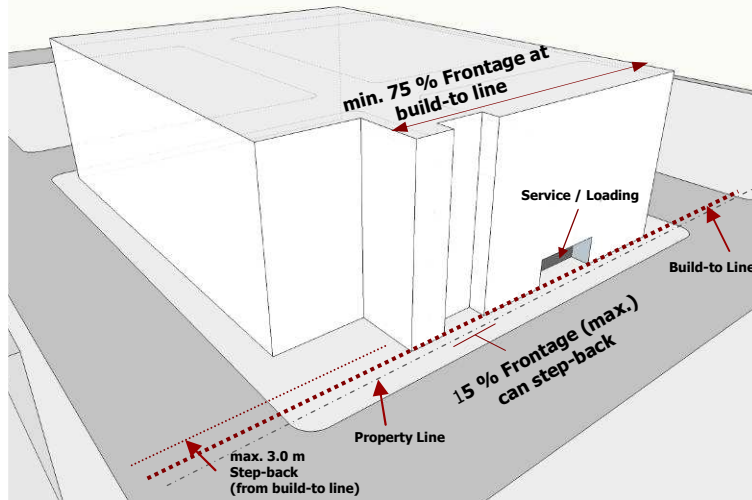


Figure 16: Illustration of a building streetwall positioned on 'B' frontage.



Figure 18: Access to below grade parking is discretely executed at grade without compromise to the pedestrian environment or manner in which the building relates to the street.

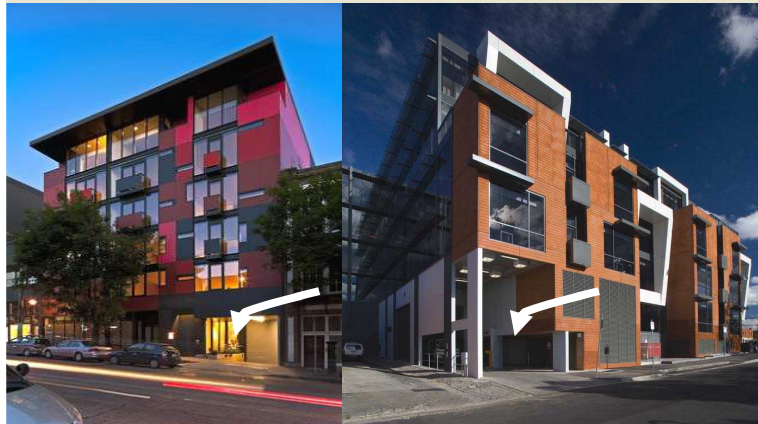


Figure 17: 'B' frontages in the Downtown Core allow for service , loading and access, but are also envisioned to be pedestrian oriented with an attractive character.



Figure 19: Functioning principal entrances can be accommodated on 'B'-Streets subject to the Frontage Standards.

5.3 Design for Access, Parking, Service and Loading

Access for loading, garbage, parking and servicing to buildings will be located on 'B' Streets (*for exceptions, refer to Section 5.1, 'Specific Standards for Buildings on 'A' Frontages, STD. A3*). In general, new development will coordinate the location of service areas for delivery, parking, loading and garbage pick-up and will reduce vehicular interruptions along the public street in order to improve the aesthetic appeal of the streetscape.

The following standards will apply:

- SL1** Service, parking and loading should be coordinated on sites by providing consolidated locations that can serve a number of buildings simultaneously from one area;
- SL2** Loading, garbage and service spaces will be located internal to the building to avoid noise and visual impacts;
- SL3** Loading, servicing and other vehicular related functions should not detract from the use, safety or attractiveness of the pedestrian realm;
- SL4** The height required for overhead loading for bulk refuse within a collection area should conform to the Region on Peel's standards for overhead clearance;
- SL6** Negative impacts on the public realm are to be avoided by incorporating special architectural treatment around service areas and providing safe levels of illumination and lighting; and
- SL7** Use landscape treatments to screen loading, garbage pick-up and service areas, where designs permit.



6.0 Street Frontage Design for Parks and Open Space

New buildings which front onto, or have proximity to parks and open space will require special attention to their frontage treatments and architectural design in order to achieve the kind of character, sense of place and pedestrian experience warranted for these important elements of the public realm.

The following standards will apply:

- OS1** If the build-to line of a development fronts onto the edge, or street adjoining a public park or open space, then 'A' frontage requirements will apply (*refer to Section 5.1 'Specific Standards for Buildings on 'A' Frontages'*);
- OS2** Building frontages along parks and open spaces will have uses on the ground floor to animate and activate the frontage such as restaurant and retail uses;
- OS3** Buildings which surround, have proximity, or front-onto parks and open space will have the highest level of architectural expression, articulation and use of materials;
- OS4** At grade level residential uses fronting onto parks and open space will have individual unit entrances in order to activate the street environment (*refer to Section 11.9 Design for At grade Residential Uses*);
- OS5** Parking structures or surface parking lots will not be permitted to front onto or address parks and open spaces; and
- OS6** The design of building massing will protect for maximum sun exposure onto parks and open space. (*Refer to Section 11.5 & 11.6 Tall Buildings - 'Site and Park Orientation'*).

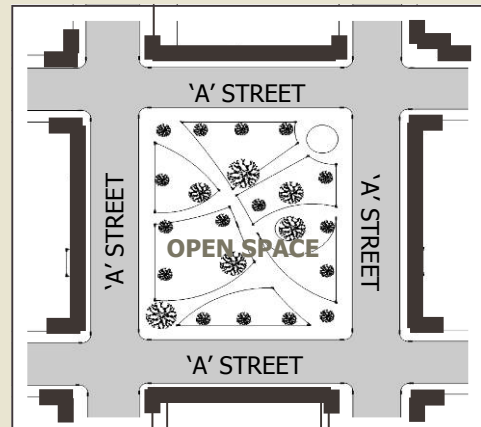


Figure 21: The facades of buildings that address open space will be treated like 'A' frontages, incorporating the highest level of architecture, articulation and use of materials.



7.0 Design for Retail & Commercial Street Frontage (at grade)

To ensure vibrant, great streets in the Downtown Core, at grade retail and commercial uses must be carefully designed to animate the street edge and support the pedestrian experience.

The following standards will apply:

- RC1** Ground floor heights will be a minimum of 4.5 metres (floor-to-floor, measured from established grade) to accommodate retail and commercial uses with windows that correspond to the height of ground floors;
- RC2** At grade principal entrances and lobbies that serve residential uses above the ground storey will be limited in size to a width of 5 m in order to allow for maximum activation of the street;
- RC3** Where areas of retail focus have been strategically identified in the Downtown Core, lobbies and principal entrances serving residential uses above the ground storey will be located on streets where the least amount of retail is required, or where no amount of retail frontage is specified (*refer to Retail Activation Schedule, Downtown Core Local Area Plan*).

- RC4** The primary entrance to each street-level tenant space that has its frontage along a public street will be provided from that street;
- RC5** Entrances to retail and commercial tenant spaces will be operational, flush and directly related to the grade of the public sidewalk;
- RC6** Provide narrow storefronts to a maximum of 10 m in order to discourage large single use retail frontages, achieve visual interest and a frequent rhythm that supports the pedestrian experience at the street level;
- RC7** Design the spacing of functioning at grade retail and commercial entrances along the length of the building facade to a maximum of 10 m;
- RC8** Ensure that storefront openings and structural elements (such as columns and pilasters) between storefront bays are articulated using reveals and subtle recesses that repeat across retail frontages to create interest for the pedestrian;
- RC9** Where Retail Streets intersect other streets, the ground floor retail space should wrap the corner onto the intersecting streets;



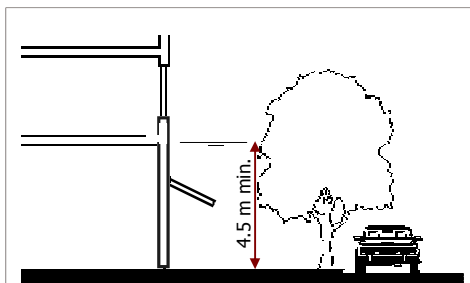


Figure 22: The ground floor of retail and commercial uses will have a minimum height of 4.5 m.



Figure 23: Storefronts when well executed promote a vibrant street life and the pedestrian experience.



7.1 Design for Storefronts

At grade retail and commercial uses should incorporate the highest standard of storefront design consisting of:

- RC8** Durable, high quality materials such as wood, metal (steel and anodized aluminium, etc), glass, natural stone and brick;
- RC9** Clear transparent windows and doors that allow for views into and out of storefronts. It should be noted that dark tinted, reflective or opaque glazing or 'appliqué lifestyle' panels placed on storefront windows are not permitted in the Downtown Core;
- RC10** Elements such as mullions, glazing bars and transoms to help frame, divide and define storefront window sections and apertures;
- RC11** When part of a larger single development or individual tenancy, provide coordinated and consistent signage and lighting that integrates with the storefront design, and which compliments or acts as an extension of the buildings architectural character;

RC12 The incorporation of architectural cantilevers, fixed canopies, awnings or similar features into a proposed development will be provided in order to achieve pedestrian scale, comfort and weather sheltered pedestrian routes. These should compliment the architectural character of the building and be made of highly durable materials, extending a min. of 1.5 m to a max. of 2.75 m, protrusion beyond the retail edge and/or into the public sidewalk area (refer to Figure 25); however, such installations should not conflict or interfere with any streetscaping elements or public realm treatments;

RC13 Where appropriate, patios and outdoor amenity space should be provided adjacent to retail/commercial frontages or entrances to promote activation of the street and vibrancy; and

RC14 Patios should have a depth of at least 2.5 m. and can be recessed into the building as part of the permitted setback at the build-to frontage, or placed beyond the build-to line along the spill-out zone providing that there is sufficient space. A clear sidewalk width is required to ensure pedestrian traffic flow is not disrupted.

FRONTAGE STANDARDS (cont'd)

DESIGN for STOREFRONTS

Buildings incorporating at grade commercial and/or retail uses will be designed to ensure that such uses will be directly related to the public sidewalk and will incorporate operating entrances, doorways and storefront windows oriented to the street; along with signage and lighting to provide animation, interest and variety in the streetscape;

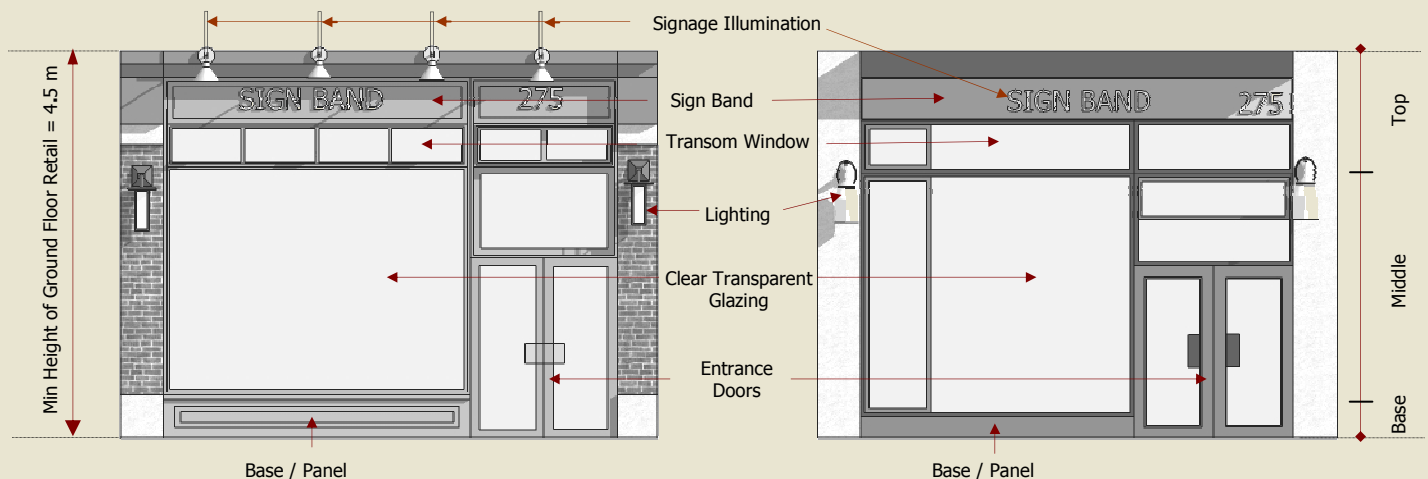
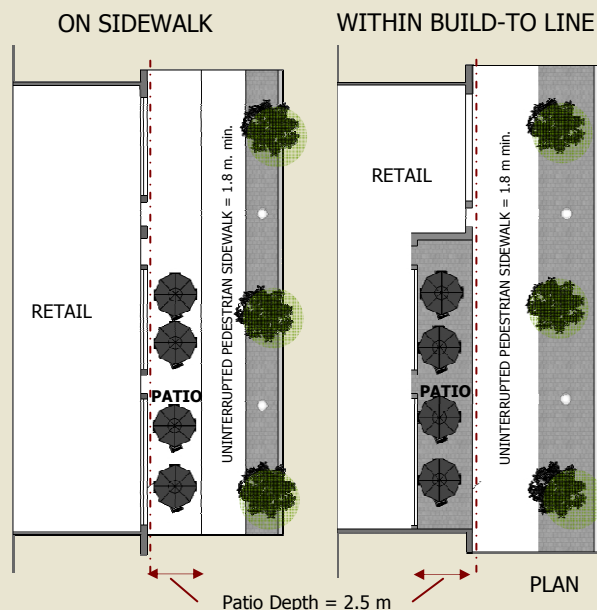
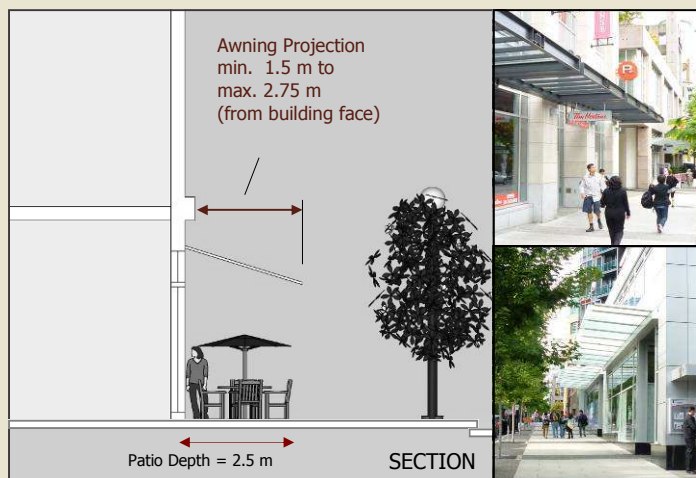


Figure 24: Storefronts can incorporate a variety of styles and character to animate the street edge. When well-executed, they contribute to the identity of the street and make retail and commercial frontages inviting and accessible to pedestrians.

DESIGN for OUTSIDE PATIO AREAS & PEDESTRIAN COMFORT



Patios can be recessed into the building as part of the permitted setback at the build-to frontage, or placed beyond the build-to line along the 'shy zone' providing that there is sufficient space and where pedestrian traffic flow is not disrupted.



Developments will incorporate architectural cantilevers, fixed canopies, awnings or similar features in order to achieve pedestrian scale, comfort and weather sheltered pedestrian routes. These should complement the architectural character of the building and be made of highly durable materials.



Patios and amenity space should be provided adjacent to retail /commercial entrances to promote activation and street vibrancy.



Figure 25: Design elements such as awnings and patios extend from storefronts to activate the street and create the urban experience for pedestrians.

8.0 Frontage Design for Structured Parking Facilities

Parking facilities will have an important role to play in supporting key uses, attractions and urban amenities in the Downtown Core. In general, development will locate structured parking and vehicular access to minimize impacts on the property and on surrounding properties and to improve the safety and attractiveness of adjacent streets, parks and open spaces.

The following standards will apply:

- P1** Parking structures will not directly front onto 'A' Streets, but will be screened by 'liner' buildings incorporating a mix of uses between the parking structure and street space in accordance with Section 5.0 – 5.1 for 'A' Frontages;
- P2** On the ground floor, parking structures will have active uses such as commercial or retail services with an appropriate scale and architectural expression to support activity on the streets, including those fronting onto parks and/or open spaces (*refer to Section 7.0 . Design for Retail and Commercial Uses*);
- P3** For a given development block, parking structures will only directly front onto one 'B' Street where more than one 'B' street frontage exists;
- P4** Parking structures and their facades above the ground floor will be designed to the highest level of architectural treatment and façade animation to mask the parking and screen views of the interior;
- P5** Entrances, lobbies and passageways that provide a convenient means of access to parking facilities will be:
 - located on streets where the least amount of retail is required, or where no retail frontage requirement is specified in accordance with the Retail Activation Schedule; and
 - fully enclosed, appropriately signed and integrated into the façade design without appearing as dominant elements on the street frontages;

- P6** At grade exhaust vents serving structured parking facilities will not be permitted to front onto 'A' streets, but may be located on 'B' Streets provided that they are architecturally treated and vertically integrated into a building's exterior wall and cladding system; and
- P7** Consult the PEEL CPTED (Crime Prevention Through Environmental Design) Guidelines for the Design of Parking Garages.

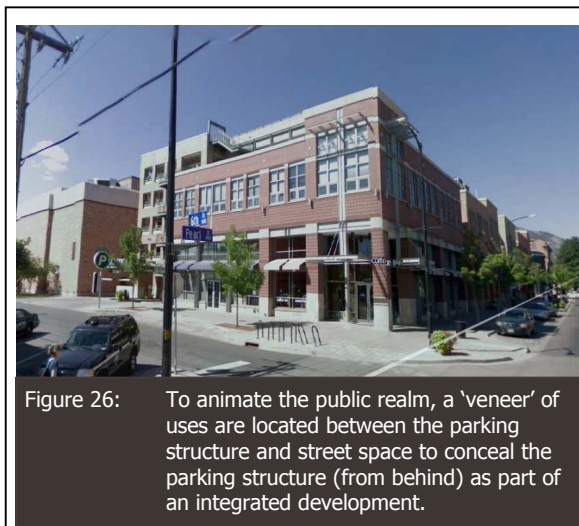
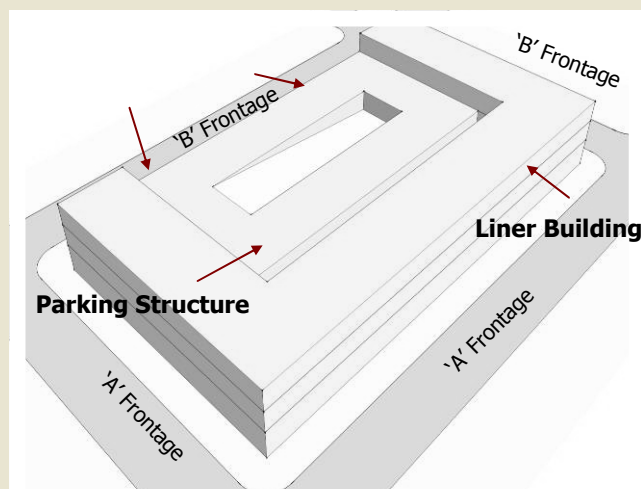




Figure 27: Facades that are articulated with architectural treatment to resemble authentic buildings, and/or more animated approaches are used to mask structured parking (applicable to 'B' Streets).

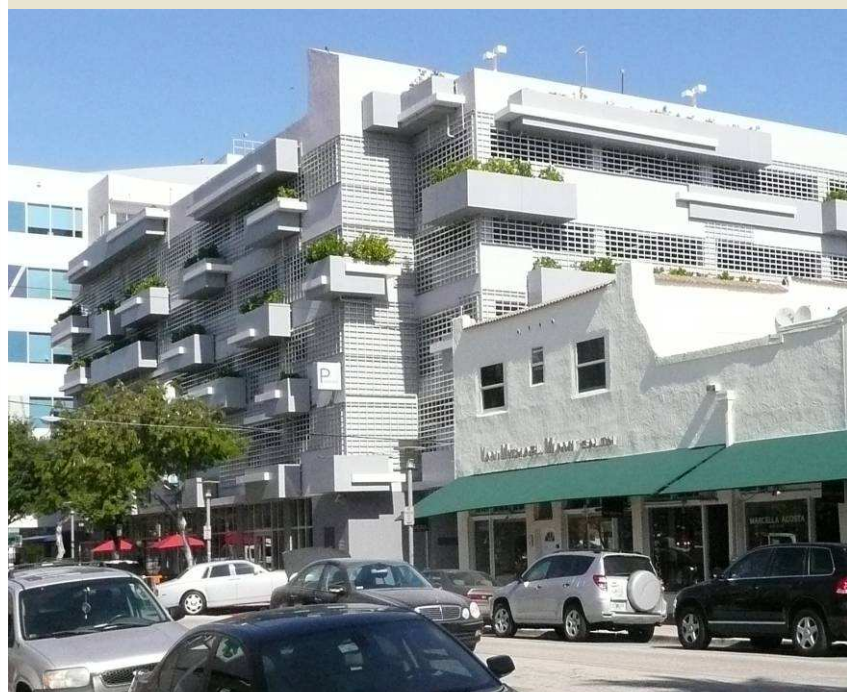


Figure 28 A typical condition for a 'B' frontage in which at grade retail is incorporated at the base and the structured parking above is enclosed by façade treatments to screen the stalls and interior parking areas.

9.0 Minimum Building Height

Mississauga's vision to urbanize the Downtown Core is intended to provide, a compact, mixed-use urban form that puts people in close proximity to jobs, transit, and a broad range of uses and urban amenities. To achieve this, a minimum building height of three storeys needs to be deployed to ensure the most efficient use of land and to create a pedestrian environment through street walls that contain the street and supports transit. In a downtown environment, one-storey temporary format buildings, such as big-box retail or entertainment facilities, served by surface parking, are examples of building typologies which are inefficient, suburban and do not lend themselves to creating a pedestrian friendly environment.

The following standards will apply:

BH1 All new buildings in the Downtown Core must achieve a minimum height of 10.7 metres (3 Storeys) both at the street frontage and across the entire area of the building.

N.B. Applicants should also refer to 'A' and 'B' frontage standards concerning build-to requirements, building placement, step-backs, etc.

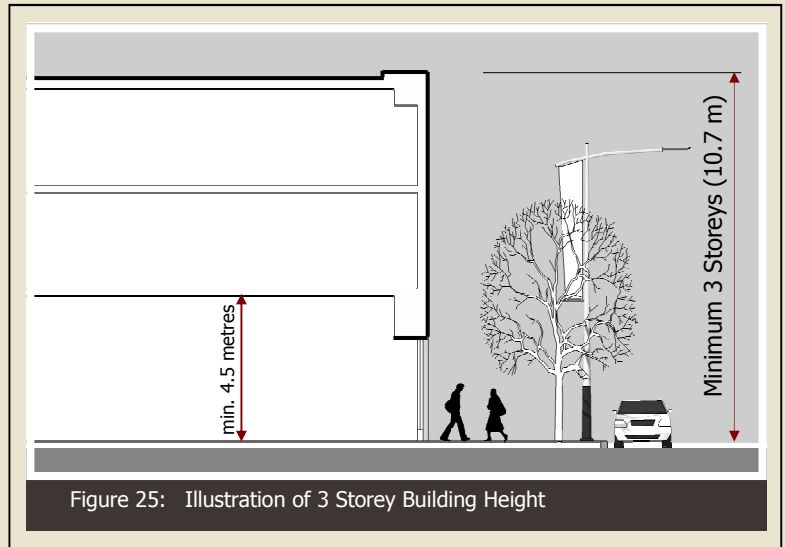


Figure 25: Illustration of 3 Storey Building Height



Figure 29: A 3 Storey mixed-use building with active ground floor retail/commercial uses contains the street and provides a comfortable pedestrian scale.

BUILDING HEIGHT & BUILT FORM



Figure 30: Buildings deploy a tri-partite configurations (base, middle and top), including horizontal and vertical expression lines to support and reinforce pedestrian scale aspects and an expression well suited to urban environments.

10.0 General Built Form Standards

In addition to achieving the minimum building height, development in the Downtown Core will achieve the following standards:

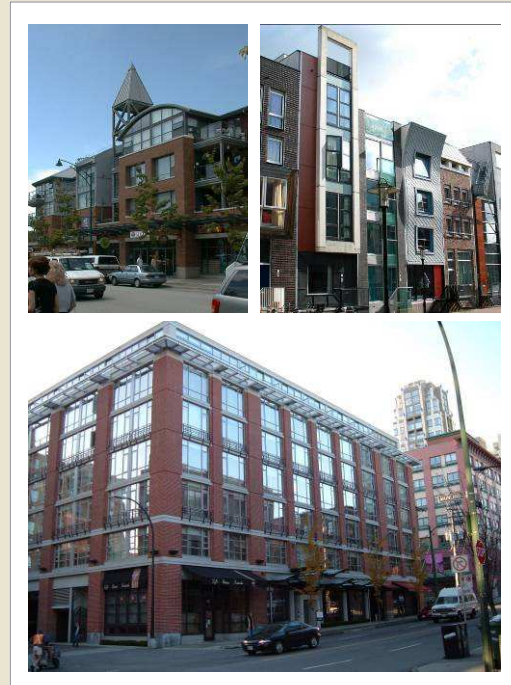
- BF1** Mitigate the perception of large buildings and long frontages by:
- ensuring buildings and their streetwalls are well proportioned to address human scale;
 - incorporating variation in the overall design of the streetwall;
 - using special massing to articulate the built form; and
 - providing a change in materials, textures, patterns, colours and details on building frontages to create a sense of smaller scale buildings;
- BF2** Ensure that the design of streetwalls in mixed-use buildings deploy a:
- **Base** - to strongly define the at grade level;
 - **Middle** - to define the upper storeys; and
 - **Top** - to express the termination of the streetwall at the roof-line, parapet, related floor juncture or storey, in a way which reinforces the architectural character or style of the building;
 - Accentuating floor and/or ceiling junctures and transitions between subsequent storeys by using horizontal expression lines, or visual elements such as belt-courses, cornices, banding(s) or architectural treatments on the streetwall.
- BF3** Establish a rhythm with frequency and articulation across the streetwall by deploying elements that are vertically oriented and human scaled to include:
- all forms of fenestration, including transparent windows, apertures, bow, bay, storefronts, dormer and monitor-style windows; and
 - pilasters, engaged columns, recesses, reveals, expression lines or other architectural treatments to create interest for the pedestrian and which serve to break up elongated facades.
- BF4** Consider incorporating a balance in the proportion of glazing to solid wall materials in the design of facades in order to achieve pleasing symmetries and legible asymmetries.

BUILDING HEIGHT & BUILT FORM (cont'd)

- BF7** Design corner lot buildings with special massing and architectural treatments on both streets to give prominence along the frontages and visually distinguish these sites;
- BF8** Express principal building entrances with a height, proportion and level of articulation that is compatible with the ground floor storey height, so that entrances are visible and serve as focal points for orientation and access from the street (*also, see Section 5.0 - 5.2, Standards for Street Frontage*);
- BF9** Incorporate upgraded doors for entrances, including egress and fire exit doors fronting onto streets;
- BF10** Design balconies to be recessed and/or architecturally integrated into the design of the building façade;
- BF11** Conceal and screen roof equipment to be designed as an extension of the building with materials and design treatments compatible with the building as a whole;
- BF12** Deploy a hierarchy of materials with solid, or 'heavier' materials located closer to the ground as a means to visually anchor the building to grade;



Figure 31: A strong vertical emphasis in the façade of this development creates rhythm and definition at the street edge.



- BF13** Construct building exteriors from enduring, natural materials such as clay brick, stone and wood in order to create durable buildings which can be adaptively reused over time;
- BF14** Building materials such as stucco, metal siding, embossed face brick panels, vinyl, and EIFS (Exterior Insulation Finish Systems) are not to be used in the Downtown Core;
- BF15** Consider using building materials that are energy efficient and/or those that have been re-used or recycled (*Please consult the City of Mississauga's Green Development Standards for sustainable approaches and further direction*);
- BF16** Building signage will be visually integrated with the development as sign-bands or fascia signs, and as an extension of the building's architectural expression; and
- BF17** Except for wayfinding signage and commemorative plaques, pylon and stand-alone signs are not permitted in the Downtown Core.

11.0 Tall Buildings ~ Overview

Tall buildings characterize a good part of the Downtown Core landscape and have a role to play among other building types and land uses in the emerging downtown context.

Tall buildings are accompanied with significant civic obligations and responsibilities compared to other structures in the built environment. Spatially, they articulate the city pattern and urban structure, while visually reinforcing the civic importance of the Downtown Core and the image of the City as a whole.

Tall buildings must consider location, placement, relationship to the street, height ranges, built form qualities, architectural treatment and their ability to enhance the pedestrian environment by minimizing shadowing of the public realm. They have a role to play in terms of protecting important views, impacts on cultural resources, accommodating a range of uses and supporting transit. Tall buildings must also ensure that fundamental quality of life aspects are provided for, such as access to natural light, sky views and privacy for those that live, work and visit the Downtown Core.

11.1 Massing Standards

Tall buildings in the Downtown Core will be designed to consist of three parts:

Podium

The Podium will enclose and provide definition and appropriate scale for adjacent streets, parks and open spaces, and integrate with adjacent buildings and address the impacts of parking, loading and servicing uses.

The Podium includes the street level to midrise portion of the development which includes the streetwall. The Podium will be designed in accordance with the frontage and massing standards set out in this document.

Middle Shaft (Tower)

The floor plate size and shape of the middle shaft (tower) will be designed with appropriate dimensions for the site; the middle shaft will be located and oriented on the site and in relationship to the Podium and adjacent buildings.

Towers above the Podium height are governed by the massing standards which provide direction on slenderness, orientation and separation.

Top

Design the top of the tall building to contribute to the skyline character and integrate roof top mechanical systems into the design.

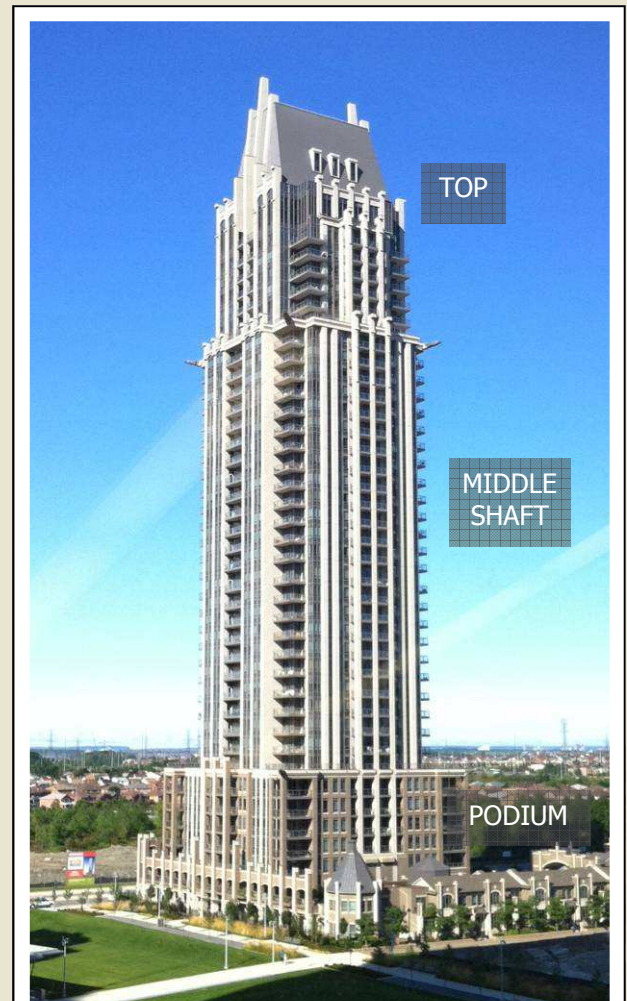
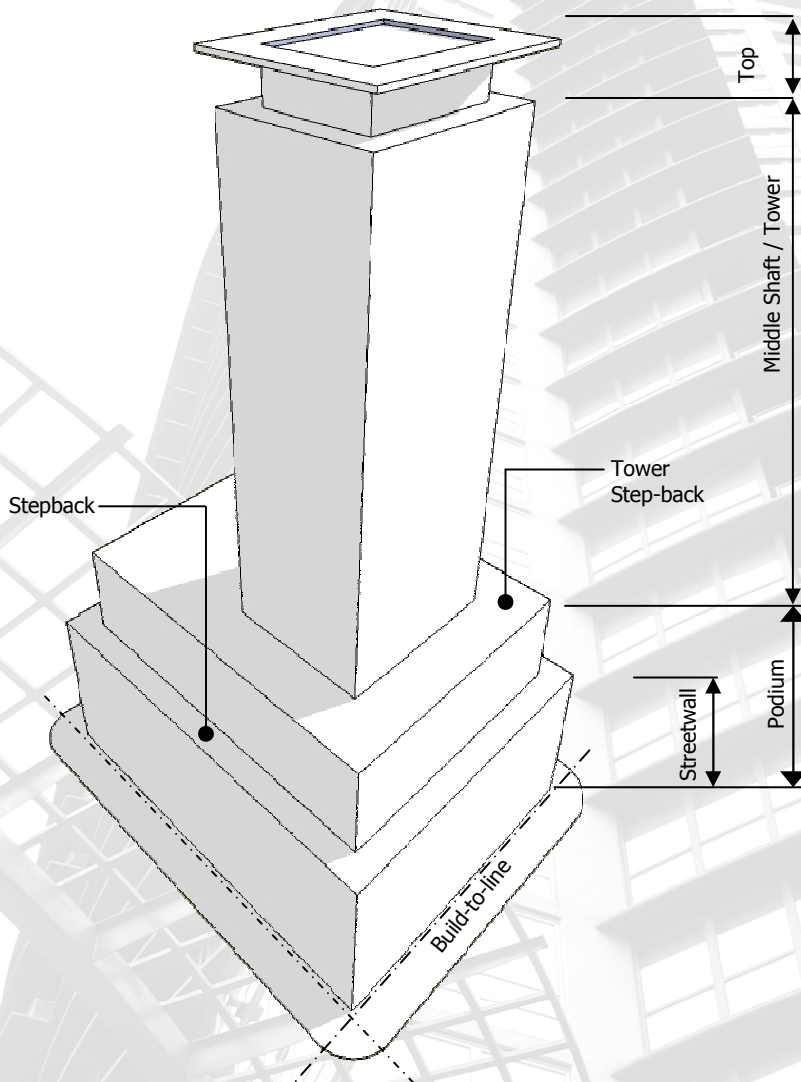


Figure 32: The tripartite form of the Tall Building is well articulated in this Downtown Mississauga Development.



Key Terms

Build-to-line

Podium

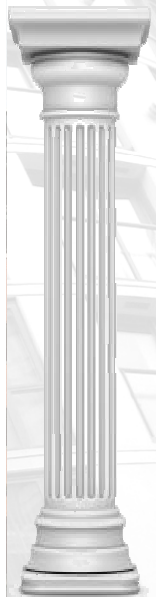
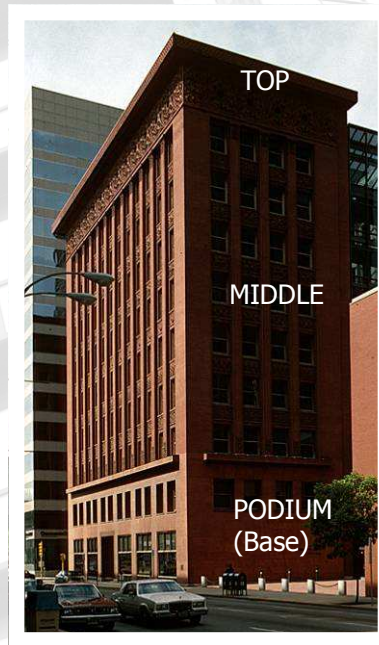
Streetwall

Step-back

** Above terms to be defined*

Figure 33:

Above - Tall buildings are defined by a number of form giving elements that contributes to their overall shape and massing. *Right* - An example of an historic skyscraper that set the precedent for the tri-partite configuration of podium, middle shaft and top - based on classical orders of architecture stemming from antiquity.



11.2 Podium Design

The design and placement of the podium requires careful consideration in terms of its role in helping to contain the street and achieving pedestrian scale qualities. Podiums will need to consider appropriate height in relation to the street right of way, streetwalls, and step-backs at the upper storeys, including ways in which sunlight can reach sidewalks to support a light-filled, vibrant public realm.

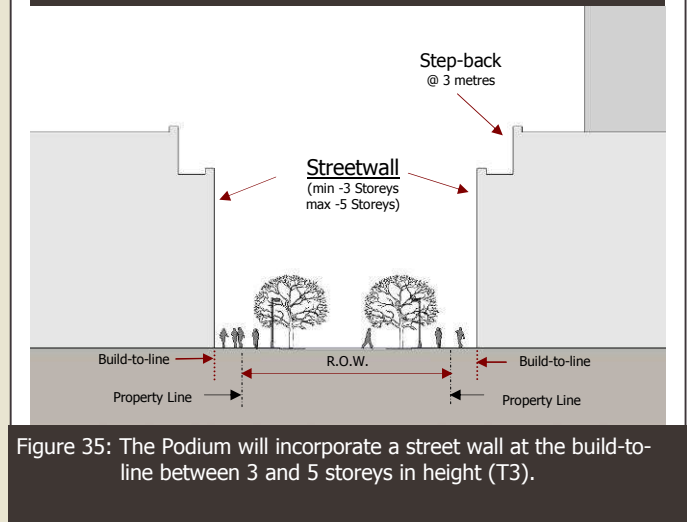
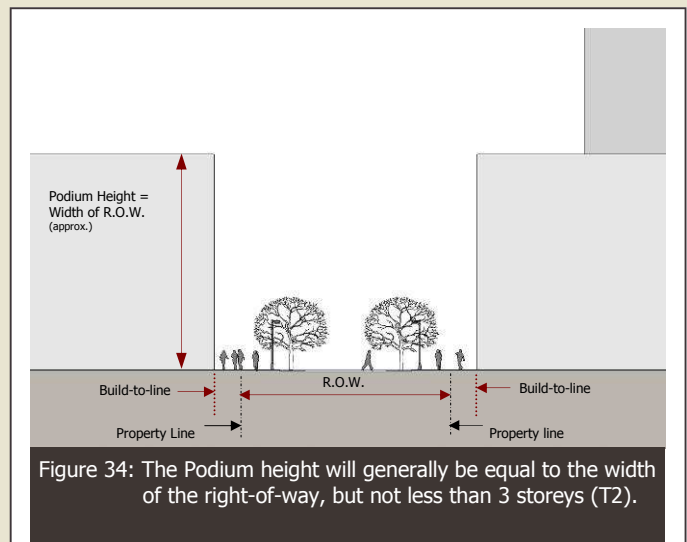
The following standards will apply:

- T1** Podiums will be located and massed to fit harmoniously into their existing and/or planned context and will limit impacts on adjacent streets, parks, open spaces and adjacent properties;
- T2** The podium height will generally be equal to the width of the right-of-way, but not less than 3 storeys (refer to Figure 34);
- T3** To support a pedestrian scale and contain the street, Podiums will also incorporate a street wall placed at the build-to-line, having a minimum of 3 storeys to a maximum of 5 storeys in height. Where right-of-ways exceed 60 m., a street wall of 6 storeys should be used in accordance with Standard 'T2' (refer to Figure 35 and 36);
- T4** Notwithstanding the above, the design of Podiums and their street walls will be articulated and massed at the upper storeys using step-backs to:
 - To mitigate the perception of height and create a comfortable scale for pedestrians;
 - Maximize sunlight on the public realm in accordance with the parameters set out in the *City of Mississauga's Standards for Shadow Studies*;
 - Mitigate the effects of wind on the pedestrian environment.

T5 Step backs of 3.0 m will be applied to the the street wall and podium design in order to achieve the angular plane (Refer to City of Mississauga Standards for Shadow Studies);

T6 Towers should be set back a minimum of 3.0 m from the top tier of the Podium edge in order to establish a clear definition between the podium and tower;

N. B. Refer to Figure 36 for illustration of Podium Design.



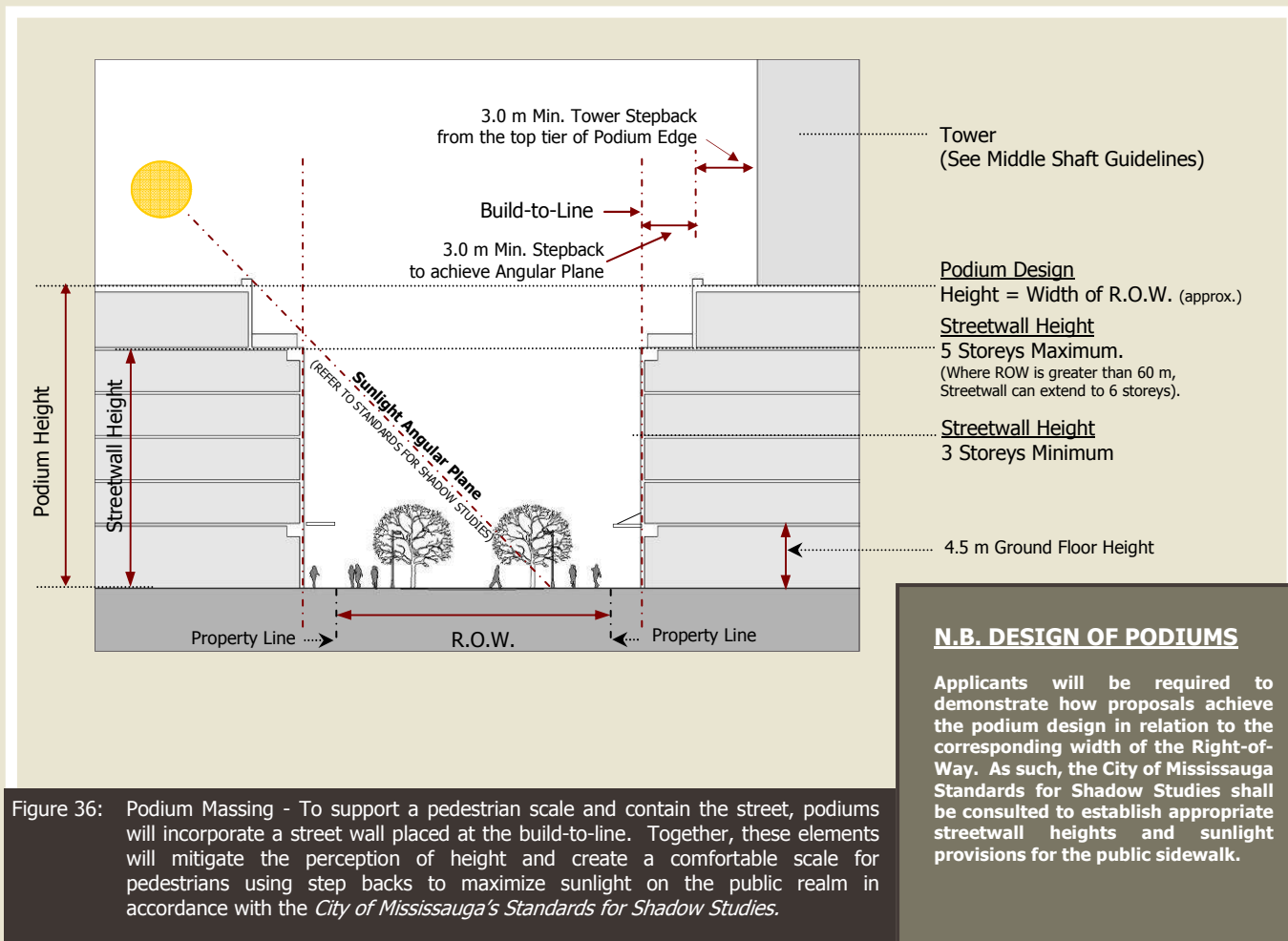


Figure 36: Podium Massing - To support a pedestrian scale and contain the street, podiums will incorporate a street wall placed at the build-to-line. Together, these elements will mitigate the perception of height and create a comfortable scale for pedestrians using step backs to maximize sunlight on the public realm in accordance with the *City of Mississauga's Standards for Shadow Studies*.

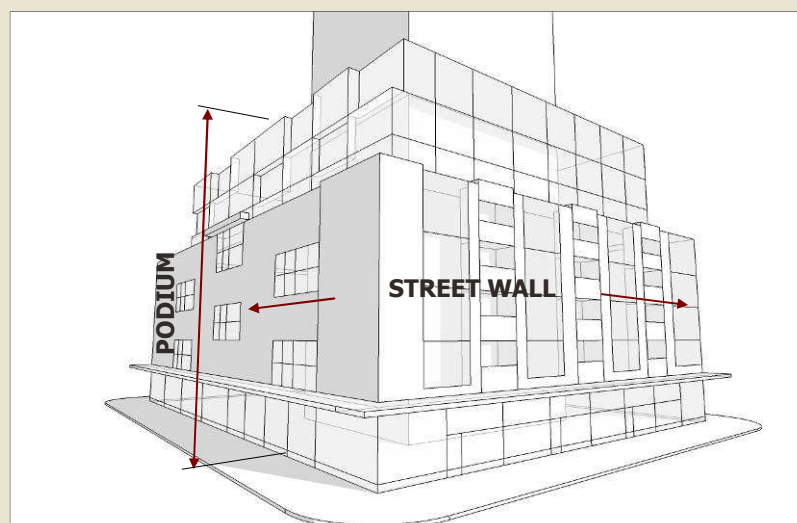


Figure 37: Podiums when well designed and executed contribute positively to the pedestrian environment.

11.3 Middle Shaft (Residential Point Towers)

Compact slim towers and small floor plates minimize shadowing, maximize separation, and views between buildings. They also improve privacy and reduce overlook impacts on adjacent streets, parks, open spaces and properties.

The following standards will apply:

- T7** Design and articulate the floor plates to break down the mass of the building and to create 'street interest' and enhance skyline character;
- T8** Residential tower floor plates above the podium height will not exceed a maximum area of 750 m² (average floor plate);
- T9** Provide a minimum spatial separation 30 m between towers to maximize access to sky views, natural daylighting, adequate privacy, minimize wind conditions and collective shade on the streets, parks and open spaces;

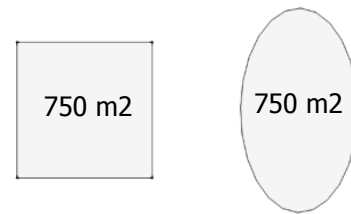


Figure 38: **Slenderness & Floor Plate** — Compact slim towers with small floor plates minimize shadowing, maximize separation and views between buildings, and reduce privacy and overlook impacts. To achieve the objective of compact slimmer towers, yet allow for tower massing flexibility, floor plates above the podium will be designed at 750 m² (average floor plate).

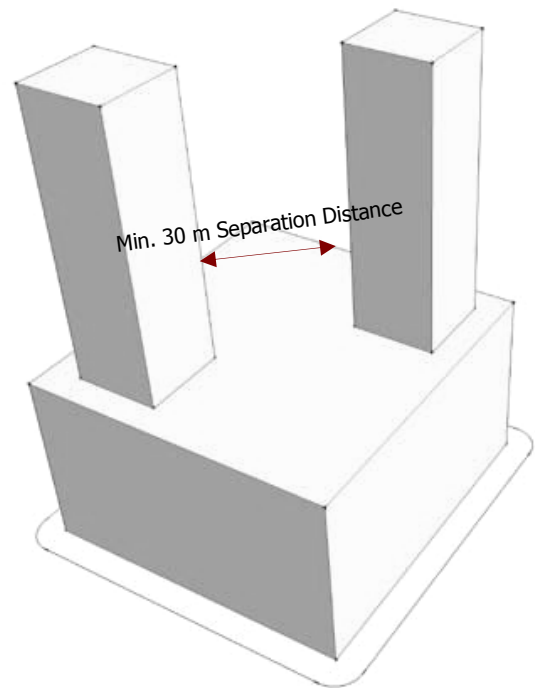
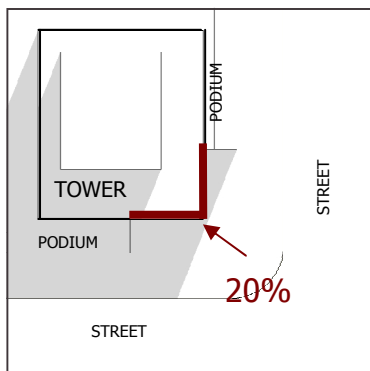
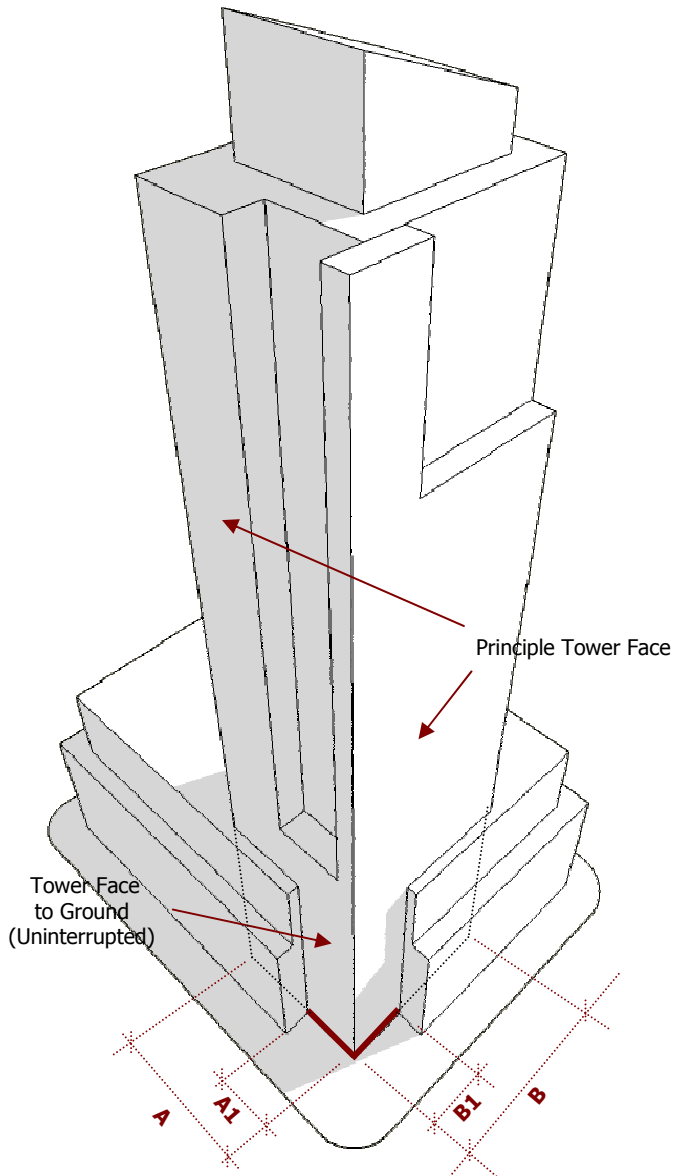


Figure 39: **Tower Separation** — Spacing towers with minimum separation will minimize collective shade and sunlight impacts of closely-spaced towers on streets, parks and adjacent residential buildings. This will also achieve increased access to sky views and natural lighting as well as increased privacy. The minimum spacing between point towers will be a minimum of 30m.



The maximum exposed Tower Face is calculated as follows:

$$\frac{A1 + B1}{A + B} \leq 20\%$$

Where:
A and B = Length of Principle Tower Faces
A1 and B1 = Exposed Face of the Tower (Uninterrupted)

Figure 40: Calculation of uninterrupted tower shafts.

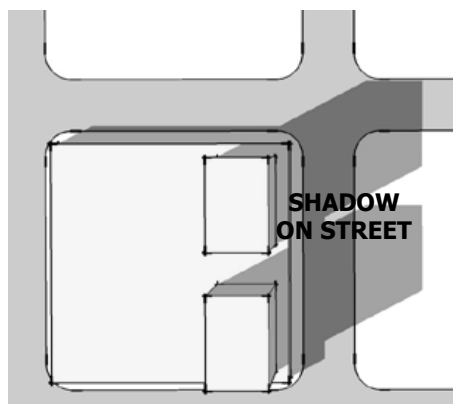
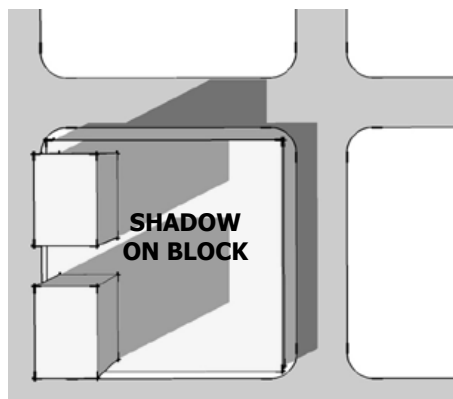
11.4 Uninterrupted Tower Shafts

To allow for different tower expressions and juxtaposition of form in the Downtown Core, a portion of the tower shaft may vertically extend down to grade in order to allow for an uninterrupted tower expression when incorporated into a podium or larger development scheme, subject to the following:

- T10** A maximum of 20% of the principle tower face may extend to grade without interruption by a Podium. Refer to Figure 40 for calculation of maximum allowable area of Uninterrupted Tower Shaft;
- T11** The placement of the tower does not overshadow and impact sunlight on the public sidewalk;
- T12** The setback to accommodate an uninterrupted tower shaft will count towards the allowable setback set out by the frontage standards for 'A' and 'B' Streets (see Section 5.1—5.2, *Street Frontage*);
- T13** When deployed at corner locations, the exposed at grade portion of the tower will be designed to create a highly animated condition in order to positively reinforce and celebrate the corner;
- T14** Tall building proposals must demonstrate that uninterrupted tower treatments do not introduce adverse wind impacts on the pedestrian realm by virtue of the tower location itself;
- T15** Introducing design elements to mitigate the effects of wind that are integrated with the architecture of the building.



CORRECT PLACEMENT



NOT DESIRABLE

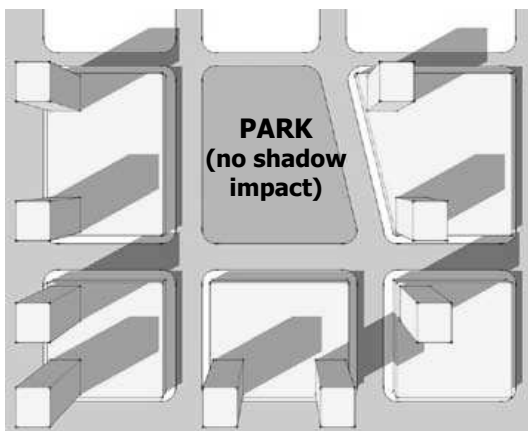


Figure 41: Towers should be located on development blocks to mitigate shadow impacts on the public realm.

11.5 Site Orientation

Point towers should be located on their sites to minimize shadow impacts on adjacent streets and open space. The following standards apply:

- T16** Point towers should be located on the north-west and south-west sides of a block or development site so that shadows fall primarily within the block itself rather than on the street;
- T17** Elongated floor plates should be oriented in a manner which minimizes shadow impacts - this should be demonstrated in accordance with any sun/shadow studies that are required by the City;



11.6 Park Orientation

- T18** Locate towers to minimize shadow impacts on adjacent parks and open spaces;
- T19** For blocks on the southeast and southwest sides of a park, towers should be located on the farthest side of the block from the park;
- T20** For blocks on the northeast and northwest sides of a park, point towers should be located on the southeast and southwest sides of the block;
- T21** Buildings fronting along parks and open spaces will establish a maximum podium height and point tower location/massing that maximizes opportunities for sunlight exposure in accordance with the *City of Mississauga's Standards for Shadow Studies*;



Figure 41: Delineate the top of the tower with a change in architectural detail and have the top meet the sky with a distinctive profile.



11.7 Tower (Roof) Top

- T22** Design the uppermost floors of the tops of tall buildings to achieve a distinctive skyline profile;
- T23** Design the tops of tall buildings to integrate and screen the mechanical penthouse function and other roof top units into the complete building design;
- T24** Use materials, finishes and patterns that are consistent with the overall building design and architectural expression;
- T25** Incorporate ways in which the roof top expression can be subtly highlighted and enhanced with architectural lighting effects during evening hours;

11.8 Built Form Compatibility & Transition to Adjacent Development

Taller buildings should be sited and organized to provide a desirable transition to adjacent lower form buildings and open space. They should also ensure compatible relationships between buildings of differing scale through adequate spatial separation.

Where a significant difference in scale exists between building heights, development will be required to deploy transition strategies through massing and built form, to achieve a harmonious relationship between proposed and existing development, and/or adjacent open spaces.

The following standards will apply:

- T26** Consider the size of the development area and the planned intensity of the use;
- T27** Consider the context of adjacent low scale development and other aspects such as the street width or adjacent open space;
- T28** Where a proposed development incorporates multiple buildings, design the buildings to step down in height from high to low, and where it abuts lower scale development;

T29 For large properties, use an angular plane of 45 degrees from the closest property line of lower scaled residential development, or open space, to determine the minimum setback and height of a building within a development;

T30 For single properties, deploy a stepping down of the building height and mass to achieve a transition to adjacent lower scale development or open space;

T31 Design the development to address the impacts of shadow, sky-views and how sunlight can be maximized on the private and public realm; and

T32 Where a group of buildings and/or spaces act collectively to create a special architectural context (i.e. related by similar scale, heights, materials, colours, architectural character, landscaping and open space patterns or qualities) ensure that proposed infill development respects the context by deploying a strategy for building height that is compatible, and which positively contributes to the existing and/or anticipated pattern of development.

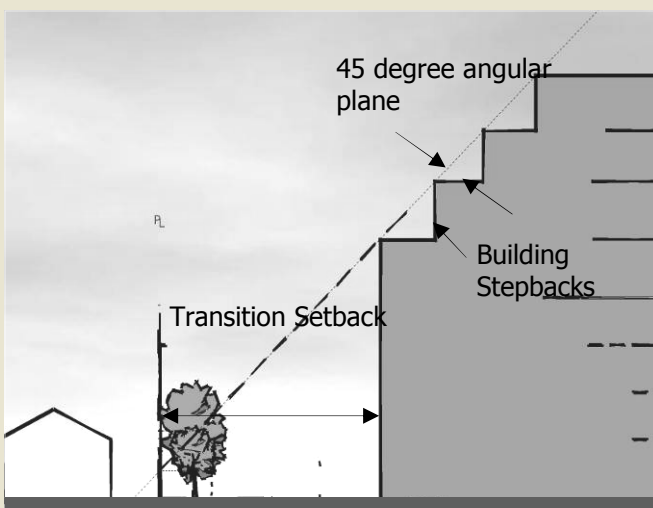


Figure 42: Taller buildings will mitigate height through step-down built form treatments in order to transition to adjacent lower scale development or open space.



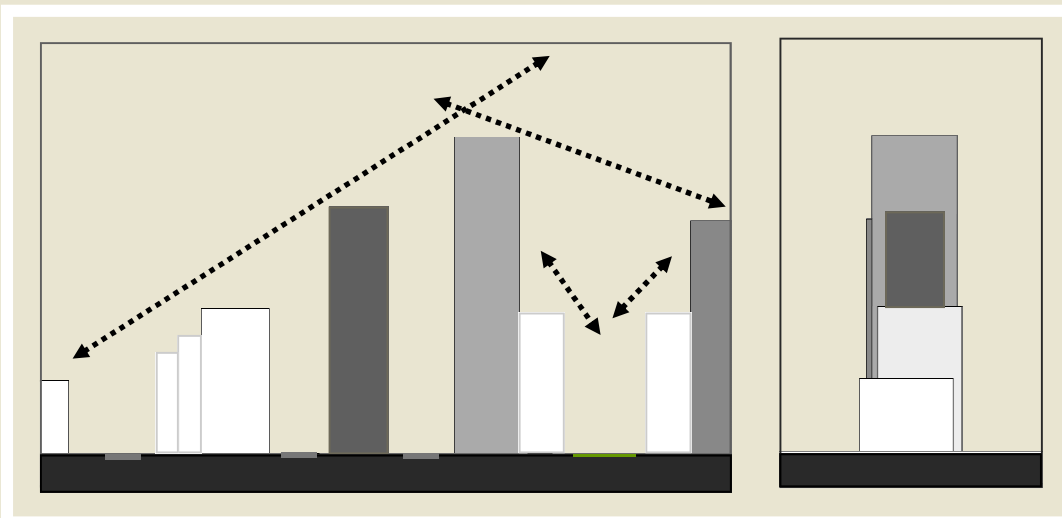
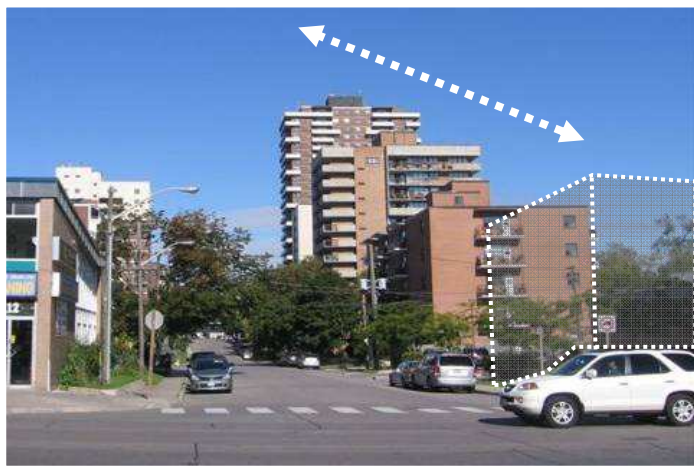
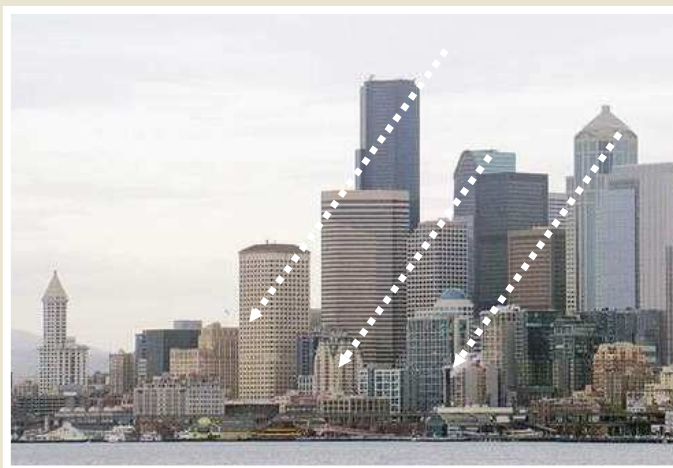


Figure 43: A progression in building heights over large blocks and areas can achieve more orderly arrangements and patterns in the built environment. Heights can be used to articulate and define the highest and most intense use of land, to the least intensive of uses.



11.9 Design for At Grade Residential (Podiums)

There are locations in the Downtown Core where frontages can accommodate townhouse-form (horizontal multiple units) buildings integrated with tall buildings—particularly where retail uses may not be feasible today or in the future. Where at grade residential uses are proposed that form a part of podium design, development will be designed to:

- RD1** Create a high quality interface and transition between the public sidewalk and front door to ensure adequate separation, definition and privacy;
- RD2** Provide high quality landscape and treatments within frontages, transition zones and set-backs;
- RD3** Provide functioning, individual entrances to residential units and animate the frontage with windows on the ground floor that look out onto the street or open space;
- RD4** Incorporate ways in which ground floor residential uses may transition to commercial uses in the future;



Figure 44: Above - Townhouse units address the street and incorporate landscaped transition areas between private individual entrances and the public sidewalk.

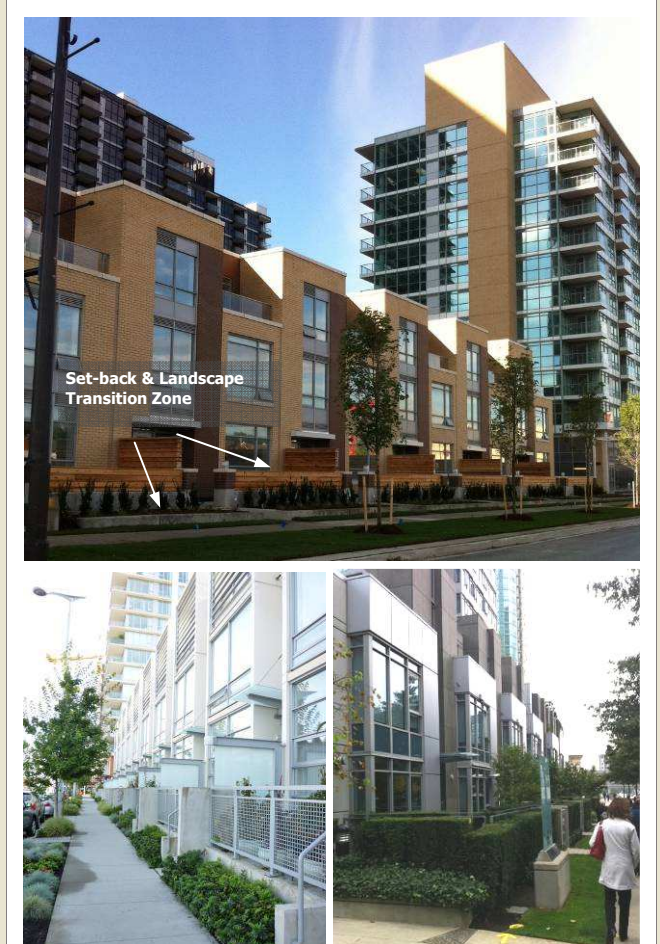


Figure 45: Examples of landscape treatments within transition zones.

Approach I

(Standard Design for Raised Residential Units)

- Provide individual unit entrances from the sidewalk;
- Incorporate a minimum setback of 3.0 metres, which includes front steps (maximum 6 risers) and porch;
- Buffer to include landscape treatments and other design treatments such as a raised planter, low fencing or landscaping to augment the transition zone; and
- Ground floor residential uses raised between 0.9 – 1.2 metres above the sidewalk level;

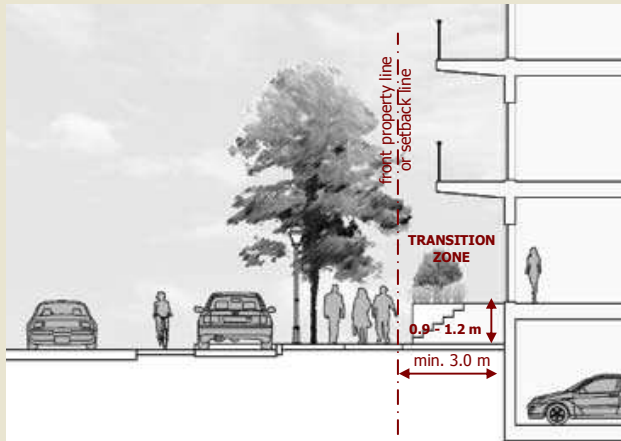


Figure 46: Approach I: Residential uses are elevated above the ground floor, but directly linked to the public sidewalk through individual points of access serving each unit.

Approach II

(Retail Conversion as Future Use)

- Provide individual unit entrances connected and level with the public sidewalk;
- Incorporate a minimum setback of 4.5 metres;
- Buffer to include landscape treatments and other design treatments such as a raised planter, low fencing or landscaping to augment the transition zone; and
- Incorporate a minimum floor-to-floor height (ground floor to second floor) of 4.5 metres.

N.B. The City of Mississauga recognizes that there may be other approaches which achieve similar objectives for the conversion of at grade residential development to commercial/retail uses. These will be considered and assessed for their merit at the application stages.

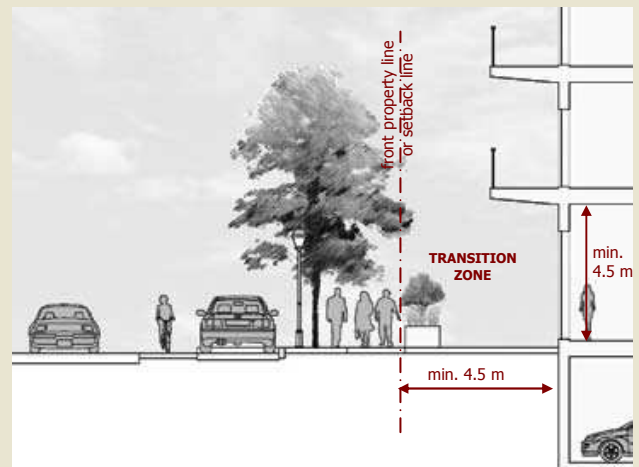


Figure 47: Approach II: Residential uses are located at the same level of the public sidewalk, with individual entrances, but designed for potential conversion to retail and or commercial uses.

<i>Articulation:</i>	<i>Public realm</i>
<i>At grade</i>	<i>Scale</i>
<i>Build-to-line</i>	<i>Setback</i>
<i>Built form</i>	<i>Sidewalk</i>
<i>Compatible/Compatibility</i>	<i>Spill-Out Zone</i>
<i>Curb cut</i>	<i>Streetscape</i>
<i>Edge (Façade</i>	<i>Street frontage</i>
<i>Frontage Design</i>	<i>Streetwall</i>
<i>Glazing</i>	<i>Step back</i>
<i>Liner Building</i>	<i>Storefront</i>
<i>Patio:</i>	<i>Transom</i>
<i>Pedestrian scale:</i>	<i>Transparent</i>
	<i>Transit-supportive land uses</i>
	<i>* ABOVE TERMS TO BE DEFINED</i>



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