



## **AGENDA**

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THE CORPORATION OF THE CITY OF MISSISSAUGA

### **PLANNING & DEVELOPMENT COMMITTEE**

**MONDAY, FEBRUARY 3, 2014 - 7:00 P.M.**

COUNCIL CHAMBER, 2<sup>ND</sup> FLOOR - CIVIC CENTRE  
300 CITY CENTRE DRIVE, MISSISSAUGA, ONTARIO L5B 3C1  
<http://www.mississauga.ca>

#### Members

Mayor Hazel McCallion	
Councillor Jim Tovey	Ward 1
Councillor Pat Mullin	Ward 2
Councillor Chris Fonseca	Ward 3
Councillor Frank Dale	Ward 4 <b>(Chair)</b>
Councillor Bonnie Crombie	Ward 5
Councillor Ron Starr	Ward 6
Councillor Nando Iannicca	Ward 7
Councillor Katie Mahoney	Ward 8
Councillor Pat Saito	Ward 9
Councillor Sue McFadden	Ward 10
Councillor George Carlson	Ward 11

Mumtaz Alikhan, Legislative Coordinator, Office of the City Clerk  
905-615-3200 ext. 5425 / Fax 905-615-4181  
email: [mumtaz.alikhan@mississauga.ca](mailto:mumtaz.alikhan@mississauga.ca)

**LIVE STREAMING:** <http://www.mississauga.ca/videos>



## **PLANNING & DEVELOPMENT COMMITTEE – FEBRUARY 3, 2014**

**PUBLIC MEETING STATEMENT:** In accordance with the *Planning Act, R.S.O. 1990, c.P13*, as amended, if a person or public body does not make oral submissions at a public meeting or make written submission to the City of Mississauga with respect to proposed Official Plan Amendment, Zoning By-law Amendment and/or Plan of Subdivision or Condominium, the person or public body is not entitled to appeal the decision of the City of Mississauga to the Ontario Municipal Board (OMB); and may not be added as a party to the hearing of an appeal before the OMB unless, in the opinion of the Board, there are reasonable grounds to do so.

To make written submissions or to be notified of future meetings, please direct your correspondence to Mississauga City Council, c/o Planning and Building Department at 300 City Centre Drive, Mississauga, ON, L5B 3C1, or by fax at 905-896-5553, or by email at [application.info@mississauga.ca](mailto:application.info@mississauga.ca).

### **CALL TO ORDER**

### **DECLARATIONS OF (DIRECT OR INDIRECT) PECUNIARY INTEREST**

### **APPROVAL OF PREVIOUS MINUTES**

Planning and Development Committee Meeting of January 13, 2014

### **MATTERS TO BE CONSIDERED**

1. Sign Variance Applications – Sign By-law 0054-2002, as amended  
File: BL.03-SIG (2014)
2. Draft Lakeview Local Area Plan (January 2014)  
File: CD.03.LAK
3. PUBLIC MEETING - Information Report on Proposed Housekeeping Amendment – Mississauga Zoning By-law 0225-2007  
City of Mississauga, **Bill 51**, (Wards 1-11)  
File: BL.09-COM
4. PUBLIC MEETING - Information Report on Rezoning and Draft Plan of Subdivision Applications to permit 15 detached dwellings, 6565 Ninth Line, east side of Ninth Line, south of Derry Road West  
Owner: Argo Trail Corporation  
Applicant: Weston Consulting, **Bill 51**, (Ward 10)  
File: OZ 13/014 W10 and T-M13004 W10
5. SUPPLEMENTARY REPORT - Official Plan Amendment and Rezoning to permit revisions to the Parkside Village Master Plan to redeploy density from the addition of Square One Drive, Part of Lot 19, Concession 2, N.D.S., west side of Confederation Parkway, north of Burnhamthorpe Road West  
Owner: Amacon Development (City Centre) Corp.  
Applicant: City of Mississauga, **Bill 51**, (Ward 4)  
File: CD.21.AMA W4

### **ADJOURNMENT**





# Corporate Report

Clerk's Files

Originator's  
Files

BL.03-SIG (2014)

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**DATE:** January 14, 2014

**TO:** Chair and Members of Planning and Development Committee  
Meeting Date: February 3, 2014

**FROM:** Edward R. Sajecki  
Commissioner of Planning and Building

**SUBJECT:** **Sign By-law 0054-2002, as amended**  
**Sign Variance Applications**

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**RECOMMENDATIONS:** That the Report dated January 14, 2014 from the Commissioner of Planning and Building regarding Sign By-law 0054-2002, as amended, and the requested one (1) Sign Variance Application described in Appendix 1 to the Report, be adopted in accordance with the following:

1. That the following Sign Variance **be granted:**

- (a) Sign Variance Application 13-06207  
Ward 3  
1127792 Ontario Limited  
2225 Dundas St. E.

To permit the following:

- (i) One (1) ground sign located on the city road allowance subject to the issuance of an encroachment agreement with the City.
- (ii) One (1) ground sign with a sign area of 15.34 sq. m. (165.16 sq. ft.) per sign face.

**BACKGROUND:**

The *Municipal Act* states that Council may, upon the application of any person, authorize minor variances from the Sign By-law if in the opinion of Council the general intent and purpose of the By-law is maintained.

**COMMENTS:**

The Planning and Building Department has received one (1) Sign Variance Application (see Appendix 1) for approval by Council. The application is accompanied by a summary page prepared by the Planning and Building Department which includes information pertaining to the site location; the applicant's proposal; the variance required; an assessment of the merits (or otherwise) of the application; and a recommendation on whether the variance should or should not be granted.

**FINANCIAL IMPACT:**

Not applicable.

**CONCLUSION:**

Council may authorize minor variances from Sign By-law 0054-2002, as amended, if in the opinion of Council, the general intent and purpose of the By-law is maintained. Sign By-law 0054-2002, as amended, was passed pursuant to the *Municipal Act*. In this respect, there is no process to appeal the decision of Council to the Ontario Municipal Board, as in a development application under the *Planning Act*.

**ATTACHMENTS:**

1127792 Ontario Limited  
Appendix 1-1 to 1-5



Edward R. Sajecki  
Commissioner of Planning and Building

Prepared By: Darren Bryan, Supervisor Sign Unit





**SIGN VARIANCE APPLICATION REPORT**  
**Planning and Building Department**

January 7, 2014

**FILE:** 13-06207

**RE:** 1127792 Ontario Limited  
 2225 Dundas Street East – Ward 3

The applicant requests the following variances to section 13 of Sign By-law 0054-2002, as amended.

Section 13	Proposed
A ground sign must be located on private property and setback a minimum of 1.0m (3.28 ft.) from the property line.	One (1) ground sign located on the city road allowance.
A ground sign shall have a maximum sign area of 15.0 sq. m. (161.46 sq. ft.) per sign face.	One (1) ground sign with a sign area of 15.34 sq. m. (165.16 sq. ft.) per sign face.

**COMMENTS:**

With respect to the proposal to install the sign on the City road allowance, the neighbouring property (2215 Dundas Street East) received an Encroachment Agreement dated December 1, 2005, for a five (5) year term with two 5 year renewal options. The placement of the proposed sign for the subject property (2225 Dundas Street East) maintains a consistent street line and placement as the adjacent property. The Planning and Building Department finds the requested variance acceptable from a design perspective provided an encroachment agreement is entered into.

With respect to the sign area; the request to permit a ground sign with an area of 15.34 sq. m where 15.0 sq. m is permitted is minor in nature. The sign is similar in size and design as others in the area. The Planning and Building Department finds the variance for increased size acceptable from a design perspective.

**1127792 ONTARIO LTD.**

44 ZORRA ST.  
TORONTO, ON M8Z 4Z7  
(416) 503-4444

October 30, 2013

To whom it may concern,

I, Stan Collini, the owner of the property at 2225 Dundas St E Mississauga ON request a variance be granted for our pylon sign.

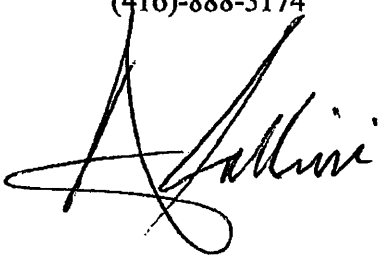
The overall size and the placement of the sign in the location requested will ensure my sign is not hidden by existing trees and will advertise for our tenants and increase their exposure for better business.

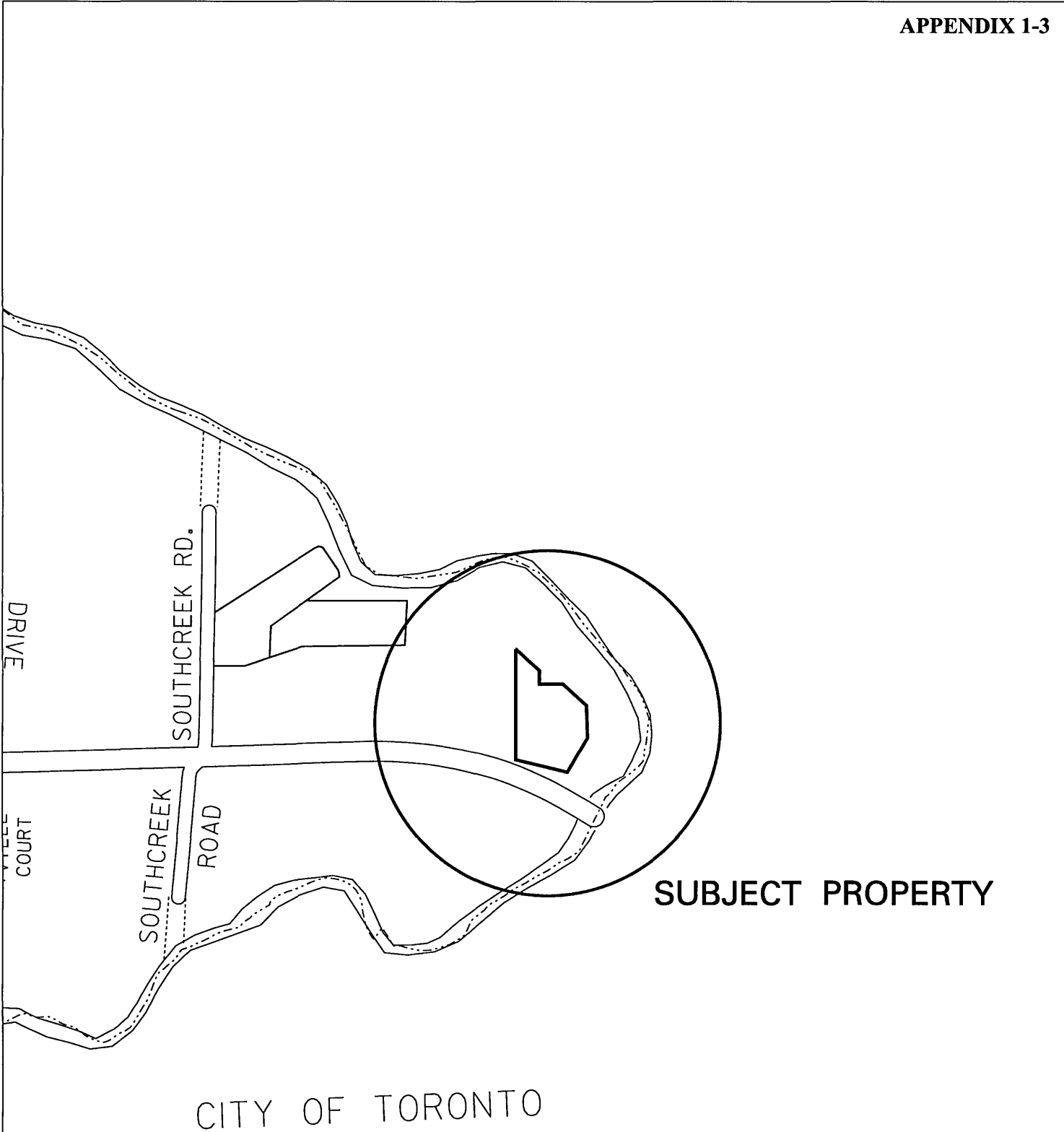
As well, my proposed placement of the sign will match the landscape of my neighbor's pylon to the west of us.

Should you have any further questions please feel free to contact me directly

Thank you,

Stan Collini  
(416)-888-3174

A handwritten signature in black ink, appearing to read 'Stan Collini', with a large, stylized initial 'S'.



CITY OF TORONTO



Planning and Building  
Sign Unit

2225 Dundas Street East  
#13-06207  
Multi Tenant





**2225 Dundas St. East**

3048.0 mm

5080.0 mm 200.00 in

7467.58 mm

1 1/2" x 1 1/2" STEEL ANGLE  
FRAME DRILLED and TAPPED  
onto H.S.S. POLES with 1/2" BOLTS  
(x9) MOUNTING BOLTS PER FRAME  
(x9) MOUNTING BOLTS PER POLE

ELECTRICAL RACEWAYS

8" x 8" x .250" Steel Post

2641.60 mm

1661.0 mm

9618 BALLASTS 2 PER SIDE

14.0 in

GROUND LINE

aluminum base caps

concrete min 25 mpa

STEEL HOOKED ANCHOR bolts  
1 1/4" DIAMETER x 48" with DOUBLE  
NUTS and WASHERS x 4 per plate  
8 anchor bolts total

12mm rebar

96.00 in





30.07 in

**BASE PLATE DETAIL**

1 3/4" HOLES  
13" C to C

16" x 16" x 1 1/4" H.S.S. STEEL  
BASE PLATE cut out for POLE  
with a MIN. 1.2" FILLET WELD

Total sign size  
13.4 metres sq.

CUSTOMER: <b>1127792001 Ontario Ltd</b>		CONTACT: <b>Stan Collini</b>	REVISION #	 <b>905.420.0504</b> <b>1.888.746.KWIK</b> <small>Tel 905 420 0515</small> <b>1735 Orangevale Court, Pickering, ON L1W 3G8</b> <small>ADDITIONAL CHARGES AT THE INSTALLER, INCLUDING DRAWING LAYOUT AND TECHNICAL DETAIL. LAYOUT NOT NECESSARY FOR ALL PROJECTS. A LAYOUT AND CHARTER BE OBTAINED BEFORE THE INSTALLATION.</small>
ORDER #		PHONE: <b>416-888-3174</b>	SITE ADDRESS:	
DATE: <b>9/27/13</b>		EMAIL:	<b>2225 Dundas St E.</b>	
		REP: <b>Doug</b>	CHECKED BY:	
		DESIGNER: <b>Darren</b>		
  				



# Corporate Report

Clerk's Files

Originator's  
Files

CD.03.LAK

**DATE:** January 14, 2014

**TO:** Chair and Members of Planning and Development Committee  
Meeting Date: February 3, 2014

**FROM:** Edward R. Sajecki  
Commissioner of Planning and Building

**SUBJECT:** **Draft Lakeview Local Area Plan (January 2014)**

**RECOMMENDATION:** That the Draft Lakeview Local Area Plan (January 2014), under separate cover to the report titled "Draft Lakeview Local Area Plan" dated January 14, 2014 from the Commissioner of Planning and Building be circulated for comment to City Departments, agencies and stakeholders for review, and further, that a public consultation program, including an open house and statutory public meeting, be held.

## REPORT HIGHLIGHTS:

- The Draft Lakeview Local Area Plan ("Area Plan") was prepared under the planning framework of Mississauga Official Plan. Key modifications and additions proposed in the Area Plan include a vision statement, directing growth to specific areas, and protecting, enhancing and restoring the Green System in Lakeview. Additional policies on complete communities, planning for multi-modal transportation, urban form, and employment uses are also proposed.

contd....



- Key land use changes recommended include:
  - the “Mixed Use” designation for the Lakeshore Corridor – Core Area to require commercial uses at grade. The Core Area is from Seneca Avenue to Hydro Road and is envisioned to have a concentration of street-related commercial uses;
  - the “Business Employment” designation has been revised to clarify uses that are not permitted, such as a cardlock fueling dispensing facility, motor vehicle commercial uses, and motor vehicle rental, and require that permitted uses operate within enclosed buildings;
  - a number of parcels have been redesignated in order to reinforce the policies in the Area Plan. In general, redesignations provide additional uses while allowing any existing uses that do not conform to remain. Appendix 1 and 2 provide a summary of these changes;
  - a number of Special Sites and Exempt Sites have been revised. New Special Site policies for the waterfront and the Arsenal properties have been introduced. Sites that are not representative of the vision, direction and planning policies but nonetheless are recognized because they contain established land uses have been identified as Exempt Sites; and
  - recognizing residential buildings legally constructed prior to the approval date of the Area Plan. This policy allows the Area Plan to recognize existing residential uses that do not conform, thereby removing a number of current special site policies.

contd....

- The next step is to circulate the Area Plan and initiate a public consultation process. The public consultation program will include a statutory public meeting as required by the *Planning Act*, public open house(s), and meeting(s) with the Local Advisory Panel.

**BACKGROUND:**

The Area Plan Review is City-initiated as part of the continuing program to keep the Lakeview policies valid and current. The Area Plan is revised to reflect changes in local circumstances and maintain consistency with Provincial Planning initiatives, such as the Provincial Policy Statement, *Planning Act*, Growth Plan for the Greater Golden Horseshoe, and the Metrolinx's Regional Transportation Plan (*The Big Move*). The review also takes into consideration corporate studies such as the *Employment Land Review Study*, and Inspiration Lakeview.

The Area Plan incorporates public input provided through extensive consultation, including stakeholder interviews, visioning sessions, placemaking workshops, youth outreach exercises and an open house. Several meetings were also held with the Lakeview Local Advisory Panel to discuss planning topics such as the Community Node, townhouse development, transportation, and brownfield development. The feedback from the panel members has helped to inform the Area Plan policies.

The Area Plan reflects, where applicable, the policy direction identified through the public engagement process in the report, *Lakeview and Port Credit District Policies Review and Public Engagement Process – Directions Report*, and feedback received from members of the Lakeview Local Advisory Panel.

The new Official Plan, Mississauga Official Plan (MOP) is now in effect, with the exception of the appealed policies. For the policies under appeal, the relevant policies in Mississauga Plan (2003) will remain in effect. The Plan now replaces references to “planning districts” and “district policies” with “character areas” and “local area plans,” respectively.

Current City projects, such as Inspiration Lakeview and the Parking Strategy – Phase II Port Credit and Lakeview, may require future amendments to the Area Plan. Other Regional and Provincial projects such as, the Lakeview Waterfront Connection Project and the Queen Elizabeth Way Preliminary Design and Class Environmental Assessment Study, are underway. Following the completion of these studies, further modifications may be required to the Area Plan.

The Inspiration Lakeview study area includes the Ontario Power Generation Lands (“OPG Lands”) and the employment lands on the south side of Lakeshore Road East. The study is in progress and the next phase of the study will include a land use review that will examine the appropriateness of a Community Node in that area, the land uses, built form and transportation, and prepare a Phase Two municipal comprehensive review. As such, the underlying designation will remain until the conclusion of Inspiration Lakeview. The Area Plan has included a special site policy that recognizes the components of the land use review and realizes the opportunities for the waterfront area. It also emphasizes the core principles of the Inspiration Lakeview vision.

In instances where an application for an Official Plan Amendment has been approved by City Council, the land use designation has been reflected in the Area Plan. If the application is under appeal, and has not been approved by City Council, the land use designation remains unchanged and will be dealt with through the appeal process.

Updates to the “Greenbelt” land use designation are being reviewed as part of the Mississauga Official Plan – General Amendment as noted in the report dated October 22, 2013 to Planning and Development Committee. As such, additional lands identified in watercourse corridors and along the Lake Ontario shoreline that are subject to natural hazards, reflect the changes proposed in the General Amendment. Therefore, the land use changes to “Greenbelt” in the General Amendment have not been specifically identified in the Area Plan as a “proposed redesignation” as it will be dealt with through a general amendment.

On July 8, 2013, the Ontario Municipal Board issued a decision regarding the appeals to MOP, which includes modifications to drive-through policies. Drive-through facilities are permitted as they existed on the day Mississauga Official Plan came into effect. Through the settlement agreement, new policies on drive-through facilities will be added to the principal document. As such, the special sites allowing drive-through facilities in Lakeview are redundant and have been removed.

**COMMENTS:****1. Purpose and Organization of the Local Area Plan**

MOP contains planning policies to manage and guide the city's development – Local Area Plans form part of MOP and are intended for areas which require a more extensive local planning framework. Local Area Plans contain policies to address unique circumstances particular to the area.

The Area Plan (attached under separate cover) must be read in conjunction with the Plan. Parts one to four, and the schedules and appendices of the principal document are applicable to the Lakeview area, unless modified by the Area Plan. For the ease of locating the policies, the Area Plan follows the same general organization as the principal document.

Many Lakeview issues (e.g., brownfield sites, rail noise, community infrastructure, heritage planning, etc.) are addressed in the principal document. Although these policies are not restated in the Area Plan, additional description may be provided to reflect the Lakeview context and reinforce policy direction. The Area Plan also contains policies specific to Lakeview, and in the event of conflict with the principal document, the policies of the Area Plan take precedence.

There are a number of development applications that are in process and unapproved. These applications will be reviewed through the development approvals process. It is not the purpose of the Area Plan to approve or deny individual development applications. As such, the original designation remains applicable for these sites.

Schedules 1 to 3 are new schedules to the Area Plan. Schedule 1 is the boundary of the precincts and sub-areas in Lakeview. Schedule 2 shows the minimum and maximum building heights permitted in Lakeview. Schedule 3 is the long term road and transit network. Schedule 4 is the land use designations; most designations have carried over from the existing Local Area Plan that form part of MOP.

Appendices to the Area Plan include the Lakeview Built Form Standards and the Executive Summary from the Lakeshore Road Transportation Review Study. While these documents are not considered part of the Area Plan, they provide important information in understanding and interpreting the policies. Some guidelines have been incorporated into the Area Plan and represent policy.

## **2. Overview of Policy Directions**

The Area Plan incorporates and builds upon the policies in the previous Lakeview District Policies of Mississauga Plan (2003) and the subsequent Lakeview Local Area Plan. Many existing policies and land use designations have been carried forward. There are a number of proposed key modifications and additions in the Area Plan.

### A New Vision (Section 5.0)

The Vision section provides a description of the desired future of Lakeview. The Vision is based on six key principles: reconnecting Lakeview to the waterfront; distinct neighbourhoods; complete communities; community health; social well-being; and, sustainability.

To implement the Vision, the Community Concept has five main elements: the Green System, a Community Node, Neighbourhoods, Employment Area, and Corridors.

### Direct Growth (Section 6.0)

The Area Plan directs growth to the following areas: Lakeview Community Node; modest infilling in neighbourhoods; and the redevelopment of sites along Lakeshore Road East (e.g., northeast corner of Lakeshore Road East and Cawthra Road, and the northwest corner of Lakeshore Road East and Dixie Road) and other commercial

sites (e.g., the plaza north of Lakeshore Road East and west of Cawthra Road, Applewood Village Plaza, and Dixie Outlet Mall).

Policies and the boundaries for the Community Node will be determined following the next phase of study for Inspiration Lakeview. Principles for the future development of the Community Node have been included in the Area Plan which recognize that:

- the Lakeview Community Node will have a gross density between 100 and 200 residents and jobs combined per hectare, and a population and employment range of 2:1 to 1:2;
- residential neighbourhoods are stable areas, and infill or redevelopment will be sensitive to the existing character;
- intensification in the Lakeshore Corridor will consider matters relating to complete communities, mainstreet character, heritage, and protecting views to the waterfront; and
- redevelopment of existing commercial sites will address issues such as retaining commercial space and appropriate transitions to adjacent residential uses.

#### Value the Environment (Section 7.0)

Area Plan policies identify the components of the Green System and reinforce the importance of protecting, enhancing and restoring the natural environment, including:

- natural systems within waterfront parks; and
- urban forests.

The Area Plan encourages partnerships to improve the environment and refers to the City's Green Development Strategy as a means to incorporate sustainable development practices.

Complete Communities (Section 8.0)

Lakeview contains many of the attributes associated with complete communities. Policies are intended to recognize, protect and enhance these attributes to fully realize the complete community aspects including:

- preserving and encouraging additional affordable housing;
- recognizing cultural heritage and the opportunity to use Community Improvement Plans to enhance these resources;
- providing a mix of uses and services to residents in the Community Node and Lakeshore Corridor;
- enhancing the distinct identity and character of the area through improvements to the public realm; and
- providing public access to the waterfront.

Multi-Modal City (Section 9.0)

Planning for improvements to the road network and active transportation in Lakeview will be essential in linking Lakeview to the neighbouring communities and in achieving a city-wide finer grain network with additional road connections. A number of transportation policies have been introduced in the Area Plan including:

- acquiring lands for a public transit right-of way along Lakeshore Road East;
- undertaking a Transportation Master Plan for Lakeshore Road (between the east and west City limit) that will address improving all modes of transportation, the implications of future growth in the network and consider placemaking initiatives that promote the animation of the corridor;

- connecting the public transit along key north-south corridors e.g., Cawthra Road, Ogden Avenue and Dixie Road to Lakeshore Road East; and
- recognizing that reduced parking requirements and maximum parking standards may be considered in the Community Node and the Lakeshore Corridor.

#### Desirable Urban Form (Section 10.0)

The desirable urban form policies reflect the planned function and local context. Some key urban form policies include the following:

- identifying on Schedule 2 the specific location and range of permitted heights (minimum and maximum);
- ensuring that development in Neighbourhoods is sensitive to the existing low rise context;
- recognizing that Inspiration Lakeview is underway, and development of the OPG site and adjacent lands should have regard for the form and scale appropriate to a waterfront location, view corridors, and public open space along the shoreline;
- demonstrating an appropriate transition to properties adjacent to the Lakeshore Corridor to ensure the character of the mainstreet is preserved; and
- meeting the criteria of the various types of built form for residential developments.

#### Strong Economy (Section 11.0)

Employment opportunities are an important component of Lakeview as employment contributes to a healthy mix of uses, while providing an opportunity for people to live and work in the community. These opportunities include:



- providing for continued operation of employment uses along the Lakeshore Corridor; and
- developing a waterfront for various activities suitable for recreational, retail, and cultural purposes.

#### Land Use Designations (Section 12.0)

As mentioned earlier, most of the existing land use designations and permitted uses have been brought forward from the existing Local Area Plan and previous District Policies that form part of the new Mississauga Official Plan. Key proposed changes include:

- the “Mixed Use” designation for the Lakeshore Corridor – Core Area to require commercial uses at grade. The Core Area is from Seneca Avenue to Hydro Road and is envisioned to have a concentration of street-related commercial uses;
- the “Business Employment” designation has been revised to clarify uses that are not permitted, such as a cardlock fueling dispensing facility, motor vehicle commercial uses, and motor vehicle rental, and require that permitted uses operate within enclosed buildings;
- a number of parcels have been redesignated in order to reinforce the policies in the Area Plan. In general, redesignations provide additional uses while allowing any existing uses that do not conform to remain. Appendix 1 and 2 provide a summary of these changes; and
- recognizing residential buildings legally constructed prior to the approval date of the Area Plan. This policy allows the Area Plan to recognize existing residential uses that do not conform, thereby removing a number of current special site policies.

Special Sites and Exempt Sites (Section 13.0)

There are sites that merit special attention and are classified as “special sites.” Special sites are areas with unique circumstances and support the long term vision of the Area Plan. There are also sites identified as “exempt sites.” Exempt sites reflect unique circumstances that are not representative of the vision, direction and planning policies of the Area Plan, but nonetheless are recognized because they contain established land uses. In the event the exempt site use is discontinued, the permission for this use no longer applies. In the Area Plan, key proposed changes to the special site and exempt site policies include:

- removing a number of special sites which are currently permitted within the land use designations and in the principal document;
- adding a special site policy for a key waterfront location (i.e. the OPG Lands and a portion of the Lakefront Business Park sub-area) and for the Arsenal property; and
- identifying a number of sites as exempt sites. Exempt sites are not representative of the vision, direction and planning policies but nonetheless are recognized because they contain established land uses.

Appendices

The Area Plan includes the following appendices for information purposes:

1. Appendix I: Lakeview Built Form Standards is to be used during the review of development applications. This guide demonstrates how the urban form policies may be achieved; and,
2. Appendix II: The Executive Summary from the Lakeshore Road Transportation Review Study identifies the findings regarding how Lakeshore Road can accommodate alternative modes of transportation.

### 3. Co-ordination with Inspiration Lakeview

The Area Plan recognizes that the next phase of Inspiration Lakeview is in progress. That study includes a land use review of the OPG Lands and a portion of the lands in the Lakefront Business Park sub-area. Inspiration Lakeview will also address the appropriateness of a Community Node in that area. The underlying designation will remain until the conclusion of Inspiration Lakeview.

### 4. Next Step: Circulation and Public Consultation

The next step is for the Area Plan to be circulated for formal comment and to have a public consultation program conducted, which will include:

- a statutory public meeting as required by the *Planning Act*;
- public open house(s); and
- meeting(s) with the Local Advisory Panel.

Additional meetings will be held as required based on requests from stakeholders or for issue resolution.

The objectives of the public consultation program are to inform, consult, involve and collaborate with stakeholders.

### STRATEGIC PLAN:

The Area Plan is an important tool to implement the land use components of the Strategic Plan and to refine the policies in the Official Plan. The Area Plan aligns with the following goals and actions of the Strategic Plan:

#### MOVE - Developing a Transit-Oriented City

- Develop Environmental Responsibility
- Connect our City
- Build a Reliable and Convenient System
- Increase Transportation Capacity
- Direct Growth

BELONG - Ensuring Youth, Older Adults and New Immigrants Thrive

- Ensure Affordability and Accessibility
- Support Aging In Place
- Attract and Retain Youth

CONNECT - Completing Our Neighbourhoods

- Develop Walkable Connected Neighbourhoods
- Build Vibrant Communities
- Create Great Public Spaces
- Celebrate Our Community
- Provide Mobility Choices
- Nurture “Villages”

PROSPER - Cultivating Creative and Innovative Businesses

- Meet Employment Needs
- Strengthen Arts and Culture

GREEN – Living Green

- Lead and Encourage Environmentally Responsible Approaches
- Conserve, Enhance and Connect Natural Environments
- Promote a Green Culture

**FINANCIAL IMPACT:** N/A

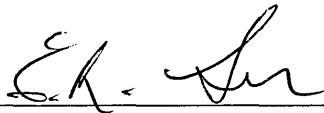
**CONCLUSION:**

The Area Plan reflects the policy direction identified through the public engagement process to date and summarized in the Directions Report. It also builds on the policies of Mississauga Official Plan, ensuring the unique characteristics of Lakeview are reflected and strengthened to guide future development.

**ATTACHMENTS:** UNDER SEPARATE COVER: Draft Lakeview Local Area Plan  
(January 2014)

APPENDIX 1: Draft Lakeview Local Area Plan -  
Proposed Land Use Redesignations  
(table)

APPENDIX 2: Proposed Land Use  
Redesignations – Draft Lakeview  
Local Area Plan (map)



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Edward R. Sajecki  
Commissioner of Planning and Building

*Prepared By: Karin Phuong,  
Planner  
Policy Planning Division*



## APPENDIX 1

Draft Lakeview Local Area Plan Proposed Land Use Redesignations <sup>1</sup>					
Location No.	Location	Current Designation	Proposed Designation	Explanation	Effect on Property
1	East side of Northmount Avenue, south of the South Service Road	Residential Low Density II	Residential Medium Density	The property is located within the Neighbourhood Character Area. The proposed designation recognizes the existing built form.	The proposed designation allows for the townhouses and no longer requires a special site policy.
2	Southeast quadrant of Cawthra Road and Atwater Avenue	Residential High Density	Residential Medium Density	The properties are situated within the Neighbourhood Character Area. The proposed designation recognizes the existing built form.	The proposed designation allows for townhouses. A special site policy is required for the street townhouses.
3	East of Cawthra Road, south of the Canadian National Railway tracks	Residential Low Density II	Residential Medium Density	The properties are located within the Neighbourhood Character Area. The proposed designation recognizes the existing built form.	The "Residential Medium Density" designation in the Draft Lakeview Local Area Plan ("Area Plan") permits the low-rise apartment.
4	North side of Lakeshore Road East, west of Cawthra Road and on the east side of Cooksville Creek	Business Employment	Residential Medium Density, Greenbelt, and Mixed Use	This site was considered in the Employment Land Review Study as an area of managed change that may be suitable for a broader mix of use. Planning staff's Supplementary Report, dated June 4, 2013 to Planning and Development Committee recommended	The implementation of the Official Plan Amendment, Rezoning, and Draft Plan of Subdivision will be completed through the development approvals process. The Area Plan is showing the recommendation adopted by City Council.

<sup>1</sup> Updates to the "Greenbelt" land use designation proposed in the report *Mississauga Official Plan – General Amendment* dated October 22, 2013 to Planning and Development Committee are not identified in this table, but are reflected on Schedule 4 of the Draft Lakeview Local Area Plan and Appendix 2 in the Corporate Report.

Draft Lakeview Local Area Plan Proposed Land Use Redesignations <sup>1</sup>					
Location No.	Location	Current Designation	Proposed Designation	Explanation	Effect on Property
				approval of the applications, Files OZ 03/038 and T-M06006 – and adopted by City Council on July 3, 2013.	
5	North side of Lakeshore Road East, west of Cawthra Road and on the west side of Cooksville Creek	Business Employment, Greenbelt, and Residential Low Density II	Mixed Use, Residential High Density and Greenbelt	This site was considered in the Employment Land Review Study as an area of managed change that may be suitable for a broader mix of use. Planning staff's Supplementary Report, dated August 13, 2013 to Planning and Development Committee recommended approval of the application, File OZ 11/017– and adopted by City Council on September 18, 2013.	The implementation of the Official Plan Amendment and Rezoning will be completed through the development approvals process. The Area Plan is showing the recommendation adopted by City Council.
6	Northwest corner of Lakeshore Road East and Roosevelt Road	Residential High Density	Mixed Use	The "Mixed Use" designation along Lakeshore Road East is in keeping with the vision for the Lakeshore Corridor.	Through the Area Plan policy, the existing apartment building will be permitted. In the future, should the site be redeveloped, a broader range of uses would be required. The site will require rezoning (e.g., C4 with an exception zone permitting the existing apartment building).

Draft Lakeview Local Area Plan Proposed Land Use Redesignations <sup>1</sup>					
Location No.	Location	Current Designation	Proposed Designation	Explanation	Effect on Property
7	Northwest corner of Lakeshore Road East and Shaw Drive	Residential High Density	Mixed Use	The "Mixed Use" designation along Lakeshore Road East is in accordance with the vision for the Lakeshore Corridor.	Through the Area Plan policy, the existing apartment buildings will be permitted. In the future, should the site be redeveloped, a broader range of uses would be required. The site will require rezoning (e.g., C4 with an exception zone permitting the existing apartment buildings).
8	Southeast corner of Lakeshore Road East and Enola Avenue	Residential High Density	Mixed Use	The "Mixed Use" designation along Lakeshore Road East is in keeping with the vision for the Lakeshore Corridor.	Through the Area Plan policy, the existing apartment building will be permitted. In the future, should the site be redeveloped, a broader range of uses would be required. The site may require rezoning (e.g., C4 with an exception zone permitting the existing apartment building).
9	Northwest corner of Lakeshore Road East and Greaves Avenue	Residential High Density	Mixed Use	The "Mixed Use" designation along Lakeshore Road East is consistent with the vision for the Lakeshore Corridor.	Through the Area Plan policy, the existing apartment building will be permitted to continue. A special site policy is required for the townhouses.



Draft Lakeview Local Area Plan Proposed Land Use Redesignations <sup>1</sup>					
Location No.	Location	Current Designation	Proposed Designation	Explanation	Effect on Property
10	Southwest corner of Lakeshore Road East and East Avenue	Residential Medium Density	Mixed Use	The "Mixed Use" designation along Lakeshore Road East is in keeping with the vision for Lakeshore Corridor.	Through the Area Plan policy, the existing apartment buildings will be permitted to continue. However, should the site be redeveloped in the future, a broader range of uses would be required. The site will require rezoning (e.g., C4 with an exception zone permitting the existing apartment buildings).
11	North side of Lakeshore Road East, east of Strathy Avenue	Residential Low Density II	Residential Medium Density	The property is located within the Neighbourhood Character Area. The proposed designation recognizes the existing built form.	The proposed designation allows for townhouses and no longer requires a special site policy.
12	East of Fergus Avenue, north of Lakeshore Road East	Residential High Density	Residential Medium Density	The property is located within the Neighbourhood Character Area. The proposed designation recognizes the existing built form.	A portion of the property is proposed to be designated to allow for townhouses; a special site will no longer be required. The site is currently zone exception RA3-23 and will require revisions to the zoning (e.g., RM4 for the townhouse dwellings)

Draft Lakeview Local Area Plan Proposed Land Use Redesignations <sup>1</sup>					
Location No.	Location	Current Designation	Proposed Designation	Explanation	Effect on Property
13	North side of Atwater Avenue, west of Haig Boulevard	Residential Medium Density	Residential Low Density II	“Residential Medium Density” in the Draft Lakeview Local Area Plan generally follows the uses permitted in the principal document and allows for townhouse and all forms of horizontal multiple dwellings. The existing built form is detached dwellings, semi-detached dwellings and street townhouses which are permitted in the “Residential Low Density II” designation.	The proposed designation permits various forms of housing including detached dwellings, semi-detached dwellings, and street townhouses.



Location No. 1  
From : Residential Low Density II  
To : Residential Medium Density

Location No. 2  
From : Residential High Density  
To : Residential Medium Density

Location No. 3  
From : Residential Low Density II  
To : Residential Medium Density

Location No. 4  
From : Business Employment and Greenbelt  
To : Residential Medium Density, Mixed Use, and Greenbelt

Location No. 5  
From : Residential Low Density II, Business Employment and Greenbelt  
To : Residential High Density, Mixed Use and Greenbelt

Location No. 6  
From : Residential High Density  
To : Mixed Use

Location No. 7  
From : Residential High Density  
To : Mixed Use

Location No. 8  
From : Residential High Density  
To : Mixed Use














Location No. 9  
From : Residential High Density  
To : Mixed Use

Location No. 10  
From : Residential Medium Density  
To : Mixed Use

Location No. 11  
From : Residential Low Density II  
To : Residential Medium Density

Location No. 12  
From : Residential High Density  
To : Residential Medium Density

LAND USE DESIGNATIONS

	Residential Low Density I		Office
	Residential Low Density II		Business Employment
	Residential Medium Density		Public Open Space
	Residential High Density		Private Open Space
	Mixed Use		Greenbelt
	Convenience Commercial		Utility
	Motor Vehicle Commercial		

BASE MAP INFORMATION

	Provincial Highway and Interchange		Natural Hazards
	Arterial		Public School
	Major Collector		Catholic School
	Major Collector (Scenic Route)		Community Facilities
	Minor Collector		Local Area Plan Boundary
	Local Road		
	Existing Commuter Rail		
	GO Transit Station		
	Higher Order Transit Corridor		

Notes:

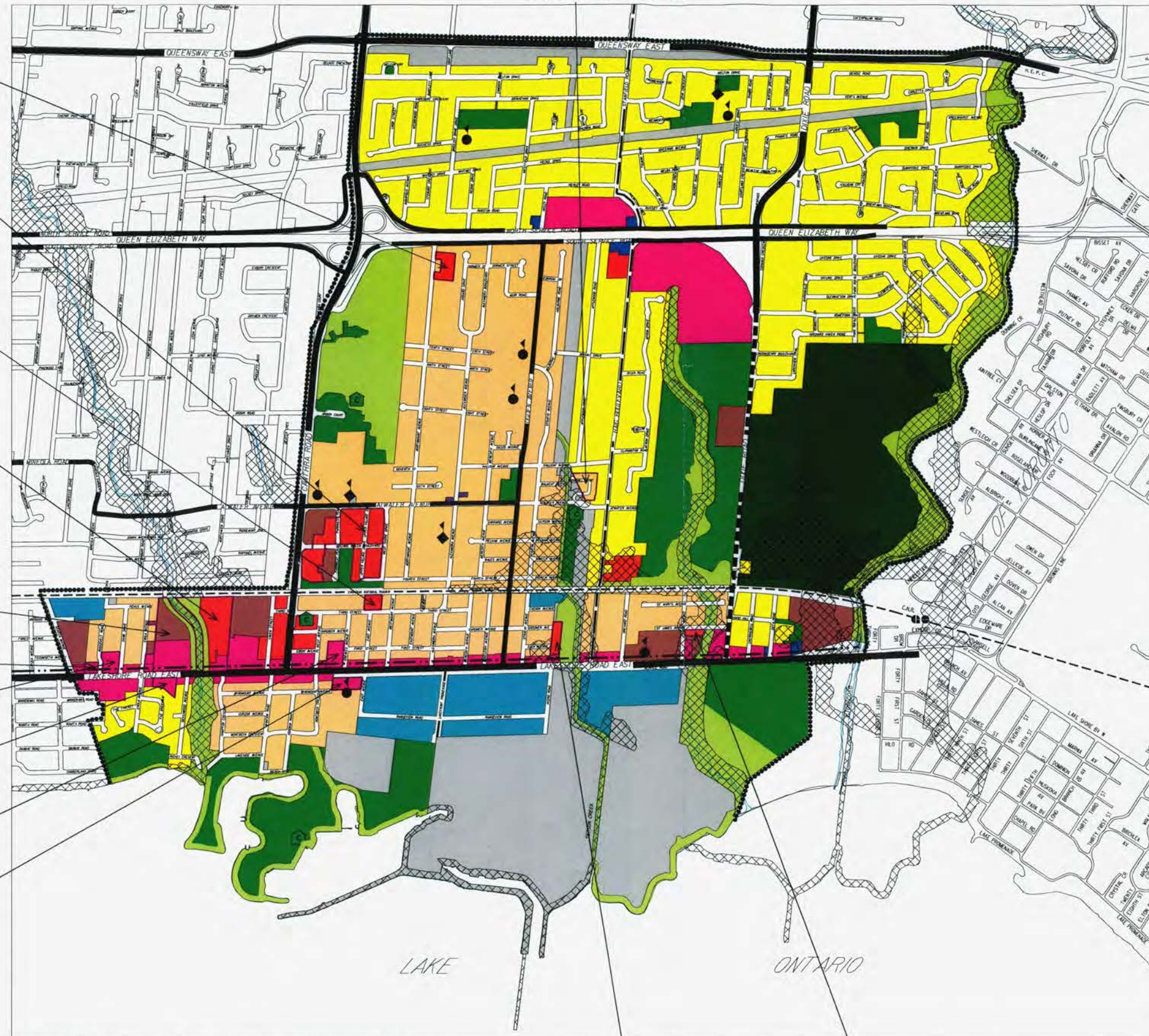
1. Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.
2. Refer to Schedule 3, Natural System of the principal document for the location of the Natural Areas System and Natural Hazards.
3. The limits of the Natural Hazards shown on this map are for illustrative purposes only. The appropriate Conservation Authority should be consulted to determine their actual location.
4. The Community Node boundary is to be determined.

Proposed Land Use Redesignations - Draft Lakeview Local Area Plan



City of Mississauga

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## Lakeview Local Area Plan

Draft – January 2014

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## **OTHER INFORMATION:**

**Credits**

# LAKEVIEW

## 1.0 How to Read the Lakeview Local Area Plan

Mississauga Official Plan (“the Plan”) consists of a principal document and a series of local area plans, provided under separate cover. This is the Lakeview Local Area Plan (“Area Plan”) and provides policies for lands located in southeast Mississauga as shown in Map 1. It includes lands identified in the City Structure as a Community Node, Neighbourhood Area and Employment Area.

There are some instances where the policies and schedules of the principal document do not address all circumstances particular to Lakeview. In these cases, this Area Plan elaborates on, or provides exceptions to, the policies or schedules of the principal document.

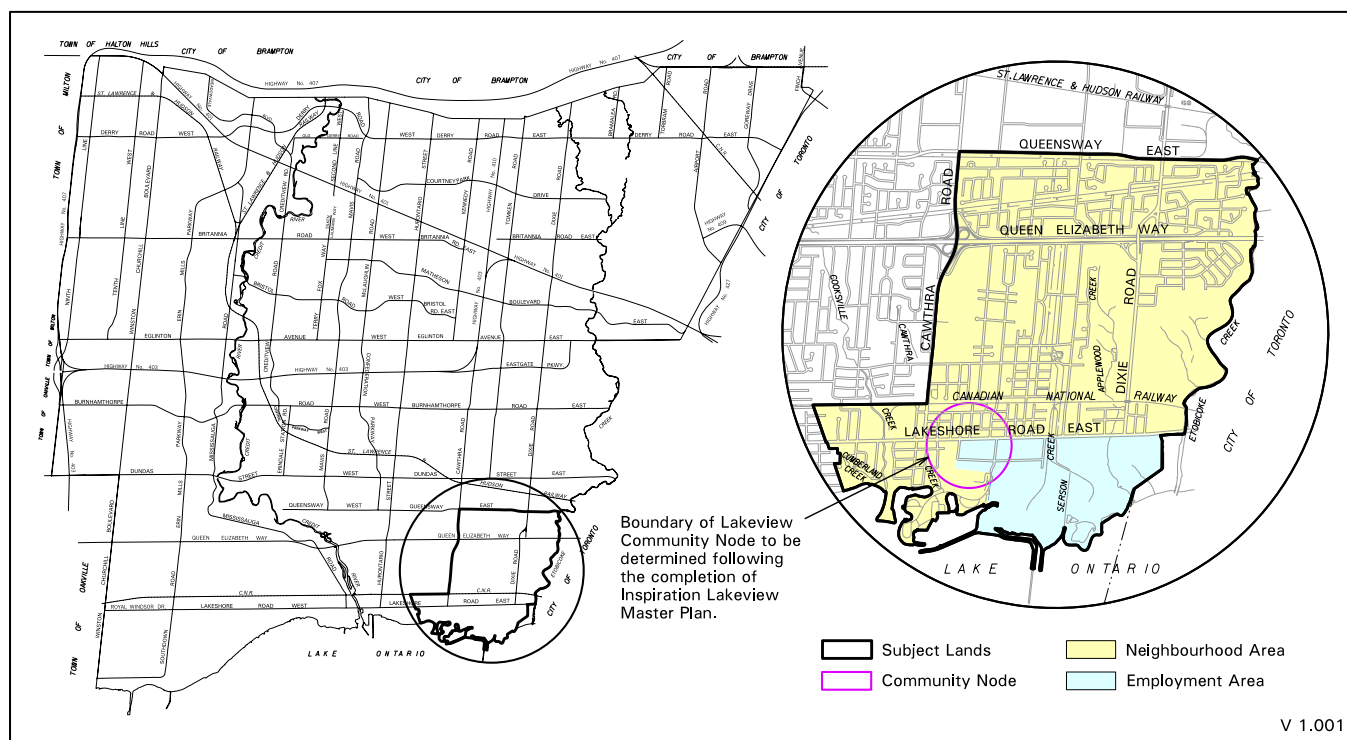
This Area Plan must be read in conjunction with the principal document. Parts one to four, the

schedules, and the appendices of the principal document are applicable to the Lakeview area unless modified by this Area Plan. For example, the policies of this Area Plan must be read in conjunction with the environmental, multi-modal, urban form and land use policies of parts two and three of the principal document. In the event of a conflict, the policies of this Area Plan take precedence.

For the purpose of this Area Plan, when Lakeview is referenced, it includes lands within the Lakeview Community Node, Neighbourhood Area and Employment Area.

Policies and the boundaries for the Community Node will be determined following the next phase of study for Inspiration Lakeview. Principles for the future development of the Community Node have been included in this Area Plan.

Appendices attached to this Area Plan have been included for information purposes.



Map 1: Lakeview is located in the southeast corner of Mississauga and includes areas identified in the City Structure as Community Node, Neighbourhood Area, and Employment Area.

Included in the appendices is the Lakeview Built Form Standards which will be used during the review of development applications. This document demonstrates how the urban form policies can be achieved.

The Built Form Standards document is not considered part of this Area Plan. However, selected content has been incorporated and represents policy.

The Executive Summary from the Lakeshore Road Transportation Review Study has been included in the Appendix to provide additional information on how the Lakeshore Road **corridor** within the Lakeview and Port Credit Character Areas can accommodate alternative modes of transportation.

## 2.0 Historical Context

In the early 1800s, the Lakeview community consisted of large family owned farms. The Cawthra, Cavan, Haig, Ogden, Halliday, and Shaw families all owned land in the area. Many of the street names today bear the family names of the farmers and land owners in the area. The Cawthra-Elliot Estate remains a significant heritage site in the City of Mississauga.

Lake Shore Road first opened in 1804, and in 1917 it was the first concrete road in Ontario, and one of

the longest cement roads between two cities in the world. By 1944, the road became what we know it as today: Lakeshore Road. Historically, commercial development has occurred on Lakeshore Road and primarily in a strip form along the **corridor**.

In 1915, Curtiss Aeroplanes and Motors Ltd. established Canada's first aerodrome, officially known as the Long Branch Aerodrome and Flying School on the site now owned by Ontario Power Generation Incorporated.

Lakeview has a history of significant industrial and military presence. The Long Branch Rifle Ranges, located at the terminus of Dixie Road, on the south side of Lakeshore Road East were used as an active site for militia training and a Cadet program used by the Department of National Defence during World War I. Remnants of the bunkers today remain on the site.

In 1935, the Department of Defence purchased the site at Lakeshore Road East and Dixie Road, and Small Arms Limited, a munitions factory, opened in 1940. The company manufactured armaments, munitions and materials through the Second World War until 1974. This site is commonly known today as the Arsenal Lands. The Small Arms Inspection Building was saved from being demolished in 2009 by a community group in Lakeview.

Some early subdivisions within the Lakeview area date to the Second World War time period, War-



Figure 1: Heritage sites such as the Cawthra-Elliot Estate (left photo) and Lakeview Park School (right photo) provide a glance of Lakeview's past when it was once a village community.



time houses were built to alleviate the housing shortage for workers at the munitions factory and for returning veterans.

The many heritage sites provide a glance of Lakeview's past as a village community: the Lakeview Golf and Country Club (1907), Adamson Estate (1919), Pallett-McMaster House (1911), Lakeview Park School (1923), Cawthra-Elliot Estate (1926), and a 1950s contempo gas station.

By 1950, Lakeview had transformed from a rural area into a suburban landscape with land used for industrial, residential, commercial and public spaces. New subdivisions were being built such as Orchard Heights and Applewood Acres. New businesses and industries, such as what is now Dixie Outlet Mall were established as a result of the increasing population. In 1958, the Lakeview Generating Station was erected on the Long Branch Rifle Ranges land providing electricity for the people of Ontario for 43 years before being decommissioned in 2005.

For decades, Lakeview has provided water and wastewater servicing for Mississauga and the surrounding municipalities. The Lakeview Water Treatment Plant was constructed in 1952, and the Lakeview Wastewater Treatment Plant (later renamed, G.E. Booth Wastewater Treatment Facility) opened in 1961. To date, both plants continue to play an important role in providing regional servicing needs.

### 3.0 Current Context

Lakeview is made up of stable residential neighbourhoods characterized by detached and semi-detached housing. Many homes built in the 1950s and 1960s are being renovated today or replaced largely with new detached housing, and some assembly for townhouses is occurring. Townhouses are found in pockets throughout Lakeview between Lakeshore Road East and the Queen Elizabeth Way. Apartment buildings are located mainly near the Canadian National Railway

tracks at Cawthra Road, on Dixie Road, and several are built along Lakeshore Road East. There are a few multi-unit residential dwellings in Lakeview, including duplex, triplex and quadruplex.

The area is served by commercial facilities concentrated along Lakeshore Road East. However, it is fragmented by other uses such as motor vehicle repair garages and motor vehicle sales and service. The area along Lakeshore Road East to the east of Cawthra Road is in its early stages of revitalization to mainstreet retail, with newly built and proposed mixed use buildings. Neighbourhoods to the north are served by commercial facilities located along the Queen Elizabeth Way, namely Dixie Outlet Mall and Applewood Village Plaza.

An employment area is located south of Lakeshore Road East and east of East Avenue to Applewood Creek. This area contains a mix of business uses in construction, manufacturing, retailing, and wholesaling. Remnant industrial uses continue to operate on properties south of the Canadian National Rail Line.

The site of the former Lakeview Generating Station and the current G.E. Booth Wastewater Treatment Facility, situated south of Lakeshore Road East, occupies a major portion of the Lake Ontario **shoreline**. A major utility use in the area is the Lakeview Water Treatment Facility which is bordered by several parks including A.E. Crookes Park, Lakefront Promenade Park, and Douglas Kennedy Park.

The open space system predominately consists of golf courses, natural areas, creeks, trails, and parks along the Lake Ontario waterfront. These lands are culturally and recreationally significant and connect to Mississauga's parks system.

The waterfront is one of the distinctive elements of Lakeview, and physical and visual accessibility to the waterfront is integral to the community.

Cultural and heritage resources include heritage buildings, cultural landscapes associated with the scenic parks and golf courses, Lakeview's industrial

**Figure 2:**  
**LAKEVIEW AREA STATISTICS**

	North Residential Neighbourhood Precinct	Central Residential Neighbourhood Precinct	South Residential Neighbourhood, Lakeshore Corridor and Employment Precincts	Total Lakeview
Land Area <sup>1</sup> (ha)	261	489	397	1,147
Population <sup>2</sup>	5,230	8,320	9,200	22,750
Employment <sup>3</sup>	580	1,790	3,100	5,470

<sup>1</sup> Land area is a gross figure and includes everything within the defined boundary such as rivers, roads, and all other land uses.

<sup>2</sup> Population is adapted from Hemson Consulting, Growth Forecast, 2008. Population figures are based on the 2011 mid year forecast and include a 4.2% undercount from Statistics Canada.

<sup>3</sup> Employment figures include a work from home assumption of 31 jobs in 1,000 population, adapted from Hemson Consultants, Growth Forecast, 2008. Employment figures also include an adjustment factor for non-reporting businesses adapted from City of Mississauga, Mississauga Employment Survey, 2011. Note: Numbers have been rounded

past, former residential estates, and the Dixie Road Scenic Route.

The road network has few east-west and north-south connections through the district. The east-west linkages are Lakeshore Road East, an arterial road identified as a **higher order transit corridor**, Queen Elizabeth Way, a provincial highway, Queensway East, a regional arterial road, and Atwater Avenue, a major collector road. The north-south networks are Cawthra Road, Dixie Road, Ogden Road, and Haig Boulevard, although the latter two do not extend north of the Queen Elizabeth Way. Cawthra Road and Dixie Road are regional arterial roads, Dixie Road (Scenic Route) from Rometown Drive to Lakeshore Road East is a regional major collector road, Atwater Avenue, Ogden Avenue, the North and South Service Roads are major collector roads and Haig Boulevard and Stanfield Road are minor collector roads.

Population, employment, and land area statistics of the Lakeview area are summarized in Figure 2. For the purpose of this area plan, Lakeview is arranged by Precinct: North Residential Neighbourhood, Central Residential Neighbourhood, South Residential Neighbourhood, Lakeshore Corridor, and Employment Precinct, as shown on Schedule 1: Lakeview Local Area Plan Precincts and Sub-Areas.

## 4.0 Collaboration

This Area Plan incorporates public input provided through extensive consultation, including stakeholder interviews, visioning sessions, placemaking workshops, youth outreach exercises and an open house.

The results of the visioning process are contained in the report, "Lakeview and Port Credit Directions Report" (Directions Report). The Directions Report includes a range of policy recommendations that have informed the preparation of this Area Plan.



Figure 3: Public engagement, such as the placemaking exercise held at the Lakeview Golf Course provides valuable input into the planning process.

Preparation included the formation of a Local Advisory Panel which provided a forum for the discussion of planning and related issues.

Mississauga will ensure that consultation with the public and stakeholders on further initiatives and implementation pertaining to this Area Plan will continue.

## 5.0 Vision

The Vision for Lakeview is a connection of neighbourhoods with views to the lake and public

access to the shores and waters of Lake Ontario. The neighbourhoods of Lakeview will be connected through a network of parks and open spaces.

The Lakeview Community Node will be a focus for community activities, and will serve the surrounding neighbourhoods. With an attractive mainstreet, the node will be a prominent location to live, work, shop, and for the community to connect and gather.

Neighbourhoods in Lakeview are stable and offer a variety of housing choices. It is recognized that some change will occur, and new development should provide appropriate transition to the existing stable areas, and protect the existing character and



Figure 4: The Lakeview Vision is based on six principles.

heritage features.

The Vision and policies of this Area Plan advance the goals of the Strategic Plan, including:

- Move – directing growth to support transit;
- Belong – providing a range of housing options;
- Connect – promoting a village mainstreet;
- Prosper – encourage employment uses; and
- Green – promote conservation, restoration, and enhancement of the natural environment.

## 5.1 Guiding Principles

The Vision for Lakeview is based on the following six guiding principles that provide local context and supplements the Guiding Principles of the principal document:

**5.1.1 Reconnect Lakeview** to the waterfront by protecting view **corridors** to the lake and along the shoreline, providing a mix of uses and public access to the waterfront.

**5.1.2 Strengthen distinct neighbourhoods** by preserving heritage features, protecting established stable neighbourhoods and ensuring appropriate built form transitions for new development.

**5.1.3 Support complete communities** in the Community Node through compact, mixed use development and a pedestrian oriented mainstreet that offers a range of residential and employment opportunities.

**5.1.4 Promote community health** by encouraging public transit, cycling, walking, and the active use of parks, open spaces, and **community facilities**.

**5.1.5 Support social well-being** by providing facilities and services for a diverse population of all ages and cultures, promoting public spaces as places for social interaction, and encouraging public participation.

**5.1.6 Achieve leadership in sustainability** by supporting new development that is energy efficient and environmentally responsible, and to support the economic health, social equity and cultural vitality of Lakeview.

## 5.2 Community Concept

The focus of the policies in this Area Plan is to strengthen and revitalize the Lakeshore **corridor** for mainstreet commercial development, to preserve the character of the existing residential areas while allowing for modest infilling, and to enhance the transportation system.

This Area Plan recognizes that areas of the community are unique and serve a different purpose. The elements that make up Lakeview's community concept consist of the following:

- Green System;
- Community Node;
- Neighbourhoods;
- Employment Area; and
- **Corridors**.

For the purpose of this Area Plan, Lakeview is considered to be composed of precincts which are further divided into sub-areas that are identified on Schedule 1: Lakeview Local Area Plan Precincts and Sub-Areas. They contain a variety of land use designations and may include specific urban form guidelines for the sub-area.

### 5.2.1 Green System

The Green System consists of an interconnected open space network including, parks, golf courses, creeks, hydro corridors, school yards, and the Lake Ontario shoreline, which are all key features in the identity of the area and provide places for recreation and social interaction.

Implications of new development on the Green System are an important consideration in the review of any development application. The Green System



overlaps the Community Node, Neighbourhood, Employment Area and **Corridor** elements.

### 5.2.2 Community Node

The Community Node will be an area to accommodate future intensification. The Community Node will be the centre of both residential and employment activity for Lakeview, combining a mix of uses including residential uses, cultural activities, shopping, dining, office, commerce, and recreation. The location will be determined following the next phase of study for Inspiration Lakeview.

### 5.2.3 Neighbourhoods

Infill and redevelopment in Neighbourhoods will be facilitated and be encouraged in a manner consistent with existing land uses in the surrounding area. Neighbourhoods are considered to be primarily stable residential areas that may include a commercial centre to serve the surrounding area.

The North Residential Neighbourhood Precinct consists of two smaller neighbourhoods or sub-areas – Applewood Acres and Sherway West. These areas consist predominantly of detached housing, with several schools serving the area.

The Central Residential Neighbourhood Precinct contains large open space areas, including two golf courses and a woodlands area. The three sub-areas



Figure 5: Neighbourhoods are non-intensification areas with limited growth. New development in Neighbourhoods should fit into the existing character, respecting the existing low density and one to two storey building heights in Lakeview.

that make up this precinct are Cawthra Village, Serson Terrace, and Orchard Heights. Housing is comprised of mostly detached dwellings on deep lots, and a small number of semi-detached homes and apartment buildings. This area also contains some newer subdivisions with townhouse condominium dwellings.

The South Residential Neighbourhood Precinct, composed of the sub-areas of Lakeview West, North Beach Residential, Creekside and Lakeside Residential, contain a mix of different forms of housing including detached, semi-detached, duplexes, triplexes, quadruplexes, and townhouses. There are also apartment clusters in this area.

### 5.2.4 Employment Area

Lakeview's Employment Area is situated in southeast Mississauga. This area has various operating uses including manufacturing and wholesale, retail and restaurant, banquet hall, utility, and marina and parkland. The future use of these lands will be reviewed through the next phase of study for Inspiration Lakeview and remains unchanged in this Area Plan.

### 5.2.5 Corridors

The principal document identifies Lakeshore Road East, Cawthra Road, and Dixie Road as **corridors**. These **corridors** link together the neighbourhoods of Lakeview. **Corridors** that run through the Community Node should develop with mixed uses oriented towards the **corridor**.

Lakeshore Road East is an important **corridor** in the future development of Lakeview. This area will be strengthened by concentrating additional commercial, residential and community uses, and by improving transportation connections with the surrounding neighbourhoods.

This Area Plan identifies Lakeshore Road East (also referred to as the Lakeshore Corridor), as the Lakeshore Corridor Precinct. Although Lakeshore Corridor is a non-intensification area, the Area Plan has identified sites along the **corridor** which are appropriate for intensification.

Maintaining Lakeshore Road East as a four lane roadway during peak travel times is a transportation priority. At the same time, Lakeshore Road East is a constrained corridor that requires a context sensitive design approach. Trade-offs will be required to accommodate the envisioned multi-modal function of the corridor.

## 6.0 Direct Growth

Intensification is the development of a property, site or area at a higher intensity than what currently exists. Intensification should be consistent with the planned function for the area as reflected by the City Structure and urban hierarchy of the Plan.

The amount of intensification will vary in accordance with the policies of this Area Plan. In Lakeview, some growth is directed to the following areas: Lakeview Community Node, modest infilling in neighbourhoods, and the redevelopment of some sites along Lakeshore Road East, and other larger commercial sites. The specific manner in which new development will be accommodated is further explained in subsequent sections of this Area Plan.

### 6.1 Community Node Character Area

The yet to be established Lakeview Community Node will offer a variety of services for daily living. The Community Node is intended to serve the local community, providing a variety of commercial,

community and cultural infrastructure. The Community Node, among other things, will:

- be an area of intensification for housing, employment, culture, and recreational uses;
- have a gross density between 100 and 200 residents and jobs combined per hectare;
- have a population and employment ratio of 2:1 to 1:2;
- provide a pedestrian-oriented environment and function as a mixed use centre with compact development to serve the adjacent neighbourhoods;
- provide a variety of housing choices including affordable, assisted and special needs; and
- offer a concentration of community and cultural uses.

6.1.1 The City will monitor the gross density in the Community Node and will assess its ability to accommodate further growth through the development approval process.

### 6.2 Neighbourhood Character Areas

Neighbourhoods are stable areas, primarily residential in nature, and not expected to experience significant change. Where **corridors** traverse through Neighbourhoods, intensification may occur along **corridors** where appropriate.



Figure 6: The Lakeshore Corridor has redevelopment potential. Development that provides a continuous street frontage, enhances the **streetscape** through landscaping, and promotes an active pedestrian environment will be encouraged.

6.2.1 Intensification will be through modest infilling, redevelopment along the **corridors**, or on commercial sites.

6.2.2 Neighbourhoods are encouraged to provide a variety of housing forms to meet the needs of a range of household types.

6.2.3 Intensification will be sensitive to the existing character of the residential areas and the planned context.

### 6.3 Lakeshore Road Corridor

The principal document identifies Lakeshore Road East through Lakeview as a **Higher Order Transit Corridor**. This **corridor** will accommodate multi-modal transportation facilities and a mix of commercial, office, residential and cultural uses.

The Lakeshore Corridor crosses through the Neighbourhood and Employment Character Areas. It generally includes the properties that front onto Lakeshore Road East.

Development in the Lakeshore Corridor Precinct should have regard for the character of the Neighbourhoods, providing appropriate transitions in height, built form, and density.

6.3.1 Intensification will occur through infilling or redevelopment.

6.3.2 Intensification will be sensitive to the existing and planned context of the **corridor** and adjacent residential uses.

6.3.3 Intensification will address matters such as:

- a. contribution to a complete community;
- b. contribution to the mainstreet character;
- c. respecting heritage; and
- d. protecting views to the waterfront.



Figure 7: Trees provide many benefits to the community including improving air quality, protecting our water, conserving energy, and providing aesthetic benefits.

## 7.0 Value the Environment

Reconnection to the waterfront and environmental sustainability are guiding principles of this Area Plan. The Green System is an important element in the Community Concept; it is located within a variety of land use designations in Lakeview.

Schedule 1A of the principal document identifies elements of the Green System:

- Natural Areas System;
- **Natural Hazard Lands**, and
- Parks and Open Space.

Schedule 3 of the principal document identifies the Natural System, which includes Natural Areas and Natural Hazards. Schedule 4 identifies Parks and Open Spaces.

In Lakeview, the Natural Areas System includes the Cawthra Woods, which is one of a few large forested areas remaining in Mississauga. It is identified as a Provincially Significant Wetland, a Regional Area of Natural and Scientific Interest (ANSI) and an Environmentally Significant Area (ESA). The area contains a diversity of plant species and is known as a migratory stopover.

The Natural Hazards are associated with features such as Lake Ontario Shoreline and five **watercourses**: Etobicoke Creek, Applewood Creek, Serson Creek, Cawthra Creek and Cooksville Creek.

City parks and open spaces, such as golf courses, are places for social interaction and recreation. Waterfront Parks, which serve a city and regional wide function, are also an important component of the Parks and Open Space System. Lakeview contains seven waterfront parks: Park 358 (Not yet named) on the Arsenal property, Lakeview Park, Douglas Kennedy Park, Lakefront Promenade, A.E. Crookes Park, R.K. McMillan Park, and Adamson Estate.

Trees are invaluable and are essential to our quality of life. Protecting and enhancing the urban forest will leave a legacy for future generations.

Hydro corridors can provide linear connections through neighbourhoods. In Lakeview, there are three hydro corridors. The “Queensway Corridor” is generally adjacent to the Queensway East, the “Applewood Junction Corridor” extends diagonally, north of the Queen Elizabeth Way, and the “Lakeview/Haig Junction Corridor” extends from the former Lakeview Generating Station north to the Queen Elizabeth Way.

## 7.1 Green System

7.1.1 Mississauga will give priority to actions that protect, enhance, and restore the Green System in Lakeview.

7.1.2 Opportunities to create an interconnected network of open spaces, including parks, trails, and bicycle facilities, with linkages to the surrounding areas will be encouraged.

7.1.3 Within the waterfront parks system, the protection, preservation, and restoration of existing natural systems will be prioritized and balanced to direct and guide the planning of existing and future waterfront activities.

7.1.4 Through the processing of development applications, lands may be acquired to provide for a continuous Waterfront Trail along the water’s edge.

7.1.5 The development of Park 358 (Not yet named) on the Arsenal property and the management of R.K. McMillan Park will include naturalization techniques, where appropriate.

## 7.2 Urban Forest

7.2.1 Trees provide important environmental benefits and contribute to the character of the area. Improvements to the urban forest are encouraged.

7.2.2 The City may require **streetscape** improvements along **corridors** to expand and enhance the urban forest canopy along the public right-of-ways.

7.2.3 Improvements to the tree canopy along Lakeshore Road East will be a priority when undertaking **streetscape** improvements.

## 7.3 Living Green

7.3.1 Mississauga will encourage partnerships and stewardships in order to improve the health of the Green System.

7.3.2 The City will support initiatives that promote environmental sustainability, integrity and conservation and incorporate sustainable development practices in accordance with the City’s Green Development Strategy and the Water Quality Control Strategy.

## 8.0 Complete Communities

Lakeview contains many of the attributes associated with complete communities, including among other things:

- recreational facilities such as **community facilities**, parks, and golf courses;





Figure 8: Lakeview has few seniors' facilities and housing, and as an increasing number enter retirement there will be a demand for more.

- schools as central places in the community, for academic, social and community, service, and family support;
- a range of housing options with a mixture of housing forms and densities, including rental housing;
- cultural resources such as heritage buildings and landscapes which help retain a connection to the past; and
- a significant waterfront area providing a high level of public accessibility.

The policies are intended to protect and enhance the attributes of a complete community for Lakeview.

Lakeview is a community with a growing aging population. As seniors enter retirement, there will be changing residential needs, and providing housing choices for seniors, along with other additional **community facilities** and services such as community centres, libraries and medical offices, will help meet some of the needs for daily living.

The future Community Node and the Lakeshore Corridor will provide a mix of uses and services to residents in the adjacent neighbourhoods.

## 8.1 Housing

8.1.1 Preservation of existing affordable housing will be a priority. Where development applications are proposing the removal of existing affordable

housing, the replacement of these units will be encouraged on site or within the community.

8.1.2 The Community Node and Lakeshore Corridor are encouraged to develop using a range of housing choices in terms of type, tenure and price.

8.1.3 Mississauga will encourage the provision of affordable housing, including rental housing and seniors' housing within the Community Node and in the Lakeshore Corridor.

## 8.2 Cultural Heritage

Lakeview contains a number of important cultural heritage resources including properties identified or recognized on the City's Heritage Register as being Designated or Listed. The Heritage Register further classifies the sites as a cultural feature or a cultural landscape:

- Cultural features such as the Water Tower on the Arsenal Lands, Bowstring Bridge over the Etobicoke Creek, and Lakefront Promenade provide scenic views and are significant features and landmarks; and
- Cultural landscapes such as the Lakeview Golf Course, Cawthra Estate, and Adamson Estate are associated with the natural environment or are significant historical resources.

8.2.1 Cultural heritage sites are places that have the opportunity to provide attractive **streetscape**.



Figure 9: Entryways are emphasized with distinctive design and landscaping.

**Streetscape** improvements are encouraged to accentuate the site through landscaping, signage, lighting, benches or other means.

8.2.2 The City will explore through a Community Improvement Plan, incentives to protect, preserve, and reuse cultural heritage sites in Lakeview.

8.2.3 Development adjacent to heritage sites should integrate and enhance the character of the cultural heritage resource.

### 8.3 Community and Cultural Infrastructure

8.3.1 The Community Node and Lakeshore Corridor are preferred locations for community and cultural infrastructure.

8.3.2 Community infrastructure is encouraged to have integrated uses, combining uses such as a library, neighbourhood gallery space, and a café. To promote social interaction within the community, the provision of meeting rooms should be considered, such as a library with multi-functional meeting space.

8.3.3 Mississauga will encourage partnerships and collaboration with the local community, arts organizations and creative enterprises to further develop the cultural aspects in Lakeview, including the adaptive reuse of buildings as a community or cultural heritage resource.

## 8.4 Distinct Identity and the Waterfront

Lakeview has a long and important history as a waterfront community; the waterfront has created a distinct identity for Lakeview. Lakeview has opportunities to develop a strong community character that promotes and connects the neighbourhoods and **corridors** to the waterfront.

8.4.1 Development within the Community Node and the Lakeshore Corridor will be encouraged to create a unique identity by providing distinctive architecture, high quality public art, **streetscape**, and cultural heritage resources.

8.4.2 The distinct identity of the existing Neighbourhoods will be maintained by preserving the scale and character of the built environment.

8.4.3 The City, in collaboration with Ontario Power Generation Incorporation, the Region of Peel and other levels of government, will pursue the acquisition of waterfront lands for public use, including a waterfront trail connection along the water's edge.

8.4.5 To create a visual identity in Lakeview, gateway features and signage along Lakeshore Road East will be promoted.

8.4.6 Mississauga supports water dependent activities and related employment uses such as



Figure 10: Public art expresses a community's positive sense of identity and value. Carvings found along the Waterfront Trail through Lakeview enhance the sense of place and pride.

marinas, facilities in support of recreational boating and sport fishing, and uses that benefit from being near the shoreline, parks and the Waterfront Trail.

8.4.7 Mississauga waterfront parks are a significant element of the Lakeview character. Planning for the waterfront parks system will be guided by the Waterfront Parks Strategy, 2008.

## 9.0 Multi-Modal City

As population and employment growth is anticipated in the Lakeshore Corridor and within the future Community Node, increasing constraint will be placed on the transportation network. Planning for transit and **active transportation** along Lakeshore Road will become a priority to ensure efficient movement for multi-modal travel.

The road network is shown on the Lakeview Local Area Plan Long Term Road and Transit Network (Schedule 3). Tables 8-1 to 8-4 and Schedule 8 (Designated Right-of-Way Width) of the principal document identify the basic road characteristics. The long-term multi-modal transportation system is shown on Schedule 5 (Long Term Road Network), Schedule 6 (Long Term Transit Network), and Schedule 7 (Long Term Cycling Network) of the principal document.

A future **higher order transit corridor** along Lakeshore Road East is identified on Schedule 3, extending from Hurontario Street to the City of Toronto border. The preferred transit solution (e.g. bus or rail) has not yet been identified for this **corridor**.

Planning for improvements to the road network and active transportation routes that provide connectivity for transit, pedestrians, cyclists and vehicles will be essential in linking Lakeview to the neighbouring communities and in achieving a city-wide finer grain network.

The City's Cycling Master Plan is a guide to promote active transportation and identifies existing and planned trails in Lakeview, including municipal

connections such as the trail along the Etobicoke Creek.

Opportunities for passive recreational uses along hydro corridors, such as multi-use trails, are encouraged through Lakeview.

The City is proposing to undertake a Lakeshore Road Transportation Master Plan ("Transportation Master Plan") which will include Lakeshore Road (between the east and west City limit) and an examination of the transportation issues specific to the Lakeview area. As part of the future study, the City will review the higher order transit need in the Lakeview area and opportunities to improve the transportation system for all modes.

### 9.1 Corridor Protection

Appendix 2 contains the executive summary from the Lakeshore Road Transportation Review Study. That study provided a comprehensive and technical transportation review regarding how Lakeshore Road East can accommodate alternative modes of transportation.

9.1.1 Lakeshore Road East will be planned to accommodate, to the extent possible, all modes of transportation, including pedestrian facilities, cycling facilities, and **higher order transit** facilities.

9.1.2 The City may acquire lands for a public transit right-of-way along Lakeshore Road East where the creation of a public transit right-of-way separate from, adjacent to, or in addition to, a road right-of-way is deemed appropriate.

9.1.3 The City will, through the review of development applications, eliminate and/or consolidate vehicular turning movements to and from Lakeshore Road East and direct traffic towards signalized intersections, where appropriate. Vehicle access to redevelopment opportunities should be considered from existing north-south side streets or existing or proposed laneways parallel to Lakeshore Road East.

9.1.4 The Transportation Master Plan for Lakeshore Road may address improving current



mobility for all modes of transportation, the implications of future growth on the network and consider placemaking initiatives that would promote the animation of the corridor. The Transportation Master Plan may assess improvements to the Lakeview road network and higher order transit needs in the Lakeview area.



Figure 11: Lakeshore Road Corridor has opportunities to accommodate multi-modes of transportation including pedestrian, cycling, and **higher order transit** facilities.

## 9.2 Road Network

9.2.1 During the review of development applications, consideration will be given to the manner in which development proposals provide additional public roads in order to continue the existing fine-grained street network.

9.2.2 During the review of development applications, streets will be designed to provide connectivity through neighbourhoods.

9.2.3 Improvements to the road network and active transportation routes that provide connectivity and a fine grain network through Lakeview may be identified through a future Transportation Master Plan for the Lakeshore Road Corridor or through the development applications process. Improved connections will provide pedestrian, cyclists and vehicles a greater variety of routes and accessibility within the area. Potential opportunities for network

improvements include but are not limited to the following:

- a. Byngmount Avenue, east to Rangeview Road;
- b. Casson Avenue, east to St. Mary's Avenue;
- c. Ebony Avenue, east to First Street;
- d. Ella Avenue, east to St. James Avenue;
- e. First Street, east to Ella Avenue;
- f. Fourth Street, east to Haig Boulevard;
- g. Harcourt Crescent, south to Duchess Drive;
- h. Pelham Avenue, west to Alexandra Avenue, and east to Haig Boulevard;
- i. Seventh Street, east to Halliday Avenue; and
- j. Third Street, east to Casson Avenue.

9.2.4 When reviewing the appropriateness of potential road connections, the City will consider the volume and type of traffic that will be accommodated on the road.

9.2.5 Mississauga will work with the Province and the Region to coordinate and make improvements to the Dixie Road and Queen Elizabeth Way interchange for the realignment of Dixie Road.

9.2.6 Development applications will be accompanied by transportation and traffic studies. Studies will address, among other things, strategies for limiting impacts on the transportation network, where appropriate, including measures such as:

- reduced parking standards;
- transportation demand management;
- transit-oriented design of the development;
- pedestrian/cycling connections; and
- access management plan.

## 9.3 Transit Network

9.3.1 Providing public transit connections along key north-south corridors (e.g., Cawthra Road, Ogden Avenue and Dixie Road) to Lakeshore Road East will be a transit priority and will help shape and support the future development of Lakeshore Corridor and the Community Node.

## 9.4 Parking and Transportation Demand Management

9.4.1 For development in the Lakeshore Corridor, parking is encouraged to locate below grade, or at the rear of the site.

9.4.2 Development with surface parking should be screened from adjacent streets and properties by using landscaping materials such as hedges, shrubs, trees, and other structural materials.

9.4.3 The City will identify appropriate locations for on-street parking in the Lakeshore Corridor and develop an implementation strategy for parking.

9.4.4 Where on-street parking and lay-by parking can be accommodated, it is to be incorporated into the **streetscape** design.

9.4.5 Reduced parking requirements and maximum parking standards may be considered within:

- a) the Community Node; and
- b) the Lakeshore Corridor.

9.4.6 Public parking lots in the Lakeshore Corridor will be maintained and supplemented where appropriate.

9.4.7 The City will encourage Transportation Demand Management measures, where appropriate, in the Lakeshore Corridor and as a part of any significant redevelopment projects outside of the **corridor**.

## 10.0 Desirable Urban Form

This section reflects the planned function and local context and provides the framework for shaping the Lakeview community, with an emphasis on urban form. The urban form policies are organized as follows:

- a) Neighbourhoods;
- b) Employment;
- c) Lakeshore Corridor;
- d) Community Node; and
- e) Built Form.

Development will be guided by the Lakeview Built Form Standards contained in Appendix 1.

Development will be in accordance with the minimum and maximum height limits as shown on Schedule 2. The appropriate height within this range will be determined by the other policies of this Area Plan.

### 10.1 Neighbourhoods

Neighbourhoods are stable residential areas where the existing character is to be preserved and enhanced. Development may occur through modest infilling or redevelopment of existing commercial plazas and vacant sites.

Neighbourhood policies are intended to reflect a number of objectives, including among other things:

- to ensure development is sensitive to the existing low rise context and reinforce the planned character of the area;
- to ensure Lakeshore Road East will provide appropriate development and public realm that reinforces its planned role as a connected community and fosters an active pedestrian and cycling environment; and

- to appropriately balance the constraints associated with both the Canadian National Railway line and adjacent residential uses.

10.1.1 New development is encouraged to reflect 1 to 2 storey residential building heights and should not exceed 3 storeys.

#### **10.1.2 North Residential Neighbourhood Precinct (Applewood Acres, Sherway West)**

The North Residential Neighbourhood Precinct is bordered by Cawthra Road to the west and the Etobicoke Creek to the east, Queensway East to the north and the Queen Elizabeth Way to the south. This residential area will be maintained while allowing for infill which enhances and is compatible with the character of the area.

10.1.2.1 The existing commercial site at North Service Road and Stanfield Road is a site where intensification could be accommodated to a maximum height of 4 storeys. Future redevelopment of the properties will address issues, including but not limited to:

- retention of commercial space; and
- appropriate transition to adjacent residential uses.

#### **10.1.3 Central Residential Neighbourhood Precinct (Cawthra Village, Orchard Heights, Serson Terrace)**

The Central Residential Neighbourhood Precinct is bordered by Cawthra Road to the west and Etobicoke Creek to the east, the Queen Elizabeth Way to the north and the Canadian National Railway line to the south. Originally, these homes were summer houses for families from the Toronto Township. Also predominant in the area are homes built on small lots that were constructed during the Second World War time period (these homes are also known as “war time housing”).

10.1.3.1 The existing commercial site at South Service Road and Dixie Road represents a site for potential intensification, to a maximum height of 4

storeys. Future redevelopment of the property will address issues including but not limited to:

- the addition of public roads to connect and improve the neighbourhood’s fine grain road pattern;
- retention of commercial space; and
- appropriate transition to adjacent residential uses.

#### **10.1.4 South Residential Neighbourhood Precinct (Creekside, Lakeside Residential, Lakeview West, North Beach Residential)**

The South Residential Neighbourhood Precinct is bordered by Seneca Avenue to the west and Etobicoke Creek to the east, the Canadian National Railway line to the north and Lake Ontario to the south. Many of the homes constructed dating to World War II exists today.

10.1.4.1 Lakeview West has potential for **intensification**, particularly on the lands east and west of Cooksville Creek. Development should, among other matters, address the following:

- ensure transition to Lakeshore Road East, adjacent stable residential neighbourhoods, and Cooksville Creek;
- ensure visual connection to Cooksville Creek; and
- ensure larger sites are sub-divided with public roads and walkways.

10.1.4.2 Development along the Canadian National Railway line for lands designated Business Employment, or for an industrial use, will:

- be encouraged to improve the transition of height and use to adjacent residential areas;
- have a maximum height generally equivalent to a 2-storey residential building;
- ensure lighting, noise levels, loading and garbage areas do not negatively impact adjacent residential areas;



Figure 12: The Small Arms Inspection Building has historical significance, defining Canada's military history during the Second World War and the history of women.

- d. provide appropriate landscaped buffers to adjacent residential areas; and
- e. provide a **streetscape** that is compatible with adjacent neighbourhoods.

## 10.2 Employment

The Employment Precinct is divided into three sub-areas as identified on Schedule 1. It contains the areas of Arsenal Woodlands, Ontario Power Generation Lands (OPG Lands), and Lakefront Business Park.

### 10.2.1 Arsenal Woodlands Precinct

In accordance with the Waterfront Parks Strategy, Park 358 (Not yet named) but historically known as the Arsenal property, is identified as a Gateway Park. A park master plan will be required to determine the programming and design of the park. Also, in partnership with the City, the Toronto and Region Conservation Authority, and local community groups, a plan has been initiated for the re-adaptive use of the Small Arms Inspection Building for varying cultural, educational, retail, and office purposes. The reuse of the building will provide an important resource for the Lakeview community.

10.2.1.1 The Arsenal Lands should provide for placemaking opportunities, such as the development of a square or open space that integrates with the surrounding environment and provides interaction for pedestrians.

10.2.1.2 Development of community and cultural uses will be encouraged and public art will be incorporated as appropriate.

### 10.2.2 Ontario Power Generation (OPG) Lands Precinct and Lakefront Business Park Precinct

The OPG Lands were formerly used for a coal-fired electricity generating station. This area represents a significant parcel of land along the Lake Ontario shoreline, with the opportunity to create a vibrant mixed use destination, enhancing the existing community and improving access to the waterfront.

The Lakefront Business Park sub-area includes a regional wastewater treatment facility and an employment area on the south side of Lakeshore Road East, overlapping with the Lakeshore Corridor Precinct. The employment lands and the OPG Lands are under review through the next phase of study for Inspiration Lakeview.

Phase one of the Inspiration Lakeview study (Inspiration Lakeview Vision), initiated by the City, outlines the desired vision for the OPG Lands and the employment area, as guided by the community.

The next step in this process is a comprehensive master plan to further develop the vision. The precinct policies and special site policies provide additional direction regarding issues that need to be addressed.

10.2.2.1 The next phase of study for Inspiration Lakeview should have regard for the following:

- a. form and scale of development will be appropriate to the waterfront location;
- b. provide opportunities for new view corridors;
- c. creation of public roads, parks, walkways, and paths within the site and to enhance connectivity to the neighbourhoods to the north and to the west, and along the waterfront;
- d. provision of public open space along the shoreline, and a continuous Waterfront Trail to maximize public access to and along the Lake Ontario waterfront; and
- e. for the lands along Lakeshore Road East, the Lakeshore Corridor Precinct policies in this Area Plan will also apply.

### 10.3 Lakeshore Corridor

The Lakeshore Corridor Precinct is intended to be the primary area for street-related commercial development, with a mixture of uses and pedestrian-oriented built form. The extent of the Lakeshore Corridor is from Seneca Avenue to the east end of the municipal boundary at Etobicoke Creek. Given the length of this **corridor**, it is divided into sections: the Core and Outer Core (see Schedule 1). The Core is from Seneca Avenue to Hydro Road and is envisioned to have a concentration of street-related commercial uses. The Outer Core, from Hydro Road to Etobicoke Creek, is to be a pedestrian-friendly area. Similar to the Core, it allows for mixed-use development, however, commercial uses are not required.

In order to achieve the intended function of the Lakeshore Corridor Precinct, redevelopment will address among other matters, the following:

- creating a pedestrian-oriented environment;
- ensuring built form compatibility and transition in heights to adjacent neighbourhoods;
- minimizing access points along Lakeshore Road East;
- preserving light and sky views; and
- creating an attractive public realm.

10.3.1 Development should preserve and enhance the views and vistas to the natural environment.

10.3.2 The City will seek opportunities for views to Lake Ontario through development applications for new north-south roads and road extensions, including among others, Ogden Avenue and Hydro Road.

10.3.3 Development will be encouraged to locate parking to the rear of buildings or underground.

10.3.4 Development along Lakeshore Road East is encouraged to be 2 to 4-storeys in height; however, some sites will be permitted building heights greater than 4 storeys as shown on Schedule 2.

10.3.5 Appropriate transition to adjacent low density residential will be required.

10.3.6 To promote a pedestrian-friendly mainstreet environment, street related commercial uses will front onto and be located along Lakeshore Road East. New development should address the following, among other items:

- a. maintaining an appropriate average lot depth for mainstreet commercial;
- b. buildings should be closely spaced with minimal breaks to ensure a continuous building or street frontage;



- c. buildings should incorporate active uses at grade, in order to animate the public realm and pedestrian environment; and
- d. building entrances should be located along and face Lakeshore Road East, and should be clearly identifiable with direct access from the sidewalk.

10.3.7 Development will provide an appropriate **streetscape** treatment of the public realm that supports pedestrian activity and provides an attractive character to the street. This may include, among other things:

- a. landscaping and planting;
- b. street furnishings;
- c. public art features;
- d. quality building materials; and
- e. building design elements and features including articulated rooflines such as parapets and towers.

10.3.8 Development will be encouraged to provide placemaking opportunities, such as public squares, plazas, and open spaces, including among other locations, at Cooksville Creek, Cawthra Road, East Avenue, Alexandra Avenue, Ogden Avenue, Hydro Road, Dixie Road, and Etobicoke Creek.

10.3.9 The assembly of adjacent low density residential land to enlarge properties fronting Lakeshore Road East is discouraged. Should assembly occur, however, the primary purpose of these lands will be an enhanced landscape buffer to the adjacent residential uses and for amenity space and/or parking if required through the development.

10.3.10 The Intensification Areas policies of the Plan will apply to development within the Core area.

10.3.11 Single use residential buildings are permitted in the Outer Core area, subject to the following:

- a. buildings are set back from the street;
- b. provision of a well-landscaped front yard;
- c. an appropriate **streetscape**; and
- d. parking at the rear of the property or underground.

## 10.4 Community Node

As part of the Urban System, the principal document includes a Community Node for the Lakeview area in southeast Mississauga.

The Community Node boundaries and specific urban form policies will be determined through the next phase of study for Inspiration Lakeview.

Development within the Community Node is intended to contain a mix of uses, densities, and high urban design standards, and have regard for the following objectives, among other things:

- a. building heights will reflect an appropriate transition to the stable low density neighbourhoods, the Lakeshore Corridor, and Lake Ontario shoreline;
- b. provision of public access and protection of views to Lake Ontario;
- c. providing a variety of heights;
- d. overall development of the Community Node will be at a scale that reflects its role in the urban hierarchy;
- e. provision of public art that enhances the built environment and enriches the culture and history of the community; and
- f. the development of symbolic gateways or landmarks to define entrance to and arrival at the Community Node will be encouraged.

## 10.5 Built Form Types

10.5.1 For the development of detached, semi-detached, duplex and triplex dwellings, the following will be addressed, among other things:

- a. new housing within Lakeview should maintain the existing character of the area; and
- b. new development will fit the scale of the surrounding area and take advantage of the features of a particular site, such as topography, contours, and mature vegetation.

10.5.2 Criteria for the development of street townhouses or freehold townhouses will include, among other things:

- a. they fit into the existing lotting pattern of the community;
- b. they provide an appropriate transition from low built form to higher built forms; and
- c. they are located on, or in proximity to transit routes.

10.5.3 For the development of standard and common element condominium townhouse dwellings, the following will be addressed, among other items:

- a. they can fit into the existing lotting pattern of the community;
- b. they provide an appropriate transition from low built form to higher built forms;
- c. they have an appropriate minimum lot depth to accommodate elements such as landscaping and parking; and
- d. they are located on, or in proximity to transit routes.

10.5.4 Horizontal multiple dwellings, such as stacked townhouses, may be developed, subject to, among other things:

- a. a minimum lot depth to ensure internal circulation;

- b. area to accommodate appropriate parking, amenity space, landscaping;
- c. utilities can be accommodated internal to the site; and
- d. they are located on, or in proximity to transit routes.

10.5.5 Criteria for apartment development will include, among other things:

- a. a minimum separation distance to ensure light and permeability;
- b. a maximum floor plate to ensure minimal impact on low density residential areas; and
- c. transition to adjacent lower built forms.

10.5.6 Criteria for commercial development will include, among other things:

- a. the maximum height of buildings will be 4 storeys;
- b. transition to existing stable residential areas;
- c. ensure the continuation of a mixed use community;
- d. on sites with multiple commercial buildings, development will provide dedicated pedestrian walkways to building entrances, and to public roads and transit routes, where appropriate; and
- e. a coordinated character will be required on sites with multiple commercial buildings, including the use of similar building materials, colours and architectural elements.

10.5.7 The redevelopment of existing industrial sites for industrial uses will address, among other things:

- a. a minimum setback to residential areas ensuring an appropriate buffer area can be accommodated to screen the use;
- b. a minimum landscape area will be required in front of any employment use; and
- c. existing industrial uses along the railway tracks are encouraged to improve their transition to the adjacent residential areas. The maximum permitted height will be the equivalent to a 2 storey residential building. New development will include appropriate buffers and ensure lighting, noise levels, loading and garbage areas do not negatively impact adjacent residential uses.

10.5.8 High quality building materials will be required in new developments. The first 4 storeys will be of durable material such as brick or stone. Concrete blocks or painted concrete blocks are not permitted to be exposed.

## 11.0 Strong Economy

Employment uses are an important component of Lakeview as they contribute to a mix of uses while providing an opportunity for people to live and work in the community.

Employment in Lakeview is generally located along Lakeshore Road East, the railway corridor and on commercial and community infrastructure sites. Lakeview also has an important and unique employment function with utility operations, such as the water treatment facility and the wastewater treatment facility, that serve a regional population.

The future Community Node will be an area for population and employment growth, ensuring an appropriate balance while creating a vibrant community focus.

Lakeview's Employment Area is located on the south side of Lakeshore Road East, between East Avenue and Applewood Creek. Through the next phase of study for Inspiration Lakeview, the City will be initiating a Phase Two municipal comprehensive review to determine the appropriate land uses in this area. Following this review, revised policies will be incorporated into this Area Plan.



Figure 13: Lakeview has a wide range of built environments, which are reflected in the Area Plan policies.



Figure 14: Providing a range of employment opportunities will help attract new businesses that contribute to a healthy employment base.

11.1 Lands designated Business Employment will provide for the continued operation of employment uses. Development on adjacent sites will be compatible to employment uses.

11.2 Opportunities for water recreational uses or waterfront retail commercial activities should be considered in the development of the waterfront.

## 12.0 Land Use Designations

This section provides additional policy direction on the permitted land uses within this Area Plan. Schedule 4: Lakeview Local Area Plan Land Use Designations identifies the uses permitted and is to be read in conjunction with the other schedules and policies in the Plan.

### 12.1 General

12.1.1 Residential buildings legally constructed prior to the approval date of this Area Plan are permitted.

### 12.2 Residential Medium Density

12.2.1 Notwithstanding the Residential Medium Density policies of the Plan, the following additional use may be permitted:

- a. low-rise apartment dwellings.

### 12.3 Mixed Use

12.3.1 Notwithstanding the Mixed Use policies of the Plan, the following policies applies to the Lakeshore Corridor Precinct – Core area:

- a. motor vehicle rental is not permitted; and
- b. commercial will be required at grade.

12.3.2 Notwithstanding the Mixed Use policies of the Plan, the following policy will apply in the Lakeshore Corridor Precinct – Outer Core area:

- a. residential uses may be permitted on the ground floor.

### 12.4 Business Employment

12.4.1 Notwithstanding the Business Employment policies of the Plan, the following uses will not be permitted:

- a. adult entertainment establishments;
- b. animal boarding establishments which may include outdoor facilities;
- c. body rub establishments;

- d. cardlock fueling dispensing facility;
- e. motor vehicle body repair facilities;
- f. motor vehicle commercial uses;
- g. motor vehicle rental;
- h. transportation facilities;
- i. trucking terminals; and
- j. **waste processing stations** or **waste transfer stations** and composting facilities.

12.4.2 Notwithstanding the Business Employment policies of the Plan, the following policies will apply:

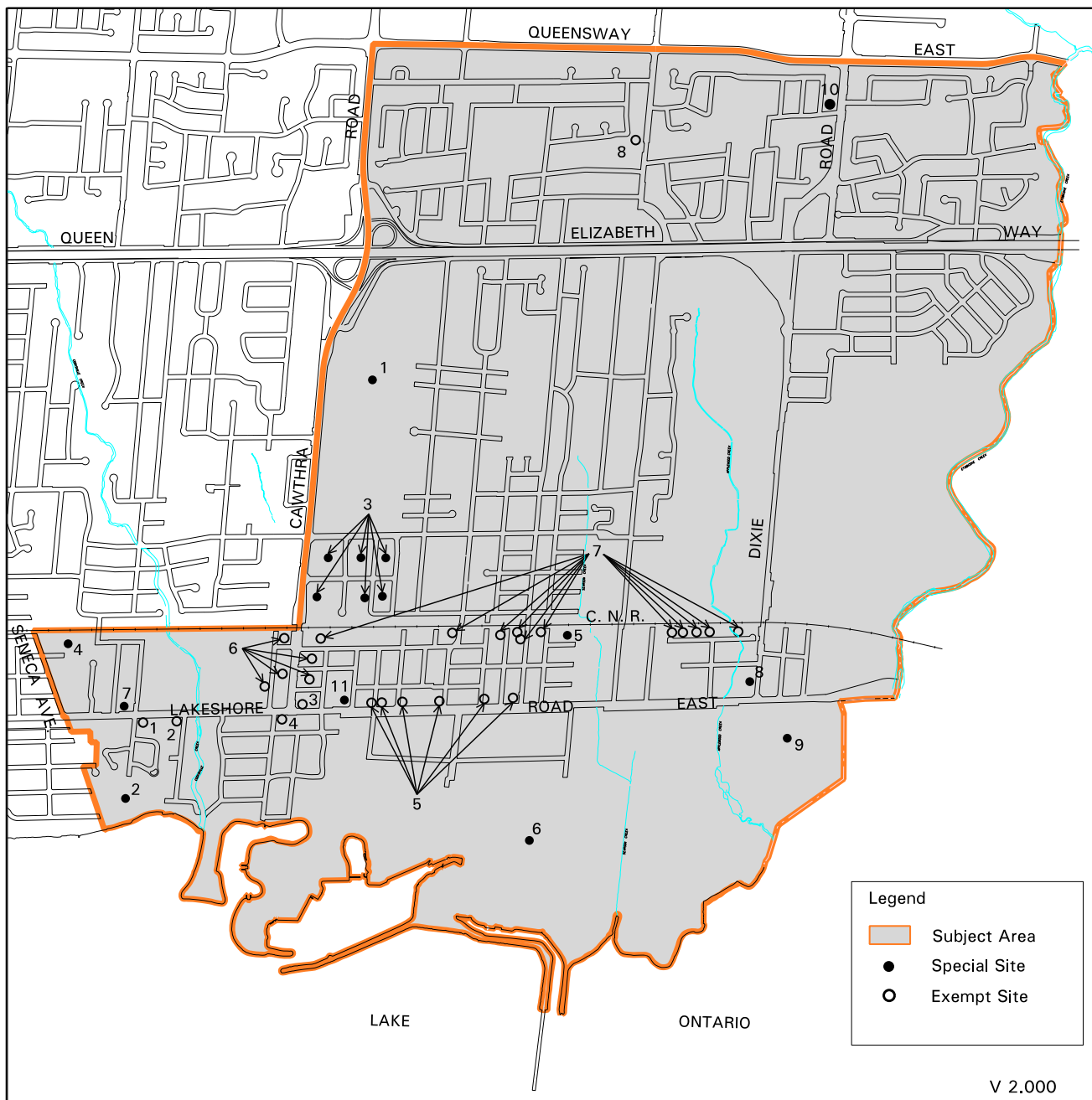
- a. permitted uses will operate entirely within enclosed buildings.



## 13.0 Special Site and Exempt Site Policies

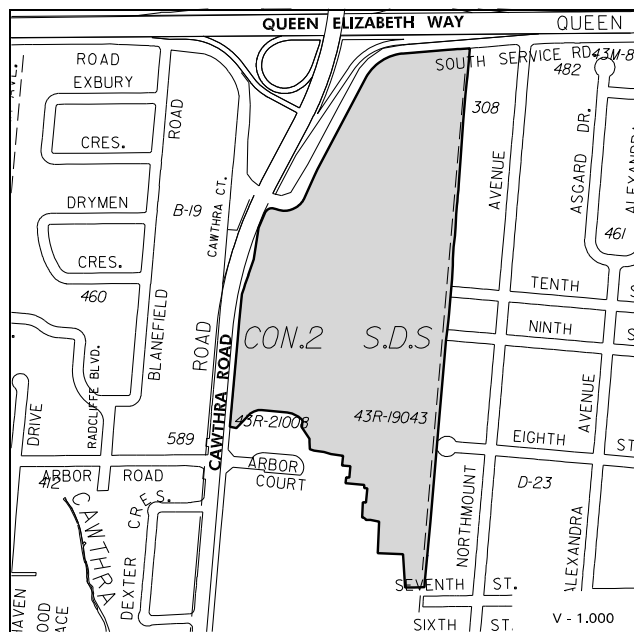
### 13.1 Special Sites

There are sites within Lakeview, as shown in Map 2, that merit special attention and are subject to the following policies.



Map 2: Location of Special Sites and Exempt Sites within the Lakeview Local Area.

### 13.1.1 Special Site 1



The lands identified as Special Site 1 are east of Cawthra Road and south of the South Service Road.

The lands are known as the Cawthra Woods and comprise both heritage and natural areas. The Cawthra Estate is located within the Cawthra Woods and includes the Cawthra-Elliot Estate House and formal gardens on the north and south sides of the estate house, the walled garden, and sugar maple forest, all of which form a cultural heritage landscape which is designated under the *Ontario Heritage Act*. The site contains a Provincially Significant Wetland, an **Environmentally Significant Area (ESA)** as identified by Credit Valley Conservation, and a Regionally Significant Area of Natural and Scientific Interest (ANSI).

13.1.1.1 Notwithstanding the Public Open Space designation, the following additional uses may be permitted in the Cawthra-Elliot Estate House:

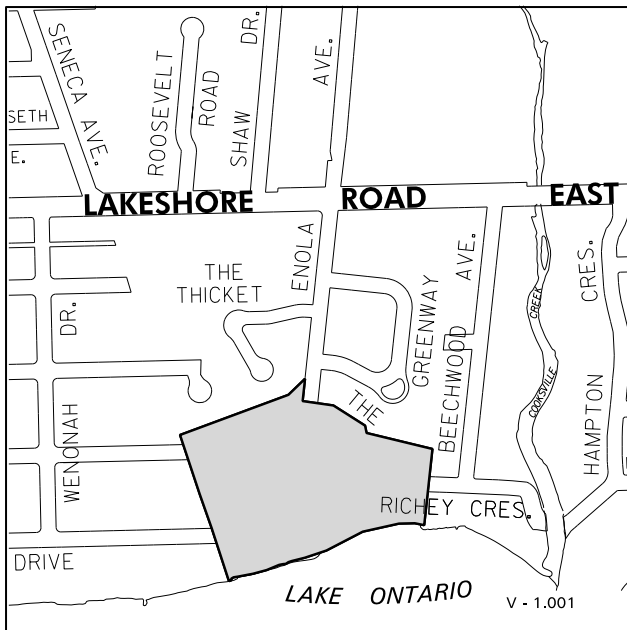
- community or cultural infrastructure, including an academy for the performing arts;
- secondary offices**;
- a conference centre;
- art gallery or studio; and

- commercial school that may include a business school, driving school, dance school, music school, arts school, crafts school or a martial arts school, but shall not include a trade school or a private school.

13.1.1.2 Development of the Cawthra-Elliot Estate House will address the following:

- the Environmental Policies of this Plan;
- retention of the existing forest in a natural condition in accordance with an approved Cawthra Woods Management and Implementation Plan;
- an approved Parks Master Plan;
- preservation and rehabilitation of the Cawthra-Elliot cultural heritage landscape, including the existing house, outbuildings and formal gardens. Any minor development or minor site alterations will be limited in size and to a design compatible with the historically designated structure, will be designed and located in a manner that minimizes removal of forest cover and will be subject to completion and circulation of an Environmental Impact Study (EIS) and site plans to the Region of Peel and Credit Valley Conservation; and
- a heritage permit, including a **Heritage Impact Assessment**, will be required for any alterations.

### 13.1.2 Special Site 2



The lands identified as Special Site 2 known as the Adamson Estate and Derry Property, are located along the shore of Lake Ontario, east of Cumberland Drive and south of Enola Avenue.

13.1.2.1 Notwithstanding the provisions of the Public Open Space and Greenbelt designations and the Natural Area classification of this Plan, the following additional uses may be permitted:

- a. **secondary offices;**
- b. a conference centre; and
- c. community or cultural infrastructure, including an academy for the performing arts.

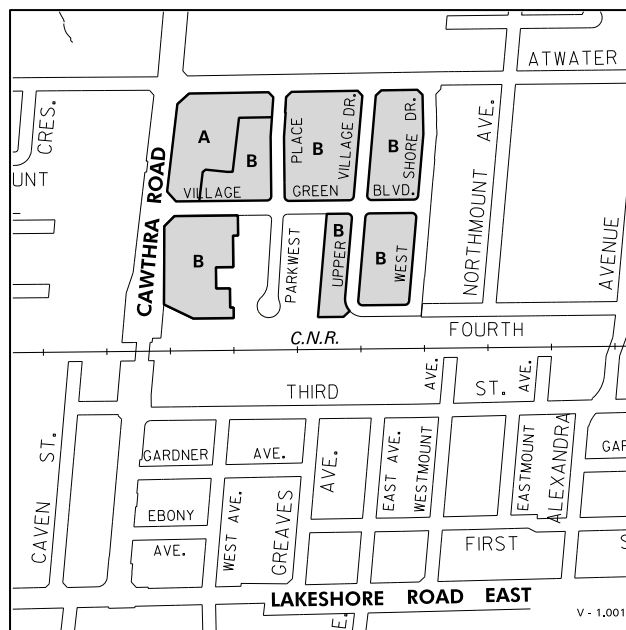
13.1.2.2 Development of this site will address, among other things, the following:

- a. restoration and preservation of the historically designated main house, gatehouse, barn, Derry House, and pet cemetery;
- b. maintenance of public access along the waterfront as well as the grounds of the estate;

- c. enhancement of recreation opportunities throughout the balance of Lakefront Promenade Park;
- d. recognition and respect of the existing character of the surrounding residential land uses;
- e. servicing requirements of the Region of Peel with respect to the sanitary sewer outlet and the use of backflow preventers in the buildings to prevent basement flooding; and
- f. a heritage permit, including a **Heritage Impact Assessment**, will be required for any alterations.



### 13.1.3 Special Site 3

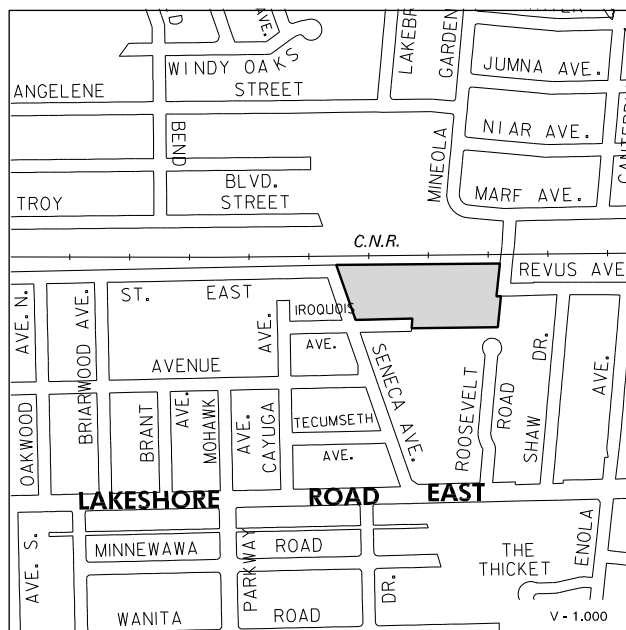


The lands identified as Special Site 3 are located in the southeast quadrant of Atwater Road and Cawthra Road.

13.1.3.1 Notwithstanding the provisions of the Residential High Density designation applying to Area A, all types of multiple family dwellings including townhouses, street townhouses, other forms of horizontal multiple dwellings, apartments, or any combination of these uses with individual frontages or in cluster arrangements may be permitted.

13.1.3.2 Notwithstanding the provisions of the Residential Medium Density designation applying to Area B, street townhouses may be permitted.

### 13.1.4 Special Site 4



The lands identified as Special Site 4 are located west of Revus Avenue, south of the Canadian National Railway tracks.

13.1.4.1 Notwithstanding the provisions of the Business Employment designation, the following uses will not be permitted:

- banquet hall;
- broadcasting, communication, and utility rights-of-way;
- conference centre;
- entertainment, recreation and sports facilities;
- financial institution;
- outdoor storage;
- overnight accommodation; and
- restaurants.

### 13.1.5 Special Site 5

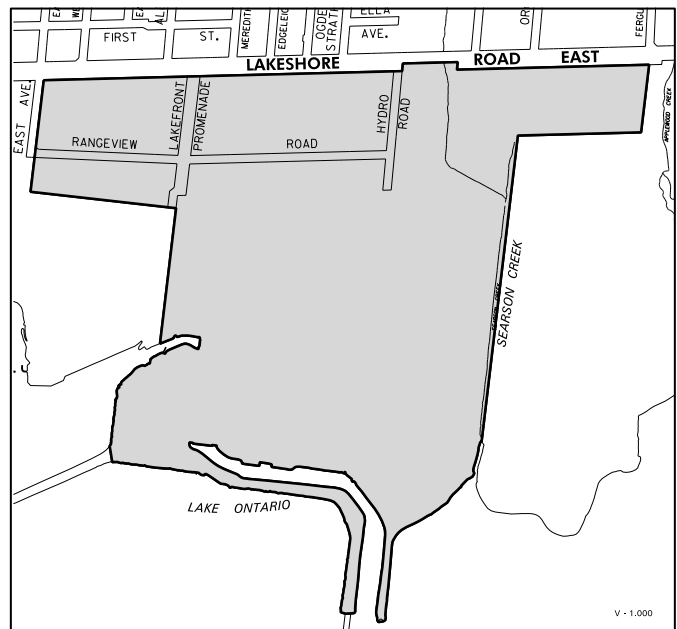


The lands identified as Special Site 5 are located along Casson Avenue, west of the utility corridor.

13.1.5.1 Notwithstanding the provisions of the Business Employment designation, the following uses will not be permitted:

- a. banquet hall;
- b. broadcasting, communication, and utility rights-of-way;
- c. conference centre;
- d. entertainment, recreation and sports facilities;
- e. financial institution;
- f. outdoor storage;
- g. overnight accommodation; and
- h. restaurants.

### 13.1.6 Special Site 6



The lands identified as Special Site 6 are located east of East Avenue and south of Lakeshore Road East. The lands are currently designated Business Employment, Utility and Greenbelt and are under review through the next phase of study for Inspiration Lakeview.

13.1.6.1 Notwithstanding the provisions of the Utility designation, the following additional use may be permitted:

- a. an electric power generating facility.

13.1.6.2 Through the next phase of study for Inspiration Lakeview, a Phase Two municipal comprehensive review will be prepared to address the potential conversion of the lands designated Business Employment. In addition to this component of the review, the comprehensive master plan will address land use, built form and transportation to the City's satisfaction. The master plan will also:

- a. evaluate existing site conditions;
- b. have regard for other City plans, policies, studies and reports such as the Lakeview and Port Credit District Policies Review and Public

Engagement Process – Directions Report, Employment Land Review Study, Waterfront Parks Strategy, and Lakeshore Road Transportation Review Study;

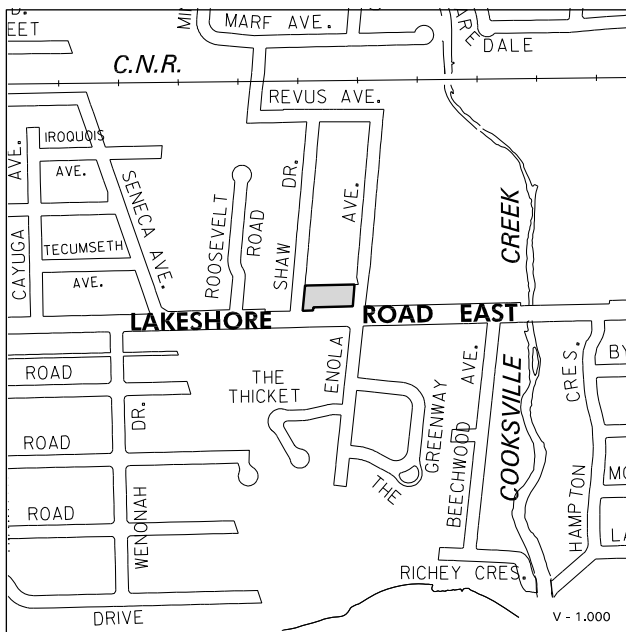
- c. examine opportunities for new view **corridors** extending from Lakeshore Road East to Lake Ontario;
- d. include the provision of significant public parklands along the waterfront including the extension of the Waterfront Trail;
- e. provision of cultural infrastructure and public art;
- f. examine opportunities to continue Lakeshore Road East as a mixed use mainstreet;
- g. consideration for the development of a Community Node, providing an appropriate population to employment ratio as described in this Area Plan;
- h. provision of a range of housing types and affordable housing;
- i. develop a road network that appropriately integrates with the surrounding neighbourhoods; and
- j. provide protection, enhancement and restoration of the natural environment.

13.1.6.3 Development of this site will consider the following eight core principles outlined in the *Inspiration Lakeview - Vision*:

- a. link the City and the water;
- b. open the site with a wealth of accessible public spaces;
- c. create a green, sustainable, innovative, and model community;
- d. create a vibrant community;
- e. connect in multiple ways through transit, walking, cycling, and the car;
- f. create a destination to draw local, regional, and international visitors;

- g. commemorate history while creating a legacy; and
- h. balance public and private investment to be economically viable and sustainable.

### 13.1.7 Special Site 7



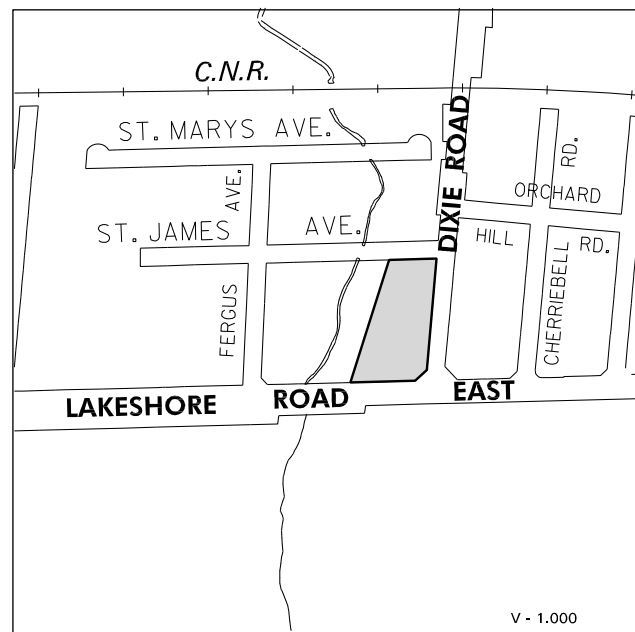
The lands identified as Special Site 7 are located north of Lakeshore Road East between Shaw Drive and Enola Avenue.

The property located at 411 Lakeshore Road East is listed on the City's Heritage Register.

13.1.7.1 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. a motor vehicle repair garage may be permitted.

### 13.1.8 Special Site 8

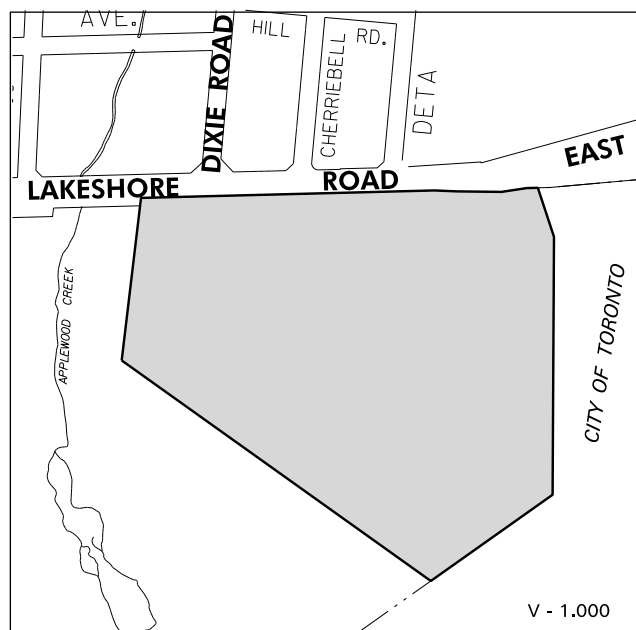


The lands identified as Special Site 8 are located at the northwest corner of Lakeshore Road East and Dixie Road.

13.1.8.1 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. townhouse dwellings may be permitted on St. James Avenue to provide appropriate transition to the existing surrounding residential.

### 13.1.9 Special Site 9



The lands identified as Special Site 9 are located east of Applewood Creek and south of Lakeshore Road East.

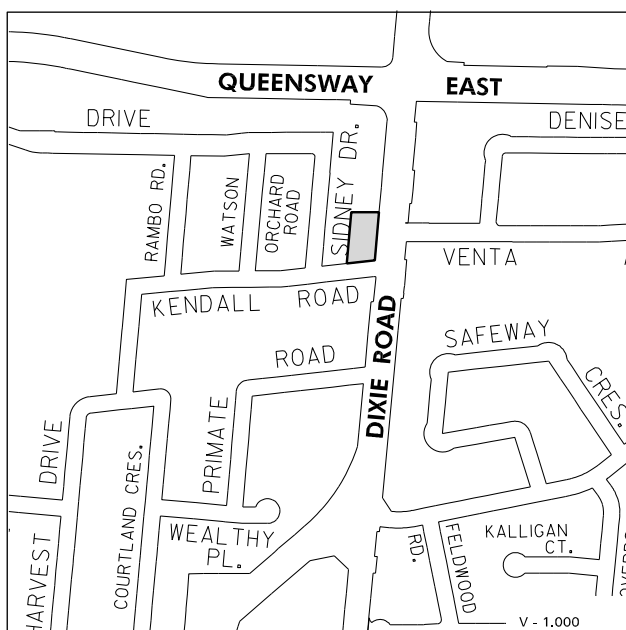
The Small Arms Inspection Building located at 1352 Lakeshore Road East is designated under the Ontario Heritage Act.

13.1.9.1 A heritage permit, including a **Heritage Impact Assessment**, will be required for any alterations.

13.1.9.2 Notwithstanding the provisions of the Public Open Space designation, the following additional uses may be permitted at 1352 Lakeshore Road East, subject to a master plan and the review of the remediation plan:

- a. commercial schools;
- b. *community facilities*, including art studios and art galleries;
- c. a conference centre;
- d. restaurants; and
- e. **secondary offices**.

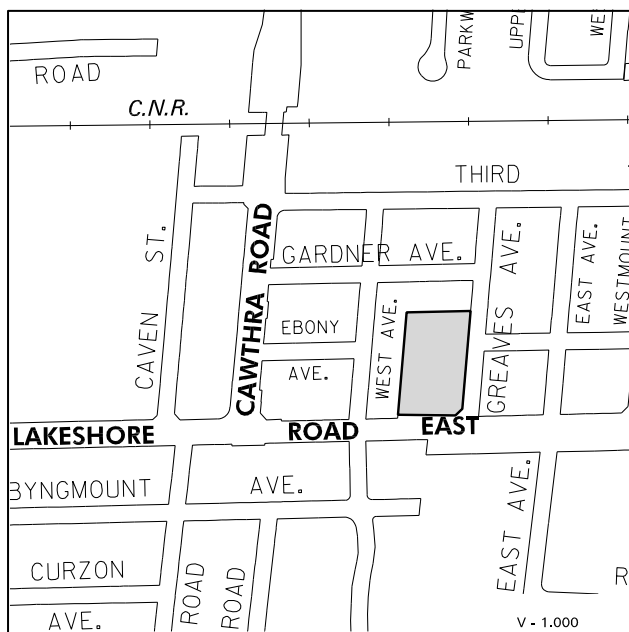
### 13.1.10 Special Site 10



The lands identified as Special Site 10 are located at the northwest corner of Kendall Road and Dixie Road.

13.1.10.1 Notwithstanding the provisions of the Residential Low Density I designation, an office for a maximum of four medical practitioners may be permitted.

### 13.1.11 Special Site 11



The lands identified as Special Site 11 are located at the northwest corner of Lakeshore Road East and Greaves Avenue.

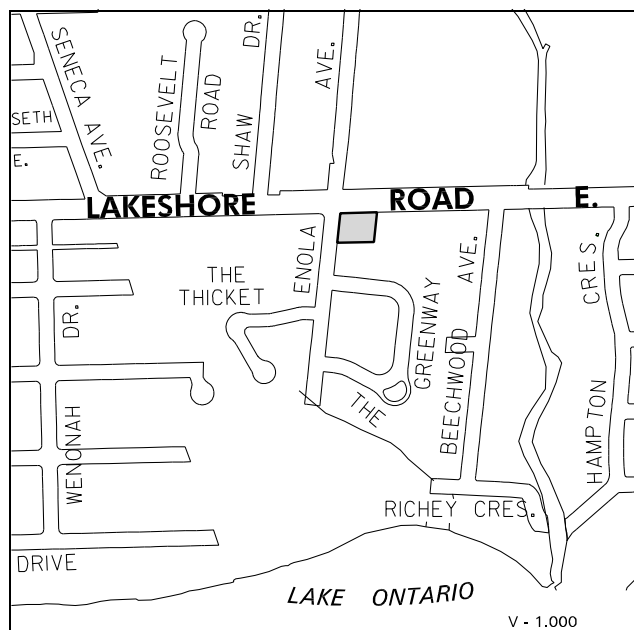
13.1.11.1 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. the existing townhouses may be permitted.

## 13.2 Exempt Sites

Exempt Sites reflect unique circumstances that are not representative of the vision, direction and planning policies of the Plan, but nonetheless are recognized because they contain established land uses. The Exempt Sites in Lakeview, as shown in Map 2 are subject to the following policies.

### 13.2.1 Exempt Site 1

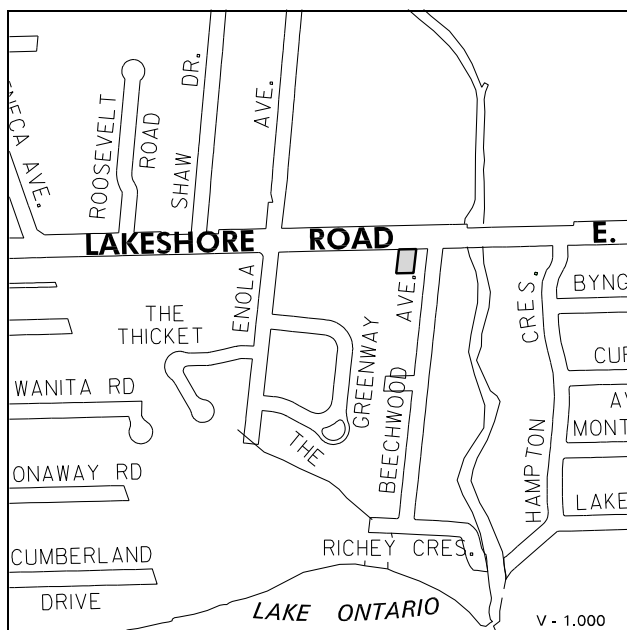


The lands identified as Exempt Site 1 are located at the southeast corner of Lakeshore Road East and Enola Avenue.

13.2.1.1 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. the existing motor vehicle service station may be permitted.

### 13.2.2 Exempt Site 2

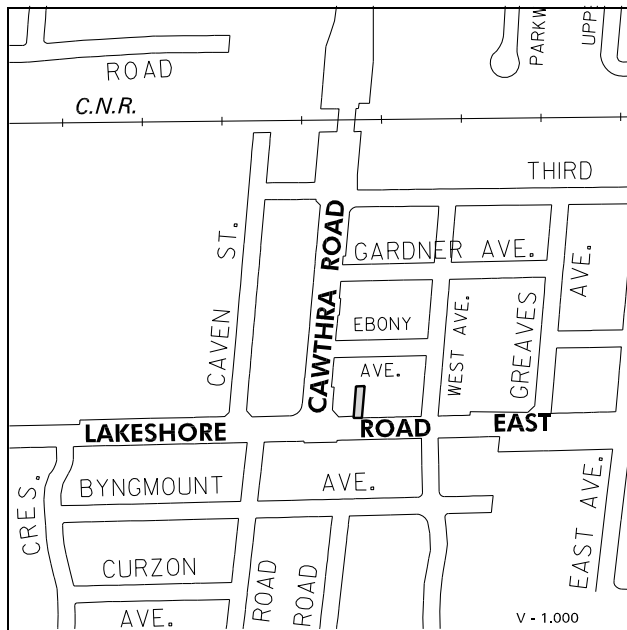


The lands identified as Exempt Site 2 are located south of Lakeshore Road East and west of Beechwood Avenue.

13.2.2.1 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. a motor vehicle repair garage may be permitted.

### 13.2.3 Exempt Site 3

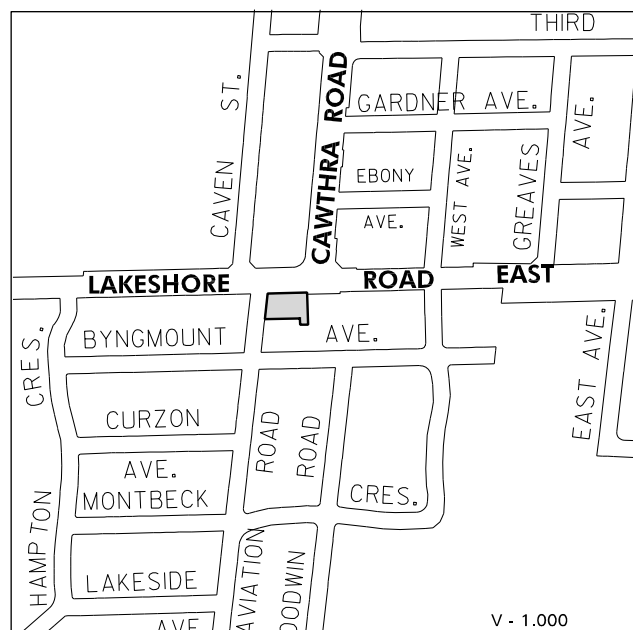


The lands identified as Exempt Site 3 are located north of Lakeshore Road East and east of Cawthra Road.

13.2.3.1 Notwithstanding the provisions of Mixed Use designation, the following additional policy will apply:

- a. the existing motor vehicle repair garage may be permitted.

### 13.2.4 Exempt Site 4



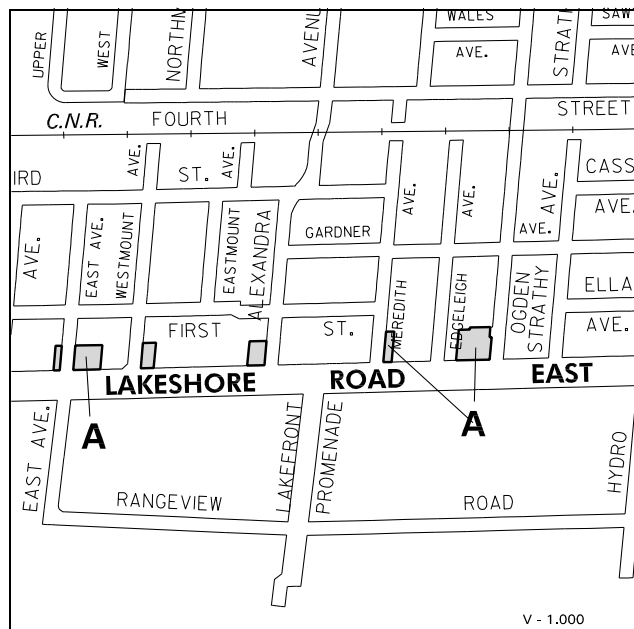
The lands identified as Exempt Site 4 are located at the southeast corner of Lakeshore Road East and Aviation Road.

13.2.4.1 Notwithstanding the provisions of the Mixed Use designation, the following additional policy will apply:

- a. the existing motor vehicle commercial use may be permitted.



### 13.2.5 Exempt Site 5

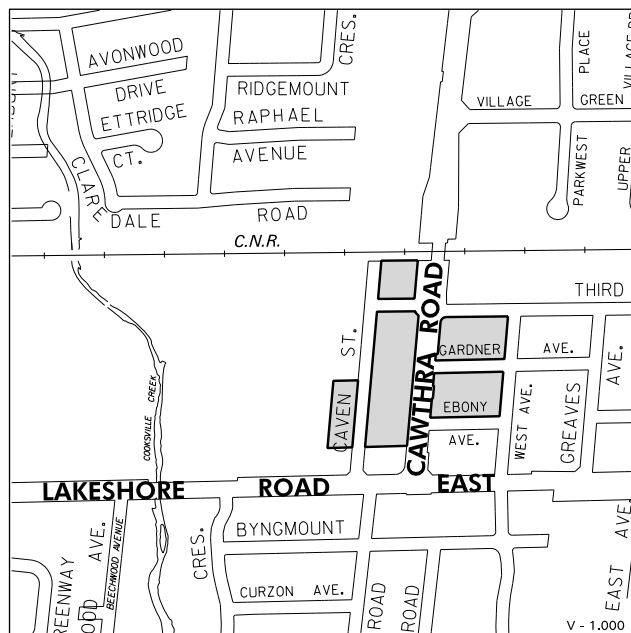


The lands identified as Exempt Site 5 include six sites located north of Lakeshore Road East between Greaves Avenue and Ogden Avenue.

13.2.5.1 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. the existing motor vehicle repair garage may be permitted; and
- b. the existing motor vehicle sales and rental uses identified in Area A may be permitted

### 13.2.6 Exempt Site 6

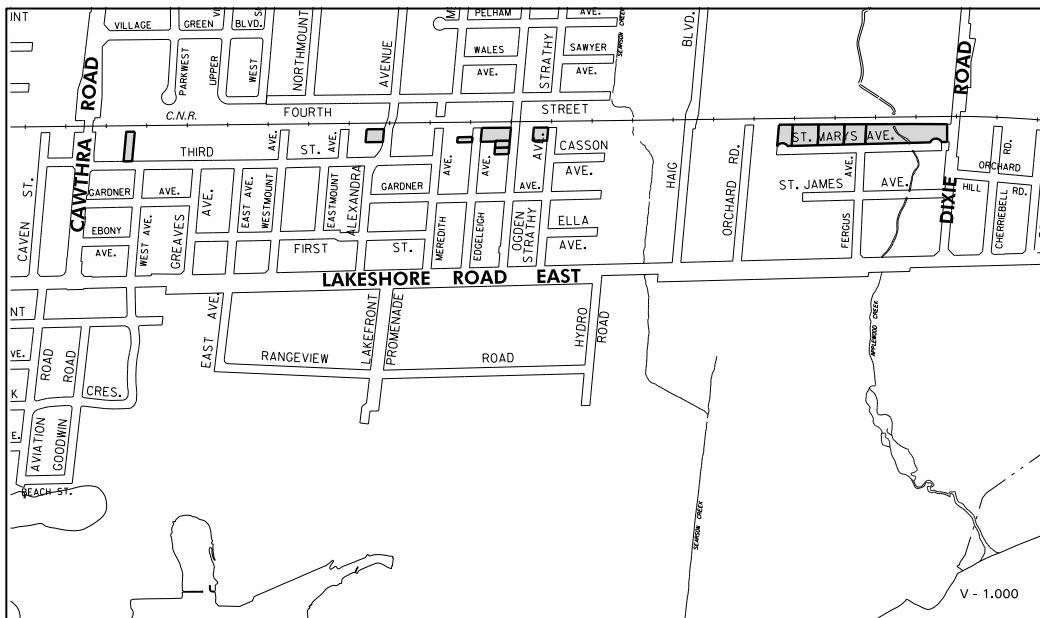


The lands identified as Exempt Site 6 are located north of Lakeshore Road East between Cooksville Creek and West Avenue.

13.2.6.1 Notwithstanding the provisions of the Residential Medium Density designation, the following additional policy will apply:

- a. the existing detached, semi-detached, and duplex dwellings may be permitted.

### 13.2.7 Exempt Site 7



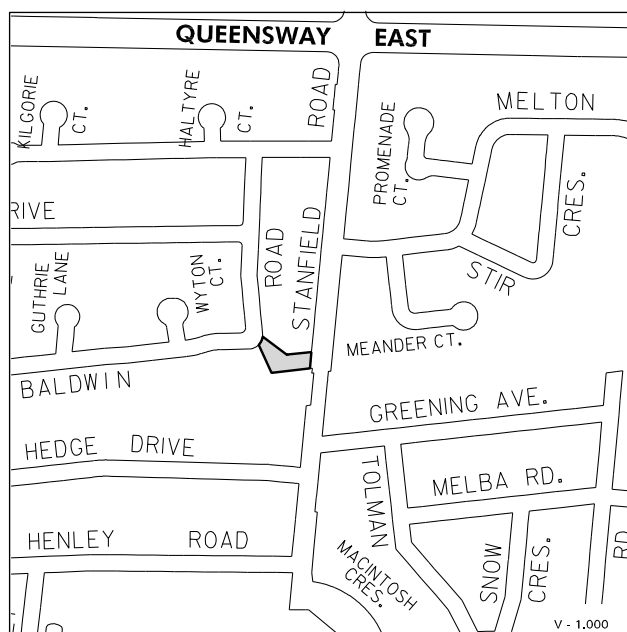
The lands identified as Exempt Site 7 are located south of the Canadian National Railway line between Cawthra Road and Dixie Road.

13.2.7.1 Notwithstanding the provisions of the Residential Low Density II designation, the following additional policies will apply:

- the existing manufacturing uses may be permitted;
- the existing motor vehicle body repair facility may be permitted; and
- the existing warehousing, distributing and wholesaling uses may be permitted; and
- the existing retail store may be permitted.

13.2.9.2 Permitted uses will operate within enclosed buildings.

### 13.2.8 Exempt Site 8



The lands identified as Exempt Site 8 are located on the west side of Stanfield Road, south of Queensway East.

13.2.8.1 Notwithstanding the provisions of the Residential Low Density I designation, the following additional policies will apply:

- a. the existing hair care and aesthetics services may be permitted;
- b. the existing **secondary office** uses, excluding medical offices, may be permitted;

## 14.0 Implementation

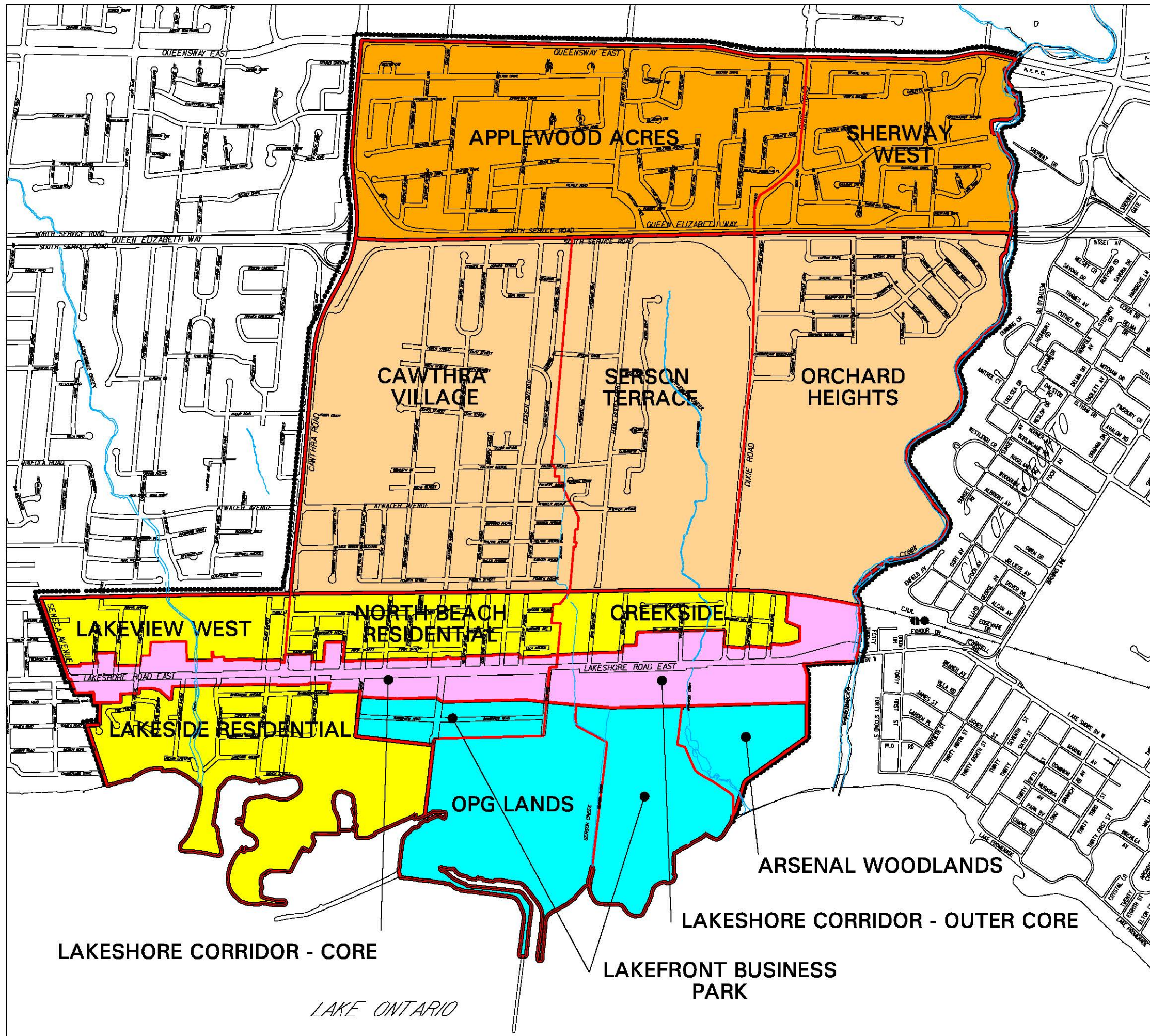
14.1 Mississauga will monitor development in Lakeview, including population density and the population to employment ratio.

14.2 Mississauga, in conjunction with public consultation, may develop a list of facilities/matters that could be exchanged for granting bonuses in height and/or density.

14.3 Mississauga, in conjunction with public consultation, may prepare a Community Improvement Plan, including a plan to promote Lakeview's mainstreet with **streetscape** improvements and symbolic gateways, and incentives to promote and enhance cultural heritage sites in Lakeview.

14.4 Prior to development, master plans for the Ontario Power Generation site and the adjacent Business Employment area will be prepared to the City's satisfaction.





**Legend**

**Precinct Boundaries :**

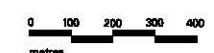
- North Residential Neighbourhood
- Central Residential Neighbourhood
- South Residential Neighbourhood
- Lakeshore Corridor
- Employment

Sub-Area Boundaries

Local Area Plan Boundary

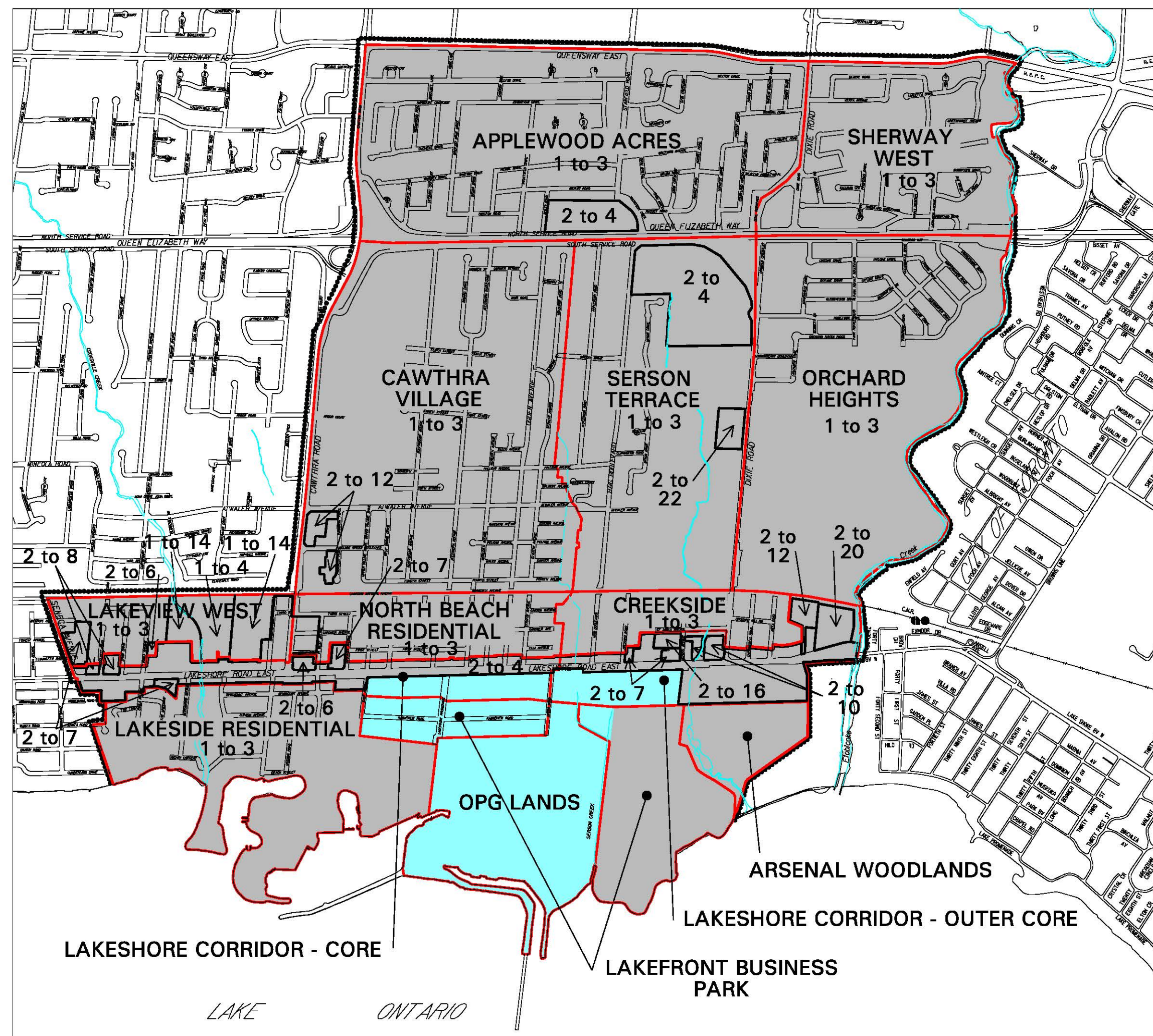
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**Schedule 1  
Lakeview Local Area Plan  
Precincts and Sub-Areas**



City of Mississauga





Legend



Sub-Area Boundary



To Be Determined

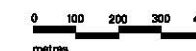


Local Area Plan Boundary

Note:

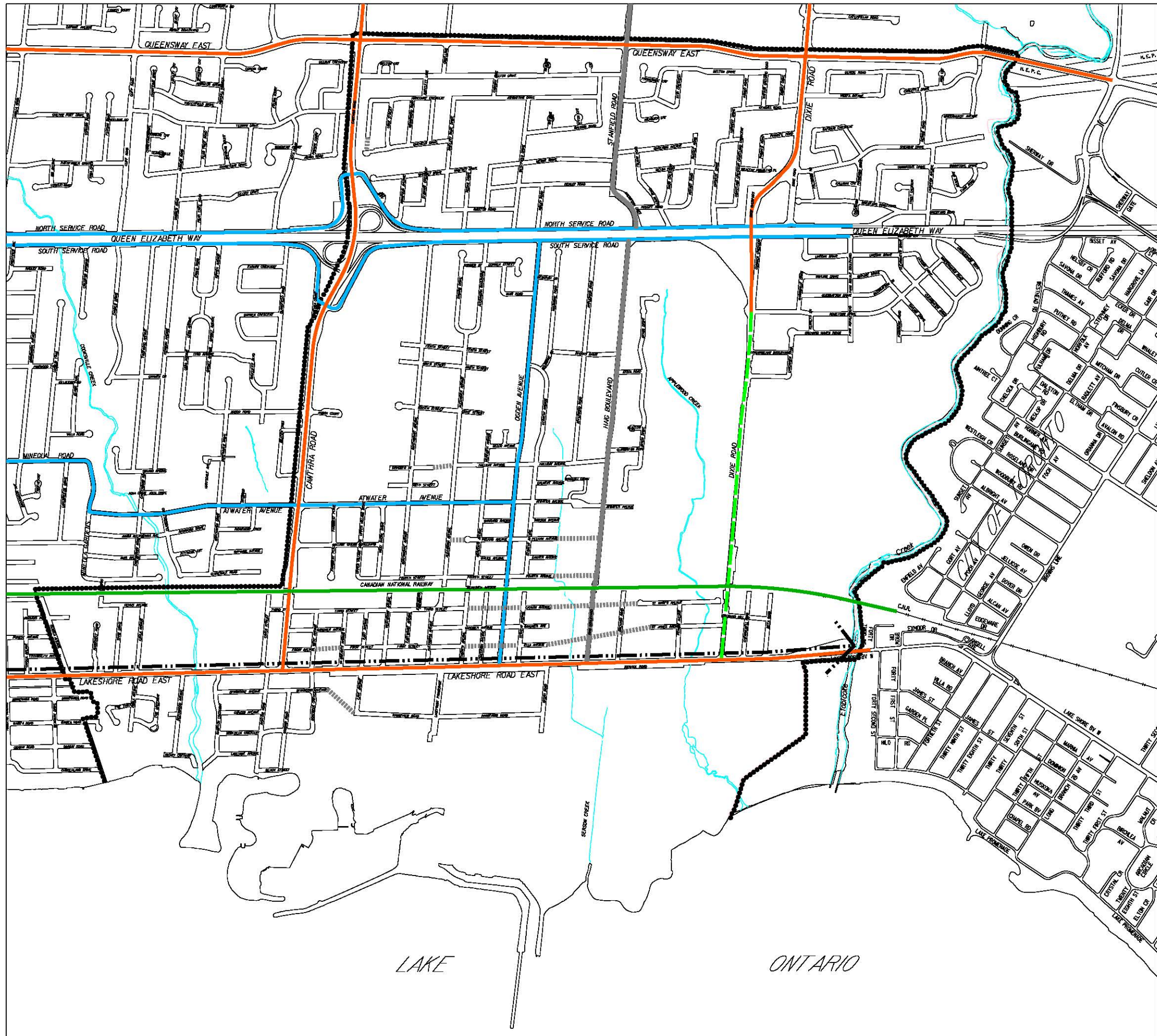
1. Height limits represent the minimum and maximum number of storeys permitted.

Schedule 2  
Lakeview Local Area Plan  
Height Limits



City of Mississauga





TRANSPORTATION LEGEND

- Provincial Highway and Interchange
- Arterial
- Major Collector
- Major Collector (Scenic Route)
- Minor Collector
- Future Local Roads
- Existing Commuter Rail
- Higher Order Transit Corridor

BASE MAP INFORMATION

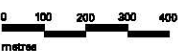
- Local Road
- Local Area Plan Boundary

Notes:

1. All lines shown are conceptual.
2. Base Map information (eg. railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.

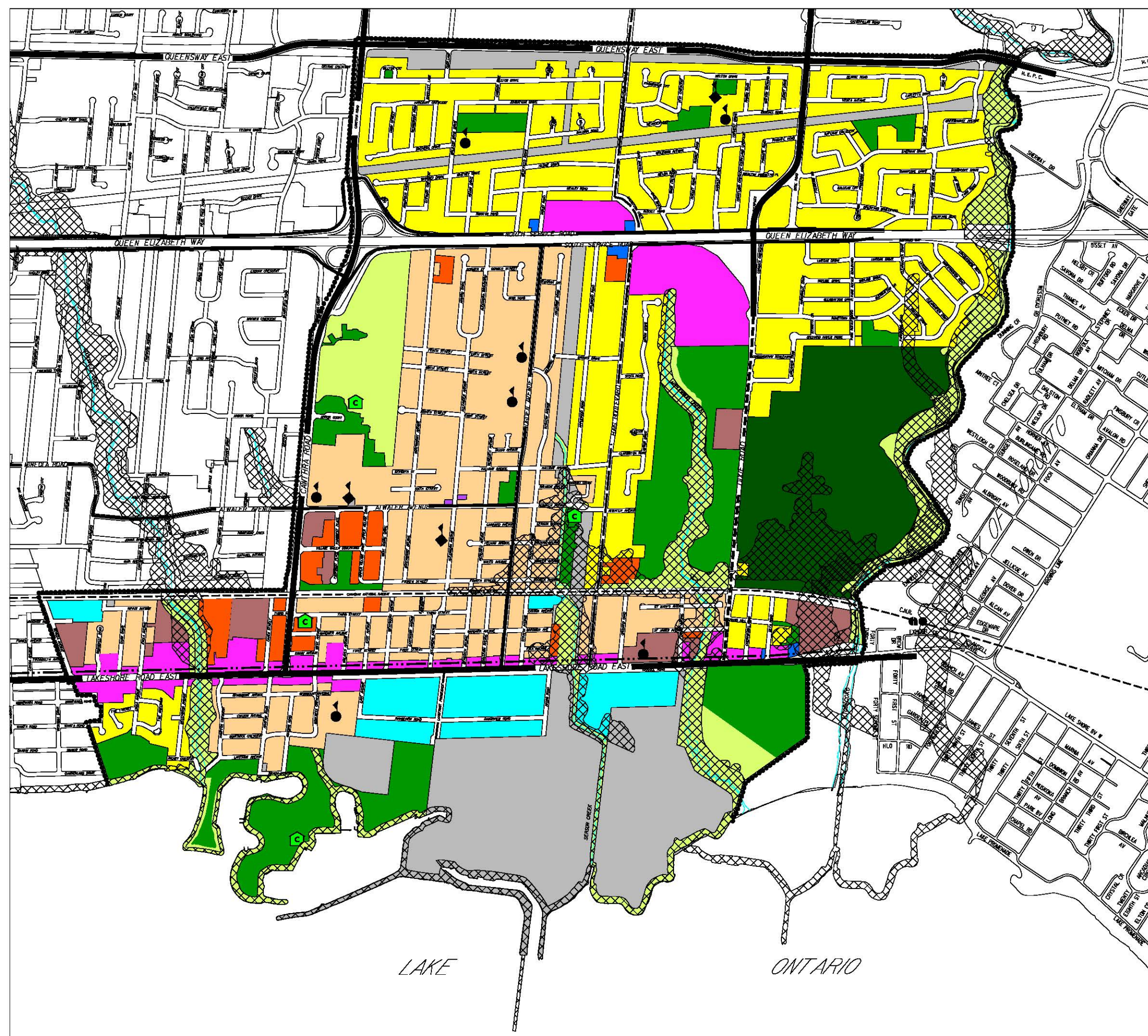
Draft

Schedule 3  
Lakeview Local Area Plan  
Long Term Road and Transit Network



City of Mississauga





#### LAND USE DESIGNATIONS

	Residential Low Density I		Office
	Residential Low Density II		Business Employment
	Residential Medium Density		Public Open Space
	Residential High Density		Private Open Space
	Mixed Use		Greenbelt
	Convenience Commercial		Utility
	Motor Vehicle Commercial		

#### BASE MAP INFORMATION

	Provincial Highway and Interchange		Natural Hazards
	Arterial		Public School
	Major Collector		Catholic School
	Major Collector (Scenic Route)		Community Facilities
	Minor Collector		Local Area Plan Boundary
	Local Road		
	Existing Commuter Rail		
	GO Transit Station		
	Higher Order Transit Corridor		

#### Notes:

1. Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.
2. Refer to Schedule 3, Natural System of the principal document for the location of the Natural Areas System and Natural Hazards.
3. The limits of the Natural Hazards shown on this map are for illustrative purposes only. The appropriate Conservation Authority should be consulted to determine their actual location.
4. The Community Node boundary is to be determined.

### Schedule 4 Lakeview Local Area Plan Land Use Designations

0 100 200 300 400  
metres



City of Mississauga

V - 2.001



# APPENDIX 1

## Lakeview Built Form Standards



# Appendix I

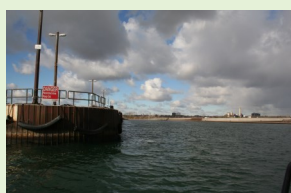
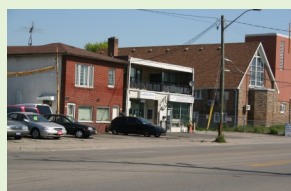
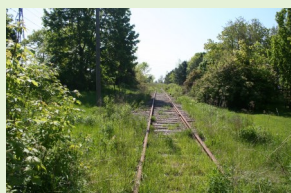
## LAKEVIEW

### Built Form Standards

**DRAFT**  
January 2014



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# 1.0 Introduction

## 1.1 How to Read the Built Form Standards

The Lakeview Built Form Standards (The Standards) are to be read in conjunction with the policies in Mississauga Official Plan (The Plan) and the Lakeview Local Area Plan (Area Plan). The Standards are to be used during the design and review of development applications.

This Standards demonstrate how the urban form policies in the Area Plan can be achieved. The Standards are not considered a part of the Area Plan; however, selected content from the Standards has been incorporated into the Area Plan and represents policy. Applicants must also refer to the Zoning By-law, and the Ontario Building Code to ensure that the applicable requirements in these documents have been met. In addition, there may be other City initiatives and directions (e.g. Design Guidelines, Green Development Strategy) which need to be consulted.

## 1.2 Purpose

Building a desirable urban form is a key principle of the Mississauga Official Plan. The Standards are intended to provide further guidance of the policies in the Mississauga Official Plan and the Lakeview Local Area Plan. The Standards establishes and illustrates general requirements to achieve a high quality urban form, site development and public realm.

The Standards are intended to ensure development is appropriate for Lakeview and reflects the unique characteristics of the area.



Figure A1 - Images of built form typologies in Lakeview

# 1.0 Introduction

## 1.3 Expectations of the Standards

The Built From Standards provide further direction in the Urban Design Policies set out in the Official Plan, the Lakeview Local Area Plan in addition to other City initiatives that support land use decisions and other strategies within the City.

The Standards set out detailed requirements to achieve a high quality built form in Lakeview that interfaces with the public realm in a seamless fashion. The Built Form Standards have been developed to communicate the design expectations, in advance of an application being filed, related to the quality and outcome of development.

The standards, in addition to the dimensions indicated, are to be addressed and achieved by development proponents through the planning application process. Depending on the context or site size, exceptions to the Standards may be considered at the discretion of the City, in whole or part, when there are extenuating circumstances and/or where proposals are able to demonstrate urban design excellence.

It should be noted that the standards may be amended, modified or updated on an as need basis to provide clarity on the intent of the Lakeview Local Area plan, provisions of the zoning by-law including the outcome of other studies or initiatives that impact the Lakeview area.



Figure A2 - Images of built form typologies in Lakeview



# 1.0 Introduction

## 1.4 Lakeview Local Area Plan

The Area Plan includes lands identified in the Mississauga Official Plan City Structure as Neighbourhood and Employment Character Areas.

Both the Neighbourhood and Employment Character Areas are divided into 5 precincts and 13 sub-areas which recognize different attributes of these areas and contain different policy directions. These are organized as follows:

1. *North Residential Precinct*
  - *Applewood Acres*
  - *Sherway West*
2. *Central Residential Precinct*
  - *Cawthra Village*
  - *Orchard Heights*
  - *Serson Terrace*
3. *South Residential Precinct*
  - *Creekside*
  - *Lakeview West*
  - *Lakeside Residential*
  - *North Beach Residential*

### 4. Lakeshore Corridor Precinct

- *Core*
- *Outer Core*

### 5. Employment Precinct

- *Arsenal Woodlands*
- *Lakefront Business Park*
- *Ontario Power Generation Lands (OPG Lands)*

This Standards outline various general built form typologies that are found in Lakeview.

Individual direction is given for the **Lakeshore Corridor Precinct**, where the Neighbourhood Character Area and the Employment Character Area overlap. The Lakeshore Corridor Precinct is an area of change and specific direction is given in section 3.0 of this Standards.

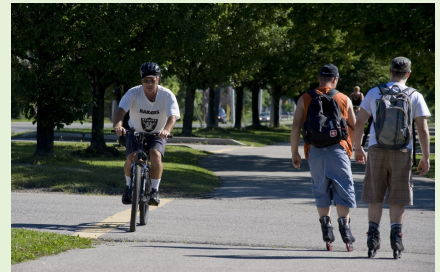


Figure A3 - Images of built form and cultural resources in Lakeview



# 1.0 Introduction

## 1.4.1 Community Node

The Mississauga Official Plan identifies Lakeview as an area that will contain a Community Node. Once the location of the Community Node has been defined, it will be the focus of activity for Lakeview. The combination of residential uses, employment opportunities, cultural activities, shopping, dining, commerce and recreation will be encouraged to concentrate in the Community Node.

The Community Node will be the primary focus for intensification and density, however, the form and scale of development will vary within the Community Node in accordance with the location and surrounding context. A more detailed Built Form Standards will be established once the Community Node boundaries have been determined.

## 1.4.2 Inspiration Lakeview

For years, public access to the Lakeview area waterfront has been limited by the location of the Ontario Power Generation's coal-fired generation station. With the demolition of the power plant this part of the waterfront is ready for a new vision to create "a model sustainable creative community on the waterfront".

The City of Mississauga, the Province and Ontario Power Generation (OPG) have signed a memorandum of understanding that commits them to working together on a shared vision for the future of these lands.

These lands include a portion of the Lakefront Business Park Precinct, a portion of the Lakeshore Corridor Precinct and the OPG Lands.

To date the Inspiration Lakeview project has developed a Community Vision. A more detailed land use plan, including the appropriate location of the Community Node, will be undertaken in the next phase.



Figure A4 - Images of built form and cultural resources in Lakeview

## 2.0 Lakeview Character Areas

### 2.1 Neighbourhood and Employment Character Areas

Lands identified as Neighbourhood Areas are considered to be generally stable areas where the existing character is to be preserved and enhanced. They have various operating uses including manufacturing and wholesale, retail and restaurant, banquet hall, utility, marina and parkland. The future use of these lands will be reviewed through the next phase of study for Inspiration Lakeview and remains unchanged in this Area Plan.

These areas will be maintained while allowing for infill which is compatible with and enhances the character of the area.

The following sections provide additional context and information on the built form typologies found in each of the Neighbourhood and Employment Character Areas.

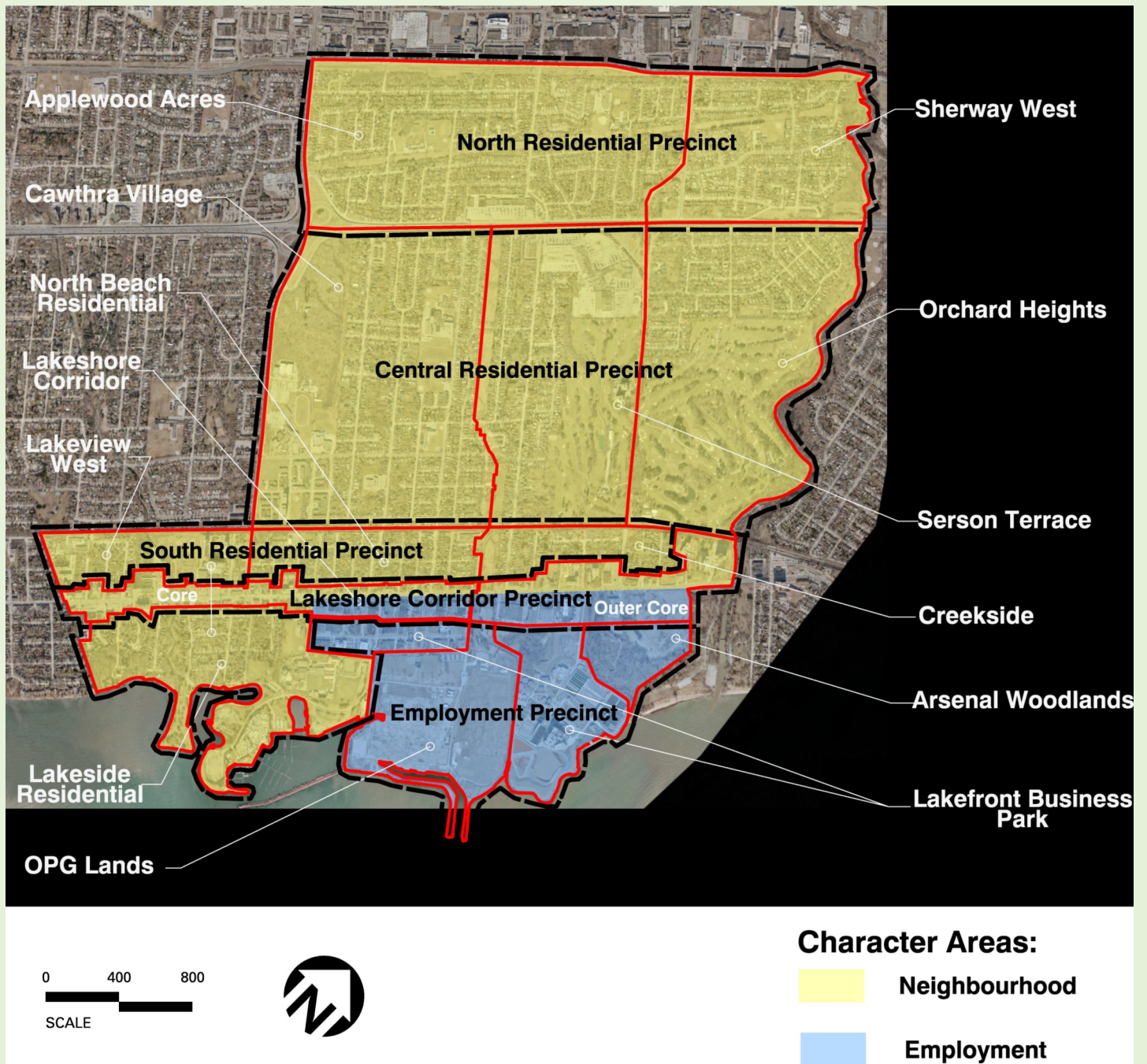
A separate section entitled “Lakeshore Corridor Precinct” will address specific issues in regards to the Lakeshore Corridor Precinct which crosses through Neighbourhood and Employment Character Areas.



Figure B1 -Examples of built form typologies within Lakeview



## 2.0 Lakeview Character Areas



**Figure B2** -Lakeview consists of 5 precincts and 14 sub-areas that are located in Neighbourhood and/or Employment Character Areas.

# 2.0 Lakeview Character Areas

## 2.2 Built Form Type

New developments will be compatible with and enhance the character of the neighbourhood by integrating with the surrounding area. This can be done by maintaining the existing lotting fabric layout and using consistent and transitional heights.

The following building typologies will be discussed in this section:

- i) Detached Dwellings, Semi-Detached Dwellings, Duplexes and Triplexes;
- ii) Street Townhouses;
- iii) Standard and Common Element Condominium Developments - Single detached; and,  
- townhouse;
- i) Horizontal Multiple Dwellings;
- ii) Apartment;
- iii) Commercial; and
- iv) Industrial.

In addition to these Standards, the City of Mississauga Urban Design Guidelines and Reference Notes should be reviewed. These include, but are not limited to:

- Green Development Strategy;
- Urban Design Handbook for Low Rise Multiple Dwellings;
- Design Guidelines for High Density Apartments;

- Balconies in Medium and High Density Developments;
- Condominium Townhouse Design Standards;
- Screening for Roof Top Mechanical Units;
- Standards for Seniors Outdoor Amenity Area;
- Standards for Children’s Outdoor Play Spaces;
- Standards for Shadow Studies; and
- Design Guidelines for Industrial buildings.



Figure B4 - Example of a Duplex in Lakeview



Figure B3 - Example of a Bungalow in Lakeview



Figure B5 -Example of detached dwellings in Lakeview



## 2.0 Lakeview Character Areas

### 2.2.1 Detached and Semi-Detached Dwellings, Duplexes and Triplexes

To preserve neighbourhoods with predominantly low density residential character, applicants will be required to provide the greater of the following:

- i) The average lot frontage and lot area of residential lots on both sides of the same street within 120 m of the subject property. In the case of a corner lot, lots on both sides of the streets within 120 m will be considered; or
- ii) The requirements of City by-laws, including Zoning By-law 0225-2007.

New detached, semi-detached, duplex and triplex dwellings within Lakeview will maintain the existing character of the area. The following criteria will apply:

- a. The maximum height of any dwelling should be 10.7 m. The design of the building will de-emphasize the height of the house and be designed as a composition of small architectural elements, i.e. projecting dormers and bay windows;
- b. New development will preserve and enhance the generous front, rear and side yard setbacks;
- c. New development will ensure that existing grades and drainage conditions are preserved;
- d. New development will fit the scale and character of the surrounding area, and take advantage of the features of a particular site, i.e. topography, contours, mature vegetation, location to railway tracks;
- e. Garages will be recessed or located behind the main face of the house. Alternatively, garages will be located in the rear of the property;
- f. New development will have minimal impact on its adjacent neighbours with respect to overshadowing and overlook;
- g. New development will minimize the hard surface areas in the front yard;
- h. New development will preserve existing high quality trees to maintain the existing established nature of these areas;
- i. New house designs which fit with the scale and character of the local area, and take advantage of the particular site features are encouraged.
- j. The use of standard, repeat designs is strongly discouraged; and
- k. The building mass, side yards and rear yards will respect and relate to those of adjacent lots.



Figure B6 -Example of a Triplex in Lakeview



Figure B7- Example of a semi-detached dwelling in Lakeview

# 2.0 Lakeview Character Areas

## 2.2.2 Street Townhouses

Development of Street Townhouses or Freehold Townhouses should meet the following criteria:

- i) They fit into the existing lotting pattern of the community;
- ii) They provide an appropriate transition from low built form to higher built forms; and
- iii) They are located on or in proximity to transit routes

The following are requirements for new infill street townhouses within the Lakeview area to ensure that the character of the existing community is maintained:

- a. The minimum lot area for an interior townhouse unit will be 200 m<sup>2</sup> and 280 m<sup>2</sup> for a corner lot;
- b. The maximum height for a street townhouse will be 10.7 m;
- c. The maximum number of townhouses in a consecutive row will be 6 units per block;
- d. The minimum width of a townhouse unit will be 6.8 m;
- e. The minimum width of a lot will be 6.8 m for an interior lot and 9.8 m for a corner lot;
- f. The minimum front yard setback for any unit will be 7.5 m;
- g. The maximum number of stairs to the front door of any unit will be 3 risers from the established grade;
- h. Garages will not project beyond the main face of the dwelling unit. They may be flushed, recessed or located at the rear of the unit;
- i. The garage of any townhouse unit will not be more than 50% of the width of the unit;
- j. The driveway width of a townhouse unit will not be more than 50% of the front yard or 5.2 m which ever is smaller;
- k. A minimum of 3.0 m will be required between blocks of units. A minimum of 4.5 m will be required between blocks of units where a walkway is proposed;
- l. Front to rear access to internal townhouse units will be provided through the individual unit. The interior design of the unit must ensure this can be accommodated;
- m. The minimum rear yard setback of a street townhouse from a property line will be 7.5 m. Where a townhouse unit is accessed by a laneway, the minimum distance from the rear face of the garage to the rear face of the dwelling unit will be 7.5m;
- n. End units that are exposed to a public road or park will be required to have upgraded elevations equal to the front of the unit;
- o. The minimum landscape area of a street townhouse dwelling will be 25% of the lot area; and



Figure B8 - Example of Street Townhouses in Mississauga



Figure B9- Example of Street Townhouses in Mississauga



## 2.0 Lakeview Character Areas

- p. Fencing requirements will be minimized with built form acting as the prominent feature along all frontages. All fencing is to be returned within a maximum of 3.0 m of the rear corner of the dwelling.



**Figure B11** - Street Townhouse example. Corner lot upgrade to appear as a detached dwelling and fit in with the lotting pattern and built form pattern of the area.



**Figure B12** - Street Townhouses adjacent to detached dwellings



**Figure B10** - Example of Street Townhouse developments in Mississauga



**Figure B13** - Side elevation of Detached Dwelling adjacent to Street Townhouses.



## 2.0 Lakeview Character Areas

### 2.2.3 Standard and Common Element Condominium

Where development is proposed on a condominium road, new dwellings should fit the scale and character of surrounding development with respect to frontage, area, setback and side yards. Development standard and common element condominium townhouses should demonstrate that:

- i. They fit into the existing lotting pattern of the community;
- ii. They provide an appropriate transition from low built form to higher built forms;
- iii. Have a minimum lot depth of 90 m; and
- iv. They are located on or in proximity to transit routes.

Development of such sites requires careful consideration regarding site planning and building massing, including the height and setbacks from adjacent developments and maintaining a consistent streetscape and built form along their frontages. New buildings will minimize shadowing and overlook onto adjacent properties. New infill standard and common element condominium townhouses will adhere to the City's *Urban Design Handbook for Low-Rise Multiple Dwellings* and the *Design Reference Note for Single Detached and Common Element Condominium*.

The following are criteria for new infill condominium and common element developments within the Lakeview area

which will help maintain the character of the existing community. These are broken down into two categories:

- Single Detached Standard and Common Element Condominium developments; and
- Townhouses Standard and Common Element Condominiums

#### 2.2.3.1 Single Detached Standard and Common Element Condominium

- a. The width and massing of the proposed unit will be similar to that of the existing character of the neighbourhood.
- b. The maximum height for a dwellings will be 10.7 m;
- c. The maximum stairs to the front door of any unit is 3 risers from the established grade of the dwelling unit;
- d. Garages will not project beyond the main face of the dwelling unit. They will be flushed, recessed or may be located at the rear of the unit;
- e. The driveway width of a dwelling unit will not be more than 50% of the front yard or 1.0 m wider than the width of the garage whichever is smaller;



**Figure B14** - Example of a standard condominium detached development along Stavebank Road. The streetscape is continuous as the condominium units have been designed to be in character with the street



**Figure B15** - side elevation of the detached condominium is upgraded to look like the front of a single detached dwelling to fit in with the lotting pattern of the existing street.

## 2.0 Lakeview Character Areas

- f. Visitor parking will be centrally located, not visible from a public road and will be well screened from existing and proposed dwellings;
- k. No service/loading, mailboxes or garbage area will be located along the frontage of the public road or visible from the public road;
- o. Entrances to new development will not be through established or existing lots, but will be from major roads and routes. The entrances to new developments will be flanked by dwellings within the new development itself;
- p. Fencing requirements will be minimized with built form acting as the prominent feature along all frontages. All fencing is to be returned within a maximum of 3.0 m of the rear corner of the dwelling; and
- q. End units exposed to an external or internal road will be required to have upgraded elevations.
- r. Amenity spaces will be in the rear of the unit and not on public roads.
- s. All common Element units must have a private amenity space of minimum of 11 m<sup>2</sup>.



**Figure B16** -Example of detached condominium developments



**Figure B17** -Example of condominium detached developments



**Figure B18**- Single detached units fronting onto a public road to ensure the lotting pattern and form are maintained on the residential street.



**Figure B19**- Single detached units fronting onto a Condominium Road to ensure the lotting pattern and form are maintained on the residential street.



## 2.0 Lakeview Character Areas

### 2.2.3.2 Townhouse Standard and Common Element Condominium

- a. The maximum height for a townhouse dwellings will be 10.7 m;
- b. The minimum unit width of a townhouse unit will be 6.0 m;
- c. The minimum front yard setback from a street will be 6.0 m;
- d. The maximum number of townhouses in a consecutive row will be 8 units;
- e. The maximum stairs to the front door of any unit is 3 risers from the established grade of the dwelling unit;
- f. Garages will not project beyond the main face of the dwelling unit. They will be flushed, recessed or may be located at the rear of the unit;
- g. The garage of any townhouse unit will not be more than 50% of the width of the unit;
- h. The driveway width of a townhouse unit will not be more than 50% of the front yard or 1.0 m wider than the width of the garage whichever is smaller;
- i. All units will have a designated parking space in front of their unit or located underground;
- j. Visitor parking will be centrally located, not visible from a public road and will be well screened from existing and proposed dwellings;
- k. Condominium townhouse developments greater than 20 units will provide a centrally located private amenity space;
- l. A minimum of 3.0 m will be required between blocks of units. A minimum of 4.5 m will be required between blocks of units that have a walkway;
- m. Hydro and gas metre walls will be required to be located internal to the site and will not be visible from the street;
- n. No service/loading, mailboxes or garbage area will be located along the frontage of the public road or visible from the public road;
- o. Entrances to new development will not be through established or existing lots, but will be from major roads and routes. The entrances to new developments will be flanked by dwellings within the new development itself;



**Figure B20** - Example of a Standard condominium townhouse development in Lakeview



**Figure B21** - side elevation of the townhouse condominium is upgraded to look like the front of a single detached dwelling to fit in with the lotting pattern

## 2.0 Lakeview Character Areas

- p. Fencing requirements will be minimized with built form acting as the prominent feature along all frontages. All fencing is to be returned within a maximum of 3.0 m of the rear corner of the dwelling; and
- q. End units exposed to an external or internal road will be required to have upgraded elevations.



**Figure B22** -Example of condominium townhouse developments in Lakeview



**Figure B23** -Example of condominium townhouse developments in Lakeview



**Figure B24**- Single detached units fronting onto a public road to ensure the lotting pattern and form are maintained on the residential street.

## 2.0 Lakeview Character Areas

### 2.2.4 Horizontal Multiple Dwellings

Horizontal multiple dwellings (often referred to as stacked units) may be located in existing neighbourhoods along major transit routes and corridors.

Appropriate sites are those that have a depth of 40 m or greater to ensure internal circulation, parking, amenity space, landscaping and utilities can be appropriately accommodated. The following criteria will be used when designing horizontal multiple dwellings:

- a. The building will be oriented to face the major public road and not be designed as a flankage condition;
- b. Garages will be located in a laneway and will not face the front door of another unit. Garages will face each other;
- c. Condominium blocks will not be more than 8 units wide;
- d. The minimum required landscape area will be 40% of the lot area;
- e. The preservation of existing trees on the street frontage and perimeter of the site will be required;
- f. Each unit will be required to have its own private amenity space, in the form of a balcony or roof top;
- g. A maximum of 3 stairs will be located at the entrance of any building. All other stairs will be required to be designed so that they are internal to the dwelling;
- h. A common amenity space will be required for a development with over 20 units. The common amenity space will be centrally located and will be the greater of 5.6 m<sup>2</sup> per dwelling unit or 10% of the site area. A minimum of 50% of the required amenity space will be provided in one contiguous area;
- i. A minimum of 3.0 m will be required between blocks of units. A minimum of 4.5 m will be required between blocks of units that have a walkway;
- j. A minimum of 15 m shall be required between the faces of buildings located along mews;
- k. Hydro and gas metre walls will be required to be internal to the site and not be visible from the street. In addition, utilities will not be located within the required landscape area or along the frontage of a public road;
- l. No common visitor parking, air-conditioning units, mailboxes or garbage area will be located along the frontage of the public road or visible from the public road; and,
- m. Fencing requirements will be minimized with built form acting as the prominent feature along all frontages. All fencing is to be returned within a maximum of 3.0 m of the rear corner of the dwelling.



**Figure B25** - Example of Horizontal Multiple Dwelling in Lakeview from an internal road



**Figure B26** Example of Horizontal Multiple Dwelling in Lakeview from Lakeshore Road East



## 2.0 Lakeview Character Areas



**Figure B27** - Example of Horizontal Multiple Dwelling in Lakeview, from Lakeshore Road East



**Figure B28** - Example of an entrance at grade of a Horizontal Multiple Dwelling in Lakeview



**Figure B29** - Example of Horizontal Multiple Dwelling in Lakeview from Deta Road. Example of Tree Preservation



**Figure B30** - Example of Horizontal Multiple Dwelling in Lakeview. Example of tree preservation within the new development.



**Figure B31** - Example of Horizontal Multiple Dwelling in Lakeview. Four entrances designed to appear as two



**Figure B32** - Example of Horizontal Multiple Dwelling in Lakeview. Rear lane condition with private amenity space above

# 2.0 Lakeview Character Areas

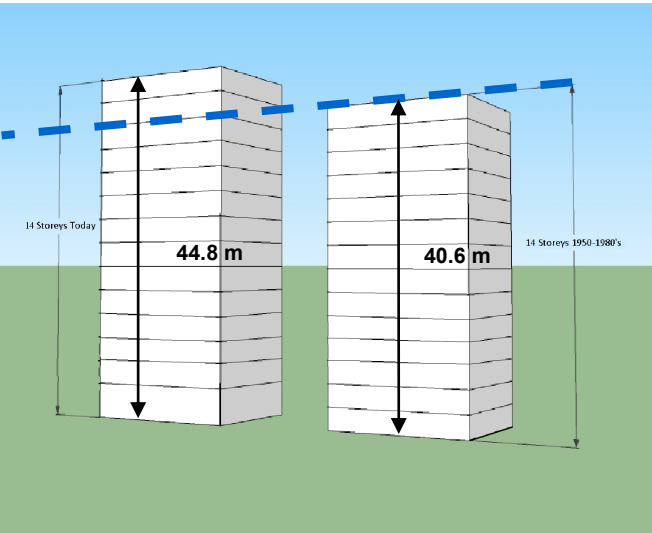
## 2.2.5 Apartment Dwellings

There are a number of apartment sites within the Lakeview area. These sites generally contain buildings 5 storeys and over and are located along arterial roads, major routes or in cluster developments.

Generally, high-rise residential cluster developments in Lakeview are considered “towers in the park”. These apartment buildings are surrounded by sunlight, open space and well landscaped yards. Additional sites may be considered for apartments due to their size and location, however these sites will be required to demonstrate that there is minimal impact on adjacent areas.

### 2.2.5.1 Building Heights

The maximum building height for any new high rise residential building in the Lakeview neighbourhood areas will be 14 storeys or 44.8 m. Sites that may be suited for high density will be required to demonstrate that they can accommodate a maximum of 14 storeys or 40.6 m.

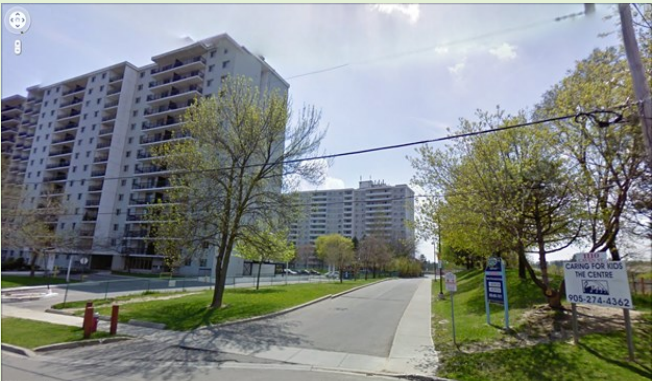


Era	Floor to Floor Heights
1950-1980's	Approx. 2.75 m (9.02 ft.)
1990-Current	Approx. 3.1 m (10.17 ft.)

**Figure B33** - Building heights from the 1950's to 1980's are significantly different than building heights from 1990 to today. Floor to ceiling heights during the 1950's and 80's were lower which reduced the overall heights of the buildings. The majority of buildings constructed in Lakeview were constructed between 1950 and 1980 and are therefore lower in height



**Figure B34** - 7 storey building. Example of existing high built form within Lakeview



**Figure B35** - 14 Storey building. Example of existing higher built form within Lakeview



## 2.0 Lakeview Character Areas

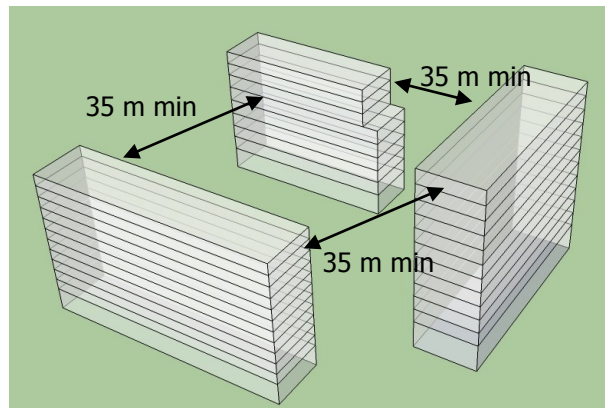
### 2.2.5.2 Building Separation Distances

There are a number of higher built form apartments existing in the Lakeview area. They are characterized by large landscape areas and significant separation distances to ensure light and permeability. This concept should be continued for new developments.

A building over 6 storeys or 20 m should have a minimum separation distance of 35 m to a building equivalent to, or greater than 6 storeys or 20 m.

### 2.2.5.3 Floor Plates

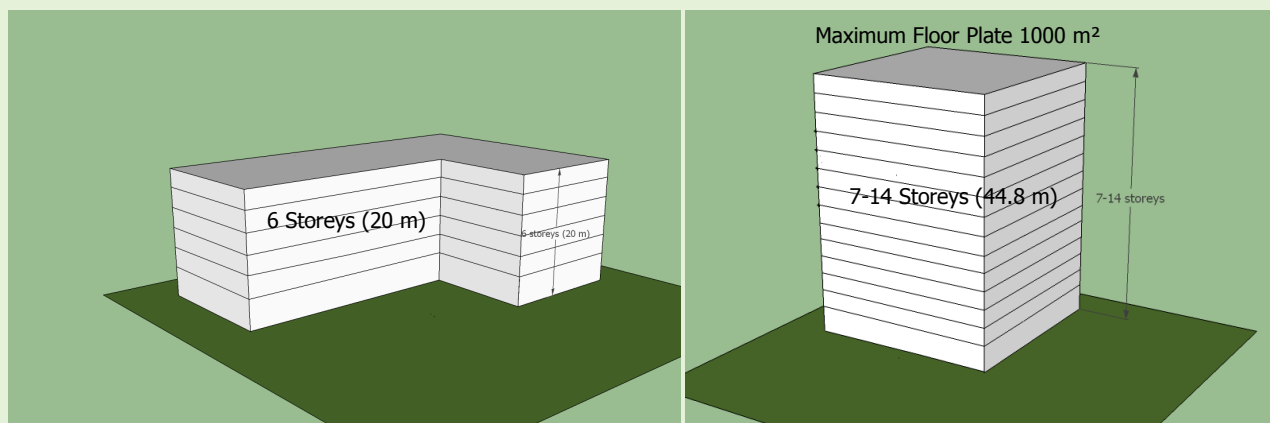
A building between 7 storeys (23 m) and 14 storeys (38.6 m) will have a maximum floor plate of 1000 m<sup>2</sup>, including the balconies, to ensure minimal impact on adjacent low rise residential and to maintain sky views.



**Figure B36** - Examples of existing building separation distances in Lakeview



**Figure B31** -Example of existing building separation distances in Lakeview



**Figure B37** -Example of building floor plates in relation to height

## 2.0 Lakeview Character Areas

### 2.2.5.4 Transition to Lower Forms

Taller buildings should be sited and organized in a way that provides desirable transition to adjacent lower form buildings and open space to ensure appropriate spatial separation between buildings.

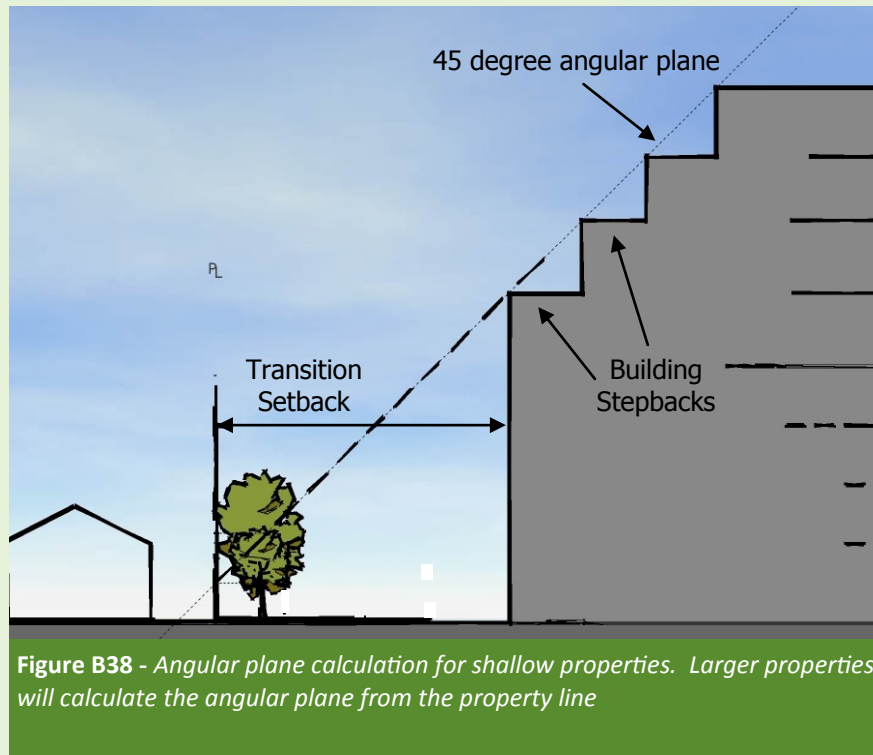
Where a significant difference in scale exists between building heights, developments will be required to deploy transition strategies through massing and built-form to achieve a harmonious relationship between proposed and existing development, and/or adjacent open space.

Appropriate transition can be achieved through various design methods. The larger the difference in scale the greater the need for transition. These may include:

- a. The use of an angular plane of 45 degrees from the closest property line of sites with lower scaled building or open space will be used to determine the minimum setback and height of a building within a development;
- b. To increase the building setback from a low rise development to ensure that the impact of the larger development is minimal; and

- c. The use of building step backs to ensure minimal impact from overshadowing and from a new building overwhelming the site.

Each of these controls can vary according to the size of the development area, the planned intensity of the use, the context of the low scale development, and the street width. Impacts to sunlight, shade and sky views will also be considered and will adhere to the City's *Standards for Shadow Studies*.



## 2.0 Lakeview Character Areas

### 2.2.5.5 Microclimatic Conditions

#### Shadow Impact

Shadow studies will be requested in support of Official Plan Amendments/Rezoning and Site Plan applications to demonstrate that the height and/or location of a proposed apartment building will not generate excessive shadows over adjacent lands.

Shadow studies will be required for buildings greater than 10.7 m in height which may cause new shadow impact on adjacent residential properties, public parkland, open space and the public realm. Particular attention will be focused on Lakeshore Road East and where a pedestrian oriented environment is strongly encouraged. See the City's *Standards for Shadow Studies*.

#### Wind Comfort

Wind studies will be requested for development over 3 storeys or 16 m in height to ensure appropriate comfort and safety levels are maintained in the pedestrian realm, streetscapes, public spaces and areas immediately adjacent to, and surrounding the proposed development.

Evaluation of existing wind conditions in the immediate and surrounding area, prior to the proposed development will be required along with a comparison of the wind conditions based on the proposed development. The criteria to be used for the analysis will be signed and sealed by a certified engineer.

*The Terms of Reference for Pedestrian Wind Comfort Studies will be used.*



**Figure B39** - Example of shadow on the public realm from an existing building



**Figure B40** - Example of wind in an urban environment

## 2.0 Lakeview Character Areas

### 2.2.6 Commercial

Lakeview has a number of commercial uses. Some of these larger commercial areas include, but are not limited to, the Dixie Outlet Mall and Applewood Village Plaza.

The following criteria will apply to the redevelopment of these areas:

- a. The maximum height of a building or structure will be 4 storeys or 13.8 m;
- b. New developments will transition to existing stable residential developments;
- c. When redevelopment occurs, larger sites will be broken up into smaller parcels. These smaller parcels should be bisected by public roads that interconnect with the existing community;
- d. New development should generally follow the pattern and character of the existing community. Higher built form will transition both in scale and lotting pattern to existing community uses;
- e. New development should ensure the continuation of a mixed use community;
- f. Where retail commercial is located on a mixed use street, entrances will face the street and be the dominant feature of the building. Loading and service areas shall not be visible from the street or existing residential; and,
- g. Mixed use developments which include townhouses, stacks or apartment dwellings will adhere to the Urban Design Handbook for Low-Rise Multiple Dwellings and the Design Reference note for Standards for Children's Outdoor Play Spaces.



**Figure B41** - Dixie Outlet Mall, example of existing commercial



**Figure B42** - Applewood Plaza, example of existing commercial



## 2.0 Lakeview Character Areas

### 2.2.7 Industrial

Lakeview has a number of historic industrial uses on the south side of Lakeshore Road East and along the rail line just north of Lakeshore Road East. A significant portion of the lands south of Lakeshore Road East are being reviewed under a separate study, *Inspiration Lakeview*.

This study will determine the built form, height and land use of these lands, and the Area Plan and Standards will be revised accordingly.

However, in the interim, the following general built form guidelines will be required when considering new developments in business employment areas:

- a. Industrial uses adjacent to residential areas will require a minimum 15 m setback to ensure an appropriate buffer area can be accommodated to screen the intensity of the use;
- b. A minimum landscape area ranging in depth from 4.5 m to 7.5 m of landscape area will be required in front of any employment use;
- c. Site access will be minimized and will be consolidated where possible;
- d. Loading, garbage and service areas will not face public roadways or residential uses. These services will be located behind the building, or, where this cannot be accommodated, these may be permitted at the side of the building. Landscaping will be required to screen service areas visible from the street;
- e. Parking will be located at the rear of the development and not between the front of the building and the street;
- f. Roof top units will not be visible from any street. The addition of parapet walls to screen these units is required;
- g. Buildings listed on the City's Heritage Register will be preserved and enhanced in their existing location;
- h. The preservation and enhancement of existing natural features will be a priority;
- i. All lighting will be contained within the site; and
- j. Existing industrial uses along the railway tracks are encouraged to improve their transition to the adjacent residential areas. The maximum permitted height will be the equivalent to a 2 storey residential building. New development will include appropriate buffers, ensure lighting, noise levels, loading and garbage areas do not negatively impact adjacent residential uses.

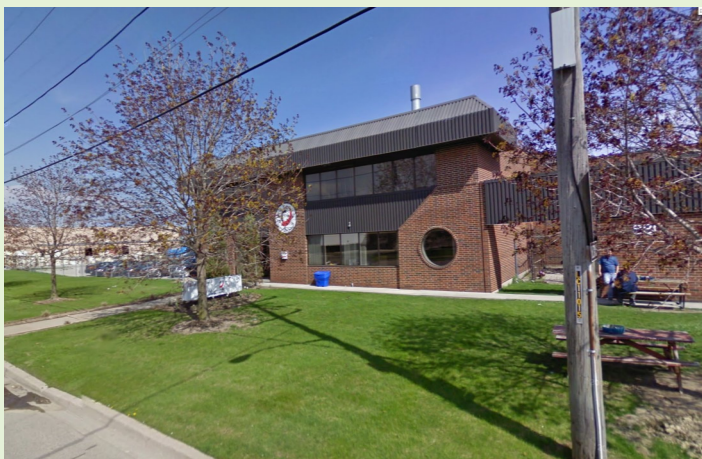


Figure B43- Example of existing Industrial, in Lakeview



Figure B44 - Example of Industrial Built form along the Rail Line north of Lakeshore Road East in Lakeview



## 2.0 Lakeview Character Areas

### 2.3 Routes, Landmarks and Views

Development will ensure views and routes are maintained and enhanced. Views to Lake Ontario from Lakeshore Road are important and will be enhanced and protected.

Landmarks are places, buildings or structures that are recognizable by people and that may have historical significance.

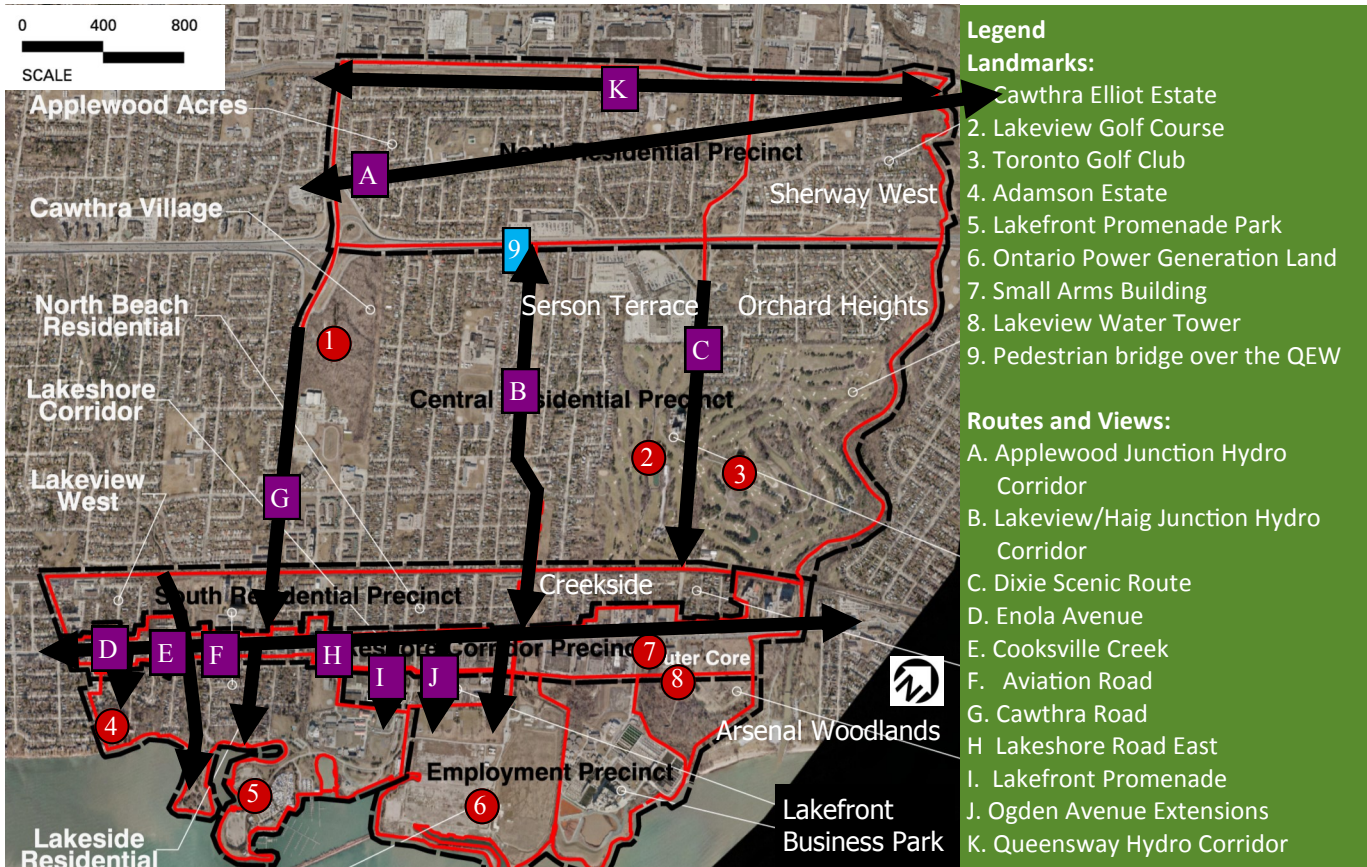
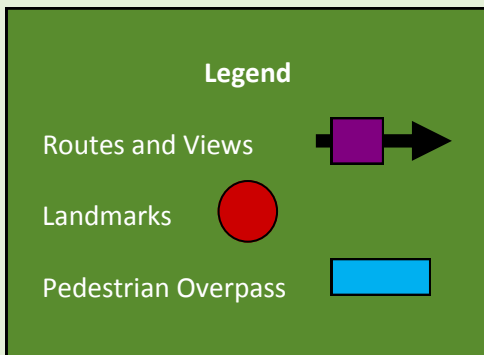


Figure B40 - Routes, Landmarks and Views





## 2.0 Lakeview Character Areas



Figure B47 - Cawthra Estates,



Figure B48 -Lakeview Golf Course



Figure B51 - Lakefront Promenade Park



Figure B49 -Adamson Estate



Figure B50- Lakefront Promenade Park



Figure B52 -Lakeshore Road East



Figure B53 -Water Tower



Figure B54 –Cooksville Creek Bridge

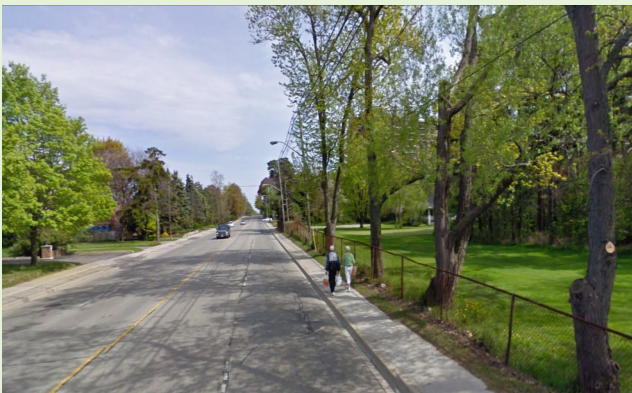


Figure B55 - Dixie Road Scenic Route



Figure B56 - Ontario Power Generation Lands

## 2.0 Lakeview Character Areas

### 2.4 Cultural Heritage

Heritage is an important characteristic of the Lakeview Local Area Plan. Within Lakeview there are a number of properties listed on the City's Heritage Register. The Register contains two types of properties:

- Designated (recognized by the City through by-law as being of cultural heritage value or interest); and
- Listed (identified but not fully researched as to heritage significance and has potential heritage value).

Lakeview contains *Cultural Landscapes* and *Cultural Features*. *Cultural Landscapes* are defined as a setting that enhance a community's vibrancy, aesthetic quality, distinctiveness, sense of history or sense of place. *Cultural Features* can be defined as visually distinctive objects and unique places within a cultural landscape. They are not necessarily consistent with their immediate natural surroundings, adjacent landscape, adjacent buildings or structures.

Properties designated or listed on the Heritage Register will be preserved in their existing location. Any development will incorporate these structures in the design of the proposal. Any changes to these structures or developments adjacent to these structures will require a Heritage Impact Statement and may have additional requirements. Additional requirements may include, but are not limited to, a review and approval from the Heritage Advisory Committee and City Council.

New buildings will not visually impede the setting of listed/designated heritage buildings and cultural landscapes. Where heritage buildings are low-scale, taller buildings will respect and reflect the unique character, topography and materials of the surrounding historic buildings. All new buildings will preserve and enhance the character and appearance of the setting of the adjacent listed/properties.

The following figures provides examples of properties listed or designated on the Heritage Register.



**Figure B57** - Arsenal Lands Water Tower, example of a Cultural Feature



**Figure B58** - Lakefront Promenade Park, example of a Cultural Landscape



## 2.0 Lakeview Character Areas



**Figure B59 - Johnston Residence**  
1414/1416 South Service Road



**Figure B60 - McGillion House and stable,**  
1559 Cormack Crescent



**Figure B61 - Cawthra Estate**



**Figure B62 - Small Arms Inspection Building,**  
Lakeshore Road East



**Figure B63 - Lakeview Park School,**  
1239 Lakeshore Road East



**Figure B64 - Pallett-McMaster House,**  
1346/1348/1400 Dixie Road



**Figure B65 - 11 Lakeview Golf Residence**



**Figure B66 - Capraru Residence,**  
1256 Dixie Road



**Figure B67 - Lakeview Golf Course,**  
example of a Cultural Landscape



**Figure B68 - Stone Bungalow,**  
1047 Dixie Road



**Figure B69 - Waseem Residence,**  
1273 St. James Avenue



**Figure B70 - Long Branch Indoor Rifle Range,**  
1300 Lakeshore Road East



## 2.0 Lakeview Character Areas

### 2.5 Pedestrian Realm/Streetscape

Neighbourhood Character Areas have an established streetscape particularly in the residential areas. These consist of a sidewalk on one or both sides adjacent to the curb edge (Figure B71); or set back from the street edge by landscape areas (Figure B73). A significant number of the residential streets however do not have sidewalks giving the image of a rural setting (Figure B72) which will be maintained.

At grade private amenity space will not be visible from the street for *Standard* and *Common Element Condominium Townhouse Developments*, horizontal multiple units and apartment developments.

All entrances to buildings will be prominently located on the street and designed in such a manner that it becomes the most important element of the building.

Tree preservation and protection of healthy trees, particularly City trees, within any development is a priority.



**Figure B71** - Local residential street with sidewalks on one side of the street, adjacent to the curb



**Figure B72** - Hedge Drive, local residential street, with no sidewalks



**Figure B73** - Atwater, local residential street with 2 sidewalks on either side with a grass buffer

## 2.0 Lakeview Character Areas

### 2.6 Environmental Sustainability

New developments must be sustainable in all aspects, taking into account social and economic impact, based on whole life costs and benefits. Advances in construction technology combined with a growing body of architectural knowledge mean that sustainable practices are easier to achieve.

On July 7, 2010, City Council adopted the Green Development Strategy which focuses on achieving sustainability and environmental responsibility in new development in Mississauga. The City strongly encourages applicants to incorporate green sustainable elements into proposed buildings, site works, construction methods and long-term maintenance programs. Further, applicants are encouraged to pursue LEED-NC credits required to achieve Silver certification.

For more information, visit the Canada Green Building Council web site for the LEED-NC Program Sustainable Technologies for Low Impact Development Stormwater Management Planning and Design Guide, and the City of Mississauga web site for the Green Development Strategy.



**Figure B74** — *Example of vertical parking grate screened by landscape*



**Figure B75** — *Example of enhanced dry grass swale*



**Figure B76** — *Typical rain water barrel*



**Figure B77** — *Extensive Green Roof Above — Mountain Equipment Co-op*



## 2.0 Lakeview Character Areas

### 2.7 Building Materials

Lakeview has a mixture of building materials throughout the neighbourhood areas. These include, brick, wood siding, stone, and siding. These materials should be used in the redevelopment of any site. Materials that are not predominant in Lakeview are discouraged, including architectural concrete block and stucco.

High quality building materials will be required in all new developments in Lakeview. The first 4 storeys of any new development will be of durable material such as brick or stone. Concrete block or painted concrete block are not permitted to be exposed for any new development.

The entrances to buildings will be prominent and treated with greatest priority. Entrances will be located on the dominant street they are located on.

Balconies should either be partially screened or have glass tinting so that materials that are stored on them are not visible from the public realm.

For mainstreet, vision glass will be required for all store fronts. Wood features are permitted as accent pieces. Canopies and architectural features area encouraged within the property line.

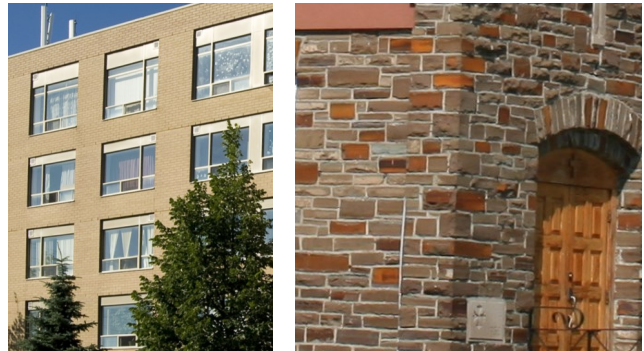


Figure B74 — Brick and Stone

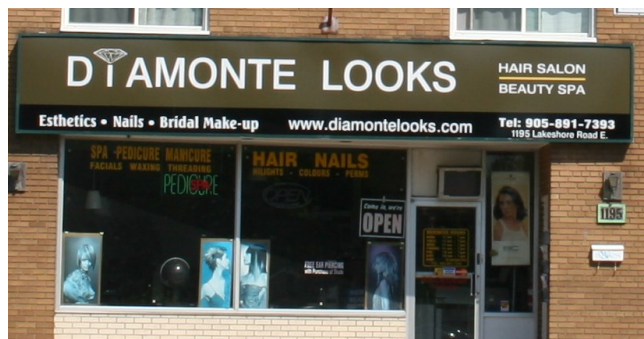


Figure B79 — Various brick types



Figure B80 — Balconies that are either partially screened or tinted are encouraged



Figure B81 — Visually translucent balconies are discouraged as they display balcony contents

## 2.0 Lakeview Character Areas



Figure B82 — *Stone*



Figure B83 — *Plaster*



Figure B84 — *Wood siding and shutters*



Figure B85 — *Siding*



Figure B86 — *Combination*



Figure B87 — *Aluminium*



Figure B88 — *Brick*



Figure B89 — *Brick and wood*



Figure B90 — *Brick*



Figure B91 — *Exposed concrete block or painted concrete block will not be permitted.*



Figure B92 — *Architectural Concrete Block is highly discouraged as an exterior building material.*



# 3.0 Lakeshore Corridor Precinct

## 3.1 Lakeshore Corridor Precinct

The Lakeshore Corridor Precinct has a unique identity and function in the community. It contains a mix of uses and a variety of built form. The Lakeshore Corridor Precinct contains a larger neighbourhood area and a portion of the employment lands.

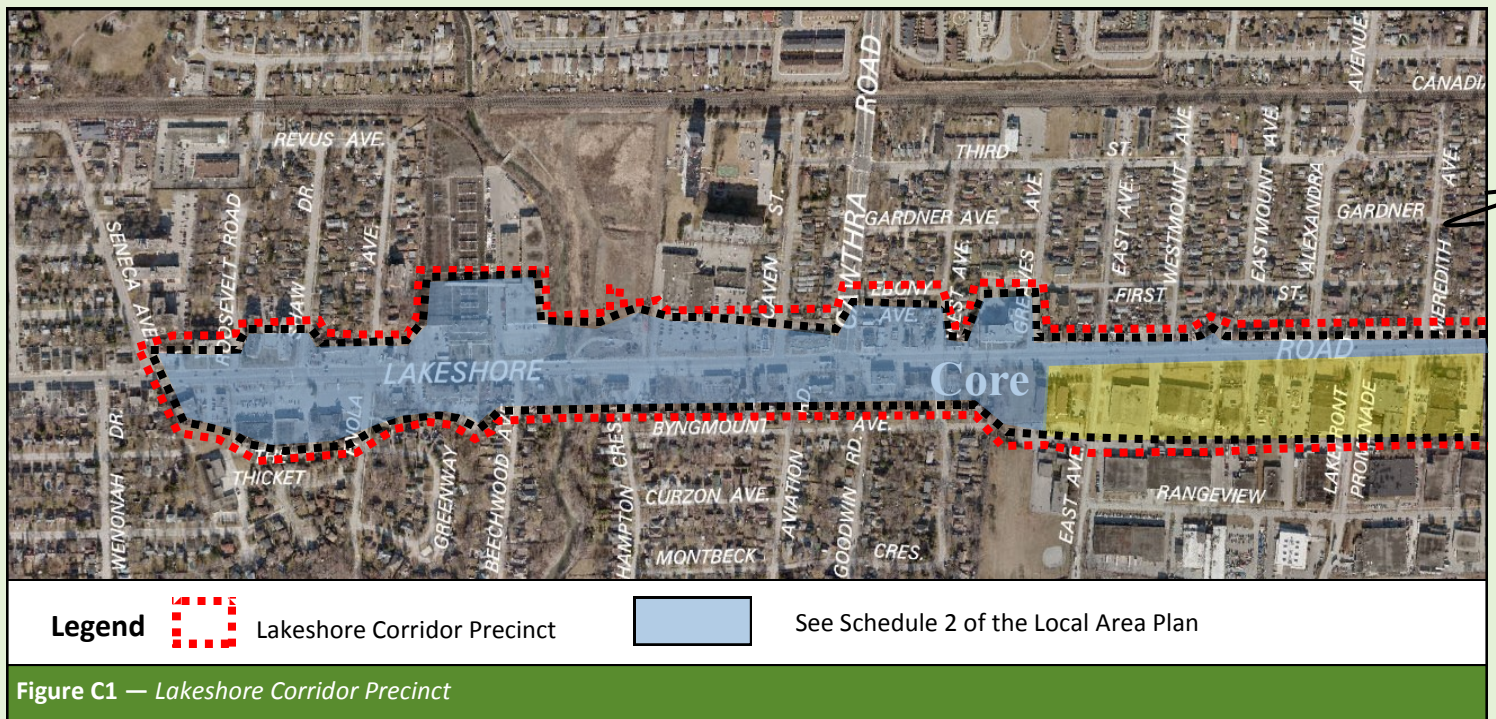
While the Neighbourhood policies in the previous section also apply to this precinct, additional development criteria must be adhered to.

The principles of built form along Lakeshore Road East will include:

- i) A pedestrian oriented environment;
- ii) Closely spaced buildings fronting onto Lakeshore Road East;

- iii) Minimize access points;
- iv) No parking between the building and the street;
- v) Design that enhances a mainstreet retail environment; and
- vi) On-street parking along Lakeshore Road East where appropriate.

In recent years, development interest is gradually changing this area into a new mainstreet, with new mixed use buildings along the corridor. The Lakeshore Corridor Precinct is linear, and includes properties fronting along Lakeshore Road East from Seneca Avenue to the eastern boundary of Mississauga.



## 3.0 Lakeshore Corridor Precinct

The Lakeshore Road East Corridor is broken down into two sub areas:

- **The Core**, which is described as the area from Seneca Avenue to Hydro Road. Retail will be required at grade fronting onto Lakeshore Road East. Buildings should be set back 0.6 m to 3.0 m; and,
- **The Outer Core**, which is described as the area from Hydro Road to the Etobicoke Creek and the eastern boundary of the City of Mississauga. Retail is encouraged to front onto Lakeshore Road East but not required. Where residential fronts onto Lakeshore Road East, in the Outer Core area, buildings should be set back from the street to ensure a well landscaped front yard and appropriate streetscape.

In addition, the lands highlighted in yellow are lands that are affected by the Inspiration Lakeview Study and will be dealt with through that process.

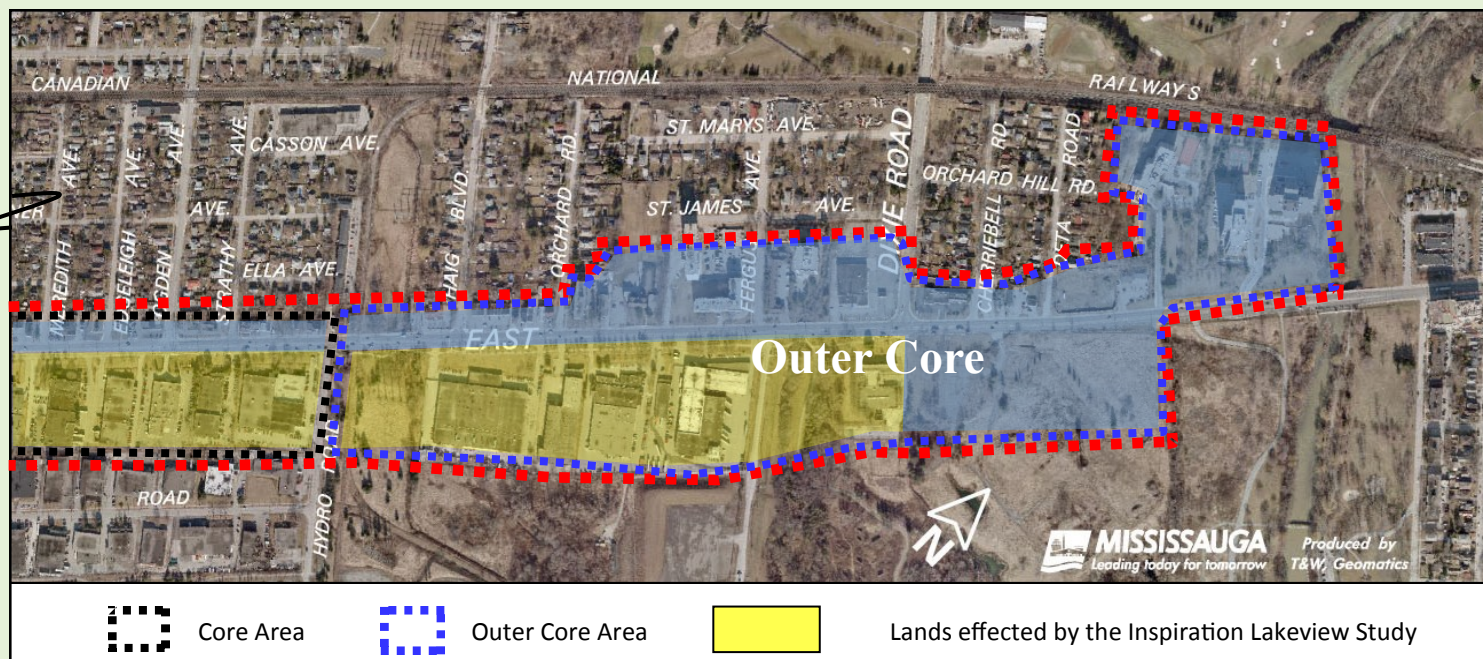


Figure C1 (Continued) — Lakeshore Corridor Precinct



# 3.0 Lakeshore Corridor Precinct

## 3.2 Building Heights

It is anticipated that the majority of significant redevelopment within Lakeview will occur along Lakeshore Road East. The corridor will be the focus of activity for Lakeview, combining a mix of uses including residential uses, cultural activities, shopping, dining, commerce and recreation. Development along Lakeshore Road East will be linear and maintain lower building forms to ensure that developments transition appropriately to the neighbourhood lands both north and south of Lakeshore Road East. The lands highlighted in yellow are lands affected by the Inspiration Lakeview Study.

The following criteria will apply to development in the Lakeshore Corridor Precinct:

- a) The minimum building height along Lakeshore Road East highlighted in blue will be 2 storeys and the maximum building height permitted is 4 storeys, however some sites will be permitted to have building heights of more than 4 storeys as shown on Schedule 2 of the Local Area Plan;
- b) Development along Lakeshore Road East will be close to the street and have a minimum setback of 0.6 m and a maximum setback of 3.0 m from the property line. The appropriate setback will be

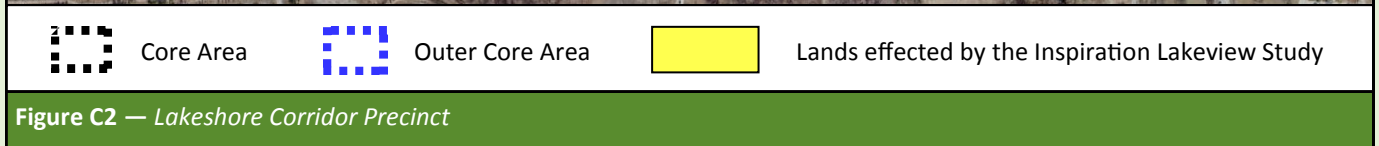
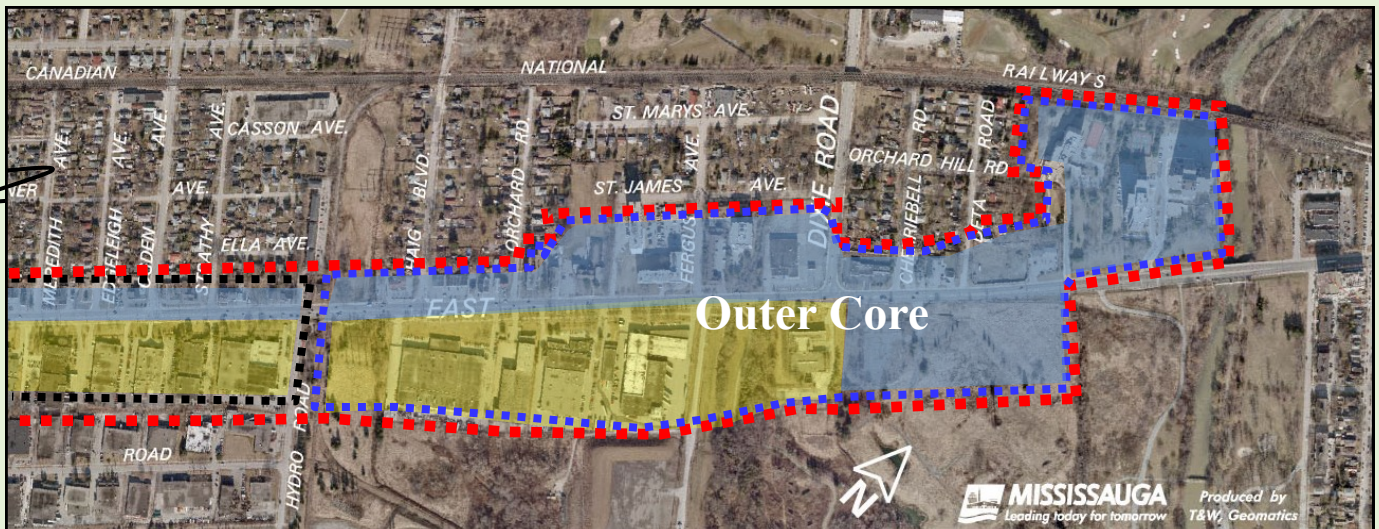
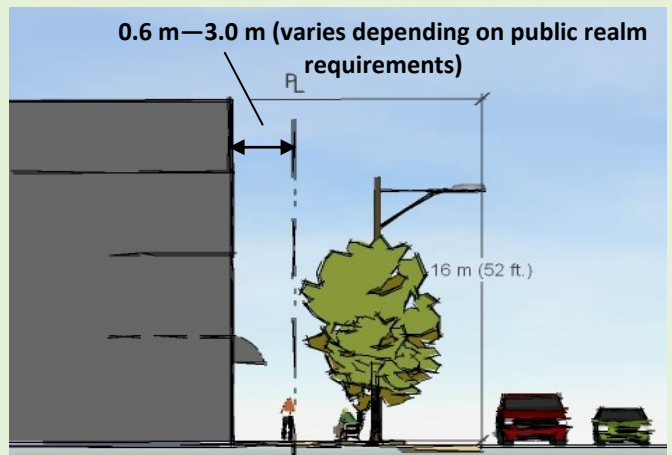
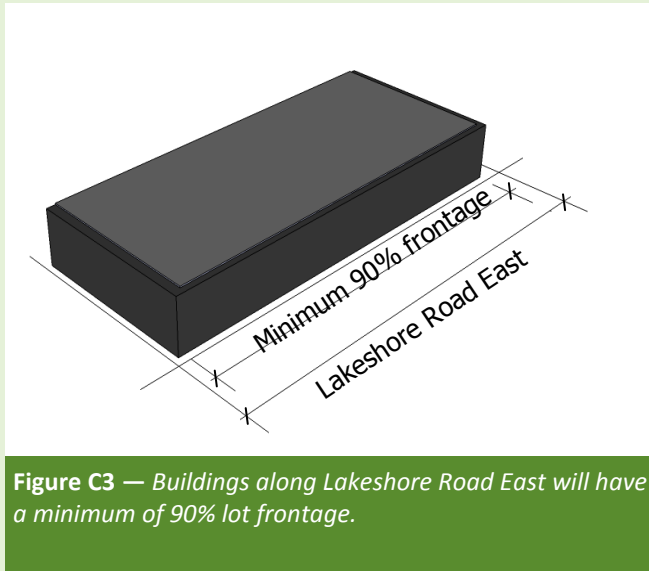
determined through an analysis of the public realm and streetscape treatments. Additional setbacks may be required to ensure an appropriate pedestrian realm can be accommodated due to the location of the utilities and right-of-way widths;

- c) Buildings fronting onto Lakeshore Road East should have a minimum of 90% of the building wall within 0.6 m to 3.0 m from the front property line;
- d) Building entrances will be located along Lakeshore Road East;
- e) Canopies, overhangs and signage will be designed so that they are located within the private property limits;
- f) Where residential buildings are permitted a minimum setback of 7.5 m from Lakeshore Road East will be required to ensure appropriate transition to Lakeshore Road East; and
- g) Buildings will transition down to stable residential areas (see Section 3.3 for details).



**Legend**  
  Lakeshore Corridor Precinct  
  See Schedule 2 of the Local Area Plan  
**Figure C2 — Lakeshore Corridor Precinct**

# 3.0 Lakeshore Corridor Precinct





## 3.0 Lakeshore Corridor Precinct

### 3.3 Transition to Lower Built Form and Open Space

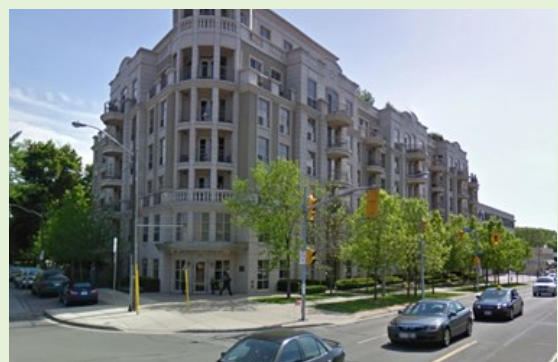
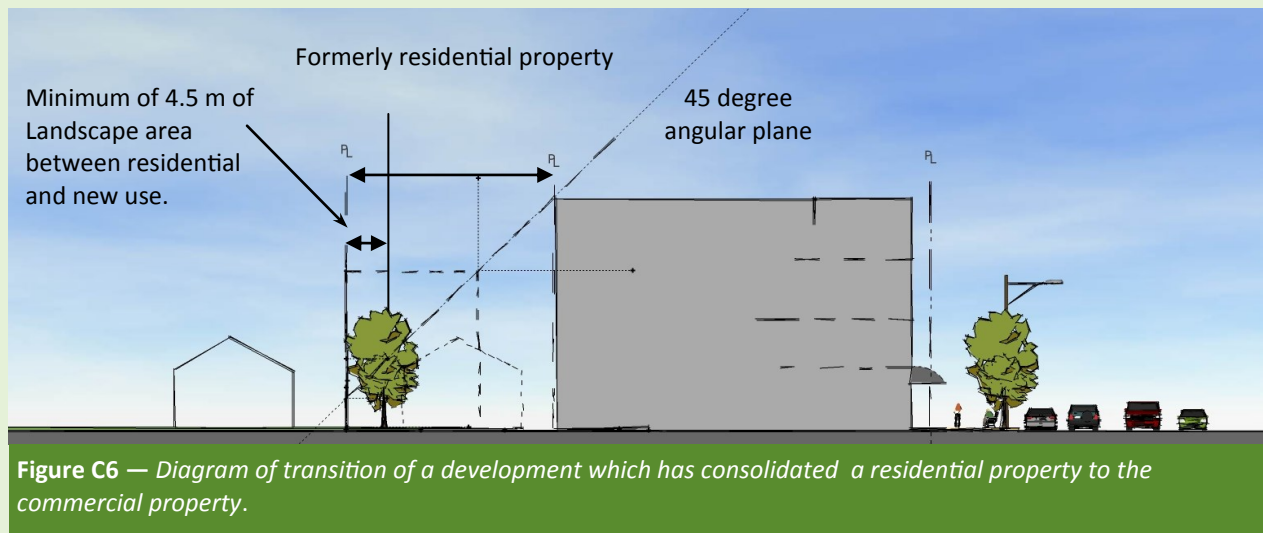
The assembly of adjacent stable residential lands to enlarge properties fronting Lakeshore Road East is discouraged. However, if this does occur, no building or structure will be permitted on the former residential property.

To ensure that residential properties have adequate light, view and privacy, a 45 degree angular plane will be required (see sketch below C6).

### 3.4 Rear Yard Landscape Buffer

A minimum of 4.5 m wide unobstructed landscape buffer will be required when a mixed use zone abuts a residential zone to screen buildings from adjacent residential properties. Through the site plan process, additional recommendations, such as the location and type of planting will be provided to ensure effective screening. This helps to ensure that trees and vegetation on the existing property and adjacent properties are preserved and enhanced. A 1.2 m fence will be required between the residential and mixed use zone to further ensure buffering of uses.

Utilities, walkways, amenity space and garbage areas will not be permitted in the 4.5 m landscape buffer.



## 3.0 Lakeshore Corridor Precinct

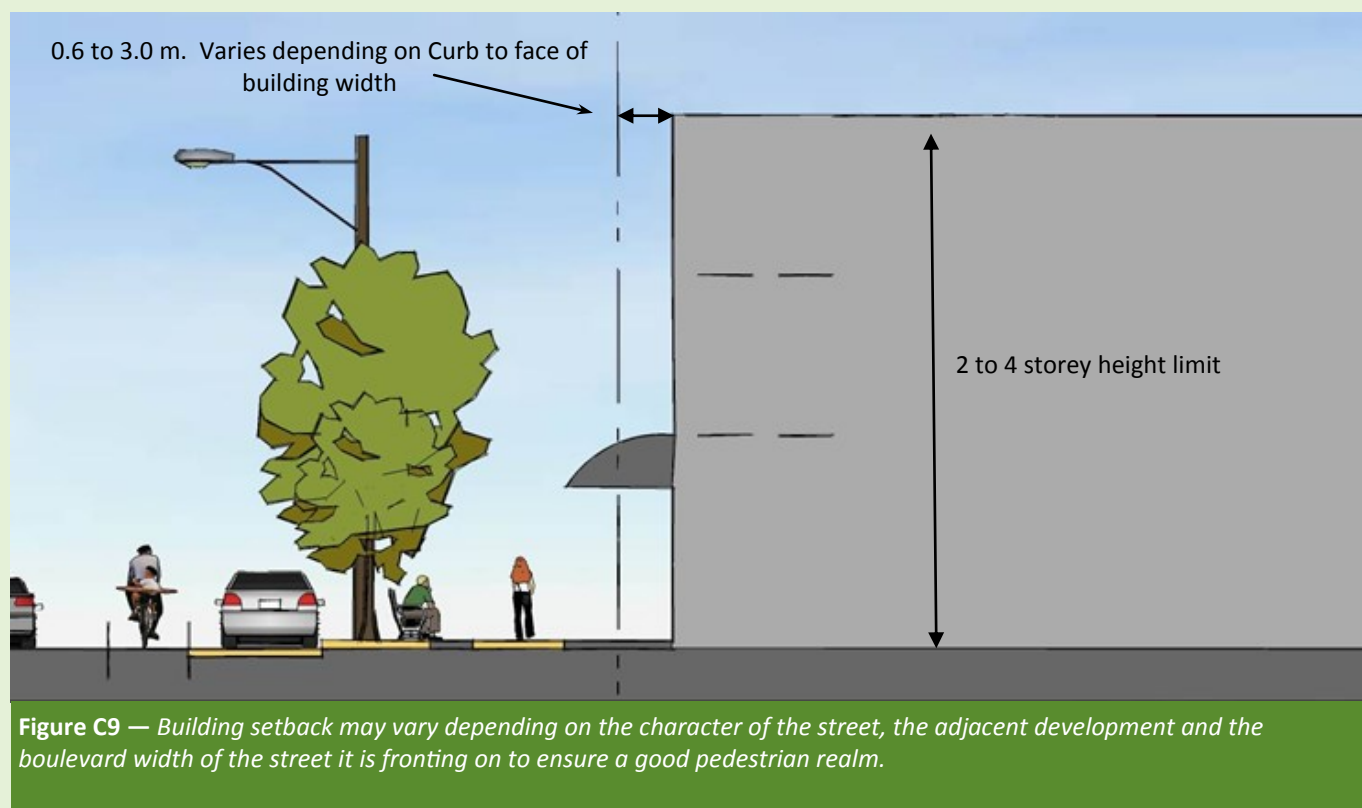
### 3.5 Pedestrian Realm/Streetscape

Building setbacks along Lakeshore Road East are to provide a consistent street edge. Where buildings are discontinuous along the street, the street edge should be defined through landscape elements such as street trees, plantings, low-level walls and decorative fences, pergolas, or acceptable alternatives.

- a) Building setback along Lakeshore Road East will be a minimum of 0.6 m to 3.0 m. The exact building setback will be determined through streetscape analysis to ensure that the boulevard width is a minimum of 5.6 m from the street curb to the face of the building to ensure a consistent and viable pedestrian sidewalk and the potential for a tree zone and street furniture. Depending on the location of the utilities, the boulevard width may need to be

increased;

- b) New buildings should form a continuous street wall. There should be minimal breaks in the streets frontages to encourage a pedestrian friendly environment. 90% of the frontage should be occupied by the building façade; and
- c) Street trees, street furniture, such as benches, banners, waste receptacles, and bike racks, will be required within the boulevard.



## 3.0 Lakeshore Corridor Precinct

### 3.6 At Grade Commercial Requirements

To promote a pedestrian friendly mainstreet environment, street related retail commercial uses will be located along, and front onto Lakeshore Road East. A maximum lot depth of 55 m for commercial uses will be maintained.

- a) Building entrances should be located along and face Lakeshore Road East;
  - b) Generally, retail areas require a minimum of 4.5 m of clear height from grade and a minimum of 15 m width;
  - c) Minimum of 60% glass will be required for retail storefronts along the street wall;
  - d) Minimum 6 m store front extension around the corner
- e) Signs will be limited to the first floor level;
  - f) Tenant signage will be of a consistent design if there is more than one tenant in a building;
  - g) Retail tenants signs will be designed of high quality material, colour and scale to compliment the remainder of the building;
  - h) Ground signs are prohibited;
  - i) Store front window signage is permitted up to 25% of the glass surface area and will not block the clear view of entrances; and
  - j) Tables and other active uses adjacent to storefront windows are encouraged.



Figure C10 — Examples of retail commercial at grade

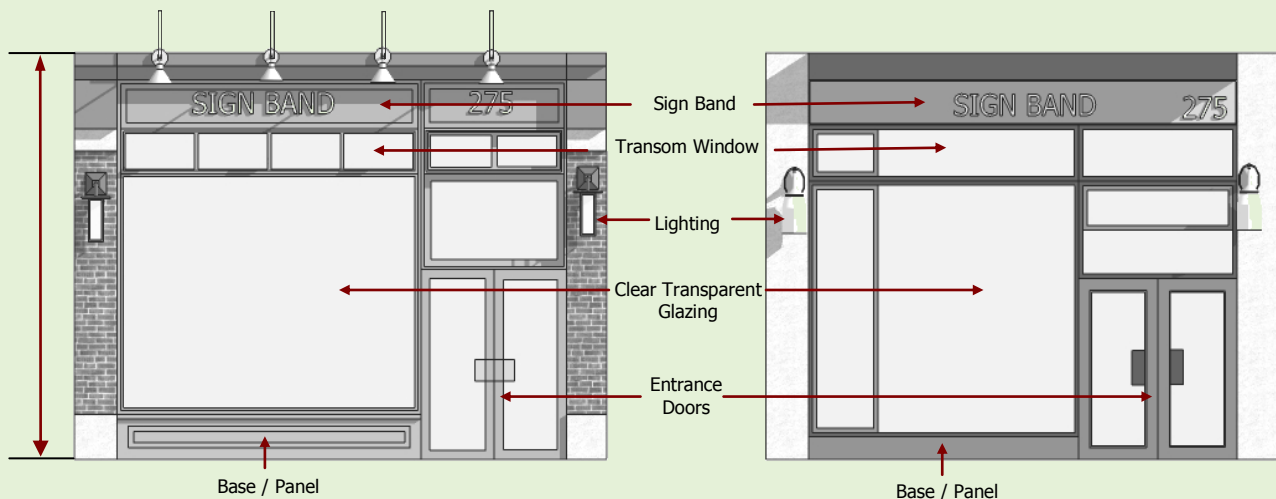


Figure C11 — Examples of retail treatment at grade.



## 3.0 Lakeshore Corridor Precinct

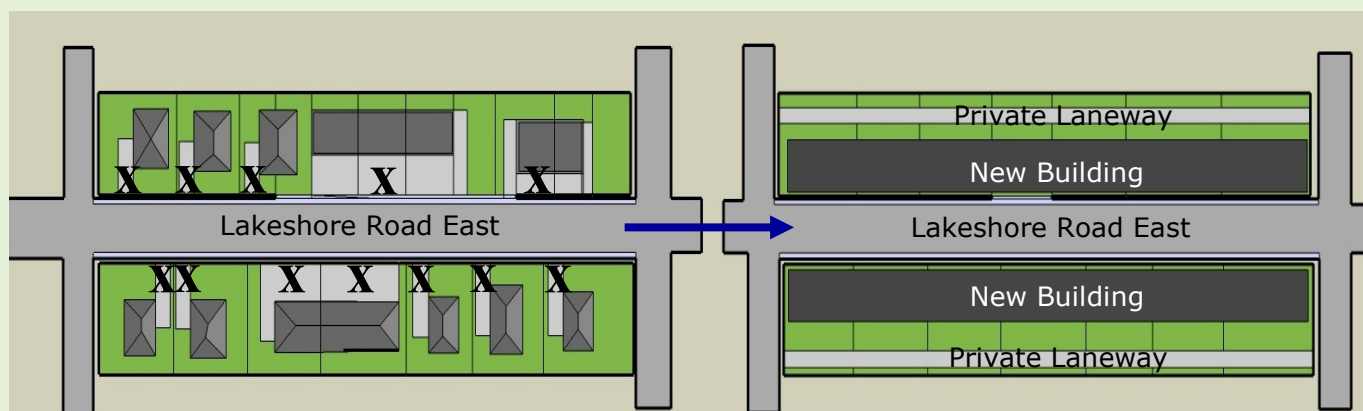
### 3.7 Access Points

Consolidation of vehicle access points for properties fronting along Lakeshore Road East will be encouraged to minimize the requirement for mid-block access points from Lakeshore Road East.

Vehicle access for redevelopment should be considered from existing north/south side streets.



**Figure C12** — *Parking at the front of the property along Lakeshore Road East with Access Points all along the front*



**Figure C13** — *Example of Access consolidation which is required to make an urban street pedestrian oriented and safe.*



**Figure C14** - *Example of multiple vehicle access conditions along Lakeshore Road East*



**Figure C15** - *Example of multiple vehicle access conditions along Lakeshore Road East*

## 3.0 Lakeshore Corridor Precinct

### 3.8 Parking, Loading and Service Areas

The design of parking, servicing and loading areas for non-residential uses is a key component in the development of sites. These areas serve a functional need, but should be designed in a manner that screens these areas and provides high quality treatment of exposed areas while addressing safe and efficient movement of pedestrians and vehicles.

- a) Parking should be located underground, internal to the building or to the rear of the building where it is not visible from the streets, particularly on Lakeshore Road East;
- b) Above grade parking structures should be screened in such a manner that vehicles are not visible to the public, be designed to compliment adjacent buildings and materials, and with appropriate directional signage to the structure;
- c) Service, loading and garbage storage areas should be integrated into the building or located at the rear of the building and screened from the public realm and adjacent residential uses. Screen walls may be used, provided they are the same material as the building. Alternatively, landscape material may be used where there is ample room for generous treatment.





## 3.0 Lakeshore Corridor Precinct

### 3.9 Location of On-Street and Lay-By Parking

On-street and lay-by parking will provide accessible parking in proximity to retail commercial and office space. Where on-street and lay-by parking can be accommodated, it is to be incorporated into the streetscape design.

Lay-by parking should be delineated by islands to ensure safety for pedestrian and vehicles (see Figure C21).



Figure C18 — Example of On-street parking



Figure C19 — Example of Lay-by Parking



Figure C20 — Example of Lay-by Parking



Figure C21 — Example of Lay-by Parking







## 3.0 Lakeshore Corridor



**Figure C25** — Former Rail line looking south from Lakeshore Road -Place Making Opportunities



**Figure C26** — Waterfront Trail along the south side of Lakeshore Road East



**Figure C27** — Small Arms Inspection Building, Place Making Opportunities



**Figure C28** — Etobicoke Creek, Mississauga eastern border



**Figure C22 (Continued)** — Place Making Opportunities

**Legend**

- 5. 1019 and 1041 Lakeshore Road East
- 6. Hydro Road, adjacent to the railway tracks
- 7. Lakeshore Road Bicycle Path
- 8. Small Arms Building
- 9. Etobicoke Creek



## 4.0 Appendix A

Page	Description	Source
Cover	Church on Lakeshore Road East	Dan Magee, City of Mississauga
	Region of Peel, 5 Storey Building	Adishesan Shanker from ASAP Photography
	Adamson Estate	Adishesan Shanker from ASAP Photography
Page 2	Historical House on North Service Road	City of Mississauga Image Library
	OPG Lands, old railway lands	Sharon Mittmann, City of Mississauga
	Lakeshore Road East Buildings	Sharon Mittmann, City of Mississauga
	Lakefront Promenade	Adishesan Shanker from ASAP Photography
	OPG Pier	Sharon Mittmann, City of Mississauga
	Hydro Corridor	Adishesan Shanker from ASAP Photography
	Live work Units on Lakeshore Road East	Adishesan Shanker from ASAP Photography
	Townhouse development on Northmount Avenue	Adishesan Shanker from ASAP Photography
Page 3	Cawthra Estate	Adishesan Shanker from ASAP Photography
	Townhouse development on Northmount Avenue	Adishesan Shanker from ASAP Photography
	White Building on Lakeshore Road	Adishesan Shanker from ASAP Photography
	Tower in the Park, Lakeview	Sharon Mittmann, City of Mississauga
	Lakefront Promenade	Adishesan Shanker from ASAP Photography
	Bungalow residential	Sharon Mittmann, City of Mississauga
	Bungalow residential	Sharon Mittmann, City of Mississauga
Page 4	Lakeview Entry Sign	City of Mississauga Image Library
	Region of Peel, 5 Storey Building	Adishesan Shanker from ASAP Photography
	Townhouse development on Northmount Avenue	Adishesan Shanker from ASAP Photography
	Industrial Building	Adishesan Shanker from ASAP Photography
	Detached Bungalow, Lakeview	City of Mississauga Image Library
Page 5	Region of Peel Waste Water Treatment Plant	Adishesan Shanker from ASAP Photography
	Adamson Estate	Adishesan Shanker from ASAP Photography
	Lakefront Promenade Park sign	Sharon Mittmann, City of Mississauga
	Lakeshore Road East Buildings	Sharon Mittmann, City of Mississauga
	OPG Pier	Sharon Mittmann, City of Mississauga
Page 6	Mississauga Transit Bus	Sharon Mittmann, City of Mississauga
	Region of Peel Waste Water Treatment Plant	Adishesan Shanker from ASAP Photography
	Lakeshore Road East Building	Sharon Mittmann, City of Mississauga
	Detached Bungalow	Sharon Mittmann, City of Mississauga
	Dixie Outlet Mall	Adishesan Shanker from ASAP Photography
	Cyclist	Adishesan Shanker from ASAP Photography
Page 7	Bus Shelter	Sharon Mittmann, City of Mississauga
	Buildings on Lakeshore Road West	Sharon Mittmann, City of Mississauga
	Arsenal Building	Sharon Mittmann, City of Mississauga
	Lakeshore Road East cyclists	Sharon Mittmann, City of Mississauga
	Waterfront Trail along Lakeshore Road	City of Mississauga Image Library
	OPG Lands Railway	Sharon Mittmann, City of Mississauga
Page 8	Townhouse development on Northmount Avenue	Adishesan Shanker from ASAP Photography
	Region of Peel Waste Water Treatment Plant	Adishesan Shanker from ASAP Photography
	Industrial Building	Adishesan Shanker from ASAP Photography
	One and a half storey dwelling	Dan Magee, City of Mississauga
	Bungalow	Dan Magee, City of Mississauga
	Two storey semi detached dwelling	Dan Magee, City of Mississauga
	Single Detached Dwelling	Dan Magee, City of Mississauga
	Single Detached Dwelling	Dan Magee, City of Mississauga
	Bungalow Dwelling	Dan Magee, City of Mississauga



## 4.0 Appendix A

	7 Storey Apartment Dwelling	Dan Magee, City of Mississauga
	Industrial Building	Dan Magee, City of Mississauga
	Two Storey Detached	Dan Magee, City of Mississauga
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Page 10	Duplex Dwelling	Dan Magee, City of Mississauga
	Bungalow Dwelling	Dan Magee, City of Mississauga
	Two Storey Dwelling	Dan Magee, City of Mississauga
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	Semi Detached Dwelling	Dan Magee, City of Mississauga
Page 12	Townhouse developments in Churchill Meadows	Google Maps
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	Side Elevation of a Townhouse Development	Google Maps
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Page 14	Example of a Standard Detached Condominium	Google Maps
	Side elevation of a Standard Condominium	Google Maps
Page 15	Example of detached condominium streetscape	Google Maps
	Example of detached condominium from a public road	Google Maps
	Single detached units facing a public road	Google Maps
	Single detached units facing a condominium road.	Google Maps
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	side elevation of the townhouse condominium	Adiseshan Shanker from ASAP Photography
Page 17	Examples of condominium townhouse developments in Lakeview	Adiseshan Shanker from ASAP Photography
	Examples of condominium townhouse developments in Lakeview	Adiseshan Shanker from ASAP Photography
	Single detached units fronting onto a public road to ensure the lotting pattern and form are maintained on the residential street.	Adiseshan Shanker from ASAP Photography
Page 18	Examples of Horizontal Multiple Dwelling in Lakeview from an internal road	Dan Magee, City of Mississauga
	Examples of Horizontal Multiple Dwelling in Lakeview from Lakeshore Road East	Dan Magee, City of Mississauga
Page 19	Horizontal Multiple Dwelling in Lakeview from Deta Road	Dan Magee, City of Mississauga
	Horizontal Multiple Dwelling in Lakeview, from Lakeshore Road East	Dan Magee, City of Mississauga
	Horizontal Multiple Dwelling in Lakeview. Four entrances designed to appear as two.	Dan Magee, City of Mississauga
	entrance at grade of a Horizontal Multiple Dwelling in Lakeview,	Dan Magee, City of Mississauga
	Horizontal Multiple Dwelling in Lakeview. Example of tree preservation within the new development.	Dan Magee, City of Mississauga
	Horizontal Multiple Dwelling in Lakeview. Rear lane condition with private amenity space above.	Dan Magee, City of Mississauga

## 4.0 Appendix A

Page	Description	Source
Page 20	Graphic – Building Heights	Sharon Mittmann, City of Mississauga
	7 storey building. Example of existing high built form within Lakeview	Sharon Mittmann, City of Mississauga
	14 Storey building. Example of existing higher built form within Lakeview	Sharon Mittmann, City of Mississauga
Page 21	Example of building floor plates in relation to height.	Sharon Mittmann, City of Mississauga
	Examples of existing building separation distances in Lakeview	Sharon Mittmann, City of Mississauga
	Examples of existing building separation distances in Lakeview.	Sharon Mittmann, City of Mississauga
Page 22	Angular plane calculation for shallow properties taken. Larger properties will calculate the angular plan from the property line	Sharon Mittmann, City of Mississauga
Page 23	Examples of shadow on the public realm from an existing building	Sharon Mittmann, City of Mississauga
	Examples of wind in an urban environment	Google Images
Page 24	Dixie Outlet Mall -example of existing commercial	Adishesan Shanker from ASAP Photography
	Applewood Plaza, example of existing commercial	Adishesan Shanker from ASAP Photography
Page 25	Example of existing Industrial, in Lakeview	Adishesan Shanker from ASAP Photography
	Example of Industrial Built form along the Rail Line north of Lakeshore Road East in Lakeview	Karin Phuong, City of Mississauga
Page 26	Routes, Landmarks and View Map	Geomatics, City of Mississauga
	A. Hydro Corridor from Breezey Brae Drive	Adishesan Shanker from ASAP Photography
	B. Hydro Corridor from Halliday Avenue	Adishesan Shanker from ASAP Photography
Page 27	Cawthra Estate	Adishesan Shanker from ASAP Photography
	Lakeview Golf Course	Adishesan Shanker from ASAP Photography
	Adamson Estate	Adishesan Shanker from ASAP Photography
	Lakefront Promenade Park	Adishesan Shanker from ASAP Photography
	Lakefront Promenade Park	Sharon Mittmann, City of Mississauga
	Lakeshore Road East	Sharon Mittmann, City of Mississauga
	Water Tower	Adishesan Shanker from ASAP Photography
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	Dixie Road Scenic Route	Google Maps
	Ontario Power Generation Lands	Mississauga Image Library
Page 28	Arsenal Lands Water Tower, example of a Cultural Feature	Adishesan Shanker from ASAP Photography
	Lakefront Promenade Park, example of a Cultural Landscape	Adishesan Shanker from ASAP Photography
Page 29	Johnston Residence 1414/1416 South Service Road	City of Mississauga, Image Library
	McGillion House and stable, 1559 Cormack Crescent	City of Mississauga, Image Library
	Cawthra Estate	City of Mississauga, Image Library
	Small Arms Inspection Building, Lakeshore Road East	Sharon Mittmann, Image Library
	Lakeview Park School, 1239 Lakeshore Road East	City of Mississauga, Image Library
	Pallett-McMaster House, 1346/1348/1400 Dixie Road	City of Mississauga, Image Library
	11 Lakeview Gold Residence	Adishesan Shanker from ASAP Photography
	Capraru Residence, 1256 Dixie Road	City of Mississauga, Image Library
	Lakeview Golf Course, example of a Cultural Landscape	City of Mississauga, Image Library
	Stone Bungalow, 1047 Dixie Road	City of Mississauga, Image Library
	Waseem Residence, 1273 St. James Avenue	City of Mississauga, Image Library
	Long Branch Indoor Rifle Range, 1300 Lakeshore Road East	City of Mississauga, Image Library



## 4.0 Appendix A

Page 30	Local residential street with sidewalks on one side of the street, adjacent to the curb	Google Maps
	Hedge Drive, local residential street, with no sidewalks	Google Maps
	Atwater, local residential street with 2 sidewalks on either side with a grass buffer	Google Maps
Page 31	Example of vertical parking grate screened by landscape	Green Development Strategy - Image
	Example of enhanced dry grass swale	Green Development Strategy - Image
	Typical rain water barrel	Green Development Strategy - Image
	Extensive' Green Roof Above — Mountain Coop Toronto	Green Development Strategy - Image
Page 32	Brick and Stone	Google Images
	Various brick types	Google Images
	Balconies that are either partially screened or tinted are encouraged	Google Images
	Visually translucent balconies are discouraged as they display balcony contents	Google Images
Page 33	Stone Material	City of Mississauga, Image Library
	Plaster Material	City of Mississauga, Image Library
	Wood and siding shutters	City of Mississauga, Image Library
	Siding Material	City of Mississauga, Image Library
	Combination Brick and metal	City of Mississauga, Image Library
	Aluminum Material	City of Mississauga, Image Library
	Brick Material	City of Mississauga, Image Library
	Brick and Wood Material	City of Mississauga, Image Library
	Brick Material	City of Mississauga, Image Library
	Exposed concrete block or painted concrete block will not be permitted in this area.	City of Mississauga, Image Library
	Architectural Concrete Block is highly discouraged as an exterior building material.	City of Mississauga, Image Library
Page 34-35	Lakeshore Corridor Precinct	City of Mississauga, Geomatics
Page 36	Lakeshore Corridor Building Heights	City of Mississauga, Geomatics
Page 37	Canopies, overhangs and signage	City of Mississauga, Image Library
	Buildings along Lakeshore Road East, will have a minimum of 90% frontage.	City of Mississauga, Sharon Mittmann
	Buildings along Lakeshore Road East, will have a minimum of 90% frontage.	City of Mississauga, Sharon Mittmann
Page 38	Diagram of transition of a development which has consolidated a residential property to the commercial property.	City of Mississauga, Sharon Mittmann
	Example of the above diagram using the old residential property to buffer the higher built form.	City of Mississauga, Sharon Mittmann
	Front of the higher building form shown to the left	City of Mississauga, Sharon Mittmann
Page 39	Building setback may vary depending on the character of the street, the adjacent development and the boulevard width of the street it is fronting on to ensure a good pedestrian realm.	City of Mississauga, Sharon Mittmann
Page 40	Examples of retail commercial at grade	Google images
	Examples of retail treatment at grade	City of Mississauga, Steven Bell



## 4.0 Appendix A

Page	Description	Source
Page 41	Parking at the front of the property along Lakeshore Road East with Access Points all along the front	City of Mississauga, Sharon Mittmann
	Example of Access consolidation which is required to make an urban street pedestrian oriented and safe.	City of Mississauga, Sharon Mittmann
	Existing examples of access along Lakeshore Road	City of Mississauga, Sharon Mittmann
	Existing examples of access along Lakeshore Road	City of Mississauga, Sharon Mittmann
Page 42	Parking, Loading and Service Areas at the Rear of the site	City of Mississauga, Sharon Mittmann
	Parking, Loading and Service Areas at the Rear of the site	City of Mississauga, Sharon Mittmann
Page 43	Example of On-street parking	City of Mississauga, Image Library
	Example of Lay-by Parking	City of Mississauga, Image Library
	Example of Lay-by Parking	City of Mississauga, Image Library
	Example of Lay-by Parking	City of Mississauga, Image Library
Page 44	Place Making Opportunities	City of Mississauga, Geomatics
	Place Making Opportunities, Lakeshore Road East and Cawthra Road	City of Mississauga, Sharon Mittmann
	Place Making Opportunities	City of Mississauga, Sharon Mittmann
Page 45	Former Rail line looking south from Lakeshore Road -Place Making Opportunities	City of Mississauga, Sharon Mittmann
	Place Making Opportunities	City of Mississauga, Sharon Mittmann
	Small Arms Inspection Building, Place Making Opportunities	City of Mississauga, Sharon Mittmann
	Place Making Opportunities	City of Mississauga, Sharon Mittmann
Back	Queenscorp. Development	City of Mississauga, Dan Magee

## 4.0 Appendix A

## Notes

This image shows a full page of blank handwriting practice paper. It features multiple sets of horizontal lines, each consisting of a solid top line, a dashed midline, and a solid bottom line, providing a guide for letter height and placement. The lines are evenly spaced across the entire page.





# **APPENDIX II**

## **Lakeshore Road Transportation Review Study – Executive Summary**

City of Mississauga

# LAKESHORE ROAD



## Transportation Review Study

FINAL REPORT

# EXECUTIVE SUMMARY

## **Study Purpose**

The overall goal of this study was to provide a comprehensive and technical transportation review to identify how the Lakeshore Road corridor can accommodate alternative modes of transportation and provide input and background into the preparation of Official Plan policies for Lakeview and Port Credit and address stakeholder comments provided in the “*Directions Report*”.

As the City looks to the future, long term plans for corridors such as Lakeshore Road will include providing the necessary facilities for all modes of transportation. As such, pedestrian facilities, cycling facilities and higher order transit facilities all need to be accommodated for a revitalized and more active Lakeshore Road.

The project conceptually reviewed the feasibility of future higher order transit within the existing Lakeshore Road right-of-way from Hurontario Street to the Long Branch TTC / Mississauga Transit terminal (the study area stops at east municipal boundary, just west of the Long Branch terminal at Etobicoke Creek).

## **History**

Lakeshore Road was formally part of the Provincial King’s Highway network as Highway 2 continuous through the GTA. It has also supported the ongoing development of Port Credit, Lakeview and Clarkson urban villages as well as industrial and residential areas along the corridor. Construction of the QEW subsequently relieved most of the through traffic from Lakeshore Road.

## **Current Role**

The roadway has an important role to play in supporting the variety of tourist, commercial, industrial and residential land uses along the corridor. There are no alternative parallel continuous east-west routes except for the access controlled QEW some 2 kms to the north.

Currently Lakeshore Road provides for autos, bus transit and pedestrians, but not explicitly for bicycles. An evaluation of alternative modes and how these modes can be accommodated within the changing Lakeshore Road ROW has been undertaken. The following conclusion and results were developed in the course of this evaluation and functional design exercise.

Currently, around 75% of the traffic crossing the Credit River in the AM peak originates from the southwest part of Mississauga. Approximately 50% of the trips crossing the Credit River on Lakeshore Road are destined to a work / school locations in eastern Mississauga. On occasions when the QEW is blocked, traffic overflows onto Lakeshore Road which exacerbates the already congested traffic entering Port Credit. Traffic flow improves east of Hurontario Street through Port Credit and Lakeview.



## **Traffic Trends**

All day traffic volumes have not risen in the last 10 years. Forecasts show that for the most part, peak direction volumes will not increase in the next 20 years, except for a small increase west of Port Credit as a result of increased GO Train ridership out of the Port Credit GO Station. Traffic volumes in the off-peak direction are forecast to increase resulting in more balanced flows along Lakeshore Road.

## **Future Transit**

On November 28, 2008, Metrolinx released their Draft Regional Transportation Plan entitled *“The Big Move: Transforming Transportation in the Greater Toronto and Hamilton area”*. This document sets out the actions to build new transportation infrastructure and improve transit service in the Greater Toronto and Hamilton area. One such initiative is the Waterfront West Rapid Transit project which will be a new east-west rapid transit connection from Union Station to the Port Credit GO Station. Although not currently financed to be programmed, this initiative is included in their 15-Year Plan. The technology to be utilized and its ultimate alignment were not determined however, it was assumed to be accommodated within the road right-of-way for Lakeshore Road.

## **LRT**

Higher order transit cannot be achieved west of Hurontario Street without compromising other modes and / or streetscaping. The long term plan includes LRT in mixed traffic (similar to Queen / King Streetcar service in Toronto) in the restricted parts of the corridor between Hurontario Street and Greaves Avenue. East of Greaves Avenue, LRT on its own right of way is indicated in the long term functional plans

## **Right-of-way Challenges**

Between Broadview Avenue and Seneca Avenue, a right of way (ROW) width of only 26.2 m is available, with no likelihood of increasing that width. An additional constraint exists between Cawthra Road and Greaves Avenue; however there will be opportunities to achieve the ultimate 30 m ROW in the future. Therefore, in the near term even with a potential narrowing of vehicular lane widths, it is not feasible to implement on-street bicycle lanes in this stretch while maintaining 4 lanes of traffic, on-street parking and the current boulevard / sidewalk widths (which are considered the minimum desirable already). Between Seneca Avenue and Cawthra Road and again between Greaves Avenue and the east municipal boundary, greater ROW widths are available (or could be available) and therefore do not provide the same challenge that the Port Credit and Cawthra Road to Greaves Avenue sections pose.

A series of cross-section alternatives were developed and assessed for use in the short term (based on bus transit in mixed traffic) and long term (with LRT east of Hurontario Street). Many of the more desirable options were not feasible due to inadequate ROW availability. One of the main ROW challenges was to accommodate cyclists more safely than the current situation without adversely impacting space requirements for vehicles, pedestrians and streetscaping.

### **Lane Widths**

In order to better accommodate bicycles and in some scenarios transit, lane widths on Lakeshore Road were reduced in the short and long term functional designs. Typically 3.35 m lanes were used (3.0 m for left turn lanes) except for the locations where sharrows are proposed in wider 4.0 m curb lanes.

### **Centre Two-Way-Left-Turn-Lane (CTWLTL)**

The existing CTWLTL in place between Seneca Avenue and the east municipal boundary provides a measurable and significant safety advantage compared with a scenario of having no left turn storage. In the future, should it be possible to reduce the frequency of access points along this stretch, it may be possible to implement a raised median with channelized left turns at the remaining access points to provide a further safety advantage over the CTWLTL. This would also have the advantage of providing better definition (delineation) for pedestrians.

In the long term plan, the CTWLTL would have to be removed in the section east of Greaves Avenue as it would be replaced by an LRT in an exclusive ROW along the centre of the road. In the long term plan west of Greaves Avenue, left turn pockets could potentially be made between the tracks, or more likely the left turns would take place on the tracks (delaying transit). More analysis would be needed to determine the appropriate solution at specific locations.

### **Bicycle Plan**

The Mississauga Cycling Master Plan was recently completed which has highlighted Lakeshore Road as a future on-street bicycle facility. Input from this study will help determine how cycling will be accommodated.

This study has determined that a wide curb lane with sharrows is the near term preferred application through the areas with a narrow right of way – from Broadview Avenue to Greaves Avenue.

The exception being a small section between Seneca Avenue and Cawthra Road that is able to accommodate on-street bicycle lanes; however, this will require a 2 metre widening of the road pavement which appears to be feasible given the 31 metre ROW available. Again east of Greaves Avenue to the municipal boundary, on-street bicycle lanes are recommended which would require a road widening on both sides.

The long term functional plan shows on-street bicycle lanes throughout the corridor, this is made possible by an assumption that on-street parking (on one side of the road) through Port Credit may ultimately be removed and may be replaced with off-street parking lots / garages.

Although not explicitly assessed in this study, bicycle lanes are also suggested for the section of Lakeshore Road between Clarkson and Port Credit (Johnson's Lane to Shawnmarr Road) as a result of higher vehicular speeds and the availability of a wider ROW.

### **Credit River Bridge**

The Credit River Bridge will need widening to accommodate a relocation of bicycles from the street to the current sidewalks. A number of structural alternatives to achieve this will need to be investigated further in a Class Environmental Assessment (EA) study.

### **On-Street Parking**

Current on-street paid parking in Port Credit provides an important support function to the commercial core and tourist area of Port Credit. Over the longer term, there may come a time when changes in modal usage may reduce the need for on-street parking. There is also a parking management plan underway for the Port Credit area that includes among other things an examination of the feasibility of replacing a portion of the on-street parking supply with off-street / side street parking.

In the meantime, as long as 4 traffic lanes are required and on-street parking is present, it is extremely difficult to provide either on-street bicycle lanes or higher order transit through Port Credit.

A strategic parking plan for the Lakeshore Road corridor through Lakeview is currently underway which will set the parking framework for this area.

Although the near term recommended plan for the corridor includes retention of the existing parking spaces, in order to provide more opportunity for other modes, at least one side of the on-street parking may need to be replaced by adequate and convenient off-street parking. This provides a longer term challenge and is reflected in the long term plan which includes bicycle lanes with parking on the south side only. This results in the removal of 88 on-street parking spaces on the north side.

### **Recommended Plans**

Two design plans were developed for the corridor. One is considered to be a long term plan which includes the LRT and also removal of on-street parking from one-side of the road, while a near term option was developed which did not consider LRT but maintained the on-street parking.

### **Near Term Plan**

The basic cross-section alternative selected for the near term option through Port Credit and another narrow ROW section east of Cawthra Road is shown below in **Exhibit E1**.

The cross-section includes the addition of sharrows in a 4 m curb lane and involves a slight modification of the existing cross-section (mainly a reduction in vehicular lane and parking widths) to accommodate a wide curb lane for sharrows to better accommodate cyclists. For areas with a wider ROW, between Greaves Avenue and Cawthra Road and again east of Greaves Avenue to the municipal boundary, on-street bicycle lanes are included instead of sharrows. Minor curb adjustments are required to implement the cycling facilities.



## CONCEPT FOR: BUSES IN MIXED TRAFFIC + SHARROW LANE + ON-STREET PARKING

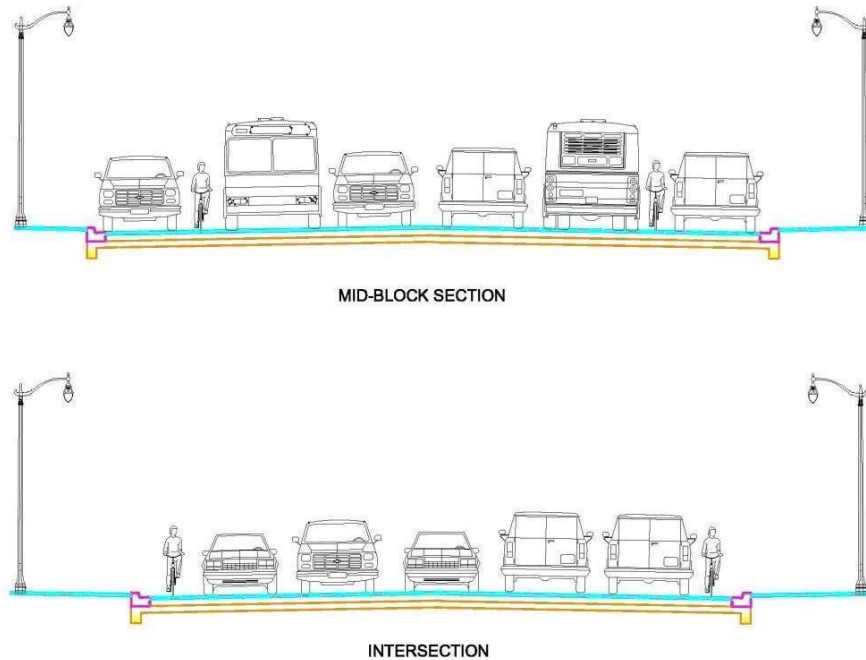


EXHIBIT E1: NEAR TERM CONCEPT THROUGH RESTRICTED ROW

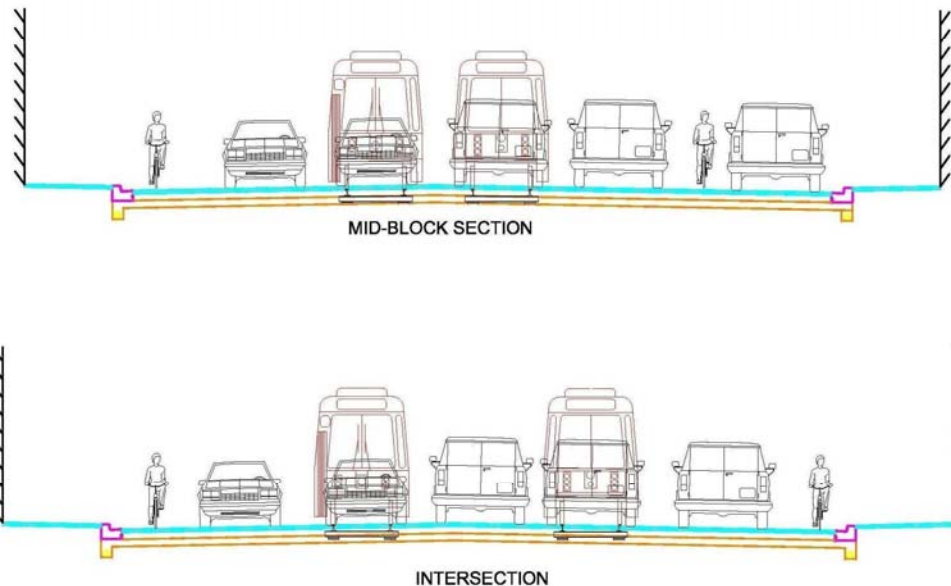
**Note: Subject to further review including Community consultation and preliminary design.**

The functional design for this near term option is shown in the upper half of Plates 1 through 20 at the back of this report.

### Long Term Option

Between Hurontario Street and Greaves Avenue, due to the constricted ROW sections, the basic cross-section alternative selected for the long term option is LRT in mixed traffic (e.g. similar to Queen St / King St. in Toronto). East of Greaves Avenue to the east municipal boundary, LRT on its own ROW is feasible. A short section between Seneca Avenue and Cawthra Road could also have accommodated LRT, but the section is too short to accommodate a transition to and from LRT / mixed traffic. Bicycle lanes are shown throughout the corridor. This is made possible through the 26.2 m ROW sections by removing parking on the north side (choice of the north side was arbitrary). The recommended typical cross-section for the long term between Hurontario Street and Greaves Avenue is shown below in **Exhibit E2**.

## CONCEPT FOR: LRT IN MIXED TRAFFIC + ON-STREET BICYCLE LANES + ONE SIDE ON STREET PARKING



### EXHIBIT E2: LONG TERM CONCEPT THROUGH RESTRICTED ROW (EAST OF HURONTARIO STREET)

**Note: Subject to further review including Community consultation and preliminary design.**

The functional design for the long term option is shown in the lower half of Plates 1 through 20 at the back of this report.

#### Implementation

##### Near Term

To implement near term changes, including curb adjustments required for cycling facilities, it is anticipated this would occur in phases as part of the Transportation and Works Department road rehabilitation / reconstruction program. These minor curb adjustments are required to accommodate the on-road bicycle facilities (sharrows / bike lanes) based on the assumption that traffic lanes will be maintained at a minimum width of 3.25 m to 3.35 m in width.

Notwithstanding the above, there may be an opportunity to implement cycling on Lakeshore Road without curb adjustments. This would be accomplished by reducing the inside through lane to a width of 3.05 m to 3.10 m and widening the curb lane to 4.0 m with the addition of bicycle sharrows markings.

This appears to be feasible based on existing road plans but would need to be confirmed in the field. The reduced width would be subject to safety considerations, including but not limited to speed and access / intersection proximity.

On the assumption that sufficient pavement width is available, these changes would be implemented through the road resurfacing program or independently when appropriate through the removal and re-application of the pavement markings.

### **Long Term**

The long term plan proposes an LRT system operating in mixed traffic through Port Credit (west of Greaves Avenue) and in a dedicated ROW east of Greaves Avenue. In this light, an adjustment to the ROW from the existing 35 m to 44.5 m (44.5 m is currently available through this section of Lakeshore Road, with the exception of property fronting the OPG and Metropolitan Toronto and Region Conservation Authority lands) will be required. There is no time frame at this point for the study required for the consideration of an LRT system. This information is important with regard to the implementation of future curb adjustments relative to the ROW needs for on-road bicycle lanes in the absence of an LRT system.

The long term plan also assumes implementation of on-road bicycle lanes throughout the corridor which will impact on-street parking through Port Credit (Broadview Avenue to Seneca Avenue). The study concludes that parking would need to be eliminated from one side of the road and relocated to side streets and / or new surface parking lots. On-street parking will be an issue even if an LRT system is not implemented, since the LRT is proposed to operate in mixed traffic through Port Credit. It is the addition of the on-road bicycle lanes that will precipitate the need for changes. Additional parking information is anticipated in 2011 upon completion of a parking management plan currently underway in Port Credit.

The study speaks to the ultimate removal of the centre turn lane from Seneca Avenue to the east City boundary and its replacement with a dedicated LRT ROW. This will require a detailed review of the feasibility of access consolidation throughout this section of Lakeshore Road.



# OTHER INFORMATION

## Credits

## OTHER INFORMATION: Credits

Figure #	Description	Source
1	Cawthra-Elliot Estate (left photo) and Lakeview Park School (right photo)	City of Mississauga (Adishesan Shankar, ASAP Photography)
2	Lakeview Area Statistics	City of Mississauga, Planning and Building Department, Policy Planning, Information Planning
3	Lakeview Place-making Workshop, Lakeview Golf Course	City of Mississauga (BMI Pace Architects)
4	(a) Lakefront Promenade (Reconnect Lakeview); (b) Detached dwelling in Lakeview (Create Distinct Neighbourhoods); (c) Downtown Oakville (Support Complete Communities); (d) Waterfront Trail in Lakeview (Promote Community Health); (e) Lakefront Promenade (Support Social Well-Being); and (f) Cooksville Creek (Achieve Leadership in Sustainability).	(a), (d), and (e) City of Mississauga (Adishesan Shankar, ASAP Photography)  (b) and (f) Karin Phuong (City of Mississauga)  (c) Irena Rostkowska (City of Mississauga)
5	Detached dwelling in Lakeview	Karin Phuong (City of Mississauga)
6	Lakeshore Road East in Lakeview	City of Mississauga (Adishesan Shankar, ASAP Photography)
7	Landscape area at 1535 Lakeshore Road East	Sharon Mittmann (City of Mississauga)
8	Mississauga Senior Citizens' Centre (left photo) and Greenway Lodge Retirement Home (right photo)	Left photo, City of Mississauga (Adishesan Shankar, ASAP Photography)  Right photo, Karin Phuong (City of Mississauga)
9	Adamson Estate	Karin Phuong (City of Mississauga)
10	Waterfront Trail in Lakeview	Karin Phuong (City of Mississauga)
11	Lakeshore Road East	City of Mississauga (Adishesan Shankar, ASAP Photography)
12	Small Arms Inspection Building at 1352 Lakeshore Road East	Karin Phuong (City of Mississauga)
13	(a) 885 Lakeshore Road East (top left photo) (b) Townhouses on Northmount Avenue (top right photo) (c) Apartment on Greaves Avenue (d) Plex in Lakeview (Bottom centre photo) (e) Caven Street (Bottom right photo)	(a) and (d) Karin Phuong (City of Mississauga)  (b) and (c) City of Mississauga (Adishesan Shankar, ASAP Photography)  (e) Sharon Mittmann (City of Mississauga)
14	Businesses in Lakeview's Employment Area – on Rangeview Road (left photo) and on Lakeshore Road East (right photo)	Karin Phuong (City of Mississauga)



# Corporate Report

Clerk's Files

Originator's  
Files BL.09-COM

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**DATE:** January 14, 2014

**TO:** Chair and Members of Planning and Development Committee  
Meeting Date: February 3, 2014

**FROM:** Edward R. Sajecki  
Commissioner of Planning and Building

**SUBJECT:** **Information Report**  
**Proposed Housekeeping Amendment –**  
**Mississauga Zoning By-law 0225-2007**  
**City of Mississauga**  
**Bill 51**

**Public Meeting** **Wards 1 - 11**

---

**RECOMMENDATION:** That the Report dated January 14, 2014, from the Commissioner of Planning and Building regarding a proposed Housekeeping Amendment to Zoning By-law 0225-2007, be received for information and notwithstanding planning protocol, that the Supplementary Report be brought directly to a future Council meeting.

**BACKGROUND:** Mississauga Zoning By-law 0225-2007 was passed by Council on June 20, 2007. To ensure the Zoning By-law remains up-to-date, regular Housekeeping Amendments are prepared for City-initiated amendments to the Zoning By-law. In some instances, an Amendment may be required to address issues with interpretation of the By-law that arise in between the preparation of the larger Housekeeping Amendments.



**COMMENTS:**

Since the latest Housekeeping Amendment was approved in December 2013, it was noted that the current definition of "Overnight Accommodation" does not permit ensuite kitchens as part of an overnight accommodation use. Private kitchens located in hotel suites are widely permitted in a number of other municipalities, including Ottawa and Toronto. Further, All-suite hotels are a growing occurrence in the hotel industry.

The revised definition allows suites in hotels in Mississauga, but ensures that a differentiation is made between an all-suite hotel and an apartment dwelling unit, as both have similar definitions and characteristics, but are intended for very different uses. The revised definition is noted below, and the draft By-law is attached to this Report as Appendix I-1.

**Overnight Accommodation** - means a **building, structure** or part thereof, used for the purpose of providing temporary accommodation, that contains at least twenty (20) bedrooms or suites, or any combination thereof, for transient guests, and may contain **accessory uses**, such as a general kitchen and dining area, meeting rooms, convention and banquet facilities, **personal service establishments** and recreational facilities.

**FINANCIAL IMPACT:**

Not applicable.

**CONCLUSION:**

Once the public meeting has been held, the Planning and Building Department will be in a position to make a recommendation regarding this amendment. Given the nature of the proposed City-initiated amendment to the Zoning By-law, it is recommended that notwithstanding planning protocol, the Supplementary Report be brought directly to a future Council meeting.

**ATTACHMENTS:**

Appendix I-1: Draft Zoning By-law to amend Zoning By-law  
0225-2007



Edward R. Sajecki

Commissioner of Planning and Building

*Prepared By: Lisa Christie, Planning Services Centre Planner*



A by-law to amend By-law Number 0225-2007, as amended.

WHEREAS pursuant to section 34 of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, the council of a local municipality may pass a zoning by-law;

NOW THEREFORE the Council of The Corporation of the City of Mississauga ENACTS as follows:

1. By-law Number 0225-2007, as amended, being a City of Mississauga Zoning By-law, is amended by amending the definition of "**Overnight Accommodation**" in Section 1.2 as follows:

<b>Overnight Accommodation</b>	means a building, structure or part thereof, used for the purpose of <del>supplying food and/or</del> providing temporary sleeping accommodation, that contains at least twenty (20) bedrooms or suites, or any combination thereof, for transient guests, and may contain, <del>and</del> accessory uses, such as <del>but not limited to,</del> a general kitchen and dining area, meeting rooms, convention and banquet facilities, <del>personal service establishments</del> and recreational facilities.
--------------------------------	--

ENACTED and PASSED this \_\_\_\_\_ day of \_\_\_\_\_ 2014.

\_\_\_\_\_  
MAYOR

\_\_\_\_\_  
CLERK

**APPENDIX "A" TO BY-LAW NUMBER \_\_\_\_\_**

Explanation of the Purpose and Effect of the By-law

This By-law amends Mississauga Zoning By-law 0225-2007 by modifying and expanding the definition of "Overnight Accommodation".

Location of Lands Affected

All lands in the City of Mississauga.

Further information regarding this By-law may be obtained from Lisa Christie of the City Planning and Building Department at 905-615-3200 ext. 5542.

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DRAFT





# Corporate Report

Clerk's Files

Originator's

Files OZ 13/014 W10

T-M13004 W10

**DATE:** January 14, 2014

**TO:** Chair and Members of Planning and Development Committee  
Meeting Date: February 3, 2014

**FROM:** Edward R. Sajecki  
Commissioner of Planning and Building

**SUBJECT:** **Information Report**  
**Rezoning and Draft Plan of Subdivision Applications**  
**To permit 15 detached dwellings**  
**6565 Ninth Line**  
**East side of Ninth Line, south of Derry Road West**  
**Owner: Argo Trail Corporation**  
**Applicant: Weston Consulting**  
**Bill 51**

**Public Meeting** **Ward 10**

**RECOMMENDATION:** That the Report dated January 14, 2014, from the Commissioner of Planning and Building regarding the application to change the Zoning from "R1" (Detached Dwellings - Typical Lots) to "R4-Exception" (Detached Dwellings - Typical Lots) under file OZ 13/014 W10 and a Draft Plan of Subdivision to permit 15 detached dwellings under file T-M13004 W10, Argo Trail Corporation, 6565 Ninth Line, be received for information.

**REPORT  
HIGHLIGHTS:**

- The applications are to allow for the development of 15 detached dwellings and the extension of Berryman Trail as a public road.
- Community concerns identified to date relate to insufficient parking, potential flooding and mitigation measures, vehicular

access to Ninth Line and construction access.

- Prior to the Supplementary Report, matters to be addressed include the appropriateness of the proposed Zoning By-law amendment and Draft Plan of Subdivision and satisfactory resolution regarding buffer blocks, noise attenuation, drainage, stormwater management, grading, retaining walls, walkway connections and other design details.

**BACKGROUND:**

The above-noted applications have been circulated for technical comments and a community meeting has been held.

The purpose of this report is to provide preliminary information on the applications and to seek comments from the community.

**COMMENTS:**

Details of the proposal are as follows:

<b>Development Proposal</b>	
Applications submitted:	Received: September 25, 2013 Deemed complete: October 25, 2013
Number of units:	15 detached dwellings
Net Density:	21.4 units/ha 8.8 units/acre
Maximum Height:	2 storeys/10.7 m (35.1 ft.)
Anticipated Population:	51* *Average household sizes for all units (by type) for the year 2011 (city average) based on the 2008 Growth Forecasts for the City of Mississauga.
Supporting Documents:	Planning Justification Report Draft Plan of Subdivision Aerial/Context Plan Survey Servicing Plans Parking Plan Phase 1 Environmental Site Assessment Functional Servicing Report

<b>Development Proposal</b>	
	Noise Control Feasibility Study Stage 1-2 Archeological Assessment Tree Inventory, Preservation Report and Plan

<b>Site Characteristics</b>	
Frontage:	121.9 m (399.9 ft.) along Ninth Line
Depth:	61.3 m (201.1 ft.)
Net Lot Area:	0.57 ha (1.41 ac.)
Existing Use:	Vacant

### **Abutting Lands**

Blocks 370, 371 and 372 on Registered Plan 43M-1052, located on the abutting lands to the east, form the temporary turning circle at the end of Berryman Trail (see Appendix I-5). Blocks 373, 374 375, on Registered Plan 43M-1052 are remnant residential dwelling blocks on the abutting lands that are currently being held by the City in escrow until Berryman Trail is permanently constructed. The completion of Berryman Trail will allow the development of Blocks 370 to 375, inclusive, for 3 detached dwellings. The applicant is seeking to purchase these lands from the owner of the abutting lands in order to build the dwellings at the same time as the proposed plan of subdivision that is subject of this application.

Additional information is provided in Appendices I-1 to I-11.

### **Neighbourhood Context**

The subject property is located in the Lisgar Neighbourhood Character Area and is surrounded by an existing low density residential neighbourhood and community uses. The subject site is vacant and relatively flat. Information regarding the history of the site is found in Appendix I-1.

The surrounding land uses are described as follows:

North: Detached dwellings

East: Detached dwellings, Lisgar Middle School, Lisgar Fields  
(City Park P-359)

South: Montessori school and semi-detached dwellings

West: Across Ninth Line, vacant lands and a Union Gas facility

### **Official Plan**

Mississauga Official Plan (2012) was adopted by City Council on September 29, 2010 and partially approved by the Region of Peel on September 29, 2011. The Plan was appealed in its entirety; however, on November 14, 2012 the Ontario Municipal Board issued a Notice of Decision approving Mississauga Official Plan, as modified, save and except for certain appeals which have no effect on the subject applications.

### **Current Mississauga Official Plan Designation and Policies for the Lisgar Neighbourhood Character Area (November 14, 2012)**

**"Residential Low Density II"** which permits detached, semi-detached and duplex dwellings, triplexes, street townhouses and other forms of low-rise dwellings with individual frontages.

The applications are in conformity with the land use designation and no official plan amendment is proposed.

### **Existing Zoning**

**"R1" (Residential – Detached Dwellings – Typical Lots)**, which permits detached dwellings on lots with frontages of 22.5 m (73.8 ft.) and minimum lot areas of 750 m<sup>2</sup> (8,072.9 sq. ft.).

### **Proposed Zoning By-law Amendment**

**"R4 - Exception" (Residential – Detached Dwellings – Typical Lots)**, to permit detached dwellings in accordance with the proposed zone standards contained within Appendix I-10.



Buffer blocks may be required immediately adjacent to Ninth Line for separation, noise attenuation and landscaping purposes. The "B" (Buffer) zone is the most appropriate zoning category. A determination for their inclusion will be made prior to the preparation of the Supplementary Report.

Further, the proposal will require relief from Council approved resolution CPD 121-91, which requires 3 on-site parking spaces per unit for lot frontages less than 12.0 m (39.4 ft.).

### **COMMUNITY ISSUES**

A community meeting was held by Ward 10 Councillor, Sue McFadden on October 2, 2013. Residents expressed concerns regarding potential flooding and mitigation measures, vehicular access to Ninth Line and construction access. In addition, one letter was received from an area resident expressing concerns with respect to insufficient parking and the proposed minimum lot frontage and area zoning standards.

Issues raised by the Community will be addressed in the Supplementary Report.

### **DEVELOPMENT ISSUES**

Agency comments are summarized in Appendix I-8 and school accommodation information is contained in Appendix I-9. Based on the comments received and the applicable Mississauga Official Plan policies, the following matters will have to be addressed:

- Satisfactory arrangements regarding buffer blocks, noise attenuation, drainage, stormwater management, grading, retaining walls, walkway connections and other design details;
- Arrangements for the construction and dedication of the extension of Berryman Trail as a local residential public road by way of a servicing agreement; and
- Arrangements for the dedication of a road widening and 0.3 m (1 ft.) reserve along Ninth Line.

**OTHER INFORMATION****Development Requirements**

In conjunction with the proposed development, there are certain other engineering matters with respect to servicing, grading, road construction and stormwater which will require the applicant to enter into appropriate agreements with the City, the details of which will be dealt with during the processing of the plan of subdivision.

**FINANCIAL IMPACT:** Development charges will be payable in keeping with the requirements of the applicable Development Charges By-law of the City as well as financial requirements of any other official agency concerned with the development of the lands.

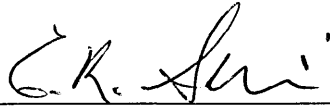
**CONCLUSION:** Most agency and City department comments have been received and after the public meeting has been held and all issues are resolved, the Planning and Building Department will be in a position to make a recommendation regarding these applications.

**ATTACHMENTS:**

- Appendix I-1: Site History
- Appendix I-2: Aerial Photograph
- Appendix I-3: Excerpt of Lisgar Neighbourhood Character Area Land Use Map
- Appendix I-4: Excerpt of Existing Land Use Map
- Appendix I-5: Draft Plan of Subdivision
- Appendix I-6: Concept Plan Rendering
- Appendix I-7: Elevations
- Appendix I-8: Agency Comments
- Appendix I-9: School Accommodation

Appendix I-10: Proposed Zoning Standards

Appendix I-11: General Context Map



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Edward R. Sajecki

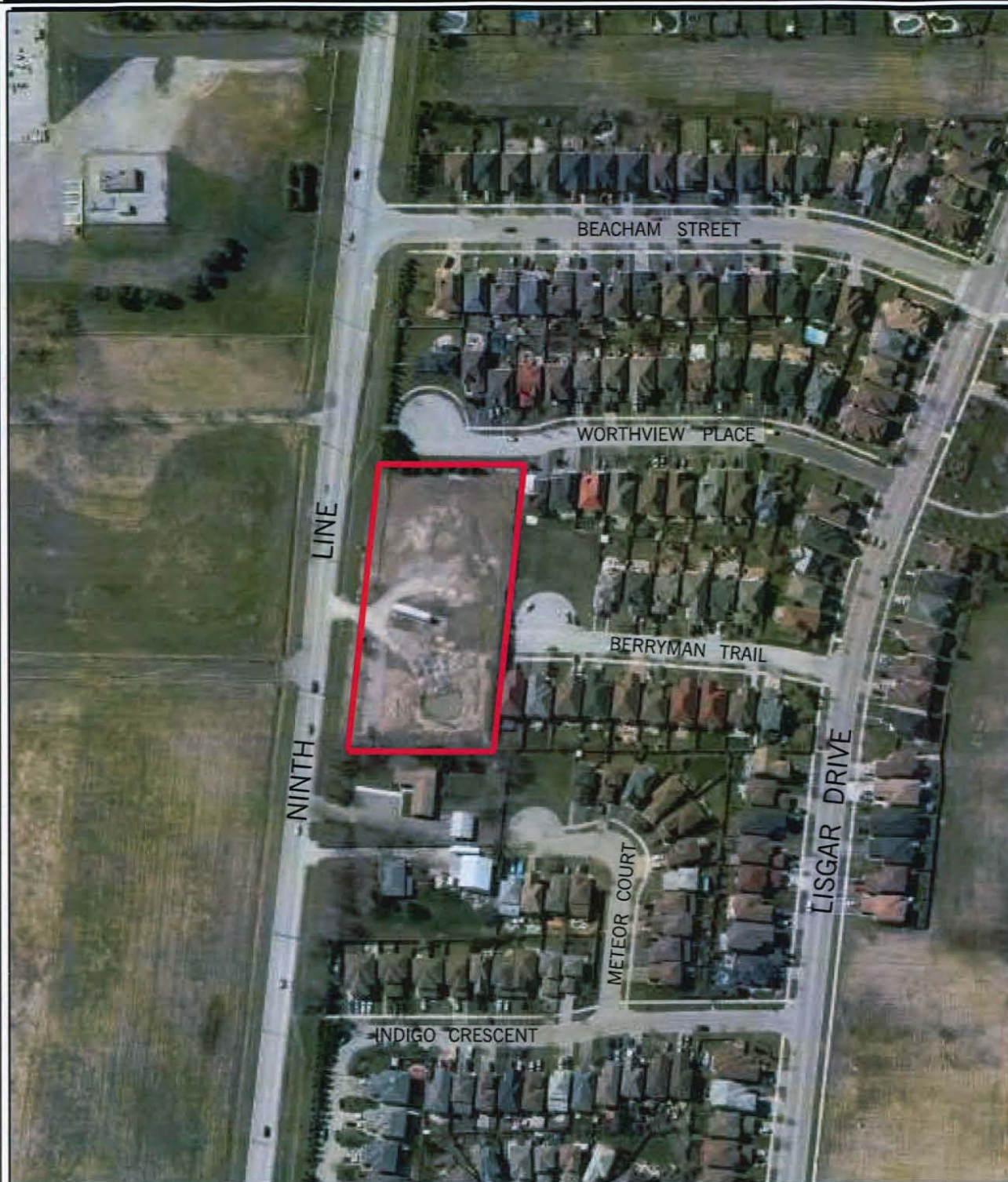
Commissioner of Planning and Building

*Prepared By: Antonia Krijan, Development Planner*

**Argo Trail Corporation****Files: OZ 13/014 W10  
T-M13004 W10****Site History**

- July 13, 2005 – Harvest Fellowship Baptist Church applied for site plan approval under file SP 05/218 W10 to permit a place of religious assembly in the existing detached dwelling. The site plan application was cancelled on April 3, 2006.
- June 20, 2007 – Zoning By-law 0225-2007 came into force except for those sites which have been appealed. The subject lands are zoned "R1" (Detached Dwelling – Typical Lots).
- April 7, 2008 – St. Jude's Academy Private School applied for a building permit which was cancelled on April 3, 2012.
- August 8, 2011 – Mississauga International Baptist Church applied for site plan approval under file SP 11/134 W10 to permit the construction of a new 906.6 m<sup>2</sup> (9,758.6 sq. ft.) place of religious assembly. The application was cancelled on September 13, 2013.
- March 28, 2012 – A demolition permit was issued for the existing detached dwelling.
- November 14, 2012 – The Ontario Municipal Board approved Mississauga Official Plan except for those policies under appeal. The subject lands are designated "Residential Low Density II" in the Lisgar Neighbourhood Character Area.





# LEGEND:



SUBJECT LANDS

NOTE: DATE OF AERIAL PHOTO: 4 2013



## SUBJECT:

ARGO TRAIL CORPORATION



FILE NO:  
OZ 13014 W10  
T-M13004 W10

DWG. NO:  
13014A

SCALE:  
1:2500

PDC DATE:  
2014 02 03

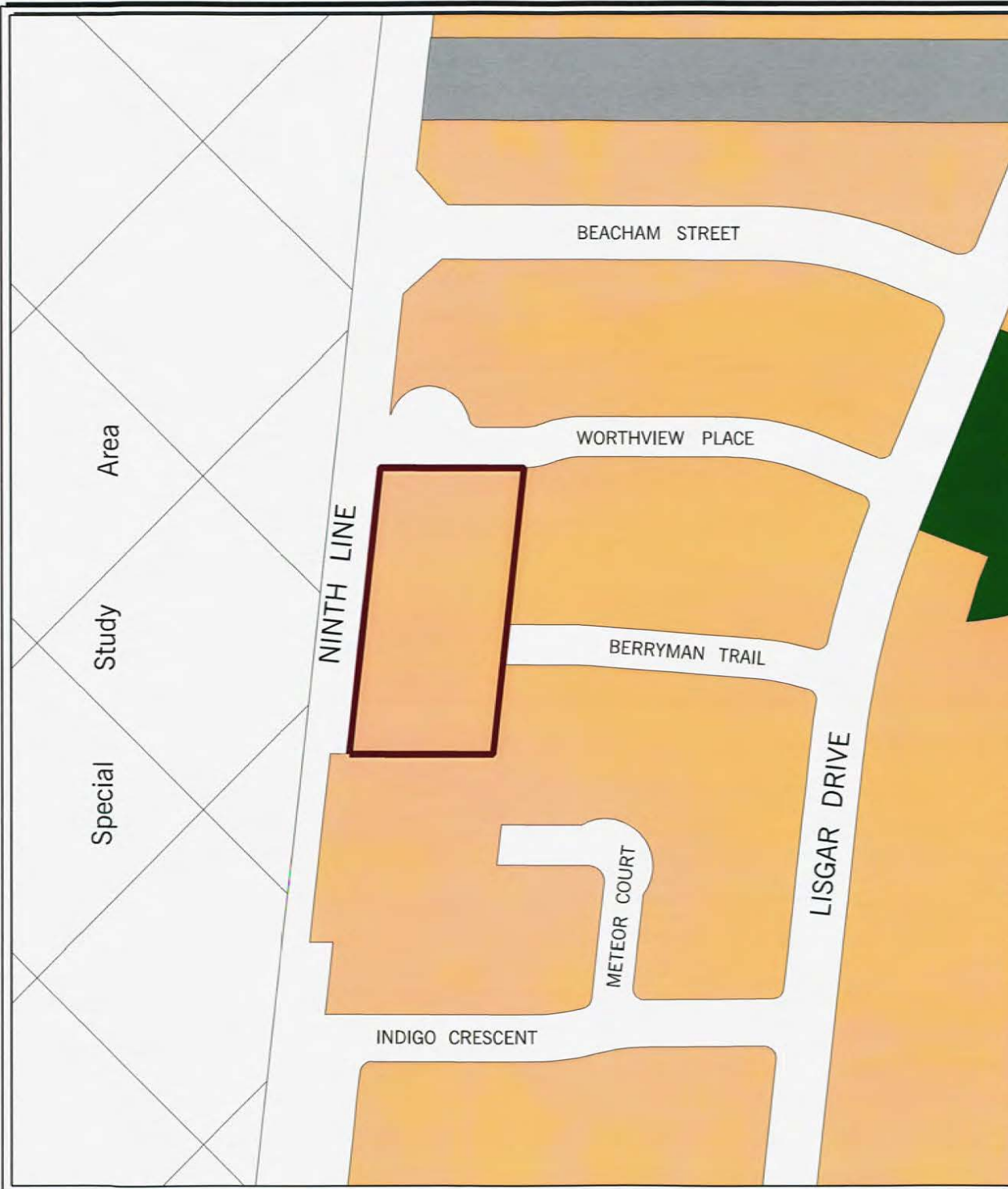
DRAWN BY:  
B. KRUGER

APPENDIX 1-2

**MISSISSAUGA**  
Planning and Building

Produced by  
T&W, Geomatics





# PART OF SCHEDULE 10 LAND USE DESIGNATIONS OF MISSISSAUGA OFFICIAL PLAN

## LAND USE DESIGNATIONS

Residential Low Density I	Business Employment
Residential Low Density II	Industrial
Residential Medium Density	Airport
Residential High Density	Institutional
Downtown Mixed Use	Public Open Space
Downtown Core Commercial	Private Open Space
Mixed Use	Greenbelt
Convenience Commercial	Parkway Belt West
Motor Vehicle Commercial	Utility
Office	To Be Determined

## LAND USE LEGEND

Heritage Conservation District	Civic Centre (City Hall)
1986 NEP/2000 NEF Composite Noise Contours	City Centre Transit Terminal
LBPIA Operating Area Boundary See Aircraft Noise Policies	GO Rail Transit Station
Area Exempt from LBPIA Operating Area	Public School
Natural Hazards	Catholic School
	Hospital
	Community Facilities

## CITY STRUCTURE

### Elements

Downtown	Corporate Centre
Major Node	Employment Area
Community Node	Special Purpose Area
Neighbourhood	

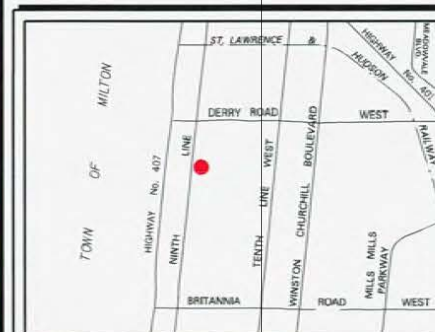
ND2 Region of Peel Non-Decision

**SUBJECT LANDS**



**SUBJECT:**

**ARGO TRAIL CORPORATION**



**FILE NO:**  
OZ 13014 W10  
T-M13004 W10

**DWG. NO:**  
13014L

**SCALE:**  
1:2500

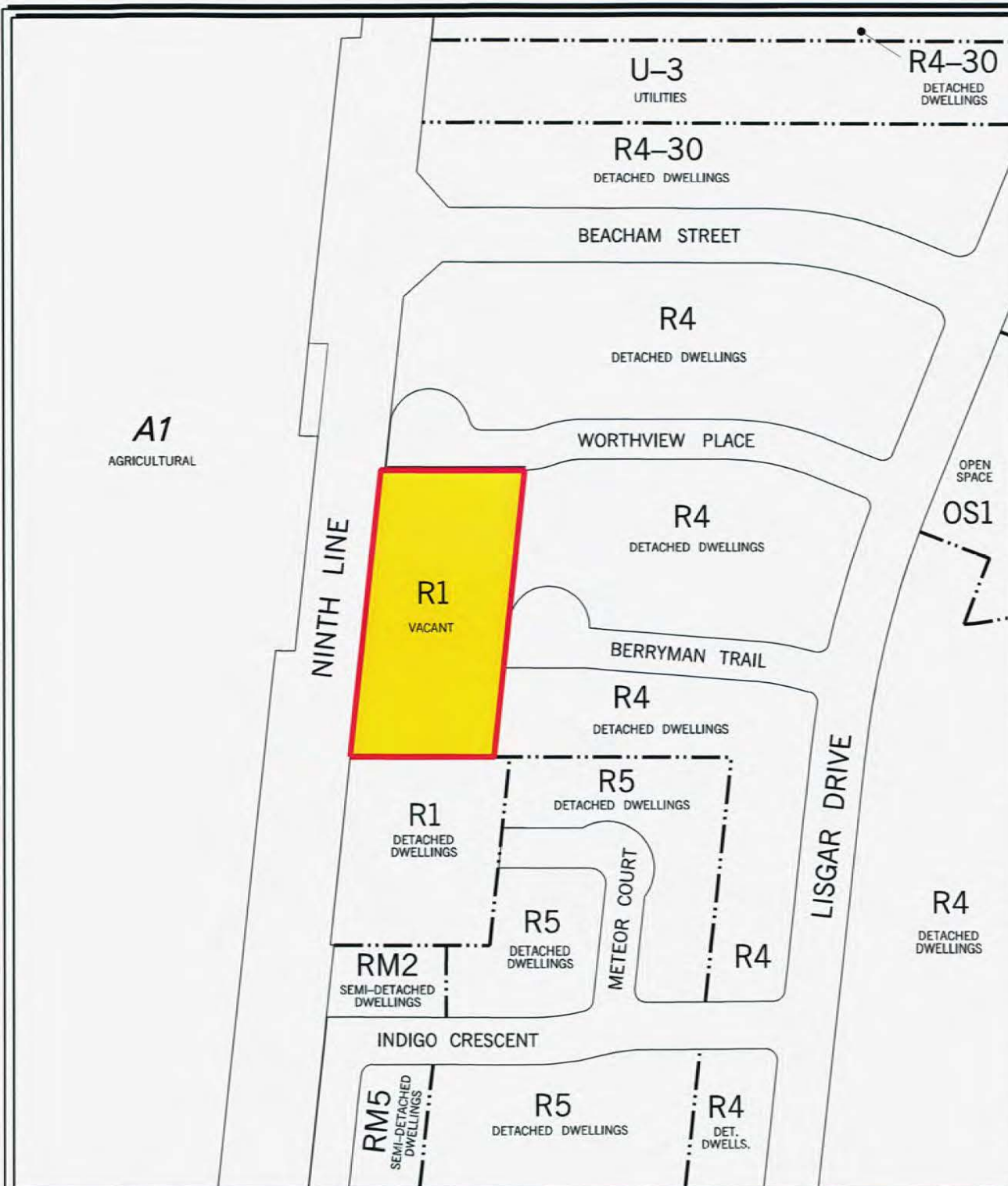
**PDC DATE:**  
2013 02 03

**DRAWN BY:**  
B. KRUGER

**APPENDIX 1-3**

**MISSISSAUGA**  
Planning and Building

Produced by  
**T&W, Geomatics**



**LEGEND:**



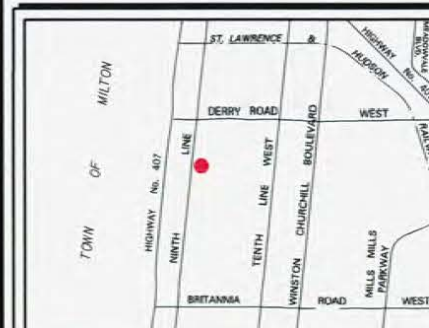
PROPOSED REZONING FROM 'R1' (DETACHED DWELLINGS - TYPICAL LOT) TO 'R4 - EXCEPTION' (DETACHED DWELLINGS - TYPICAL LOT) TO PERMIT 15 DETACHED DWELLING UNITS.

NOTE: EXISTING ZONING DELINEATED ON THE PLAN  
PROPOSED ZONING INDICATED BY SHADING WITHIN  
THE APPLICATION AREA.  
THIS IS NOT A PLAN OF SURVEY.



**SUBJECT:**

**ARGO TRAIL CORPORATION**



FILE NO:  
OZ 13014 W10  
T-M13004 W10

DWG. NO:  
13014R

SCALE:  
1:2500

PDC DATE:  
2013 02 03

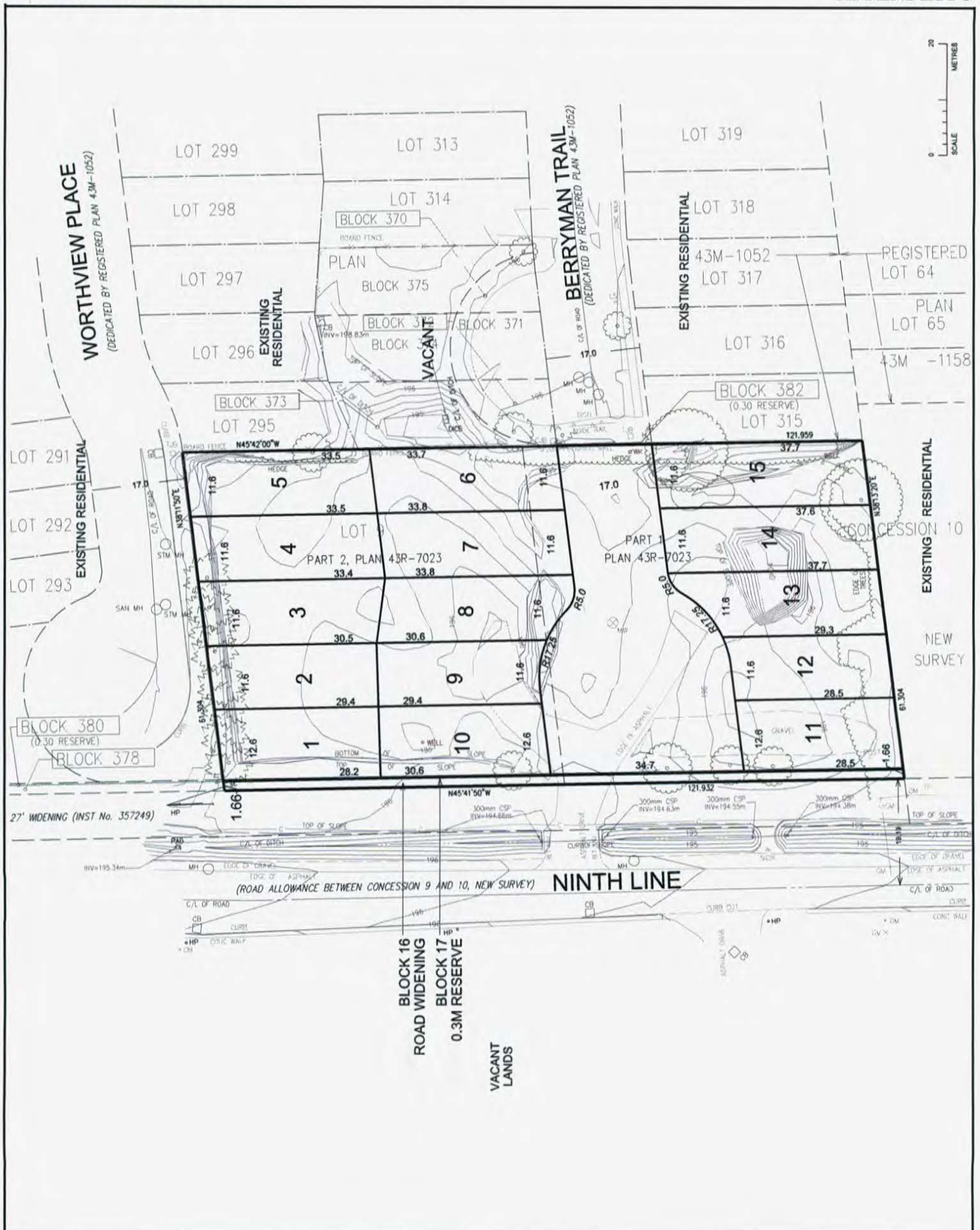
DRAWN BY:  
B. KRUGER

APPENDIX 1-4

**MISSISSAUGA**  
Planning and Building

Produced by  
T&W, Geomatics









Artists Rendering  
Aerial Photo:  
Google Earth circle 2008

6565 Ninth Line

Rezoning and Subdivision Application

Argo Trail Corporation



# PROPOSED SUBDIVISION





## ELEVATIONS

## APPENDIX I-7



11 M (36 FT.) SINGLE DETACHED LOT ELEVATION



11.6 M (38 FT.) SINGLE DETACHED LOT ELEVATION



12.5 M (41 FT.) SINGLE DETACHED LOT ELEVATION

Argo Trail Corporation

Files: OZ 13/014 W10  
T-M13004 W10**Agency Comments**

The following is a summary of comments from agencies and departments regarding the applications.

Agency / Comment Date	Comment
Region of Peel (November 20, 2013)	The Developer will be required to enter into a Subdivision Agreement with the local Municipality and the Region for the construction of municipal sewer and water associated with the lands. These services will be in accordance with the latest Region standards and requirements. Prior to the issuance of building permits for all lots and blocks, satisfactory arrangements must be made with the Regional Municipality of Peel with regard to water service applications and payments of the required connection charges. The Region will not accept payment for building permits until fire protection for the development is available and all securities for the development are in place.
Dufferin-Peel Catholic District School Board (November 7, 2013)  Peel District School Board (November 20, 2013)	Both School Boards responded that they are satisfied with the provision of educational facilities for the catchment area and, as such, the school accommodation condition as required by City of Mississauga Council Resolution 152-98 pertaining to satisfactory arrangements regarding the adequate provision and distribution of educational facilities need not be applied for these development applications.  In addition, if approved, both School Boards also require that warning clauses regarding temporary school accommodation and transportation arrangements be included in any Agreement of Purchase and Sale as well as the Development and/or Servicing Agreements.

Argo Trail Corporation

Files: OZ 13/014 W10  
T-M13004 W10

Agency / Comment Date	Comment
<p>City Community Services Department – Parks and Forestry Division/Park Planning Section (December 9, 2013)</p>	<p>Lisgar Fields (P-359), zoned OS1, is located 235 m (771 ft.) from the site and contains a play site, ball diamond, basketball hoops, soccer fields and tennis courts.</p> <p>Prior to registration, street tree contributions for Berryman Trail, Ninth Line and Worthview Place will be required, in addition to buffer planting along Ninth Line.</p> <p>Further, prior to the issuance of building permits, for each lot or block, cash-in-lieu for park or other public recreational purposes is required pursuant to Section 42(6) of the <i>Planning Act</i> (R.S.O. 1990, c.P. 13, as amended) and in accordance with City Policies and By-laws.</p>
<p>City Transportation and Works Department (December 18, 2013)</p>	<p>This Department confirmed receipt of a Functional Servicing Report, Phase 1 Environmental Site Assessment, and Noise Control Feasibility Study, which are currently under review. Notwithstanding the findings of these reports, additional technical details have been requested confirming the compatibility with the adjacent lands and revisions to the proposed cross-sectional detail incorporating the required noise buffer block to accommodate the acoustic berm/fence barrier adjacent to Ninth Line.</p> <p>Further detailed comments/conditions will be provided prior to the Supplementary Meeting pending receipt and review of the foregoing.</p>
<p>Other City Departments and External Agencies</p>	<p>The following City Departments and external agencies offered no objection to these applications provided that all technical matters are addressed in a satisfactory manner:</p> <ul style="list-style-type: none"> <li>- Heritage Planning, Culture Division, Community Services Department</li> <li>- Mississauga Transit</li> <li>- Rogers Cable</li> <li>- Canada Post</li> </ul>



**Argo Trail Corporation****Files: OZ 13/014 W10  
T-M13004 W10**

Agency / Comment Date	Comment
	<ul style="list-style-type: none"><li>- Enersource</li><li>- Bell Canada</li><li>- Halton Region, Planning Services</li><li>- Town of Milton</li></ul>
	<p>The following City Departments and external agencies were circulated the applications but provided no comments:</p> <ul style="list-style-type: none"><li>- City Community Services Department – Fire and Emergency Services Division</li><li>- Conseil Scolaire de District Catholique Centre-Sud</li><li>- Conseil Scolaire de District Centre-Sud-Ouest</li><li>- Ministry of Transportation</li><li>- Union Gas Limited</li></ul>

Argo Trail Corporation

Files: OZ 13/014 W10  
T-M13004 W10**School Accommodation**

The Peel District School Board	The Dufferin-Peel Catholic District School Board
<ul style="list-style-type: none"> <li>Student Yield: <ul style="list-style-type: none"> <li>3 Kindergarten to Grade 5</li> <li>2 Grade 6 to Grade 8</li> <li>2 Grade 9 to Grade 12/OAC</li> </ul> </li> <li>School Accommodation: <ul style="list-style-type: none"> <li>Trelawny Elementary Public School <ul style="list-style-type: none"> <li>Enrolment: 353</li> <li>Capacity*: 389</li> <li>Portables: 0</li> </ul> </li> <li>Lisgar Middle School <ul style="list-style-type: none"> <li>Enrolment: 672</li> <li>Capacity*: 688</li> <li>Portables: 16</li> </ul> </li> <li>Meadowvale Secondary School <ul style="list-style-type: none"> <li>Enrolment: 1,319</li> <li>Capacity*: 1,497</li> <li>Portables: 0</li> </ul> </li> </ul> </li> </ul> <p>* Note: Capacity reflects the Ministry of Education rated capacity, not the Board rated capacity, resulting in the requirement of portables.</p>	<ul style="list-style-type: none"> <li>Student Yield: <ul style="list-style-type: none"> <li>2 Junior Kindergarten to Grade 8</li> <li>1 Grade 9 to Grade 12/OAC</li> </ul> </li> <li>School Accommodation: <ul style="list-style-type: none"> <li>St. Simon Stock Catholic Elementary School <ul style="list-style-type: none"> <li>Enrolment: 414</li> <li>Capacity: 584</li> <li>Portables: 0</li> </ul> </li> <li>Our Lady of Mount Carmel Catholic Secondary School <ul style="list-style-type: none"> <li>Enrolment: 1753</li> <li>Capacity: 1320</li> <li>Portables: 16</li> </ul> </li> </ul> </li> </ul>

Argo Trail Corporation

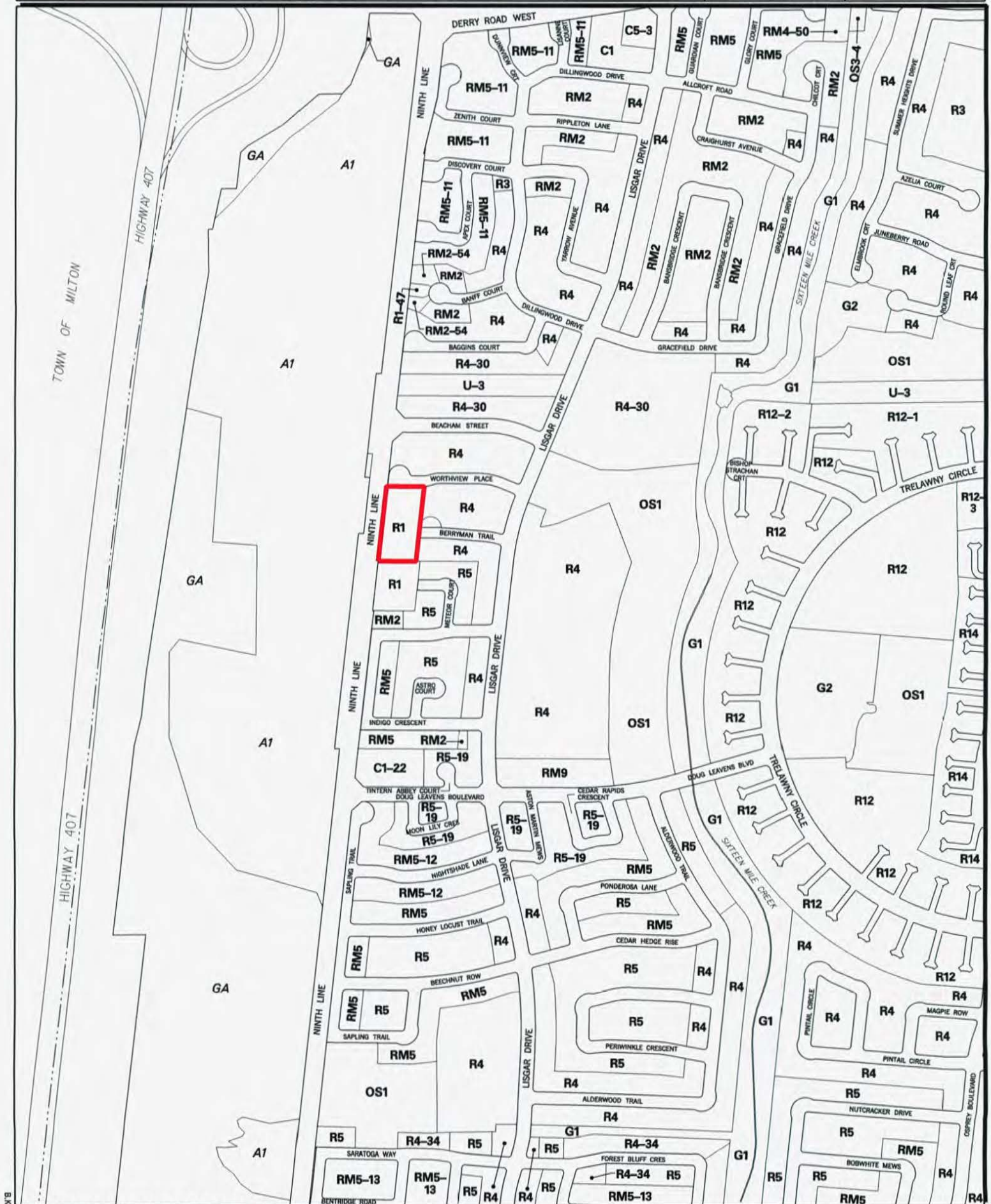
Files: OZ 13/014 W10  
T-M13004 W10**Zoning By-law Standards**

	<b>Existing "R1" Zoning Standards</b>	<b>"R4" Zoning Standards</b>	<b>Proposed "R4 – Exception" Zoning Standards</b>
Permitted Use	Detached Dwellings	Detached Dwellings	Detached Dwellings
Minimum Lot Area – Interior Lot	750 m <sup>2</sup> (8,072.9 sq. ft.)	365 m <sup>2</sup> (3,928.8 sq. ft.)	308 m <sup>2</sup> (3,315.3 sq. ft.)
Minimum Lot Frontage – Interior Lot	22.5 m (73.8 ft.)	12.0 m (39.4 ft.)	11.0 m (36.1 ft.)
Minimum Interior Side Yard – Interior Lot	1.8 m (5.9 ft.) on the one side of the lot and 4.2 m (13.8 ft.) on the other side	1.2 m (3.9 ft.)	1.2 m (3.9 ft.) on one side of the lot and 0.61 m (2 ft.) on the other side
Maximum Lot Coverage	25%	40%	45%

## GENERAL CONTEXT MAP

OZ 13014 W10  
T-M13004 W10

APPENDIX I-11







# Corporate Report

Clerk's Files

Originator's  
Files CD.21.AMA W4

---

**DATE:** January 14, 2014

**TO:** Chair and Members of Planning and Development Committee  
Meeting Date: February 3, 2014

**FROM:** Edward R. Sajecki  
Commissioner of Planning and Building

**SUBJECT:** **Official Plan Amendment and Rezoning**  
**To permit revisions to the Parkside Village Master Plan**  
**to redeploy density resulting from the addition of**  
**Square One Drive**  
**Part of Lot 19, Concession 2, N.D.S.**  
**West side of Confederation Parkway, north of**  
**Burnhamthorpe Road West**  
**Owner: Amacon Development (City Centre) Corp.**  
**Applicant: City of Mississauga**  
**Bill 51**

**Supplementary Report**

**Ward 4**

---

**RECOMMENDATION:** That the Report dated January 14, 2014, from the Commissioner of Planning and Building recommending approval of the applications under File CD.21.AMA W4, Amacon Development (City Centre) Corp., Part of Lot 19, Concession 2, N.D.S., be adopted in accordance with the following:

1. That notwithstanding that subsequent to the public meeting, changes to the amendments have been proposed, Council considers that the changes do not require further notice and, therefore, pursuant to the provisions of subsection 34(17) of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, any

further notice regarding the proposed amendment is hereby waived.

2. That the application to amend Mississauga Official Plan from "Public Open Space" to "Downtown Mixed Use" and "Public Open Space", as amended, and to add a new Minor Collector road (Square One Drive), be approved, as per Appendix S-3 of this Report. This amendment will permit revised park limits and add a new road.
3. That the application to change the Zoning from "CCOS" (City Centre – Open Space) to "CC4-5" (City Centre – Mixed Use) and "CCOS" (City Centre – Open Space), as amended, and change the CC4 (City Centre – Mixed Use) zoning provisions to permit revised tower placement, heights and floor plates, be approved in accordance with Appendix S-4 of this Report.
4. That Legal Services be directed to prepare an amendment to the Agreement dated December 14, 2005, relating to the Urban Design Control Document to incorporate the revisions as per Appendix S-5, to accommodate the revised master plan.
5. That the decision of Council for approval of the rezoning application be considered null and void, and a new development application be required unless a zoning by-law is passed within 18 months of the Council decision.

**REPORT  
HIGHLIGHTS:**

- Since the Public Meeting on October 15, 2012 the City has held many meetings with the owner (Amacon Development (City Centre) Corp. to determine the means of acquiring the lands to accommodate the extension of Square One Drive;
- A Section 30 Agreement under the Expropriations Act is to be executed, following all planning approvals mentioned in the Recommendations of this Report, in order to transfer the Square One Drive lands to the City;
- As more than 9 months have lapsed since the Public Meeting, the public have been re-notified of this meeting and

**Supplementary Report.**

- This report provides responses to the public comments received and outlines the recommendations for approval of revisions to the Official Plan and Zoning By-law to accommodate the revised master plan.

**BACKGROUND:**

A public meeting was held by the Planning and Development Committee on October 15, 2012, at which time a Planning and Building Department Information Report (Appendix S-1) was presented and received for information.

At the public meeting, the Planning and Development Committee passed Recommendation PDC-0061-2012 which was subsequently adopted by Council and is attached as Appendix S-2. As more than 9 months have passed since the Public Meeting, full notice has been sent regarding this Supplementary Report and meeting.

On January 16, 2013 Council passed the By-law to remove the "H" Holding symbol from Phase 2 of the Parkside Village Subdivision (T-M04001 W4) as the required Development and Servicing Agreements were approved on November 14, 2012.

Further to the Downtown21 Master Plan, the City of Mississauga has held many discussions with Amacon Development (City Centre) Corp., the owner of the subject lands, to explore the acquisition of a new road (Square One Drive extension) from Confederation Parkway to Rathburn Road West. The owners and the City have now come to an agreement for the transfer of the Square One Drive lands to the City as per the terms of a Section 30 Agreement under the Expropriations Act, which takes the place of the Density Transfer and Road Dedication Agreement mentioned within the Information Report. With the terms of this agreement established, the City is recommending that the Official Plan and Zoning By-law be amended to accommodate the revised Parkside Village Master Plan, the details of which were outlined in the Information Report (see Appendix S-1).

There have been changes to the Zoning By-law amendment from the version within the Information Report in order to incorporate

the latest site plan designs (see Appendix S-4 for the revised draft By-law).

**COMMENTS:**

See Appendix S-1 - Information Report prepared by the Planning and Building Department.

**COMMUNITY ISSUES****Comment**

Concern was raised with revised tower location and height transition within Block 1, and how it may block views and shadow the existing apartment towers to the north (see Appendices I-5 and I-6 within Appendix S-1 of this report).

**Response**

The existing master plan and Zoning By-law allows for two towers at maximum heights of 35 storeys (closest to Confederation Parkway) and 18 storeys (closest to the Enersource Hydro Station) at zero setback to the property line with 330 and 350 Rathburn Road West, and a total of 7 towers within Block 1. The revised plan, which incorporates Square One Drive, proposes that these two towers be removed and a total of 5 towers be permitted within Block 1 set back a minimum of 23.5 m (77 ft.) from the property line. The only tower within 23.5 m (77 ft.) is proposed to be 20 storeys versus 35 storeys at zero setback in the previous plan, however it is shifted further west from Confederation Parkway to provide sufficient distance separation from the tower to the south.

As a result of the reduced block size, the loss of two towers and the need for minimum separation distances between towers, some of the other tower heights have increased with original towers 1e (18 storeys), 1f (25 storeys) and 1g (18 storeys), now proposed to be 24 storeys, 34 storeys, and 24 storeys respectively.

An updated Shadow Study has been provided for the revised proposal which concludes that the revised plan provides for reduced shadow impacts.



**Comment**

There was concern with the potential for road noise and potential lighting impacts from traffic lights that might be located at Confederation Parkway and Square One Drive.

**Response**

Due to the proximity of major roads such as Rathburn Road West and Confederation Parkway, and Highway 403, traffic noise already exists within the immediate area. Traffic noise from the proposed Square One Drive (a minor collector) will not exceed the background noise levels from the existing major roads in the vicinity. Traffic lights will be installed at the intersection when traffic levels warrant it for safety and traffic purposes and must be located at heights and locations required by Ministry of Transportation standards.

**Comment**

There was concern that residents and visitors of the Amacon Parkside Village subdivision may park on Wallenberg Crescent (existing neighbourhood to the west) and walk through the linear park to the subject lands.

**Response**

Each development block within the Amacon Parkside Village subdivision must meet zoning requirements for resident, visitor, commercial and office parking before any building permits are issued. In addition, the streets within the Parkside Village subdivision have been designed to provide for on-street municipal parking within immediate proximity of the subject lands. There is no vehicular access connection between the Parkside Village subdivision and Wallenberg Crescent, only pedestrian walkways through surrounding parks. Should residents along Wallenberg Crescent wish to have on-street parking removed from their street they can submit a petition to Council to pass a By-law to eliminate on-street parking on their street.

**Comment**

Some residents wanted more forested/natural areas within park spaces, and some were concerned with crime in the parks.

**Response**

Detailed design has not been completed for the park blocks, however the desire for more forested/natural areas was forwarded to the Community Services Department. It should be noted that the intent is to have urban parks that have clear visibility to improve safety, and as such, Crime Prevention Through Environmental Design principles will be used in the design stage.

**Comment**

The Condominium Corporation for 330 and 350 Rathburn Road West (immediately to the north of the proposal) expressed concerns with the following:

- (a) the potential loss of their existing access to Confederation Parkway and impacts of potentially having only one access (Rathburn Road);
- (b) damages to their underground garage adjacent to their south property line and proposed Square One Drive;
- (c) potential impact on their electrical supply from the Enersource Hydro duct which runs immediately south of their lands and under the area for proposed Square One Drive; and,
- (d) the impact the proposed development would have on their storm sewer.

**Response**

Ward 4 Councillor Dale and Transportation and Works and Planning staff met with the Condominium Board for 330 and 350 Rathburn Road West to review the proposal and address any

questions and concerns. The following responses are provided as a summary of the responses to the issues raised:

- (a) Prior to the extension of Square One Drive being built, the City will have to undertake an Environmental Assessment, which is a public process, in order to assess various options, issues and concerns. When the design is prepared for the road, the existing access from 330 and 350 Rathburn Road to Confederation Parkway will likely need to be relocated due to its close proximity to the proposed intersection and safety and access constraints. The access would then be relocated to Square One Drive with all costs covered by the City of Mississauga, not the condominium corporation;
- (b) Through the Environmental Assessment and detailed design, potential impacts on the existing underground parking garage will be reviewed to ensure appropriate protection and mitigation of damage;
- (c) The City is aware of the Enersource Hydro supply running below grade within a 5 m (16 ft.) easement immediately south of 330 and 350 Rathburn Road West, within the proposed right-of-way for Square One Drive. This easement and its hydro service will be maintained within the north boulevard and sidewalk for Square One Drive.
- (d) The proposed road will not drain into the storm drain for 330 and 350 Rathburn Road West, and no additional stormwater impacts are anticipated.

## **UPDATED AGENCY AND CITY DEPARTMENT COMMENTS**

### **Transportation and Works**

Comments updated January 6, 2014, state that within the Information Report comments dated September 12, 2012 indicated that the proposed density transfer within the Amacon plan, to accommodate the acquisition and construction of Square One

Drive west of Confederation Parkway, to ultimately intersect with a round-about at Rathburn Road, would have impact on the current location of the existing Enersource Mississauga (Confederation MS) hydro sub-station and the existing configuration of Zonta Meadows Park.

These concerns and a number of impacts to the application and adjacent properties associated with the conceptual alignment for Square One Drive, as per the Downtown21 Master Plan have been evaluated, including the estimated costs. The Transportation and Works Department are satisfied with the proposed Section 30 Expropriations Act Agreement between the applicant and the City for the acquisition of Square One Drive. The principles to be included within the agreement were presented to the General Committee of Council on December 4, 2013, modified in accordance with Recommendation GC-0669-2013 and adopted by City Council on December 11, 2013.

### **Community Services**

Comments updated January 7, 2014 from Community Services state that within the Information Report, comments dated September 12, 2012, this department had indicated that in exchange for the applicant retaining ownership of Block 4, the City would require a public easement over the entire block and the applicant is required to incur all the costs associated with the design and construction of this block, to the satisfaction of the City. Furthermore, the requirement for the applicant to dedicate an area of 250 m<sup>2</sup> (2,691 sq. ft), to be added to Block 3, Plan 43M-1808, for partial fulfilment of land for park or public recreational purposes is addressed in the proposed Section 30 Expropriation Act Agreement between the applicant and the City. The principles to be included in the agreement were presented to the General Committee of Council on December 4, 2013, modified in accordance with Recommendation GC-0669-2013 and adopted by City Council on December 11, 2013.



## PLANNING COMMENTS

### Official Plan

Mississauga Official Plan (2012) was adopted by City Council on September 29, 2010 and partially approved by the Region of Peel on September 29, 2011. The Plan was appealed in its entirety; however, on November 14, 2012 the Ontario Municipal Board issued a Notice of Decision approving Mississauga Official Plan, as modified, save and except for certain appeals which have no effect on the proposed amendments.

MOPA 8, which implements the vision of the Downtown21 Master Plan following the Interim Control By-law for the Downtown, was approved by Council on March 6, 2013 and is currently under appeal and not in force.

The proposal requires an amendment to Mississauga Official Plan. Section 19.5.1 of Mississauga Official Plan provides criteria for evaluating site specific Official Plan Amendments. Each criterion is summarized below along with a discussion of how the proposed amendment addresses the intent of the criteria.

***Will the proposal adversely impact or destabilize the overall intent, goals and objectives of the Official Plan; and the development or functioning of the remaining lands which have the same designation, or neighbouring lands?***

These City initiated amendments for the addition of the new Square One Drive extension are fully in keeping with the intent, goals and objectives of the Official Plan to obtain a finer grained road network within the Downtown Core to provide for better connectivity. The reduced park block size from a 30 metre (98 ft.) frontage to a 20 metre (65 ft.) frontage between the urban townhomes will not impact adjacent lands, as this is a passive parkette which provides an open space connection and is not a park for programmed activities.

*Are the lands suitable for the proposed uses, and are the proposed land uses compatible with existing and future uses of the surrounding lands?*

There are no new land uses being introduced through these amendments, and the revised limits of development are appropriate and compatible with existing and surrounding lands.

*Is there adequate engineering services, community infrastructure and multi-modal transportation systems to support the proposed application?*

The proposed amendments will provide needed improvements to infrastructure through the addition of the Square One Drive extension providing a key east-west link into the Downtown Core.

*Has a planning rationale with reference to Mississauga Official Plan policies, other relevant policies, good planning principles and the merits of the proposed amendment in comparison with the existing designation been provided by the applicant?*

The proposed City initiated amendments are consistent with the goals and objectives of Mississauga Official Plan and MOPA 8 which implements the Downtown21 Master Plan vision.

### **Zoning**

The proposed "CC4-5" (City Centre – Mixed Use) and "CCOS" (City Centre – Open Space), as amended zone limits, and amended CC4 (City Centre – Mixed Use) zoning provisions, as outlined in Appendix S-4, are appropriate to accommodate a revised master plan with revised tower placement, heights and floor plates.

### **FINANCIAL IMPACT:**

Development charges will be payable in keeping with the requirements of the applicable Development Charges By-law of the City as well as financial requirements of any other official agency concerned with the development of the lands.

Financial impacts regarding the acquisition and construction of Square One Drive were outlined to General Committee of Council

on December 4, 2013, and modified in accordance with Recommendation GC-0669-2013 and adopted by City Council on December 11, 2013.

**CONCLUSION:**

The creation of a finer street grid and improved connectivity is a fundamental cornerstone of the Downtown21 Master Plan to support growth in a more urban form. The extension of Square One Drive provides an important east-west routing alternative into the Downtown Core, particularly given the number of traffic constraints on Rathburn Road West through the addition of Bus Rapid Transit (BRT), Light Rail Transit (LRT), and Multi-Use Trail (MUT).

In accordance with subsection 34(17) of the *Planning Act*, R.S.O. 1990, c.P. 13, as amended, Council is given authority to determine if further public notice is required. Since the change is to implement the latest design of the site plans by Amacon to not require a loading space for the urban townhomes within Blocks 3 and 5 under SP 12/052 W4, and to allow for seasonal display of goods as well as minor height and setback adjustments for Block 7 under SP 12/072 W4, it is recommended that no further public meeting needs to be held regarding the proposed changes (see updated Zoning By-law within Appendix S-4). In addition, this report was given full circulation and notification as more than 9 months have lapsed since the Public Meeting.

The proposed Official Plan Amendment and rezoning are acceptable from a planning standpoint and should be approved for the following reasons:

1. The proposed changes are compatible with the surrounding land uses by maintaining the same land uses and providing improved connectivity with the addition of Square One Drive. The revised master plan provides for fewer towers and better spacing to improve sun/shadow conditions, while increasing heights in areas deemed appropriate.

2. The proposed Official Plan provisions and zoning standards are appropriate to accommodate the required changes and update the Parkside Village Master Plan.

**ATTACHMENTS:**

Appendix S-1: Information Report  
Appendix S-2: Recommendation PDC-0061-2012  
Appendix S-3: Mississauga Official Plan Amendment Schedule  
Appendix S-4: Revised Draft Zoning By-law Amendment  
Appendix S-5: Revisions to Urban Design Control Document



Edward R. Sajecki  
Commissioner of Planning and Building

*Prepared By: Jonathan Famme, Development Planner*







# Corporate Report

Originator's  
Files CD.21.AMA W4

**PDC** OCT 15 2012

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**DATE:** September 25, 2012

**TO:** Chair and Members of Planning and Development Committee  
Meeting Date: October 15, 2012

**FROM:** Edward R. Sajecki  
Commissioner of Planning and Building

**SUBJECT:** **Information Report**  
**Official Plan Amendment and Rezoning**  
**To permit revisions to the Parkside Village Master Plan**  
**to redeploy density resulting from the addition of**  
**Square One Drive**  
**Part of Lot 19, Concession 2, N.D.S.**  
**West side of Confederation Parkway, north of**  
**Burnhamthorpe Road West**  
**Owner: Amacon Development (City Centre) Corp.**  
**Applicant: City Initiated**  
**Bill 51**

**Public Meeting**

**Ward 4**

---

**RECOMMENDATION:** That the Report dated September 25, 2012, from the Commissioner of Planning and Building regarding the amendment of the Official Plan from "Public Open Space" to "Mixed Use" and "Public Open Space", as amended, and to add a new Minor Collector road (Square One Drive), and to change the Zoning from "H-CCOS" (City Centre – Open Space) to "H-CC4-5" (City Centre – Mixed Use) and "H-CCOS" (City Centre – Open Space), as amended, and change the CC4 (City Centre – Mixed Use) zoning provisions in accordance with Appendix I-9, to permit revisions to the Parkside Village Master Plan to redeploy density resulting from the addition

of Square One Drive under file CD.21.AMA W4, Amacon Development (City Centre) Corp., Part of Lot 19, Concession 2, N.D.S., be received for information.

**REPORT  
HIGHLIGHTS:**

- The Downtown21 Master Plan establishes the vision for the development of the downtown and identifies Square One Drive extending westerly from Confederation Parkway through the north portion of the previously approved Amacon Parkside Village subdivision to Rathburn Road.
- Additional road network will improve connectivity, facilitate traffic movement, and create capacity to off-set vehicle capacity lost with the addition of Bus Rapid Transit and Light Rail Transit along Rathburn Road West.
- The addition of Square One Drive impacts the approved Parkside Village subdivision development.
- In order to facilitate the acquisition of Square One Drive and alleviate the impacts to previously approved planning permissions, the Planning and Building Department has brought forward City initiated amendments to redistribute the previously approved density of 5,321 units throughout the remainder of the lands as per the revised master plan.
- A Density Transfer and Road Dedication Agreement is required to be entered into between the City of Mississauga and Amacon to provide the terms and conditions regarding the transfer of the required lands for Square One Drive and redistribution of density.
- A value assessment is required for the Square One Drive lands, and budget approval for any compensation for the transfer of the lands from Amacon to the City of Mississauga.

**BACKGROUND:**

The Official Plan Amendment and Rezoning of lands comprising the Amacon Parkside Village subdivision were approved by Council on March 9, 2005, revised December 14, 2005, and the plan of subdivision was approved July 4, 2005 by the Commissioner of Planning and Building under files OZ 04/013 W4 and T-M04001 W4.

Through the Downtown21 initiative a vision was created to assist in developing the Downtown from a suburban auto-oriented place

to a vibrant urban area. The Downtown21 Master Plan was received by Council on April 28, 2010, and showed Square One Drive extending through the north portion of the Amacon Parkside Village lands to connect with Rathburn Road West.

City staff have held meetings with Amacon to explore the option of creating the additional road network as envisioned by the Downtown21 Master Plan. Additional road network will assist to offset the impacts due to the loss of traffic lanes on existing roads such as Rathburn Road West from Bus Rapid Transit (BRT) and potential Light Rail Transit (LRT).

The addition of Square One Drive, comprising a right-of-way width of 23.5 m (77 ft.), removes approximately 0.32 ha (0.79 ac.) of developable land within Block 1 at the north of the Amacon Parkside Village subdivision, and requires revisions to the Master Plan. In order to maintain the density and population planned for the Amacon lands and associated amenities that were approved, the current approved 5,321 units should be re-deployed throughout the blocks of the remaining lands (see Appendix I-5 Existing/Approved Master Plan, and Appendix I-6 Revised Master Plan). City initiated amendments are proposed to the Official Plan and Zoning By-law to reflect the Revised Master Plan.

The above-noted City initiated amendments have been circulated for technical comments.

The purpose of this report is to provide preliminary information on the amendments and to seek comments from the community.

#### COMMENTS:

Details of the proposal are as follows:

<b>Development Proposal</b>	
Amendments submitted:	City Initiated – circulated April 11, 2012
Height:	<b>Block 1</b> 5 towers (45 storeys, 34 storeys, 20 storeys, and two 24 storeys)

Development Proposal	
	<p><i>whereas</i>  <i>7 towers (45 storeys, 35 storeys, 31 storeys, 25 storeys, and three 18 storeys) previously approved</i></p> <p><b>Block 2</b>  Park Block (unchanged)</p> <p><b>Block 3</b>  3 storey horizontal multiple units  <i>whereas</i>  <i>4 to 5 storey mid-rise apartments previously approved</i></p> <p><b>Block 4</b>  Open Space Block (20 m frontage)  <i>whereas</i>  <i>Park Block (30 m frontage) previously approved</i></p> <p><b>Block 5</b>  3 storey horizontal multiple units  <i>whereas</i>  <i>4 to 5 storey mid-rise apartments previously approved</i></p> <p><b>Block 6</b>  2 towers (48 storeys and 40 storeys)  <i>whereas</i>  <i>1 tower (25 storeys) previously approved</i></p> <p><b>Block 7</b>  2 towers (48 storeys and 42 storeys)  <i>whereas</i>  <i>2 towers (45 storeys and 37 storeys) previously approved</i></p>



<b>Development Proposal</b>	
	<p><b>Block 8</b>  3 towers (45 storeys and two 36 storeys)  approved by variance and  under construction  <i>whereas</i>  3 towers (40 storeys and two 32 storeys)  <i>previously approved</i></p> <p><b>Block 9</b>  2 towers (two 22 storeys)  <i>whereas</i>  1 tower (13 storeys) <i>previously approved</i></p> <p><b>Block 10</b>  Park Block (unchanged)</p> <p><b>Block 11</b>  3 towers (55 storeys, 40 storeys and  12 storeys)  <i>whereas</i>  3 towers (50 storeys, 40 storeys and  12 storeys) <i>previously approved</i></p> <p>NOTE: All blocks, except park blocks,  have podium heights ranging from a  minimum of 3 storeys to a maximum of  7 storeys (2 to 7 storeys <i>previously</i>  <i>approved</i>)</p>
Net Density:	806 units/ha 326 units/acre
Number of units:	5,321 (same as originally approved)
Anticipated Population:	13,075* *Average household sizes for all units (by type) for the year 2011 (city average) based on the 2008 Growth Forecasts for the City of Mississauga.

<b>Development Proposal</b>	
Parking Required:	1.0 resident space per unit 0.15 visitor spaces per unit (permitted to be shared with non-residential spaces) 4.3 spaces per 100 m <sup>2</sup> – retail or personal service (excluding restaurants)
Parking Provided:	same as required (see above)
Supporting Documents:	Planning Justification Revised Master Plan Rev. Urban Design Control Document

<b>Site Characteristics</b>	
Frontage:	185 m (607 ft.) – Burnhamthorpe Rd W. 542 m (1,778 ft.) – Confederation Pkwy
Net Lot Area:	6.6 ha (16.3 ac.) - does not include roads (Square One Drive = 0.32 ha (0.79 ac.))
Existing Use:	Predominantly vacant, Block 8 under construction along with Arbutus Way, Brickstone Mews and Curran Place surrounding Block 8

Additional information is provided in Appendices I-1 to I-10.

### **Neighbourhood Context**

The property is located at the western limit of the Downtown, west of Confederation Parkway and north of Burnhamthorpe Road West. The site is currently vacant save for Block 8, which is currently under construction comprising three residential towers (45 storeys and two 36 storeys) with some office in the podium and at-grade retail. Information regarding the history of the site is found in Appendix I-1.

The surrounding land uses are described as follows:

North: Enersource Hydro station, existing apartment building (19 storeys), Rathburn Road West

- East: Confederation Parkway, Chicago apartments and at-grade retail (36 storeys), One Park Tower apartments (38 storeys), Community Common Park, Amica Retirement Dwelling (7 storeys), YMCA parking, and Daniels Sales Centre
- South: Burnhamthorpe Road West, vacant future mixed use development (Rogers Garden City)
- West: Zonta Meadows Park, Wallenberg Crescent, detached dwellings (2 storeys)

**Current Mississauga Plan Designation and Policies for City Centre (May 5, 2003)**

**"Mixed Use"** which permits a wide range of land uses and activities grouped either within a development parcel or an individual building. Permitted uses include all forms of high density residential development, offices, civic and cultural facilities, hotels, conference facilities, all types of restaurants, entertainment facilities, commercial uses, community facilities and open space.

**Special Site 3** requires one or more areas of public parkland. Two park blocks (Blocks 2 and 10) and one publicly accessible open space block via easement (Block 4) have been provided through the plan of subdivision for AMACON Parkside Village (T-M04001 W4) to address this policy.

**"Public Open Space"** which permit parks and related public spaces which are urban in character and may accommodate parking and a variety of programs such as music and theatre events, displays and exhibitions, festivals and other special events.

The subject lands are located within the **Urban Growth Centre**, which is a designated **Intensification Area**.

There are other policies in the Official Plan which also are applicable in the review of these amendments including:

### Urban Growth Centre

#### 3.13.2.2

A minimum building height of three (3) storeys is required on lands within the Urban Growth Centre.

#### 3.13.2.5

Reduction in densities within the Urban Growth Centre will not be permitted unless considered through a municipal comprehensive planning study.

#### 3.13.2.7

Lands immediately adjacent to or within the Urban Growth Centre should provide a transition between the higher density and height of development within the Urban Growth Centre and lower density and height of development in the surrounding area.

#### 3.13.2.

The Urban Growth Centre will develop as a major regional centre and the primary location for mixed use development. The Urban Growth Centre will contain the greatest concentration of activities and variety of uses including the highest residential and employment densities and building height and largest commercial component in the City.

### Urban Design Policies

#### 3.18.2.1

The most prominent, most intensive and highest built form in terms of density and height will be encouraged in the City Centre.

### Roads

#### 3.17.5.5

Additional minor collector roads and local roads may be identified during the review of development applications.



## 3.17.5.1d.

Minor Collectors will be designed to accommodate intra-neighbourhood traffic. Direct access will be controlled. Minor Collectors may serve as transit routes.

## 3.17.4.8

The design of roads may vary to suit local conditions, such as a distinctive natural or built environment. This Plan will establish the location of such roads and policies to ensure that their design is compatible with the surrounding development.

## Criteria for Site Specific Official Plan Amendments

Section 5.3.2 of Mississauga Plan contains criteria which requires an applicant to submit satisfactory planning reports to demonstrate the rationale for the proposed amendment as follows:

- the proposal would not adversely impact or destabilize the following: the overall intent, goals and objectives of the Official Plan; and the development and functioning of the remaining lands which have the same designation, or neighbouring lands;
- the lands are suitable for the proposed uses, and compatible with existing and future uses of surrounding lands;
- there is adequate infrastructure and community services to support the proposed development.

**New Mississauga Official Plan**

Mississauga Official Plan was adopted by City Council on September 29, 2010. Until the new Mississauga Official Plan is approved by the Region of Peel and comes into force, Mississauga Plan continues to be in effect. While the existing Official Plan is the plan of record against which the application is being reviewed, regard should also be given to the new Mississauga Official Plan. Under the new Mississauga Official Plan, the subject lands are designated "**Downtown Mixed Use**" and "**Public Open Space**"

subject to **Special Site 3** of the Downtown Core District Policies. In the event that Mississauga Official Plan comes into force and effect as it pertains to the subject lands, it is recommended that Mississauga Official Plan be amended from "Downtown Mixed Use" and "Public Open Space" to "Downtown Mixed Use" and "Public Open Space", as amended, and add a new road (Square One Drive).

### **Proposed Official Plan Designation and Policies**

An Official Plan amendment is proposed to add a new Minor Collector road (Square One Drive) to extend from Confederation Parkway westwards through the Amacon lands to Rathburn Road West at a 23.5 m (77 ft.) Right-of-Way, and alter the limits of Block 4 "Public Open Space" from 30 m (98.4 ft.) to 20 m (65.6 ft.) of frontage.

### **Existing Zoning**

**"CC4-1" (City Centre - Mixed Use)**, which permits seven towers ranging in height from 18 to 45 storeys and a mix of residential, office, and retail uses.

**"CC4-2" (City Centre - Mixed Use)**, which permits three towers ranging in height from 12 to 45 storeys and a mix of residential, office, and retail uses.

**"CC4-3" (City Centre - Mixed Use)**, which permits four towers ranging in height from 13 to 40 storeys and a mix of residential, office, and retail uses.

**"CC4-4" (City Centre - Mixed Use)**, which permits three towers ranging in height from 18 to 45 storeys and a mix of residential, office, and retail uses.

**"CC4-5" (City Centre - Mixed Use)**, which permits horizontal multiple dwellings at a maximum height of 5 storeys.

"CCOS" (City Centre – Open Space), which permits active and passive recreational uses and below grade parking structures.

### **Proposed Zoning By-law Amendment**

In addition to amending Map 29 of Schedule B of the Zoning By-law to show Square One Drive extending from Confederation Parkway westwards to Rathburn Road West, and altering the limits of Block 4 "CCOS" from 30 m (98.4 ft.) to 20 m (65.6 ft.) of frontage, there are amendments to the various exception schedules to govern the height and design of the blocks in accordance with the revised Master Plan.

The proposed revisions to the Zoning By-law are contained in Appendix I-9.

### **COMMUNITY ISSUES**

Any comments received at the public meeting or through correspondence will be outlined within the Supplementary Report.

### **DEVELOPMENT ISSUES**

Agency comments are summarized in Appendix I-8. Based on the comments received and the applicable Mississauga Plan policies the following matters will have to be addressed:

- the preparation of a reference plan to legally describe the lands to be conveyed to the City for Square One Drive, to ultimately be deposited in the Land Registry Office;
- a Phase 1 Environmental Assessment is required for the Square One Drive lands to ensure the lands are clear of any contamination;
- a Density Transfer and Road Dedication Agreement is required to be entered into between the City of Mississauga and Amacon to provide the terms and conditions regarding the transfer of the required lands for Square One Drive and redistribution of the density rendered undevelopable from Block 1;

- Value assessment of the lands for Square One Drive, and budget approval for any compensation for the transfer of the lands from Amacon to the City of Mississauga;
- Amendment to the Agreement dated December 14, 2005 relating to the Urban Design Control Document and phasing, as a result of revisions to the master plan;
- finalization of the revisions to the Urban Design Control Document;
- finalization of the Official Plan Amendment and Zoning By-law Amendment to incorporate changes to the master plan.

### **OTHER INFORMATION**

#### **Development Requirements**

In conjunction with the proposed development, there are certain items with respect to the transfer of density, dedication of Square One Drive, and amendment to the Urban Design Control Document, resulting from revisions to the master plan which will require the applicant to enter into appropriate agreements with the City.

**FINANCIAL IMPACT:** Development charges will be payable in keeping with the requirements of the applicable Development Charges By-law of the City as well as financial requirements of any other official agency concerned with the development of the lands.

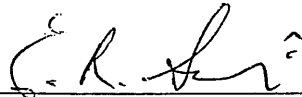
**CONCLUSION:** All agency and City department comments have been received and after the public meeting has been held and all issues are resolved, the Planning and Building Department will be in a position to make a recommendation regarding the amendments.

**ATTACHMENTS:**

- Appendix I-1: Site History
- Appendix I-2: Aerial Photograph
- Appendix I-3: Excerpt of City Centre District Land Use Map
- Appendix I-4: Excerpt of Existing Land Use Map
- Appendix I-5: Existing/Approved Master Plan
- Appendix I-6: Revised Master Plan



Appendix I-7: Conceptual Rendering  
Appendix I-8: Agency Comments  
Appendix I-9: Proposed Zoning Standards  
Appendix I-10: General Context Map



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Edward R. Sajecki  
Commissioner of Planning and Building

*Prepared By: Jonathan Famme, Development Planner*



**Amacon Development (City Centre) Corp.****File: CD.21.AMA W4****Site History**

- May 5, 2003 - Mississauga Plan, with the exception of certain policies that were appealed to the Ontario Municipal Board (OMB), came into full force and effect. Mississauga Plan applies to the subject lands.
- March 9, 2005 – Council approved the Official Plan Amendment and Rezoning applications under file OZ 04/013 W4 to expand the City Centre District, thus reducing the Creditview District and amend the designations and policies to suit the Master Concept Plan and block pattern, and approved the Plan of Subdivision under file T-M04001 W4.
- July 4, 2005 – Draft Plan of Subdivision under T-M04001 W4 is approved by the Commissioner of Planning and Building.
- December 14, 2005 – Council approved minor modifications to the approval under OZ 04/013 W4 and authorized the execution of the Urban Design Control Document for Amacon Parkside Village.
- June 20, 2007 – Zoning By-law 0225-2007 came into force except for those sites which have been appealed. As no appeals have been filed the provisions of the new By-law apply.
- March 6, 2008 - Committee of Adjustment approved minor variances for Block 8 (currently under construction) for changes to the percentage of the building located within the build-to-areas, maximum building heights (36, 36, and 45 storeys), removal of the setback of a tower from the podium, and to allow for a shared loading space ("A"78/08).
- March 23, 2010 – Phase 1 of the Draft Plan of Subdivision under T-M04001 W4 is registered under Plan 43M-1808 for Block 8 and those portions of Arbutus Way, Brickstone Mews, and Curran Place which surround the Block, as well as Park Blocks.
- March 31, 2010 – By-law to Remove "H" Holding Symbol was approved by Council for Blocks 2, 8, and 10 under file H-OZ 06/008 W4.
- April 28, 2010 – Downtown21 Master Plan is received by Council setting forth the vision for establishing an vibrant urban downtown in Mississauga City Centre, and showing additional road network including Square One Drive extending through the Amacon Parkside Village subdivision.
- April 2010 to present – City initiate meetings with Amacon regarding revisions to the Master Plan for Parkside Village to accommodate Square One Drive and re-deploy density.
- January 26, 2011 – Site Plan Approval was issued for Block 8 under file SP 07/050 W4.



# LEGEND:



SUBJECT LANDS

NOTE: DATE OF AERIAL PHOTO IS APRIL 2012.



## SUBJECT: **AMACON DEVELOPMENT (CITY CENTRE) CORPORATION**



FILE NO:  
**CD. 21. AMA**

DWG. NO:  
**CD-21A**

SCALE:  
**1:4000**

PDC DATE:  
**2012 10 15**

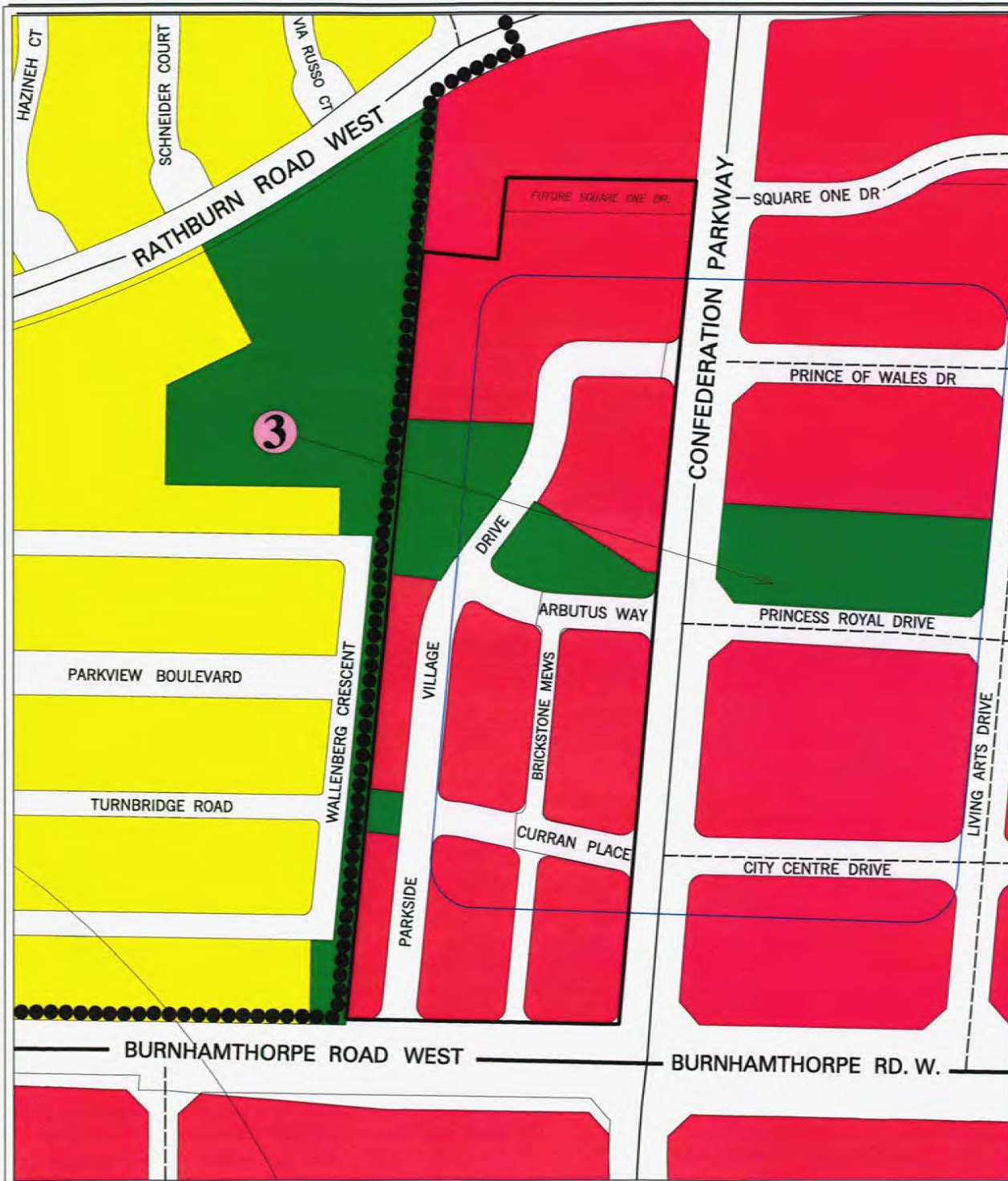
DRAWN BY:  
**B. KRUGER**

APPENDIX I-2

**MISSISSAUGA**  
Planning and Building

Produced by  
**T&W, Geomatics**





**PART OF CITY CENTRE DISTRICT LAND USE MAP  
CITY CENTRE DISTRICT POLICIES OF MISSISSAUGA PLAN**

**LAND USE DESIGNATIONS**

- Mixed Use
- Retail Core Commercial
- Public Open Space
- Greenbelt
- Parkway Belt West

- ① Option 1, Conceptual Centre View Drive connection to EB Hwy. 403 concept.
- ② Conceptual N-W ramp terminal relocation

**Note:**

All road proposals within the Highway 403 Corridor are conceptual and require review and approval of the Provincial Government.

-1996 NEP /2000 NEF Composite Noise Contours

Planning District

Special Site Areas (See Special Site Policies)

**SUBJECT LANDS**

**TRANSPORTATION LEGEND**

- Provincial Highway and Interchange
- Arterial
- Major Collector
- Minor Collector
- Local Road
- Bus Rapid Transit Corridor
- Bus Rapid Transit Station
- City Centre Transit Terminal
- Proposed Grade Separation
- Major Transit Corridor

Amendment No. 95 Appealed to the Ontario Municipal Board.  
Delete and replace with Higher Order Transit Corridor

Amendment No. 95 Appealed to the Ontario Municipal Board

**SUBJECT: AMACON DEVELOPMENT  
(CITY CENTRE) CORPORATION**



**FILE NO:**  
CD.21.AMA

**DWG. NO:**  
CD-21L

**SCALE:**  
1:4000

**PDC DATE:**  
2012 10 15

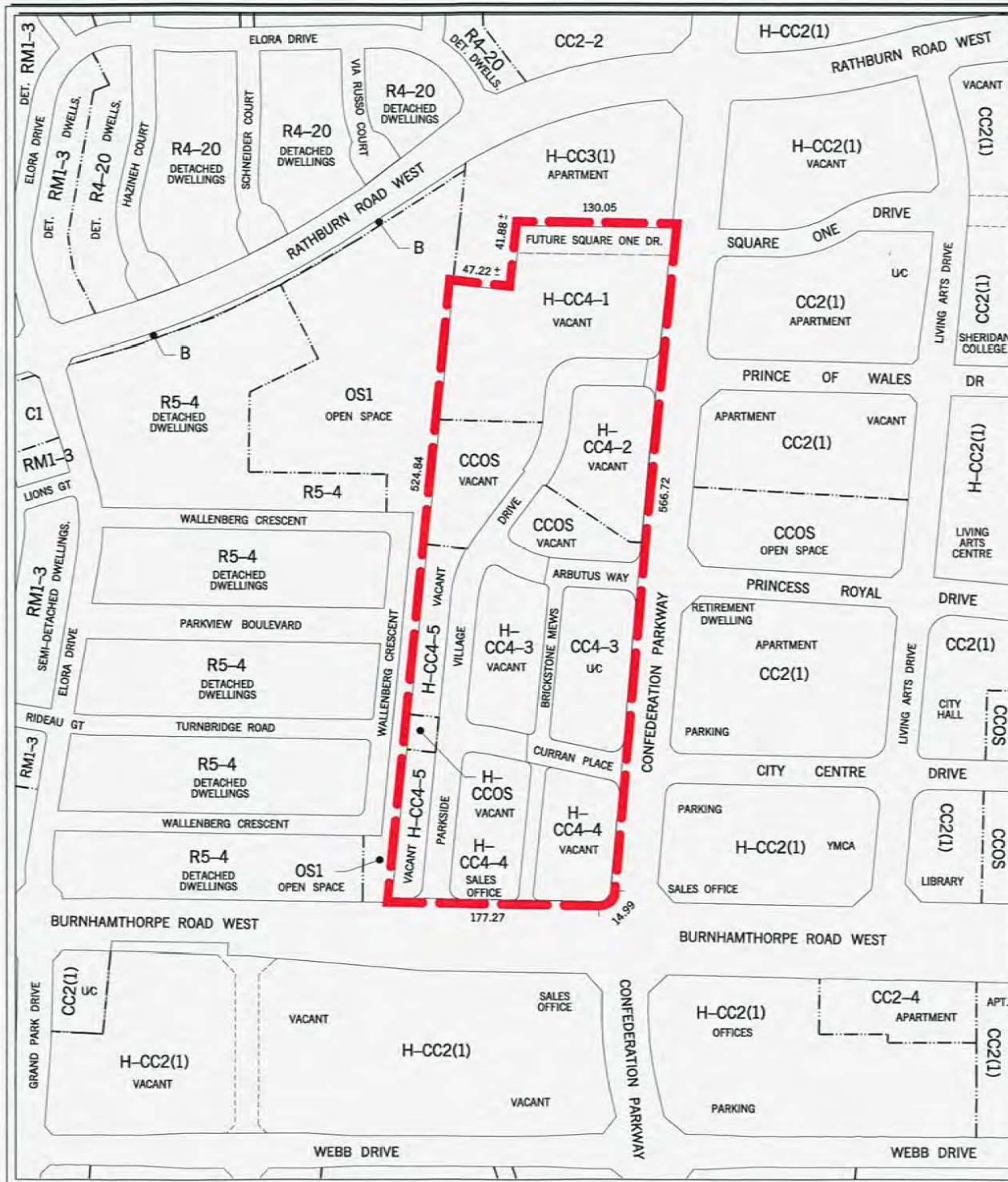
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B. KRUGER

**MISSISSAUGA**  
Planning and Building

Produced by  
**T&W, Geomatics**

**APPENDIX I-3**





# LEGEND:



PROPOSED OFFICIAL PLAN AMENDMENT FROM 'PUBLIC OPEN SPACE' TO 'MIXED USE' AND 'PUBLIC OPEN SPACE', AS AMENDED, AND ADD A NEW MINOR COLLECTOR ROAD (SQUARE ONE DRIVE), AND TO CHANGE THE ZONING FROM 'H-CCOS' (CITY CENTRE - OPEN SPACE) TO 'H-CC4-5' (CITY CENTRE - MIXED USE) AND 'H-CCOS' (CITY CENTRE - OPEN SPACE), AS AMENDED, AND CHANGE THE 'CC4' (CITY CENTRE - MIXED USE) ZONING PROVISIONS TO PERMIT REVISIONS TO THE PARKSIDE VILLAGE MASTER PLAN TO REDEPLOY DENSITY RESULTING FROM THE ADDITION OF QUARE ONE DRIVE.

5 - 29

## SUBJECT: **AMACON DEVELOPMENT (CITY CENTRE) CORPORATION**



FILE NO:  
**CD. 21. AMA**

DWG. NO:  
**CD-21R**

SCALE:  
**1:4000**

PDC DATE:  
**2012 10 15**

DRAWN BY:  
**B. KRUGER**

APPENDIX 1-4

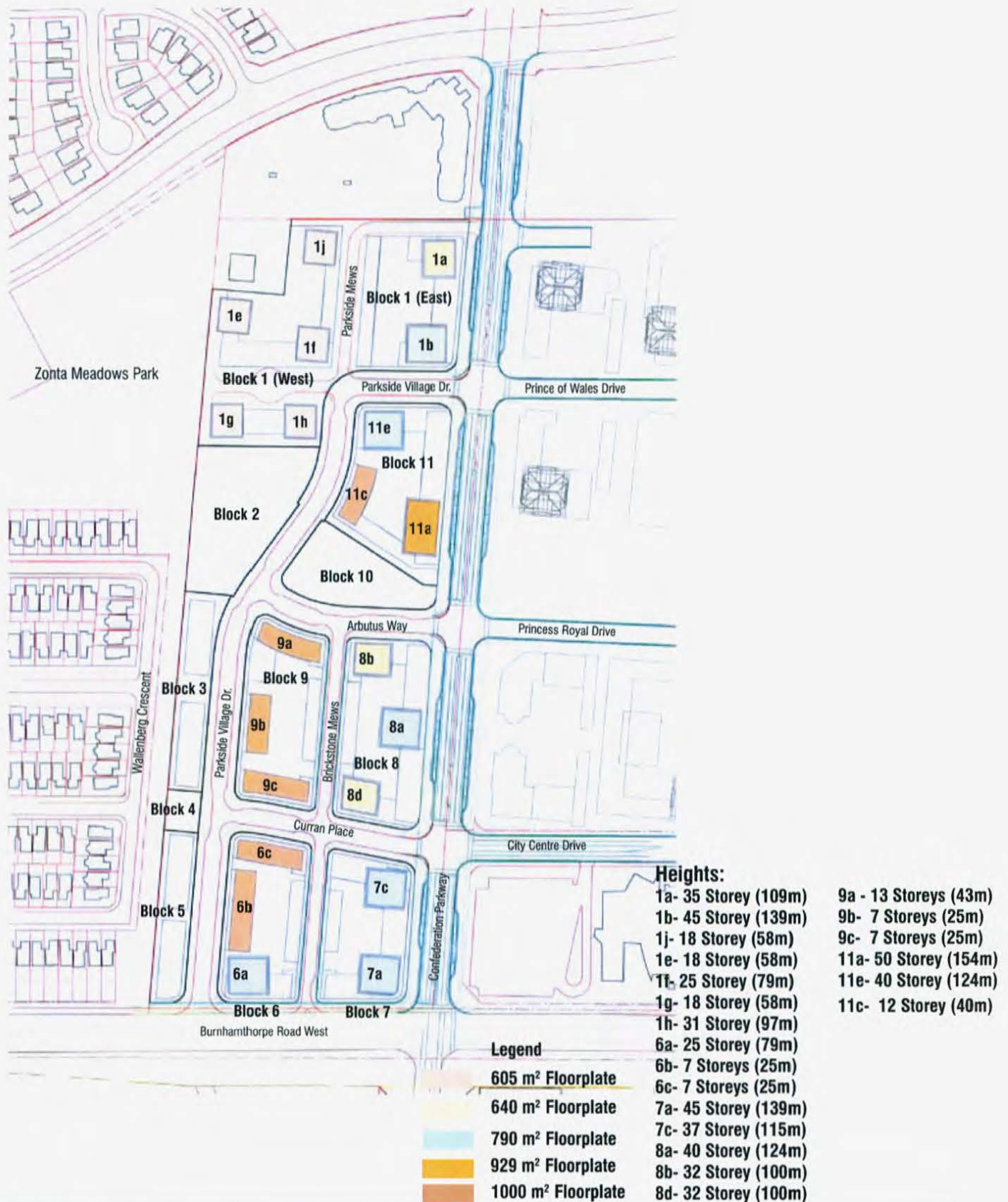
**MISSISSAUGA**  
Planning and Building

Produced by  
**T&W, Geomatics**

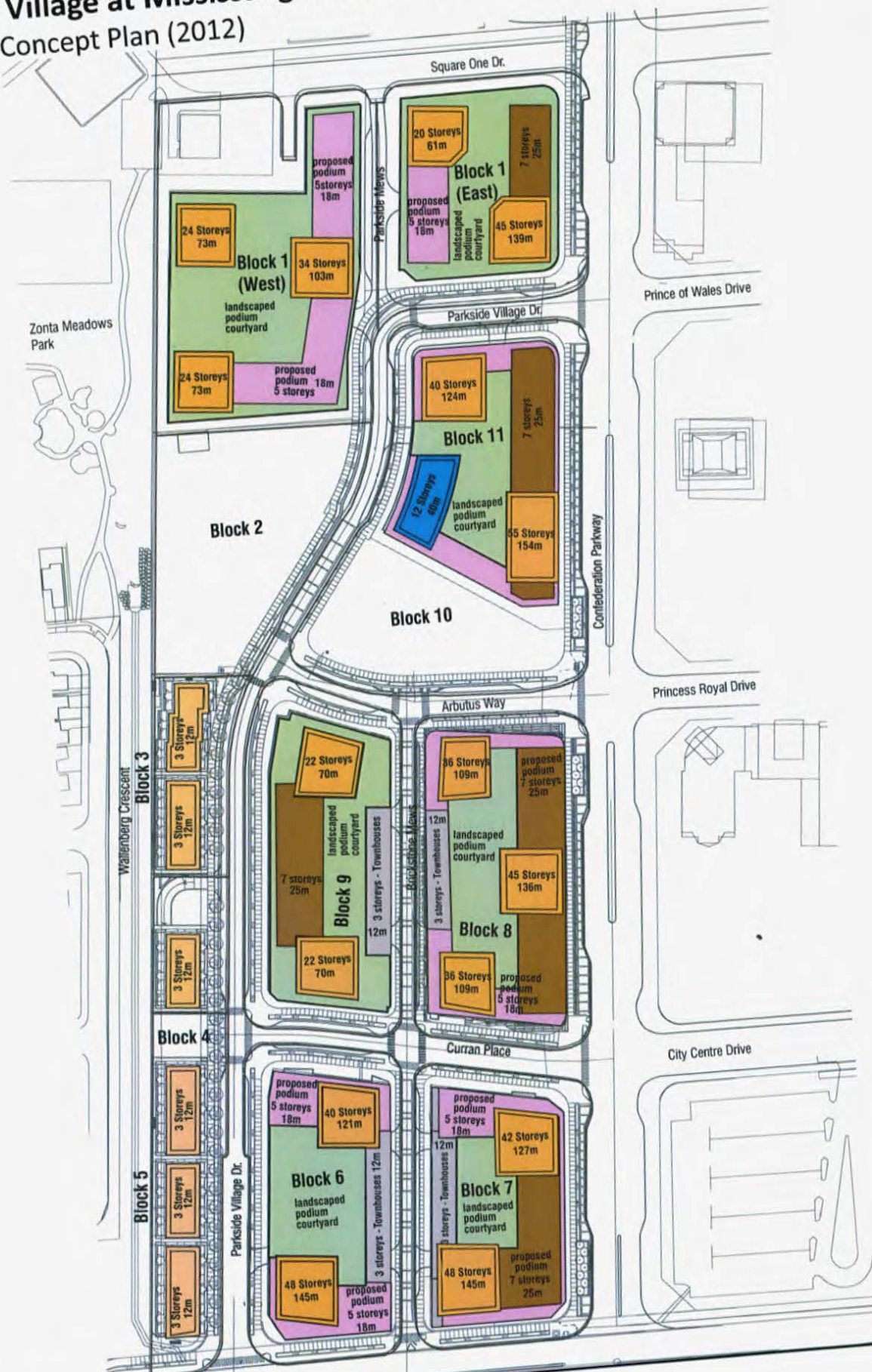


# Parkside Village at Mississauga City Centre

Excerpt from Urban Design Control Document (2005)











View Looking North West

11.171

A-28

November 22, 2011

3D View

Scale :

Parkside Village - Block 7

Mississauga Ontario

**TURNER FLEISCHER**  
 ARCHITECTS  
 40 Leslie Blvd. Toronto, ON Canada M2H 3P2  
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 www.turnerfleischer.com



**Amacon Development (City Centre) Corp****File: CD.21.AMA W4****Agency Comments**

The following is a summary of comments from agencies and departments regarding the amendments.

Agency / Comment Date	Comment
<p>City Community Services Department – Planning, Development and Business Services Division/Park Planning Section (September 12, 2012)</p>	<p>The draft approved plan, dated February 22, 2005, along with draft plan conditions of approval dated July 4, 2005 under file T-M04001, requires the dedication of Block 4 (also described as Block 1, Plan 43M-1808) for partial fulfillment of the requirements for park and recreational purposes. The developer has requested that the frontage of this park block be reduced from 30 m (98 ft.) to 20 m (65.6 ft.), and that it remain in private ownership, for the purposes of accommodating a continuous underground parking structure between development blocks. In exchange, the City would require a public easement over the entire block, to ensure that the land is developed and functions as public open space. The developer is required to incur all costs associated with the design and construction of this block, to the satisfaction of the City, and is advised that no credits for cash-in-lieu of parkland or development charges will be provided for this block.</p> <p>In exchange for the applicant retaining ownership of Block 4, the applicant will be required to gratuitously dedicate land having an area of 250 sq. m (2,691 sq. ft.), to be added to Block 3, Plan 43M-1808, for partial fulfilment of land for park or other public recreational purposes. The location and configuration of the 250 sq. m (2,691 sq. ft.) of land to be dedicated for park or other public recreational purposes shall be determined in conjunction with the preparation of detailed design plans for the development of Block 11 and Park Block 3, Plan 43M-1808. A revised draft plan condition reflecting the revised parkland dedication requirements will be required, and the payment of cash-in-lieu for the under dedication of parkland shall be outlined within the Development Agreement to the satisfaction of the Community Services Department.</p>
<p>City Transportation and Works Department (September 12, 2012)</p>	<p>In comments dated September 12, 2012 this department confirmed that the proposed density transfer within the Amacon plan, to accommodate the acquisition and construction of Square One Drive west of Confederation</p>

Amacon Development (City Centre) Corp

File: CD.21.AMA W4

Agency / Comment Date	Comment
	<p>Parkway, ultimately to intersect with a round-about at Rathburn Road will have impact on the current location of the existing Enersource Mississauga (Confederation MS) hydro sub-station and the existing configuration of the Zonta Meadows Park.</p> <p>There are a number of impacts to adjacent properties associated with the conceptual alignment for Square One Drive, as per the Downtown 21 Master Plan. This department is currently evaluating these details and the implications associated with the future implementation of this road link. Details will be addressed in the Supplementary Report.</p>
Other City Departments and External Agencies	<p>The following City Departments and external agencies offered no objection to these applications provided that all technical matters are addressed in a satisfactory manner: Region of Peel, Enersource Hydro Mississauga.</p>

A by-law to amend By-law Number 0225-2007, as amended.

WHEREAS pursuant to section 34 of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, the council of a local municipality may pass a zoning by-law;

NOW THEREFORE the Council of The Corporation of the City of Mississauga ENACTS as follows:

1. By-law Number 0225-2007, as amended, being a City of Mississauga Zoning By-law, is amended by deleting Table 7.1.5.2 and substituting the following therefor:

Table 7.1.5.2 – Underground Parking Structures

Column	A	B	C
Line	Street Width	Maximum Encroachment	Minimum Vertical Depth
1.0	12.5 m	6.25 m on each side of the street	0.5 m

2. By-law Number 0225-2007, as amended, is further amended by adding to Article 7.1.5.5 the following Sentence:

7.1.5.5.2 In a CC4 zone each block is to be considered as one (1) lot for the purposes of Zoning By-law compliance.

3. By-law Number 0225-2007, as amended, is further amended by deleting Sentence 7.2.5.1.2 in Exception Table 7.2.5.1 and substituting the following therefor:

7.2.5.1	Exception: CC4-1	Map # 29	By-law:																																																		
<b>Regulations</b>																																																					
7.2.5.1.2	Maximum heights and floor plates of all <b>buildings</b> and <b>structures</b> on Parcel Blocks 1A to 1I as identified on Schedule CC4-1 of this Exception shall conform to the following schedule:																																																				
	<table><tr><th>Parcel Block</th><th>Maximum Building Height</th><th>Maximum Tower Floor Plate</th><th>Minimum Podium Height</th><th>Maximum Podium Height</th></tr><tr><td>1A</td><td>81.0 m 24 storeys</td><td>670 m<sup>2</sup></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>1B</td><td>81.0 m 24 storeys</td><td>670 m<sup>2</sup></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>1C</td><td>18.0 m 5 storeys</td><td></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>1D</td><td>106.0 m 34 storeys</td><td>740 m<sup>2</sup></td><td>12.0 m 3 storeys</td><td>25.0 m 7 storeys</td></tr><tr><td>1E</td><td>18.0 m 5 storeys</td><td></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>1F</td><td>98.0 m 20 storeys</td><td>740 m<sup>2</sup></td><td>12.0 m 3 storeys</td><td>25.0 m 7 storeys</td></tr><tr><td>1G</td><td>18.0 m 5 storeys</td><td></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>1H</td><td>18.0 m 5 storeys</td><td></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>1I</td><td>139.0 m 45 storeys</td><td>790 m<sup>2</sup></td><td>12.0 m 3 storeys</td><td>25.0 m 7 storeys</td></tr></table>	Parcel Block	Maximum Building Height	Maximum Tower Floor Plate	Minimum Podium Height	Maximum Podium Height	1A	81.0 m 24 storeys	670 m <sup>2</sup>	7.0 m 3 storeys	18.0 m 5 storeys	1B	81.0 m 24 storeys	670 m <sup>2</sup>	7.0 m 3 storeys	18.0 m 5 storeys	1C	18.0 m 5 storeys		7.0 m 3 storeys	18.0 m 5 storeys	1D	106.0 m 34 storeys	740 m <sup>2</sup>	12.0 m 3 storeys	25.0 m 7 storeys	1E	18.0 m 5 storeys		7.0 m 3 storeys	18.0 m 5 storeys	1F	98.0 m 20 storeys	740 m <sup>2</sup>	12.0 m 3 storeys	25.0 m 7 storeys	1G	18.0 m 5 storeys		7.0 m 3 storeys	18.0 m 5 storeys	1H	18.0 m 5 storeys		7.0 m 3 storeys	18.0 m 5 storeys	1I	139.0 m 45 storeys	790 m <sup>2</sup>	12.0 m 3 storeys	25.0 m 7 storeys		
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4. By-law Number 0225-2007, as amended, is further amended by deleting Sentences 7.2.5.2.1 in Exception Table 7.2.5.2 and substituting the following therefor:

7.2.5.2	Exception: CC4-2	Map # 29	By-law:																									
Regulations																												
7.2.5.2.1	Maximum heights and floor plates of all <b>buildings</b> and <b>structures</b> on Parcel Blocks 2A to 2D as identified on Schedule CC4-2 of this Exception shall conform to the following schedule:																											
	<table><tr><th>Parcel Block</th><th>Maximum Building Height</th><th>Maximum Tower Floor Plate</th><th>Minimum Podium Height</th><th>Maximum Podium Height</th></tr><tr><td>2A</td><td>124.0 m 40 storeys</td><td>790 m<sup>2</sup></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>2B</td><td>46.0 m 12 storeys</td><td>1 000 m<sup>2</sup></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>2C</td><td>25.0 m 7 storeys</td><td></td><td>12.0 m 3 storeys</td><td>25.0 m 7 storeys</td></tr><tr><td>2D</td><td>154.0 m 55 storeys</td><td>929 m<sup>2</sup></td><td>12.0 m 3 storeys</td><td>25.0 m 7 storeys</td></tr></table>	Parcel Block	Maximum Building Height	Maximum Tower Floor Plate	Minimum Podium Height	Maximum Podium Height	2A	124.0 m 40 storeys	790 m <sup>2</sup>	7.0 m 3 storeys	18.0 m 5 storeys	2B	46.0 m 12 storeys	1 000 m <sup>2</sup>	7.0 m 3 storeys	18.0 m 5 storeys	2C	25.0 m 7 storeys		12.0 m 3 storeys	25.0 m 7 storeys	2D	154.0 m 55 storeys	929 m <sup>2</sup>	12.0 m 3 storeys	25.0 m 7 storeys		
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5. By-law Number 0225-2007, as amended, is further amended by deleting Sentences 7.2.5.3.1 in Exception Table 7.2.5.3 and substituting the following therefor:

7.2.5.3	Exception: CC4-3	Map # 29	By-law:																																																					
<b>Regulations</b>																																																								
7.2.5.3.1	Maximum heights and floor plates of all <b>buildings</b> and <b>structures</b> on Parcel Blocks 3A to 3J as identified on Schedule CC4-3 of this Exception shall conform to the following schedule:																																																							
	<table><tr><th>Parcel Block</th><th>Maximum Building Height</th><th>Maximum Tower Floor Plate</th><th>Minimum Podium Height</th><th>Maximum Podium Height</th></tr><tr><td>3A</td><td>70.0 m 22 storeys</td><td>730 m<sup>2</sup></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>3B</td><td>21.0 m 5 storeys</td><td></td><td>7.0 m 3 storeys</td><td>21.0 m 5 storeys</td></tr><tr><td>3C</td><td>70.0 m 22 storeys</td><td>730 m<sup>2</sup></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>3D</td><td>12.0 m 3 storeys</td><td></td><td>7.0 m 3 storeys</td><td>12.0 m 3 storeys</td></tr><tr><td>3E</td><td>109.0 m 36 storeys</td><td>663 m<sup>2</sup></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>3F</td><td>14.0 m 3 storeys</td><td></td><td>7.0 m 3 storeys</td><td>14.0 m 3 storeys</td></tr><tr><td>3G</td><td>109.0 m 36 storeys</td><td>663 m<sup>2</sup></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>3H</td><td>30.0 m 7 storeys</td><td></td><td>12.0 m 3 storeys</td><td>30.0 m 7 storeys</td></tr><tr><td>3I</td><td>136.0 m 45 storeys</td><td>790 m<sup>2</sup></td><td>12.0 m 3 storeys</td><td>30.0 m 7 storeys</td></tr><tr><td>3J</td><td>30.0 m 7 storeys</td><td></td><td>12.0 m 3 storeys</td><td>30.0 m 7 storeys</td></tr></table>	Parcel Block	Maximum Building Height	Maximum Tower Floor Plate	Minimum Podium Height	Maximum Podium Height	3A	70.0 m 22 storeys	730 m <sup>2</sup>	7.0 m 3 storeys	18.0 m 5 storeys	3B	21.0 m 5 storeys		7.0 m 3 storeys	21.0 m 5 storeys	3C	70.0 m 22 storeys	730 m <sup>2</sup>	7.0 m 3 storeys	18.0 m 5 storeys	3D	12.0 m 3 storeys		7.0 m 3 storeys	12.0 m 3 storeys	3E	109.0 m 36 storeys	663 m <sup>2</sup>	7.0 m 3 storeys	18.0 m 5 storeys	3F	14.0 m 3 storeys		7.0 m 3 storeys	14.0 m 3 storeys	3G	109.0 m 36 storeys	663 m <sup>2</sup>	7.0 m 3 storeys	18.0 m 5 storeys	3H	30.0 m 7 storeys		12.0 m 3 storeys	30.0 m 7 storeys	3I	136.0 m 45 storeys	790 m <sup>2</sup>	12.0 m 3 storeys	30.0 m 7 storeys	3J	30.0 m 7 storeys		12.0 m 3 storeys	30.0 m 7 storeys
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6. By-law Number 0225-2007, as amended, is further amended by deleting Sentences 7.2.5.4.1 in Exception Table 7.2.5.4 and substituting the following therefor:

7.2.5.4	Exception: CC4-4	Map # 29	By-law:																																											
Regulations																																														
7.2.5.4.1	Maximum heights and floor plates of all <b>buildings</b> and <b>structures</b> on Parcel Blocks 4A to 4H as identified on Schedule CC4-4 of this Exception shall conform to the following schedule:																																													
	<table><tr><th>Parcel Block</th><th>Maximum Building Height</th><th>Maximum Tower Floor Plate</th><th>Minimum Podium Height</th><th>Maximum Podium Height</th></tr><tr><td>4A</td><td>121.0 m 40 storeys</td><td>740 m<sup>2</sup></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>4B</td><td>18.0 m 5 storeys</td><td></td><td>7.0 m 3 storeys</td><td>18.0 m 5 storeys</td></tr><tr><td>4C</td><td>145.0 m 48 storeys</td><td>838 m<sup>2</sup></td><td>7.0 m 3 storeys</td><td>25.0 m 7 storeys</td></tr><tr><td>4D</td><td>12.0 m 3 storeys</td><td></td><td>7.0 m 3 storeys</td><td>12.0 m 3 storeys</td></tr><tr><td>4E</td><td>12.0 m 3 storeys</td><td></td><td>7.0 m 3 storeys</td><td>12.0 m 3 storeys</td></tr><tr><td>4F</td><td>127.0 m 42 storeys</td><td>810 m<sup>2</sup></td><td>7.0 m 3 storeys</td><td>25.0 m 7 storeys</td></tr><tr><td>4G</td><td>25.0 m 7 storeys</td><td></td><td>7.0 m 3 storeys</td><td>25.0 m 7 storeys</td></tr><tr><td>4H</td><td>156.0 m 48 storeys</td><td>838 m<sup>2</sup></td><td>12.0 m 3 storeys</td><td>25.0 m 7 storeys</td></tr></table>	Parcel Block	Maximum Building Height	Maximum Tower Floor Plate	Minimum Podium Height	Maximum Podium Height	4A	121.0 m 40 storeys	740 m <sup>2</sup>	7.0 m 3 storeys	18.0 m 5 storeys	4B	18.0 m 5 storeys		7.0 m 3 storeys	18.0 m 5 storeys	4C	145.0 m 48 storeys	838 m <sup>2</sup>	7.0 m 3 storeys	25.0 m 7 storeys	4D	12.0 m 3 storeys		7.0 m 3 storeys	12.0 m 3 storeys	4E	12.0 m 3 storeys		7.0 m 3 storeys	12.0 m 3 storeys	4F	127.0 m 42 storeys	810 m <sup>2</sup>	7.0 m 3 storeys	25.0 m 7 storeys	4G	25.0 m 7 storeys		7.0 m 3 storeys	25.0 m 7 storeys	4H	156.0 m 48 storeys	838 m <sup>2</sup>	12.0 m 3 storeys	25.0 m 7 storeys
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7. By-law Number 0225-2007, as amended, is further amended by deleting Sentences 7.2.5.5.2, 7.2.5.5.10, 7.2.5.5.11, and 7.2.5.5.12 in Exception Table 7.2.5.5 and substituting the following therefor:

7.2.5.5	Exception: CC4-5	Map # 29	By-law:									
Regulations												
7.2.5.5.2	Maximum heights and floor plates of all <b>buildings and structures</b> on Parcel Blocks 5A and 5B as identified on Schedule CC4-5 of this Exception shall conform to the following schedule:											
	<table><tr><th>Parcel Block</th><th>Maximum Building Height</th><th>Minimum Podium Height</th></tr><tr><td>5A</td><td>16.0 m 4 storeys</td><td>7.0 m 3 storeys</td></tr><tr><td>5B</td><td>16.0 m 4 storeys</td><td>7.0 m 3 storeys</td></tr></table>	Parcel Block	Maximum Building Height	Minimum Podium Height	5A	16.0 m 4 storeys	7.0 m 3 storeys	5B	16.0 m 4 storeys	7.0 m 3 storeys		
Parcel Block	Maximum Building Height	Minimum Podium Height										
5A	16.0 m 4 storeys	7.0 m 3 storeys										
5B	16.0 m 4 storeys	7.0 m 3 storeys										
7.2.5.5.10	Minimum setback from a <b>horizontal multiple dwelling</b> to the <b>lot lines</b> abutting the lands zoned OS1		4.5 m									
7.2.5.5.11	Notwithstanding Sentence 7.2.5.5.10, an underground <b>parking structure</b> , open staircase, <b>porch, balcony</b> , bay window, canopy, retaining walls, stairs, and air intake may encroach into the required setback											
7.2.5.5.12	Minimum setback from a <b>horizontal multiple dwelling</b> to the <b>lot lines</b> abutting the lands zoned CCOS		3.0 m									
7.2.5.5.13	Notwithstanding Sentence 7.2.5.5.12, an underground <b>parking structure</b> , open staircase, <b>porch, balcony</b> , bay window, canopy, retaining walls, stairs, and air intake may encroach into the required setback											

8. By-law Number 0225-2007, as amended, is further amended by deleting Schedule CC4-1 to Exception Table 7.2.5.1 and substituting the attached Schedule CC4-1 therefor.
9. By-law Number 0225-2007, as amended, is further amended by deleting Schedule CC4-2 to Exception Table 7.2.5.2 and substituting the attached Schedule CC4-2 therefor.
10. By-law Number 0225-2007, as amended, is further amended by deleting Schedule CC4-3 to Exception Table 7.2.5.3 and substituting the attached Schedule CC4-3 therefor.
11. By-law Number 0225-2007, as amended, is further amended by deleting Schedule CC4-4 to Exception Table 7.2.5.4 and substituting the attached Schedule CC4-4 therefor.
12. By-law Number 0225-2007, as amended, is further amended by deleting Schedule CC4-5 to Exception Table 7.2.5.5 and substituting the attached Schedule CC4-5 therefor.
13. Map Number 29 of Schedule "B" to By-law Number 0225-2007, as amended, being a

City of Mississauga Zoning By-law, is amended by changing thereon from "H-CCOS" to "H-CCOS" and "H-CC4-5", the zoning of Block 1, Plan 43M-1808, in the City of Mississauga, PROVIDED HOWEVER THAT the "H-CCOS" and "H-CC4-5" zoning shall only apply to the lands which are shown on the attached Schedule "A", which is deemed to be an integral part of this By-law, outlined in the heaviest broken line with the "H-CCOS" and "H-CC4-5" zoning indicated thereon.

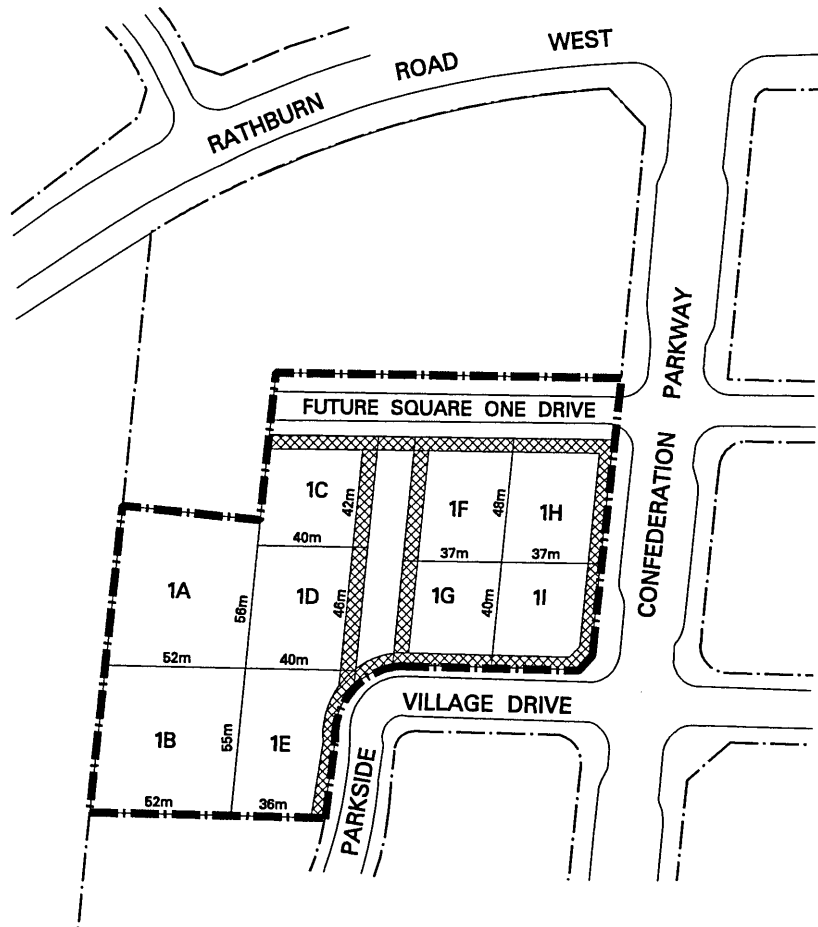
14. This By-law shall not come into force until Mississauga Plan (Official Plan) Amendment Number 128 is in full force and effect.

ENACTED and PASSED this \_\_\_\_\_ day of \_\_\_\_\_ 2012.

\_\_\_\_\_  
MAYOR

\_\_\_\_\_  
CLERK

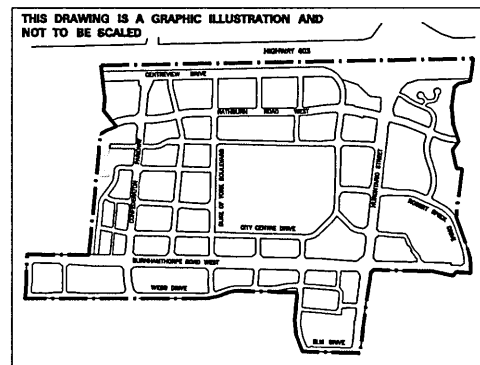
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**REQUIREMENTS:**

 BUILD-TO AREA  
[0 - 4.5 FROM STREETLINE]

**DRAFT****This is not a Plan of Survey.**

Note:  
All measurements are in metres  
and are minimum setbacks,  
unless otherwise noted.

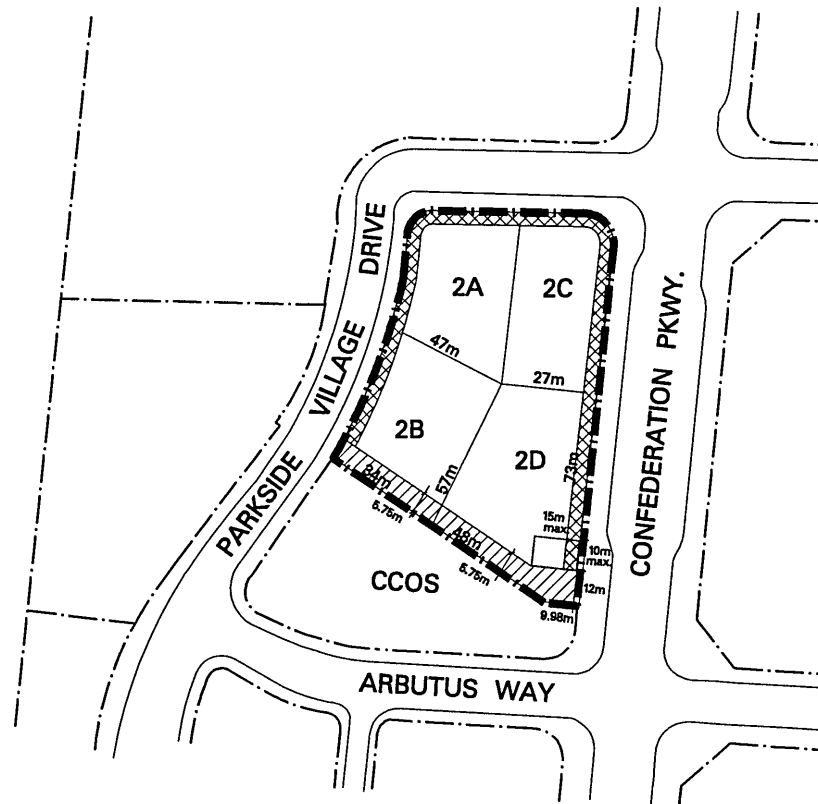





**SCHEDULE CC4-1  
MAP 29**

**THIS IS SCHEDULE " CC4-1 "****AS ATTACHED TO BY-LAW \_\_\_\_\_****PASSED BY COUNCIL ON \_\_\_\_\_**



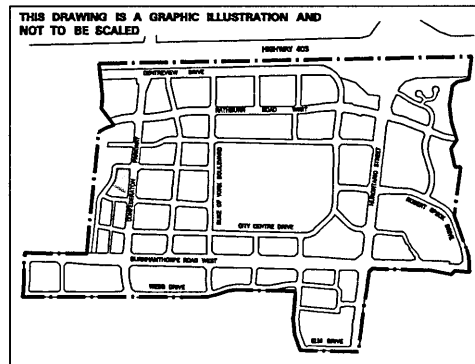
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**REQUIREMENTS:**

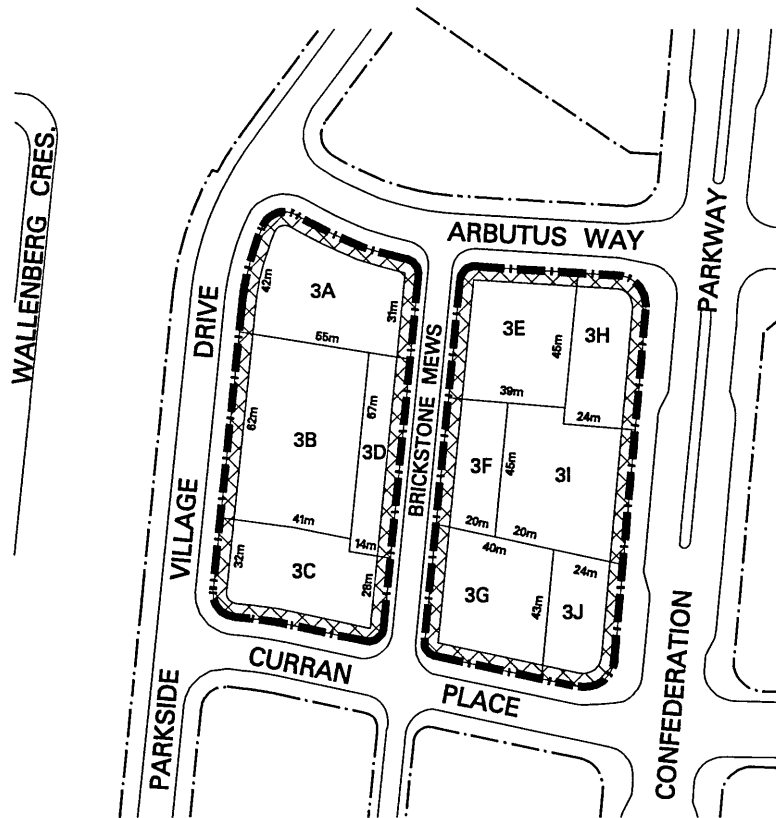
-  BUILD-TO AREA  
[0 - 4.5 FROM STREETLINE]
-  BUILDING SETBACK  
TO LANDS ZONED "CCOS"
-  SHADED AREA


**DRAFT****This is not a Plan of Survey.**

**Note:**  
All measurements are in metres  
and are minimum setbacks,  
unless otherwise noted.

**SCHEDULE CC4-2  
MAP 29****THIS IS SCHEDULE " CC4-2 "****AS ATTACHED TO BY-LAW \_\_\_\_\_****PASSED BY COUNCIL ON \_\_\_\_\_**

t:\cad\Projects\ReportMap\CD\_21\_AMA\_RPT\Vector\Framed cc's\CC4-3\_new.dgn

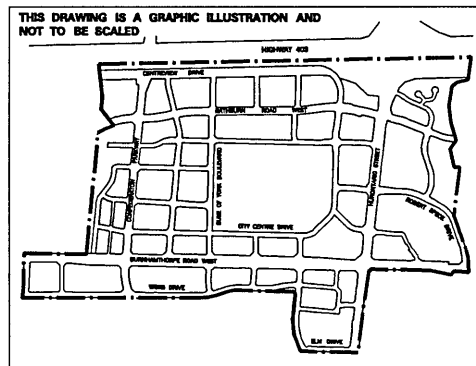
**REQUIREMENTS:**

 BUILD-TO AREA  
[0 - 4.5 FROM STREETLINE]

DRAFT

This is not a Plan of Survey.

Note:  
All measurements are in metres  
and are minimum setbacks,  
unless otherwise noted.

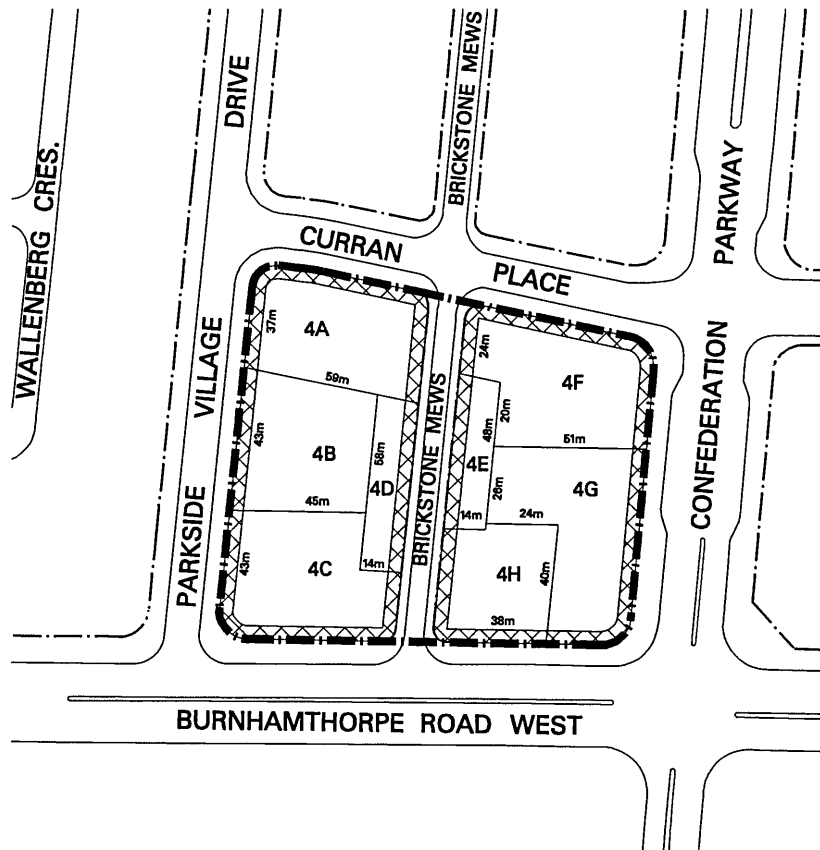
SCHEDULE CC4-3  
MAP 29

THIS IS SCHEDULE " CC4-3 "

AS ATTACHED TO BY-LAW \_\_\_\_\_

PASSED BY COUNCIL ON \_\_\_\_\_

design file



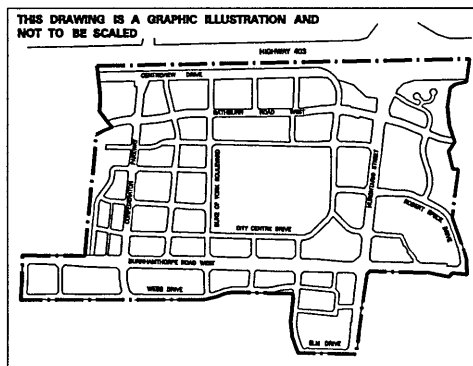
## REQUIREMENTS:

 BUILD-TO AREA  
[0 - 4.5 FROM STREETLINE]

DRAFT

This is not a Plan of Survey.

Note:  
All measurements are in metres  
and are minimum setbacks,  
unless otherwise noted.



SCHEDULE CC4-4  
MAP 29

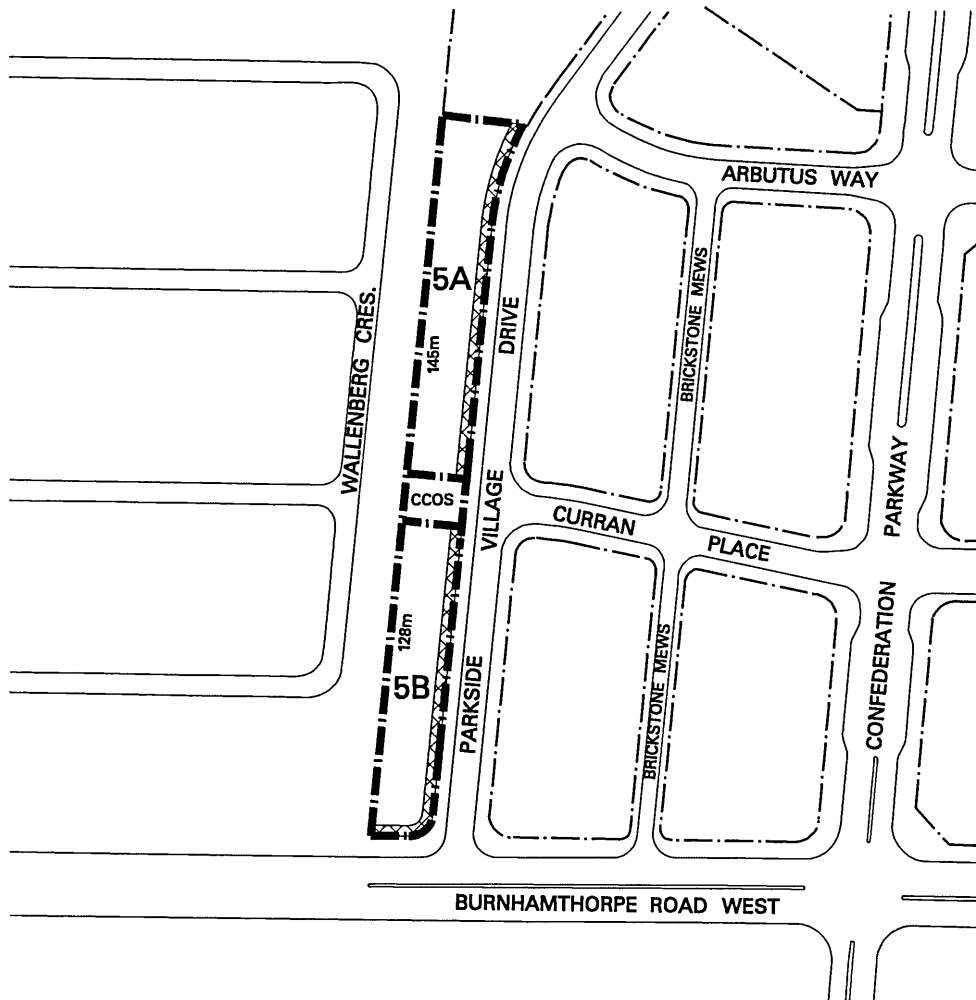


THIS IS SCHEDULE " CC4-4 "

AS ATTACHED TO BY-LAW \_\_\_\_\_

PASSED BY COUNCIL ON \_\_\_\_\_

I:\cadd\Projects\ReportMap\122128\_CD\_21\_AMA\_RPT\Vector\Framed\_cc's\CC4-5\_new.dgn

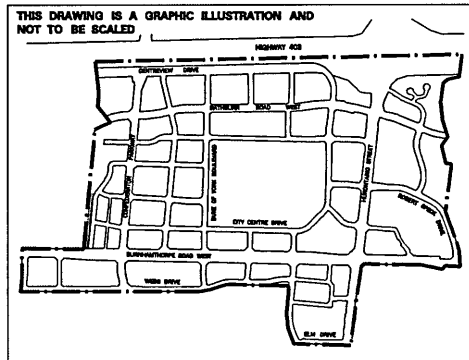
**REQUIREMENTS:**

BUILD-TO AREA  
[0 - 4.5 FROM STREETLINE]

**DRAFT**

This is not a Plan of Survey.

Note:  
All measurements are in metres  
and are minimum setbacks,  
unless otherwise noted.



**SCHEDULE CC4-5  
MAP 29**

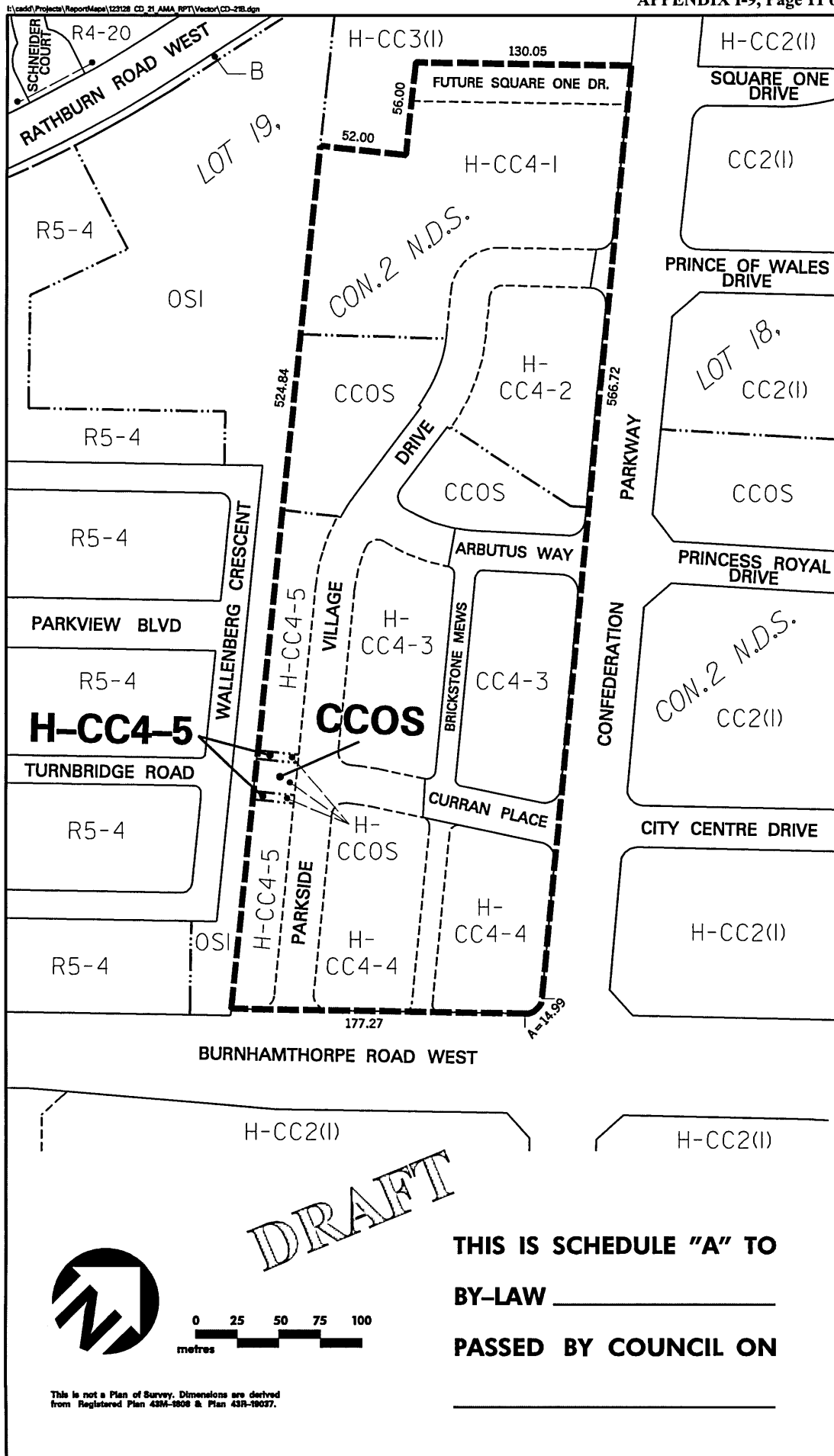


**THIS IS SCHEDULE " CC4-5 "**

**AS ATTACHED TO BY-LAW \_\_\_\_\_**

**PASSED BY COUNCIL ON \_\_\_\_\_**





**APPENDIX "A" TO BY-LAW NUMBER \_\_\_\_\_**Explanation of the Purpose and Effect of the By-law

This By-law amends the zoning provisions of the "CC4-1", "CC4-2", "CC4-3", "CC4-4" and "CC4-5" zones to redistribute the density, tower heights and placement, resulting from the addition of a new road (Square One Drive) through the north part of the subdivision, and amends the limits of the "CCOS" zone.

"CC4-1", as amended, permits five towers ranging in height from 20 to 45 storeys, versus seven towers ranging in height from 18 to 45 storeys previously approved, with revised tower placement and a mix of residential, office, and retail uses.

"CC4-2", as amended, permits three towers ranging in height from 12 to 55 storeys, versus three towers ranging in height from 12 to 45 storeys previously approved, and a mix of residential, office, and retail uses.

"CC4-3", as amended, permits five towers ranging in height from 22 to 45 storeys, versus four towers ranging in height from 13 to 40 storeys previously approved, with revised tower placement and a mix of residential, office, and retail uses.

"CC4-4", as amended, permits four towers ranging in height from 40 to 48 storeys, versus three towers ranging in height from 18 to 45 storeys previously approved, with revised tower placement and a mix of residential, office, and retail uses.

"CC4-5", as amended, permits horizontal multiple dwellings at a maximum height of 4 storeys, versus horizontal multiple dwellings at a maximum height of 5 storeys as previously approved, with minor expansion to the zone limits.

"CCOS", as amended, permits active and passive recreational uses and below grade parking structures. The amended limits of Block 4 provides an open space block 20 metres (65.6 feet) in frontage along Parkside Village Drive, versus 30 metres (98.4 feet) previously approved.

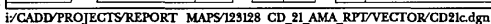
Location of Lands Affected

North side of Burnhamthorpe Road West, west of Confederation Parkway, in the City of Mississauga.

Further information regarding this By-law may be obtained from Jonathan Famme of the City Planning and Building Department at 905-615-3200 ext. 4229.

K:\PLAN\DEV\CONTL\GROUP\WPDATA\BYLAWS\CD.21.AMA Zoning Bylaw Amendments Amacon.jf.doc

DRAFT





**Amacon Development (City Centre) Corp.**

**File: CD.21.AMA**

**Recommendation PDC-0061-2012**

PDC-0061-2012

- "1. That the Report dated September 25, 2012, from the Commissioner of Planning and Building regarding the amendment of the Official Plan from "Public Open Space" to "Mixed Use" and "Public Open Space", as amended, and to add a new Minor Collector road (Square One Drive), and to change the Zoning from "H-CCOS" (City Centre – Open Space) to "H-CC4-5" (City Centre – Mixed Use) and "H-CCOS" (City Centre – Open Space), as amended, and change the CC4 (City Centre – Mixed Use) zoning provisions in accordance with Appendix I-9, to permit revisions to the Parkside Village Master Plan to redeploy density resulting from the addition of Square One Drive under file CD.21.AMA W4, Amacon Development (City Centre) Corp., Part of Lot 19, Concession 2, N.D.S., be received for information.
2. That the emails and attachments dated October 10, 2012 from Susan Howard, Brookfield Residential Services Ltd. including a letter dated October 10, 2012 from Laura Raynor, President Peel Condominium Corporation No. 417 be received."



#### LAND USE DESIGNATIONS

	Residential Low Density I		Business Employment
	Residential Low Density II		Industrial
	Residential Medium Density		Airport
	Residential High Density		Institutional
	Downtown Mixed Use		Public Open Space
	Downtown Core Commercial		Private Open Space
	Mixed Use		Greenbelt
	Convenience Commercial		Parkway Belt West
	Motor Vehicle Commercial		Utility
	Office		To Be Determined

#### LAND USE LEGEND

	Heritage Conservation District		Civic Centre (City Hall)
	1998 NEP/2000 NEF Composite Noise Contours		City Centre Transit Terminal
	LSIPA Operating Area Boundary See Aircraft Noise Policies		GO Rail Transit Station
	Area Exempt from LSIPA Operating Area		Public School
	Natural Hazards		Catholic School
			Hospital
			Community Facilities

#### CITY STRUCTURE

##### Elements

	Downtown		Corporate Centre
	Major Node		Employment Area
	Community Node		Special Purpose Area
	Neighbourhood		
	ND2 Region of Peel Non-Decision		

AREA OF AMENDMENT NO. 18

FROM:

PUBLIC OPEN SPACE

TO:

DOWNTOWN MIXED USE

AREA OF ROAD ADDITION

**DRAFT**



**SCHEDULE 'A'**

Part of  
Schedule 10  
Land Use Designations  
of Mississauga Official Plan  
City of Mississauga

A by-law to amend By-law Number 0225-2007, as amended.

WHEREAS pursuant to section 34 of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, the council of a local municipality may pass a zoning by-law;

NOW THEREFORE the Council of The Corporation of the City of Mississauga ENACTS as follows:

1. By-law Number 0225-2007, as amended, being a City of Mississauga Zoning By-law, is amended by deleting Table 7.1.5.2 contained in Article 7.1.5.2 and substituting the following therefor:

**Table 7.1.5.2 - Underground Parking Structures**

Column	A	B	C
Line	Street Width	Maximum Encroachment	Minimum Vertical Depth
1.0	12.5 m	6.25 m on each side of the street	0.5 m

2. By-law Number 0225-2007, as amended, is further amended by deleting Sentence 7.2.5.1.2 and substituting the following therefor and adding Sentences 7.2.5.1.17 and 7.2.5.1.18 to Exception Table 7.2.5.1 as follows:

7.2.5.1	Exception: CC4-1	Map # 29	By-law: 0105-2013																																																		
<b>Regulations</b>																																																					
7.2.5.1.2	Maximum heights and floor plates of all <b>buildings</b> and <b>structures</b> on Parcel Blocks 1A to 1I as identified on Schedule CC4-1 of this Exception shall conform to the following schedule:																																																				
	<table><tr><th>Parcel Block</th><th>Maximum Building Height</th><th>Maximum Tower Floor Plate</th><th>Minimum Podium Height</th><th>Maximum Podium Height</th></tr><tr><td>1A</td><td>81.0 m and 24 storeys</td><td>670 m<sup>2</sup></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>1B</td><td>81.0 m and 24 storeys</td><td>670 m<sup>2</sup></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>1C</td><td>18.0 m and 5 storeys</td><td></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>1D</td><td>106.0 m and 34 storeys</td><td>740 m<sup>2</sup></td><td>12.0 m and 3 storeys</td><td>25.0 m and 7 storeys</td></tr><tr><td>1E</td><td>18.0 m and 5 storeys</td><td></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>1F</td><td>98.0 m and 20 storeys</td><td>740 m<sup>2</sup></td><td>12.0 m and 3 storeys</td><td>25.0 m and 7 storeys</td></tr><tr><td>1G</td><td>18.0 m and 5 storeys</td><td></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>1H</td><td>18.0 m and 5 storeys</td><td></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>1I</td><td>139.0 m and 45 storeys</td><td>790 m<sup>2</sup></td><td>12.0 m and 3 storeys</td><td>25.0 m and 7 storeys</td></tr></table>	Parcel Block	Maximum Building Height	Maximum Tower Floor Plate	Minimum Podium Height	Maximum Podium Height	1A	81.0 m and 24 storeys	670 m <sup>2</sup>	7.0 m and 3 storeys	18.0 m and 5 storeys	1B	81.0 m and 24 storeys	670 m <sup>2</sup>	7.0 m and 3 storeys	18.0 m and 5 storeys	1C	18.0 m and 5 storeys		7.0 m and 3 storeys	18.0 m and 5 storeys	1D	106.0 m and 34 storeys	740 m <sup>2</sup>	12.0 m and 3 storeys	25.0 m and 7 storeys	1E	18.0 m and 5 storeys		7.0 m and 3 storeys	18.0 m and 5 storeys	1F	98.0 m and 20 storeys	740 m <sup>2</sup>	12.0 m and 3 storeys	25.0 m and 7 storeys	1G	18.0 m and 5 storeys		7.0 m and 3 storeys	18.0 m and 5 storeys	1H	18.0 m and 5 storeys		7.0 m and 3 storeys	18.0 m and 5 storeys	1I	139.0 m and 45 storeys	790 m <sup>2</sup>	12.0 m and 3 storeys	25.0 m and 7 storeys		
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7.2.5.1.18	For the purposes of this By-law, Parcel Blocks 1F to 1I inclusive identified on Schedule CC4-1 of this Exception shall be considered one (1) property																																																				



3. By-law Number 0225-2007, as amended, is further amended by deleting Sentence 7.2.5.2.1 and substituting the following therefor and adding Sentence 7.2.5.2.22 to Exception Table 7.2.5.2. as follows:

7.2.5.2	Exception: CC4-2	Map # 29	By-law:																									
<b>Regulations</b>																												
7.2.5.2.1	Maximum heights and floor plates of all <b>buildings and structures</b> on Parcel Blocks 2A to 2D as identified on Schedule CC4-2 of this Exception shall conform to the following schedule:																											
	<table><tr><th>Parcel Block</th><th>Maximum Building Height</th><th>Maximum Tower Floor Plate</th><th>Minimum Podium Height</th><th>Maximum Podium Height</th></tr><tr><td>2A</td><td>124.0 m and 40 storeys</td><td>790 m<sup>2</sup></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>2B</td><td>46.0 m and 12 storeys</td><td>1 000 m<sup>2</sup></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>2C</td><td>25.0 m and 7 storeys</td><td></td><td>12.0 m and 3 storeys</td><td>25.0 m and 7 storeys</td></tr><tr><td>2D</td><td>154.0 m and 55 storeys</td><td>929 m<sup>2</sup></td><td>12.0 m and 3 storeys</td><td>25.0 m and 7 storeys</td></tr></table>	Parcel Block	Maximum Building Height	Maximum Tower Floor Plate	Minimum Podium Height	Maximum Podium Height	2A	124.0 m and 40 storeys	790 m <sup>2</sup>	7.0 m and 3 storeys	18.0 m and 5 storeys	2B	46.0 m and 12 storeys	1 000 m <sup>2</sup>	7.0 m and 3 storeys	18.0 m and 5 storeys	2C	25.0 m and 7 storeys		12.0 m and 3 storeys	25.0 m and 7 storeys	2D	154.0 m and 55 storeys	929 m <sup>2</sup>	12.0 m and 3 storeys	25.0 m and 7 storeys		
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7.2.5.2.22	For the purposes of this By-law, Parcel Blocks 2A to 2D inclusive identified on Schedule CC4-2 of this Exception shall be considered one (1) property																											

4. By-law Number 0225-2007, as amended, is further amended by deleting Sentence 7.2.5.3.1 and substituting the following therefor and adding Sentences 7.2.5.3.18 and 7.2.5.3.19 to Exception Table 7.2.5.3 as follows:

7.2.5.3	Exception: CC4-3	Map # 29	By-law: 0089-2010, 0105-2013																																																							
<b>Regulations</b>																																																										
7.2.5.3.1	Maximum heights and floor plates of all <b>buildings</b> and <b>structures</b> on Parcel Blocks 3A to 3J as identified on Schedule CC4-3 of this Exception shall conform to the following schedule:																																																									
	<table><tr><th>Parcel Block</th><th>Maximum Building Height</th><th>Maximum Tower Floor Plate</th><th>Minimum Podium Height</th><th>Maximum Podium Height</th></tr><tr><td>3A</td><td>70.0 m and 22 storeys</td><td>730 m<sup>2</sup></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>3B</td><td>21.0 m and 5 storeys</td><td></td><td>7.0 m and 3 storeys</td><td>21.0 m and 5 storeys</td></tr><tr><td>3C</td><td>70.0 m and 22 storeys</td><td>730 m<sup>2</sup></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>3D</td><td>12.0 m and 3 storeys</td><td></td><td>7.0 m and 3 storeys</td><td>12.0 m and 3 storeys</td></tr><tr><td>3E</td><td>109.0 m and 36 storeys</td><td>663 m<sup>2</sup></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>3F</td><td>14.0 m and 3 storeys</td><td></td><td>7.0 m and 3 storeys</td><td>14.0 m and 3 storeys</td></tr><tr><td>3G</td><td>109.0 m and 36 storeys</td><td>663 m<sup>2</sup></td><td>7.0 m and 3 storeys</td><td>18.0 m and 5 storeys</td></tr><tr><td>3H</td><td>30.0 m and 7 storeys</td><td></td><td>12.0 m and 3 storeys</td><td>30.0 m and 7 storeys</td></tr><tr><td>3I</td><td>136.0 m and 45 storeys</td><td>790 m<sup>2</sup></td><td>12.0 m and 3 storeys</td><td>30.0 m and 7 storeys</td></tr><tr><td>3J</td><td>30.0 m and 7 storeys</td><td></td><td>12.0 m and 3 storeys</td><td>30.0 m and 7 storeys</td></tr></table>	Parcel Block	Maximum Building Height	Maximum Tower Floor Plate	Minimum Podium Height	Maximum Podium Height	3A	70.0 m and 22 storeys	730 m <sup>2</sup>	7.0 m and 3 storeys	18.0 m and 5 storeys	3B	21.0 m and 5 storeys		7.0 m and 3 storeys	21.0 m and 5 storeys	3C	70.0 m and 22 storeys	730 m <sup>2</sup>	7.0 m and 3 storeys	18.0 m and 5 storeys	3D	12.0 m and 3 storeys		7.0 m and 3 storeys	12.0 m and 3 storeys	3E	109.0 m and 36 storeys	663 m <sup>2</sup>	7.0 m and 3 storeys	18.0 m and 5 storeys	3F	14.0 m and 3 storeys		7.0 m and 3 storeys	14.0 m and 3 storeys	3G	109.0 m and 36 storeys	663 m <sup>2</sup>	7.0 m and 3 storeys	18.0 m and 5 storeys	3H	30.0 m and 7 storeys		12.0 m and 3 storeys	30.0 m and 7 storeys	3I	136.0 m and 45 storeys	790 m <sup>2</sup>	12.0 m and 3 storeys	30.0 m and 7 storeys	3J	30.0 m and 7 storeys		12.0 m and 3 storeys	30.0 m and 7 storeys		
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7.2.5.3.19	For the purposes of this By-law, Parcel Blocks 3E to 3J inclusive identified on Schedule CC4-3 of this Exception shall be considered one (1) property																																																									

5. By-law Number 0225-2007, as amended, is further amended by deleting Sentence 7.2.5.4.1 and substituting the following therefor and adding Sentences 7.2.5.4.10 to 7.2.5.4.14 to Exception Table 7.2.5.4 as follows:

7.2.5.4	Exception: CC4-4	Map # 29	By-law: 0105-2013																																													
<b>Regulations</b>																																																
7.2.5.4.1	Maximum heights and floor plates of all <b>buildings and structures</b> on Parcel Blocks 4A to 4H as identified on Schedule CC4-4 of this Exception shall conform to the following schedule:																																															
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7.2.5.4.10	For the purposes of this By-law, Parcel Blocks 4A to 4D inclusive identified on Schedule CC4-4 of this Exception shall be considered one (1) property																																															
7.2.5.4.11	For the purposes of this By-law, Parcel Blocks 4E to 4H inclusive identified on Schedule CC4-4 of this Exception shall be considered one (1) property																																															
7.2.5.4.12	That outdoor display of seasonal products be permitted within Parcel Blocks 4F to 4G inclusive identified on Schedule CC4-4																																															
7.2.5.4.13	Minimum setback from the exterior face of a podium of <b>buildings and structures</b> , or parts thereof, located above the podium structure for Parcel Blocks 4E to 4H inclusive identified on Schedule CC4-4	0.0 m																																														
7.2.5.4.14	Maximum encroachment of portions of a <b>building or structure</b> into the required setback for Parcel Blocks 4E to 4H inclusive identified on Schedule CC4-4	3.0 m																																														



6. By-law Number 0225-2007, as amended, is further amended by deleting Sentences 7.2.5.5.2, 7.2.5.5.10, 7.2.5.5.11, and 7.2.5.5.12 and substituting the following therefor and adding Sentences 7.2.5.5.13 and 7.2.5.5.14 to Exception Table 7.2.5.5 as follows:

7.2.5.5	Exception: CC4-5	Map # 29	By-law: 0105-2013									
<b>Regulations</b>												
7.2.5.5.2	Maximum heights and floor plates of all <b>buildings</b> and <b>structures</b> on Parcel Blocks 5A and 5B as identified on Schedule CC4-5 of this Exception shall conform to the following schedule:											
	<table><tr><th>Parcel Block</th><th>Maximum Building Height</th><th>Minimum Podium Height</th></tr><tr><td>5A</td><td>16.0 m and 4 storeys</td><td>7.0 m and 3 storeys</td></tr><tr><td>5B</td><td>16.0 m and 4 storeys</td><td>7.0 m and 3 storeys</td></tr></table>	Parcel Block	Maximum Building Height	Minimum Podium Height	5A	16.0 m and 4 storeys	7.0 m and 3 storeys	5B	16.0 m and 4 storeys	7.0 m and 3 storeys		
Parcel Block	Maximum Building Height	Minimum Podium Height										
5A	16.0 m and 4 storeys	7.0 m and 3 storeys										
5B	16.0 m and 4 storeys	7.0 m and 3 storeys										
7.2.5.5.10	Minimum setback from a <b>dwelling unit</b> to the <b>lot lines</b> abutting the lands zoned OS1	4.5 m										
7.2.5.5.11	Notwithstanding Sentence 7.2.5.5.10, required setback of an underground <b>parking structure</b> , parking stairwell and enclosure, and retaining walls	0.0 m										
7.2.5.5.12	Minimum setback from a <b>dwelling unit</b> to the <b>lot lines</b> abutting the lands zoned CCOS	3.0 m										
7.2.5.5.13	Notwithstanding Sentence 7.2.5.5.12, required setback of an underground <b>parking structure</b> , parking stairwell and enclosure, and retaining walls	0.0 m										
7.2.5.5.14	Minimum number of loading spaces	0										

7. By-law Number 0225-2007, as amended, is further amended by deleting Schedule CC4-1 to Exception Table 7.2.5.1 and substituting the attached Schedule CC4-1 therefor.
8. By-law Number 0225-2007, as amended, is further amended by deleting Schedule CC4-2 to Exception Table 7.2.5.2 and substituting the attached Schedule CC4-2 therefor.
9. By-law Number 0225-2007, as amended, is further amended by deleting Schedule CC4-3 to Exception Table 7.2.5.3 and substituting the attached Schedule CC4-3 therefor.
10. By-law Number 0225-2007, as amended, is further amended by deleting Schedule CC4-4 to Exception Table 7.2.5.4 and substituting the attached Schedule CC4-4 therefor.
11. By-law Number 0225-2007, as amended, is further amended by deleting Schedule CC4-5 to Exception Table 7.2.5.5 and substituting the attached Schedule CC4-5 therefor.

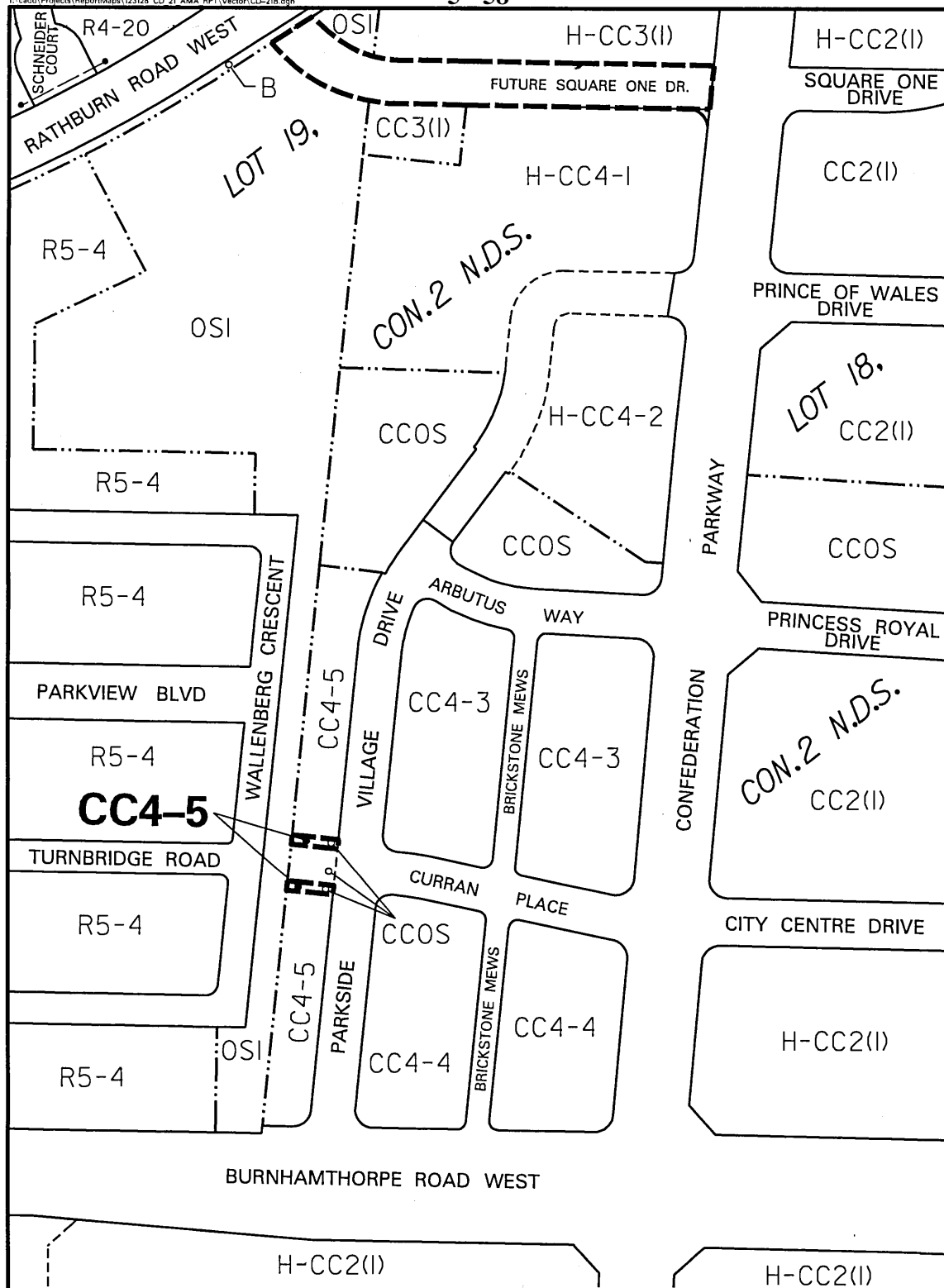


12. Map Number 29 of Schedule "B" to By-law Number 0225-2007, as amended, being a City of Mississauga Zoning By-law, is amended by changing thereon from "CCOS" to "CC4-5" the zoning of Block 1, Plan 43M-1808, in the City of Mississauga, and from "H-CC4-1", "H-CC3(1) and "OS1" to road allowance, the zoning of Part of Lot 19, Concession 2, North of Dundas Street, in the City of Mississauga, PROVIDED HOWEVER THAT the "CC4-5" zoning shall only apply to the lands which are shown on the attached Schedule "A", which is deemed to be an integral part of this By-law, outlined in the heaviest broken line with the "CC4-5" zoning indicated thereon.
  
13. This By-law shall not come into force until Mississauga Official Plan Amendment Number 18 is in full force and effect.

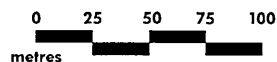
ENACTED and PASSED this \_\_\_\_\_ day of \_\_\_\_\_ 2014.

\_\_\_\_\_  
MAYOR

\_\_\_\_\_  
CLERK

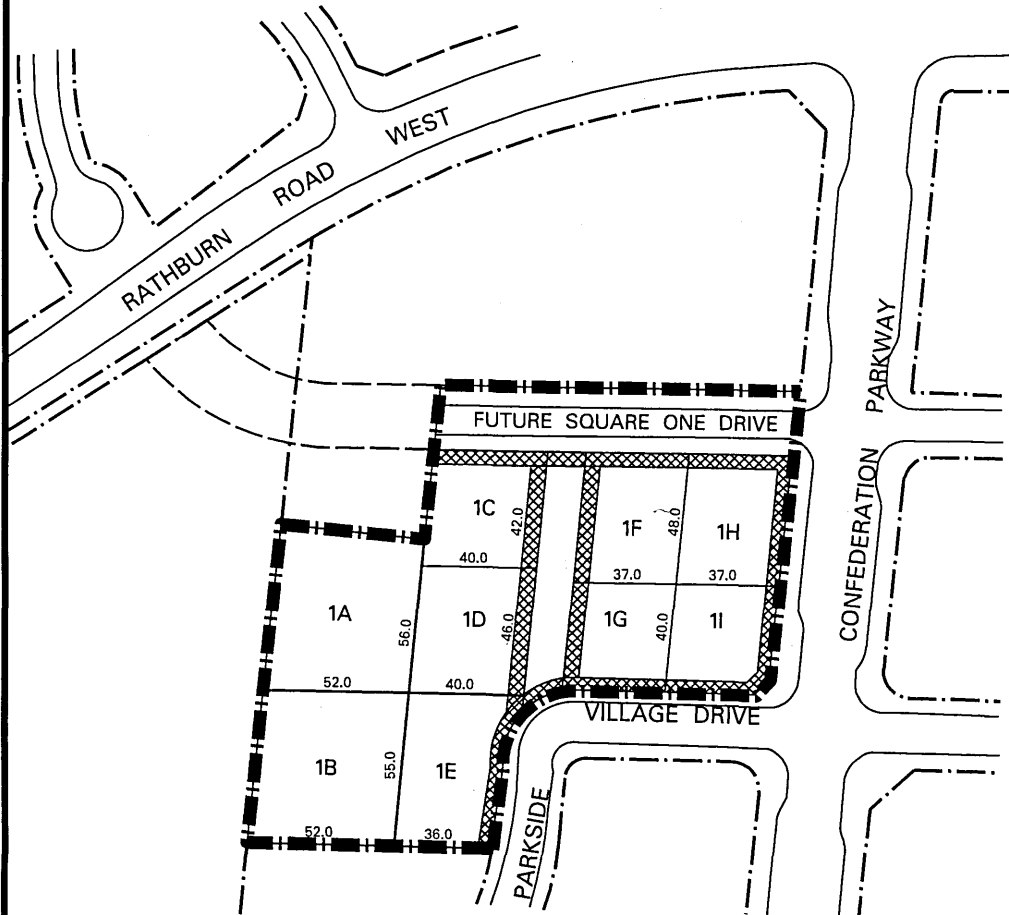


**DRAFT**




This is not a Plan of Survey. Dimensions are derived from Registered Plan 43M-1808 & Plan 43R-19037.

**THIS IS SCHEDULE "A" TO  
BY-LAW \_\_\_\_\_  
PASSED BY COUNCIL ON  
\_\_\_\_\_**

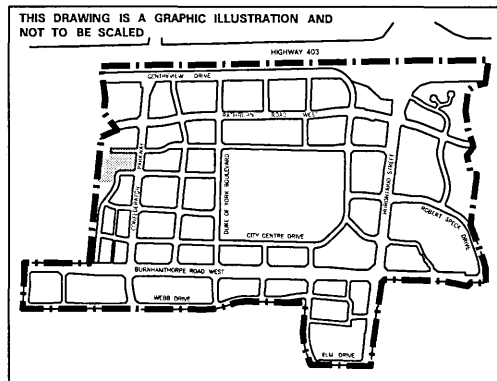


**REQUIREMENTS:**

 BUILD-TO AREA  
[0 - 4.5 FROM STREETLINE]

**This is not a Plan of Survey.**

Note:  
All measurements are in metres  
and are minimum setbacks,  
unless otherwise noted.



**SCHEDULE CC4-1  
MAP 29**

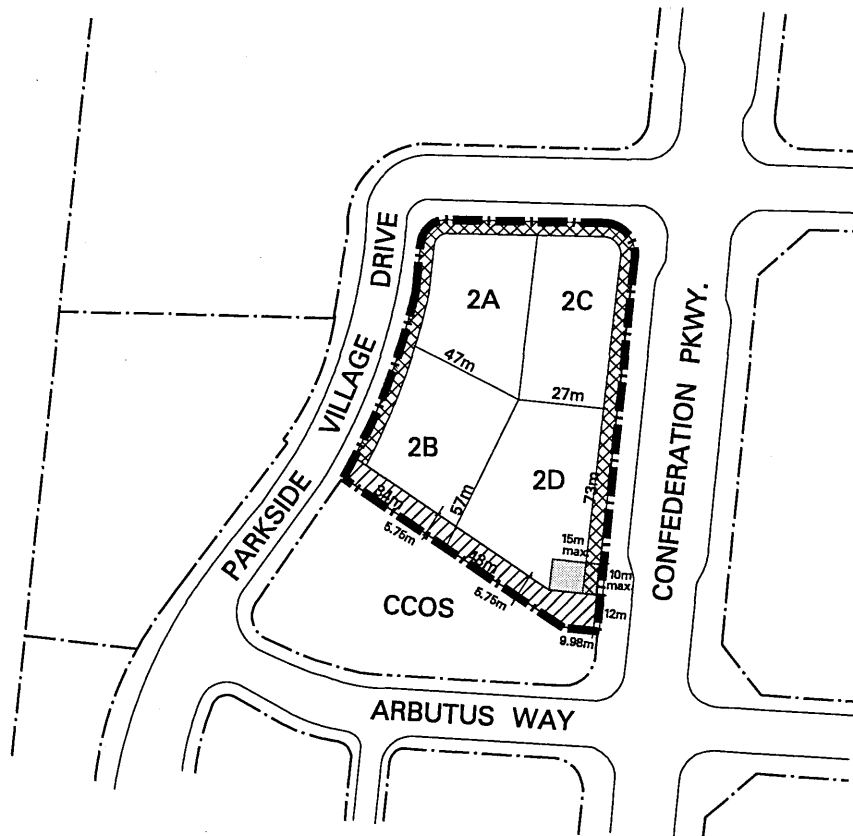
**DRAFT**






**THIS IS SCHEDULE " CC4-1 "**

**AS ATTACHED TO BY-LAW \_\_\_\_\_**

**PASSED BY COUNCIL ON \_\_\_\_\_**



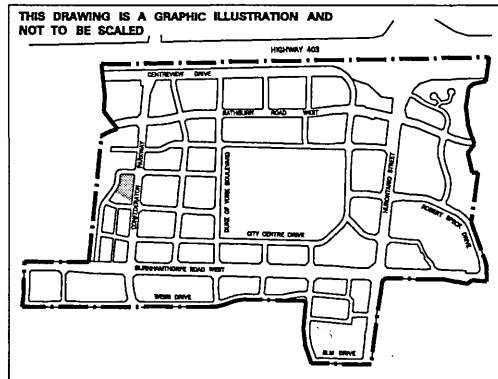
**REQUIREMENTS:**

-  BUILD-TO AREA  
[0 - 4.5 FROM STREETLINE]
-  BUILDING SETBACK  
TO LANDS ZONED "CCOS"
-  SHADED AREA

**DRAFT**

This is not a Plan of Survey.

Note:  
All measurements are in metres  
and are minimum setbacks,  
unless otherwise noted.



**SCHEDULE CC4-2  
MAP 29**

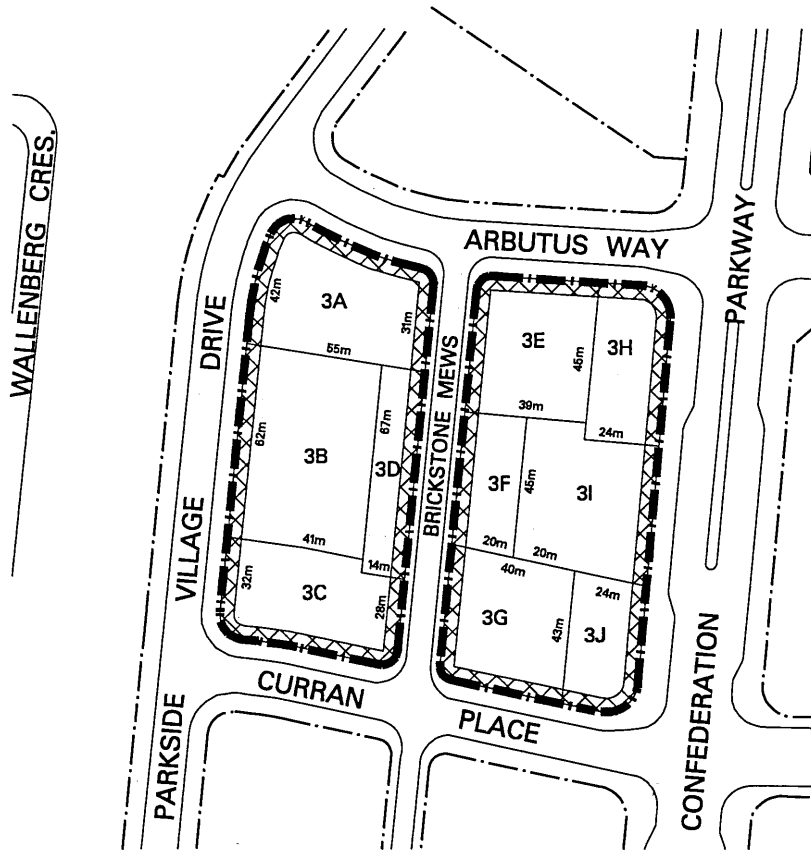


**THIS IS SCHEDULE " CC4-2 "**


**AS ATTACHED TO BY-LAW \_\_\_\_\_**

**PASSED BY COUNCIL ON \_\_\_\_\_**





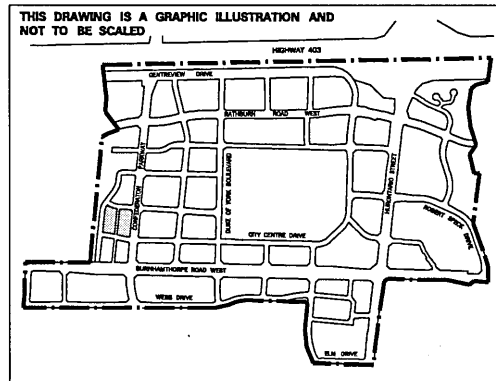
**REQUIREMENTS:**

 BUILD-TO AREA  
[0 - 4.5 FROM STREETLINE]

**DRAFT**

This is not a Plan of Survey.

Note:  
All measurements are in metres  
and are minimum setbacks,  
unless otherwise noted.



**SCHEDULE CC4-3  
MAP 29**



**THIS IS SCHEDULE " CC4-3 "**

**AS ATTACHED TO BY-LAW \_\_\_\_\_**

**PASSED BY COUNCIL ON \_\_\_\_\_**



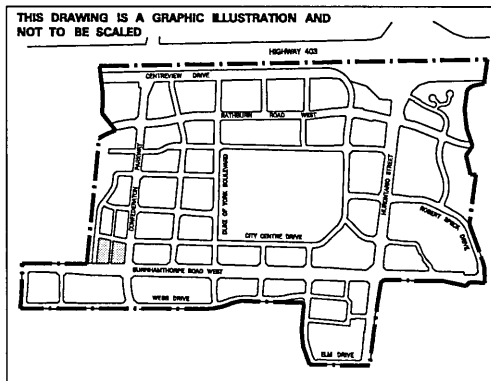
**REQUIREMENTS:**

 BUILD-TO AREA  
[0 - 4.5 FROM STREETLINE]

**DRAFT**

**This is not a Plan of Survey.**

Note:  
All measurements are in metres  
and are minimum setbacks,  
unless otherwise noted.



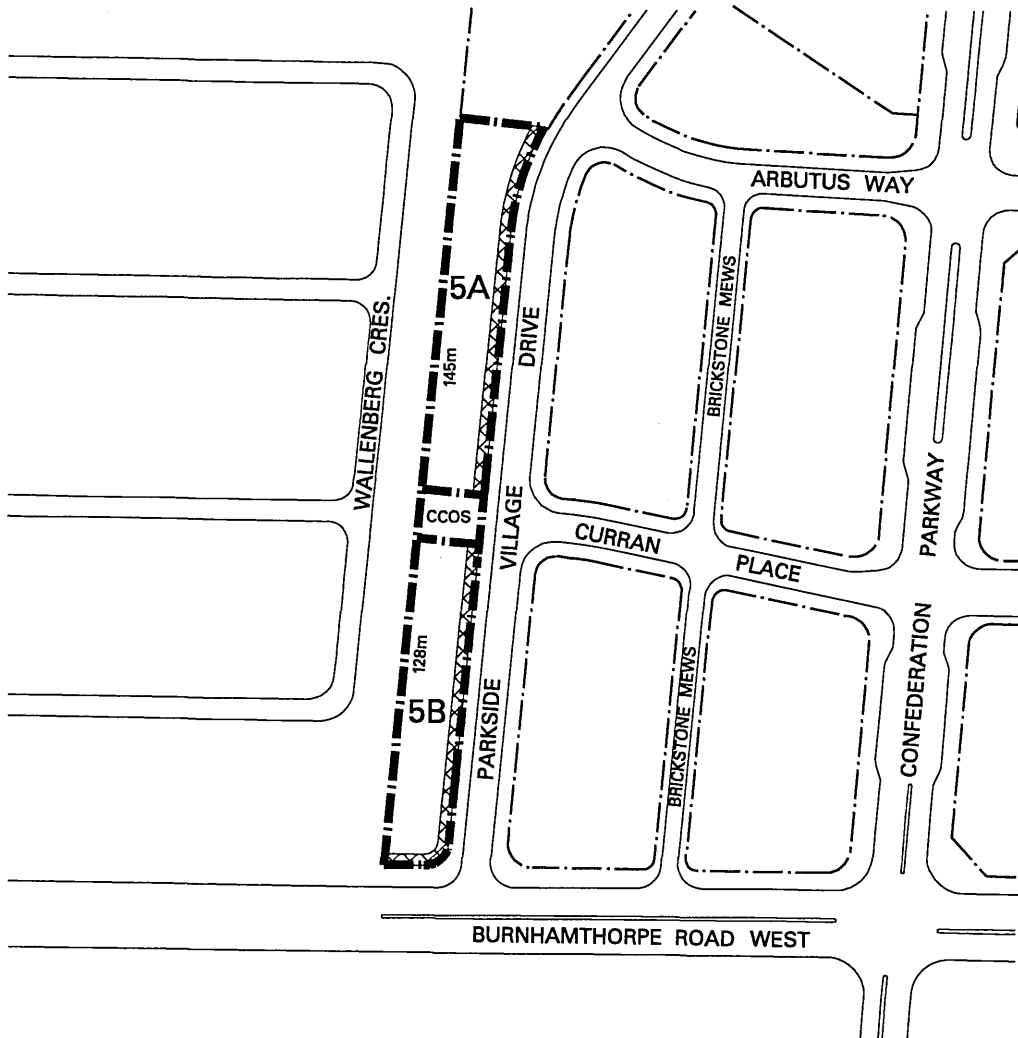
**SCHEDULE CC4-4  
MAP 29**



**THIS IS SCHEDULE " CC4-4 "**

**AS ATTACHED TO BY-LAW \_\_\_\_\_**

**PASSED BY COUNCIL ON \_\_\_\_\_**



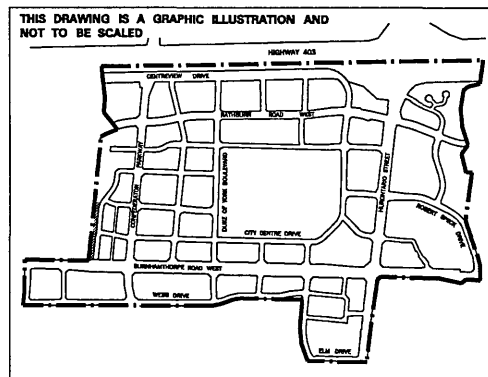
**REQUIREMENTS:**

 **BUILD-TO AREA**  
[0 - 4.5 FROM STREETLINE]

**DRAFT**

**This is not a Plan of Survey.**

**Note:**  
All measurements are in metres  
and are minimum setbacks,  
unless otherwise noted.



**SCHEDULE CC4-5  
MAP 29**



**THIS IS SCHEDULE " CC4-5 "**

**AS ATTACHED TO BY-LAW \_\_\_\_\_**

**PASSED BY COUNCIL ON \_\_\_\_\_**

## APPENDIX "A" TO BY-LAW NUMBER \_\_\_\_\_

### Explanation of the Purpose and Effect of the By-law

This By-law amends the zoning provisions of the "CC4-1", "CC4-2", "CC4-3", "CC4-4" and "CC4-5" zones to redistribute the density, tower heights and placement, resulting from the addition of a new road (Square One Drive) through the north part of the subdivision, and amends the limits of the "CCOS" zone.

"CC4-1", as amended, permits five towers ranging in height from 20 to 45 storeys, versus seven towers ranging in height from 18 to 45 storeys previously approved, with revised tower placement and a mix of residential, office, and retail uses.

"CC4-2", as amended, permits three towers ranging in height from 12 to 55 storeys, versus three towers ranging in height from 12 to 45 storeys previously approved, and a mix of residential, office, and retail uses.

"CC4-3", as amended, permits five towers ranging in height from 22 to 45 storeys, versus four towers ranging in height from 13 to 40 storeys previously approved, with revised tower placement and a mix of residential, office, and retail uses.

"CC4-4", as amended, permits four towers ranging in height from 40 to 49 storeys, versus three towers ranging in height from 18 to 45 storeys previously approved, with revised tower placement and a mix of residential, office, and retail uses with seasonal outdoor display.

"CC4-5", as amended, permits apartment dwellings at a maximum height of 4 storeys, versus apartment dwellings at a maximum height of 5 storeys as previously approved, with minor expansion to the zone limits, revised setbacks and no loading space required.

"CCOS", as amended, provides an open space block 20 metres (65.6 feet) in frontage along Parkside Village Drive, versus 30 metres (98.4 feet) previously approved.

### Location of Lands Affected

North side of Burnhamthorpe Road West, west of Confederation Parkway, in the City of Mississauga.

Further information regarding this By-law may be obtained from Jonathan Famme of the City Planning and Building Department at 905-615-3200 ext. 4229.



## REVISED UDCD SECTION 7, January 2014.

The Revised UDCD Section 7, January 2014 supersedes UDCD Section 7 December 2005 UDD.

The UDCD should be read with this section 7 amendment .

This revised UDCD Section 7 also amends and supercedes the following sections of the December 2005 UDCD:

- 3.32 Detailed Concept Plan
- 6.2 Building Hieght and Massing
- 6.3.3 Maximum Hieght up to Podium Level
- 6.4.3 Floorplates / Tower Heights

This section complements the general open space and built form guidelines. Its purpose is to identify and target more detailed guidelines particular to each parcel and suggest approaches for achieving the Parkside Village urban design vision. Each block plan identifies general block statistics, detailed building envelopes, possible massing, building locations and heights. Frontages at prominent locations, where opportunities exist for architectural focal points and details, are identified, as are primary building frontages that relate to the street. For each block, block-specific conditions to be addressed in development proposals are also identified. Pedestrian level wind analysis and sun/shadow studies will be required at the time of site plan applications for each block, to assess and mitigate potential impacts on parks and public spaces.

The intent of the block plans is to suggest one potential massing approach, based on Zoning By-law controls for setbacks, stepbacks, height, floorplate sizes, land use, building spacing and landscaping provision. Many other design approaches are possible. Plans provided in this section illustrate how massing and design details might be implemented on each block to respond to the urban design guidelines provided in this document, as well as block-specific conditions, adjacencies, daylighting and view corridors, park spaces and environmental character. Final massing and detailing of the blocks can differ from the plans illustrated in this section, but should, as much as possible, maintain the overall urban design vision for Parkside Village and be consistent with the intent of the urban design guidelines.

## 7.1 Block 1

Block 1 is the largest of the development blocks. It is divided into Block 1 West and Block 1 East by a municipal road with underground garage route or driveway. The form and details of the future physical or visual access towards Zonta Meadows is to be determined through site plan application process.

### 7.1.1 Block 1 West

**Uses:** Predominantly residential, with a mix of point towers, podium levels and low-rise apartments.

**Edge Conditions:** Provides key frontages to Park Block 2 and Zonta Meadows Park, with parkland setbacks. At the north-west corner, the block abuts a hydro-transmission sub-station. The northern edge of the block abuts the 23.5m ROW extension of Square One Drive and the existing high density residential.

#### Height and Massing:

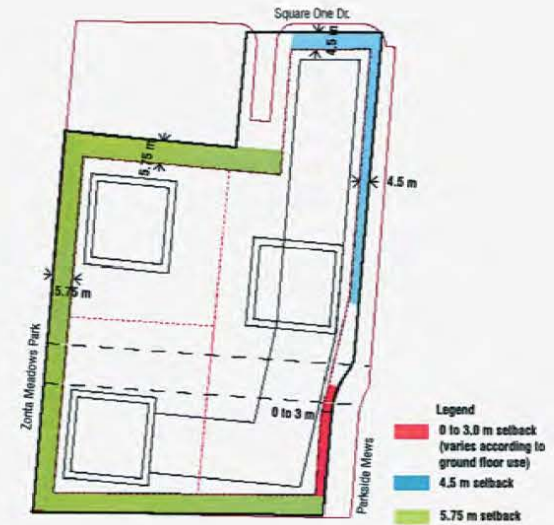
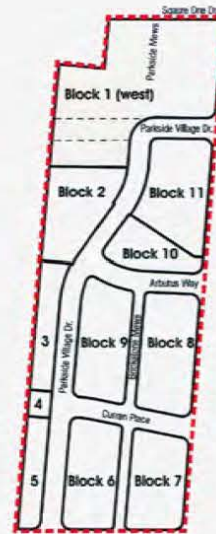
36

- Low-rise to mid-rise buildings and podiums: 3 to 5 storeys
- High-rise towers: 3 small floorplate towers with a maximum floorplate of 740 m<sup>2</sup>.

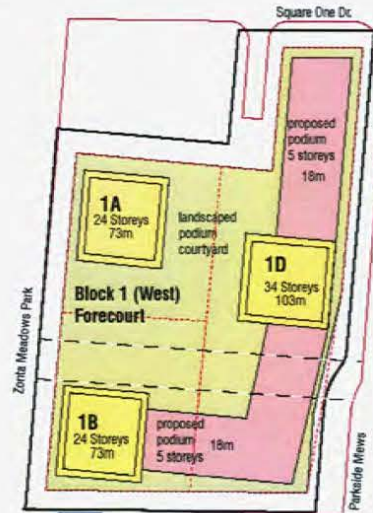
#### Special Considerations:

- Buildings in this block should have strong orientation toward Zonta Meadows Park and the Park Block 2 with ground floor amenities
- No vehicular access will be provided on park frontages.
- Units and common areas will have windows facing the park.
- Landscaping adjacent to park areas will provide a transition between the private and public realms.
- Utilities, parking and building service areas will not be exposed to parkland and public streets.
- Access to building entrances, including walkways, canopies, retaining walls and stairs will be accommodated on private property and will not encroach on the park.
- Opportunity to continue Parkside Village Dr. as a visual and service access to be studied.
- Any transformers should be located within private lands, rather than on park lands.

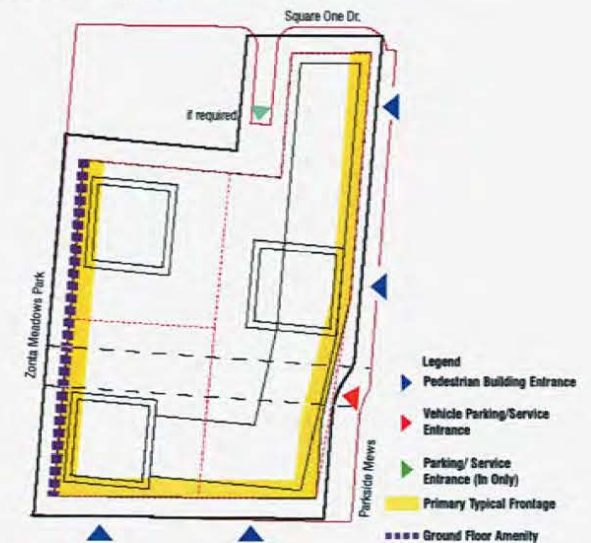
Key Plan



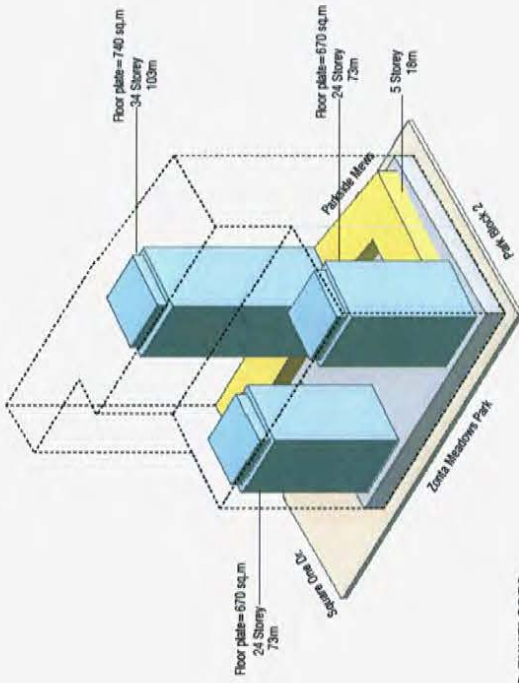
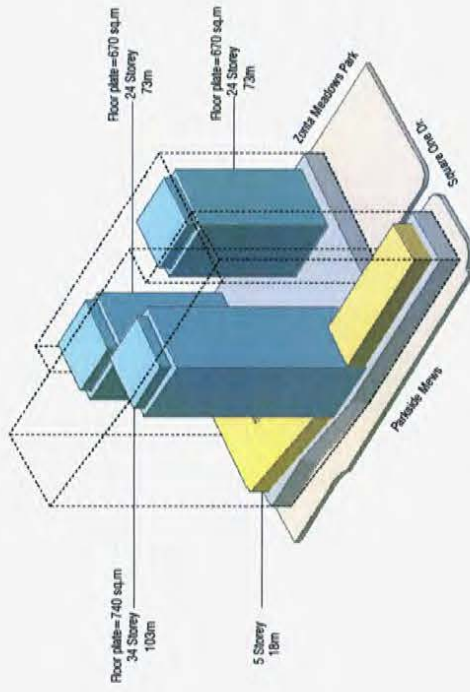
Block 1 West - SETBACKS



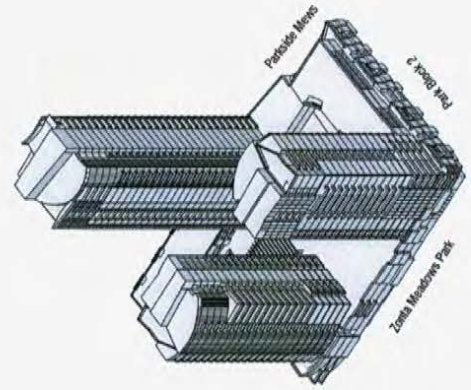
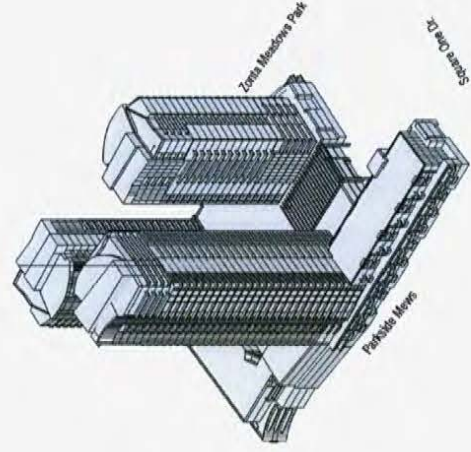
Block 1 West - BUILDING TYPES AND HEIGHTS



Block 1 West - FRONTAGES AND ENTRY POINTS



Block 1 West - BUILDING ENVELOPES



Block 1 West - POSSIBLE MASSING



## 7.1.2 Block 1 East

Block 1 East, consisting of two point towers and low-rise apartments oriented around a central courtyard, provides key frontages along Confederation Parkway and Parkside Village Dr.

**Edge Conditions:** The primary frontage of this block is on the upper extent of Confederation Parkway adjacent to future high-density residential uses. Primary vehicular access to this block is provided via the extension of Parkside Village Drive (Parkside Mews), located at the centre of Block 1. Two existing high-density residential buildings are located at the north-west and south-east edges of the block.

**Uses:** Predominantly residential with the potential for commercial/retail uses along Confederation Parkway.

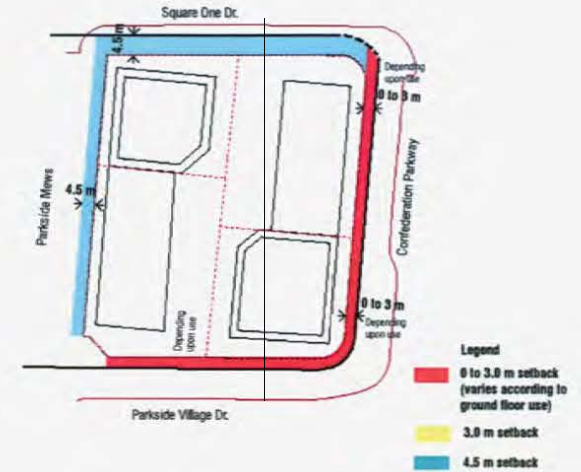
### Height and Massing:

- Mid-rise buildings and podiums: 5-Storey podiums along the extension Parkside Village Dr. and Parkside Mews and 7-storey podium along Confederation Parkway.
- High-rise towers: 1 small floorplate point tower (1F) with a maximum floorplate of 740m<sup>2</sup> and 1 larger floorplate tower s(11) (max. 790m<sup>2</sup>).

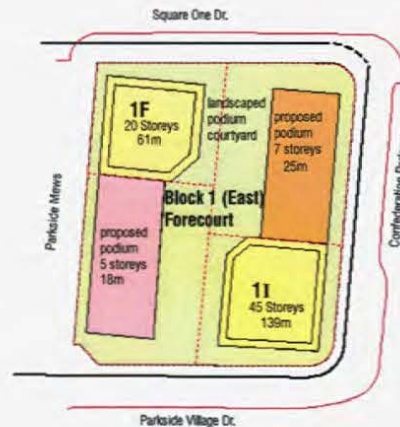
### Special Conditions:

- The access road layout for this block may change and may affect block layout.
- Utilities, parking and building service areas will not be exposed to public streets.
- Any transformers should be located within private lands.

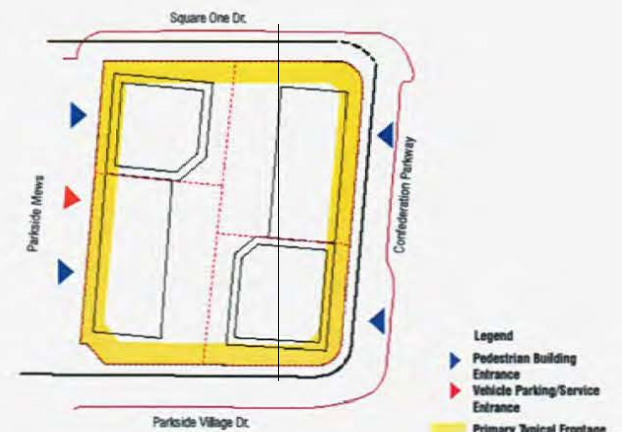
### Key Plan



Block 1 East- SETBACKS

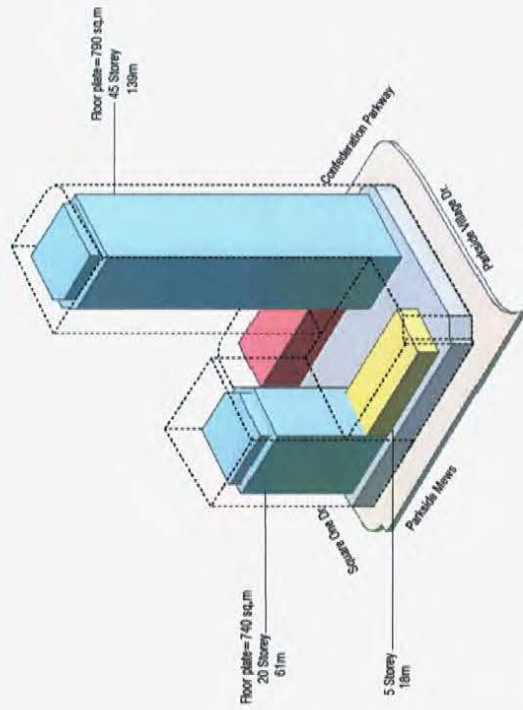
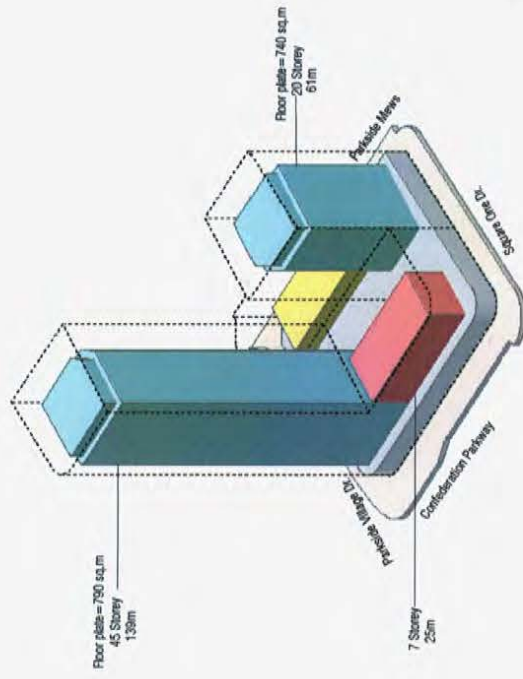


Block 1 East - BUILDING TYPES AND HEIGHTS

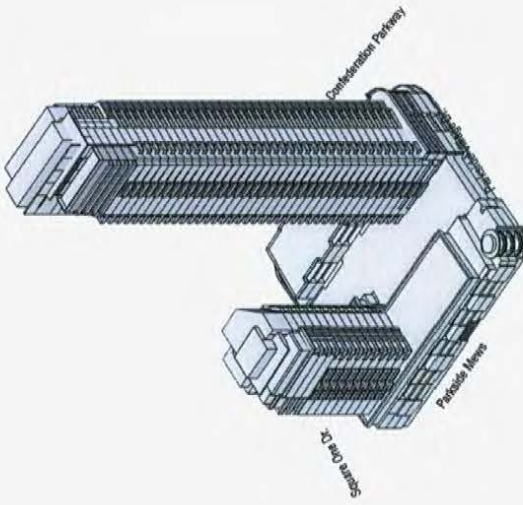


Block 1 East - FRONTAGES AND ENTRY POINTS

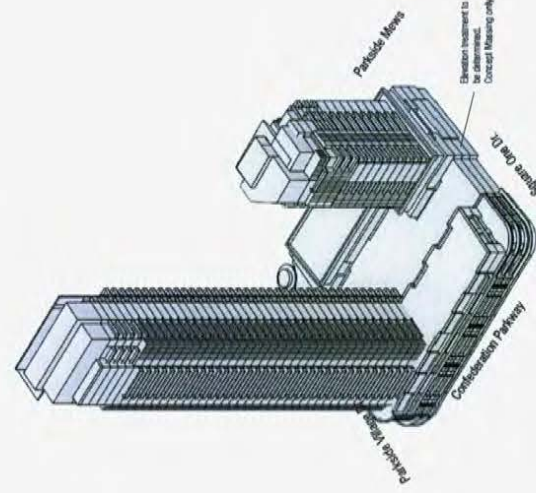




Block 1 East - BUILDING ENVELOPES



Block 1 East - POSSIBLE MASSING



## 7.2 Blocks 3 and 5

Blocks 3 and 5 are located along the western edge of existing Parkside Village Drive. These narrow development blocks play a transitional role from existing low-rise residential areas to the much more intensive City Centre. Each block is divided with three distinct buildings allowing for visual permeability.

**Edge Conditions:** Many of the edges in these two blocks consist of public open spaces and will require consideration for the landscape treatment to ensure pedestrian comfort and safety and enhance the sense of public space. Along the Burnhamthorpe Street frontage, the City has plans for a public multi-purpose trail.

**Uses:** Residential; no commercial uses are permitted.

### Height and Massing:

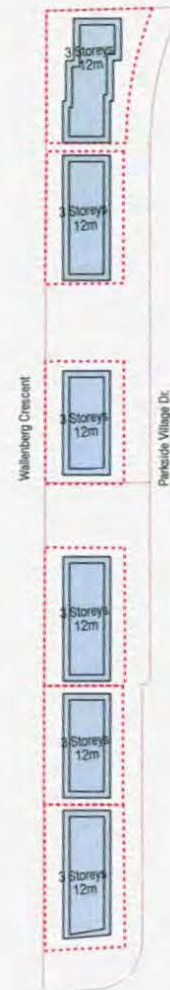
- Low-rise buildings and podiums: 3-storey heights are allowed for both block 3 and block 5.
- Mid-rise buildings and podiums: n/a
- High-rise towers: n/a

### Special Conditions:

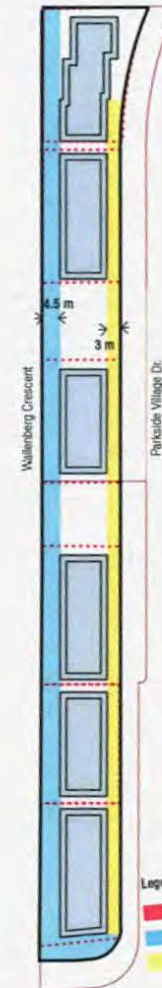
- Buildings along Burnhamthorpe Road West will wrap the corner.
- Special consideration must be given to building facades flanking public parks to allow for front door suite access.
- All buildings must front Parkside Village Drive and have frontage elements onto the linear green space.
- Residences will front onto parks wherever possible and avoid blank facades.
- The frontage of Blocks 3 and 5 requires attention to underground access and service locations. Where loading bays and refuse collection areas are at grade they must be integrated with the building or screened by landscaping or other treatment.
- Building articulation and landscaping should respond to existing and future pedestrian and cyclist uses along the Burnhamthorpe Road West frontage.
- Utilities, parking and building service areas will not be exposed to parkland and public streets.
- Access to building entrances, including walkways, canopies, retaining walls and stairs will be accommodated on private property and will not encroach on the park.
- Any transformers should be located within private lands, rather than on park lands.



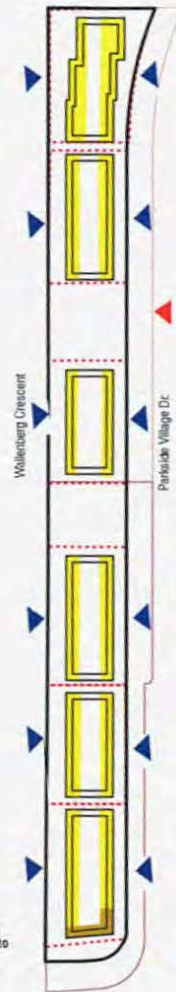
Key Plan



Blocks 3 and 5 - BUILDING TYPES AND HEIGHTS

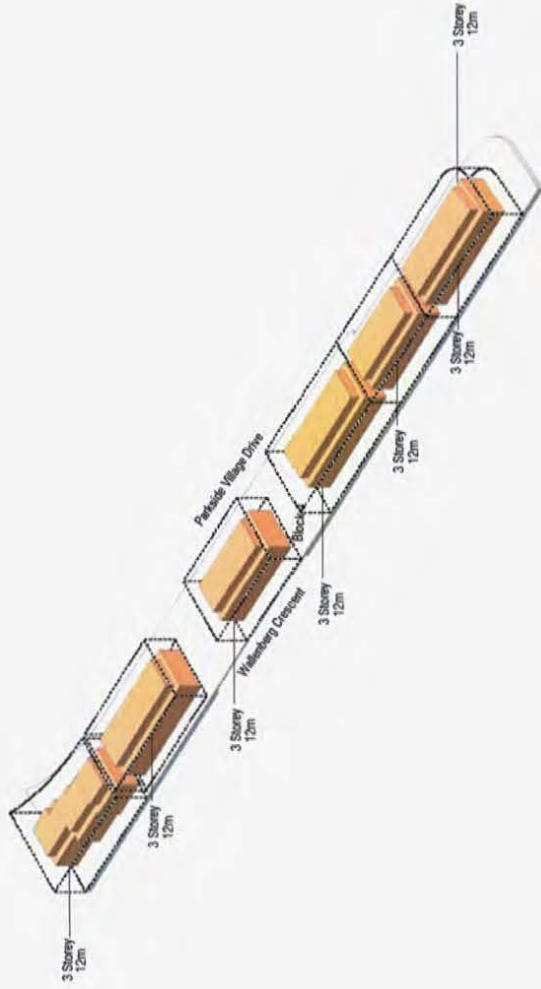


Blocks 3 and 5 - SETBACKS

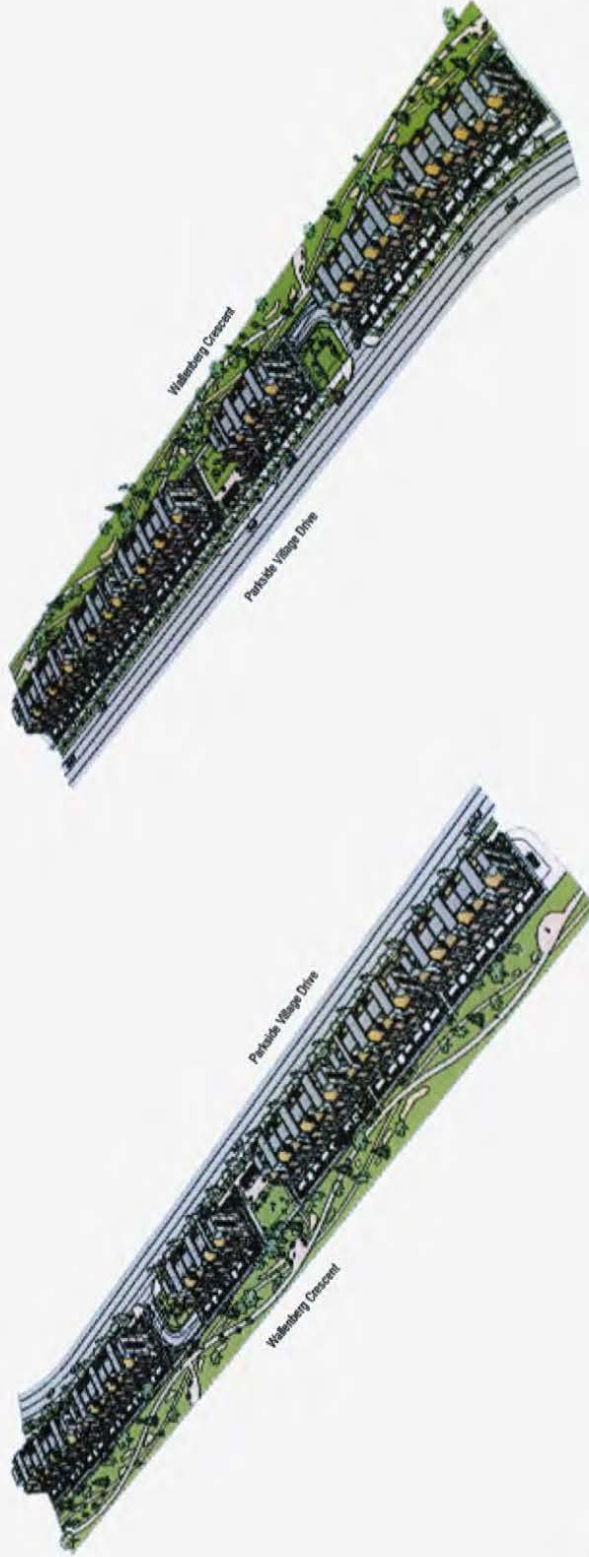


Blocks 3 and 5 - FRONTAGES AND ENTRY POINTS

- Legend
- Pedestrian Building Entrance
  - Vehicle Parking/Service Entrance
  - Primary Typical Frontage
  - Prominent Frontage



Block 3 - BUILDING ENVELOPES



Block 3 - POSSIBLE MASSING



### 7.3 Block 6

Block 6 begins to introduce more significant increases in height toward the taller uses to the east. Building 4C provides a key role as one of the primary façades of Burnhamthorpe Road.

**Edge Conditions:** It is a perimeter block with varied public edges on all 4 sides. The Burnhamthorpe Road West edge consists of more intensive built form, while Brickstone Mews edge, elucidates a more intimate urban residential character.

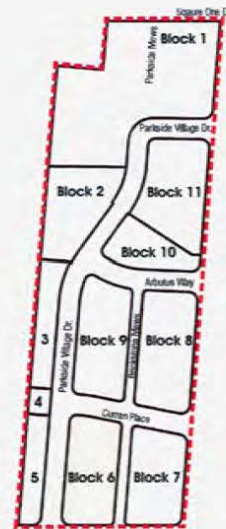
**Uses:** Predominantly residential with potential for ground-related commercial uses fronting on Curran Place and Burnhamthorpe Road.

#### Height and Massing:

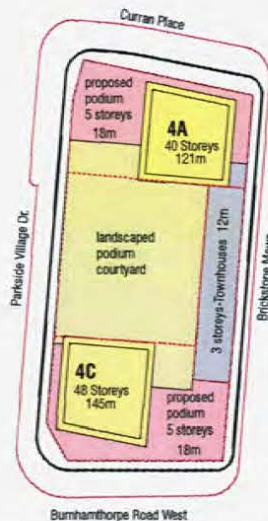
- Low-rise buildings and podiums: 3-storey units along Brickstone Mews and 5 storey podiums along Curran Place and Burnhamthorpe Road.
- Mid-rise buildings and podiums: n/a
- High-rise towers: Maximum floorplates (740 m<sup>2</sup> at Brickstone Mews and 838 m<sup>2</sup> at Parkside Village Dr.)

#### Special Conditions:

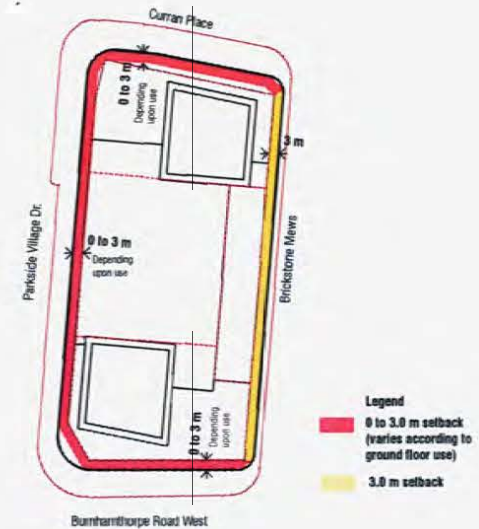
- Buildings along Burnhamthorpe Road West will wrap the corner and incorporate specialized corner features articulated by the massing, signage or canopies.
- Where practical, the corner tower (4C) will step back away from Parkside Village Dr.
- Commercial frontages along Burnhamthorpe Road West are permitted and encouraged.
- Utilities, parking and building service areas will not be exposed to public streets.
- Service/parking access from Brickstone Mews will be limited.
- Any transformers should be located within private lands.



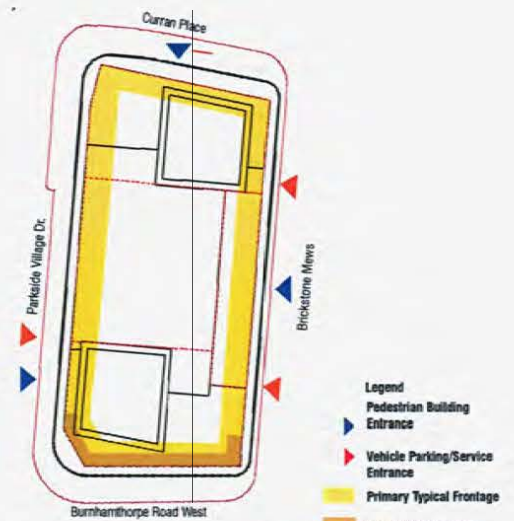
Key Plan



Block 6 - BUILDING TYPES AND HEIGHTS

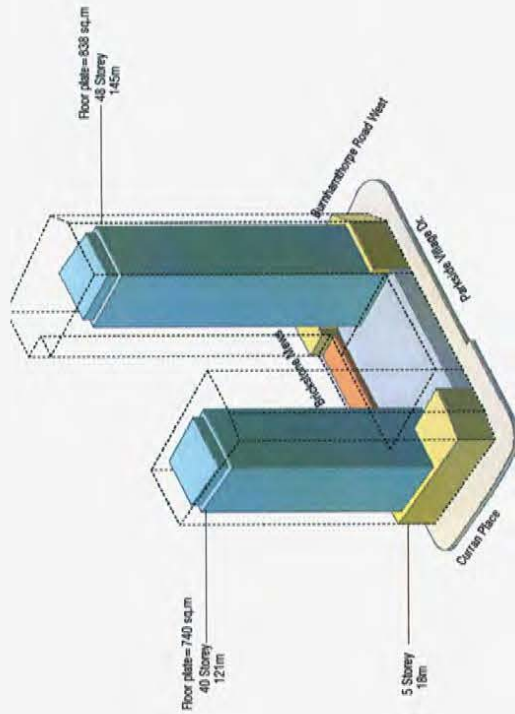
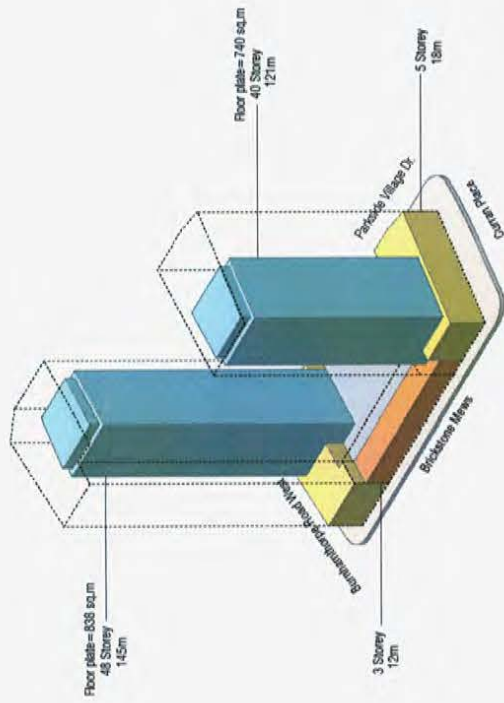


Block 6 - SETBACKS

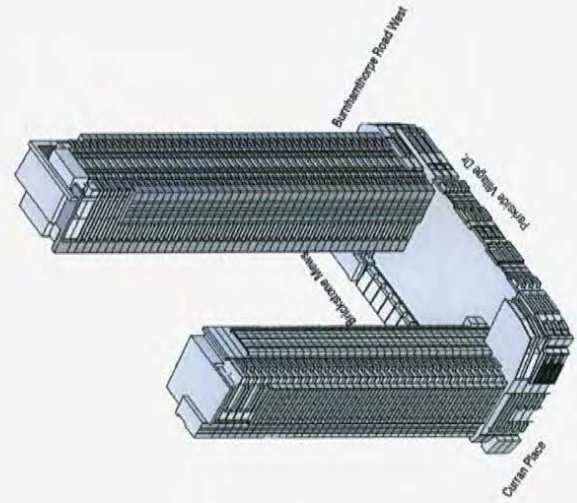
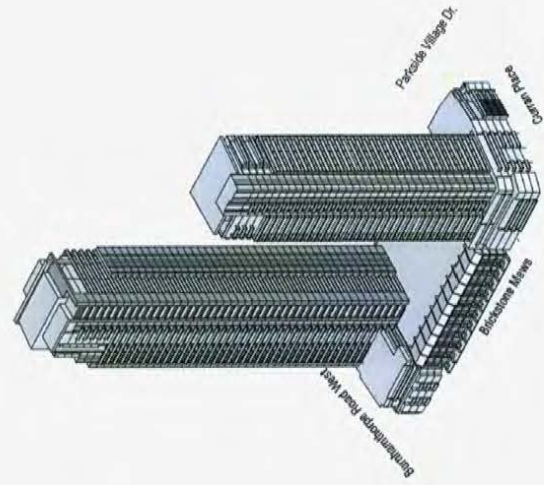


Block 6 - FRONTAGES AND ENTRY POINTS





Block 6 - BUILDING ENVELOPES



Block 6 - POSSIBLE MASSING

## 7.4 Block 7

Block 7 is located at the key intersection of Burnhamthorpe Road West and Confederation Parkway. This forms a primary entry block and gateway to Parkside Village.

**Edge Conditions:** A variety of public edges on all 4 sides from high intensity urban edges along Confederation Parkway and Burnhamthorpe Road West to the low-scaled internal Brickstone Mews on the west side.

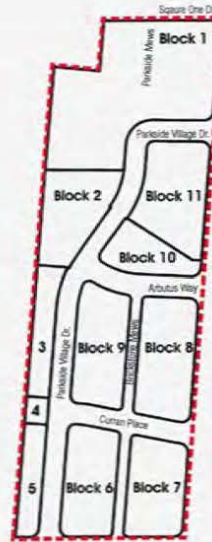
**Uses:** A mix of residential building types and ground-floor commercial uses. Commercial buildings may also be considered along Confederation Parkway.

### Height and Massing:

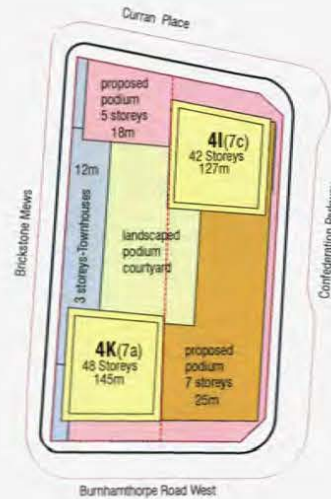
- Low-rise buildings and podiums: 3-storey units along Brickstone Mews and 5-storey podiums along Curran Place and Burnhamthorpe Road.
- Mid-rise buildings and podiums: 7-storey podium along Burnhamthorpe Road West and Confederation Parkway
- High-rise towers: 2 medium floorplate towers (810m<sup>2</sup> at Confederation Parkway and City Centre Dr. and an 838m<sup>2</sup> floorplate along Burnhamthorpe Rd. West.)

### Special Conditions:

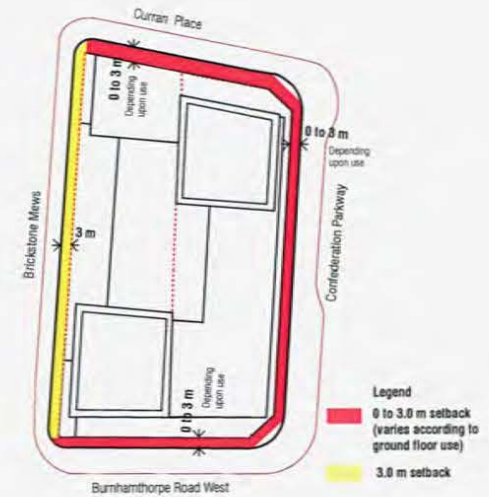
- Buildings at the corner of Confederation Parkway and Burnhamthorpe Road West will wrap the corner and should incorporate specialized corner features articulated by the massing, signage or canopies.
- Vehicular access into the Block from Confederation Parkway is not permitted.
- Utilities, parking and building service areas will not be exposed to public streets.
- Any service/parking access from Brickstone Mews will be as small as possible.
- Any transformers should be located within private lands, rather than on park lands.



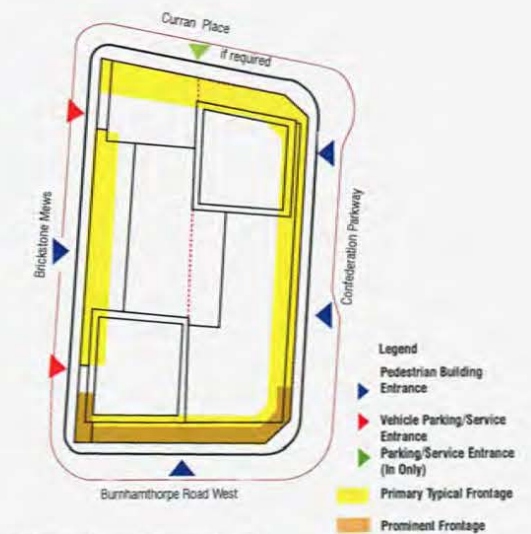
Key Plan



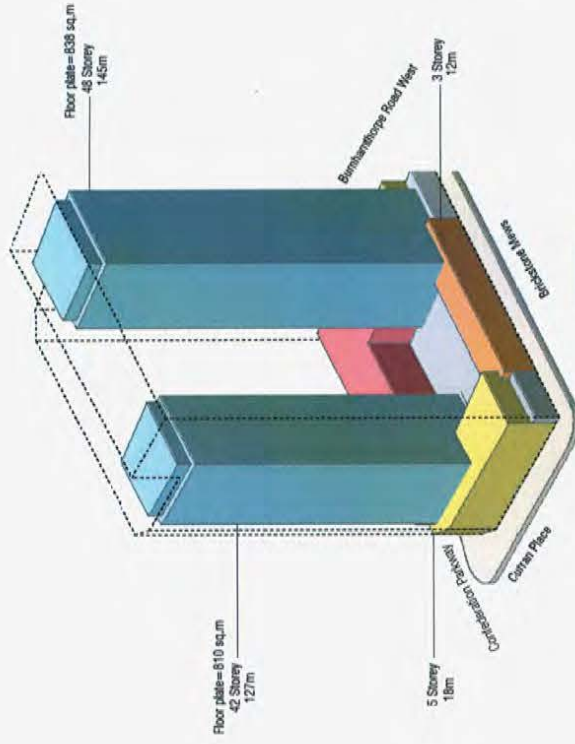
Block 7 - BUILDING TYPES AND HEIGHTS



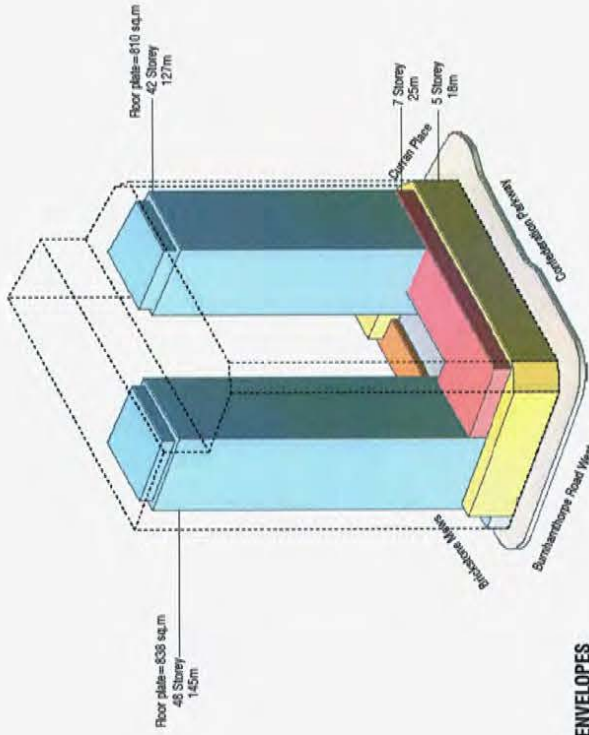
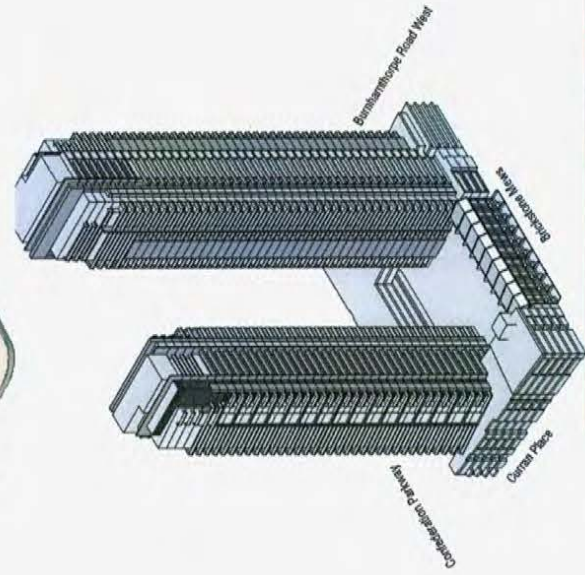
Block 7 - SETBACKS



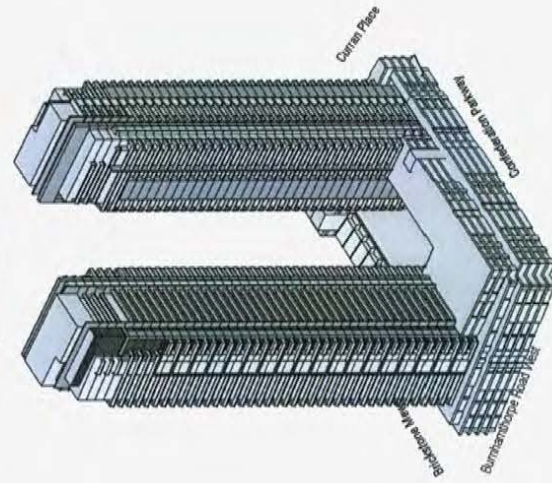
Block 7 - FRONTAGES AND ENTRY POINTS



Block 7 - BUILDING ENVELOPES



Block 7 - POSSIBLE MASSING





## 7.5 Block 8

Block 8 is located north of the Curran Place and Confederation Parkway intersection and is on the eastern edge of Parkside Village. This block forms primary frontages along Confederation Parkway and Arbutus Way, where ground floor commercial uses are required.

**Edge Conditions:** Block 8 is a perimeter block fronting on public streets ranging from the narrow 12.5m Brickstone mews to the 40m wide Confederation Parkway. The northern edge of the block also fronts onto Park Block 10, the primary open space feature of the site.

**Uses:** A mix of residential types, ground-level commercial services and office uses.

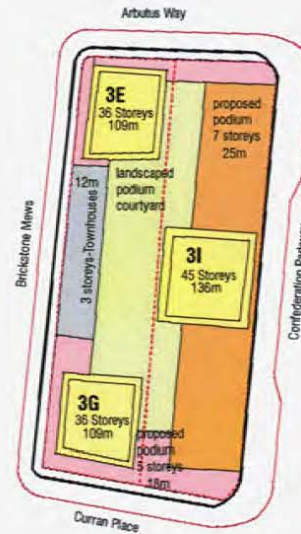
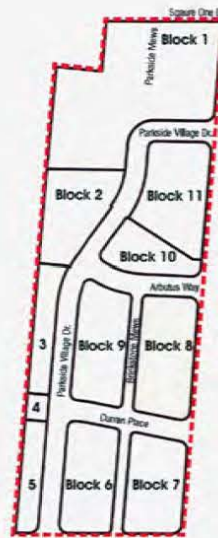
### Height and Massing:

- Low-rise buildings and podiums: 3-storey units along Brickstone Mews and 5 storey podium along internal 23.5m streets.
- Mid-rise buildings and podiums: 7-storey podium along the Confederation Parkway frontage.
- High-rise towers: Three point towers, each with a maximum floorplate of 790 m<sup>2</sup>.

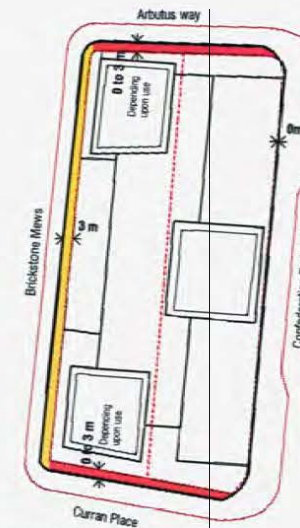
### 46 Special Conditions:

- 60% of the Confederation Parkway frontage must contain commercial or retail services at grade.
- 70% of the Arbutus Way frontage must contain commercial or retail services at grade.
- Building 8c will be oriented toward the corner and complement Building 11a to create a gateway into the site.
- Service/parking access will be from Brickstone Mews.
- Utilities, parking and building service areas will not be exposed to public streets.
- Any transformers should be located within private lands.

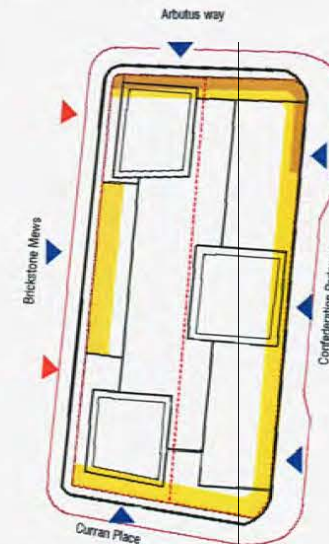
Key Plan



Block 8 - BUILDING TYPES AND HEIGHTS



Block 8 - SETBACKS



Block 8 - FRONTAGES AND ENTRY POINTS





## 7.6 Block 9

Block 9 is a medium density block continuing the transition between the community to the west and the City Centre to the east.

**Edge Conditions:** It is an internal block with no frontages onto major arterial roads, 3 frontages onto 23.5m internal roads and frontage onto the 12.5m Brickstone Mews. The northern edge of the block fronts onto Park Block 10, the primary open space feature of the site.

**Uses:** A mix of residential, podium level commercial and retail uses along the Arbutus Way frontage as a complement to commercial and public open space uses around Park Block 10.

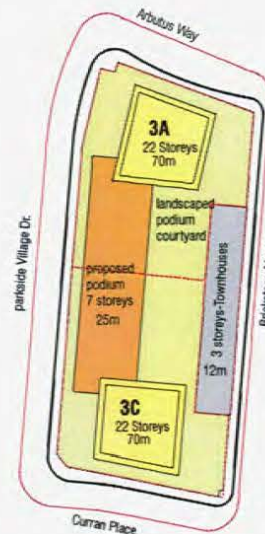
### Height and Massing:

- Low-rise buildings and podiums: 3-storey units along Brickstone Mews and 5 storey podium along Parkside Village Dr.
- Mid-rise buildings: n/a
- High-rise towers: Two 22 storey towers with footprint of 730 m<sup>2</sup>.

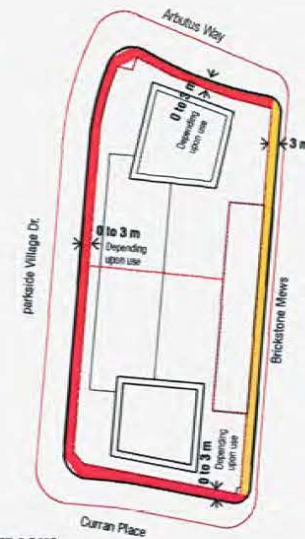
### Special Conditions:

- Building 3A will wrap the corner with strong orientation toward both the Park Block 2 and Park Block 10.
- Utilities, parking and building service areas will not be exposed to public streets.
- Any transformers should be located within private lands.

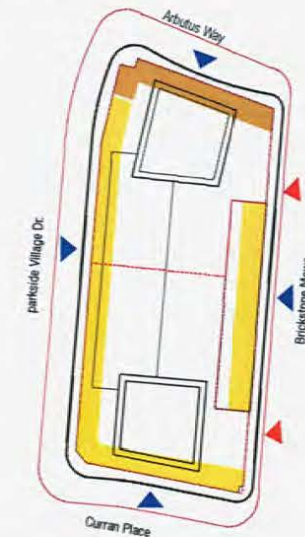
### Key Plan



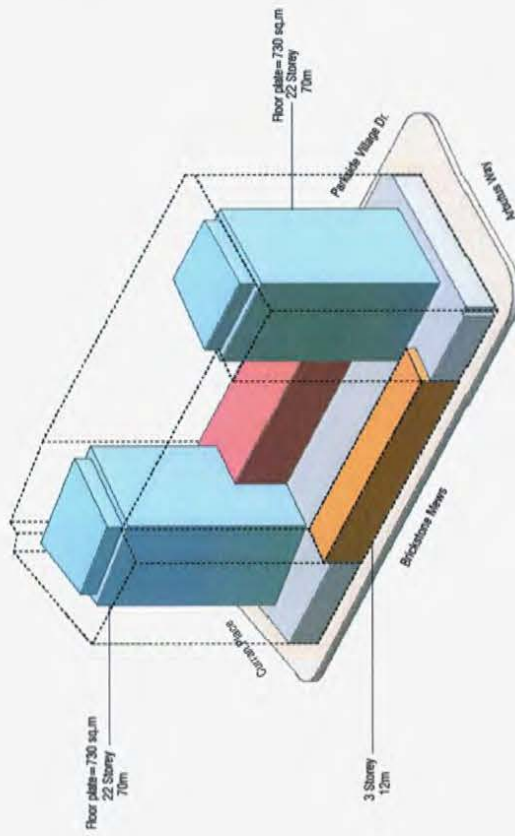
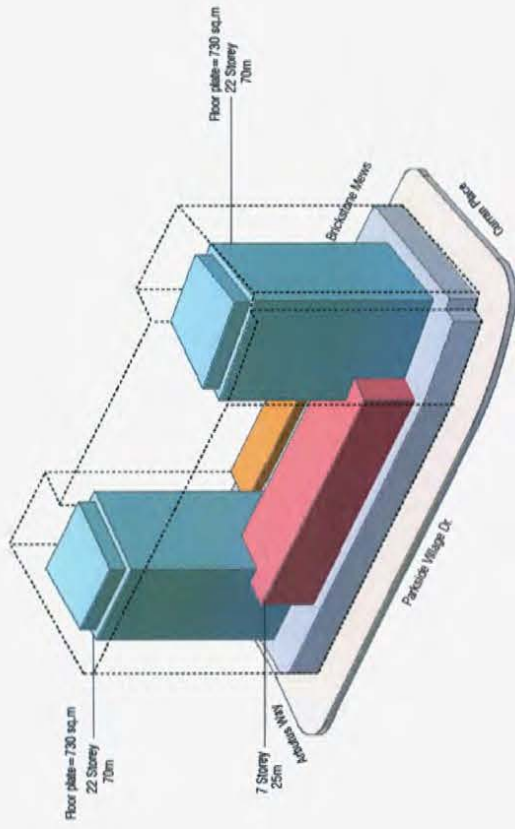
Block 9 - BUILDING TYPES AND HEIGHTS



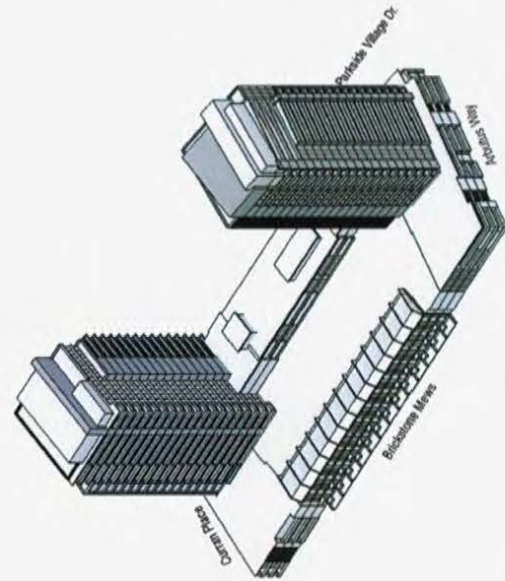
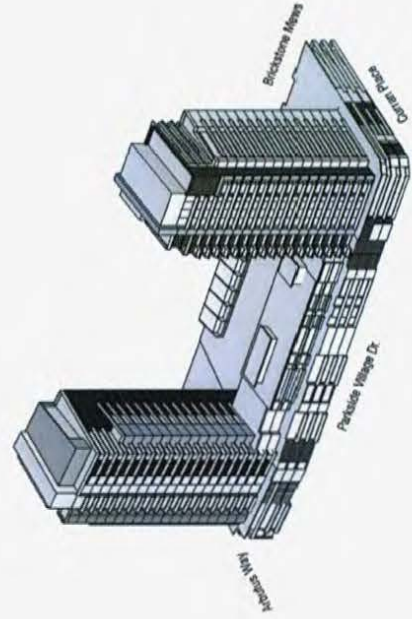
Block 9 - SETBACKS



Block 9 - FRONTAGES AND ENTRY POINTS



Block 9 - BUILDING ENVELOPES



Block 9 - POSSIBLE MASSING



## 7.7 Block 11

Block 11 forms the centre of activity of Parkside Village. It encompasses the primary architectural focal point and most important corner on the site, the Confederation Parkway and Arbutus Way intersection. It is the primary frontage onto the park at the heart of Parkside Village, and plays a vital landmark role in animating the area.

**Edge Conditions:** This block contains some of the most significant public open space and primary street frontages in Parkside Village. It contains the primary Confederation Parkway Frontage and also fronts future high density development and a public park that links farther east to the public green space surrounding the Living Arts Centre. The western edge of the site fronts onto Parkside Village Dr. and Park Block 2. At the south end, the block contains the primary commercial area and setback area, which is part of the Gateway area and fronts onto Park Block 10.

**Uses:** This block can accommodate a broad mix of uses including podium, mid-rise and point tower residential structures and podium-level commercial, retail and entertainment uses. The Landmark Tower (2D) is well suited to hotel, conference centre and spa/fitness facilities that serve the residents of Parkside Village, as well as supporting City Centre and regional functions. Building 2D could also be developed as a commercial tower, if appropriate market conditions exist. Restaurant and associated active uses will be located at the southeast corner of Block 11, fronting the park on Block 10.



Key Plan

### Height and Massing:

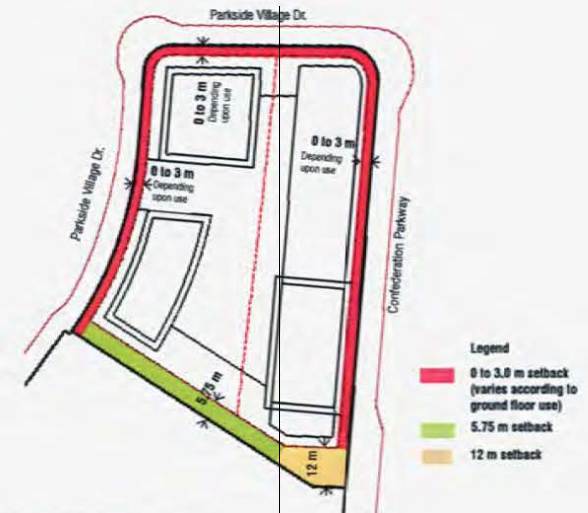
- Low-rise buildings and podiums: 5-storey podiums internal to the site.
- Mid-rise buildings and podiums: 7-storey podiums defining the Confederation Parkway frontage and 12-storey building fronting Park Block 10.
- High-rise towers: 1 medium floorplate point tower (max. 790 m<sup>2</sup>) and the "Landmark Tower" (2D) with maximum floorplate of 929 m<sup>2</sup> and a maximum height of 55 storeys.

### Special Conditions:

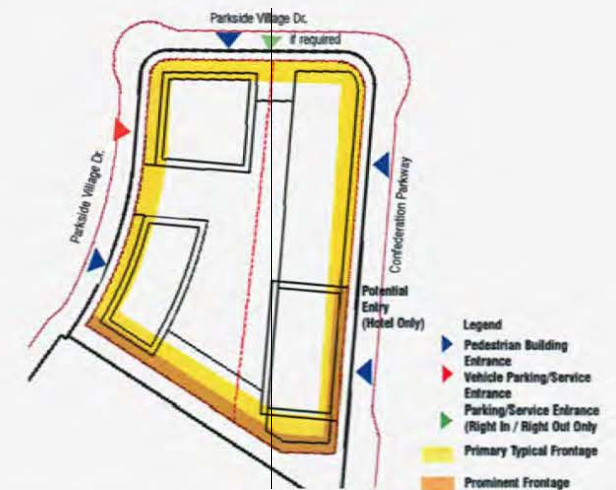
- Building 2D will be designed as Parkside Village's "Landmark Tower"
- Building 2D must address Confederation Parkway and Arbutus Way.
- The primary pedestrian entrance to Building 2D will be on Confederation Parkway if a hotel is developed as part of that site.
- The commercial setback area shall be designed so as to encourage use of outdoor spaces and reinforce the role of Block 10 Park (see Master Plan Concept and Land Use, Built Form and Architectural Detailing Guidelines (Sections 3.3.2, 4.2.1, 5.2, 7.0, 9.0)).
- Building 2B will address both Park Block 10 and Park Block 2 frontages and consider views into the public open space system.
- Utilities, parking and building service areas will not be exposed to parkland and public streets.
- An additional vehicular access point from Confederation Parkway may be provided for access to the Landmark Tower. Where the tower is developed as a hotel, this entrance will be designed to reinforce the high quality character and prominence of this location.
- Any transformers should be located within private lands, rather than on park lands.



Block 11 - BUILDING TYPES AND HEIGHTS

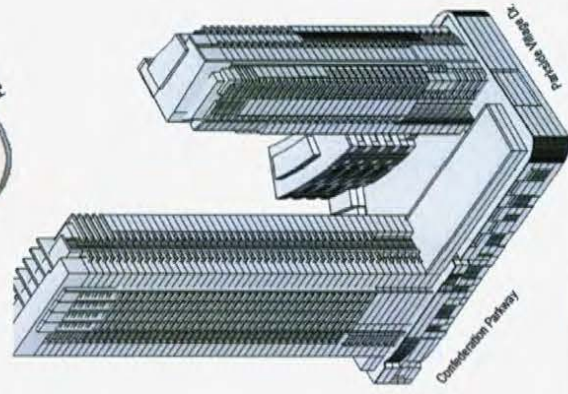
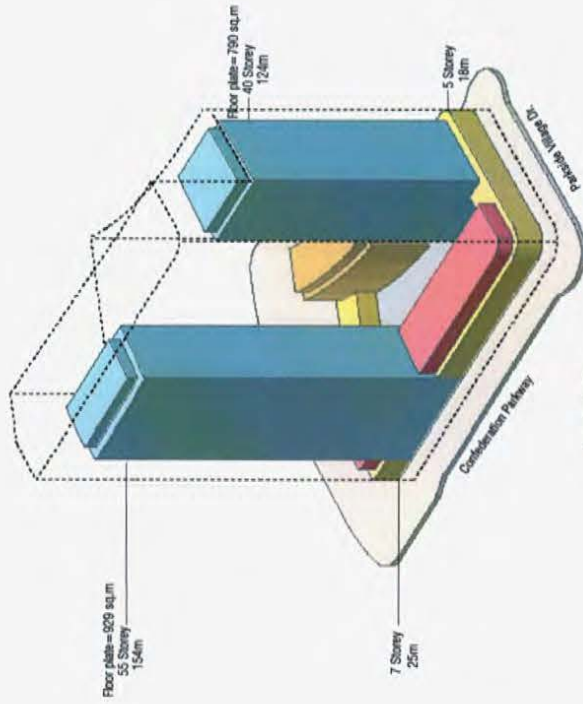


Block 11 - SETBACKS

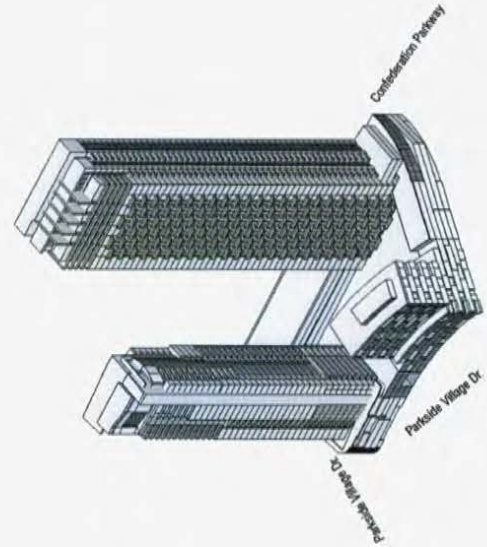
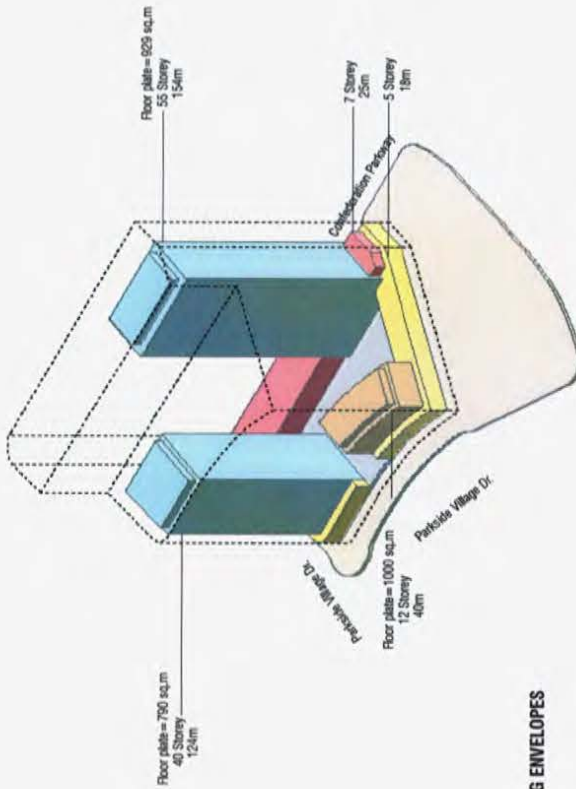


Block 11 - FRONTAGES AND ENTRY POINTS





Parkside Village at Mississauga City Centre



Block 11 - BUILDING ENVELOPES

Block 11 - POSSIBLE MASSING