



AGENDA

PLANNING & DEVELOPMENT COMMITTEE

THE CORPORATION OF THE CITY OF MISSISSAUGA

TUESDAY, APRIL 2, 2013 - 7:00 P.M.

COUNCIL CHAMBER, 2ND FLOOR - CIVIC CENTRE
300 CITY CENTRE DRIVE, MISSISSAUGA, ONTARIO L5B 3C1
<http://www.mississauga.ca>

Members

Mayor Hazel McCallion	
Councillor Jim Tovey	Ward 1 (Chair)
Councillor Pat Mullin	Ward 2
Councillor Chris Fonseca	Ward 3
Councillor Frank Dale	Ward 4
Councillor Bonnie Crombie	Ward 5
Councillor Ron Starr	Ward 6
Councillor Nando Iannicca	Ward 7
Councillor Katie Mahoney	Ward 8
Councillor Pat Saito	Ward 9
Councillor Sue McFadden	Ward 10
Councillor George Carlson	Ward 11

Mumtaz Alikhan, Legislative Coordinator, Office of the City Clerk
905-615-3200 ext. 5425 / Fax 905-615-4181
email: mumtaz.alikhan@mississauga.ca

PLANNING & DEVELOPMENT COMMITTEE – APRIL 2, 2013

CALL TO ORDER

DECLARATIONS OF (DIRECT OR INDIRECT) PECUNIARY INTEREST

MATTERS TO BE CONSIDERED

1. DEFERRED REPORT

This report was deferred from March 18, 2013 Planning and Development Committee Meeting - reference PDC-0017-2013.

Appeal to the Ontario Municipal Board – Committee of Adjustment Decision on Consent Application 'B' 5/13 W1 and Minor Variance Applications 'A' 9/13 and 'A' 10/13, Freida and Emma Fischer, 1238 Strathy Avenue, North of Lakeshore Road East, east of Cawthra Road (Ward 1)
File: 'B' 5/13 W1, 'A' 9/13 & 'A' 10/13 W1

2. PUBLIC MEETING

Information Report - Rezoning Application to permit eight (8) detached dwellings on a CEC Private Road, 2167 Gordon Drive, East side of Gordon Drive, south of Queensway West
Owner: Raffi Konialian
Applicant: Weston Consulting Group Inc., **Bill 51**, (Ward 7)
File: OZ 12/002 W7

3. PUBLIC MEETING

Information Report - Official Plan Amendment and Rezoning Applications to permit retail commercial and office uses fronting Lakeshore Road East, apartment and townhouse dwellings to the rear and public greenspace bordering Cooksville Creek, 447, 453, 501 Lakeshore Road East and 1021, 1027, 1077 Enola Avenue, Northeast corner of Lakeshore Road East and Enola Avenue
Owner: 501 Lakeshore Inc., Trinity Properties Lakeshore Inc. and 1716336 Ontario Inc.
Applicant: Korsiak and Company Inc., **Bill 51**, (Ward 1)
File: OZ 11/017 W1

4. East Bloor Corridor Review – Background and Interim Strategy – Opportunities for Neighbourhood Revitalization (Ward 3)
File: CD.04.BLO

5. SUPPLEMENTARY REPORT
Rezoning Application to permit ten (10) street townhouse dwellings and maintain the existing apartment building, 1440 Bloor Street, Southeast corner of Bloor Street and Dixie Road
Owner: Tapes Investments
Applicant: Peter Favot Architect Ltd., **Bill 51**, (Ward 3)
File: OZ 11/012 W3

6. ADDENDUM SUPPLEMENTARY REPORT
Official Plan Amendment, Rezoning and Draft Plan of Subdivision Applications to permit apartments with ground related commercial uses, and townhouses under standard and common element condominium tenure, 5081 Hurontario Street, East side of Hurontario Street, north of Eglinton Avenue East
Owner: Summit Eglinton Inc.
Applicant: Jim Lethbridge, Lethbridge and Lawson Inc., **Bill 51**, (Ward 5)
File: OZ 09/011 W5 and T-M09004 W5

7. SUPPLEMENTARY REPORT
Official Plan Amendment and Rezoning Applications to permit a two-storey motor vehicle repair facility, Part of Lot 11, Concession 1, W.H.S. designated as Parts 1& 2, Plan 43R-13493, Northwest corner of Derry Road West and Hurontario Street
Owner: Antorisa Investments Ltd.
Applicant: Bousfields Inc., **Bill 51**, (Ward 5)
File: OZ 11/018 W5

ADJOURNMENT



Corporate Report

PLANNING & DEVELOPMENT COMMITTEE

MAR 18 2013

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PLANNING & DEVELOPMENT COMMITTEE
APR 02 2013

Originator's
Files

'B' 5/13 W1,
'A' 9/13 & 'A' 10/13 W1

DATE: February 26, 2013

TO: Chair and Members of Planning and Development Committee
Meeting Date: March 18, 2013

FROM: Edward R. Sajecki
Commissioner of Planning and Building

SUBJECT: **Appeal to the Ontario Municipal Board
Committee of Adjustment Decision
Consent Application 'B' 5/13 W1 and
Minor Variance Applications 'A' 9/13 and 'A' 10/13
Freida and Emma Fischer
1238 Strathy Avenue
North of Lakeshore Road East,
east of Cawthra Road**

Ward 1

RECOMMENDATION: That the Report dated February 26, 2013, from the Commissioner of Planning and Building regarding the appeal filed by Legal Services by letter be adopted, and that Legal Services, together with other appropriate City staff attend the Ontario Municipal Board hearing in support of the appeal of the decisions of the Committee of Adjustment under files 'B' 5/13 W1, 'A' 9/13 and 'A' 10/13 W1, regarding the property at 1238 Strathy Avenue.

**REPORT
HIGHLIGHTS:**

- The subject consent application ('B' 5/13 W1) and minor variance applications ('A' 9/13 and 'A' 10/13 W1) were approved by the Committee of Adjustment on January 3, 2013.
- The Planning and Building Department recommended that the applications be refused since they did not maintain the intent of the Official Plan and Zoning By-law, and were not minor in nature.

- A "Placeholder" appeal has been filed by Legal Services as these decisions could set an undesirable precedent with respect to the interpretation of the Official Plan and Zoning By-law in the context of other Committee of Adjustment matters being considered by the City.

BACKGROUND:

On January 3, 2013, the Committee of Adjustment considered severance application 'B' 5/13 W1 to convey a parcel of land having a frontage of approximately 11.55 m (37.89 ft.) and a lot area of approximately 387.40 m² (4,170.07 sq. ft.), for the purpose of creating a new residential lot. Minor Variance applications, under files 'A' 9/13 and 'A' 10/13 W1 were also submitted to permit lot frontages of 11.55 m (37.89 ft.) in each instance, lot areas of 387.40 m² (4,170.07 sq. ft.), lot coverage of 40% for each lot, and side yard setbacks of 1.20 m (3.93 ft.) for each new proposed dwelling.

At the Committee of Adjustment meeting, the applicant indicated that the variances for lot coverage and side yard setbacks would no longer be required since the size of both dwellings would be reduced. The amended applications were approved on January 3, 2013 by the Committee of Adjustment with variances for lot frontage and lot area.

A "Placeholder" appeal was submitted on January 25, 2013 by Legal Services. The purpose of this report is to seek direction on this matter.

Background information is provided in Appendices 1 to 7.

COMMENTS:

The applicant's authorized agent attended the Committee of Adjustment meeting on January 3, 2013 to present the applications. The authorized agent expressed the opinion that the proposed frontages and lot areas were compatible with the existing lot pattern of the surrounding properties. He further stated that in his view the proposed dwellings were compatible with the size and scale of other dwellings on this portion of Strathy Avenue.

The Planning and Building Department recommended that the severance and minor variance applications be refused on the basis that they do not maintain the general intent and purpose of the Official Plan and are not desirable for the appropriate development of the land.

Official Plan

The subject property is designated "Residential Low Density II" in the Lakeview Neighbourhood which permits detached dwellings. The Neighbourhood policies of Mississauga Official Plan further outline specific requirements for consent applications.

Section 16.1.2.1 states:

"To preserve the character of lands designated Residential Low Density I and Residential Low Density II, the minimum frontage and area of new lots will generally represent the greater of:

- a. the average lot frontage and lot area of residential lots on both sides of the same street within 120 m (393.70 ft.) of the subject property. In the case of a corner lot, lots on both streets within 120 m (393.70 ft.) will be considered; or
- b. the requirements of the Zoning By-law."

The purpose of this policy is to ensure that the lot frontages and lot areas that define and characterize the streetscape in this neighbourhood are maintained.

The Planning and Building Department reviewed the applications and calculated the average of the lot frontages and lot areas within 120 m (393.70 ft.) of the subject lands as per the Mississauga Official Plan policy, and the results are as follows:

Average Lot Frontage = approximately 23.30 m (76.44 ft.)

Average Lot Area = approximately 820.01 m² (8,826.80 sq. ft.)

In this particular instance, the Official Plan policy would be applicable to the consent application, as the proposal does not maintain the average lot frontage or lot area within 120 m (393.70 ft.) of the subject property.

Based on the information provided in the severance application, the retained and severed lots would both have lot frontages of approximately 11.55 m (37.89 ft.) and lot areas of approximately 387.40 m² (4,170.07 sq. ft.).

Therefore, the proposed severance would result in the creation of two lots that do not represent the greater of the average lot frontage and area, and consequently, do not maintain the general intent and purpose of the Official Plan.

Zoning By-law

The subject property is zoned "R3" (Residential), which permits detached dwellings. Subsection 4.2.1 of Zoning By-law 0225-2007, as amended, specifies that the minimum required lot area for an interior lot is 550 m² (5,920.34 sq. ft.) and the minimum required lot area for a corner lot is 720 m² (7,750.26 sq. ft.). The minimum lot frontage for the subject property is 15.00 m (49.21 ft.) and 19.50 m (63.97 ft.) for corner lots. The retained and severed lots do not comply with the minimum required lot area and lot frontage requirements in the Zoning By-law.

Criteria for Consents

An application for consent must meet the criteria set out under subsection 51(24) of the *Planning Act*. One of the criteria for evaluating the proposal is whether or not the proposal conforms to the Official Plan. As discussed previously, the proposed severance does not conform to Section 16.1.2.1 of Mississauga Official Plan with respect to lot frontage and lot area.

While there are some lots located along Strathy Avenue that have similar lot areas to those proposed, these were not created by way of consent (Appendix 7). The intent of the Official Plan policy is to prevent the gradual division of lots which are not consistent with the character of the area. As the proposed severance does not conform to the Official Plan policies of Mississauga Official Plan, it does not meet this criterion.

Further criteria under the *Planning Act* are to have regard to the dimensions and shapes of the proposed lots. When taking into consideration the context of the surrounding area, the proposed severance would result in lots that are smaller in area than the average size of the lots along Strathy Avenue (Appendix 7). In this respect, the requested consent does not maintain the character of the neighbourhood and does not lend itself to the suitable development of lots that are appropriate in terms of size and configuration. Therefore, the proposed severance does not meet these criteria.

Notwithstanding the above, the Committee granted provisional consent, subject to conditions.

With respect to the requested minor variances, the Committee was satisfied that the request was desirable for the appropriate development of the subject property; that the general intent and purpose of the Zoning By-law and the Official Plan would be maintained; and that the requested variances were minor in nature. Accordingly, the Committee granted the requests, as presented.

Ontario Municipal Board Appeal

The Committee of Adjustment's decision to approve the consent was to be final and binding on February 5, 2013, and January 31, 2013 for the minor variances. Based on Council endorsed protocol, the Planning and Building Department prepares a Corporate Report to the Planning and Development Committee recommending that the City appeal a decision of the Committee of Adjustment, when in the Department's opinion, the decision does not maintain the general intent and purpose of the Official Plan. Accordingly, the Planning and Building Department requested that Legal Services prepare the appropriate Notice of Appeal to the Ontario Municipal Board (OMB) and file a "Placeholder" appeal prior to the appeal period expiring pending further instruction from Council.

CONCLUSION:

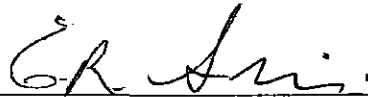
The consent approved by the Committee of Adjustment under file 'B' 5/13 W1 does not meet the general intent of Mississauga Official Plan.

The variances approved under files 'A' 9/13 and 'A' 10/13 W1 do not meet the requirements of the Zoning By-law for lot frontage or lot area, and do not conform to Section 16.1.2.1 of Mississauga Official Plan with respect to lot frontage and lot area.

These approvals by the Committee have broad implications and may have significant impacts on future development in the City, resulting in undesirable development patterns in stable residential neighbourhoods.

ATTACHMENTS:

- Appendix 1: Committee of Adjustment Decisions
'B' 5/13 W1, 'A' 9/13 and 'A' 10/13 W1
- Appendix 2: Land Use Map
- Appendix 3: Zoning Map
- Appendix 4: General Context Map
- Appendix 5: Aerial Photograph
- Appendix 6: Proposed Severance/Concept Plan
- Appendix 7: Lotting Pattern



Edward R. Sajecki
Commissioner of Planning and Building

*Prepared By: Lauren Eramo-Russo,
Committee of Adjustment Planner*

MB



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COMMITTEE OF ADJUSTMENT DECISION

APPENDIX I

City of Mississauga

'B' - 5 / 13

"B" 005/13

Ward 1

COMMITTEE OF ADJUSTMENT

IN THE MATTER OF SECTION 50(3) AND/OR (5)
of The Planning Act R.S.O. 1990, c.P.13, as amended
- and -

IN THE MATTER OF AN APPLICATION BY

FRIEDA EMMA FISCHER

on Thursday January 3, 2013

Frieda Emma Fischer is the owner of Part of Lot 117, Registered Plan K-22, located and known as 1238 Strathy Avenue, zoned R3, Residential. The applicants request the consent of the Committee to the conveyance of a parcel of land having a frontage of approximately 11.55 m (37.89 ft.) and an area of approximately 387.40 m² (4,170.07 ft²). The effect of the application is to create a new lot for residential purposes.

The subject lands are also subject to Minor Variance Applications 'A' 009/13 and 'A' 010/13.

Mr. P. Chee, authorized agent, attended and presented the application to convey a parcel of land for the creation of a new undersized residential property. Mr. Chee advised the Committee that although both the conveyed and retained lands would be undersized, appropriately sized dwellings could be constructed on each property without requiring any variances for the dwellings. Mr. Chee noted that the proposed frontages of the conveyed and retained lands would be compatible with the frontages of other properties in the surrounding neighbourhood.

The Committee reviewed the information submitted with the application.

The Committee received comments and recommendations from the following agencies:

City of Mississauga, Planning and Building Department (December 21, 2012),
City of Mississauga, Transportation and Works Department (December 20, 2012),
City of Mississauga, Community Services Department, Park Planning (December 20, 2012),
Region of Peel, Environment, Transportation and Planning Services (December 21, 2012),

A letter was received from T. Wichert, a resident of 1200 Strathy Avenue, confirming no objection to

the subject application.

A letter was received from R. Agic, a resident of 1204 Strathy Avenue, confirming no objection to the subject application.

A letter was received from A. Proulx, a resident of 1217 Strathy Avenue, confirming no objection to the subject application.

A letter was received from F. & M. De Jesus, a resident of 1226 Strathy Avenue, confirming no objection to the subject application.

A letter was received from C. Turnbunn, a resident of 1227 Strathy Avenue, confirming no objection to the subject application.

A letter was received from M. Gaspar, a resident of 1240 Strathy Avenue, confirming no objection to the subject application.

A letter was received from S. Tapp, a resident of 1247 Strathy Avenue, confirming no objection to the subject application.

A letter was received from P. Irvani, a resident of 1264 Strathy Avenue, confirming no objection to the subject application.

A letter was received from O. Dylskyi, a resident of 1208 Ogden Avenue, confirming no objection to the subject application.

A letter was received from F. Prelec, a resident of 1211 Ogden Avenue, confirming no objection to the subject application.

A letter was received from A. Hayes, a resident of 1216 Ogden Avenue, confirming no objection to the subject application.

A letter was received from L. Fillion, a resident of 1219 Ogden Avenue, confirming no objection to the subject application.

A letter was received from I. Pettipas, a resident of 1226 Ogden Avenue, confirming no objection to the subject application.

A letter was received from A. Costache, a resident of 1236 Ogden Avenue, confirming no objection to the subject application.

A letter was received from G. Cormier, a resident of 1243 Ogden Avenue, confirming no objection to

the subject application.

A letter was received from S. Borys, a resident of 1047 Atwater Avenue, confirming no objection to the subject application.

A letter was received from H. Butt, a resident of 1059 Atwater Avenue, confirming no objection to the subject application.

A letter was received from A. Moisa, a resident of 1076 Atwater Avenue, confirming no objection to the subject application.

A letter was received from L. Bontogon, a resident of 1074 Serson Avenue, confirming no objection to the subject application.

Mr. J. Lahay, a resident of 1048 Atwater Avenue, attended and expressed his objection to the subject application. Mr. Lahay noted that the various letter of support presented by Mr. Chee were from residents outside of the municipal circulation area. He noted that the division of the lot and subsequent new dwelling represented contributed to excessive residential densities in the area. It was Mr. Lahay's opinion that the subject property could accommodate one detached dwelling of modest proportions. Mr. Lahay expressed concern with any reduced separation distances between dwellings.

No other persons expressed any interest in the application.

The Secretary-Treasurer reviewed the recommended conditions for the Committee's consideration should the application be approved.

Mr. Chee consented to the imposition of the proposed conditions. Mr. Chee indicated that all variances requested in the concurrent Minor Variance applications pertaining to the development of the dwellings would be deleted. He confirmed that his client would construct dwellings in compliance with the Zoning By-law on each of the undersized properties.

The Committee, after considering the submissions put forward by Mr. Chee, the comments received and the recommended conditions, is satisfied that a plan of subdivision is not necessary for the proper and orderly development of the municipality.

The Committee, having regard to those matters under subsection 51(24) of the Planning Act R.S.O. 1990, c. P.13., as amended, resolves to grant provisional consent subject to the following conditions being fulfilled:

1. Approval of the draft reference plan(s), as applicable, shall be obtained at the Committee of Adjustment office, and; the required number of prints of the resultant deposited reference plan(s) shall

be received.

2. An application amendment letter shall be received from the applicant or authorized agent confirming that the "severed" land shall be together with and/or subject to services easement(s) and/or right(s)-of-way, if necessary, in a location and width as determined by the Secretary-Treasurer based on written advice from the agencies having jurisdiction for any service or right for which the easement or right-of-way is required; alternatively, a letter shall be received from the applicant or authorized agent confirming that no services easement(s) and/or right(s)-of-way, are necessary.

3. A letter shall be received from the City of Mississauga, Transportation and Works Department, indicating that satisfactory arrangements have been made with respect to the matters addressed in their comments dated December 20, 2013.

4. A letter shall be received from the City of Mississauga, Manager/Supervisor, Zoning Plan Examination, indicating that the "severed" land and "retained" land comply with the provisions of the Zoning By-law, or alternatively; that any variances are approved by the appropriate authorities and that such approval is final and binding. ("A" 009/13 & "A" 010/13)

5. A letter shall be received from the City of Mississauga, Community Services Department, indicating that satisfactory arrangements have been made with respect to the matters addressed in their comments dated December 20, 2013.

6. A letter shall be received from the Region of Peel, Environment, Transportation and Planning Services, indicating that satisfactory arrangements have been made with respect to the matters addressed in their comments dated December 21, 2013.

MOVED BY:

J. Robinson SECONDED BY: R. Bennett CARRIED

Application Approved on conditions as stated.

Dated at the City of Mississauga on January 10, 2013.

THIS DECISION IS SUBJECT TO APPEAL TO THE ONTARIO MUNICIPAL BOARD BY FILING WITH THE SECRETARY-TREASURER OF THE COMMITTEE OF ADJUSTMENT A WRITTEN NOTIFICATION, GIVING REASONS FOR THE APPEAL, ACCOMPANIED WITH THE PRESCRIBED FEE ON OR BEFORE FEBRUARY 3, 2013.

Date of mailing is January 14, 2013.

S. PATRIZIO (CHAIR)
D. GEORGE

R. BENNETT
J. THOMAS

D. KENNEDY
L. DAHONICK

J. ROBINSON

I certify this to be a true copy of the Committee's decision given on January 10, 2013.

DAVID L. MARTIN, SECRETARY-TREASURER

NOTES:

The decision to give provisional consent shall be deemed to be refused if the conditions of provisional consent, have not been fulfilled on or before January 14, 2014.

See "SUMMARY OF APPEAL PROCEDURES" and "FULFILLING CONDITIONS & CERTIFICATE ISSUANCE" attached.



COMMITTEE OF ADJUSTMENT DECISION

City of Mississauga

'A' - 9 / 13

"A" 009/13

Ward 1

COMMITTEE OF ADJUSTMENT

IN THE MATTER OF SECTION 45(1) OR (2)
of The Planning Act R.S.O. 1990, c.P.13, as amended
- and -
IN THE MATTER OF ZONING BY-LAW 0225-2007
as amended
- and -
IN THE MATTER OF AN APPLICATION BY

FRIEDA EMMA FISCHER

on Thursday January 3, 2013

Frieda Emma Fischer is the owner of Part of Lot 117, Registered Plan K-22, located and known as 1238 Strathy Avenue, zoned R3, Residential. The applicant request the Committee to authorize a minor variance to permit the construction of a new two (2) storey detached dwelling on the subject property, being the conveyed lands of Consent Application 'B' 005/13, proposing:

1. a lot frontage of 11.55 m (37.89 ft); whereas By-law 0225-2007, as amended, requires a minimum lot frontage of 15.00 m (49.21 ft) in this instance,
2. a lot area of 387.40 m² (4,170.07 ft²); whereas By-law 0225-2007, as amended, requires a minimum lot area of 550.00 m² (5,920.34 ft²) in this instance,
3. a lot coverage of 40% of the lot area; whereas By-law 0225-2007, as amended, permits a maximum lot coverage of 35% of the lot area in this instance,
4. a front yard to the garage of 6.00 m (19.68 ft); whereas By-law 0225-2007, as amended, requires a minimum front yard to the garage of 7.50 m (24.60 ft) in this instance; and,
5. a southerly side yard of 1.20 m (3.93 ft); whereas By-law 0225-2007, as amended, requires a minimum side yard of 1.81 m (5.93 ft) in this instance.

Mr. P. Chee, authorized agent, attended and presented the application to allow for the creation of a new undersized residential property and for the construction of an oversized dwelling on the resultant

property. Mr. Chee noted that the proposed frontage and lot area were compatible with the existing lot pattern of the surrounding properties. He noted that the proposed dwelling was compatible with the size and scale of other dwellings on this portion of Strathly Avenue.

The Committee reviewed the information and plans submitted with the application.

The City of Mississauga Planning and Building Department commented as follows (December 21, 2012):

71.0 RECOMMENDATION

The Planning and Building Department recommends that the consent and minor variance applications be refused.

2.0 BACKGROUND

Mississauga Official Plan

Character Area: Lakeview Neighbourhood

Designation: Residential Low Density II

Discussion:

Lakeview is an established and stable residential Neighbourhood. To satisfy compatibility concerns, any proposed development is required to recognize and enhance the scale and character of the existing residential areas by having regard to lot frontages and areas, among other matters. Mississauga Official Plan encourages development in neighbourhoods to be context sensitive and respect the existing or planned character and scale of development.

To preserve the character of lands designated Residential Low Density I and Residential Low Density II, the minimum frontage and area of new lots proposed will generally represent the greater of the average lot frontage and area within 120 m, or the requirements of the Zoning By-law. The requested consent and minor variance applications will result in lots that are less than the average lot frontage within 120 m of the subject property.

The requested severance does not recognize or enhance the scale and character of the existing residential area or streetscape with respect to lot frontage or area, and therefore, does not satisfy compatibility concerns as outlined in the Mississauga Official Plan.

Zoning By-law 0225-2007

Zoning: "R3", Residential

Discussion:

The intent of the Zoning By-law is to ensure that new lots and dwellings are designed in a manner that respects the character of the area. The proposed lot frontages of 11.55 m (37.89 ft.) and lot areas of 387.4 m² (4170.07 sq ft.) are significantly less than the existing lots along Strathly Avenue. The cumulative variances that result from the proposed severance, which include insufficient lot frontage, insufficient lot area, excessive lot coverage, and dwellings with insufficient side yards, do not maintain

the intent of the Zoning By-law.

3.0 OTHER APPLICATIONS

0 Building Permit File: Required - No application received

4.0 COMMENTS

We note that in the absence of Building Permit applications for the subject dwellings, we are unable to confirm the accuracy of the requested variances or determine whether additional variances will be required. In addition, we advise that a demolition permit is required for the existing dwelling. We advise that we have serious concerns with the consent application and associated Minor Variance applications since the proposed lots are not consistent in size and character with the other properties along Strathy Avenue.

Further, we have serious concerns with the associated minor variance applications as approval would set an undesirable precedent for lots that are not in keeping with the established character of the streetscape. We advise that the proposed lot frontages of 11.55 m (37.89 ft.) are 3.45 m (11.31 ft.) less than the Zoning By-law requirement, which is not suitable for this established area. The resulting variances for the new dwellings, which include reduced lot frontages, lot areas, excessive lot coverage, and reduced side yard setbacks, are reflective of the inappropriateness of the proposed severance. As such, we advise that the requested variances are not minor in nature nor desirable for the appropriate development of the subject property.

In addition, we advise that due to the number of trees located on the subject property, this Department requires a Tree Inventory/ Preservation Plan be submitted prior to any division of land. The purpose of this report is to provide an inventory of the trees on the property, including the size, condition, and species. This will indicate which trees may be removed to accommodate construction, and which trees could be preserved.

The City of Mississauga Transportation and Works Department commented as follows (December 19, 2012):

?We are noting that any Transportation and Works Department concerns/requirements for this property will be addressed under Consent Application 'B' 5/13.?

A letter was received from T. Wichert, a resident of 1200 Strathy Avenue, confirming no objection to the subject application.

A letter was received from R. Agic, a resident of 1204 Strathy Avenue, confirming no objection to the subject application.

A letter was received from A. Proulx, a resident of 1217 Strathy Avenue, confirming no objection to the subject application.

A letter was received from F. & M. De Jesus, a resident of 1226 Strathy Avenue, confirming no

objection to the subject application.

A letter was received from C. Turnbunn, a resident of 1227 Strathy Avenue, confirming no objection to the subject application.

A letter was received from K. Zejmo, a resident of 1230 Strathy, expressing an interest in the subject application.

A letter was received from M. Gaspar, a resident of 1240 Strathy Avenue, confirming no objection to the subject application.

A letter was received from S. Tapp, a resident of 1247 Strathy Avenue, confirming no objection to the subject application.

A letter was received from P. Irvani, a resident of 1264 Strathy Avenue, confirming no objection to the subject application.

A letter was received from O. Dylskyi, a resident of 1208 Ogden Avenue, confirming no objection to the subject application.

A letter was received from F. Prelec, a resident of 1211 Ogden Avenue, confirming no objection to the subject application.

A letter was received from A. Hayes, a resident of 1216 Ogden Avenue, confirming no objection to the subject application.

A letter was received from L. Filion, a resident of 1219 Ogden Avenue, confirming no objection to the subject application.

A letter was received from I. Pettipas, a resident of 1226 Ogden Avenue, confirming no objection to the subject application.

A letter was received from A. Costache, a resident of 1236 Ogden Avenue, confirming no objection to the subject application.

A letter was received from G. Cormier, a resident of 1243 Ogden Avenue, confirming no objection to the subject application.

A letter was received from S. Borys, a resident of 1047 Atwater Avenue, confirming no objection to the subject application.

A letter was received from H. Butt, a resident of 1059 Atwater Avenue, confirming no objection to the

subject application.

A letter was received from A. Moisa, a resident of 1076 Atwater Avenue, confirming no objection to the subject application.

A letter was received from L. Bontogon, a resident of 1074 Serson Avenue, confirming no objection to the subject application.

Mr. J. Lahay, a resident of 1048 Atwater Avenue, attended and expressed his objection to the subject application. Mr. Lahay noted that the various letter of support presented by Mr. Chee were from residents outside of the municipal circulation area. He noted that the division of the lot and subsequent new dwelling represented contributed to excessive residential densities in the area. It was Mr. Lahay's opinion that the subject property could accommodate one detached dwelling of modest proportions. Mr. Lahay expressed concern with any reduced separation distances between dwellings.

No other persons expressed any interest in the application.

Mr. Chee indicated that he wished to amend the application to delete the requested variances for the proposed dwelling. He confirmed an adequately sized dwelling could be constructed on the conveyed lands in compliance with the Zoning By-law. Mr. Chee suggested that this was indicative of the appropriateness of the proposed lot frontage and area.

The Committee consented to the request and, after considering the submissions put forward by Mr. Chee and having reviewed the plans and comments received, is satisfied that the amended request is desirable for the appropriate further development of the subject property.

The Committee is satisfied that the general intent and purpose of the Zoning By-law and the Official Plan will be maintained in this instance.

The Committee is of the opinion that the amended request is minor in nature in this instance.

Accordingly, the Committee resolves to authorize and grant the amended request to permit the construction of a new two (2) storey detached dwelling on the subject property, being the conveyed lands of Consent Application 'B' 005/13, proposing:

1. a lot frontage of 11.55 m (37.89 ft); whereas By-law 0225-2007, as amended, requires a minimum lot frontage of 15.00 m (49.21 ft) in this instance; and,
2. a lot area of 387.40 m² (4,170.07 ft²); whereas By-law 0225-2007, as amended, requires a minimum lot area of 550.00 m² (5,920.34 ft²) in this instance.

MOVED BY:

J. Robinson SECONDED BY: R. Bennett CARRIED

Application Approved, as amended.

Dated at the City of Mississauga on January 10, 2013.

THIS DECISION IS SUBJECT TO APPEAL TO THE ONTARIO MUNICIPAL BOARD BY FILING WITH THE SECRETARY-TREASURER OF THE COMMITTEE OF ADJUSTMENT A WRITTEN NOTIFICATION, GIVING REASONS FOR THE APPEAL, ACCOMPANIED WITH THE PRESCRIBED FEE ON OR BEFORE JANUARY 30, 2013.

Date of mailing is January 14, 2013.

S. PATRIZIO (CHAIR)
D. GEORGE

R. BENNETT
J. THOMAS

D. KENNEDY
L. DAHONICK

J. ROBINSON

I certify this to be a true copy of the Committee's decision given on January 10, 2013.

DAVID L. MARTIN, SECRETARY-TREASURER

A copy of Section 45 of the Planning Act, as amended, is attached.

NOTES:

- A Development Charge may be payable prior to the issuance of a Building Permit.
- Further approvals from the City of Mississauga may be required i.e. a Building Permit, a Zoning Certificate, a License, etc.

6-19



COMMITTEE OF ADJUSTMENT DECISION

City of Mississauga

'A' - 10 / 13

"A" 010/13

Ward 1

COMMITTEE OF ADJUSTMENT

IN THE MATTER OF SECTION 45(1) OR (2)
of The Planning Act R.S.O. 1990, c.P.13, as amended

- and -

IN THE MATTER OF ZONING BY-LAW 0225-2007
as amended

- and -

IN THE MATTER OF AN APPLICATION BY

FRIEDA EMMA FISCHER

on Thursday January 3, 2013

Frieda Emma Fischer is the owner of Part of Lot 117, Registered Plan K-22, located and known as 1238 Strathly Avenue, zoned R3, Residential. The applicant request the Committee to authorize a minor variance to permit the construction of a new two (2) storey detached dwelling on the subject property, being the retained lands of Consent Application 'B' 005/13, proposing:

1. a lot frontage of 11.55 m (37.89 ft.); whereas By-law 0225-2007, as amended, requires a minimum lot frontage of 15.00 m (49.21 ft.) in this instance,
2. a lot area of 387.40 m² (4,170.07 ft²); whereas By-law 0225-2007, as amended, requires a minimum lot area of 550.00 m² (5,920.34 ft²) in this instance,
3. a lot coverage of 40% of the lot area; whereas By-law 0225-2007, as amended, permits a maximum lot coverage of 35% of the lot area in this instance,
4. a front yard to the garage of 6.00 m (19.68 ft.); whereas By-law 0225-2007, as amended, requires a minimum front yard to the garage of 7.50 m (24.60 ft.) in this instance; and,
5. a southerly side yard of 1.20 m (3.93 ft.); whereas By-law 0225-2007, as amended, requires a minimum side yard of 1.81 m (5.93 ft.) in this instance.

Mr. P. Chee, authorized agent, attended and presented the application to allow for the retained lands to remain and to allow for the construction of an oversized dwelling on the retained lands. Mr. Chee noted

that the proposed frontage and lot area were compatible with the existing lot pattern of the surrounding properties. He noted that the proposed dwelling was compatible with the size and scale of other dwellings on this portion of Strath Avenue.

The Committee reviewed the information and plans submitted with the application.

The City of Mississauga Planning and Building Department commented as follows (December 21, 2012):

71.0 RECOMMENDATION

The Planning and Building Department recommends that the consent and minor variance applications be refused.

2.0 BACKGROUND

Mississauga Official Plan

Character Area: Lakeview Neighbourhood

Designation: Residential Low Density II

Discussion:

Lakeview is an established and stable residential Neighbourhood. To satisfy compatibility concerns, any proposed development is required to recognize and enhance the scale and character of the existing residential areas by having regard to lot frontages and areas, among other matters. Mississauga Official Plan encourages development in neighbourhoods to be context sensitive and respect the existing or planned character and scale of development.

To preserve the character of lands designated Residential Low Density I and Residential Low Density II, the minimum frontage and area of new lots proposed will generally represent the greater of the average lot frontage and area within 120 m, or the requirements of the Zoning By-law. The requested consent and minor variance applications will result in lots that are less than the average lot frontage within 120 m of the subject property.

The requested severance does not recognize or enhance the scale and character of the existing residential area or streetscape with respect to lot frontage or area, and therefore, does not satisfy compatibility concerns as outlined in the Mississauga Official Plan.

Zoning By-law 0225-2007

Zoning: "R3", Residential

Discussion:

The intent of the Zoning By-law is to ensure that new lots and dwellings are designed in a manner that respects the character of the area. The proposed lot frontages of 11.55 m (37.89 ft.) and lot areas of

387.4 m² (4170.07 sq ft.) are significantly less than the existing lots along Strathly Avenue. The cumulative variances that result from the proposed severance, which include insufficient lot frontage, insufficient lot area, excessive lot coverage, and dwellings with insufficient side yards, do not maintain the intent of the Zoning By-law.

3.0 OTHER APPLICATIONS

0 Building Permit File: Required - No application received

4.0 COMMENTS

We note that in the absence of Building Permit applications for the subject dwellings, we are unable to confirm the accuracy of the requested variances or determine whether additional variances will be required. In addition, we advise that a demolition permit is required for the existing dwelling. We advise that we have serious concerns with the consent application and associated Minor Variance applications since the proposed lots are not consistent in size and character with the other properties along Strathly Avenue.

Further, we have serious concerns with the associated minor variance applications as approval would set an undesirable precedent for lots that are not in keeping with the established character of the streetscape. We advise that the proposed lot frontages of 11.55 m (37.89 ft.) are 3.45 m (11.31 ft.) less than the Zoning By-law requirement, which is not suitable for this established area. The resulting variances for the new dwellings, which include reduced lot frontages, lot areas, excessive lot coverage, and reduced side yard setbacks, are reflective of the inappropriateness of the proposed severance. As such, we advise that the requested variances are not minor in nature nor desirable for the appropriate development of the subject property.

In addition, we advise that due to the number of trees located on the subject property, this Department requires a Tree Inventory/ Preservation Plan be submitted prior to any division of land. The purpose of this report is to provide an inventory of the trees on the property, including the size, condition, and species. This will indicate which trees may be removed to accommodate construction, and which trees could be preserved.

The City of Mississauga Transportation and Works Department commented as follows (December 19, 2012):

?We are noting that any Transportation and Works Department concerns/requirements for this property will be addressed under Consent Application 'B' 5/13.?

A letter was received from T. Wichert, a resident of 1200 Strathly Avenue, confirming no objection to the subject application.

A letter was received from R. Agic, a resident of 1204 Strathly Avenue, confirming no objection to the subject application.

A letter was received from A. Proulx, a resident of 1217 Strathly Avenue, confirming no objection to the

subject application.

A letter was received from F. & M. De Jesus, a resident of 1226 Strathy Avenue, confirming no objection to the subject application.

A letter was received from C. Turnbunn, a resident of 1227 Strathy Avenue, confirming no objection to the subject application.

A letter was received from K. Zejmo, a resident of 1230 Strathy, expressing an interest in the subject application.

A letter was received from M. Gaspar, a resident of 1240 Strathy Avenue, confirming no objection to the subject application.

A letter was received from S. Tapp, a resident of 1247 Strathy Avenue, confirming no objection to the subject application.

A letter was received from P. Irvani, a resident of 1264 Strathy Avenue, confirming no objection to the subject application.

A letter was received from O. Dylskyl, a resident of 1208 Ogden Avenue, confirming no objection to the subject application.

A letter was received from F. Prelec, a resident of 1211 Ogden Avenue, confirming no objection to the subject application.

A letter was received from A. Hayes, a resident of 1216 Ogden Avenue, confirming no objection to the subject application.

A letter was received from L. Filion, a resident of 1219 Ogden Avenue, confirming no objection to the subject application.

A letter was received from I. Petipas, a resident of 1226 Ogden Avenue, confirming no objection to the subject application.

A letter was received from A. Costache, a resident of 1236 Ogden Avenue, confirming no objection to the subject application.

A letter was received from G. Cormier, a resident of 1243 Ogden Avenue, confirming no objection to the subject application.

A letter was received from S. Borys, a resident of 1047 Atwater Avenue, confirming no objection to the

subject application.

A letter was received from H. Butt, a resident of 1059 Atwater Avenue, confirming no objection to the subject application.

A letter was received from A. Moisa, a resident of 1076 Atwater Avenue, confirming no objection to the subject application.

A letter was received from L. Bantogon, a resident of 1074 Serson Avenue, confirming no objection to the subject application.

Mr. J. Lahay, a resident of 1048 Atwater Avenue, attended and expressed his objection to the subject application. Mr. Lahay noted that the various letter of support presented by Mr. Chee were from residents outside of the municipal circulation area. He noted that the division of the lot and subsequent new dwelling represented contributed to excessive residential densities in the area. It was Mr. Lahay's opinion that the subject property could accommodate one detached dwelling of modest proportions. Mr. Lahay expressed concern with any reduced separation distances between dwellings.

No other persons expressed any interest in the application.

Mr. Chee indicated that he wished to amend the application to delete the requested variances for the proposed dwelling. He confirmed an adequately sized dwelling could be constructed on the retained lands in compliance with the Zoning By-law. Mr. Chee suggested that this was indicative of the appropriateness of the proposed lot frontage and area.

The Committee consented to the request and, after considering the submissions put forward by Mr. Chee and having reviewed the plans and comments received, is satisfied that the amended request is desirable for the appropriate further development of the subject property.

The Committee is satisfied that the general intent and purpose of the Zoning By-law and the Official Plan will be maintained in this instance.

The Committee is of the opinion that the amended request is minor in nature in this instance.

Accordingly, the Committee resolves to authorize and grant the amended request to permit the construction of a new two (2) storey detached dwelling on the subject property, being the retained lands of Consent Application 'B' 005/13, proposing:

1. a lot frontage of 11.55 m (37.89 ft.); whereas By-law 0225-2007, as amended, requires a minimum lot frontage of 15.00 m (49.21 ft.) in this instance; and,

2. a lot area of 387.40 m² (4,170.07 ft²); whereas By-law 0225-2007, as amended, requires a minimum lot area of 550.00 m² (5,920.34 ft²) in this instance.

MOVED BY:

J. Robinson SECONDED BY: R. Bennett CARRIED

Application Approved, as amended.

Dated at the City of Mississauga on January 10, 2013.

THIS DECISION IS SUBJECT TO APPEAL TO THE ONTARIO MUNICIPAL BOARD BY FILING WITH THE SECRETARY-TREASURER OF THE COMMITTEE OF ADJUSTMENT A WRITTEN NOTIFICATION, GIVING REASONS FOR THE APPEAL, ACCOMPANIED WITH THE PRESCRIBED FEE ON OR BEFORE JANUARY 30, 2013.

Date of mailing is January 14, 2013.

S. PATRIZIO (CHAIR)
D. GEORGE

R. BENNETT
J. THOMAS

D. KENNEDY
L. DAHONICK

J. ROBINSON

I certify this to be a true copy of the Committee's decision given on January 10, 2013.

DAVID L. MARTIN, SECRETARY-TREASURER

A copy of Section 45 of the Planning Act, as amended, is attached.

NOTES:

6-25

APPENDIX 1
PAGE 18

- A Development Charge may be payable prior to the issuance of a Building Permit.
- Further approvals from the City of Mississauga may be required i.e. a Building Permit, a Zoning Certificate, a License, etc.

PART OF SCHEDULE 10 LAND USE DESIGNATIONS OF MISSISSAUGA OFFICIAL PLAN

LAND USE DESIGNATIONS

	Residential Low Density I		Business Employment
	Residential Low Density II		Industrial
	Residential Medium Density		Airport
	Residential High Density		Institutional
	Downtown Mixed Use		Public Open Space
	Downtown Core Commercial		Private Open Space
	Mixed Use		Greenbelt
	Convenience Commercial		Parkway Belt West
	Motor Vehicle Commercial		Utility
	Office		To Be Determined

LAND USE LEGEND

	Heritage Conservation District		Civic Centre (City Hall)
	1996 NEP/2000 NEF Composite Noise Contours		City Centre Transit Terminal
	LBPIA Operating Area Boundary See Aircraft Noise Policies		GO Rail Transit Station
	Area Exempt from LBPIA Operating Area		Public School
	Natural Hazards		Catholic School
			Hospital
			Community Facilities

CITY STRUCTURE

Elements

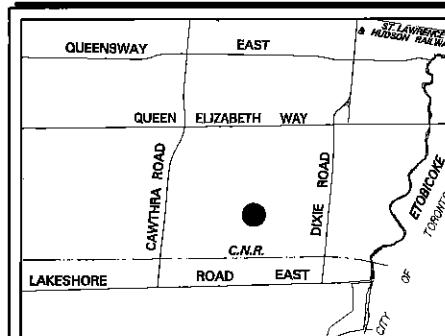
	Downtown		Corporate Centre
	Major Node		Employment Area
	Community Node		Special Purpose Area
	Neighbourhood		

ND2 Region of Peel Non-Decision

SUBJECT LANDS
1238 STRATHY AVENUE

SUBJECT:

FREIDA EMMA FISCHER



FILE NO:
'B'513, 'A'913, 'A'1013

DWG. NO:
B513 A913 A1013L

SCALE:
1:20000

DATE:
6 02 2013

DRAWN BY:
B. KRUGER

APPENDIX 2

MISSISSAUGA
Planning and Building

Produced by
T&W, Geomatics

6-26



R3
DETACHED
DWELLINGS

OS1
OPEN
SPACE

OGDEN
AVENUE

R3
DETACHED
DWELLINGS

STRATHY
AVENUE

R3
DETACHED
DWELLINGS

BALMENT AVENUE

G1
GREENBELT

R3
DETACHED
DWELLINGS

ATWATER AVENUE

ATWATER AVENUE

R3
DETACHED
DWELLINGS

33.51
R3
23.26 23.26
33.51

R3
DETACHED
DWELLINGS

GARRARD AVENUE

SERSON AVENUE

R3
DETACHED
DWELLINGS

R3
DETACHED
DWELLINGS

R3
DETACHED
DWELLINGS

AVENUE

AVENUE

PELHAM AVENUE

PELHAM AVENUE

R3
DETACHED
DWELLINGS

R3
DETACHED
DWELLINGS

R3
DETACHED
DWELLINGS

OGDEN

STRATHY

SEARSON CREEK

OS1
OPEN
SPACE

LEGEND:



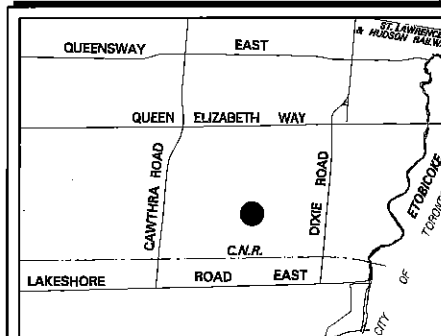
SUBJECT LANDS
1238 STRATHY AVENUE

6-27



SUBJECT:

FREIDA EMMA FISCHER



FILE NO:
'B'513, 'A'913, 'A'1013

DWG. NO:
B513 A913 A1013R

SCALE:
1:20000

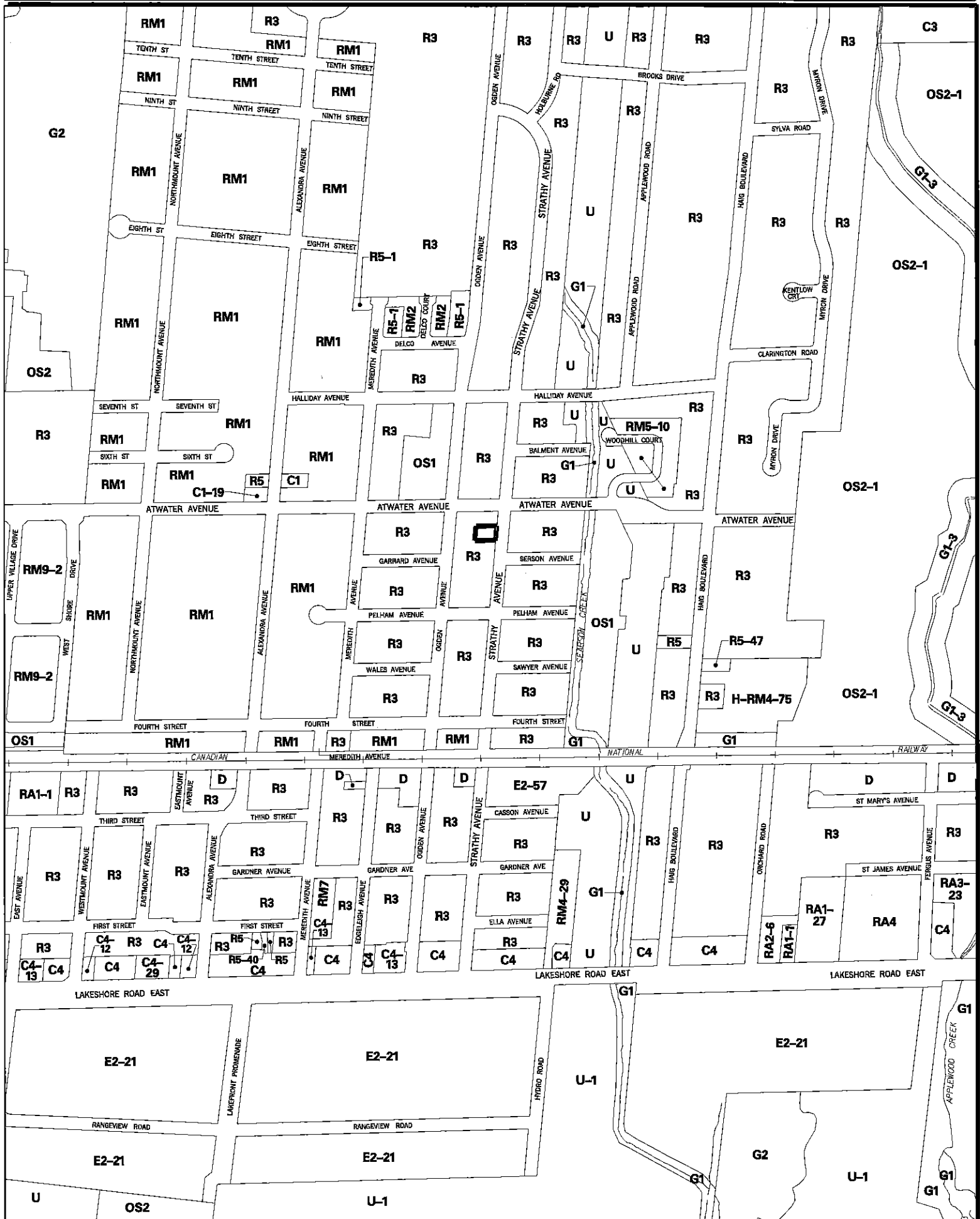
PDC DATE:
6 02 2013

DRAWN BY:
B. KRUGER

APPENDIX 3

MISSISSAUGA
Planning and Building

Produced by
T&W, Geomatics



LEGEND:



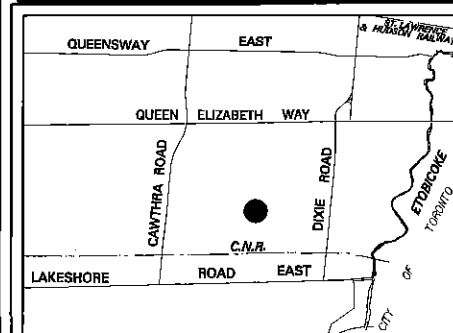
SUBJECT LANDS
1238 STRATHY AVENUE

6-29



SUBJECT:

FREIDA EMMA FISCHER



FILE NO:
'B'513, 'A'913, 'A'1013

DWG. NO:
B513 A913 A1013A

SCALE:
1:20000

PDC DATE:
6 02 2013

DRAWN BY:
B. KRUGER

APPENDIX 5

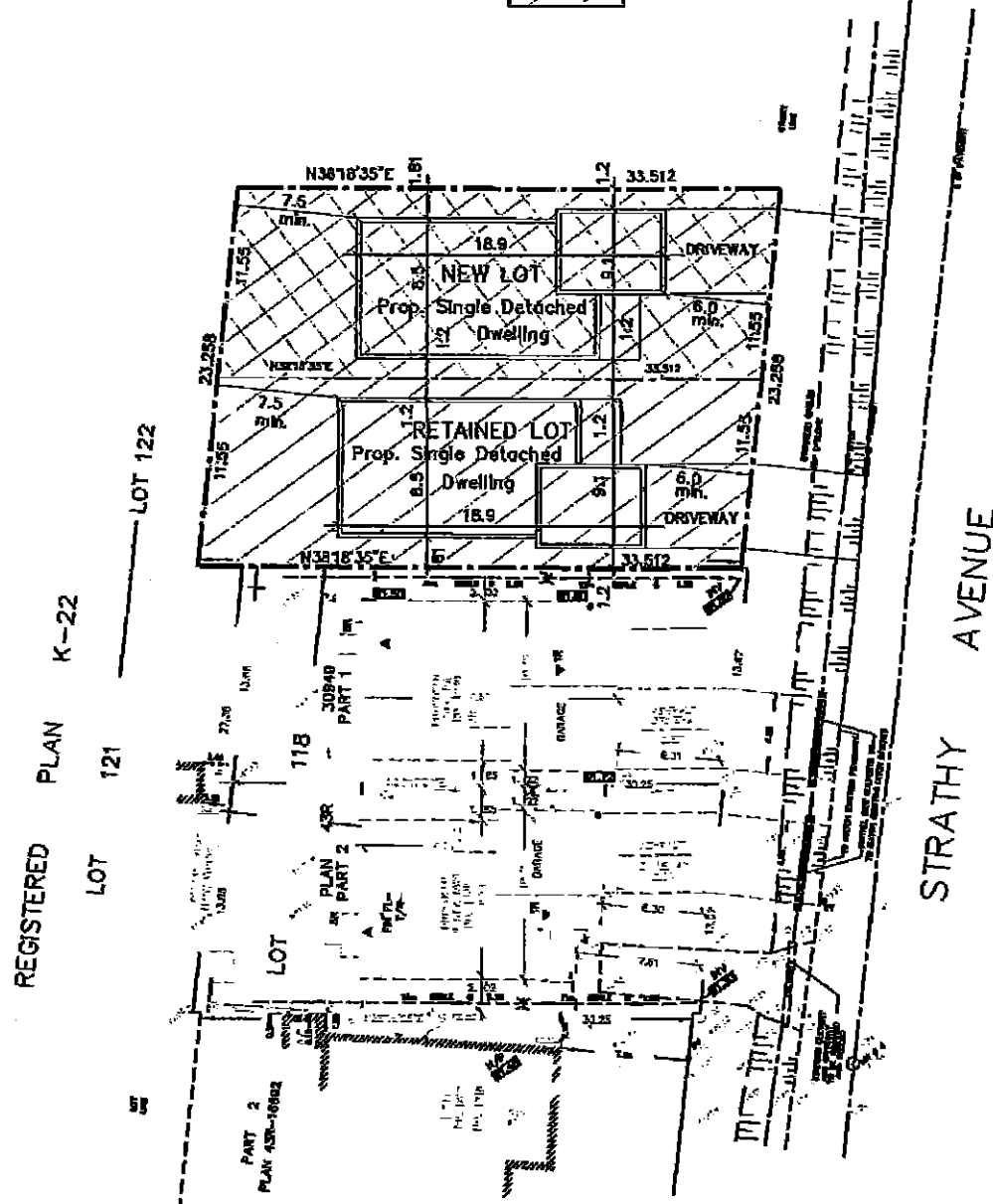
MISSISSAUGA
Planning and Building

Produced by
T&W, Geomatics

SITE SCHEDULE	
	PAPER 1 AND 2 PLAN 300
WELL OF OPEN LIFE	307.4m
WELL OF CLOSED LIFE	307.4m
WELL OF OPEN LIFE AND OPEN LIFE	(40 %)

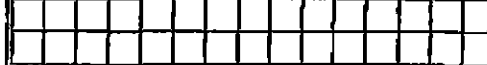
NEW LOT ☒

RETAINED LOT



SKETCH FOR COMMITTEE OF
ADJUSTMENT APPLICATIONS FOR
PART OF LOT 117
REGISTERED PLAN K-22
1238 STRATHY AVENUE
CITY OF MISSISSAUGA
REGIONAL MUNICIPALITY OF PEEL
SCALE = 1 : 350 (m)
Date: November 28, 2012

Mi-Ko Urban Consulting
Professional Planning Consultants Inc.



16 HIGH STREET
TORONTO, ONTARIO
M5Y 3N8

FH 418-230-0035

EMAIL: mikeurban@rogers.com

DETACHED DWELLINGS

OPEN SPACE

OGDEN AVENUE

DETACHED DWELLINGS

STRATHY AVENUE

DETACHED DWELLINGS

BALMENT AVENUE

GREENBELT

DETACHED DWELLINGS

ATWATER AVENUE

ATWATER AVENUE

DETACHED DWELLINGS

33.51
23.26
33.51
23.26

DETACHED DWELLINGS

GARRARD AVENUE

SERSON AVENUE

DETACHED DWELLINGS

DETACHED DWELLINGS

DETACHED DWELLINGS

PELHAM AVENUE

PELHAM AVENUE

DETACHED DWELLINGS

OGDEN AVENUE

DETACHED DWELLINGS

STRATHY AVENUE

DETACHED DWELLINGS

SEARSON CREEK

OPEN SPACE

LEGEND:



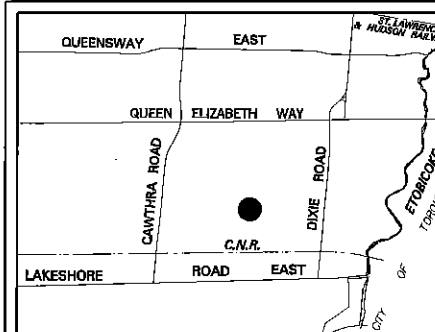
SUBJECT LANDS
1238 STRATHY AVENUE

6-31



SUBJECT:

FREIDA EMMA FISCHER



FILE NO:

'B'5'13, 'A'9'13, 'A'10'13

DWG. NO:

B513 A913 A1013lots

SCALE:

1:20000

PDC DATE:

6 02 2013

DRAWN BY:

B. KRUGER

APPENDIX 7

MISSISSAUGA
Planning and Building

Produced by
T&W, Geomatics



Corporate Report

PLANNING & DEVELOPMENT COMMITTEE
APR 02 2013 Clerk's Files

Originator's
Files OZ 12/002 W7

DATE: March 12, 2013

TO: Chair and Members of Planning and Development Committee
Meeting Date: April 2, 2013

FROM: Edward R. Sajecki
Commissioner of Planning and Building

SUBJECT: **Information Report**
Rezoning Application
To permit eight (8) detached dwellings on a CEC Private Road
2167 Gordon Drive
East side of Gordon Drive, south of Queensway West
Owner: Raffi Konialian
Applicant: Weston Consulting Group Inc.
Bill 51

Public Meeting **Ward 7**

RECOMMENDATION: That the Report dated March 12, 2013, from the Commissioner of Planning and Building regarding the application to change the Zoning from "R1-7" (Detached Dwellings - Typical Lots) to "R16 - Exception" (Detached Dwellings on a CEC Private Road) and "G1" (Greenbelt - Natural Hazards), to permit eight (8) detached dwellings on a CEC Private Road under file OZ 12/002 W7, Raffi Konialian, 2167 Gordon Drive, east side of Gordon Drive, south of Queensway West, be received for information.

**REPORT
HIGHLIGHTS:**

- Community concerns identified to date relate to maintaining the existing character of the area, protecting the Residential Woodland, and setting a precedent for future development within the Gordon Woods area.
- Prior to the preparation of a Supplementary Report, matters to be addressed include outstanding department and agency comments, resolution of tree preservation concerns, urban design considerations and the submission and review of supporting information and studies.

BACKGROUND:

The application has been circulated for technical comments and a community open house has been held.

The purpose of this report is to provide preliminary information on the application and to seek comments from the community.

COMMENTS:

Details of the proposal are as follows:

Development Proposal	
Application submitted:	February 13, 2012
Application Complete:	March 8, 2012
Application Revised:	November 30, 2012
Existing Gross Floor Area:	295 m ² (3,175.5 sq. ft.) – existing dwelling to be demolished
Height:	2 storeys
Lot Coverage:	35%
Net Density:	10.2 units/ha 4.13 units/acre
Number of units:	8 (1 additional detached dwelling proposed, but not included in the subject application, to be built fronting Gordon Drive in accordance with the existing “R1-7” zone)

Development Proposal	
Gross Floor Area:	450 to 500 m ² (4,844 to 5,382 sq. ft.) per unit
Anticipated Population:	27* *Average household sizes for all units (by type) for the year 2011 (city average) based on the 2008 Growth Forecasts for the City of Mississauga.
Parking Required:	2.0 resident spaces per unit (16 spaces) 0.25 visitor spaces per unit (2 spaces) Total: 18 spaces
Parking Provided:	4.0 resident spaces per unit (32 spaces) 0.37 visitor spaces per unit (3 spaces) Total: 35 spaces
Supporting Documents:	Functional Servicing Report and Stormwater Management Brief Phase 1 Environmental Site Assessment, Noise Control Study Scoped Environmental Impact Study Meander Belt Width Assessment: Mary Fix Creek Preliminary Tree Preservation Report Green Features List Parcel Registry Documentation Planning Justification Report

Site Characteristics	
Frontage:	21.5 m (70.5 ft.)
Depth:	216.56 m (710.5 ft.)
Net Lot Area:	0.784 ha (1.937 ac.) – Lot 1 is excluded
Existing Use:	Detached Dwelling

Green Development Initiatives

No green initiatives beyond current planning and building code requirements have been identified.

Additional information is provided in Appendices I-1 to I-11.

Neighbourhood Context

The subject property is located in the Gordon Woods community, a low density neighbourhood characterized by detached dwellings on large well-treed lots with large front, rear and side yard setbacks and a rural road profile. Mary Fix Creek transects the property near the eastern terminus of the subject lands. The natural hazard associated with the Creek is to be conveyed to the City and preserved in a natural state. Although unauthorized tree removal was undertaken on the table land portion of the subject land by the previous owner, the site remains well-treed. Information regarding the history of the site is found in Appendix I-1.

The surrounding land uses are described as follows:

North: Detached dwellings on large, well-treed lots
East: Trillium Health Centre
South: Detached dwellings on large, well-treed lots
West: Beyond Gordon Drive are detached dwellings on large, well-treed lots

Mississauga Official Plan Designation and Policies for the Cooksville Neighbourhood Character Area (November 14, 2012)

Mississauga Official Plan (2011) was adopted by City Council on September 29, 2010 and partially approved by the Region of Peel on September 22, 2011. The Plan was appealed in its entirety, however, on November 14, 2012, the Ontario Municipal Board issued a Notice of Decision approving Mississauga Official Plan, as modified, save and except for certain appeals which have no effect on the subject application.

The subject lands are located within the **Cooksville Neighbourhood Character Area** and are designated "**Residential Low Density I**" and "**Greenbelt**".

"**Residential Low Density I**" permits detached, semi-detached and duplex dwellings. Notwithstanding the general provisions for

the "Residential Low Density I" designation, only detached dwellings are permitted by the **Cooksville Neighbourhood Character Area** Policies as they pertain to the subject lands.

"Greenbelt" lands are generally associated with natural hazards or areas where development is restricted.

The lands are also part of **Special Site 4** within the **Cooksville Neighbourhood Character Area** Policies, which outline additional policies directing development to generally maintain and enhance the existing established character of the area. These and other policies in Mississauga Official Plan which are applicable in the review of this application have been outlined in Appendix I-9.

An Amendment to the Official Plan is not proposed in support of the proposed development.

Existing Zoning

"R1-7" (Detached Dwellings - Typical Lots), which permits detached dwellings with a minimum lot area of 1 140 m² (12,271 sq. ft.), a minimum lot frontage of 30.0 m (98.4 ft.) and a maximum lot coverage of 25%. **"G1" (Greenbelt – Natural Hazards)**, which permits flood control, stormwater and erosion management and Natural Heritage Features and Areas conservation.

Proposed Zoning By-law Amendment

"R16 – Exception" (Detached Dwellings on a CEC Private Road), to permit eight (8) detached dwellings on a CEC private road. Specific zone provision are contained within Appendix I-10. **"G1" (Greenbelt – Natural Hazards)** to reflect the limits of development associated with Mary Fix Creek.

COMMUNITY ISSUES

A community open house was held by Ward 7 Councillor, Nando Iannicca, on June 7, 2012. Issues raised by the Community are summarized below and will be addressed in the Supplementary Report:

- maintaining the existing character of the area;
- protecting the Residential Woodland (tree removal/preservation); and
- setting a precedent for future development within the Gordon Woods area.

A further community meeting has been scheduled for March 20, 2013.

DEVELOPMENT ISSUES

Agency comments are summarized in Appendix I-7 and school accommodation information is contained in Appendix I-8. Based on the comments received and the applicable Mississauga Official Plan policies, the following matters will have to be addressed prior to the preparation of a Supplementary Report:

Tree Preservation and Community Character

The applicant has been encouraged to make adjustments to the concept plans to reduce the size/ building foot print of the dwellings, thereby increasing interior side, rear and exterior side yards to be more consistent with the existing established character of the area and allow more tree preservation opportunities. In undertaking these revisions, the applicant has been encouraged to look at strategic opportunities for the preservation of significant and/or mature trees which would maintain the continuous tree canopy associated with the Residential Woodland.

In order to preserve the character of lands designated "Residential Low Density I", the proposed development will be evaluated in the

context of policies comparing average lot frontage and area in the surrounding area.

It should also be noted that the outdoor amenity areas for individual dwellings should be identified to have a more realistic understanding of long term tree preservation expectations and identify what other implementation measures may be necessary to ensure the implementation of the official plan policies for Residential Woodlands.

Although the concept plans were modified with a November 2012 submission of materials, the above noted matters were not satisfactorily addressed.

OTHER INFORMATION

Development Requirements

In conjunction with the proposed development, there are certain other engineering and conservation matters with respect to grading, stormwater management, site servicing, noise attenuation and tree preservation/replacement, which will require the applicant to enter into appropriate agreements with the City.

FINANCIAL IMPACT:

Development charges will be payable in keeping with the requirements of the applicable Development Charges By-law of the City, as well as financial requirements of any other official agency concerned with the development of the lands.

CONCLUSION:

All agency and City department comments have been received and after the public meeting has been held and all issues are resolved, the Planning and Building Department will be in a position to make a recommendation regarding this application.

ATTACHMENTS:

Appendix I-1: Site History
Appendix I-2: Aerial Photograph
Appendix I-3: Excerpt of Cooksville District Land Use Map
Appendix I-4: Excerpt of Existing Land Use Map
Appendix I-5: Concept Plan
Appendix I-6: Elevations
Appendix I-7: Agency Comments

Appendix I-8: School Accommodation

Appendix I-9: Applicable Mississauga Official Plan (2011)
Policies

Appendix I-10: Proposed Zoning Standards

Appendix I-11: General Context Map



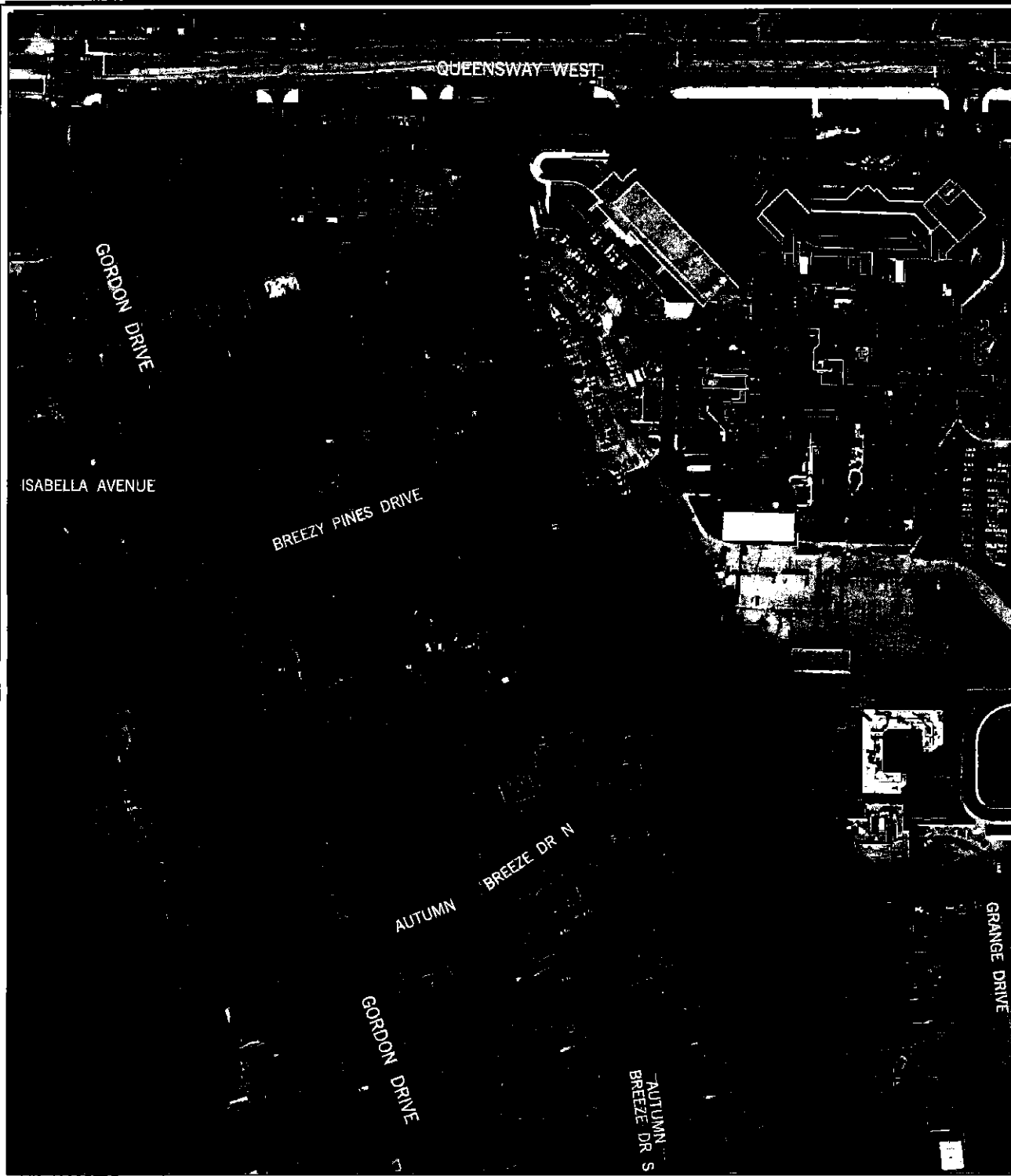
Edward R. Sajecki

Commissioner of Planning and Building

Prepared By: John Hardcastle, Development Planner

Raffi Konialian**File: OZ 12/002 W7****Site History**

- May 2, 2003 – Under consent application 'B' 22/03, a parcel with a frontage of approximately 30 m (100 ft.) on Gordon Drive was severed from the subject site to create a new property, now known as 2185 Gordon Drive.
- December 2, 2003 – Appeal B11 is approved under OMB Order No.1608, which amended the Mississauga Plan policies for Special Site 11.
- June 8, 2006 – Applications for Rezoning under file OZ 06/011 W7 and draft plan of subdivision under file 21T-M06002 W7 were submitted to accommodate the development of the lands for 5 detached dwellings on a common element condominium private road.
- June 20, 2007 – Zoning By-law 0225-2007 came into force except for those sites which have been appealed. As no appeals have been filed the provisions of the new By-law apply. The subject lands are zoned "R1-7" (Detached Dwelling – Typical Lots).
- July 2010 – Several significant/mature trees were removed from the lands in contravention of the Tree By-law and a previously issued Tree Removal Permit to remove only hazardous trees.
- January 16, 2012 – Files OZ 06/011 and 21T-M06002 W7 were cancelled due to inactivity and the expressed intention of the new owner to pursue a modified proposal.



LEGEND:



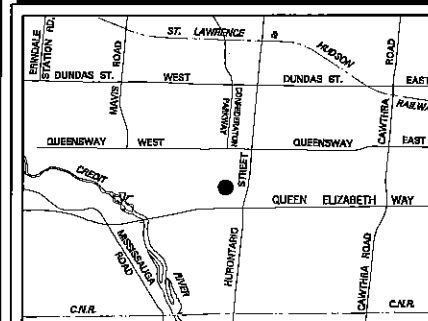
SUBJECT LANDS

DATE OF AERIAL PHOTO: SPRING 2010



SUBJECT:

RAFFI KONIALIAN

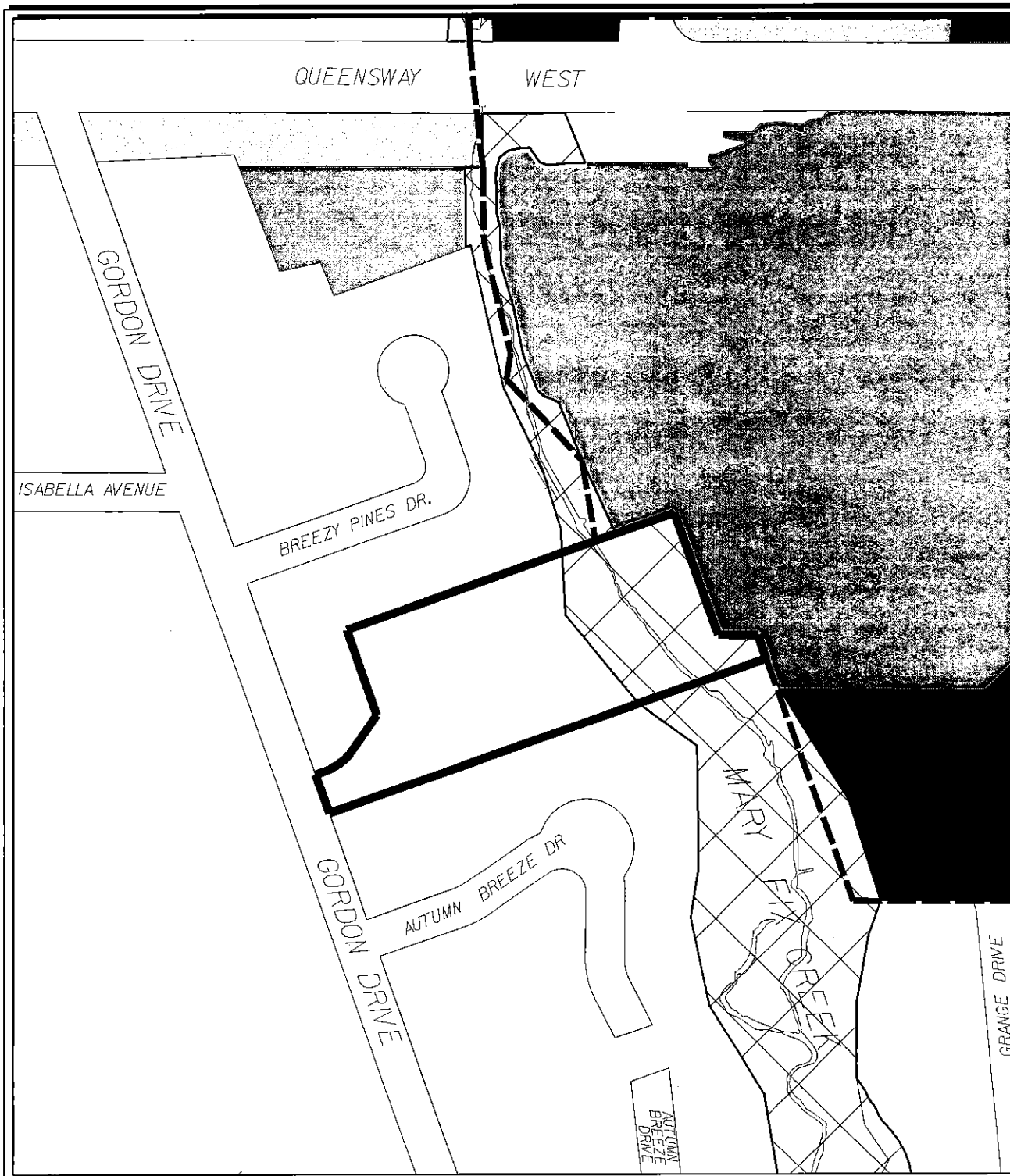


FILE NO:
OZ 12/002 W7
DWG. NO:
12002A.dgn
SCALE:
1:3000
PDC DATE:
2013 04 02
DRAWN BY:
B. KRUGER

APPENDIX I-2

MISSISSAUGA
Planning and Building

Produced by
T&W, Geomatics



PART OF SCHEDULE 10 LAND USE DESIGNATIONS OF MISSISSAUGA OFFICIAL PLAN

LAND USE DESIGNATIONS

	Residential Low Density I		Business Employment
	Residential Low Density II		Industrial
	Residential Medium Density		Airport
	Residential High Density		Institutional
	Downtown Mixed Use		Public Open Space
	Downtown Core Commercial		Private Open Space
	Mixed Use		Greenbelt
	Convenience Commercial		Parkway Belt West
	Motor Vehicle Commercial		Utility
	Office		To Be Determined

LAND USE LEGEND

	Heritage Conservation District		Civic Centre (City Hall)
	1996 NEP/2000 NEP Composite Noise Contours		City Centre Transit Terminal
	LBPIA Operating Area Boundary See Aircraft Noise Policies		GO Rail Transit Station
	Area Exempt from LBPIA Operating Area		Public School
	Natural Hazards		Catholic School
			Hospital
			Community Facilities

CITY STRUCTURE

Elements

	Downtown		Corporate Centre
	Major Node		Employment Area
	Community Node		Special Purpose Area
	Neighbourhood		

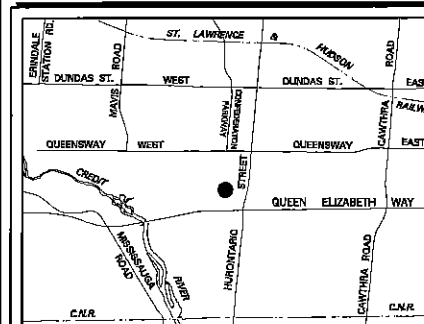
ND2 Region of Peel Non-Decision

SUBJECT LANDS



SUBJECT:

RAFFI KONIALIAN



FILE NO:
OZ 12002 W7

DWG. NO:
12002L

SCALE:
1:3000

PDC DATE:
2013 04 02

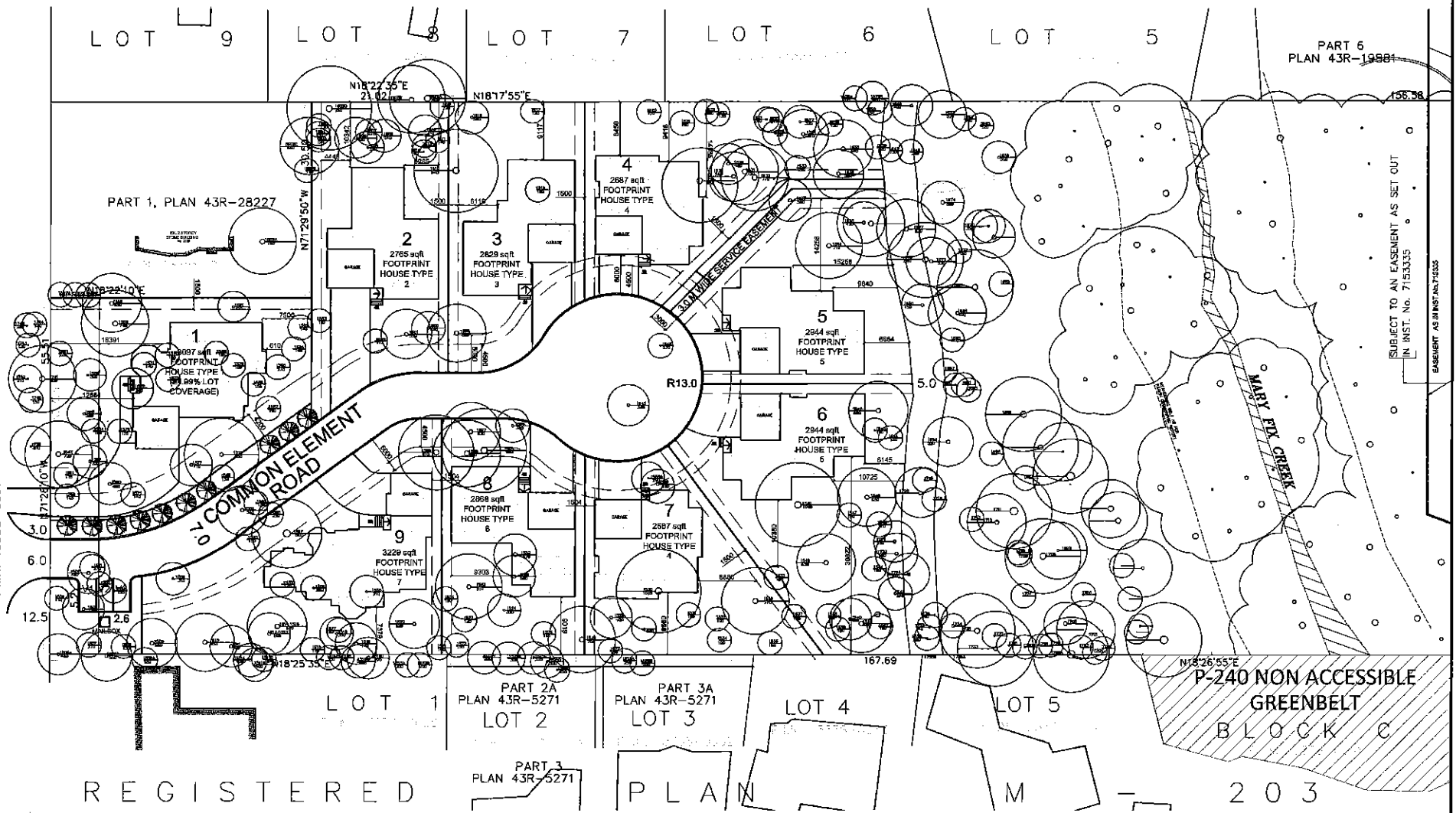
DRAWN BY:
B. KRUGER

EXHIBIT 1-3

MISSISSAUGA
Planning and Building

Produced by
T&W, Geomatics

GORDON DRIVE
(BY REGISTERED PLAN E-20)
P.L.N. 13358-0097



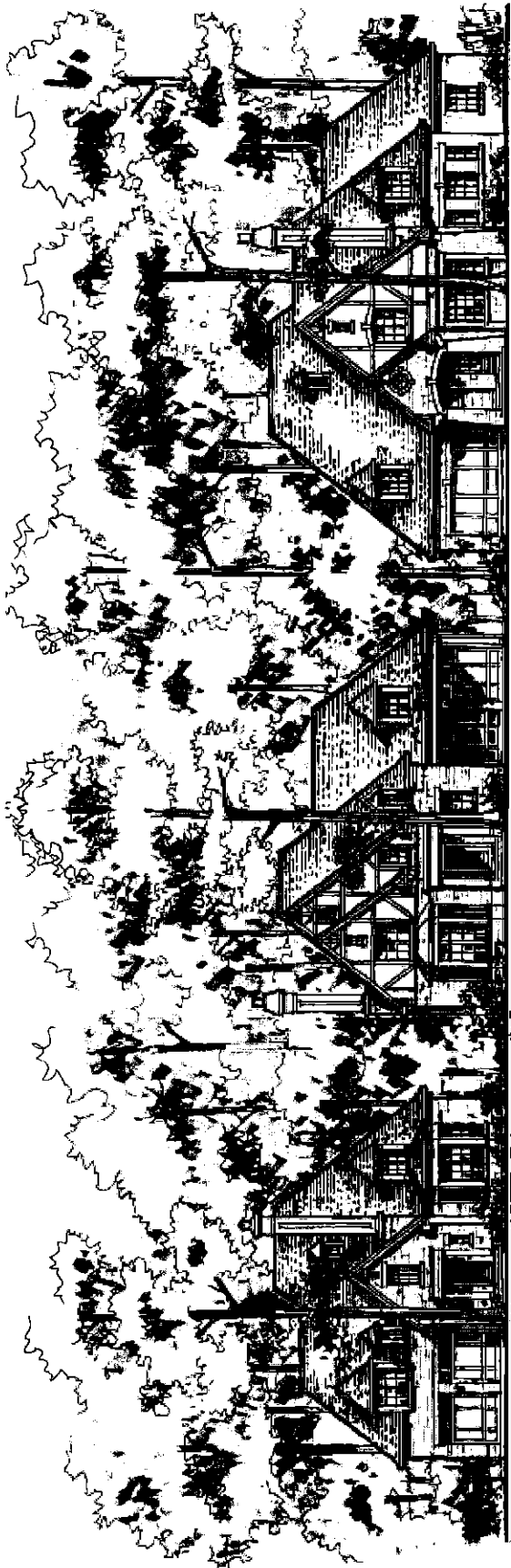
Site Statistics

Lot	House Type	Freehold/ Common Element Condo	Ground Floor GFA (Excluding Garage) (m2/sqft)	Second Floor GFA (m2/sqft)	Total GFA (m2/sqft)	Lot Area (m2/sqft)	Foot Print (m2/sqft)	Coverage (%)	Lot Frontage (Metres)	Trees Lost	Trees Damaged (Retained)
1	1	Freehold	2697 / 29035	2500 / 26910	5197 / 55940	1151 / 12389	287.76 / 3097	24.99	34.06	19	3
2	2	Common Element Condo	2366 / 25467	2134 / 22970	4500 / 48438	938 / 10101	256.97 / 2766	27.38	21.00	3	2
3	3	Common Element Condo	2479 / 26150	2071 / 22286	4500 / 48438	812 / 8745	262.86 / 2829	32.35	20.95	2	1
4	4	Common Element Condo	2287 / 24613	2213 / 23824	4500 / 48438	966 / 10399	249.6 / 2687	25.84	18.09	2	2
5	5	Common Element Condo	2544 / 27383	1956 / 21055	4500 / 48438	782 / 8417	273.5 / 2944	34.98	17.22	0	1
6	5	Common Element Condo	2544 / 27383	1956 / 21055	4500 / 48438	1116 / 12015	273.5 / 2944	24.50	18.20	4	1
7	4	Common Element Condo	2287 / 24613	2213 / 23824	4500 / 48438	784 / 8434	249.6 / 2687	31.85	18.04	5	1
8	6	Common Element Condo	2469 / 26575	2031 / 21863	4500 / 48438	784 / 8441	266.53 / 2869	33.99	22.02	5	1
9	7	Common Element Condo	2829 / 30447	2171 / 23372	5000 / 53820	1134 / 12211	299.95 / 3229	26.44	45.99	3	4
Outside site boundary										0	1
Road										19	0
Easement										2	4
Total										64	21

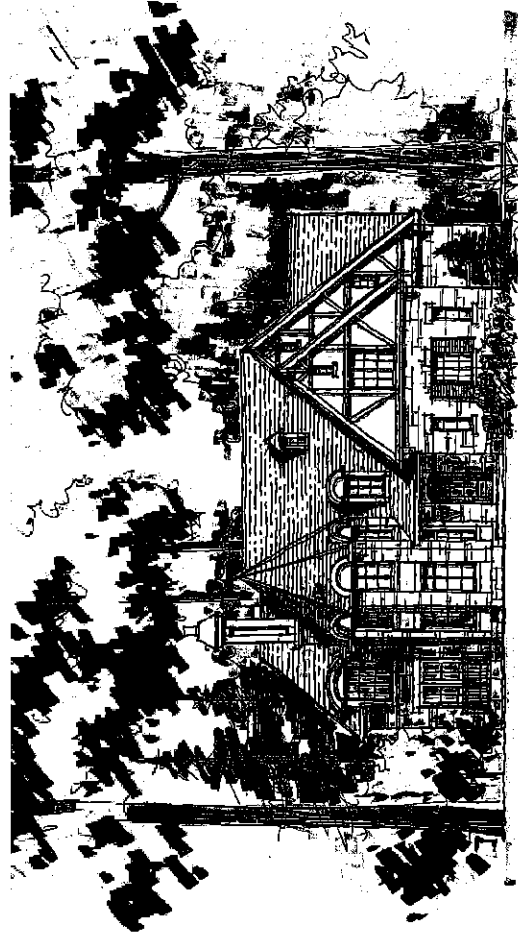


CONCEPT PLAN

APPENDIX I-5



WEST STREET ELEVATION - LOTS 2 TO 4



PLOT 1 FRONT ELEVATION -
FACING GORDON DRIVE

Raffi Konialian

File: OZ 12/002 W7

Agency Comments

The following is a summary of comments from agencies and departments regarding the application.

Agency / Comment Date	Comment
Region of Peel (February 15, 2013)	Comments detailed Regional servicing facilities in the vicinity of the site, acknowledged revision requirements to the Functional Servicing Report (FSR) necessary prior to site plan approval and garbage collection and agreement requirements. In addition, the Region advised of the need to protect Regional servicing easements from encroachments or obstructions and necessary servicing easement requirements to support the development as proposed.
Peel District School Board and Dufferin-Peel Catholic District School Board (February 19, 2013)	Both School Boards responded that they are satisfied with the current provision of educational facilities for the catchment area and, as such, the school accommodation condition as required by City of Mississauga Council Resolution 152-98 pertaining to satisfactory arrangements regarding the adequate provision and distribution of educational facilities need not be applied for this development application. In addition, if approved, both School Boards also require that warning clauses with respect to temporary school and transportation arrangements be included in any Agreements of Purchase and Sale and the Development and/or Servicing Agreements.
Credit Valley Conservation (February 15, 2013)	The proposed development is traversed by Mary Fix Creek and is therefore partially within its Regulated Area and subject to the Authority's Development, Interference within Wetlands, and Alterations to Shorelines and Watercourses Regulation. The subject lands are also located within a Residential Woodlands area as indicated on Schedule 3 Environmental Areas of the Mississauga Official Plan (2011). CVC staff are

Raffi Konialian

File: OZ 12/002 W7

Agency / Comment Date	Comment
	<p>concerned that the development as proposed does not meet the intent of the Residential Woodland policies in terms of adequately protecting existing mature and significant trees and maintaining connectivity of the tree canopy which makes up this Residential Woodland.</p> <p>CVC Planning has reviewed the proposed Zoning By-law Amendment, Condominium Development Site Plan and Condominium Development Landscape Plan and advise that: revisions to the proposed zone standards are necessary to preserve and protect perimeter trees; no accommodations have been made for outside amenity structures (pools, decks and gazebos) for the future residents within the rear and side yards without additional, significant harm or loss of the trees; the landscape concept plan and the consulting arborist report conflict in terms of the number of trees to be preserved; and that efforts should be undertaken through the design and placement of dwellings on site, the identification of private amenity areas and long term tree preservation areas and in establishing appropriate grading and servicing plans to protect existing mature and significant trees on site.</p> <p>Revisions have been requested to the concept plans, draft zoning by-law amendment, EIS Report and Tree Preservation Report prior to the preparation of a Supplementary Report. Additional requirements pertaining to restoration of the natural hazard and other matters have been identified for inclusion in any required Servicing and/or Development Agreements, to be addressed prior to By-law Enactment. Additional requirements may be identified upon the review of outstanding information and will be outlined, as appropriate, within revised comments.</p>

Raffi Konialian

File: OZ 12/002 W7

Agency / Comment Date	Comment
<p>City Community Services Department – Parks and Forestry Division/Park Planning Section (February 20, 2013)</p>	<p>Lands below the established Top of Bank and any buffer lands, as required by the City and CVC, shall be zoned Greenbelt and dedicated gratuitously to the City for long term conservation and natural hazard management.</p> <p>Should this application be approved, fencing, protective hoarding, and associated securities for the dedicated greenbelt lands will be required. Arrangements will be made to secure for any clean-up and reinstatement works that may be required within the adjacent greenbelt.</p> <p>Prior to by-law enactment, cash contribution for street tree planting will be required. Furthermore, prior to the issuance of building permits, cash-in-lieu for park or other public recreational purposes is required pursuant to Section 42 of the Planning Act (R.S.O. 1990, c.P. 13, as amended) and in accordance with City's Policies and By-laws.</p>
<p>City Community Services Department – Culture Division (February 19, 2013)</p>	<p>The property has archaeological potential due to its proximity to a watercourse or known archaeological resource. The proponent shall carry out an archaeological assessment of the subject property and mitigate, through preservation or resource removal and documenting, adverse impacts to any significant archaeological resources found. No grading or other soil disturbances shall take place on the subject lands prior to the approval authority and the Ministry of Tourism and Culture confirming that all archaeological resource concerns have met licensing and resource conservation requirements.</p> <p>In addition, photographic documentation of the existing property has been requested.</p>
<p>City Community Services Department – Fire and Emergency Services Division (February 15, 2013)</p>	<p>Fire has reviewed the rezoning application from an emergency response perspective and has no concerns; emergency response time and site water supply availability are acceptable.</p>

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File: OZ 12/002 W7

Agency / Comment Date	Comment
City Transportation and Works Department (January 28, 2013)	<p>This department advised that a Noise Control Study has been received which concludes, that with the use of appropriate attenuation measures and warning clauses the proposed development can be adequately attenuated from the surrounding noise sources in accordance with City, Regional and Ministry of Environment (MOE) guidelines.</p> <p>It was also indicated that minor revisions are required to the Site Servicing Plan and Condominium Development Plan. The Functional Servicing Report and Phase 1, Environmental Evaluation are satisfactory, however approval from the Credit Valley Conservation will be required prior to a Supplementary Meeting.</p> <p>The applicant has confirmed that the proposal for the interior eight lots fronting the future private cul-de-sac road is to be developed as a Common Element Condominium.</p>
Other City Departments and External Agencies	<p>The following City Departments and external agencies offered no objection to these applications provided that all technical matters are addressed in a satisfactory manner:</p> <ul style="list-style-type: none"> - Bell Canada - Enersource Hydro Mississauga - Rogers Cable - Credit Valley Hospital - Canada Post
	<p>The following City Departments and external agencies were circulated the applications but provided no comments:</p> <ul style="list-style-type: none"> - Development Services - Realty Services - Hydro One Networks - Conseil Scolaire de District Catholique Centre-Sud - Conseil Scolaire de District Centre-Sud-Ouest - Enbridge Gas Distribution - Trans-Northern Pipelines - The Trillium Health Centre

Raffi Konialian

File: OZ 12/002 W7

School Accommodation

The Peel District School Board	The Dufferin-Peel Catholic District School Board
<ul style="list-style-type: none"> Student Yield: <ul style="list-style-type: none"> 1 Kindergarten to Grade 6 1 Grade 7 to Grade 8 1 Grade 9 to Grade 12/OAC School Accommodation: <ul style="list-style-type: none"> Floradale Public School <ul style="list-style-type: none"> Enrolment: 725 Capacity: 711 Portables: 2 Queen Elizabeth Senior Public School <ul style="list-style-type: none"> Enrolment: 355 Capacity: 262 Portables: 5 Port Credit Secondary School <ul style="list-style-type: none"> Enrolment: 1,215 Capacity: 1,203 Portables: 1 <p>* Note: Capacity reflects the Ministry of Education rated capacity, not the Board rated capacity, resulting in the requirement of portables.</p>	<ul style="list-style-type: none"> Student Yield: <ul style="list-style-type: none"> 1 Junior Kindergarten to Grade 8 1 Grade 9 to Grade 12/OAC School Accommodation: <ul style="list-style-type: none"> St. Catherine of Siena <ul style="list-style-type: none"> Enrolment: 600 Capacity: 627 Portables: 0 St. Martin <ul style="list-style-type: none"> Enrolment: 943 Capacity: 1,026 Portables: 0

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File: OZ 12/002 W7

Applicable Mississauga Official Plan (2011) Policies

Cooksville [Neighbourhood Character Area]

Section 16.1.2.1

To preserve the character of lands designated Residential Low Density I and Residential Low Density II, the minimum frontage and area of new lots proposed along the periphery of a draft plan of subdivision, or which are subject to a consent application, will generally represent the greater of:

- a. the average lot frontage and lot area of residential lots on both sides of the same street within 120 m (393.7 ft.) of the subject property. In the case of a corner lot, lots on both streets within 120 m (393.7 ft.) will be considered; or
- b. the requirements of the Zoning By-law.

Section 16.6.5.4.1

The lands identified as Special Site 4 are located west of Hurontario Street, south of Queensway West.

Section 16.6.5.4.2

Notwithstanding the provisions of the Residential Low Density I designation, the following additional policies will apply:

- a. preserve and enhance the generous front, rear and side yard setbacks;
- b. ensure that existing grades and drainage conditions are preserved;
- c. encourage new housing to fit the scale and character of the surrounding area, and take advantage of the features of a particular site, i.e. topography, contours, mature vegetation;
- d. garages should be recessed or located behind the main face of the house. Alternatively, garages should be located in the rear of the property;
- e. ensure that new development has minimal impact on its adjacent neighbours with respect to overshadowing and overlook;
- f. encourage buildings to be one to two (1-2) storeys in height. The design of the building should de-emphasize the height of the house.

Direct Growth

Section 5.1.7

Mississauga will protect and conserve the character of stable residential Neighbourhoods.

Raffi Konialian

File: OZ 12/002 W7

Section 5.3.5.1

Neighbourhoods will not be the focus for intensification and should be regarded as stable residential areas where the existing character is to be preserved.

Section 5.3.5.5

Intensification within Neighbourhoods may be considered where the proposed development is compatible in built form and scale to surrounding development, enhances the existing or planned development and is consistent with the policies of this Plan.

Build a Desirable Urban Form

Section 9.1.3

Infill and redevelopment within Neighbourhoods will respect the existing planned character.

Section 9.2.2.4

While new development need not mirror existing development, new development in Neighbourhoods will:

- a. respect existing lotting patterns;
- b. respect the continuity of front, rear and side yard setbacks;
- c. respect the scale and character of the surrounding area;
- d. minimize overshadowing and overlook on adjacent neighbours;
- e. incorporate stormwater best management practice;
- f. preserve mature high quality trees and ensure replacement of the tree canopy; and
- g. be designed to respect the existing scale, massing, character and grades of the surrounding area.

Environmental Policies

Section 6.3.1.4

Residential Woodlands are areas within Neighbourhoods, generally in older residential areas with large lots that have mature trees forming a fairly continuous canopy. Some areas have minimal native understorey due to maintenance of lawns and landscaping.

Section 6.3.1.13

Development and site alteration will not be permitted within or adjacent to natural areas, Linkages and Special Management Areas unless it has been demonstrated that there will be no negative impacts to the features and ecological functions of the Natural Areas System.

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File: OZ 12/002 W7

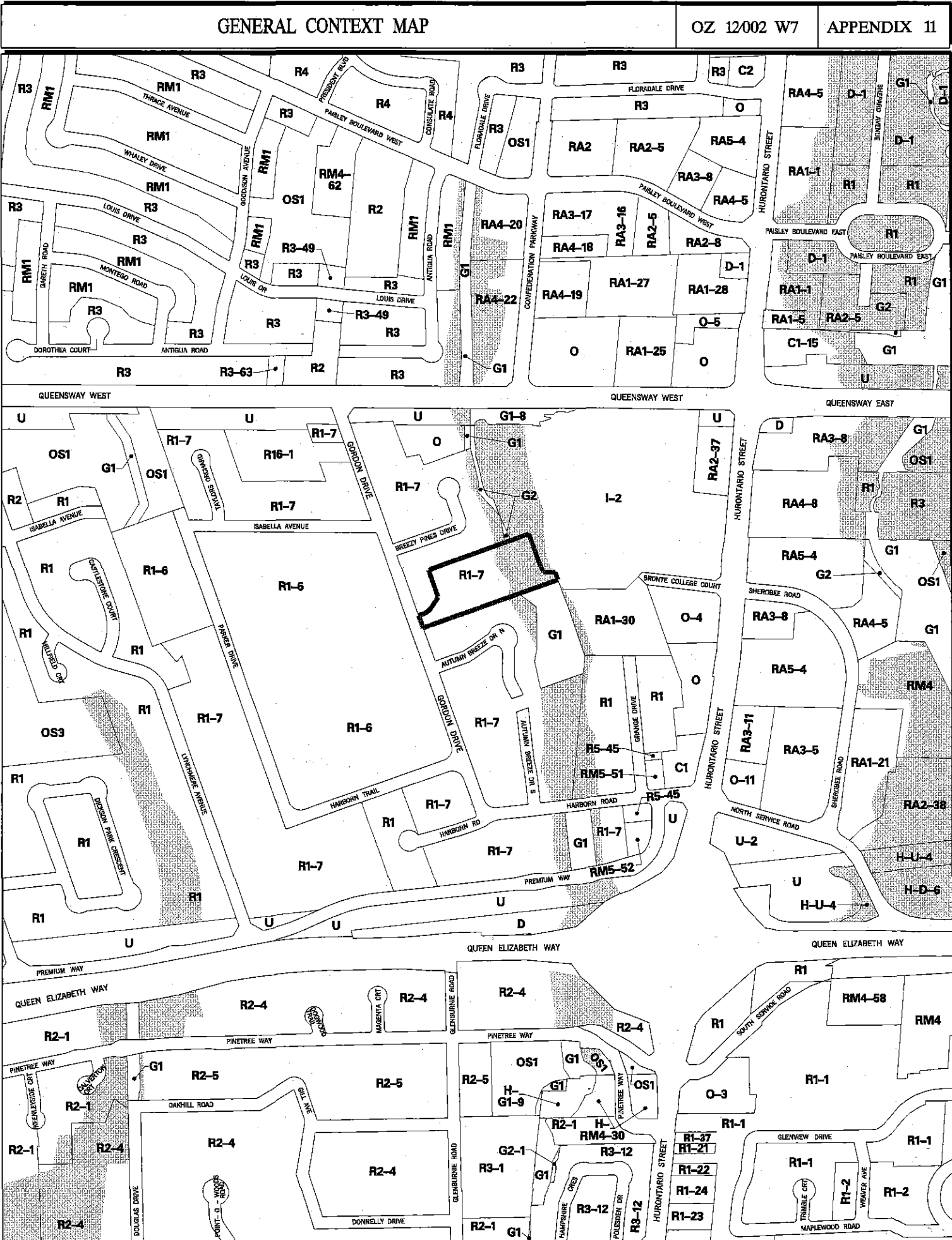
Proposed Zoning Standards

	Base "R16" Standard	Proposed "R16-Exception" Standard
Maximum Dwelling Units	n/a	8
Minimum Lot Area	550 m ² (5,920 sq. ft.) - Interior 720 m ² (7,750 sq. t.) - Exterior	750 m ² (8,073 sq. ft.)
Minimum Lot Frontage – Interior Lot	15 m (49.2 ft.)	15 m (49.2 ft.)
Maximum Lot Coverage	35%	35%
Minimum Front Yard	7.5 m (24.6 ft.)	5 m (16.4 ft.)
Minimum Front Yard to a Garage	7.5 m (24.6 ft.)	6 m (19.7 ft.)
Minimum Interior Side Yard	1.81 m (5.9 ft.)	1.5 m (4.9 ft.)
Minimum Rear Yard – Interior Lot	7.5 m (24.6 ft.)	5.5 m (18 ft.)
Maximum Height	10.7 m (35 ft.)	10.7 m (35 ft.)
An attached garage shall be provided on each lot	Attached or Detached permitted	Yes
Minimum Visitor Parking Spaces	0.25 spaces per unit (2 spaces)	3

GENERAL CONTEXT MAP	OZ 12002 W7	APPENDIX 11
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GENERAL CONTEXT MAP	OZ 12002 W7	APPENDIX 11
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GENERAL CONTEXT MAP	OZ 12002 W7	APPENDIX 11
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Corporate Report

Clerk's Files

PLANNING & DEVELOPMENT COMMITTEE

APR 02 2013

Originator's

Files OZ 11/017 W1

DATE: March 12, 2013

TO: Chair and Members of Planning and Development Committee
Meeting Date: April 2, 2013

FROM: Edward R. Sajecki
Commissioner of Planning and Building

SUBJECT: **Information Report**
Official Plan Amendment and Rezoning Applications
To permit retail commercial and office uses fronting Lakeshore
Road East, apartment and townhouse dwellings to the rear and
public greenspace bordering Cooksville Creek
447, 453, 501 Lakeshore Road East and
1021, 1027, 1077 Enola Avenue
Northeast corner of Lakeshore Road East and Enola Avenue
Owner: 501 Lakeshore Inc., Trinity Properties Lakeshore Inc.
and 1716336 Ontario Inc.
Applicant: Korsiak and Company Inc.
Bill 51

Public Meeting **Ward 1**

RECOMMENDATION: That the Report dated March 12, 2013, from the Commissioner of Planning and Building regarding the applications to amend the Official Plan from "Business Employment", "Mixed Use" and "Residential Low Density II" to "Mixed Use - Special Site", "Residential High Density - Special Site" and "Greenbelt" and to change the Zoning from "E2" (Employment), "C4" (Mainstreet Commercial) and "R3" (Detached Dwellings - Typical Lots) to "C4 - Exception" (Mainstreet Commercial), "RA5 - Exception" (Apartment Dwellings) and "G1" (Greenbelt - Natural Hazards) to

permit retail commercial and office uses fronting Lakeshore Road East; apartment and townhouse dwellings to the rear and public greenspace bordering Cooksville Creek under file OZ 11/017 W1, 501 Lakeshore Inc., Trinity Properties Lakeshore Inc. and 1716336 Ontario Inc., 447, 453, 501 Lakeshore Road East and 1021, 1027, 1077 Enola Avenue, northeast corner of Lakeshore Road East and Enola Avenue, be received for information.

**REPORT
HIGHLIGHTS:**

- The applications have been made to allow for the development of the lands for retail commercial and office uses adjacent to Lakeshore Road East and apartments and townhouses to the rear of the site;
- Community concerns include the scale of the proposed development, the impact of large format retail uses on existing retail commercial development along Lakeshore Road East within both Lakeview and Port Credit, the appropriateness of the proposed residential building heights, the transition of proposed apartment dwellings to existing low density development to the north and west; and traffic impacts on both Lakeshore Road East and Enola Avenue;
- The applications have been appealed to the Ontario Municipal Board (OMB) and a ten (10) day hearing has been scheduled to commence on June 17, 2013;
- A future Supplementary Report will address outstanding matters, the resolution of any issues, as appropriate and will seek Council's direction on the applications and the appeals, including for City staff participating in the upcoming OMB proceedings regarding this matter.

BACKGROUND:

The above-noted applications have been circulated for technical comments and a community meeting has been held.

The applications were appealed to the OMB in a letter dated August 8, 2012, citing the failure of Council to make a decision within the time-frame prescribed by the *Planning Act*. During a first prehearing conference held on November 20, 2012, a tentative hearing date of May 6, 2013 was established, notwithstanding that a revised submission of materials responding to the Regional expropriation of a portion of the lands had not yet been submitted.

In addition, a second prehearing conference was scheduled for February 11, 2013, during which the hearing date was rescheduled to June 17, 2013 to run for 10 days. It should be noted that City Legal Counsel has advised of the need for deferral of any hearing should the proposal be further revised, thereby requiring additional staff review.

The purpose of this report is to provide preliminary information on the applications and to seek comments from the community.

COMMENTS:

Details of the proposal are as follows:

Development Proposal	
Applications submitted:	November 30, 2011 (Received) December 15, 2011 (Deemed Complete) December 4, 2012 (Revised)
Height:	4 to 20 storeys - Residential 1 to 2 storeys - Commercial
Lot Coverage:	51% Commercial
Floor Space Index:	1.87 Residential
Landscaped Area:	33% - Residential 10% - Commercial
Net Residential Density:	188 units/ha 76 units/acre
Gross Floor Area:	13 622 m ² (146,630 sq. ft.) - Commercial 37 832 m ² (407,234 sq. ft.) - Residential
Number of units:	365 apartment dwelling units 15 townhouse dwelling units 380 dwelling units total
Anticipated Population:	935* *Average household sizes for all units (by type) for the year 2011 (city average) based on the 2008 Growth Forecasts for the City of Mississauga

Development Proposal	
Parking Required:	598 spaces - Residential 540 spaces - Commercial
Parking Provided:	630 spaces - Residential 467 spaces - Commercial
Supporting Documents:	Planning Justification Report, Urban Design and Streetscape Analysis, Stormwater Management Report, Functional Servicing Report, Traffic Impact Study, Noise Report, Railway Vibration Analysis, Environmental Site Assessment Reports, Parking Supply Review Report, Tree Inventory and Preservation Plan

Site Characteristics	
Frontage:	180.65 m (592.7 ft.) - irregular
Depth:	336.3 m (1,103.4 ft.) - irregular
Net Lot Area:	2.02 ha (4.99 ac.) - Residential 2.64 ha (6.52 ac.) - Commercial 4.66 ha (11.52 ac.) - Total
Existing Use:	Industrial - Former Inglis appliance manufacturing facility

In addition to the above details and the information provided on the Concept Plan submitted (see Appendix I-5), the following additional details are provided to assist in understanding the development as proposed:

- The proposed commercial development adjacent to the Lakeshore Road East frontage includes two driveway access locations on Lakeshore Road East and two on Enola Avenue; the easterly most Lakeshore Road East access is proposed to be signalized and run through both the commercial and residential components of the development before connecting with Enola Avenue.
- Four (4) separate commercial structures are proposed; two, 1 storey buildings along the east side of the signalized

private road, one large structure on the west side of the private road, which includes four buildings on the ground floor and a large second level that sits above these buildings and the central parking area. The upper level commercial space is intended for one or two large format retail tenants and makes up 8 857 m² (95,339 sq. ft.) of the 13 622 m² (146 638 sq. ft.) Gross Floor Area proposed for retail commercial and office uses on-site. The last commercial building is located at the northeast corner of Lakeshore Road East and Enola Avenue and is 2 storeys in height with ground level retail commercial and upper level office uses.

- To the rear of the site, two apartment buildings, with 8 townhouses incorporated into the front facades are proposed on the northeast side of the private road and 7 townhouses in a single block are proposed on the southwest side. The apartment buildings range in height from 6 to 20 storeys, with the lowest height to the west. The townhouses are 3 storeys in height.
- Residential parking is proposed to be provided within above grade parking structures making up the first 3 to 4 floors of the two apartment buildings. The parking structures are to be faced with townhouse dwellings along the internal private road and treated with a similar architectural treatment as the rest of the buildings on the remaining facades. Details of the architectural treatment have not yet been provided.

Additional information is provided in Appendices I-1 to I-11.

Neighbourhood Context

The subject property is located in the Lakeview Neighbourhood which is predominantly a stable established residential area. The frontage portions of the lands lie within a linear commercial area along Lakeshore Road East; whereas the rear portion of the lands lie within a residential area comprised of a mix of residential unit

types. The site, which is generally flat, slopes from the northwest to the southeast toward the Cooksville Creek which borders the site on its east side. Two large industrial buildings and associated asphalt parking and loading areas are presently located on the lands. Little vegetation and landscaping is present on site.

Information regarding the history of the site is found in Appendix I-1.

The surrounding land uses are described as follows:

- North: Detached and semi-detached dwellings beyond the CNR Mainline
- East: Vacant lands, subject to applications for Draft Plan of Subdivision, Official Plan Amendment and Rezoning to accommodate townhouse dwellings beyond Cooksville Creek
- South: A mix of commercial uses and detached and apartment dwellings along Lakeshore Road East
- West: Detached, semi-detached and apartment dwellings fronting onto Enola Avenue

Current Mississauga Official Plan Designation and Policies for Lakeview Local Area Plan (November 14, 2012)

Mississauga Official Plan was adopted by City Council on September 29, 2010 and partially approved by the Region of Peel on September 22, 2011. The Plan was appealed in its entirety, however, on November 14, 2012, the OMB issued a Notice of Decision approving Mississauga Official Plan, as modified, save and except certain appeals which have no effect on the subject applications.

The subject lands are located within a **Neighbourhood Area** (Lakeview Local Area Plan) and on a **Corridor** (Lakeshore Road East). The lands are designated "**Business Employment**", "**Mixed Use**" and "**Residential Low Density II**".

"Business Employment" permits an integrated mix of business activities that operate mainly within enclosed buildings. Business Employment activities along City boundaries, major roads, and adjacent to park, greenbelt or residential lands; will through design, siting and landscaping present a higher standard of building, landscape and streetscape design. **"Mixed Use"** permits a mixture of personal service, commercial, office, institutional and residential use. **"Residential Low Density II"** permits detached, semi-detached, duplex, triplex and street townhouse dwellings.

The applications are not in conformity with the existing land use designations.

There are other policies in the Official Plan which also are applicable in the review of these applications which have been outlined within Appendix I-9.

Proposed Official Plan Designation and Policies

"Mixed Use - Special Site" to permit a mixed use development consisting of retail commercial and office uses. Special site provisions are required to allow for one storey retail commercial buildings not directly fronting Lakeshore Road East; whereas a minimum of two storeys would be required.

"Residential High Density - Special Site" to permit the development of apartment dwellings to a maximum height of 20 storeys, with a maximum Floor Space Index (FSI) of 2.0.

"Greenbelt" to identify lands associated with the Cooksville Creek natural hazard where development is restricted.

Conversion of Employment Lands

The Provincial Policy Statement (PPS), the Growth Plan for the Greater Golden Horseshoe (Growth Plan) and the *Planning Act* encourage the retention of employment lands and require a comprehensive municipal review where employment lands are to be converted to non-employment uses. Further, the *Planning Act*

identifies the adequate provision of employment opportunities as a matter of Provincial interest.

In June 2008, an Employment Land Review Study was undertaken by Hemson Consulting Ltd. as part of the Mississauga Plan review to ensure conformity with Provincial initiatives and fulfill *Planning Act* requirements. This study identified the subject lands as being part of an area of "Managed Change Outside Existing Employment Areas" which are defined as scattered vacant or under-utilized sites outside of designated Employment Districts. They include vacant sites that are 'remnant' or otherwise may be constrained and unlikely to develop as employment land, and may be suitable for other uses.

Lakeview Local Area Plan Review

In November 2007, the City initiated Phase I – Public Engagement and Vision of the Lakeview and Port Credit District Policies Review. This process culminated in the preparation of the "Lakeview and Port Credit District Policies Review and Public Engagement Process – Directions Report" (Directions Report) which was presented to Planning and Development Committee (PDC) in November 2008. One of the recommendations was for staff to prepare revised District Policies (now Local Area Plans) based on policy recommendations outlined in the Directions Report. Staff was also directed to set up Local Advisory Panels to facilitate discussion with stakeholders.

Staff is presently preparing the draft policies of the Lakeview Local Area Plan and expect to present them to PDC late this year. At that time, staff will be requesting to circulate the Area Plan and to begin the formal public consultation process.

Criteria for Site Specific Official Plan Amendments

Section 19.5 of Mississauga Official Plan contains criteria which requires an applicant to submit satisfactory planning reports to demonstrate the rationale for the proposed amendment as follows:

- the proposal would not adversely impact or destabilize the following: the overall intent, goals and objectives of the Official Plan; and the development and functioning of the remaining lands which have the same designation, or neighbouring lands;
- the lands are suitable for the proposed uses, and compatible with existing and future uses of surrounding lands;
- there are adequate engineering services, community infrastructure and multi-modal transportation systems to support the proposed application;
- a planning rationale with reference to Mississauga Official Plan policies, other relevant policies, good planning principles and the merits of the proposed amendment in comparison with the existing designation has been provided by the applicant.

Existing Zoning

"E2" (Employment), which permits a wide variety of employment uses which function primarily within wholly enclosed buildings or structures.

"C4" (Mainstreet Commercial), which permits a mix of retail, service commercial, office and residential uses. Buildings are to be located at the street edge with front yards of 0 m (0 ft.) to 3.0 m (9.8 ft.) with a minimum building height of 2 storeys and a maximum building height of 3 storeys.

"R3" (Detached Dwelling - Typical Lots), which permits detached dwellings with a minimum lot area of 550 m² (5,920 sq. ft.), minimum lot frontage of 15.0 m (49.2 ft.) and maximum height of 10.7 m (35 ft.).

Proposed Zoning By-law Amendment

"C4 - Exception" (Mainstreet Commercial), to permit retail commercial development in accordance with standards contained within Appendix I-10.

"RA5 - Exception" (Apartment Dwellings), to permit apartment, townhouse and horizontal multiple dwellings in accordance with the standards contained within Appendix I-10.

"G1" (Greenbelt - Natural Hazards), to reflect the limits of development associated with Cooksville Creek.

Bonus Zoning

On September 26, 2012, Council adopted Corporate Policy and Procedure 07-03-01 - Bonus Zoning. In accordance with Section 37 of the *Planning Act* and policies contained in the Official Plan, this policy enables the City to secure community benefits when increases in permitted development are deemed good planning by Council through the approval of a development application. Should these applications be approved in principle by Council, or through the OMB, the City may require the provision of community benefits as a condition of approval.

COMMUNITY ISSUES

A community meeting was held by Ward 1 Councillor, Jim Tovey on April 4, 2012.

The following is a summary of issues raised by the community:

Comment

Who will occupy the large anchor tenant space on the upper level of the commercial development?

Response

The applicant has advised that several retailers have expressed interest in this space including Wal-Mart, Target, No Frills and Sobeys. This second level space is configured to accommodate either one or two large format retailers. Discussions with potential tenants are ongoing and the tenant(s) remain unknown at this point in time.

Comment

Enersource Hydro Mississauga has recently erected new overhead hydro lines along Lakeshore Road East, adjacent to the subject lands. What would happen to these new wires given that the buildings are proposed to front Lakeshore Road East with no setback?

Response

The applicant has advised that it is their intent to bury any overhead wires, including those recently erected by Enersource. The applicant has further advised that existing below grade services will be relocated to accommodate standard streetscape upgrades as a condition of approval.

Comment

Given the long industrial history of the site, are the lands contaminated, and if so, how will the contamination be addressed?

Response

The applicant has provided supporting materials which have evaluated the condition of soils and ground water on site. These reports indicate that the lands are presently contaminated. The applicant has commenced a Risk Assessment process with the Ministry of Environment. Additional details have been provided within the Transportation and Works Department comments contained within Appendix I-7 and within the Development Issues

section of this report. Commentary about this issue will be provided within a future Supplementary Report.

Comment

What is the proposed tenure of the residential units and will assisted housing be included?

Response

All residential units are proposed to be of condominium tenure. The applicant has advised that assisted housing units are not being considered at this time.

Comment

Traffic concerns were raised with regard to increased traffic volumes on both Lakeshore Road East and Enola Avenue. In addition, concerns were expressed with regard to additional delays and conflicts for vehicles exiting Beechwood Avenue to the south.

Response

Revisions have been requested to the site access configuration, including accommodating a signalized access on Lakeshore Road East which better aligns with Beechwood Avenue (see Appendix I-7, Transportation and Works Department comments and the Development Issues section of this report). A Traffic Impact Study has been submitted in support of the applications and comments will be provided in a future Supplementary Report.

Comments

What is the expected construction timing of the proposed development? If the residential is to be constructed after the commercial, what assurances will be provided that the residential will be constructed?

Response

Trinity is a commercial builder and intends to bring on another builder to undertake construction of the residential component. The applicant has advised that it is their intent to commence construction concurrently, but note that the residential could take longer to sell and construct. Construction of the commercial is expected to take 18 months to complete. Any change to the residential component of the development in the future would require new *Planning Act* applications and a further public consultation process.

Comments

A discount retailer is not appropriate and this concentration of retail will undermine existing retail stores within the Lakeview and Port Credit communities.

Response

These matters will be addressed in a future Supplementary Report.

Comment

Does the proposed development conform to the Legacy (Inspiration Lakeview) Project?

Response

Inspiration Lakeview is an ongoing process which may result in changes to the Mississauga Official Plan and other policy and regulatory documents. There are no policies from Inspiration Lakeview in effect and as such the applications must be evaluated in accordance with the existing policy framework.

DEVELOPMENT ISSUES

Agency comments are summarized in Appendix I-7 and school accommodation information is contained in Appendix I-8. Based

on the comments received and the applicable Mississauga Official Plan policies, the following will have to be addressed:

Region of Peel Sanitary Sewage Pump Station

Presently there is a lack of sanitary sewage capacity in a large portion of south-central Mississauga. In response, the Region of Peel commenced an Environmental Assessment process in 2006 to evaluate options to provide additional capacity in this catchment area. The 'preferred' location to provide additional pumping capacity was identified as 501 Lakeshore Road East and efforts have been ongoing since that time to plan, design and construct the necessary infrastructure. The proposed development cannot proceed in advance of the required pump station.

Subsequent to the submission of these applications, the Region of Peel expropriated 0.17 ha (0.43 ac.) along the Lakeshore Road East frontage of the subject lands for the purpose of constructing the pump station. These lands are now municipally known as 505 Lakeshore Road East. The subject applications were formally revised in December 2012 to reflect the Regional expropriation. The Region has submitted an application for Site Plan approval, under file SP 12/172 W1 and received approval from the Committee of Adjustment to provide reduced setback requirements to accommodate the pump station on 505 Lakeshore Road East.

The applicant and the Region of Peel have both advised that discussions are underway exploring opportunities to exchange the frontage lands on Lakeshore Road East for lands further north, within the subject site. Further revisions to the Concept Plan would be necessary should an agreement to locate the pump station elsewhere on site be reached.

Site Contamination

Studies submitted in support of these applications have concluded that soil and groundwater contamination exists on site. The applicant is presently pursuing a Risk Assessment (RA) process with the Ministry of Environment. While it is more common

through the development approval process to identify the location and extent of site contamination for the purpose of remediation, the RA process is based on leaving all or a portion of the contamination in situ and examining the risk posed to humans, plants, wildlife and the natural environment from exposure to contamination. The RA is to develop standards that will protect the uses that are being proposed on the property and may include the identification of risk management measures that must be incorporated into the development to ensure an appropriate level of public safety. Measures could include, but are not limited to, prohibitions on basements, use of specialized or contained heating and cooling systems, the use of migration barriers, or ongoing treatment options. An ongoing monitoring program implemented by the developer and maintained by the ultimate property owner is often a requirement resulting from the RA process.

It should be noted that reports submitted to date indicate that contamination has migrated off-site. Lands proposed to be conveyed to the City as greenbelt, as well as lands presently owned by Credit Valley conservation as part of the Cooksville Creek natural hazard, may be affected by this off-site migration. The ultimate owners of these lands would be responsible in perpetuity for maintaining any approved risk management measures such as groundwater or vapour monitoring.

An outside consultant with a specialization in the Risk Assessment process, soil and groundwater contamination and hydrogeology has been engaged by the City to assist in the review of these matters. The Ministry of Environment is presently reviewing the RA submission which is not expected to conclude before the planning process and, as such, additional information resulting from the consultant's review of the RA materials will be outlined within a future Supplementary Report.

Signalized Lakeshore Road East Access

Staff have expressed concerns with regard to the location of the signalized internal road, encouraging it to be located as far east as possible to align with Beechwood Avenue to the south. In this

regard, the location of the pump station lands within the southeast corner of the site would prevent a direct alignment. However, efforts have been undertaken through the review and processing of the Site Plan application for the pump station building and above grade structures to be located as far east on-site as possible. In addition, the Region has agreed to grant a public use/access easement over the westerly portion of their lands to allow most of the signalized intersection to be constructed over the westerly portion of 505 Lakeshore Road East. Although not ideal, a more functional intersection with Beechwood Avenue to the south would result. The Concept Plan has not been modified to accommodate the requested alignment.

Site Layout and Design Considerations

Planning and Urban Design concerns centre around the location and orientation of retail commercial uses that are internal to the site and away from Lakeshore Road East, the location of the main internal road and building orientation relative to Cooksville Creek, the height of residential apartment buildings and the transition of height to the existing ground based residential units fronting onto Enola Avenue. Revisions to the Concept Plan and proposed amending documents have been requested in regard to these matters but have not been satisfactorily addressed to date.

OTHER INFORMATION

Development Requirements

In conjunction with the proposed development, there are certain other engineering and conservation matters with respect to warning clauses, on-site remediation, flood plain management and restoration and streetscape works which will require the applicant to enter into appropriate agreements with the City.

FINANCIAL IMPACT:


Development charges will be payable in keeping with the requirements of the applicable Development Charges By-law of the City as well as financial requirements of any other official agency concerned with the development of the lands.

CONCLUSION:

In order to meet process requirements and to allow staff to prepare for upcoming OMB proceedings, the Planning and Building Department will be coming forward with recommendations to seek direction from Council.

ATTACHMENTS:

Appendix I-1: Site History
Appendix I-2: Aerial Photograph
Appendix I-3: Excerpt of Lakeview District Land Use Map
Appendix I-4: Excerpt of Existing Land Use Map
Appendix I-5: Concept Plan
Appendix I-6: Elevations
Appendix I-7: Agency Comments
Appendix I-8: School Accommodation
Appendix I-9: Relevant Mississauga Official Plan Policies
Appendix I-10: Proposed Zoning Standards
Appendix I-11: General Context Map

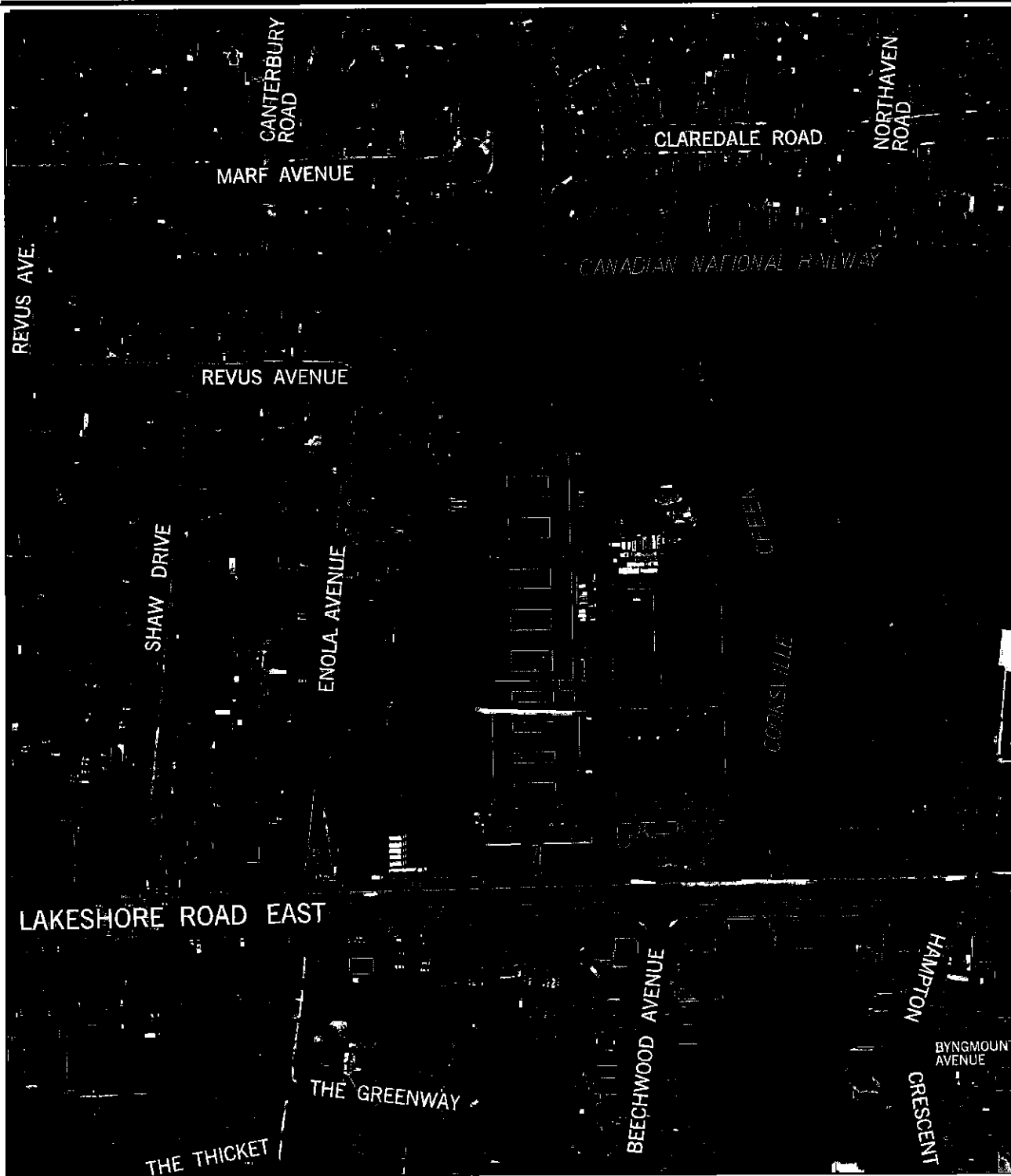


Edward R. Sajecki
Commissioner of Planning and Building

Prepared By: John Hardcastle, Development Planner

Site History

- January 8, 1999 – Region of Peel approved the Lakeview District Policies of City Plan, which designated the lands "Business Employment", "Mixed Commercial" and "Residential Low Density II".
- May 5, 2003 – Region of Peel approved the Lakeview District Policies of Mississauga Plan, which designated the lands "Business Employment", "Mainstreet Retail Commercial" and "Residential Low Density II".
- May 26, 2004 – Official Plan Amendment (OPA) #2 was approved by Council implementing the findings and recommendations of the April 2003 Special Policy Area Study for the Cooksville Creek Floodplain, prepared by Phillips Engineering. OPA #2 resulted in the creation of Special Site 21 in the Lakeview District Policies of Mississauga Plan.
- June 22, 2004 – OPA #2 was appealed to the OMB.
- March 10, 2005 – OPA #2 was approved as modified by the OMB and incorporated into the Lakeview District Policies of Mississauga Plan as Special Site 21.
- June 20, 2007 – Zoning By-law 0225-2007 came into force except for those sites which have been appealed. As no appeals were filed, the provisions of the new By-law apply. The subject lands are zoned "E2" (Employment), "C4" (Mainstreet Commercial) and "R3" (Detached Dwelling - Typical Lots).
- Summer 2009 – Floodline mapping revised by the CVC to reflect the reconstruction and upsizing of the Cooksville Creek culverts at Lakeshore Road East. Based upon revised mapping, the proposed development can achieve flood free access at certain locations on site.



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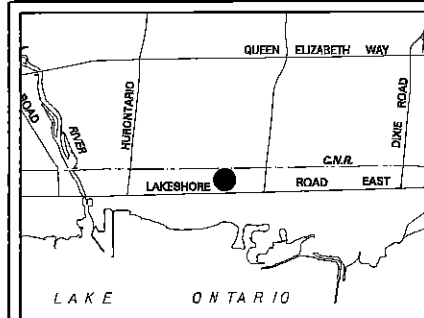


SUBJECT LANDS

DATE OF AERIAL IMAGERY: SPRING 2012



SUBJECT:
501 LAKESHORE INC., TRINITY PROPERTIES LAKESHORE INC.,
AND 1716336 ONTARIO INC.



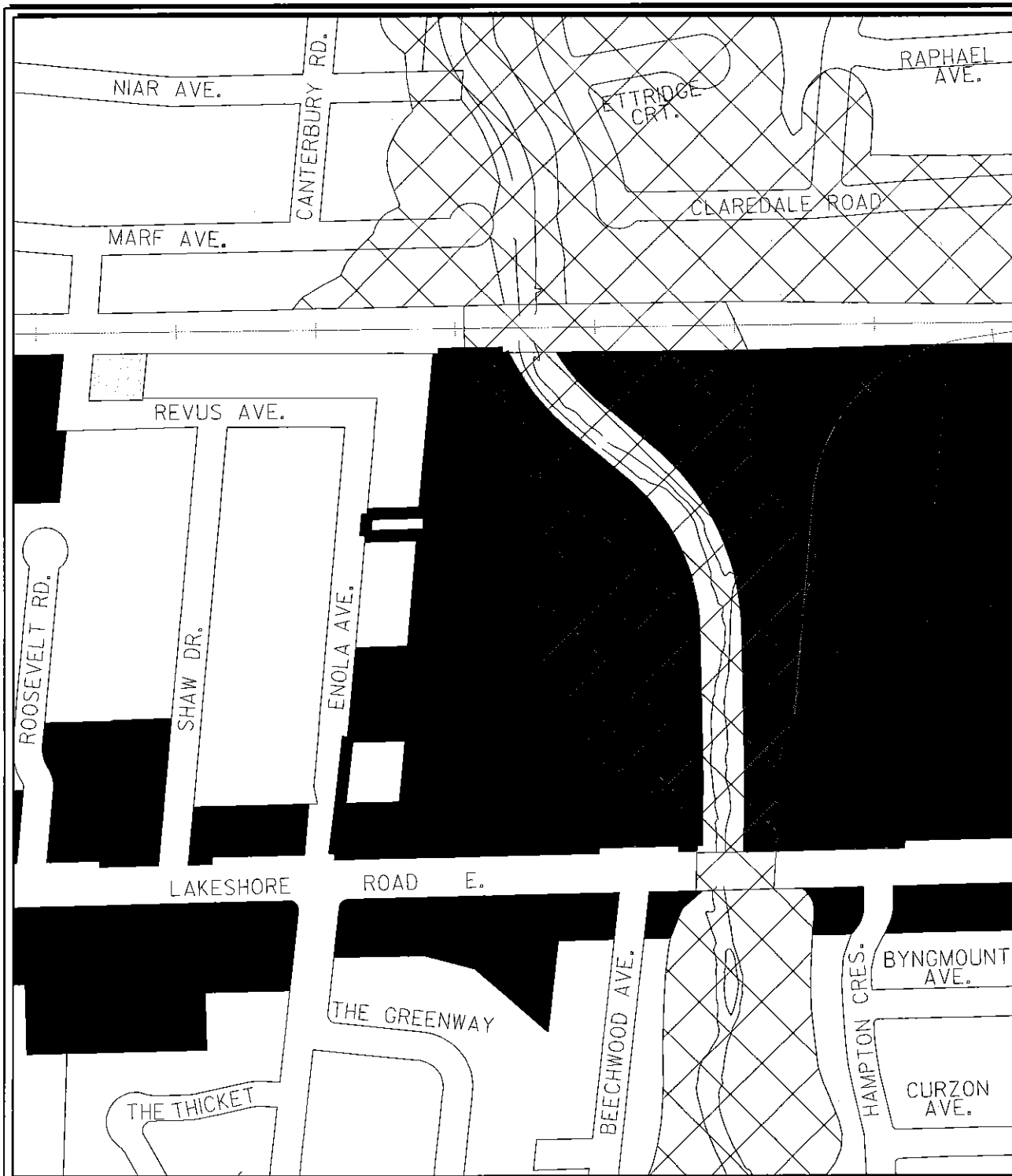
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OZ 11/017 W1
DWG. NO:
11017A
SCALE:
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PDC DATE:
2013/04/02
DRAWN BY:
A.SHAH

APPENDIX I-2

MISSISSAUGA
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PART OF SCHEDULE 10 LAND USE DESIGNATIONS OF MISSISSAUGA OFFICIAL PLAN

LAND USE DESIGNATIONS

Residential Low Density I	Business Employment
Residential Low Density II	Industrial
Residential Medium Density	Airport
Residential High Density	Institutional
Downtown Mixed Use	Public Open Space
Downtown Core Commercial	Private Open Space
Mixed Use	Greenbelt
Convenience Commercial	Parkway Belt West
Motor Vehicle Commercial	Utility
Office	To Be Determined

LAND USE LEGEND

Heritage Conservation District	Civic Centre (City Hall)
1996 NEP/2000 NEP Composite Noise Contours	City Centre Transit Terminal
LBPIA Operating Area Boundary See Aircraft Noise Policies	GO Rail Transit Station
Area Exempt from LBPIA Operating Area	Public School
Natural Hazards	Catholic School
	Hospital
	Community Facilities

CITY STRUCTURE

Elements

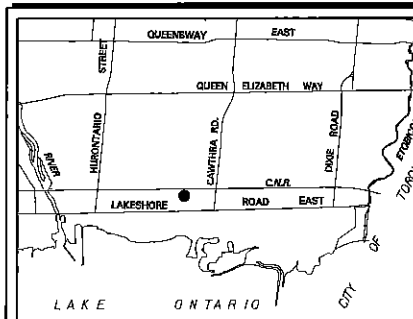
Downtown	Corporate Centre
Major Node	Employment Area
Community Nodes	Special Purpose Area
Neighbourhood	

ND2 Region of Peel Non-Decision

SUBJECT LANDS



SUBJECT:
501 LAKESHORE INC., TRINITY PROPERTIES LAKESHORE INC.,
AND 1716336 ONTARIO INC.



FILE NO:
OZ 11/017 W1

DWG. NO:
11017L

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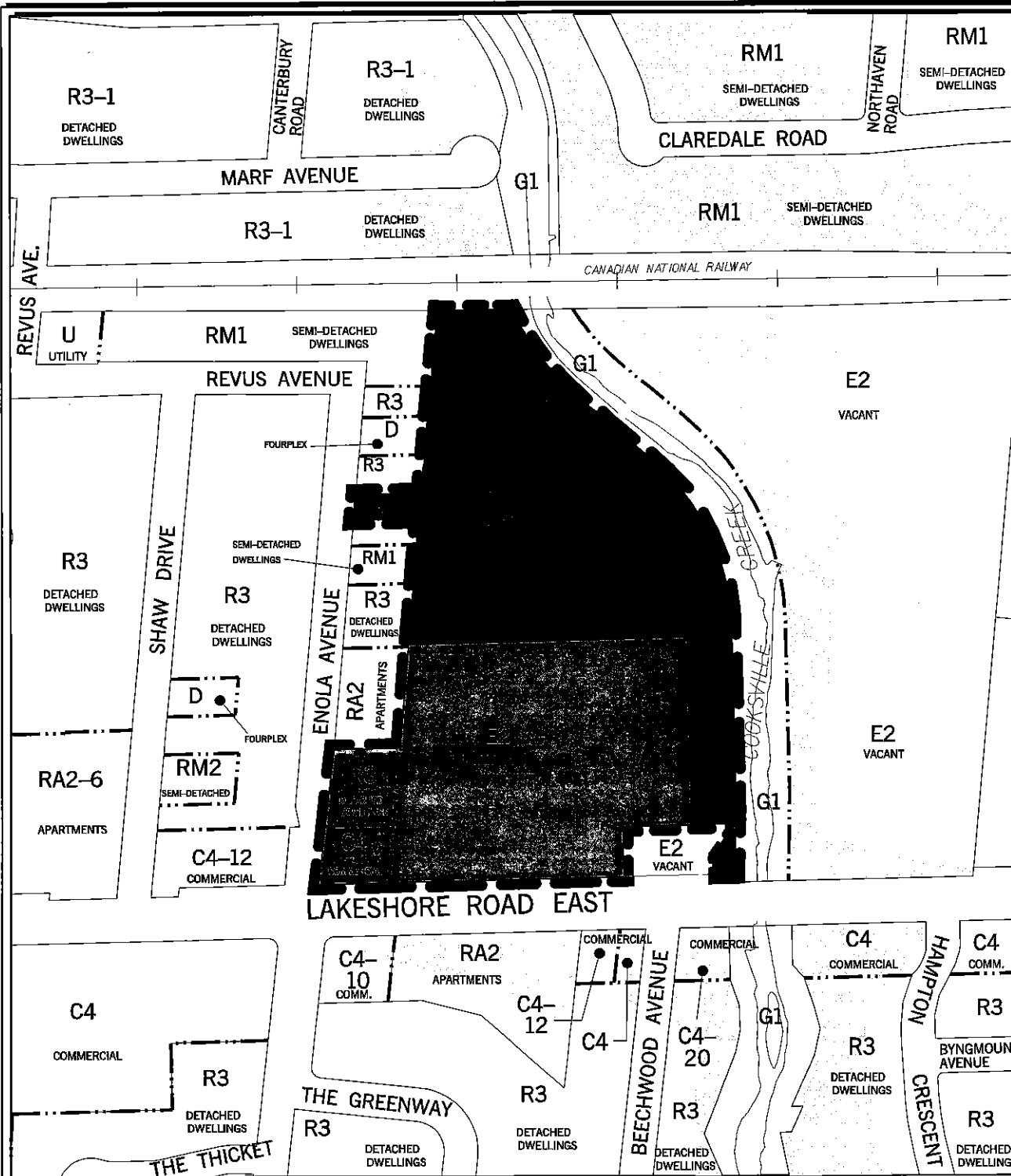
DATE:
2013/04/02

DRAWN BY:
A.SHAH

APPENDIX I-3

MISSISSAUGA
Planning and Building

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LEGEND:



SUBJECT LANDS



PROPOSED OFFICIAL PLAN AMENDMENT FROM "RESIDENTIAL LOW DENSITY II", "MIXED USE" AND "BUSINESS EMPLOYMENT" TO "MIXED USE-SPECIAL SITE" AND PROPOSED REZONING FROM "R3" (DETACHED DWELLINGS-TYPICAL LOTS), "C4" (MAINSTREET COMMERCIAL) AND "E2" (EMPLOYMENT) TO "C4-EXCEPTION" (MAINSTREET COMMERCIAL) TO PERMIT A MIX OF RETAIL COMMERCIAL AND OFFICE USES.



PROPOSED OFFICIAL PLAN AMENDMENT FROM "RESIDENTIAL LOW DENSITY II" AND "BUSINESS EMPLOYMENT" TO "RESIDENTIAL HIGH DENSITY-SPECIAL SITE" AND PROPOSED REZONING FROM "R3" (DETACHED DWELLINGS-TYPICAL LOTS) AND "E2" (EMPLOYMENT) TO "RA5-EXCEPTION" (APARTMENT DWELLINGS) TO PERMIT APARTMENT DWELLINGS, TOWNHOUSE DWELLINGS AND ALL FORMS OF LOW-RISE MULTIPLE DWELLINGS.

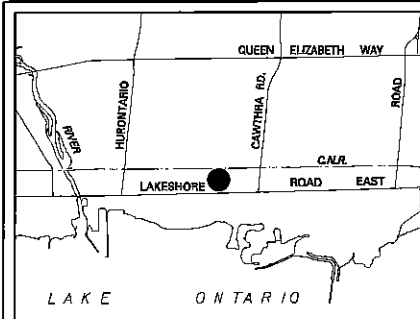


PROPOSED OFFICIAL PLAN AMENDMENT FROM "BUSINESS EMPLOYMENT" TO "GREENBELT" AND PROPOSED REZONING FROM "E2" (EMPLOYMENT) TO "G1" (GREENBELT-NATURAL HAZARDS) TO PERMIT GREENBELT.

NOTE: EXISTING ZONING DELINEATED ON THE PLAN
PROPOSED ZONING INDICATED BY SHADING WITHIN
THE APPLICATION AREA.



SUBJECT:
501 LAKESHORE INC., TRINITY PROPERTIES LAKESHORE INC.,
AND 1716336 ONTARIO INC.



FILE NO:

OZ 11/017 W1

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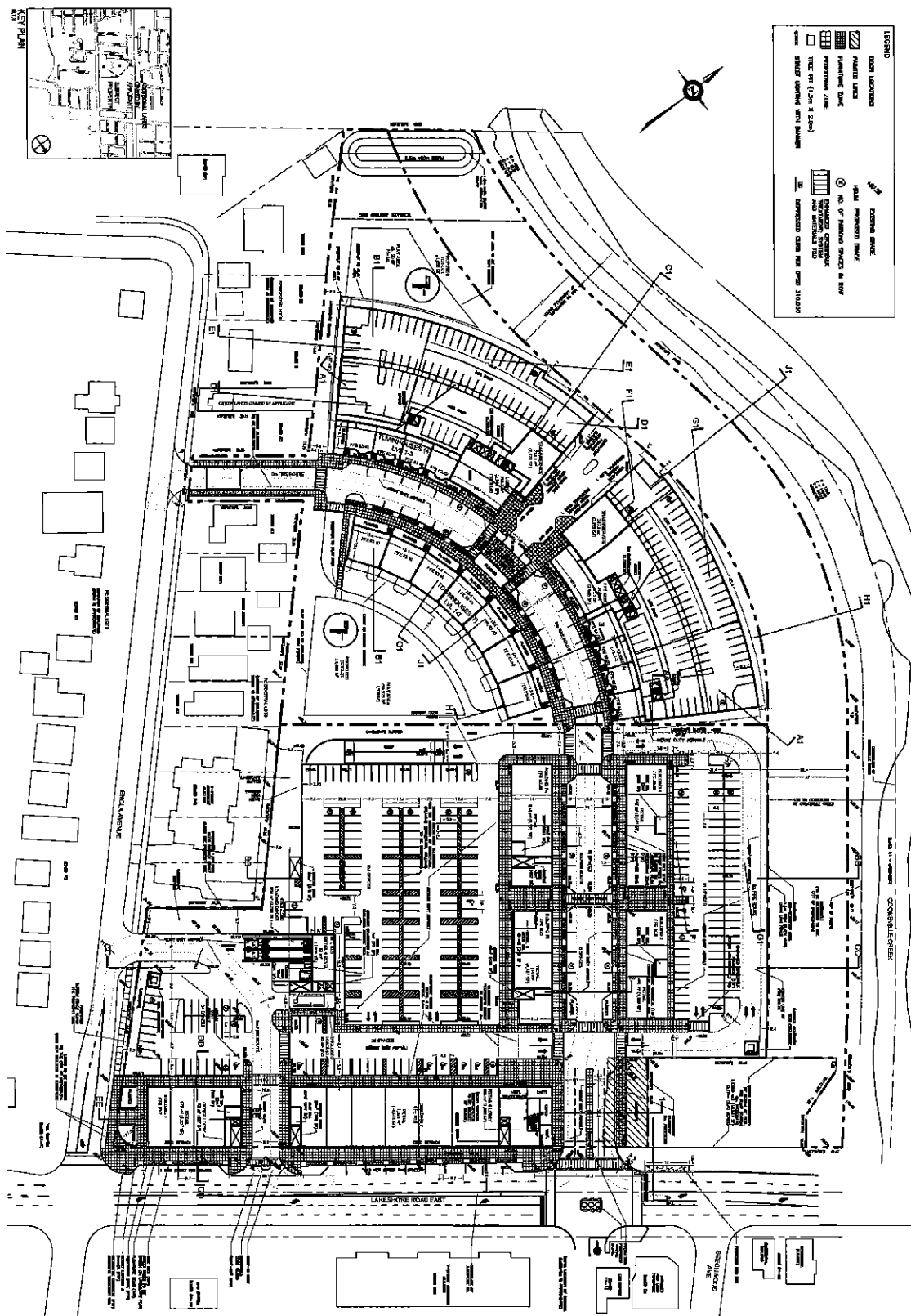
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A.SHAH

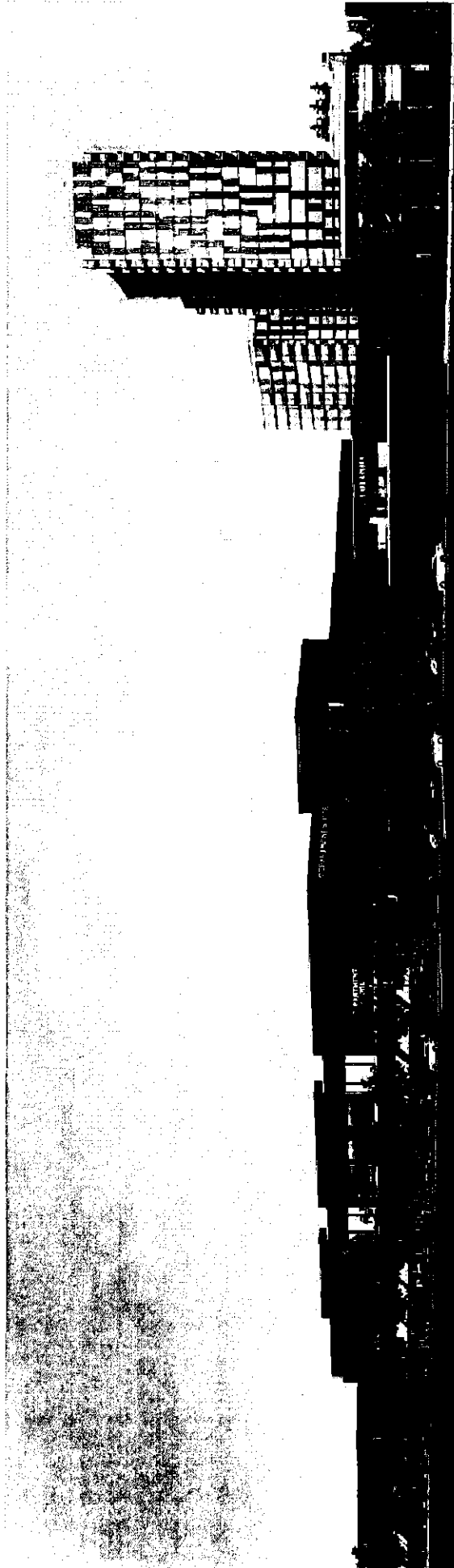
APPENDIX I-4

MISSISSAUGA
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CONCEPT PLAN



PROPOSED ELEVATIONS

501 Lakeshore Inc. et. al.

File: OZ 11/017 W1

Agency Comments

The following is a summary of comments from agencies and departments regarding the applications.

Agency / Comment Date	Comment
Region of Peel (March 1, 2013)	<p>Comments detailed Regional servicing facilities in the vicinity of the site, acknowledged revision requirements to the Functional Servicing Report (FSR) necessary prior to the preparation of a Supplementary Report and garbage collection and agreement requirements.</p> <p>In addition, comments advised that additional easement requirements for access and servicing connections for the Beechwood Pumping Station will be forthcoming upon finalization of building design and placement.</p>
Dufferin-Peel Catholic District School Board and the Peel District School Board (March 1, 2013)	<p>Both School Boards responded that they are satisfied with the current provision of educational facilities for the catchment area and, as such, the school accommodation condition as required by City of Mississauga Council Resolution 152-98 pertaining to satisfactory arrangements regarding the adequate provision and distribution of educational facilities need not be applied for these development applications.</p> <p>In addition, if approved, both School Boards also require that warning clauses with respect to temporary school and transportation arrangements be included in any Agreements of Purchase and Sale and the Development and/or Servicing Agreements.</p>
Credit Valley Conservation (February 6, 2013)	<p>The subject lands contain a portion of the Cooksville Creek corridor and are subject to Credit Valley Conservation's (CVC's) Development, Interference with Wetlands, and Alterations to Shorelines and Watercourses Regulation – Ontario Regulation 160/06. As such, a permit from CVC is required prior to any development occurring within the Regulated Area on the site.</p> <p>CVC staff is satisfied with the feasibility of the proposed works associated with the modifications to the valley corridor</p>

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File: OZ 11/017 W1

Agency / Comment Date	Comment
	<p>consistent with the recommendations of the "Special Policy Area Study For The Cooksville Creek Floodplain" (prepared for the City of Mississauga, April 2003). Accordingly, CVC staff recommends the applicant be required to enter into a development and/or servicing agreement which include provisions pertaining to the following:</p> <ol style="list-style-type: none"> 1. Appropriate Official Plan and Zoning By-law designations of the creek corridor; 2. Dedication of the creek corridor to the City; 3. Restoration of valley corridor, including the removal of the existing bridge and restoration and enhancement of the valley corridor; 4. Detailed plans related to stormwater management, grading (including proposed cut-fill and valleyland grading), landscape restoration, and erosion and sediment control measures; 5. Confirmation of the stability of the proposed flood control landform from a qualified geotechnical engineer; and 6. Confirmation that the portions of the subject property proposed for development have been removed from the flood and/or erosion hazard associated with Cooksville Creek. This provision should be required prior to final by-law enactment, implemented through a holding provision or other measure as deemed appropriate by the City. <p>It is anticipated that the CVC permitting process will be implemented in two phases. The first phase will deal with the works necessary to remove portions of the property proposed for development out of the hazards, in an effort to fulfill item no. 6 above. The second phase will deal with permitting the works associated with table land grading and construction of any buildings or structures within the Regulated Area, outside of the valley corridor. Works associated with the removal of the bridge and restoration, enhancement or site remediation of the valley corridor may be implemented through either phase of the permitting process as appropriate.</p> <p>A Risk Assessment approach to the existing soil and ground water contamination is being pursued and that a recent</p>

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Agency / Comment Date	Comment
	<p>resubmission was provided to the Ministry of Environment. Materials reviewed to date indicate that contamination may have entered the adjacent CVC lands to the east. CVC staff request that all supporting materials be provided to CVC staff for review and additional comments under separate cover may be provided.</p>
<p>City Community Services Department – Parks and Forestry Division/Park Planning Section (March 1, 2013)</p>	<p>Future residents of the proposal will receive park service at the Adamson Estate (P-169), which is located approximately 275 m (902 ft.) from the subject site and contains recreational trails that form part of the City's waterfront trail network. Spruce Park (P-029) is located approximately 390 m (1,279 ft.) from the subject property and contains basketball hoops, a play set and recreational trails. Both facilities provide public parking as well.</p> <p>In the event that the applications are approved, the Community Services Department - Park Planning note the following conditions:</p> <p>Prior to By-Law Enactment, the lands below the Regional Storm floodplain, or within the stability and/or erosion component of the valley slope, whichever is greater, shall be deeded gratuitously to the City as greenbelt and shall be appropriately zoned. The Risk Assessment Addendum submitted by the applicant states the existence of contaminants on site and recommends several monitoring responsibilities that the City would be responsible for as a condition of the aforementioned gratuitous land dedication. Prior to accepting ownership of lands below the Regional Storm Floodplain, remediation procedures and resulting site conditions must be to the satisfaction of the City's Community Services Department and Transportation and Works Department. Community Services is not prepared to take on monitoring responsibilities as outlined in the Risk Assessment Addendum. Further, the applicant is required to submit a Greenbelt Restoration Plan that will include greenbelt grading details, a rehabilitation plan, a tree inventory and preservation plan and address the removal of any encumbrances and any related underground infrastructure. Securities will be required for the reinstatement of the Greenbelt lands.</p>

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Agency / Comment Date	Comment
	Prior to the issuance of building permits for each lot or block cash-in-lieu for park or other public recreational purposes is required pursuant to Section 42 of the <i>Planning Act</i> (R.S.O. 1990, c.P. 13, as amended) and in accordance with City's Policies and By-laws.
City Community Services Department – Fire and Emergency Services Division (March 4, 2013)	Fire has reviewed the applications from an emergency response perspective and has no concerns; emergency response time to the site and water supply available are acceptable.
City Economic Development Office (February 21, 2013)	<p>The Economic Development Office advised they have no comments or concerns from an economic development perspective. As such, they have no objection to the continued processing of the applications.</p> <p>The site is not part of a homogeneous business employment area. Rather it can be viewed as an anomaly based on a historic manufacturing use within an area now characterized primarily as residential in nature with mainsheet retail uses along the north and south side of Lakeshore Road East, in proximity to this property. The long term economic viability of the retained 'business employment' land use designation on this property is limited from our perspective. The ability to attract either industrial or office commercial uses appear limited given the sites location within the City. They are of the opinion the redesignation of this property to a mixed-use development, including retail and office commercial uses, would not significantly impact the City's employment base or jeopardise or establish a domino effect within other employment land areas of the planning district; most notably those lands located south of Lakeshore Road East between Hydro Road and East Avenue.</p>
City Transportation and Works Department (February 7, 2013)	The applicant has also provided Phase 1 and Phase 2 Environmental Site Assessment (ESA) reports and a draft Risk Assessment report for review. The Phase 2 ESA has indicated that ground water on the site is contaminated with Volatile Organic Compounds (VOC's), including vinyl chloride. As the site is proposed to change from a less sensitive use to a more sensitive use, a Record of Site Condition (RSC) must be

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File: OZ 11/017 W1

Agency / Comment Date	Comment
	<p>filed on the Ministry of Environment's (MOE) Environmental Registry in accordance with Ontario Regulation 153/04, as amended.</p> <p>The applicant is proposing to use a Risk Assessment approach to deal with the contamination, rather than remediating the site and has submitted a draft Risk Assessment report to the MOE. The MOE has provided comments to the applicant and an Addendum report has recently been submitted for MOE review. This process is not likely to conclude in advance of the planning process and the MOE's position may not be known prior to the preparation of a Supplementary Report.</p> <p>It would be preferable to remediate the site rather than to utilize a Risk Assessment approach to dealing with the ground water contamination on the site. However, the services of an outside consulting firm with risk assessment expertise has been retained to review the revised Risk Assessment reports and assist with understanding the long term implications and measures which may be required through the planning review process. Additional comments, including necessary adjustments to the proposal, may be forthcoming.</p> <p>A satisfactory Utility Plan and Streetscape Plan have also been requested to determine the feasibility of the proposed boulevard works along Lakeshore Road which may also be required for PUCC approval. The owner will also be required to obtain approval from the CVC with respect to cut-fill balances and valleyland grading and any floodplain and landscape restoration work. In addition, the applicant is to provide CVC the results of the digital hydraulic assessment to confirm the feasibility of the proposed floodplain modification works.</p> <p>All lands below the established top of bank or Regional Floodline, whichever is greater, shall be deeded gratuitously to the City and zoned as greenbelt. Prior to the acceptance of any lands, the City of Mississauga requires that all lands dedicated to the City meet appropriate MOE standards.</p>

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File: OZ 11/017 W1

Agency / Comment Date	Comment
Canada Post (March 1, 2013)	<p>A detailed review of the applications has been completed and an easement may be required to service the subject property, depending on a review of more detailed applications under the <i>Planning Act</i>.</p> <p>Canada Post's delivery policy has changed as of January 1, 2013. There is now a fee per unit for all addresses assigned to mail delivery from their Community Mailboxes. Mailroom customers are exempt as their mailboxes are provided by the applicants.</p>
GO Transit (Metrolinx) (March 1, 2013)	<p>In addition to outlining detailed revisions to the supporting drawings, Environmental Noise Feasibility Study and Railway Vibration Analysis, the need for Development Agreement and Purchase and Sale Agreement warning clauses are outlined in their comments.</p>
Other City Departments and External Agencies	<p>The following City Departments and external agencies offered no objection to these applications provided that all technical matters are addressed in a satisfactory manner:</p> <ul style="list-style-type: none"> - Bell Canada - Enersource Hydro Mississauga - Rogers Cable - Credit Valley Hospital - Canada Post
	<p>The following City Departments and external agencies were circulated the applications but provided no comments:</p> <ul style="list-style-type: none"> - Culture Division - Development Services - Realty Services - Hydro One Networks - Conseil Scolaire de District Catholique Centre-Sud - Conseil Scolaire de District Centre-Sud-Ouest - Enbridge Gas Distribution - The Trillium Health Centre

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File: OZ 11/017 W1

School Accommodation

The Peel District School Board	The Dufferin-Peel Catholic District School Board
<ul style="list-style-type: none"> Student Yield: <ul style="list-style-type: none"> 26 Kindergarten to Grade 6 14 Grade 7 to Grade 8 27 Grade 9 to Grade 12/OAC School Accommodation: <ul style="list-style-type: none"> Janet I. McDougald Public School <ul style="list-style-type: none"> Enrolment: 525 Capacity: 580 Portables: 1 Allan A. Martin Sr. Public School <ul style="list-style-type: none"> Enrolment: 462 Capacity: 538 Portables: 1 Cawthra Park Secondary School <ul style="list-style-type: none"> Enrolment: 1,330 Capacity: 1,044 Portables: 6 <p>* Note: Capacity reflects the Ministry of Education rated capacity, not the Board rated capacity, resulting in the requirement of portables.</p>	<ul style="list-style-type: none"> Student Yield: <ul style="list-style-type: none"> 7 Junior Kindergarten to Grade 8 6 Grade 9 to Grade 12/OAC School Accommodation: <ul style="list-style-type: none"> St. Dominic <ul style="list-style-type: none"> Enrolment: 293 Capacity: 253 Portables: 5 St. Paul <ul style="list-style-type: none"> Enrolment: 708 Capacity: 807 Portables: 0

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Relevant Mississauga Official Plan Policies

Lakeview Local Area Plan

Section 2.1 - The scale and character of all building and landscape designs will take into consideration the guidelines established in the Lakeshore Road Design Concept.

Section 4.19 - The lands identified as Special Site 19 are located on the north side of Lakeshore Road East west of Cawthra Road and on the east and west sides of Cooksville Creek. Notwithstanding the Business Employment and Greenbelt designations of this Plan, the following additional policy will apply:

a. ingress/egress for all new development will be such that emergency vehicular and pedestrian movement is not prevented during times of flooding in order that safe access/evacuation is ensured. The determination of safe access will be made by the Credit Valley Conservation and the City, and will be based on depth and velocity factors.

Direct Growth

Section 5.1.7 - Mississauga will protect and conserve the character of stable residential Neighbourhoods.

Section 5.3.5.1 - Neighbourhoods will not be the focus for intensification and should be regarded as stable residential areas where the existing character is to be preserved.

Section 5.3.5.5 - Intensification within Neighbourhoods may be considered where the proposed development is compatible in built form and scale to surrounding development, enhances the existing or planned development and is consistent with the policies of this Plan.

Road Network

Section 8.2.2.5 - Additional roads may be identified during the review of development applications and the preparation of local area plans. The City may require the completion of road connections and where appropriate, the creation of a denser road pattern through the construction of new roads.

Section 8.2.2.7 - Future additions to the road network should be public roads. Public easements may be required where private roads are permitted.

Build a Desirable Urban Form

Section 9.1.3 - Infill and redevelopment within Neighbourhoods will respect the existing planned character.

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Non-Intensification Areas

Section 9.2.2 - Non-intensification areas will experience limited growth and change; consequently, intensive growth will not be directed to them. Non-Intensification Areas consist of:

- Neighbourhoods;
- Employment Areas;
- Special Purpose Areas; and
- Corridors.

Section 9.2.2.1 - Heights in excess of four storeys will be required to demonstrate that an appropriate transition in height and built form that respects the surrounding context will be achieved.

Section 9.2.2.3 - Tall buildings will generally not be permitted.

Section 9.2.2.4 - While new development need not mirror existing development, new development in Neighbourhoods will:

- a. respect existing lotting patterns;
- b. respect the continuity of front, rear and side yard setbacks;
- c. respect the scale and character of the surrounding area;
- d. minimize overshadowing and overlook on adjacent neighbours;
- e. incorporate stormwater best management practice;
- f. preserve mature high quality trees and ensure replacement of the tree canopy; and
- g. be designed to respect the existing scale, massing, character and grades of the surrounding area.

Green Systems

Section 9.2.3.1 - Development will be sensitive to the site and ensure that Natural Areas Systems are protected, enhanced and restored.

Public Realm

Section 9.3.1.4 - Development will be designed to:

- a. respect the natural heritage features, such as forests, ridges, valleys, hills, lakes, rivers, streams and creeks;
- b. respect cultural heritage features such as designated buildings, landmarks and districts;
- c. accentuate the significant identity of each Character Area, its open spaces, landmarks and cultural heritage resources;
- d. achieve a street network that connects to adjacent streets and neighbourhoods at regular intervals, wherever possible;
- e. meet universal design principles;
- f. address new development and open spaces;

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- g. be pedestrian-oriented and scaled and support transit use;
- h. be attractive, safe and walkable;
- i. accommodate a multi-modal transportation system; and
- j. allow common rear laneways or parallel service streets to provide direct access for lots fronting arterial roads and major collector roads, when appropriate.

Section 9.3.1.7 - Streetscapes will be designed to create a sense of identity through the treatment of architectural features, forms, massing, scale, site layout, orientation, landscaping, lighting and signage.

Site Development and Buildings

Section 9.5.1 - Context addresses how developments demonstrate compatibility and integration with surrounding land uses and vegetation by ensuring that an effective transition in built form is provided between areas of different development densities and scale, and the protection of natural features.

Section 9.5.1.2 - Developments should be compatible and provide appropriate transition to existing and planned development by having regard for the following elements:

- a. Natural Areas System;
- b. natural hazards (flooding and erosion);
- c. natural and cultural heritage features;
- d. street and block patterns;
- e. the size and configuration of properties along a street, including lot frontages and areas;
- f. continuity and enhancement of streetscapes;
- g. the size and distribution of building mass and height;
- h. front, side and rear yards;
- i. the orientation of buildings, structures and landscapes on a property;
- j. views, sunlight and wind conditions;
- k. the local vernacular and architectural character as represented by the rhythm, textures and building materials;
- l. privacy and overlook; and
- m. the function and use of buildings, structures and landscapes.

Section 9.5.1.5 - Developments will provide a transition in building height and form between intensification Areas and adjacent Neighbourhoods with lower density and heights.

Section 9.5.1.9 - Development proposals will demonstrate compatibility and integration with surrounding land uses and the public realm by ensuring that adequate privacy, sunlight and sky views are maintained and that micro-climatic conditions are mitigated.

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Section 9.5.5.2 - Above grade parking structures should be screened in such a manner that vehicles are not visible from public view and have appropriate directional signage to the structure.

Retail

Section 10.4 - The primary locations for retail uses will be the Downtown, Major Nodes and Community Nodes. Retail uses within these locations will be encouraged to contribute to a vibrant, mixed use environment and be developed in combination with residential and office uses....

Within Neighbourhoods, further retail commercial will be directed to lands designated Mixed Use. Retail uses will be encouraged to develop in combination with residential and office uses.

Section 10.4.1 - Retail uses are encouraged to locate primarily within the Downtown, Major Nodes and Community Nodes.

Section 10.4.5 - Retail uses outside the Downtown, Major Nodes and Community Nodes will be directed to Corridors and Major Transit Station Areas or in locations as identified in Character Area policies or local area plans.

Section 10.4.6 - The dispersion of retail uses beyond designated commercial areas will be discouraged.

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Proposed Zoning Standards

	Base "RA5" Standard	Proposed "RA5-Exception" Standard
To permit townhouse and horizontal multiple dwellings	Not Permitted	Yes
Minimum Floor Space Index – Apartment Dwelling Zone	1.9	1.0
Maximum Floor Space Index – Apartment Dwelling Zone	2.9	2.0
Maximum Gross Floor Area	n/a	38 000 m ² (409,042 sq. ft.)
Minimum Landscaped Area	40% of lot area	30% of lot area
Minimum Landscaped Buffer – measured from Greenbelt Zone	4.5 m (14.8 ft.)	2.4 m (7.9 ft.)
Minimum Landscaped Buffer – measured from Commercial Zone	n/a	0.0 m (0 ft.)
The lands shall be deemed one lot for zoning purposes	n/a	Yes
The lot line abutting the private road shall be deemed the front lot line	n/a	Yes
Exception scheduled proposed	n/a	Yes

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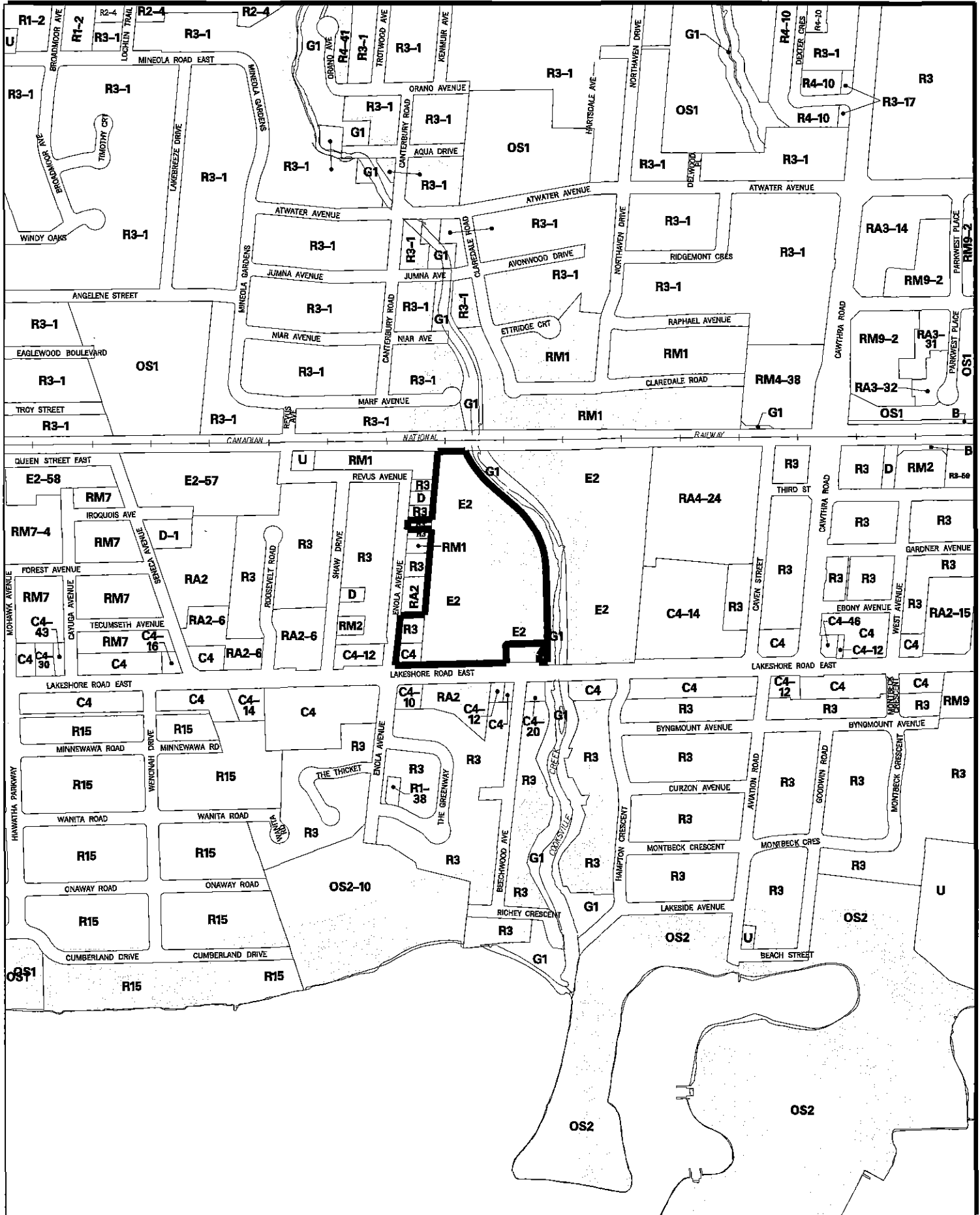
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	Base "C4" Standard	Proposed "C4 - Exception" Standard
To permit home furnishing store	Not Permitted	Yes
Minimum Height – Flat Roof	2 storeys	1 storey
Maximum Height – Flat Roof	12.5 m (41 ft.) and 3 storeys	15.6 m (51 ft.)
Minimum Landscaped Buffer – measured from Greenbelt Zone	4.5 m (14.8 ft.)	2.4 m (7.9 ft.)
Minimum Landscaped Buffer – measured from Employment Zone	3.0 m (9.8 ft.)	0.0 m (0 ft.)
Minimum Separation of Restaurant uses to a Residential Zone	60 m (197 ft.)	13 m (42.7 ft.)
Minimum Number of Loading Spaces		3 spaces
Required parking spaces per 100 m² (1,076.4 sq. ft.) Gross Floor Area-Non Residential	4.0 (Certain uses permitted in C4 Zone)	3.25
The lands shall be deemed one lot for zoning purposes	n/a	Yes
The lot line abutting the private road shall be deemed the front lot line	n/a	Yes
Exception scheduled proposed	n/a	Yes

GENERAL CONTEXT MAP

OZ 11/017 W1

APPENDIX I-11



APR 02 2013



Corporate Report

Clerk's Files

Originator's
Files

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DATE: March 12, 2013

TO: Chair and Members of Planning and Development Committee
Meeting Date: April 2, 2013

FROM: Edward R. Sajecki
Commissioner of Planning and Building

SUBJECT: **East Bloor Corridor Review–Backgrounder and Interim Strategy-
Opportunities for Neighbourhood Revitalization
Ward 3**

RECOMMENDATION: That the report titled “East Bloor Corridor Review–Backgrounder and Interim Strategy-Opportunities for Neighbourhood Revitalization” dated March 12, 2013, from the Commissioner of Planning and Building be received for information and that the findings be used in the review of development applications in the study area.

**REPORT
HIGHLIGHTS:**

- The East Bloor Corridor is an important area for affordable rental housing and is a gateway community for new Canadians;
- There is some opportunity to accommodate infill development as permitted in the Official Plan, which could positively contribute to the revitalization of the area and provide a more cohesive built form;
- Urban Design Guidelines (Interim) have been prepared to assist in the review of development applications and to ensure development contributes positively to the character of the area;

- To promote and ensure reinvestment in the existing buildings and revitalization of the neighbourhood, infill development applications should include a Property Standards Building Audit. Further, the City should investigate potential incentives to encourage property owners to undertake upgrades including securing community benefits under Section 37 of the *Planning Act*; and
- Further investigation is recommended to address issues related to the following: transportation requirements; cultural and social infrastructure; streetscape; and possible community improvement initiatives.

BACKGROUND:

The East Bloor Corridor, generally located between Dixie Road and the Etobicoke Creek, is an important neighbourhood in the City that provides significant affordable housing choices. This, along with the surrounding services and amenities, has led to part of the area becoming a gateway community for new Canadians.

The predominant land use in the corridor is mid-rise and tower apartments along with some townhouse complexes, that were generally developed approximately 40 years ago. The apartment buildings were inspired by the then popular “towers in the park” design, where buildings were situated well beyond sidewalks, leaving generous amounts of room on the property. It was assumed that adults living in these apartments would all have cars which resulted in a significant amount of land dedicated to surface parking. The buildings were intended to appeal to young professionals and young families looking for a more suburban experience. Over time the neighbourhood became increasingly attractive to new immigrants and people looking for affordable housing choices.

As areas like the East Bloor Corridor age and change, concerns are often raised regarding the condition of the buildings and neighbourhood. Due to the age of the development in the East Bloor Corridor, as well as the focus now on intensification as the City continues to urbanize, it is timely to investigate opportunities for reinvesting and revitalizing the neighbourhood.

Attached under separate cover is the *East Bloor Corridor Review - Backgrounder and Interim Strategy, March 2013*, that examines the opportunity for infill development, and the potential for reinvestment and revitalization of the area. This report outlines the key sections and findings from the review.

COMMENTS:**Study Area**

The East Bloor Corridor study area extends for approximately 1.8 km (1.1 miles) along Bloor Street, from west of Dixie Road to the municipal boundary of Etobicoke Creek (see Map 1 in the study, under separate cover). The area encompasses more than 60 land parcels with an average size of approximately 1 hectare (2.5 acres) for a total land area of 84 hectares (208 acres).

Character and Context

The general character of the Corridor is as follows:

- approximately 13,300 people live in the study area;
- much of the area is considered a gateway for new Canadians, with 43% of the residents having immigrated between 2001 and 2006;
- residents have access to a wide range of retail, personal service and community infrastructure located within, or in proximity, to the study area;
- there is a significant concentration of apartment buildings with approximately 63% of properties containing buildings that are 5 storeys or more in height; and
- most of the properties are rental buildings with an overall vacancy rate of 1.2%, which is below the City average and also below what is often considered a balanced market of 3.0%.

Key Findings

Key findings include the following:

- a preliminary assessment suggests that many sites have constraints that would limit the extent to which additional development can occur. There are properties along the corridor that have some potential for infill, depending on the specific proposal. The remaining residential sites have either limited infill potential or are significantly constrained;
- in addition to infill on apartment sites, the Official Plan permits residential development on the two commercial plazas and there are four detached houses on Dixie Road designated for medium density development;
- although the Official Plan limits infill on apartment sites to medium density uses (e.g. townhouses), there may be opportunities to allow for additional apartment development which could positively contribute to the area. Given the different characteristics of each property, a site specific review as part of an Official Plan Amendment is the recommended approach;
- the Official Plan contains policies that require infill development to address a range of issues (e.g. transition, respecting scale and character, etc.) to be reviewed when considering infill applications. In addition, as a condition of development on sites that have existing apartment buildings, the site in its entirety must meet, amongst other matters, current site plan and landscaping requirements and property standards;
- from an urban design perspective, the elements that define the study area are:
 - the type of buildings such as midrise, towers and townhouses;
 - no discernible pattern to the siting, location and placement of buildings on apartment sites;

- the location and character of landscaped open space that wraps frontages and side yards of apartment buildings;
 - the type and mix of landscape materials and species generally consisting of deciduous and evergreen trees and shrubbery on many apartment sites; and
 - the siting and location of midrise, tall towers and lower apartment buildings in proximity to Bloor Street, with some lower forms transitioning out towards the surrounding low-rise residential areas.
- recognizing that infill opportunities exist in the area, Urban Design Guidelines (Interim) are necessary to ensure the character defining features are protected and elements such as compatibility, transition and enhancement are considered;
- due to the age of the apartment stock, infill development on existing high density residential sites should include the submission of a Property Standards Building Audit to address a range of issues including, but not limited to: graffiti removal; structural soundness; and maintenance of lighting, heating, plumbing, mechanical systems, elevating devices, grass and landscaped areas in reasonable condition;
- property owners should be encouraged to undertake additional upgrades to their properties relating to items such as: green development standards; community amenities; safety audits; and, improved pedestrian infrastructure. These additional upgrades may require incentives or partnerships which need to be further examined; and
- for infill development, the opportunity may exist for community benefits to be secured under Section 37 of the *Planning Act*.

Study Conclusions and Recommendations

The study examines the existing characteristics and context of the East Bloor Corridor. It reviews the planning framework and opportunities for investment that would positively contribute to the area. A general assessment of potential infill locations has been undertaken and

interim urban design guidelines prepared. Conclusions of the review are:

- the area can benefit from sensitive infill as permitted in the Official Plan. Opportunities for infill are generally distributed around three areas along the 1.8 km (1.1 miles) corridor (see Section 5.2 and Map 5 in the study, under separate cover). There may be a few opportunities where development greater than permitted in the Official Plan may be appropriate, however, those cases should be reviewed on a site specific basis;
- the Official Plan includes policies that provide direction on intensification and infill development and, where appropriate, allows the City to require additional studies from applicants to assess a proposed development's impacts on traffic and community infrastructure;
- Urban Design Guidelines (Interim) have been prepared to assist in the review of applications, and through the Site Plan Approval process, any additional development would be evaluated to ensure it contributes positively to the area and appropriately reflects a cohesive built form; and
- mature trees along Bloor Street have created a strong presence in the corridor; this should be maintained and supplemented with significantly more green landscaping. It is proposed that the corridor evolve into a special "Tree District";
- the study recommends a number of initiatives to promote and ensure reinvestment in the existing apartment buildings and neighbourhood, such as:
 - requiring a Property Standards Building Audit;
 - encouraging further building and property upgrades (e.g. green development standards); and,
 - securing community benefits in appropriate situations, through Section 37 of the *Planning Act*.

These findings and recommendations provide the basis for reviewing applications and serve as the starting point for a second phase of the study that will include:

- transportation review: examine current transportation issues in the area, including potential changes to active transportation (e.g. potential cycling lanes);
- cultural and social infrastructure review: examine potential initiatives that can help strengthen the community (e.g. public art, additional support services);
- streetscape improvements: research and identify appropriate public realm strategies that can improve the area; and
- community improvement initiatives: examine the policy tools available to municipalities to encourage property owners to improve buildings and investigate experiences in Toronto with the Tower Renewal program.

In addition, where appropriate, further work should include a public engagement component, to identify and understand concerns as well as potentially generate ideas for improvement. As a result of any future work, it may be necessary to review the Official Plan and provide additional policies and/or guidelines.

STRATEGIC PLAN:

The East Bloor Corridor Review responds to the following pillars of the Strategic Plan:

Move – setting the stage for considering how reinvestment, infill and city initiatives in the corridor, can enhance connections and create significant, positive impacts as it relates to the manner people move about the area (e.g. enhancing connections to a well-served transit stop).

Belong – recognizing the contribution the area can play in creating a City that thrives on its social and cultural diversity. Provision of affordable housing and convenient access to services, which nurture this cultural diversity, are an important aspect to maintain.

Connect – examining intensification and revitalization within the study area and how it can help contribute to creating a city which is vibrant with safe neighbourhoods and great public spaces.

Living Green – recognizing that revitalization in the study area offers the opportunity to promote responsible stewardship through conservation, restoration and enhancement of the natural environment.

FINANCIAL IMPACT: Not Applicable

CONCLUSION:

The East Bloor Corridor is an older high density residential neighbourhood that plays an important role as a location for affordable housing and a gateway community for new Canadians. It is also an aging neighbourhood with potential development pressures. As such, the East Bloor Corridor Review was undertaken to investigate opportunities to revitalize and reinvest in the area, with specific attention to infill development.

The study concludes that there is some opportunity to accommodate infill development which in turn could benefit the area. The Official Plan provides direction on infill and allows the City to require studies to assess impacts from a specific development. To promote the positive contributions revitalization can provide to the community, the study makes a number of additional recommendations related to urban design guidelines, property standards, and community benefits from Section 37 of the *Planning Act*.

Revitalizing older high density residential neighbourhoods, such as the East Bloor Corridor, is increasingly an important issue as the housing stock ages. This study provides background information and an interim strategy for reviewing applications and addressing issues related to the potential for reinvestment and revitalization of the area. It is recognized that additional research and further investigation is required in Phase 2 of this review.

ATTACHMENTS:

Under Separate Cover:

East Bloor Corridor Review - Backgrounder and Interim Strategy



Edward R. Sajecki
Commissioner of Planning and Building

Prepared By: Paul Stewart, Policy Planner

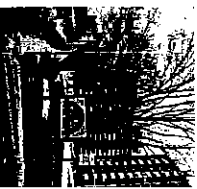
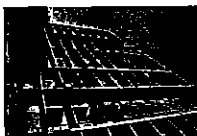
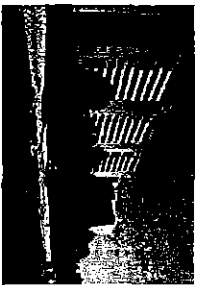
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East Bloor Corridor Review

Background and Interim Strategy

March 2013



East Bloor Corridor Review

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East Bloor Corridor Review: Backgrounder & Interim Strategy

1.0 Introduction and Purpose

Development along Bloor Street between Dixie Road and the Etobicoke Creek generally occurred approximately 40 years ago and consists of apartment buildings and townhouse complexes. The area is beginning to show its age, development pressures are starting, and with the current emphasis on intensification, requires an examination of the potential for infill development, opportunities for reinvestment and the impact on the existing community.

The purpose of this study is to:

- understand the existing characteristics and context of the area;
- review the planning framework for intensification;
- assess potential infill opportunities;
- provide information to assist in the review of development applications;
- prepare interim urban design guidelines to ensure new development contributes positively to the character of the area;
- identify opportunities for revitalization and reinvestment; and
- identify issues that require further study.

1.1 Study Area

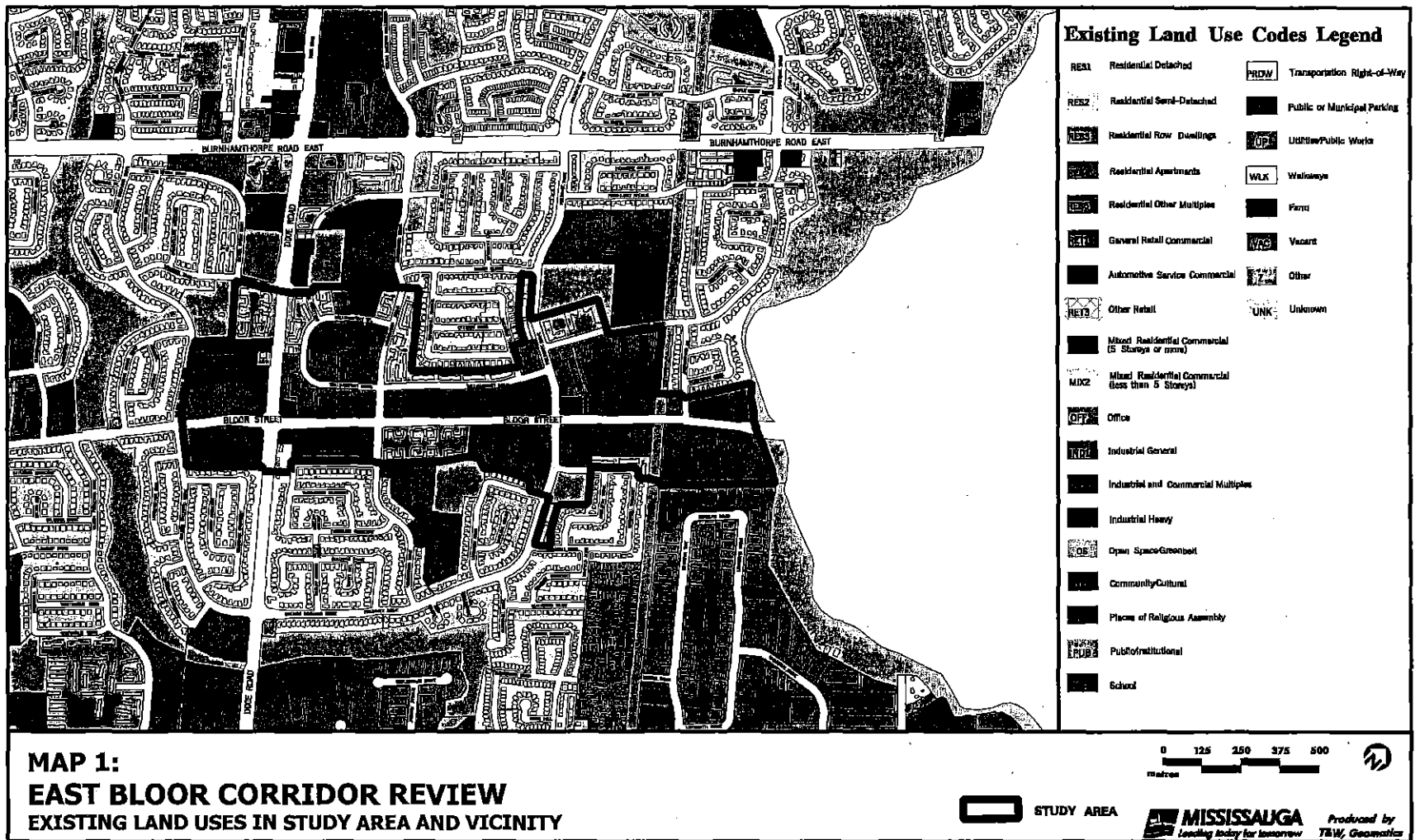
The East Bloor Corridor study area is located in Applewood, named after the apple farms that previously occupied these lands. Much of this community was developed in the 1960s and 1970s.

The study area contains a significant concentration of townhouse complexes and apartment buildings, located on the north and south side of Bloor Street, from west of Dixie Road, easterly to the municipal boundary of Etobicoke Creek (see Map 1). The study area is approximately 1.8 km (1.1 miles) in length, extending between 100 and 200 metres (328 and 656 feet) south of Bloor Street and between 150 and 400 metres (492 and 1,312 feet) north of Bloor Street. The total gross land area (including public roads) is approximately 84 hectares (208 acres).

With a population of approximately 13,300 persons, the area contains a significant concentration of population (e.g. more people live in the study area than in many planned community nodes in the City). In addition to residential uses, the study area includes neighbourhood-oriented shopping, schools and parks (see Map 1).

There are more than 60 land parcels in the study area. Properties range in size from less than 0.1 hectare (0.2 acres) to more than 4 hectares (10 acres) with an average of approximately 1 hectare (2.5 acres).

Surrounding land uses are predominately detached and semi-detached residential subdivisions, creek ravine, schools and a business area.



2.0 Understanding Character and Context

Understanding the general character and context helps inform the approach for infill and redevelopment. This section provides a summary of selected characteristics related to demographics, community infrastructure, built form, ownership, vacancy and rental rates. In addition, a brief discussion on the historical development of higher density residential areas is provided.

2.1 Selected Demographic Characteristics

Table 1 and Figure 1 indicate that the area contains a greater percentage of immigrants and visible minorities compared to figures for the City. Although demographic statistics are not available for the study area as a whole, using information for a sub-area (roughly defined by north of Bloor, south of Ponytrail/Willmsport Drive, east of Dixie and west of Bridgewood Drive), the population includes:

- 71% immigrants (vs. 51% for the City);
- 43% immigrated between 2001 and 2006 (vs. 22% for the City);
- 66% are visible minorities (vs. 49% for the City); and,
- 47% of households earn less than \$40,000 a year (vs. 24% for the City).

The above statistics are from the 2006 Census undertaken by Statistics Canada which defines “immigrants” as people who have been granted the right to live in Canada permanently by immigration authorities. The term “visible minority” is defined as people who are non-Caucasian in race or non-white in colour.

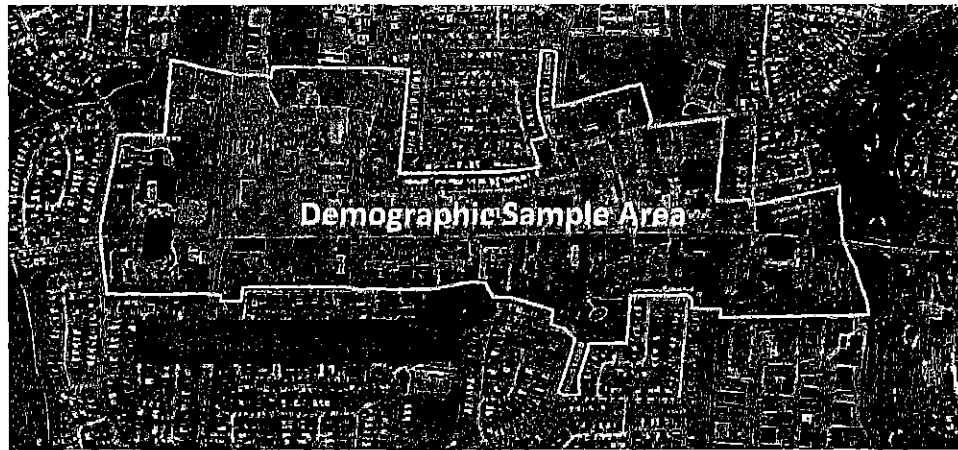
2.2 Community Infrastructure

Residents have access to a wide range of retail and personal services. There are two neighbourhood plazas which serve the day-to-day needs of residents. In proximity, there are a number of schools, the Burnhamthorpe Library, Burnhamthorpe Community Centre, and Region of Peel Learn Play Care Centre. As well, the East Bloor Neighbourhood Centre provides services and programs to meet the needs of the community including: employment (e.g. job search), newcomer (e.g. English classes), family (e.g. senior support programs), and community (e.g. landlord and tenant conflict resolution).

Interviews with service providers indicate that the area is as a gateway for new immigrants. Residents use the services available to get established in the community and the main reason people leave, is the opportunity of home ownership.

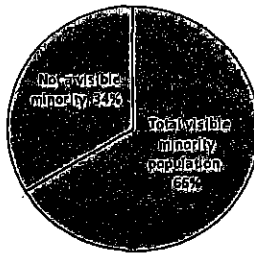
Table 1: East Bloor Sample Area – Income Characteristics

	East Bloor Sample Area	City of Mississauga
Average Household Income	\$49,647	\$88,162
Prevalence of low income	30%	14%
Percentage of Household earning less than \$20,000 /yr	16%	9%
Percentage of Households earning less than \$40,000 / yr	47%	24%
Source: Statistics Canada, Census 2006		

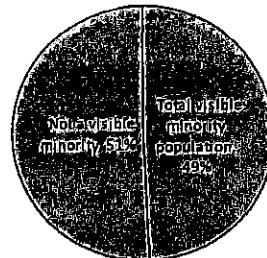


Visible Minority

East Bloor
Sample Area

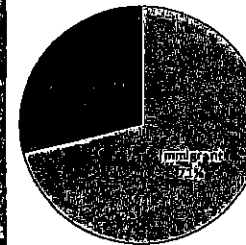


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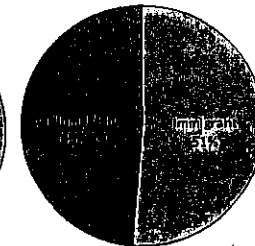


Immigration

East Bloor
Sample Area

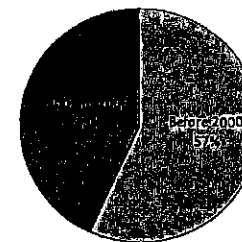


City of
Mississauga

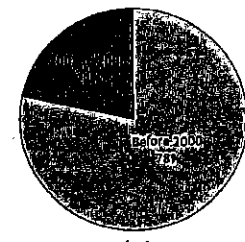


Period of Immigration

East Bloor
Sample Area



City of
Mississauga



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Figure 1: East Bloor Corridor Review – Selected Demographic Characteristics.

2.3 East Bloor Residential Buildings

The study area contains approximately 4,800 residential units distributed amongst 54 residential properties. Approximately 63% of the properties (representing 80% of all units) contain buildings that are 5 or more storeys (see Table 2 and Figure 2).

Table 2: East Bloor study area Building Typology		
Height	Properties&Units	General Description
15+ storeys	2 properties 742 units	<ul style="list-style-type: none"> - 26 and 27 storeys built in "Y" configuration - typical "tower in the park" example with combination of underground and surface parking, with outdoor amenity space (tennis courts) and significant landscaping
5 to 14 storeys	32 properties 3099 units	<ul style="list-style-type: none"> - average height is 9 storeys, taller buildings tend to locate in eastern half of the study area (e.g. 14 storey buildings located between hydro corridor and Etobicoke Creek) - sites represent a modified "tower in the park" development form where many properties include surface and underground parking, along with open space (although properties are smaller)
1 to 4 storeys	20 properties 1,002 units	<ul style="list-style-type: none"> - majority of buildings are either 2 storeys or 4 storeys in height - buildings typically following one of two types: (a) row-townhouse development with parking in front of each unit or (b) townhouses built in a courtyard format with combination of surface and underground parking - typically this built form represents a transition between higher to lower density development (i.e. parks, schools, low density residential development)
Total	54 properties 4,843 units	
<ul style="list-style-type: none"> - Urban Design Guidelines provide greater discussion of built form - Properties can have more than one building; however, within this study area buildings on the same property tend to have the same height. 		

2.4 Ownership, Vacancy & Rental Rates

Most of the residential properties (over 70%) are rental buildings (includes townhouses and apartment buildings). The two buildings at the northwest and southwest corners of Bloor Street and Dixie Road (which are 26 and 27 storeys, respectively) are condominiums and account for 742 units or 15% of existing units.

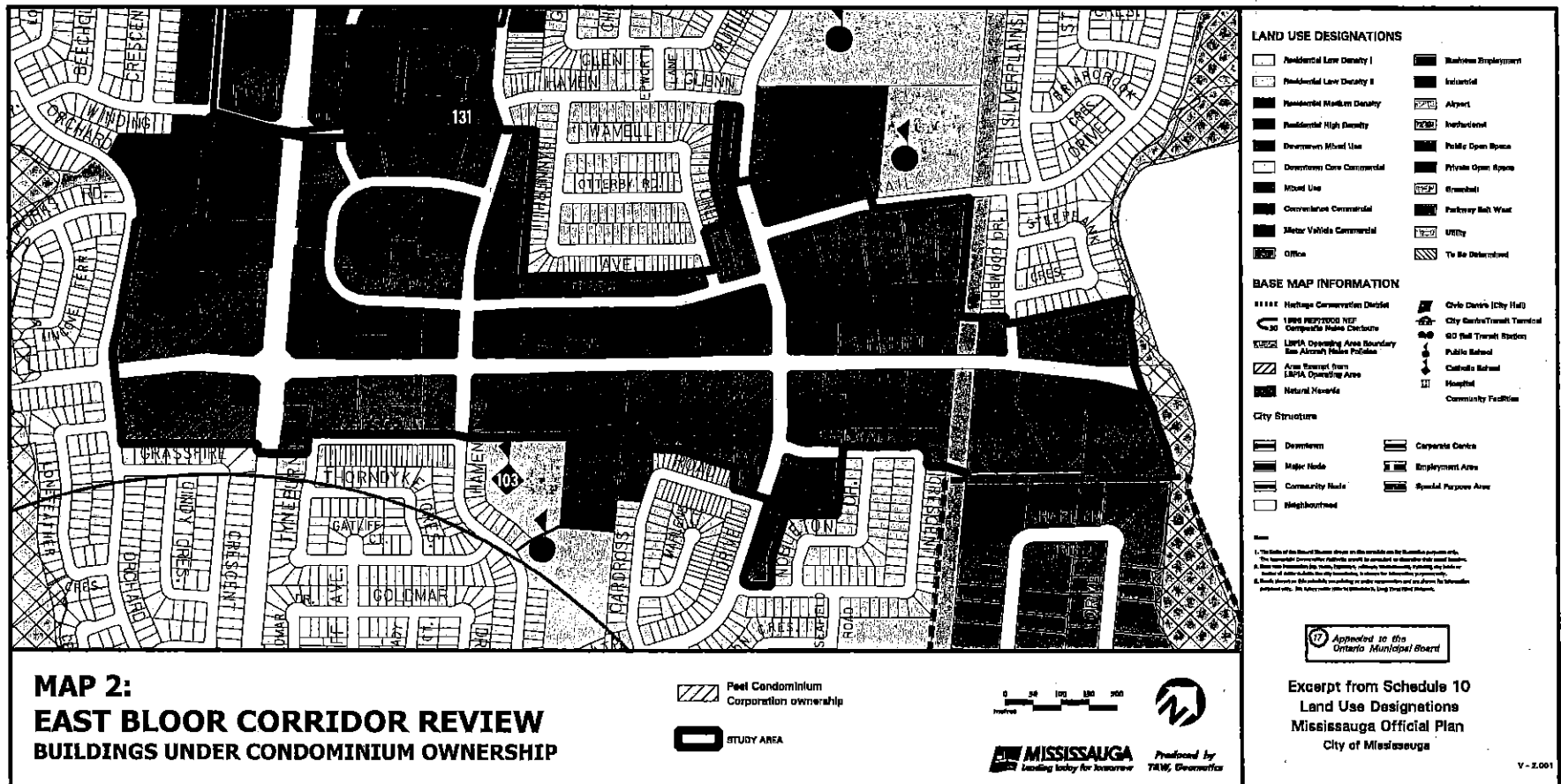
Other buildings under condominium ownership include: three properties at the southwest corner of Bloor Street and Fieldgate Drive; one property situated on the north side of Bloor Street between Fieldgate Drive and Havenwood Drive; two properties on Williamsport Drive; one property on Fieldgate Drive; and, one property on Kirkwall Crescent (see Map 2).

The vacancy rate for rental buildings was 1.2% (Oct 2011), which is below the City average of 1.4% and below what is considered a balanced market rate of 3%.

The average rent for apartment and townhouse units was \$1,046 per month, which is below the City average of \$1,094. Although rental rates are not available for condominiums, it is noted that in Peel (which is the only geographical level where information is published) units in rental apartment buildings tend to be more affordable than rental units in condominiums. In Peel the average rent for a condominium unit was \$1,472 vs. \$1,046.



Figure 2: East Bloor Corridor Review – Building heights in study area



2.5 Setting the Context

To understand the built form context, it is important to consider the history behind the development of apartments in this neighbourhood. The post-war years and the rise of personal automobile use, saw the rapid development of the suburbs throughout North America. The earliest suburbs in the Toronto area consisted of small detached homes built in the bungalow style, with split levels and larger two-storey homes emerging during later decades.

The post-war years also saw the rise of apartment style housing, representing an earlier planning model and different kind of thinking around 'complete communities'. It was intended through these developments, that neighbourhoods would have access to parks, schools, places of worship and commercial shopping centres.

New neighbourhoods and municipalities outside of Toronto experienced significant high-rise apartment building construction, as builders embraced the "towers in the park" design, invented by Le Corbusier (see Figure 3). The towers were built well beyond the sidewalks, leaving generous amounts of room on the property for park-like lawns, trees, surface parking and other landscaping features. Architecturally, they were simple, brick-clad high-rise buildings, having rectangular footprints and massing, with slab like appearances. As products of their time, embracing progress and modernity, these buildings have little ornamentation other than repeating balconies and banks of windows to highlight individual apartment units both vertically and horizontally.

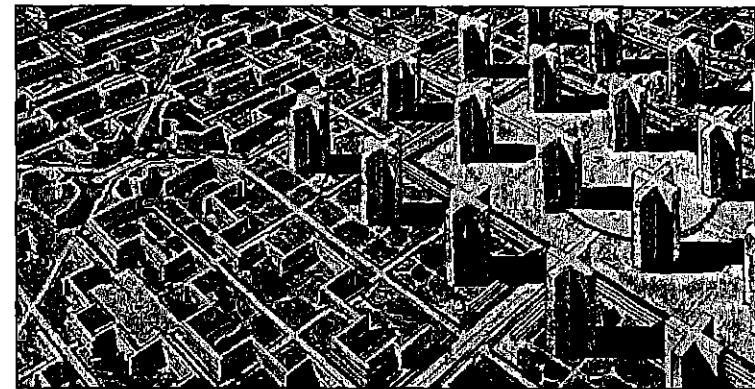
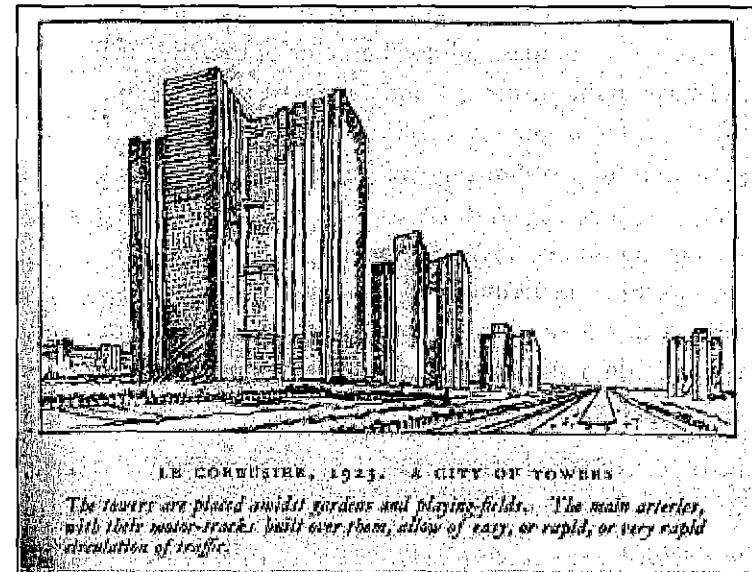


Figure 3: As a contemporary planning model, the Tower in the Park was born out of a utopian vision in the early 20th century, developed by Le Corbusier, a pioneer of the Modern Movement of Architecture.
Source of pictures: Google Images

The form of these new neighbourhoods was intended to be different from the compact urban character of the city, as buildings and uses were fairly dispersed and reflected the assumption that every adult would have possession of a personal car. In the 1960s and 1970s, this kind of housing would attract young professionals and young families. In contrast to that vision, the reality is that these neighbourhoods today, primarily serve a much greater diversity, including new Canadians and lower-income residents. As such, with a decrease in car ownership in these neighbourhoods, there tends to be a greater dependency on transit and walking for daily trips according to recent studies.

The above discussion incorporates information from the article “Toronto’s Post-war Towers Enabling Positive Change”, by Elise Hug, Graeme Stewart, Jason Thorne, published in the Ontario Planning Journal, Jan/Feb 2013 Vol 23.No.1.

2.6 Mississauga’s Towers In The Park

As Canada’s sixth largest city, Mississauga has examples of development that incorporates aspects of the “tower in the park” design. The best examples can be found in the *Mississauga Valleys* located in the southeast quadrant of Burnhamthorpe Road East and Hurontario Street. This area exhibits significantly tall slab buildings with large setbacks and generous separation distances between towers. A great amount of open space, lawns and treed landscapes also envelopes these developments.

The East Bloor Corridor is also representative of the “tower in the park” model, but comparatively speaking, is somewhat lower in scale, and appears spatially ‘tighter’. In addition, there are a variety of building types, more rental tenure, and less regimented siting

and orientation of buildings when compared to other precedents in Mississauga and the Greater Toronto Area (see Figure 4).

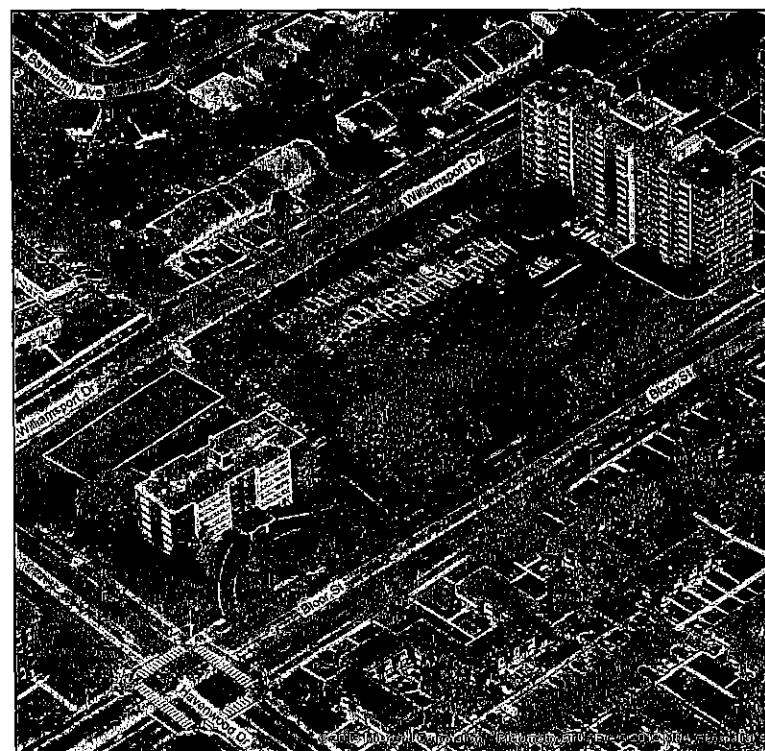


Figure 4: The East Bloor neighbourhood setting was primarily established in the 1960s and 1970s and provided generous amounts of room for landscaping and surface parking. Townhouses often formed a transition between apartment buildings and single detached dwellings. Source: Google Earthpro.

3.0 Strengths Weaknesses Opportunities Threats For Intensification

To gain a better understanding of the community as a location for future intensification, a high level analysis of the area's strengths, weaknesses, opportunities and threats (SWOT analysis) was undertaken (see Table 3). This analysis takes into consideration the character and context described in previous sections, staff observations from field visits and discussions with service providers. A more detailed analysis, as it relates to urban design was undertaken as part of the Urban Design Analysis and Interim Guidelines.

Based on the SWOT analysis, many of the strengths relate to this important location for affordable rental housing in conjunction with access to a variety of day-to-day services such as shopping, community support services and transit. Weaknesses include the area now showing its age, with little coherent unifying feel to the corridor. In some locations, large surface parking lots contribute to the disjointed appearance of the area. Traffic and parking have been raised as a concern on some local streets within and adjacent to the study area. In addition, as the City is not a housing corporation, and most residential lands are privately-owned, there is limited ability to directly invest in new construction. The City's main instruments for affecting change are to provide incentives to encourage private sector development.

Opportunity exists to capitalize on the strengths by accommodating some additional development through redevelopment and infill of large surface parking lots and underutilized portions of the larger

Table 3: East Bloor study area – SWOT Analysis

<p>Strengths:</p> <ul style="list-style-type: none"> - Affordable housing; - Access to stores, schools, and various community services; - Bus service along Bloor Street connects to Square One and Islington Subway Station; and - Diversity of built form offers a range of accommodation. 	<p>Weakness:</p> <ul style="list-style-type: none"> - Area is showing its age; - No cohesive built form vision (haphazard location of buildings); - Low vacancy rates limit access to rental units; - Concerns with safety have been raised which may be a function of poor lighting in areas and the removal of a community policing station; - Traffic and parking are issues in and adjacent to portions of the study area; - Minimizing and consolidating access on higher traffic volume roads is often a priority; and - The City is not a housing corporation and has limited ability to directly invest in new construction.
<p>Opportunities:</p> <ul style="list-style-type: none"> - Surface parking and underutilized lands, provide opportunity to accommodate infill development that can capitalize on infrastructure and reinvest in the area; - Redevelopment could be used to help upgrade site conditions, provide new investment in the community and potentially contribute Section 37 benefits; and - The City can provide incentives to encourage development. 	<p>Threats:</p> <ul style="list-style-type: none"> - New development produces negative impacts (e.g. traffic sun/shadow, privacy, skyviews, loss of amenity space); - New development may not help to unify the area and further the haphazard appearance of buildings; and - Development revenues are not reinvested in buildings.

lots. This may create an opportunity for reinvestment and improvements to the existing buildings. New development can help revitalize the area. The main threats as a location for intensification relate to concerns that new development may produce localized traffic issues and loss of amenity space.

In summary, there is opportunity for intensification and infill in the study area; however, this needs to be carefully considered in the context of a positive contribution to the character of the area itself and surrounding neighbourhoods.

4.0 Planning Framework

The City's approach to intensification, is contained in a number of policy documents such as the Mississauga Strategic Plan, the Mississauga Official Plan and the Zoning By-law. The sections below provide a summary of the applicable policies and regulations that need to be considered when infill development or intensification is proposed.

4.1 Mississauga Strategic Plan

The Mississauga Strategic Plan provides a long term vision to guide the City. This study responds to a number of the Strategic Plan's five pillars for change as outlined below:

- **Move** – setting the stage for considering how reinvestment, infill and city initiatives in the corridor, can enhance connections and create significant, positive impacts as it relates to the manner people move about the area (e.g. enhancing connections to a well-served transit stop).

- **Belong** – recognizing the contribution the area can play in creating a City that thrives on its social and cultural diversity. Provision of affordable housing and convenient access to services, which nurture this cultural diversity, are an important aspect to maintain.
- **Connect** – examining intensification and revitalization and how it can help contribute to creating a city which is vibrant with safe neighbourhoods and great public spaces.
- **Living Green** – recognizing that revitalization offers the opportunity to promote responsible stewardship through conservation, restoration and enhancement of the natural environment.

4.2 Mississauga Official Plan

Mississauga Official Plan (2011), (the Plan), provides direction for the City's growth and contains policies to guide development. The Plan contains policies that address a wide range of issues, including intensification. This section provides a summary of selected policies applicable to guide future development. Additional policies to those summarized below are found in Part Two of the Plan.

4.2.1 Direct Growth

Mississauga is planned on an urban structure that contains various elements (e.g. downtown, nodes, neighbourhoods, employment areas, etc.) which perform different functions. The East Bloor study area is located within the "Neighbourhood" element of the City

Structure. Neighbourhoods are characterized as physically stable areas with a character that is to be protected. Residential intensification within Neighbourhoods will generally occur through infilling and the development of existing commercial sites as mixed use areas. Intensification within Neighbourhoods may be considered, where the proposed development is compatible in built form and scale to surrounding development, and enhances the existing or planned character of the area.

4.2.2 Value The Environment

The study area is located adjacent to the Etobicoke Creek and as such, the Plan includes policies that recognize this area as part of the City's Natural Heritage System and, in particular, a Natural Hazard that has to be considered when evaluating infill potential. Other policies that could influence infill development speak to the opportunity to accommodate stormwater management best practices, and enhancing the urban forest.

4.2.3 Complete Communities

Complete communities meet the day-to-day needs of people throughout all stages of life. The Plan includes policies intended to create communities that enable people to not only live and work, but also thrive. It is important to consider opportunities to improve the area in terms of public art, affordable housing, and community infrastructure (e.g. schools, emergency services, daycare, places of religious assembly, etc.). In addition, the Plan states that the proponent of an intensification project may be required to provide a Community Infrastructure Impact Study that will, among other things, assess the proximity to and adequacy of existing community

infrastructure, human services and emergency services to meet increased demand caused by proposed intensification.

4.2.4 Multi-Modal City

The study area contains a transportation system that includes a combination of Regional Arterial (Dixie Road), Major Collector (Bloor Street), Minor Collector (Havenwood Drive, Fieldgate Drive /Ponytrail Drive) and local roads that are intended to accommodate a range of traffic volumes and modes of transportation. The Plan speaks to the ability to require area-wide or site specific transportation studies when reviewing development applications, and that the design of roads and streetscapes will create a safe and attractive environment for pedestrians, cyclists and motorists.

4.2.5 Desirable Urban Form

The Plan contains policies intended to achieve a sustainable urban form, with high quality urban design and a strong sense of place that is culturally vibrant, attractive, liveable and functional. For example, it is recognized that while new development within Neighbourhoods need not mirror existing development, it will, amongst other things:

- respect existing lotting patterns;
- respect continuity of front, rear and side yard setbacks;
- respect the scale and character of the surrounding area;
- minimize overshadowing and overlook on adjacent neighbours;
- incorporate stormwater best management practice;

- preserve mature high quality trees and ensure replacement of the tree canopy; and,
- be designed to respect the existing scale, massing, character and grades of the surrounding area.

4.2.6 Land Use Policies

Growth and development is also guided by land use policies in Section 3 of the Plan. This includes General Land Use policies, Neighbourhood policies, and the Applewood Character Area policies. Land Use designations are identified on Map 3.

General Land Use Policies:

Section 11 of the Plan outlines the land use designations and includes general city-wide policies related to uses permitted in the various designations, such as:

- *11.2.5.5 Lands designated Residential Medium Density will permit the following uses:*
 - a. townhouse dwelling; and*
 - b. all forms of horizontal multiple dwellings*
- *11.2.5.6 Lands designated Residential High Density will permit the following uses:*
 - a. apartment building*
- *11.2.5.12 In addition to the uses permitted in the High Density Residential designation, a convenience commercial facility will be permitted provided that:*

- a. it forms an integral part of the ground floor of the building*
- b. is oriented to pedestrian use.*

- *11.2.6.1.j Lands designated Mixed Use will permit residential uses.*

Neighbourhood Element of the City Structure:

The Plan contains a number of land use policies that are pertinent to intensification within Neighbourhoods, such as:

- *16.1.1.1 For lands within a Neighbourhood, a maximum building height of four storeys will apply unless Character Area policies specify alternative building heights requirements or until such time as alternative building heights are determined through the review of Character Area policies;*
- *16.1.1.2 Proposals for heights more than four storeys or different than established in the Character Area policies will only be considered where it can be demonstrated to the City's satisfaction, that:*
 - a. an appropriate transition in heights that respects the surrounding context will be achieved;*
 - b. the development proposal enhances the existing or planned development;*
 - c. the City Structure hierarchy is maintained; and*
 - d. the development proposal is consistent with the policies of this Plan.*

- *16.1.2.5 Proposals for additional development on lands with existing apartment buildings will be subject to the following, in addition to other policies regarding medium and high density residential development in this Plan:*

- a. on lands designated Residential High Density, development in addition to existing buildings will be restricted to uses permitted in the Residential Medium Density designation; and*
- b. as a condition of development, the site in its entirety must meet current site plan and landscaping requirements, and existing buildings must meet current building code, fire code and property standards.*

It should be noted that the City is considering revisions to this policy as it relates to issues of building code and fire code.

Applewood Character Area:

A number of policies are included in this section to address intensification, such as:

- **Urban Design Policies Bloor Street:**

16.2.1.1 East of Dixie Road, development consists primarily of high density residential uses, with some mixed uses. The following policies apply to development or infilling of this segment of the street to encourage an improved pedestrian environment:

- a. High density residential development should relate to the street, with buildings sited to minimize setbacks from Bloor Street. Efforts to develop a continuous street frontage through the construction of ground related podium structures (not including above ground parking structures), intensive landscaping at the street edge, and the orientation of buildings parallel to the street are encouraged.*
- b. the mixed use properties on Bloor Street should have a stronger relationship to Bloor Street, with the main entrances of the buildings oriented toward Bloor Street and at least half of the linear frontage occupied by built form, wherever possible.*

- **Land Use Policies:**

16.2.2.1 For Medium and High Density Development, new development should not exceed the height of any existing buildings on the property, and should be further limited in height so as to form a gradual transition in massing when located adjacent to low density residential development. Buildings immediately adjacent to low density housing forms should be limited to three storeys. In situations where the low density housing forms are separated from the high density development by a public road, park, utility corridor or other permanent open space feature, four to five storeys may be compatible.

- *16.2.2.3 Lands designated Residential Medium Density will also permit low-rise apartment dwellings.*

- the Applewood Character Area Map identifies the permitted Floor Space Index (FSI) for selected sites in the study area (primarily High Density and some medium density residential sites that were previously designated Medium Density II). The Applewood Character Area includes the following FSI ranges: 0.4-0.9, 0.5-1.2, and 1.2-1.5.

4.2.7 Other Official Plan Policies

Section 19 of the Plan establishes how policies are implemented and translated into programs. Policies that will guide revitalization include, but are not limited to:

- bonus zoning policies allow City Council to permit increases in height and density in the Plan and/or Zoning By-law to allow the City to secure specific amenities such as the provision of parkland above that which is required by the *Planning Act*, community infrastructure, public art, enhanced urban design features, affordable housing, streetscape improvements, etc. In all cases, the increase in height or density will be based on a site specific review and must demonstrate it is appropriate given the local context.
- property standards policies recognize that Mississauga will maintain and enforce a Property Standards By-law.
- Community Improvement Area policies recognize that the City can prepare Community Improvement Plans which may

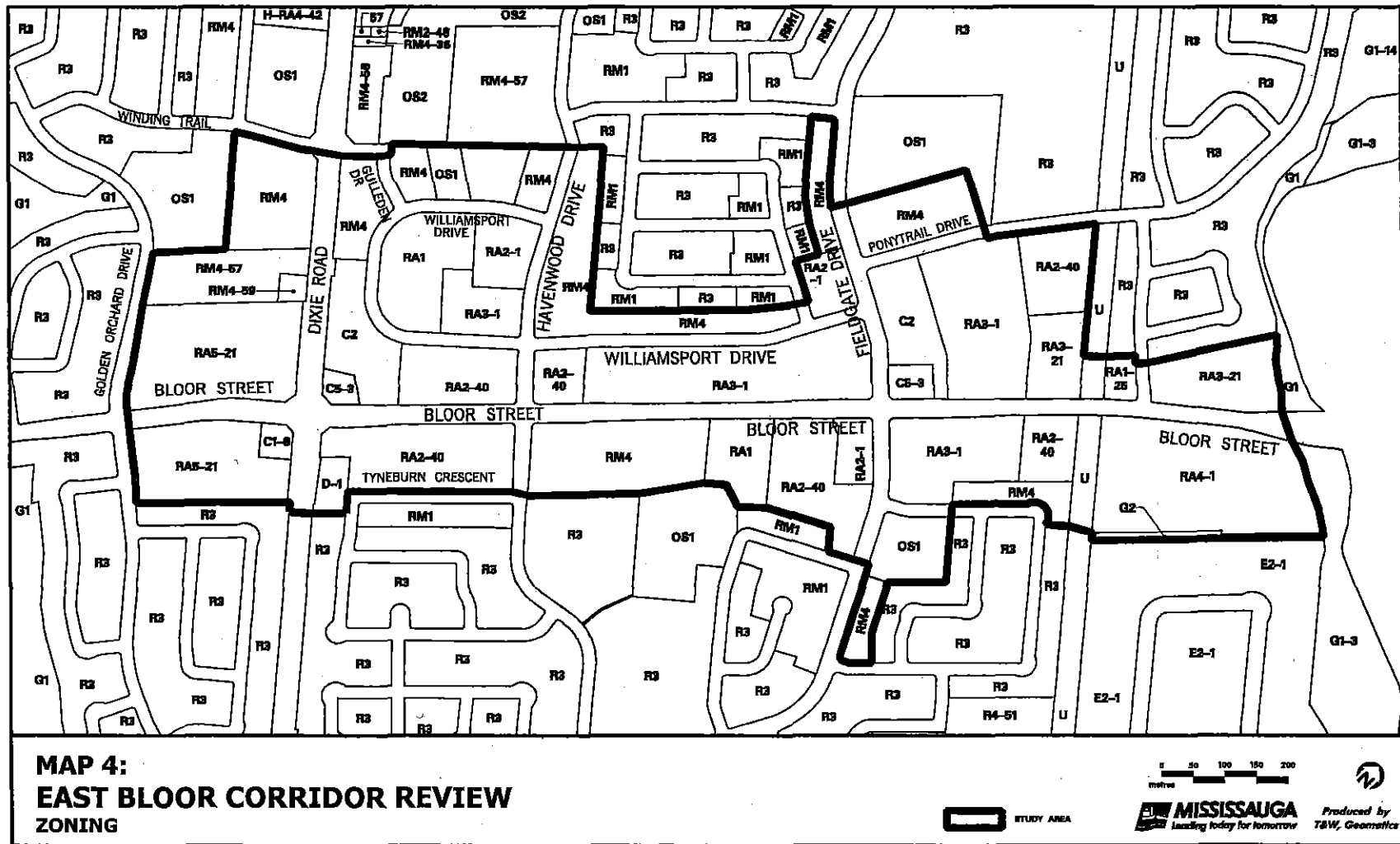
consider matters such as: the condition of the housing and building stock if poor and in need of repair; identification of the need to provide affordable housing; identification of the need to improve streetscape amenities; opportunities for infilling and development of underutilized sites, etc.

In conclusion, the Plan contains a wide range of policies that can be used to address intensification and infill.

4.3 Mississauga Zoning By-law

The purpose of the Mississauga Zoning By-law is to regulate the use of land, buildings and structures. Zoning implements the Official Plan designations and includes a range of zones for different types of land uses such as convenience commercial, motor vehicle commercial, neighbourhood commercial, apartments, townhouses, open space, greenbelt, and utility. In cases where existing uses do not correspond to the Official Plan designation, a "D" zone has been used (e.g. existing detached dwelling in a medium density designation). A "D" zone recognizes existing uses but requires a rezoning application before the site can be redeveloped. There are four adjacent lots on Dixie Road that have a "D" zoning (See Map 4).

Zoning provides greater detailed regulations as to the development of a property than found in the Official Plan and deals with issues such as setbacks, landscaped area, amenity area, and parking. A number of sites have had the standard zoning modified (i.e. Exception Zones) to change the permitted uses or regulations that are applicable to the site. Examples include:



- RA1 zone has a maximum FSI of 0.9; however, a RA1-25 zone limits the maximum FSI to 0.5. There is one property with this specific exception zoning;
- RA2 zone has a maximum FSI of 1.0; however, the RA2-40 zone limits the maximum FSI to 0.8. There are 13 properties with this specific exception zoning;
- RA3 zone has a maximum FSI of 1.0; however, the RA3-1 zone increases the maximum FSI to 1.2. There are 9 properties with this specific exception zoning;
- RA4 zone has a maximum FSI of 1.8; however, the RA4-1 zone restricts the maximum FSI to 1.0. There are 3 properties with this specific exception zoning;
- RA5 zone has a maximum FSI of 2.9 and a maximum height of 25 storeys, however, the RA5-21 zone has a maximum height of 27 storeys and a maximum FSI of 1.5. There are 2 properties with this specific exception zoning.

Typically, with respect to infill development it is through the rezoning and site plan approval processes that specific details of the proposed development are addressed and design guidelines are reviewed.

5.0 Analysis of Intensification Potential

A high level review was undertaken to understand the extent to which infill development could potentially occur and contribute in a positive manner. Although the Plan currently limits residential infill to medium density development, the analysis considers a broader perspective and includes comments on additional types of built form. It should be noted, however, that many of the building permits were issued in the 1960's and there is often limited information on buildings (e.g. exact limits of underground parking structures not available).

The ability of any specific site to accommodate additional units will be affected by matters that extend beyond the scope of this review. Issues such as the physical extent of any underground parking, lifecycle of structures (e.g. if renovations are required it may be possible to incorporate additional units, whereas this is less likely if the structure is in good condition), development costs and market conditions for new units which play an important role in determining if additional development occurs.

5.1 Evaluation Criteria

A number of property characteristics were examined as part of the evaluation of infill potential. To be considered as having infill potential, a site should satisfy a number of criteria, including the following:

- a. **Size of property:** Larger lots offer greater opportunity to accommodate additional development and mitigate any

associated impacts. The larger the lot, the greater the opportunity to accommodate more development.

- b. **Proximity of uses that could mitigate impacts:** Land uses such as arterial and collector roads, provide additional separation distances between existing uses and any potential infill development. Adjacent employment uses (e.g. commercial plaza, industrial area) are less sensitive to height and density than low density residential uses and may allow an infill opportunity to be realized, although operational issues (e.g. noise, odour) also have to be considered.
- c. **Proximity of sensitive land uses:** Land uses such as public parks and low density residential development represent a constraint as issues of sun and shadow impacts have to be addressed on adjacent properties.
- d. **Proximity of apartment buildings:** Sites that are surrounded with higher densities may have greater opportunity to accommodate additional infill as it respects the existing scale of development. For example, underutilized land located between two apartment buildings, could have greater opportunity for intensification, than land adjacent to a low density subdivision. Proximity of existing development becomes a constraint; however, should the higher density buildings be too close and/or the property too small to reasonably assure issues such as overlook, and shadow can be properly addressed, then

- e. **Location of existing building:** For smaller sites, the location of the existing building should be considered. Building placement can inhibit or promote redevelopment potential, in that situating a building in the middle of a property can result in insufficient property depth to accommodate additional buildings while maintaining sight lines, reasonable separation distances etc. from existing and surrounding properties.
- f. **Extent of existing development and potential surplus land:** Sites that are not developed to the maximum Floor Space Index (FSI) permitted in zoning, or identified in the Plan, may have potential for additional development. FSI means the ratio of the gross floor area of all buildings and structures to the lot area. In addition, sites that have significant open space may have additional surplus land available that could potentially accommodate infill development (e.g. land at rear of properties where there is no readily apparent use).
- g. **Property ownership:** If adjacent properties have the same owner, there may be opportunity to consolidate underutilized portions of the site to create a new developable parcel. It should be noted that some of the properties that contain infill potential are under condominium ownership. Although condominium ownership is a deterrent to intensification, in the future some of these sites may become more proactive as their buildings age and there is a need for additional capital expenditures.

- h. **Land Use Designation:** Sites that are not fully developed to their permitted uses are considered to have infill potential. For example, there are four residential lots located on the east side of Dixie Road which have detached residential buildings; however, the property is designated for Medium Density development.
- i. **Extent of parking:** Sites with large surface parking lots and no underground parking are less constrained than properties where underground parking provides most of the parking. It is assumed new development would provide for any required parking that is eliminated.
- j. **Frontage onto roadways:** Properties with frontage onto roadways provide greater opportunity to contribute to the community by improving streetscape and character of the area. In addition, sites with frontage onto multiple roads have better opportunities to provide access to new development or create separate lots, especially in situations where there may be issues with consolidating and minimizing additional access points onto certain roads.
- k. **Extent Of Infill:** The number of units that could potentially be added to the site was considered. Sites where only a few additional units could be added were typically considered as having limited or constrained infill potential.

5.2 Infill Analysis Findings:

Based on the criteria outlined above, the findings of the intensification review are as follows:

- most of properties adjacent to low density residential development are constrained as they typically are developed with townhouses that have little surplus land available for infill. Landscaping and parking lots are relatively small and it would be difficult to add new buildings while still providing adequate parking, open space, and separation distances between buildings;
- complete redevelopment of many medium density sites with high rise apartment buildings is not encouraged given these sites typically serve as a transition to lower density residential uses or parkland where there are greater concerns with compatibility (e.g. impact of sun and shadow);
- there are four properties on Dixie Road (south of Bloor Street) which are designated "Medium Density Residential" that are not yet developed. These sites can be expected to develop with medium density uses and represent reasonable transition between the apartment building to the north and the detached and semi-detached houses to the south.

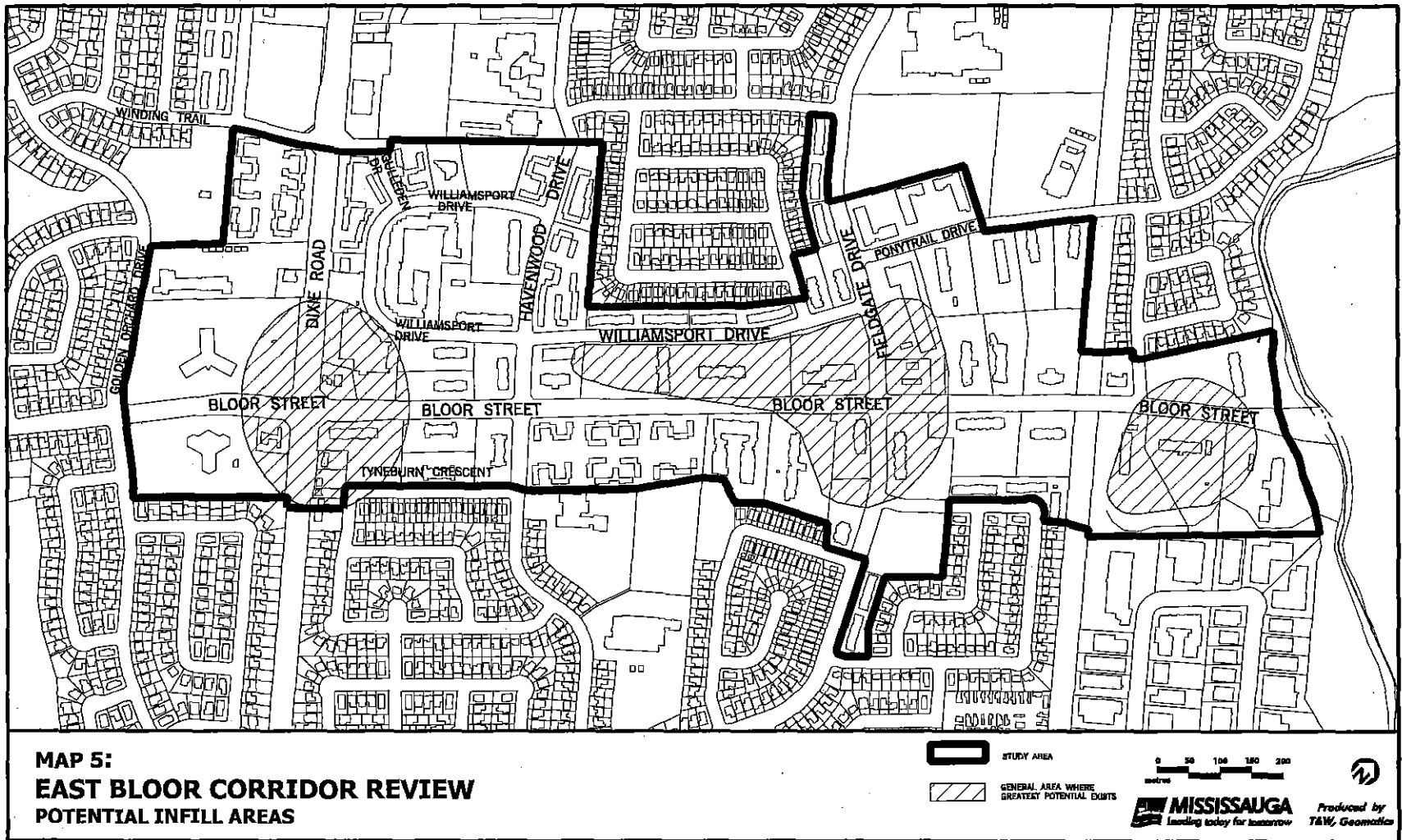
- commercial plazas have some potential for intensification, if the sites were redeveloped. The plazas, however, provide an important service to the community and any redevelopment should continue to provide neighbourhood commercial uses. Any redevelopment should give consideration to the potential for including adjacent gas stations, to create a more unified parcel. Accommodating parking for commercial uses represents a significant constraint to any redevelopment.
- properties that have access to more than one street represent reasonable candidates for considering infill development. For example, a lot that is bounded by multiple streets has greater flexibility in accommodating additional access points and increased opportunity to improve more than one streetscape than a site that has access to only one street. On some streets, such as Bloor Street and Dixie Road, minimizing and consolidating direct access is an objective that has to be considered when reviewing any infill application.
- although the policies currently restrict infill development to townhouses or in some cases a five-storey building, somewhat taller apartment buildings, may be appropriate in certain locations (e.g. when surrounded by other apartment buildings and townhouses, commercial plaza or employment areas). Attention to the appropriate height

and the need to reflect transition to adjacent properties is an important issue for consideration.

- in some cases, reconfigured onsite parking may create additional land for infill development; however, maintaining appropriate landscaping, amenity space, and sight lines need to be considered and, therefore, may limit opportunities.

It should be noted that potential infill sites are distributed throughout the study area, and are located mostly on major collector or minor collector roads which help to reduce impacts. Potential infill sites include properties at both the easterly and westerly limits of the study area, where properties are larger, as well as some larger sites more centrally located along Bloor Street (see Map 5 for general illustration of potential infill areas).

Based on the high level review, there are a number of apartment sites with some potential to accommodate additional development. In addition, the two existing commercial plazas could potentially accommodate new residential uses. As well, there are four residential lots on Dixie Road, south of Bloor which are designated for medium density residential uses that currently contain detached dwellings. The remainder of the residential properties have either limited potential (due to issues of parcel size, building placement, access issues) or are significantly constrained because minimal land is remaining to redevelop and/or full scale redevelopment with demolition of the existing building would be required.



NOTE: Site specific review required to confirm appropriateness of infill development, and may require amendments to the Official Plan and Zoning By-Law. There may be some sites outside of shaded areas where infill development could be accommodated.

6.0 Summary of Findings: Character, Planning Framework, Intensification Analysis

Based on the findings from the previous sections of this report, the following should be noted:

- This is an important area that provides affordable rental housing, and includes a gateway community for new Canadians;
- This is an area that would benefit from additional investment as it is aging and lacks a cohesive built form;
- Additional development, if done properly, can contribute to the health of the area;
- The Official Plan allows infill to occur and contains several policies that provide direction on how, and what intensification, should look like;
- There is potential for infill development within the study area;
- A high level assessment suggests that many sites have constraints that would limit the extent to which additional development can occur. There are a number of sites along the 1.8 km (1.1 miles) long corridor that have some potential for intensification, depending on the specific proposal.
- In addition to infill on apartment sites, the Plan permits residential development on the two commercial plazas and there are four detached houses on Dixie Road that are designated for medium density development;
- Although the Plan limits infill on apartment sites to medium density uses, there may be opportunities to allow for additional apartment development which could positively contribute to the area. However, given the different characteristics of each site, a site specific review as part of an Official Plan Amendment, is the recommended approach;
- The Plan contains policies that require infill development to address a range of issues (e.g. transition, respecting scale and character, etc.) that are to be reviewed when considering infill applications;
- The Plan stipulates that as a condition of additional development on sites that have existing apartment buildings, the site in its entirety must meet current site plan and landscaping requirements; and
- The opportunity may exist for a community benefit to be secured under Section 37 of the *Planning Act*.

The following section of this report includes an Urban Design Analysis and Interim Guidelines to assist in the review of development applications. Section 8 of the report identifies the next steps and issues that require further review.

7.0 Urban Design Strategy

7.1 Overview

The East Bloor Corridor can accommodate some additional infill development, as determined through an examination of the existing character and context, SWOT analysis, a review of the planning framework, and intensification potential analysis. To ensure new development respects and enhances the character of the area, as well as help unify the existing context, an urban design strategy has been prepared. The following section presents an analysis related to urban design, entailing detailed observations, Urban Design Guidelines and opportunities to guide enhancements and the evaluation of infill projects.



Figure 5: Aerial view illustrating built and open space qualities of the study area.



Figure 6: Aerial view of the study area.

7.2 Approach to Urban Design Strategy

To identify those areas where infill can make a positive contribution to the neighbourhood, an overarching analysis was undertaken that examines a number of layers and components that define its present day context, general character and sense of place. This also includes consideration of the corridor's planning and architectural origins linked to historic planning models of development, *vis-à-vis* "towers in the park", which came into vogue during a time of significant growth and expansion of suburbs in the Greater Toronto Area after WWII (*also see page 11, Section 2.5 'Setting the Context'*).

An underlying objective of the urban design strategy is to articulate Urban Design Guidelines that can be used to evaluate the potential for infill development on larger apartment sites. It should be noted that the Urban Design Guidelines have been generated with a view to ensuring that character defining features and place-defining aspects of the locale are considered and protected. Furthermore, where infill development opportunities may exist, the principles ensure that matters of compatibility, transition and 'good fit' are also addressed, so that such projects make a positive contribution to the overall presentation and enhancement of the study area. In addition to built-form aspects, this framework also addresses other components of urban design such as strategies for strengthening the public realm and 'greening' of the study area itself.

In guiding the strategy, the following have been identified as key issues to consider in the urban design analysis.



Figure 7: Figure Ground Diagram illustrating the study area.

1. What character defining elements and patterns shape the East Bloor Corridor?
2. What should be preserved or strengthened as elements that critically help to unify the existing context?; and
3. What guides the location, placement and design of infill opportunities in a manner that enhances the corridor character?

The strategy attempts to address these questions by articulating findings and observations within each component noted below and by identifying distinctive patterns and traits related to urban design, architecture and landscape in an integrated fashion. The components are listed as follows:

- streets and blocks;
- access and walkability;
- lot patterns, fabric, size and character ;
- buildings:
 - typologies and form;
 - location, siting and orientation; and
 - height patterns and transition;
- landscaped open space (disposition and distribution);
- corridor architectural character;
- existing landscape treatments; and
- the public realm

The analysis is expanded upon by articulating areas of opportunity related to proposed infill projects on larger apartment building sites with Urban Design Guidelines generated to provide direction for the components described above.

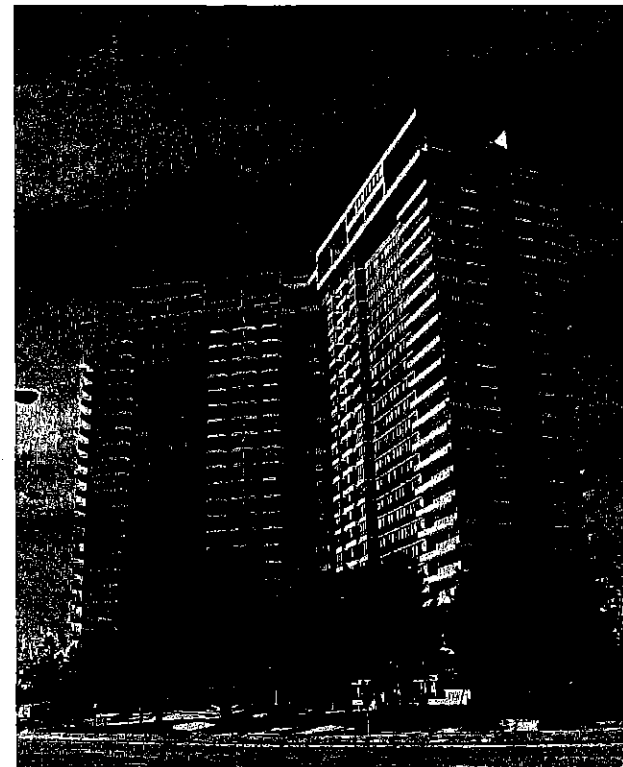


Figure 8: Tall Buildings at the western edge of the study area.

7.3 Applying the Urban Design Guidelines

The urban design strategy is best understood by reviewing all sections, text and diagrams, including the policies cited within the City's Official Plan and Zoning By-law.

The Urban Design Guidelines are organized in a hierarchy, citing higher level objectives and general intent, together with more detailed parameters that cover specific built form aspects. As a whole, the strategy reflects a context sensitive approach that guides and provides direction on the general location, disposition and treatment of proposed infill development, including strategies for the public realm. The Urban Design Guidelines have been generated to communicate the design expectations related to development applications and the expected quality and outcome of development.

It should be noted that the Urban Design Guidelines within this interim strategy are by no means exhaustive in nature. As such, development applications may be subject to more rigorous urban design requirements and review against other planning policies and urban design documents that bear on the study area, including urban design reference notes and the *Urban Design Handbook for Low-rise Multiple Dwellings* (August 2007/Revised 2013).

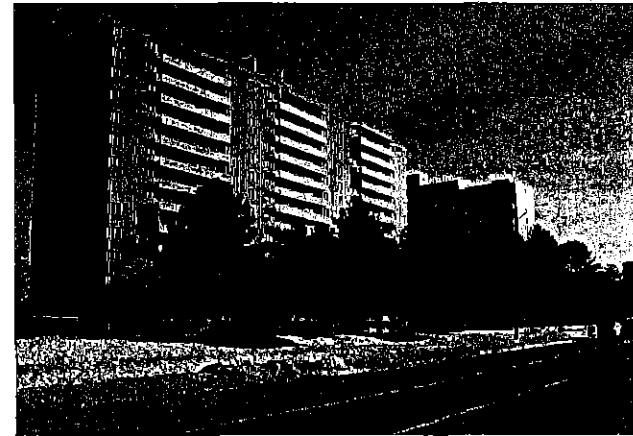


Figure 9: Midrise slab form buildings and lower apartment buildings characterize a significant portion of the study area.

7.4 Urban Design Analysis

The urban design analysis is structured around a number of scales beginning with the streets and blocks and concluding with strategies for the public realm. Within each section, observations are made related to each component, with highlights on key opportunities and corresponding Urban Design Guidelines to guide infill proposals.

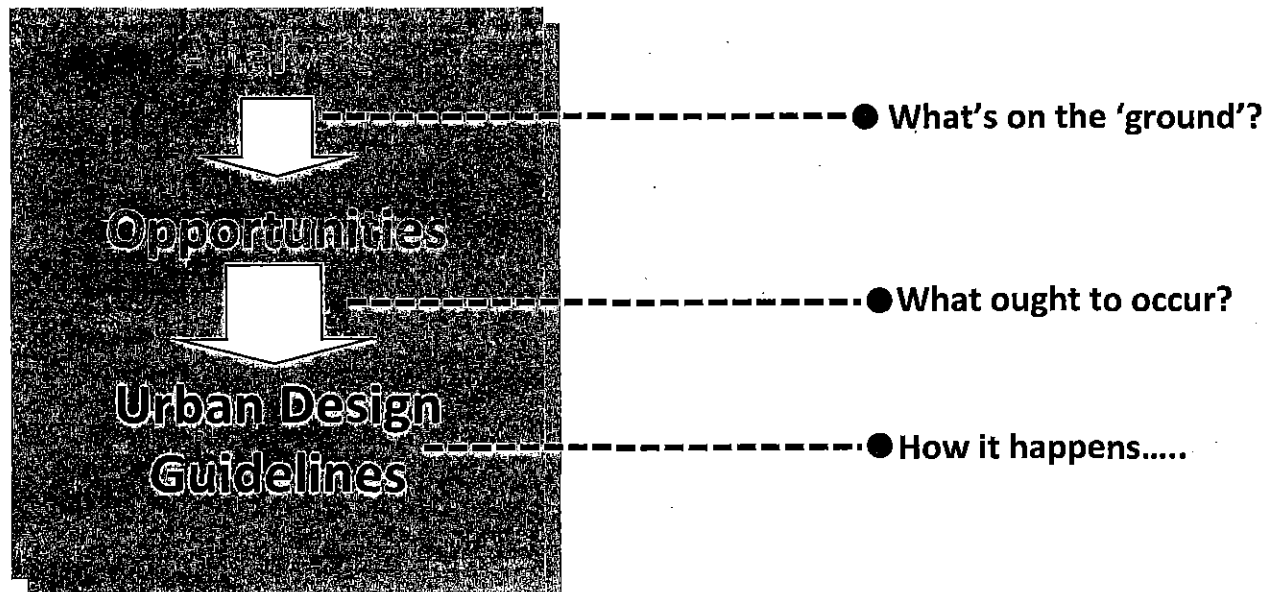


Figure 10: Framework for Urban Design Strategy.

7.5 Streets, Blocks, Access and Walkability

The existing pattern of the street layout and blocks in the East Bloor Corridor area is unassuming, and provides little in the way of clues or understanding around any original master planning scheme or comprehensive vision for the area when compared to the undertaking of planning initiatives today. The network and distribution of streets and blocks however is characteristically suburban on account of its dispersed qualities and organic-like patterns. The layout is opposite to the kind of grid network commonly found in more dense urban areas. Comparatively, these tend to be more rigid, rectilinear, dense, porous and with greater frequency in points of access and number of intersections.

The network in the study area is spatially distributed to form very large superblocks along Bloor Street (symmetrically configured on either side of the corridor). Havenwood Drive and Fieldgate Drive, are situated parallel to Dixie Road. They intersect with Bloor Street, but take on curvilinear geometries as they extend to the north and south. Streets that serve lower scale residential neighbourhoods outside the study area exhibit curvilinear organic qualities, and appear as crescents, cul-de-sacs and winding streets. These feed into north-south collectors (Havenwood and Fieldgate Drives) linking to Bloor Street, Burnamthorpe Road and Dundas Street.

Apartment building sites generally exist with little interruption along extensive frontages, forming very long super blocks and a street network that reinforces the automobile as a predominant means of mobility. This morphology fundamentally supports the Tower in the Park model, but presents a number of challenges related to

permeability, walkability and human scale attributes in contemporary planning terms.



Figure 11: Top - the street network and block structure within the study area has an organic pattern consisting of super blocks, winding streets, crescents and cul-de-sacs that provide confined points of access and little in the way of permeability. Bottom (left and right) – in contrast, the grid iron pattern of streets characterizes urban areas that have a significantly higher amount of permeability and predictability in the street network.

Driveways provide the principle means of access to apartment sites, sometimes in combination with drop offs and turn arounds, particularly where buildings have frontage along Bloor Street and some side streets. There are some walkways connected to the public sidewalk, but the site design of many of these apartment sites, as originally envisioned, suggest heavy reliance on the car as the principle means of arrival and departure.

The assembly and pattern of large land parcels, along with the configuration of residential subdivisions at the periphery, generally contribute to limited permeability through the study area. This is because the large super blocks and points of access are generally confined and relegated to main roads (Bloor Street) and thoroughfares (Havenwood Drive and Fieldgate Drive). The hydro corridor divides the study area at its eastern edge and provides a linear greenbelt between the east segment of the corridor and the western portions. Bloor Street currently serves as the only means of access and connectivity between these segments of the study area. (Also, refer to Section 7.12- Streetscape and the Public Realm).

OPPORTUNITIES

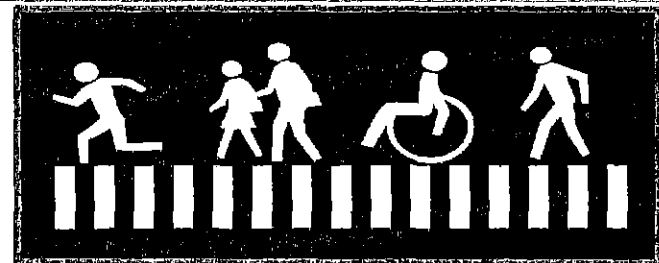
The strong influence of the automobile on the character and form of the study area gives little priority to the importance of the pedestrian environment, particularly for those residents who depend on walking, transit or cycling as a primary means of mobility and getting to and from local and/or regional destinations. As such, a strategy to enhance the pedestrian environment, within apartment sites, including linkages and connectivity through sites and to the public realm should be created to support improved walkability, access to transit and overall human health. Ensuring improved walkability within the study area is also predicated on a

need for placing limitations on road widenings to accommodate additional vehicles, particularly along Bloor Street.

Urban Design Guidelines for:

STREETS, ACCESS and WALKABILITY

1. Enhance the existing pedestrian network by strengthening
 - a. the pedestrian environment within apartment sites,
 - b. pathways and desire lines through apartment block sites with improved connections to existing public sidewalks, open space, transit stops and the surrounding community,
 - c. connectivity between adjacent apartment building sites and properties situated within super blocks
2. Utilize existing roads and streets, where possible, in providing access for proposed infill
3. Avoid road widenings to accommodate additional vehicles within the corridor.



7.6 Lot Fabric, Pattern, Size and Character

Apartment building sites and lots in the study area vary in size, proportion and orientation. Some are very large sites, others are somewhat smaller in area, taking on either square, rectangular and pie shaped configurations.

Within the north-east section of the study area, there are groupings of properties that have narrow frontages relative to the street with corresponding longer depths. These contrast with the general pattern and character of other sites in the vicinity. Outside of the study area, residential subdivision lots at the periphery of the study area have a significantly finer grain quality, with a consistent proportion.

OPPORTUNITIES

Where such opportunities exist for infill development, additional buildings may be constructed on the property as part of an evolution of the site, provided that such proposals are able to demonstrate a careful integration with existing buildings, including attention to site organization, massing aspects, quality built-form treatments, landscape treatments and public realm improvements where required. Moreover, proposals will need to demonstrate good contextual fit in terms of compatibility with existing buildings and how infill projects contribute to the study area as a whole, with a view to ensuring that character defining features such as generous front and side yards, mature trees and significant landscaping are not negatively impacted or compromised.

Urban Design Guidelines for

LOT FABRIC, PATTERN, SIZE AND CHARACTER

4. Where infill projects are considered on apartment sites, development proponents will be required to demonstrate that:

- a. there is sufficiently large enough space to accommodate additional development on the site, precluding landscaped open space frontages or areas that wrap the sides of apartment buildings that may contribute to the presentation of the apartment property and area character and
- b. such proposals are able to demonstrate how the siting of the new building(s) integrate positively with:
 - existing buildings on the property, and
 - the overall site organization and enhancement of the larger apartment property, including amenity areas, vehicular access and pedestrian circulation aspects;
- c. projects are able to achieve:
 - a good contextual fit, with adjacent properties, including built form compatibility with neighbouring buildings, open space and amenity areas, and
 - compatibility with the study area character as a whole.

7.7 Building Typology and Form

There are two predominant types of buildings within the study area which take the form of slab style apartment buildings (low and midrise) and townhouses (grade related). The following general observations are made:

- Townhouse Form: exist as two storeys, grade related units, developed within super blocks, many are organized on sites around courtyard configurations. These tend to be of exterior brick veneer construction.
- Lower Midrise Apartments exist in rectangular slab and orthogonal forms (i.e. square), generally ranging in height up to 5 storeys, generally constructed with brick exteriors and metal balconies.
- Midrise Apartments exist as elongated slab form buildings and range in height from approximately 6 – 14 storeys, generally constructed with brick exteriors and metal balconies.
- Tower Form: exist as tall slab towers constructed in 'Y' shaped floor plates (a.k.a 'Mercedes' form). These are of concrete frame and precast construction.

In addition to the above, the following observations are noted with regard to the form of buildings that interface with the study area:

- Lowrise Dwellings exist as one storey, split level and two storey single and semi-detached dwellings, are generally of modest scale, light frame construction, concrete (block)

foundations, brick veneer, and limited use of exterior vinyl or metal siding.

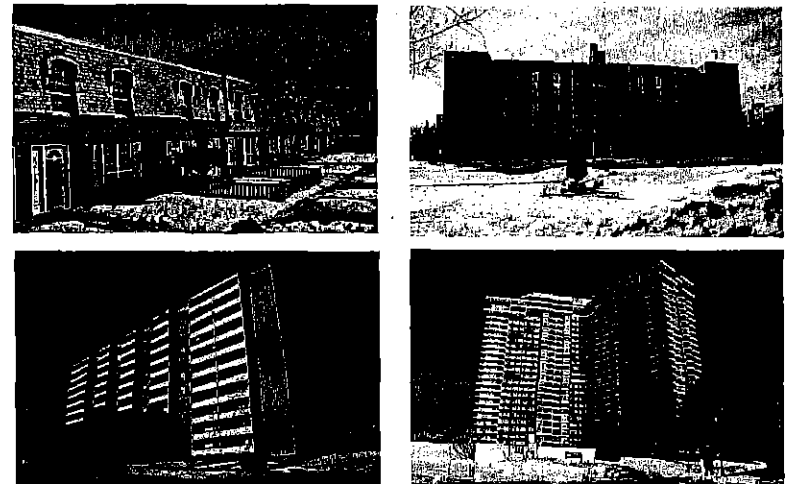
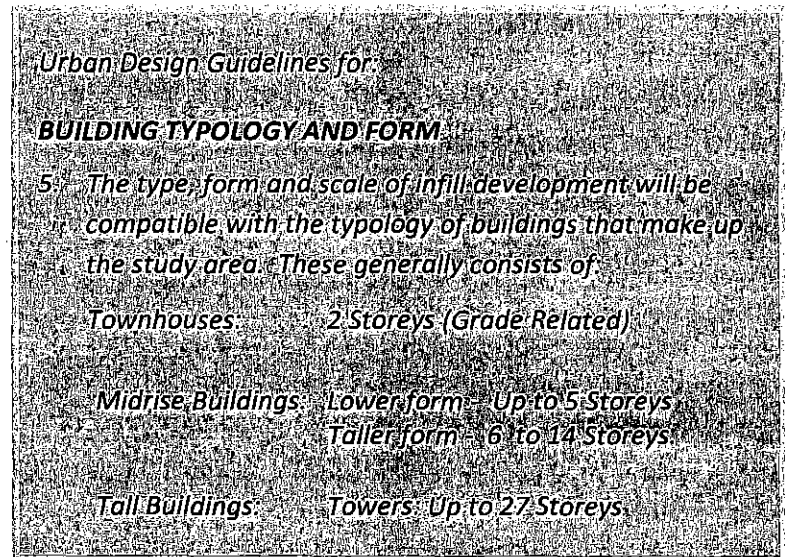


Figure 12: The study area generally consists of buildings such as townhouses, midrise slab form apartment buildings and tall towers.

OPPORTUNITIES

Infill opportunities should make use of the existing typology of buildings to ensure a general consistency of built form and legibility throughout the area. Building types should also be selected on the basis of reinforcing existing spatial patterns and by generally creating an orderly arrangement of heights within the corridor.

Where sites contain apartment buildings, townhouse form structures may be better suited to infilling opportunities because they allow better access to sky views and views through sites (above the units themselves) in addition to performing better in terms of shadow impacts. In general, townhouses may provide an appropriate form of transition between apartment buildings and lower scale buildings. Where there are areas or streets containing building heights of one storey at the street frontage, infill should not exceed 3 storeys in height, with the upper most storey integrated within the roof line or set back. The purpose of this is to provide a more gentle transition and harmonious relationship with neighbouring modest scale buildings, particularly where buildings are expressed as one storey along frontages.

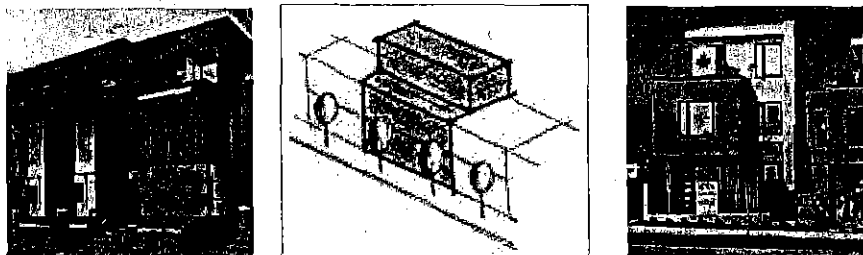


Figure 13: Townhouses can be articulated at the upper storeys in order promote better relationships with neighbouring that have a more modest height and scale.

BUILDING TYPOLOGY AND FORM (cont'd)

6. The design of infill projects will:

- a. mitigate differences in height between the proposed built form and the existing height and massing of adjacent buildings within the study area;
- b. through built-form treatments, ensure compatible relationships with the form and scale of buildings that interface with the study area.

7. Where infill fronts onto a street with existing lowrise buildings having a height of one storey at the street frontage, proposals should:

- a. not exceed 3 storeys in height;
- b. incorporate built-form treatments that strengthen relationships between new and existing building typologies, such as:
 - articulated front entrances, porches, stoops and/or verandahs on the ground floor;
 - incorporating terraces and step backs at the uppermost storey, or
 - expressing the uppermost storey within the roof line/space of the building using gable or dormer treatments.

7.8 Height Patterns & Transition

Apartment buildings are generally located closest to Bloor Street, with taller midrise buildings (approaching up to 14 storeys) occupying the stretch of the corridor east of Havenwood Drive toward the hydro corridor and schools to the north-east.

Lower building heights, i.e. 6 storeys, can also be found in very close proximity to the stable residential neighbourhoods on both the north and south sides. Sometimes these are located on opposite sides of the street at corner entries to streets (Cardross Road) as witnessed on the south side of Bloor Street. Stable residential areas outside the study area generally have heights of two storeys or less.

The tallest buildings exist on the west side of Dixie Road, opposite to one another at the corner(s) of Bloor Street. These have 'Y' form tower floor plate shape and measure 26 and 27 storeys in height respectfully.

OPPORTUNITIES

Infill projects should follow existing spatial and vertical height patterns, contributing to an orderly arrangement of heights in the study area. Furthermore, building heights should transition down to the stable residential neighbourhoods, generally maintaining a consistent scale and height (i.e. 3 Storeys) with what exists.

Urban Design Guidelines for

HEIGHT PATTERNS & TRANSITION

8. Where a difference in scale exists between building heights, development will be required to deploy transition strategies through massing and built form, to achieve a harmonious relationship between proposed and existing development, and/or adjacent open spaces.

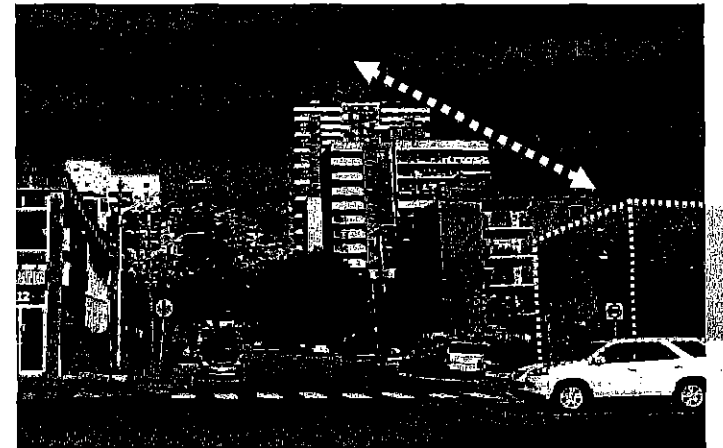


Figure 14: Example of buildings transition from high to low (Port Credit, Mississauga).

7.9 Building Location, Siting and Orientation

Within the confines of the study area, there appears to be no traceable pattern or strong rationale in the way buildings are oriented on their sites and in relation to the adjoining streets, frontage(s) or adjacent open spaces. This contrasts to areas that are characteristically urban, where patterns between buildings and their sites are discernable and often very predictable. The following general observations are noted:



Figure 15: A number of lotting patterns, building orientations and siting conditions exist within the study area.

- The length of buildings (elongated facade) is either placed parallel to the street, or in the opposite fashion with the narrower side along the frontages.
- Buildings are generally placed toward the edges (relative to street fronts) with a generous landscape setback, either at the centre of the property, or in proximity to corners where streets intersect. This allows for surface parking and landscaped areas to be accommodated.
- Front yard and side yard setbacks vary considerably, with little sense of pattern or predictability.

- Some precincts within the study area may exhibit a stronger sense of pattern and consistency in building orientation and placement as groupings of properties.

Urban Design Guidelines for

BUILDING LOCATION, SITING AND ORIENTATION

9. The location, siting and orientation of infill projects will
 - a. reinforce existing spatial qualities, setback patterns and how buildings orient themselves to streets and frontages, sideyards and property edges;
 - b. be designed to complete the street or frontage along the length of blocks and apartment sites;
 - c. where positioned at the rear of buildings or back of properties, will be located to form site layouts that strengthen the internal organization of apartment properties and create a sense of place; and
 - d. through site design and building placement, mitigate any differences in setback(s) between adjacent development or buildings where there are contrasting conditions between sites.

- The separation distance between apartment building ranges significantly, with the average occurring at 50 m between towers.

OPPORTUNITIES

Despite an absence of strong organizing Urban Design Guidelines in the East Bloor area, there are fundamental rules of good urban design that can be deployed in the context of infill projects particularly where there are a multitude of patterns that do not necessarily relate in any way.

In establishing the siting of buildings, patterns between adjacent properties should be deciphered by establishing spatial qualities, setbacks, how buildings orient themselves to frontages, sideyards and edges. Infill projects should attempt to replicate patterns where they can be easily ascertained, or mitigate differences between contrasting conditions with a view to achieving seamless relationships in the built environment, good transition and compatible fit.

Separation distances between buildings will vary depending on the height of a proposed building and its proximity to the scale, massing and height of adjacent buildings. As a general rule, taller buildings in slab form will require larger separation distances in order to secure sky views, visual permeability through sites and the benefit of having natural sunlight reach amenity areas, yards and open spaces on the ground. Separation distances also help to address

matters related to privacy between buildings and the impacts of shadows on adjacent development.

BUILDING LOCATION, SITING AND ORIENTATION (cont'd)

10. Infill development will be designed to

- ensure maximum access to sky views, natural sunlight and privacy between buildings;*
- incorporate minimum separation distances between buildings, when such proposals are located on the same development site or block, or adjacent to an existing property or site containing other buildings; and*
- include a minimum separation distance between building(s) measured between outside faces*
 - *20 m (66 feet) between an apartment building (up to 5 storeys in height) and a townhouse block*

7.10 Corridor Architectural Character

The architectural character of buildings in the study area is largely eclectic in nature, but stylistically a large number of the taller form buildings are constructed under the influence of the Tower in the Park model born out of the Modern Movement of Architecture and International Style, with strong references to Corbusien architectural precedents. As products of their time, embracing progress and modernity, these building types make little reference to the past, have little ornamentation other than repeating balconies and banks of windows to highlight individual apartment units both vertically and horizontally. From a materials perspective, these buildings were constructed out of concrete structural frames, with exposed floor reveals as an expression of structure (between successive stories) and brick colours ranging from browns, to deep reds, light beiges and contemporary whites.

OPPORTUNITIES

Today, there is a renewed interest in Modernism as a style of architecture that is seeing tremendous popularity in the execution of new residential housing in the GTA and elsewhere, particularly in midrise and highrise buildings. Lower form buildings such as townhouses, duplexes and single family homes are also being constructed in this style because of its streamline qualities, simplicity, use of materials (brick, stone, wood, etc) and appeal to contemporary tastes.

In the context of the study area, infill development should be compatible with the Modernist influences of the area wherever

Urban Design Guidelines for

CORRIDOR ARCHITECTURAL CHARACTER

- 11 The exterior design of Infill proposals should
 - a. have a well-executed architectural expression that is compatible with the contemporary character and style of buildings in the general vicinity;
 - b. use high quality exterior materials that are resilient such as brick, wood, glass and architectural metals;
 - c. use stucco, horizontal or vertical siding materials in very limited amounts; and
 - d. use high quality fenestration and apertures, including entrance doors and garage doors.

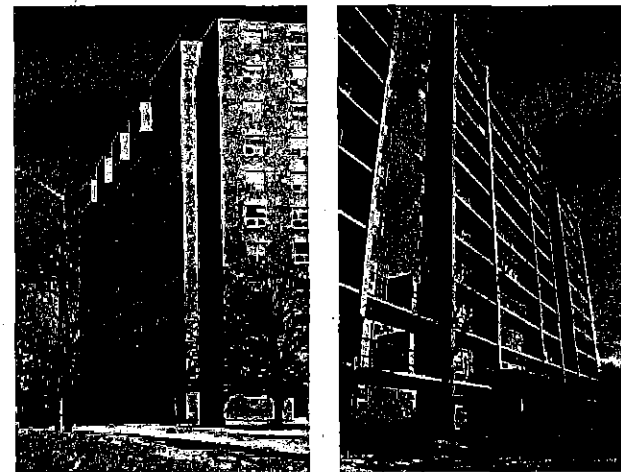


Figure 16: Existing buildings in the study area which demonstrate influences of Modernism.

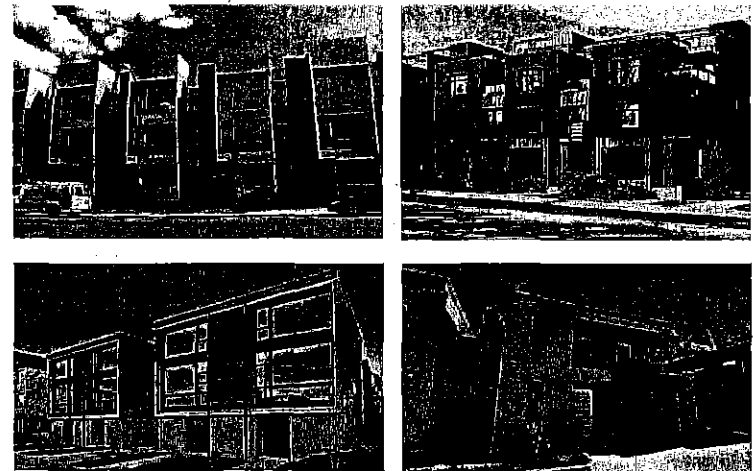
possible, maintaining a contemporary quality that relates to, yet enhances the character of apartment buildings and surrounding residential areas. Over stylized or eclectic buildings alluding to such styles as Tudor or Georgian may not be the best fit for the area.



Figure 17: Buildings in the study area were influenced by the Modern Movement of Architecture and a contemporary character that came into vogue in suburban areas after WWII. Apartment buildings were simple, brick clad buildings, with repeating balconies and banks of windows to highlight apartment units both vertically and horizontally.



Figure 18: Above/Below - Residential buildings constructed today express a renewed interest in Modernism, deploying a contemporary interpretation of design elements from that period.



7.11 Landscaped Open Space

(Frontages, Side Yards, Amenity Areas and Surface Parking)

Landscaped open space is a significant character defining element of the study area. It unites the collection of apartment buildings, townhouse blocks and community facilities, and mitigates the widespread variation in patterns that exists between buildings types, apartment building orientations and site layouts. It is the most consistent feature indicative of the Tower in the Park model throughout the study area, and should be carefully considered with regard to infill and impacts along frontages and landscaped areas that wrap the sides of buildings.

Landscaped open space is prevalent along the frontages of buildings, at side yards, between buildings and within courtyards (townhouse developments). It also takes the form of amenity areas serving apartment building residents. Depending on building orientation, the amount of open space also lends significant visual permeability between slab form buildings, yielding views between sites, sky views and the ability for sunlight to reach green spaces, amenity areas, sidewalks and paths.

A significant amount of surface parking is also dominant within many apartment blocks, with most lots occurring behind or to the sides of buildings, and often buffered by sodded areas. In terms of opportunities, these areas may be the most viable for infilling, depending on the presence of structured parking below grade. Vehicular drop offs are also associated with front entrances and lobbies, sometimes integrated into the landscape design of apartment sites, particularly where buildings address or have a direct orientation to the street, or a main drive aisle entrance.

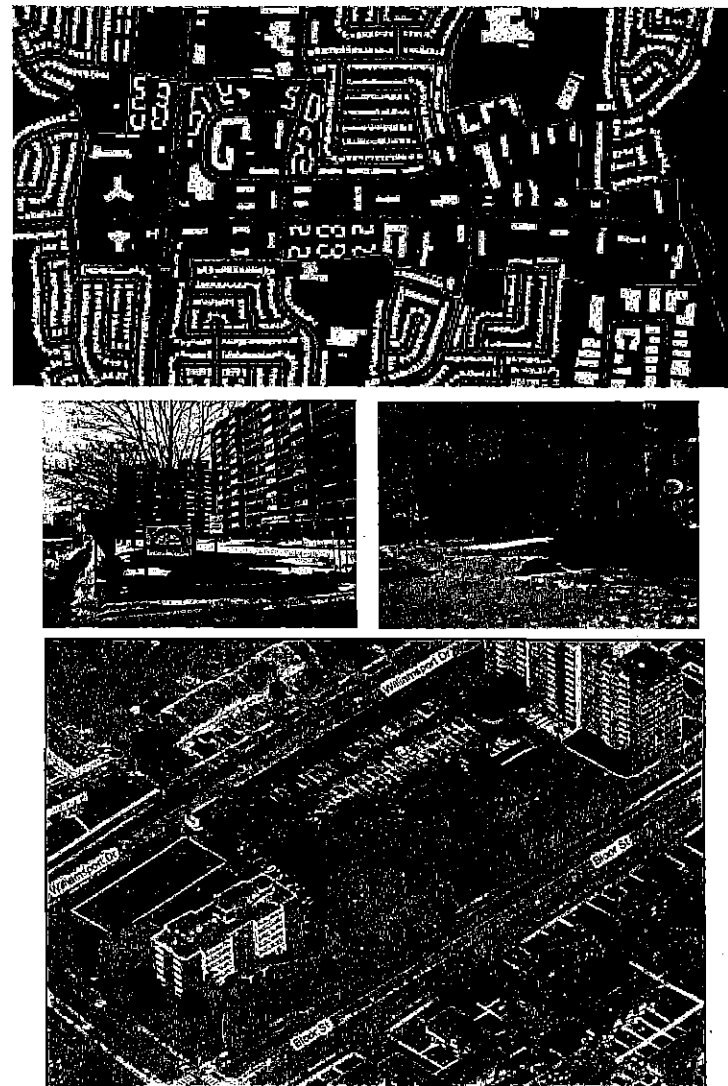


Figure 19: Landscape open space is a significant element which unites the range of building types, their orientations and siting on individual apartment sites.

7.11.1 Landscape Treatments within Apartment Sites

Landscape treatment varies depending on the site. Some apartment sites have a very barren quality, while others are better developed with mature trees and plantings that embrace the Tower in the Park concept.

Much like the placement of apartment buildings themselves, trees are planted in a random, sporadic fashion along frontages and side yards, with a mixing of deciduous and evergreen species. Sometimes trees are located within gaps between buildings, other times they form a part of extensive lawn systems that meet the public sidewalks. Often they are used to emphasize ornamental landscape treatments, placed at corners, side yards, near drop offs and front entranceways.

Within many apartment sites, there is a proliferation of silver frost free chain link fencing that borders edges, property lines and the public sidewalk along Bloor Street and other streets in the area. As barriers, these tend to detract from the curb-side presentation of the locale and from the public realm as a whole. The fencing also acts as a collector for waste, garbage and leaves. Other sites in the vicinity have upgraded metal fencing, appearing as black wrought iron grilles, placed at the edges beside the public sidewalk.

Another issue pertaining to apartment sites may relate to site lighting and the ability of residents to feel safe during evening hours while moving on foot between parking areas, walkways and apartment building entrances.

Urban Design Guidelines for

LANDSCAPED OPEN SPACE

12. Infill development will be required to:

- a. conserve the generous spatial character and setback of lawns and landscaped areas that wrap around the frontages and sides of apartment buildings;
- b. preserve existing landscapes, mature trees and vegetation that contribute positively to the existing character of individual apartment sites and the study area as a whole;
- c. provide or enhance landscape treatments and plantings for:
 - the infill project site itself; and
 - the apartment property as a whole
- d. build upon the existing pattern, siting and placement of trees using a mix of deciduous and evergreen species;
- e. ensure existing amenity areas are provided, preserved or enhanced through site furnishings, provision for playground areas and equipment;

OPPORTUNITIES

As part of this interim strategy, it is recommended that a conservative approach be undertaken with respect to infill opportunities, allowing intensification to occur on large apartment sites, but behind buildings, on surface parking lots or areas of property(s) that are significantly underutilized.

Given the sporadic orientation and pattern of buildings across sites, vis-à-vis the influence of the Tower in the Park model, the preservation of green space around the frontages and sideyards assumes particular importance as the common thread that knits together all buildings and the general character of the study area. These zones should be carefully considered through planting programs that strengthen frontages, sideyards and amenity areas with additional greening, landscape treatments, walkway connections and linkages to the public realm in order to provide benefits to the community, improve the overall attractiveness of apartment sites and the environment.

Amenity areas for residents in apartment buildings should be preserved, enhanced or provided for with appropriate site furnishings. This includes accommodating the needs of children seeking play space, with provision for new or updated playground equipment and corresponding landscape treatments.

Contextually, the curb side appeal of the study area also has the potential to be significantly improved by removing the abundance of chain link fencing that exists around apartment sites. Not only would this result in a significant aesthetic improvement, but from a neighbourhood perspective would remove visual and physical barriers and strengthen the larger community context. As an

LANDSCAPED OPEN SPACE (cont'd)

- f. *strengthen walkways within apartment sites in order to enhance wayfinding, orientation and navigation through sites for residents and visitors;*
- g. *consider the following with respect to fencing on apartment sites and improving the study area character:*
 - *removal of fencing to improve the presentation of the apartment site and streetscape; or*
 - *apply artful installations over existing fences with themes that express representations of culture, local history and/or community stories; or*
 - *relocate fencing to the building setback and provide soft landscape materials around the perimeter of properties;*



Figure 20: Art installations, depicting community themes and stories can be superimposed over chain link fencing as a means to improve the aesthetics of an area. (Location: Lansdowne Ave, Toronto).

important move, it would also restore the sense of 'spatial sequence' between the private and public realms relating to the transition between public, semi-public and private space zones.

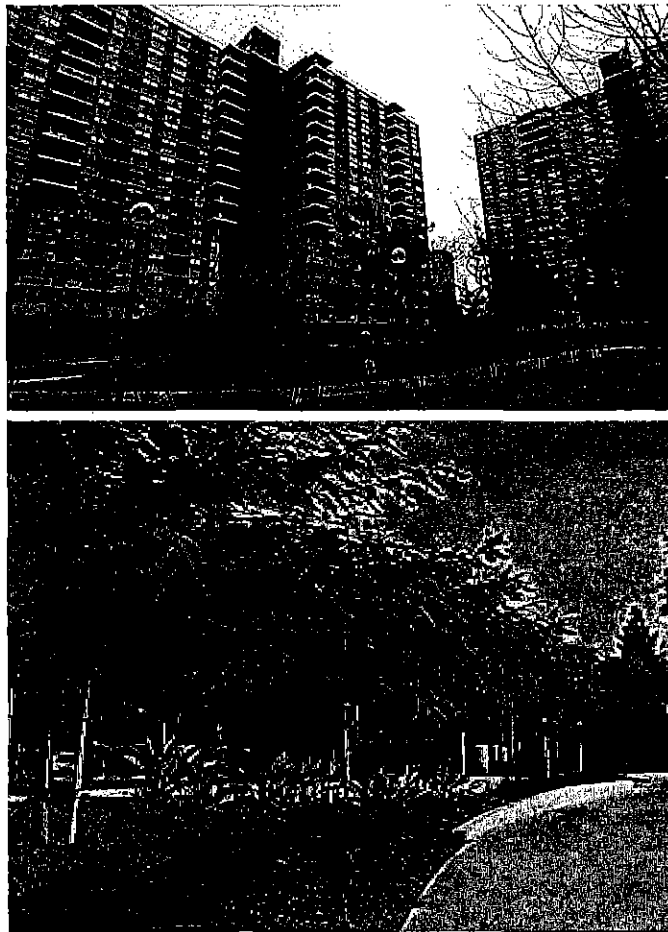


Figure 21: Top: Site Illumination can assist with safety, comfort, and navigation through apartment sites during evening hours. Bottom: Screening of surface parking lots helps to improve the attractiveness and presentation of sites.

In addition to the above, there are number of opportunities to enhance the corridor with streetscape and public realm improvements. This is described in the following section.

LANDSCAPED OPEN SPACE (cont d),

- h. provide a generous landscape buffer between infill project sites and exiting surface parking areas that serve apartment buildings*
- i. ensure that apartment sites have sufficient illumination in parking areas, playground areas, along walkways, and near entrances to improve safety and navigation through sites during evening hours,*
- j. locate and screen service areas, ramps and garbage storage areas to minimize the impact on the adjacent streets and residences,*

In addition to the above, the following will apply to parking areas within apartment sites

- k. the creation of hard surface areas for additional parking on the site (as a result of site modifications) will be discouraged, and*
- l. existing parking areas will be screened with plantings where they are visible from the street, adjacent amenity areas or open spaces*

7.12 Streetscape and the Public Realm

Bloor Street has an unassuming character. It is a major collector that serves the community with mainly apartment buildings built in the sixties and seventies. The presence of street trees is limited along Bloor Street, with the exception of plantings along the western edge and other areas.

The East Bloor Corridor neighbourhood has an established streetscape particularly in the residential area. These consist of tree lined streets with sidewalks on the street edge and private landscaping within the front yards of the houses. This should be maintained and augmented when new development occurs.

OPPORTUNITIES

Trees along Bloor Street have matured on a number of apartment sites, creating a strong presence in the community. Along with the preservation of these trees, additional plantings in key locations can collectively create a 'Tree District', resulting in a distinct area character that strengthens landscaped open space, particularly as it envelopes buildings and interfaces with the public realm. With reference to the Concept Plan shown on page 47 a number of design principles identify ways in which this can be realized, with a view to significantly enhancing Bloor Street and study area as a whole:

Urban Design Guidelines for

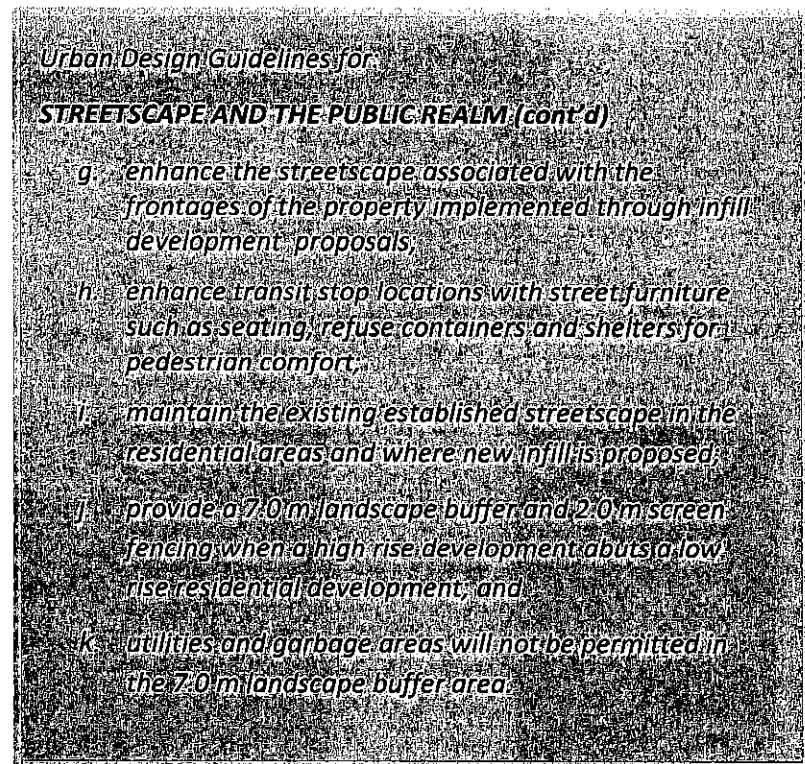
STREETSCAPE AND THE PUBLIC REALM

13. The following improvements should be undertaken:

- a. create a Tree District by supplementing additional trees with in private land parcels and the public boulevard to support the continuity of trees along the entire length of Bloor Street;
- b. provide a supplemental grove of trees for the Hydro Corridor adjacent to Bloor Street;
- c. provide a Community Entry Feature which may take the form of horticultural displays and/or artful installations at the Etobicoke Creek, Dixie Road/Bloor Street Intersection and other key locations;
- d. create enhanced pedestrian crossings at Fieldgate Drive and Havenwood Drive to slow down traffic and recognize the heavy volume of pedestrians in these areas, including artful graphic street markings;
- e. shaded sitting areas should be provided along Bloor Street;
- f. provide pedestrian scale lighting to the existing street lights to encourage area vibrancy and provide a better sense of security and comfort during evenings hours.

- Trees and Enhanced Plantings – to effect the Tree District concept, it recommended that trees be planted along the length of the corridor, specifically within the public boulevard (where feasible) and on private lands. This also includes intensified plantings within expansive lawn areas on the south side of Bloor Street (refer to concept plan) where there is minimal planting today, and where such treatment is warranted.
- Entranceway(s) – opportunities to create distinctive entranceways that incorporate tree plantings, horticultural displays, and art installations at the eastern and western edges of Bloor Street. This includes a City entrance feature at the eastern limit of Bloor Street in proximity to the Etobicoke Creek.
- Intermediate Locations – In order to reinforce the Tree District concept and enhance the pedestrian environment, improvements can be undertaken at three (3) locations along Bloor Street to include additional plantings at (a) the hydro corridor, (b) Fieldgate Drive and (c) Havenwood Drive. In addition, graphic markings placed at signalized crossings for Fieldgate and Havenwood Drive(s) can reinforce the important principle of improved walkability and connectivity outlined on page 32.

In addition to the above, transition from any residential high rise building should include a generous landscape buffer and screening of fencing when a high rise development abuts a low rise residential development. This also helps to ensure that any existing trees and vegetation on the existing property and adjacent property can be preserved. Utilities and garbage areas will not be permitted in the landscape buffer areas.



Concept for Bloor Street TREE DISTRICT

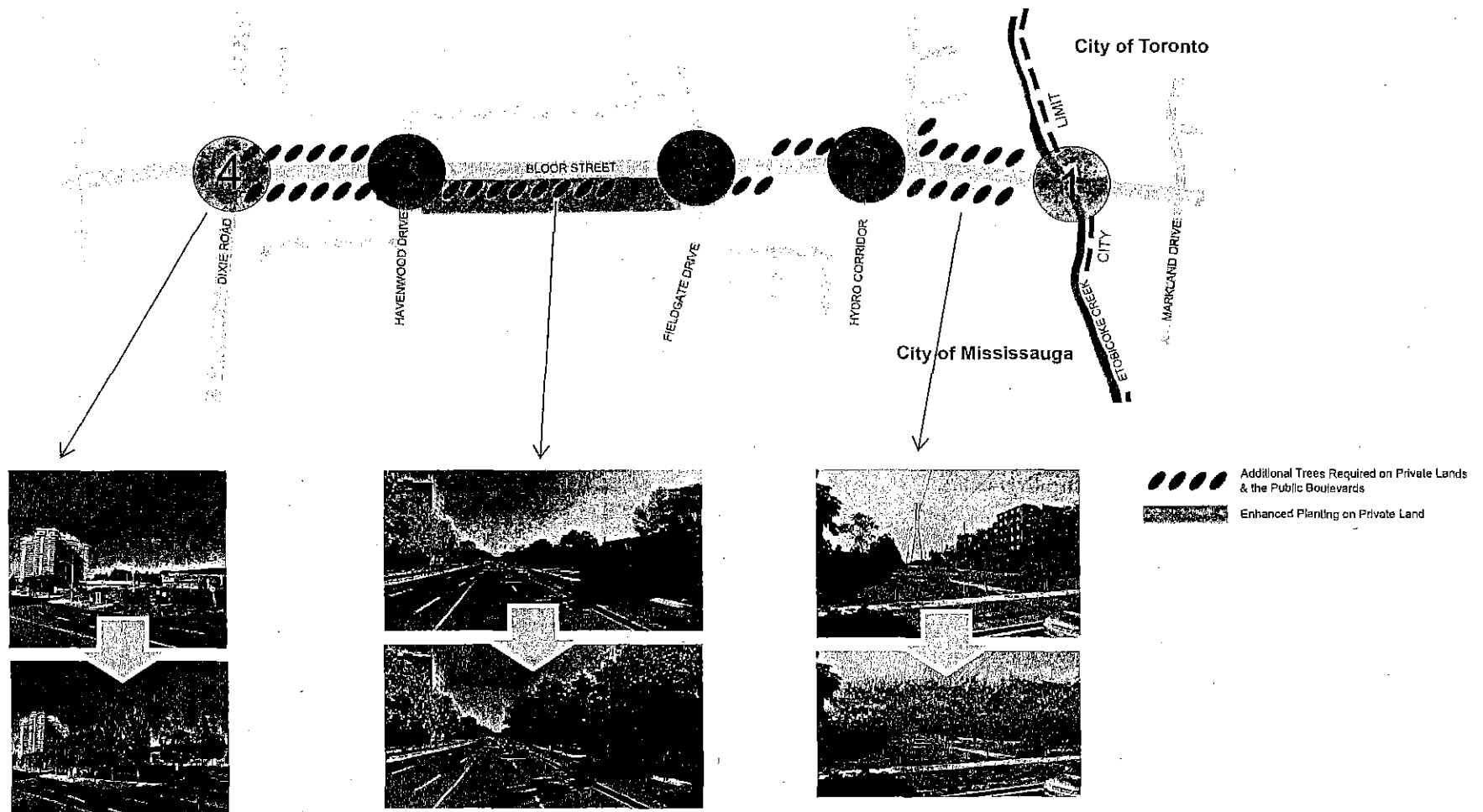


Figure 22: A concept plan for the Tree District which illustrates potential improvements along Bloor Street.

7.13 Summary - Urban Design Strategy

1. The character defining elements and patterns that shape the East Bloor Corridor are:
 - the typology of buildings which constitute midrise (slab form) buildings (5 to 14 storeys in height) and townhouse form buildings. This also includes tall buildings that rise to 27 storeys (approx.) at the western edge at Dixie Road and Bloor Street;
 - the random siting, location and placement of buildings on apartment properties that are defined by large superblocks. Collectively, these are designed and predicated on the influence of the Tower in the Park – which had significant presence as a planning model throughout the Greater Toronto Area (GTA) from 1945 - 1984, including the City of Mississauga;
 - mature landscaping that consists of trees and plantings on many apartment sites, alluding to a park like setting around buildings;
 - the siting and location of taller buildings (representing midrise slab buildings and tall towers) and lower apartment buildings in proximity to Bloor Street. Some of the lower forms transition out towards the low-rise residential areas at the periphery of the study area; and
 - surface parking located behind and/or beside apartment buildings.
2. Elements that critically help to unify the existing context include:
 - The spatial qualities and character of landscaped open space which wraps the frontages and side yards of apartment buildings; and
 - The type and mix of landscape materials and species generally consisting of deciduous and evergreen trees, and shrubbery.
3. To enhance the character of the corridor, the location, placement and design of infill on larger apartment properties should:
 - follow existing spatial patterns taking into consideration compatible heights and separation distances between buildings and adjacent properties to ensure access to sunlight, sky views, privacy, visual permeability and comfort on amenity areas, playgrounds, pathways and green areas;
 - mitigate differences in setbacks between adjacent properties, ensuring that infill projects complete streets, and follow existing patterns or orientation, particularly in the way buildings face streets and frontages;
 - ensure that proposals contribute to an orderly arrangement of heights in the study area through appropriate location and placement, and by deploying transition strategies;
 - resolve differences in height with adjacent buildings (within and concentric to the study area) through built form and/or massing treatments;

- provide ample landscape treatment in relation to the infill project; and
- provide improvements to the apartment site by:
 - improving walkability, comfort, safety, connectivity to the public realm and linkages to other apartment sites, transit, local amenities and adjacent neighbourhoods.
 - strengthen or providing landscaping, green treatments, amenity areas, playgrounds, illumination and the streetscape related to the apartment property. This includes, removing, reconsidering or replacing elements which detract from the presentation of the apartment property and study area as a whole, i.e. the prominence of frost free chain link fencing that surrounds many apartment building sites.

In addition to the above the City should pursue the creation of a “Tree District” - enhance Bloor Street into becoming a special Tree District with opportunities for entranceway treatments, planting programs, horticulture displays and improved pedestrianization of the area..

8.0 Revitalizing Existing Apartment Buildings and Neighbourhoods

Neighbourhoods evolve and change over time. In situations where change reflects a decline in the overall health of the area, action may be required.

A common concern raised with aging apartment buildings and associated neighbourhoods, is they are not adequately maintained. Infill development represents new investment in an area and can indirectly encourage surrounding owners to upgrade buildings in order to retain their tenants.

There is, however, no assurance that existing property owners will upgrade their buildings, or that individual developments will adequately address broader issues found throughout the neighbourhood (e.g. pedestrian scale street lighting).

As an issue for further review, the City should identify and assess initiatives that can encourage property owners to improve their properties as well as improve the overall health of the area. There may also be opportunities to acquire Section 37 benefits associated with increases in density from an infill development. For example, the City of Toronto Official Plan stipulates that the City may secure needed improvements and renovations to existing rental housing in accordance with policies pertaining to the use of Section 37 of the *Planning Act*, with a condition that this cost is not passed on through increased rents.

In addition to Section 37 benefits, the City could investigate the suitability of preparing a Community Improvement Plan that would

outline City initiatives intended to achieve physical improvements to the neighbourhood on both private and public lands.

There may be other initiatives that the City could examine to help revitalize older apartment areas. For example, the City of Toronto has the Office for Tower Renewal that is responsible for a program to drive broad environmental, social, economic, and cultural change by improving Toronto's concrete apartment towers and surrounding neighbourhoods.

The appropriateness of undertaking additional initiatives and programs needs further research. However, as noted earlier, policy 16.1.2.5 in the Official Plan requires as a condition of development, that existing buildings meet current property standards. Based upon this policy, and to provide further clarification, it is recommended that applications for infill development on existing apartment sites include the submission of a Property Standards Building Audit.

The audit will address issues, including but not limited, to:

- buildings, structures fences and other objects shall be kept clean of graffiti at all times;
- all structures are required to be structurally sound and all exterior walls, roofs and foundations are required to be free of water leaks and painted or otherwise treated or waterproofed;
- the interior and exterior of all buildings must have an adequate level of lighting;
- all interior hallways and stairwells of buildings must be kept clean and free of hazards;

- all heating, plumbing and mechanical systems and elevating devices must be maintained and operational in good working order;
- all driveways, walkways, ramps and parking areas on private property must be kept clear of accumulations of ice and snow and free from pot holes; and
- all land and yards are to be kept clear of inoperative vehicles and grassed and landscaped areas are to be maintained in a reasonable condition.

The possible mechanism to initiate the building audit is for the owner to obtain a letter of compliance from the City's Compliance and Licensing Enforcement staff.

The issues addressed through property standards provide an appropriate starting point for upgrades to existing buildings. Properties proposing infill development should be encouraged to undertake additional upgrades to their properties as it relates to items such as:

- green development standards (e.g. energy conservation initiatives);
- community amenities (e.g. upgrade children's playground equipment);
- undertaking a safety audit to identify measures to improve safety and implementing recommendations (e.g. trimming bushes to improve sightlines, installation of security cameras); and
- improved pedestrian infrastructure (walkways and landscaping).

Some of the additional upgrades may be appropriate to be addressed through the site plan approval process (e.g. landscaping), whereas other upgrades may require additional incentives or partnerships with various government agencies in order to implement.

9.0 Conclusion & Issues For Further Review:

This study examined the existing characteristics and context of the easterly portion of the Bloor corridor, from the west side of Dixie Road to the Etobicoke Creek. It reviewed the planning framework and the area's potential role in accommodating intensification. A general assessment of infill opportunities was completed and interim urban design guidelines prepared. Key findings included:

- The area can benefit from sensitive infill as permitted in the City's Official Plan. Opportunities for infill are generally distributed around three areas along the 1.8 kilometre (1.1 mile) corridor. There may be a few opportunities where development greater than permitted in the Official Plan is appropriate; however, those cases should be reviewed on a site specific basis;
- The Official Plan includes policies that provide direction on intensification and infill development, and, where appropriate, it allows the City to request additional studies to assess a proposed development's impacts on areas such as traffic and community infrastructure;
- Urban Design Guidelines (Interim) have been prepared to assist in the review of applications, and through the Site Plan Approval process, additional development could contribute positively to the area and appropriately reflect the existing character;

- Mature trees along Bloor Street have created a strong presence in the corridor; this should be maintained and supplemented with significantly more green landscaping. It is proposed that the corridor evolve into a special "Tree District"; and
- To promote and ensure reinvestment in the existing buildings and revitalization of the neighbourhood, infill development applications should include a Property Standards Building Audit. Further, the City should investigate potential incentives to encourage property owners to undertake upgrades including securing community benefits under Section 37 of the *Planning Act*.

These findings provide the basis for reviewing applications and potentially undertaking additional research on issues that were beyond the scope of this study. Additional areas for further investigation include:

- Transportation review: examine current transportation issues in the area, including traffic patterns, potential changes to active transportation (e.g. potential cycling lanes), and research on the extent to which people walk, rely on transit, or take a vehicle;
- Cultural and social infrastructure review: examine potential initiatives that can help strengthen the community (e.g. public art);

- Streetscape improvements: research and identify appropriate public realm strategies that can improve the area. Develop an inventory of existing trees over 100 cm in diameter along Bloor Street. The tree inventory should note tree species, present diameter, location, drip line of the tree and condition;
- Community improvement initiatives: examine the policy tools available to encourage property owners to improve buildings and investigate experiences in Toronto with the Tower Renewal program.

In addition, where appropriate, further work should consider a public engagement component, in order to identify and understand concerns as well as potentially generate ideas for improvement. As a result of any future work, it may be necessary to review the Official Plan and provide additional policies and/or guidelines.



Corporate Report

PLANNING & DEVELOPMENT COMMITTEE

APR 02 2013

Clerk's Files

Originator's

Files OZ 11/012 W3

DATE: March 18, 2013

TO: Chair and Members of Planning and Development Committee
Meeting Date: April 2, 2013

FROM: Edward R. Sajecki
Commissioner of Planning and Building

SUBJECT: **Rezoning Application**
To permit ten (10) street townhouse dwellings and maintain the existing apartment building
1440 Bloor Street
Southeast corner of Bloor Street and Dixie Road
Owner: Tapes Investments
Applicant: Peter Favot Architect Ltd.
Bill 51

Supplementary Report

Ward 3

RECOMMENDATION: That the Report dated March 18, 2013, from the Commissioner of Planning and Building recommending approval of the application under file OZ 11/012 W3, Tapes Investments, 1440 Bloor Street, southeast corner of Bloor Street and Dixie Road, be adopted in accordance with the following:

1. That the application to change the Zoning from "RA2-40" (Apartment Dwellings) to "RM5-Exception" (Street Townhouse Dwellings) and "RA2-Exception" (Apartment Dwellings) to permit ten (10) street townhouse dwellings fronting onto Tyneburn Crescent and maintain the existing apartment building in accordance with the proposed zoning

standards described in the Information Report, be approved subject to the following conditions:

- (a) That the applicant agree to satisfy all the requirements of the City and any other official agency concerned with the development;
- (b) That the school accommodation condition as outlined in City of Mississauga Council Resolution 152-98 requiring that satisfactory arrangements regarding the adequate provision and distribution of educational facilities have been made between the developer/applicant and the School Boards, not apply to the subject lands since both Boards are satisfied with the provision of educational facilities for the catchment area;
- (c) That Council Resolution 160-91, which requires that a minimum of three car spaces per dwelling, including those in a garage be required on-site and a minimum of 0.25 on-street visitor parking spaces per dwelling be required for dwellings on lots less than 12 m (39.4 ft.) of frontage for the subject development, not apply to the subject lands since there is a sufficient parking supply for the development;
- (d) That the following clauses be included within Schedule C of the Development Agreement to be registered on title:
 - i) "Prior to Site Plan approval for any building permit clearance, the owner shall include, and secure for, within the site plan for the proposed townhouses, improvements which meet or exceed those shown on the concept plan provided under file OZ 11/012 W3 regarding streetscape, landscaping, parking and playground modifications to the retained apartment lands known as 1440 Bloor Street to the satisfaction of the Planning and Building Department";

- ii) "Prior to Site Plan approval for any building permit clearance, the owner shall undertake and submit a Property Standards Building Audit to demonstrate compliance with the Property Standards By-law. A letter of compliance shall then be required to be issued by the City's Compliance and Licencing Enforcement Section, confirming that standards are met prior to site plan approval";
 - (e) That Schedule "F" of the Development Agreement provide covenants on the part of the owner for the comprehensive provision and implementation of the required site improvements referred to in paragraph (d) above together with the development of any part of the lands including provisions which require as a condition to any severance of any part of the lands by way of a consent or exemption from part lot control that:
 - i) the transferee of the severed lands execute and register an agreement to be bound by the Development Agreement;
 - ii) notwithstanding the severance, the requirement for the joint filing and implementation of one comprehensive site plan application and site plan agreement for the entire lands which provide for the required site improvements together with the townhouse development; and
 - iii) the execution and registration of such further agreements the City may require to ensure the joint provision and implementation by the separate owners of the required site improvements in conjunction with the development of any part of lands.
2. That the decision of Council for approval of the rezoning application be considered null and void, and a new

development application be required unless a zoning by-law is passed within 18 months of the Council decision.

3. That Council direct Legal Services, representatives from the appropriate City Departments and necessary consultants, to attend any Ontario Municipal Board proceedings which may take place in connection with the application and in support of the recommendations outlined in the report dated March 18, 2013.
4. That City Council provide the Planning and Building Department with the authority to instruct Legal Services staff on any modifications deemed necessary, where required, through the Ontario Municipal Board hearing process.

**REPORT
HIGHLIGHTS:**

- The owner has appealed the rezoning application to the Ontario Municipal Board for failure to make a decision within 120 days;
- The first phase of the East Bloor Corridor Review has been completed and is also scheduled for the April 2, 2013 Planning and Development Committee meeting;
- The proposed rezoning is in conformity with the Official Plan and the Planning and Building Department recommends that the application be approved subject to conditions;
- Outstanding issues of design, landscaping, and site improvements must be resolved prior to site plan approval;
- Staff are seeking direction from Council to attend any Ontario Municipal Board proceedings which may take place in connection with the application and in support of the recommendations outlined in this report.

BACKGROUND:

A public meeting was held by the Planning and Development Committee on September 4, 2012, at which time a Planning and Building Department Information Report (Appendix S-1) was presented and received for information.

At the public meeting, the Planning and Development Committee passed Recommendation PDC-0051-2012 which was subsequently adopted by Council and is attached as Appendix S-3.

On January 3, 2013, the owner appealed the application to the Ontario Municipal Board (OMB) due to failure by Council to make a decision within 120 days, pursuant to subsection 34(11) of the *Planning Act*.

COMMENTS:

See Appendix S-1 - Information Report prepared by the Planning and Building Department.

COMMUNITY ISSUES

Further to the May 16, 2012 community meeting, the September 4, 2012 Public Meeting, a petition of objection to the proposal received by Council on January 18, 2012, and other written comments, the following is a summary of issues raised by the community:

Comment

There were concerns with the amount of traffic, on-street parking, and road safety in the area.

Response

The proposed development of ten street townhouse units is not expected to create a significant impact on the current traffic pattern in the area. On-street parking is currently permitted on Tyneburn Crescent for all residents, however, residents may petition Council to prohibit on-street parking if desired. With respect to safety, the proposed driveway locations have been reviewed and are satisfactory.

Comment

The significant traffic in the area during school drop-off and pick-up times, and as a result of Golden Orchard Drive and Fieldgate Drive not being open for through access.

Response

Traffic has been observed to filter through local roads in the neighbourhood, likely as a result of closing Golden Orchard Drive and Fieldgate Drive as a neighbourhood ring-road. The East Bloor Corridor Review recommends that a transportation analysis be undertaken to review and make recommendations concerning this matter.

Comment

The townhomes would change the nature of the residential area and their possible tenure could affect property values.

Response

The area currently has rental apartments located immediately across the street from semi-detached dwellings. The addition of freehold street townhouses will provide a logical and gradual transition from the existing high density residential to the existing semi-detached dwellings and then detached dwellings further south.

Comment

There was concern with the current condition of the apartment building and how it was maintained.

Response

The existing apartment building is required to meet building code and property standards requirements. The proposal seeks to improve the outdoor spaces of the retained apartment lands through landscaping, additional plantings, and play area improvements. The City will require that the existing building complies with the current property standards as a condition of development. Additional detail is provided in the Planning Comments section of this report.

Comment

There was a concern with the reduction in landscaped open space and the lack of playground space for children at the existing apartment building.

Response

The proposal would maintain the minimum landscaped area requirement on the retained apartment lands. The By-law requires a minimum of 40% of an apartment zone to be landscaped. With the proposed townhomes, the apartment lands would provide a landscaped area of 45.4%, exceeding this requirement. The playground is to be upgraded through the site plan application associated with this development.

Comment

There was a question regarding the provision of an open park space for area residents.

Response

Burnhamthorpe Park (P-053) is located approximately 500 m (1,640 ft.) from the subject property and includes a play area, a senior soccer field, a multi pad with a basketball hoop and two tennis courts. The Community Services Department have not identified any additional park requirements in this area.

Comment

The subject lands may be contaminated with fuel from the area gas stations.

Response

A Phase 1 Environmental Site Assessment report, dated September 2012 by Pinchin Environmental, was prepared to assess any potential contamination on the property. The study recommended

that a more detailed review be undertaken based upon the two gas stations located in close proximity of the site. As a result, a Phase 2 Environmental Assessment report was prepared and 4 boreholes were drilled to test the soil, one of which was completed as a groundwater monitoring well. The worst case samples were analysed and satisfy Ministry of Environment standards, and therefore, no further subsurface investigation is required.

Comment

The proposed townhomes will be located too close to the rear of 3361 and 3351 Dixie Road at 0.9 m (3 ft.) and a request of a minimum of 6.0 m (20 ft.) was received.

Response

The setback to this adjacent property limit is proposed to be 3 m (9.8 ft.) not 3 feet, which is double the 1.5 m (4.9 ft.) standard side yard setback requirement.

UPDATED AGENCY AND CITY DEPARTMENT COMMENTS

Transportation and Works Department

Comments updated February 14, 2013, state that in previous comments the Transportation and Works department confirmed receipt of updated Grading Plan, and Phase 1 and 2 Environmental Site Assessment reports submitted in response to previous comments and matters raised at the Public Meeting held on September 4, 2012. The materials provided did not address all of the concerns identified in the previous comments.

The applicant was requested to provide updated plans, including Grading Plan and Concept Plan, an updated Functional Servicing Report, an updated Parking Plan and a detailed servicing plan in support of the development; however, the updated materials still remain outstanding.

To address environmental concerns raised at the Public Meeting, a Phase 2 Environmental Site Assessment, dated September 28, 2012 by Pinchin Environmental, was received. The report indicated that the subject site met the appropriate Ministry of Environment (MOE) standards and the possibility of contaminants migrating to the site is very low, and, therefore, no further investigation is necessary. The applicant has been requested to submit a letter of reliance to further validate the supporting reports.

School Accommodation

In comments, dated January 29, 2013 and January 30, 2013, the Peel District School Board and the Dufferin-Peel Catholic District School Board responded that they are satisfied with the current provision of educational facilities for the catchment area and, as such, the school accommodation condition as required by City of Mississauga Council Resolution 152-98 pertaining to satisfactory arrangements regarding the adequate provision and distribution of educational facilities need not be applied for this development application.

PLANNING COMMENTS

Official Plan

Mississauga Official Plan (2012) was adopted by City Council on September 29, 2010 and partially approved by the Region of Peel on September 22, 2011. The Plan was appealed in its entirety; however, on November 14, 2012 the Ontario Municipal Board issued a Notice of Decision approving Mississauga Official Plan, as modified, save and except for certain appeals which have no effect on the subject application.

The proposal does not require an amendment to the Mississauga Official Plan policies.

The subject site is designated "**Residential High Density**" and, as per Section 16.1.25, development in addition to existing buildings on Residential High Density lands will be restricted to uses

permitted in the Residential Medium Density designation, which includes townhouses and all forms of horizontal multiple dwellings, provided the site in its entirety meets current site plan, landscaping, and building code requirements.

The subject property and those properties on the north side of Tyneburn Crescent are currently designated and developed for apartment buildings (8 storeys on subject lands, 6 storeys on adjacent lands). The proposal for a maximum of ten street townhouse dwellings fronting onto Tyneburn Crescent will provide an appropriate finished street edge, buffer, and transition in height and dwelling form between the existing rental apartments on the north side of Tyneburn Crescent and the semi-detached dwellings on the south side of Tyneburn Crescent. The proposal meets the intent of the Official Plan (see Mississauga Official Plan Policies in Appendix S-2) by respecting the existing and planned character of the neighbourhood, screening unattractive views of the parking lot for the apartment site, and providing natural surveillance and active building frontages facing the public street.

Infilling of Existing Apartment Sites

The East Bloor Corridor Review recognizes the need to revitalize existing apartment buildings along this section of Bloor Street, and suggests a number of options that the City may pursue to achieve this reinvestment.

Currently, Section 16.1.2.5 of the Official Plan requires that as a condition of infill development on an apartment site, the site in its entirety must meet current site plan and landscape requirements, and the existing building must meet current property standards.

Site improvements and landscaping will be provided for the existing apartment lands and Bloor Street frontage through the site plan application associated with this infill development.

In order to ensure that the building meets current property standards and complies with the policy, it is recommended that a Property Standards Building Audit be submitted to demonstrate

compliance with the Property Standards By-law. The audit would address a range of issues related to the condition and maintenance of the premises including, but not limited to, graffiti removal, structural soundness, lighting, heating, plumbing, mechanical systems and elevating devices. A letter of compliance would subsequently be issued by Compliance and Licencing Enforcement, confirming that standards are met prior to site plan approval.

These requirements will be secured through the Development Agreement associated with this application. The Agreement will also require that as a condition of any severance, by way of consent or exemption from part lot control, that any new owners execute and register an agreement to be bound by the Development Agreement, and that a joint comprehensive site plan application be required for the entire lands in order to ensure the required site improvements are implemented.

Zoning

The proposed "RM5-Exception" (Street Townhouse Dwellings) and "RA2-Exception" (Apartment Dwellings) zones are appropriate to accommodate ten (10) street townhouse dwellings fronting onto Tyneburn Crescent and maintain the existing apartment building for the reasons discussed above.

Council Resolution 160-91

Council resolution 160-91 states that a minimum of three on-site parking spaces are required per dwelling unit and a minimum of 0.25 on-street visitor parking spaces per dwelling are required for dwellings on lots less than 12 m (39.4 ft.) in frontage. The applicant has submitted a parking plan showing two on-site parking spaces per dwelling (one within the driveway, and one within the garage), and five on-street visitor parking spaces. Given the proposal is for a maximum of ten street townhouse units, the amount of on-street visitor spaces that the applicant are providing equates to a rate of 0.5 spaces per unit, or double the 0.25 on-street requirement. As a result of the above, and since the general

Zoning By-law parking requirement is 2 spaces per unit, it is concluded that there is a sufficient parking supply for the proposed development and it is recommended that Council Resolution 160-91 not apply.

Site Plan

The applicant has not yet submitted an application for site plan approval, and has advised the elevations provided are conceptual only. Staff have expressed urban design concerns with the building elevations that were submitted, which are not acceptable for approval and should be reconsidered. Any site plan application would be subject to the Low Rise Multiple Dwelling Guidelines and the East Bloor Corridor Review recommendations which state that exterior design for infill proposals have well articulated architectural expression that is compatible with the character and style of buildings in the general vicinity.

Staff had requested that a site plan application be submitted prior to the Supplementary Report to assist in the review of the application and inform the detailed standard requirements, however, a site plan cannot be mandated in advance of rezoning.

Prior to By-law enactment a Development Agreement will need to be executed between the City and the owner. One requirement will be that the site plan provide for improvements which meet or exceed those shown on the applicant's concept plan regarding streetscape, landscaping, parking and playground modifications to the retained apartment lands, to the satisfaction of the Planning and Building Department. This requirement will also be bound to any potential purchaser as a condition of any consent to sever the lands or exemption from part lot control, in order to ensure a joint site plan application for the entire lands and implementation of the necessary site improvements.

FINANCIAL IMPACT:

Development charges will be payable in keeping with the requirements of the applicable Development Charges By-law of the City as well as financial requirements of any other official agency concerned with the development of the lands.

CONCLUSION:

The proposed rezoning is acceptable from a planning standpoint and should be approved for the following reasons:

1. The proposal for ten (10) street townhouse dwellings fronting Tyneburn Crescent and maintaining the existing apartment building with additional site improvements is compatible with the surrounding land uses as it provides for a completion of the street and a transition in scale and form from existing apartments to existing low rise dwellings.
2. The proposal meets the policies and objectives of the Official Plan and phase one of the East Bloor Corridor Review.
3. The proposed "RM5-Exception" (Street Townhouse Dwellings) and "RA2-Exception" (Apartment Dwellings) zoning standards are appropriate to accommodate the requested uses.

ATTACHMENTS:

Appendix S-1: Information Report

Appendix S-2: Mississauga Official Plan Policies

Appendix S-3: Recommendation PDC-0051-2012



Edward R. Sajecki
Commissioner of Planning and Building

Prepared By: Jonathan Famme, Development Planner



Corporate Report

PDC SEP 04 2012

Clerk's Files

Originator's
Files OZ 11/012 W3

DATE: August 14, 2012

TO: Chair and Members of Planning and Development Committee
Meeting Date: September 4, 2012

FROM: Edward R. Sajecki
Commissioner of Planning and Building

SUBJECT: **Information Report**
Rezoning Application
To permit ten (10) street townhouse dwellings and
maintain the existing apartment building
1440 Bloor Street
Southeast corner of Bloor Street and Dixie Road
Owner: Tapes Investments
Applicant: Peter Favot Architect Ltd.
Bill 51

Public Meeting **Ward 3**

RECOMMENDATION: That the Report dated August 14, 2012, from the Commissioner of Planning and Building regarding the application to change the Zoning from "RA2-40" (Apartment Dwellings) to "RM5-Exception" (Street Townhouse Dwellings) and "RA2-Exception" (Apartment Dwellings), to permit ten (10) street townhouse dwellings fronting onto Tyneburn Crescent and maintain the existing apartment building under file OZ 11/012 W3, Tapes Investments, 1440 Bloor Street, southeast corner of Bloor Street and Dixie Road, be received for information.

**REPORT
HIGHLIGHTS:**

- A rezoning application has been made to permit ten (10) street townhouse dwellings fronting onto Tyneburn Crescent in addition to maintaining an existing eight (8) storey apartment building on-site;
- Community concerns relate to increased traffic, safety concerns, loss of landscaped space, creating precedent for other

- apartment sites, and potential contamination;
- Prior to the Supplementary Report, matters to be addressed include the appropriateness of the proposed zoning by-law amendment, provision of additional details on grading, parking, servicing, and the Phase 1 Environmental Site Assessment, and review for conformance with the recommendations of the Dixie/Bloor Corridor Review.

BACKGROUND:

The above-noted application has been circulated for technical comments and a community meeting has been held.

The purpose of this report is to provide preliminary information on the application and to seek comments from the community.

The applicant intends to sever the proposed street townhouse lands from the existing apartment lands (to be retained) through the Committee of Adjustment, thus creating two (2) separate parcels, and then create the ten (10) separate freehold townhouse lots.

Ward 3 Councillor, Chris Fonseca has requested that a review be undertaken of the Dixie/Bloor Corridor. The Planning and Building Department is initiating this review to consider land use, built form, and social infrastructure, as well as physical improvements along Bloor Street from the west side of Dixie Road to the City of Toronto boundary to the east. (See page 7 for additional detail).

COMMENTS:

Details of the proposal are as follows:

Development Proposal	
Application submitted:	August 30, 2011 (Received)
	September 27, 2011 (Deemed Complete)
Gross Floor Area:	Proposed street townhouses: 220 m ² (2,368 sq. ft.) per unit x 10 units (1.15 times the individual lot areas) Existing apartment building: 8 668.6 m ² (93,308 sq. ft.)

Development Proposal	
	Total Gross Floor Area: 10 868.5 m ² (116,988 sq. ft.)
Height:	Proposed street townhouses – 3 storeys Existing apartment building – 8 storeys
Floor Space Index:	Entire Site (existing): 0.72 Retained apartment lands: 0.94 Existing apartment building plus proposed street townhouses: 0.96
Landscaped Area:	Proposed street townhouse lots: 46% per unit Retained apartment lands: 45.4%
Net Density:	Proposed severed street townhouse lands: 48.5 units/ha (19.6 units/ac.) Retained apartment lands: 100 units/ha (40 units/ac.) Total site area: 91 units/ha (37 units/ac.)
Number of units:	Proposed street townhouses – 10 Existing apartment building – 93
Anticipated Population:	30* - Proposed street townhouses *Average household sizes for all units (by type) for the year 2011 (city average) based on the 2008 Growth Forecasts for the City of Mississauga.
Parking Required:	Proposed street townhouses: 2 spaces/unit = 20 spaces (Council Resolution 160-91 requires 3 spaces per unit and 0.25 spaces per unit on-street)
Parking Provided:	Proposed street townhouses: 2 spaces/unit = 20 spaces Existing apartment building: Underground parking 42 spaces Surface parking 108 spaces Total 150 spaces
Supporting Documents:	Functional Servicing Report Noise Study

Development Proposal	
	Planning Justification Tree Inventory Grading Plan Concept Plan and Elevations Survey

Site Characteristics	
Frontages:	118.0 m (387.0 ft.) on Bloor Street 87.7 m (287.7 ft.) on Tyneburn Crescent
Depth:	Entire site: 107.94 m (354.1 ft.) Severed portion: 24.5 m (80.4 ft.)
Lot Area:	Entire site: 1.132 ha (2.80 ac.) Proposed severed street townhouse lands: 0.206 ha (0.51 ac.) Retained apartment lands: 0.926 ha (2.29 ac.) Individual street townhouse lots: 191.1 m ² (2,057 sq. ft.)
Existing Use:	8 storey apartment building

Green Development Initiatives

The applicant has identified that the following green development initiatives will be incorporated into the development:

- extensive planting of additional trees on the retained apartment lands;
- permeable paving to be considered for new parking area on retained apartment lands south of Bloor Street.

Additional information is provided in Appendices I-1 to I-11.

Neighbourhood Context

The subject property is located in an area of predominantly high density residential apartments along Bloor Street, with some commercial uses at the intersection with Dixie Road, while detached and semi-detached dwellings are located to the south. The property is currently occupied by an eight (8) storey rental apartment building. Information regarding the history of the site is found in Appendix I-1.

The surrounding land uses are as follows:

- North: Gas station, commercial plaza, seven (7) storey and six (6) storey apartment buildings across Bloor Street;
- East: Three six (6) storey apartment buildings;
- South: Semi-detached dwellings, and detached dwellings across Tyneburn Crescent;
- West: Gas station, and 26 storey apartment building across Dixie Road;

Current Mississauga Plan Designation and Policies for Applewood District (May 5, 2003)

"Residential High Density I" which permits apartment dwellings at a Floor Space Index (FSI) of 0.5 - 1.2.

Section 4.3.4.2 - Intensification of Existing Apartment Sites

Proposals for additional development on lands with existing apartment buildings will be subject to the following, in addition to other policies regarding medium and high density residential development in this Plan:

- a. on lands designated Residential High Density I or II, other than those located within the Node, development in addition to existing buildings will be restricted to the uses permitted in the Residential Medium Density I designation, up to the maximum density specified for the Residential High Density designation;

- b. as a condition of development, the site in its entirety must meet current site plan and landscaping requirements, and existing buildings must meet current building code, fire code and property standards.

The "Residential Medium Density I" designation permits townhouse dwellings.

The application is in conformity with the land use designation and no official plan amendment is proposed.

Other policies in the Official Plan which also are applicable in the review of this application are summarized in Appendix I-9.

Mississauga Official Plan (2011)

Mississauga Official Plan (2011) was adopted by City Council on September 29, 2010 and partially approved by the Region on September 22, 2011. Mississauga Official Plan (2011) has been appealed in its entirety and, as such, the existing Mississauga Plan (2003) remains in effect. The applications were originally submitted under Mississauga Plan (2003), which is the current plan in effect, but regard should be given to the new Mississauga Official Plan (2011). Under the new Mississauga Official Plan, the subject lands are designated "**Residential High Density**". The proposed ten (10) street townhouse dwellings conform to the land use designation contained in the new Mississauga Official Plan and associated policies.

Existing Zoning

"**RA2-40**" (**Apartment Dwellings**), which permits apartment dwellings, long-term care dwellings, and retirement dwellings with a Floor Space Index (FSI) of 0.5 - 0.8.

Proposed Zoning By-law Amendment

"**RM5-Exception**" (**Street Townhouse Dwellings**), to permit ten (10) street townhouse dwellings with minimum lot frontages of 7.8 m (25.6 ft.) and minimum lot areas of 191 m² (2,056 sq. ft.).

"RA2-Exception" (Apartment Dwellings), to permit the existing apartment dwelling on the retained lands with a maximum Floor Space Index (FSI) of 0.94, as a result of the reduced lot area.

The proposed draft zoning standards can be found in Appendix I-10.

Dixie/Bloor Corridor Review

In consultation with Ward 3 Councillor Chris Fonseca, the Planning and Building Department is undertaking a review of the redevelopment and infill development opportunities along Bloor Street from Dixie Road easterly to the City of Toronto boundary. The objectives of the Dixie/Bloor Corridor Review include:

- to identify potential issues with infilling, assess the existing policy framework that addresses these issues, and determine whether changes to the land use policy framework are necessary;
- to assess the existing built form and prepare a built form policy framework to guide the review of infill development applications;
- to review and identify potential improvements to the public realm including the streetscape;
- to investigate policies and/or tools that encourage property developers to undertake physical improvements to existing apartment buildings; and
- initiatives that can help ensure a strong neighbourhood which thrives on its social and cultural diversity (i.e. health of the social infrastructure).

While the subject application can be evaluated on its own merit, consideration for the objectives and recommendations of the Bloor Street Corridor Review is appropriate.

The current official plan provides some guidance for intensification of existing apartment sites provided that the site in its entirety meets current site plan and landscaping requirements. In the absence of the Corridor Review recommendations which

are intended to specifically identify what the site planning and public realm requirements would be, and a detailed site plan that includes improvements to the retained apartment lands, it has not been determined whether the existing policy framework has been sufficiently addressed. The Dixie/ Bloor Corridor Review will assist in informing this analysis.

The supplementary report for this matter will consider the application's merits and it's alignment with the Dixie/Bloor Corridor review's objectives.

COMMUNITY ISSUES

A community meeting was held by Ward 3 Councillor, Chris Fonseca, on May 16, 2012.

A petition of objection to the proposal was received by Council on January 18, 2012.

The following is a summary of issues raised by the community:

- The amount of traffic, on-street parking, and road safety on Tyneburn Crescent.
- The significant traffic in the area during school drop-off and pick-up times, and as a result of Golden Orchard Drive and Fieldgate Drive not being open for through access.
- The townhomes would change the nature of the residential area and their possible tenure could affect property values.
- The current condition of the apartment building and how it was maintained.
- The vehicular access to the proposed townhouses being provided from Tyneburn Crescent and a preference to have access from Bloor Street.

- The reduction in landscaped open space and the lack of playground space for children at the existing apartment building.
- The subject lands may be contaminated with fuel from the area gas stations.

Responses to the above comments will be provided in the Supplementary Report.

DEVELOPMENT ISSUES

Agency comments are summarized in Appendix I-7 and school accommodation information is contained in Appendix I-8. Based on the comments received and the applicable Mississauga Plan policies, the following matters will have to be addressed:

- appropriateness of the proposed application;
- provision of revised plans that address certain grading concerns and technical discrepancies;
- update to the Functional Servicing Report and submission of a servicing plan;
- update to the Parking Plan;
- validation of the Phase 1 Environmental Site Assessment;
- review this proposal for conformance with the recommendations of the Dixie/Bloor Corridor Review.

OTHER INFORMATION

Development Requirements

In conjunction with the proposed development, there are certain other engineering and servicing matters including noise mitigation, stormwater management, and cash-in-lieu for parkland, which will require the applicant to enter into appropriate agreements with the City.

FINANCIAL IMPACT: Development charges will be payable in keeping with the requirements of the applicable Development Charges By-law of the City as well as financial requirements of any other official agency concerned with the development of the lands.

CONCLUSION: Most agency and City department comments have been received and after the public meeting has been held and all issues are resolved, the Planning and Building Department will be in a position to make a recommendation regarding this application.

ATTACHMENTS:

- Appendix I-1: Site History
- Appendix I-2: Aerial Photograph
- Appendix I-3: Excerpt of Applewood District Land Use Map
- Appendix I-4: Excerpt of Existing Land Use Map
- Appendix I-5: Concept Plan
- Appendix I-6: Elevations
- Appendix I-7: Agency Comments
- Appendix I-8: School Accommodation
- Appendix I-9: Official Plan Policies
- Appendix I-10: Proposed Zoning Standards
- Appendix I-11: General Context Map



Edward R. Sajecki
Commissioner of Planning and Building

Prepared By: Jonathan Famme, Development Planner

Tapes Investments

File: OZ 11/012 W3

Site History

- 1967 – Existing apartment building constructed.
- April 14, 1992 – Site plan revision (SP 90/218) approved for two (2) apartment units created in unused portion of building and the addition of 18 parking spaces.
- May 5, 2003 – Applewood District Policies and Land Use Map (Mississauga Plan) were partially approved with modifications by the Region of Peel. The subject lands were designated "Residential High Density I.
- June 20, 2007 – Zoning By-law 0225-2007 came into force except for those sites which have been appealed. As no appeals have been filed the provisions of the new By-law apply. The subject lands are zoned "RA2-40" (Apartment Dwellings).



LEGEND:



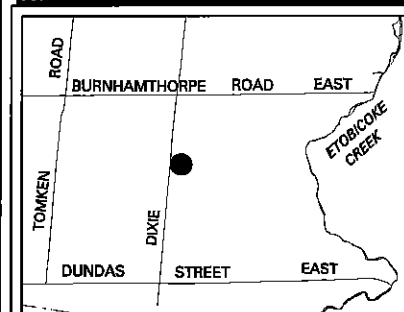
SUBJECT LANDS

NOTE: DATE OF AERIAL PHOTO IS APRIL 2012



SUBJECT:

TAPES INVESTMENTS



FILE NO:
OZ 11/012 W3

DWG. NO:
11012A

SCALE:
1:2500

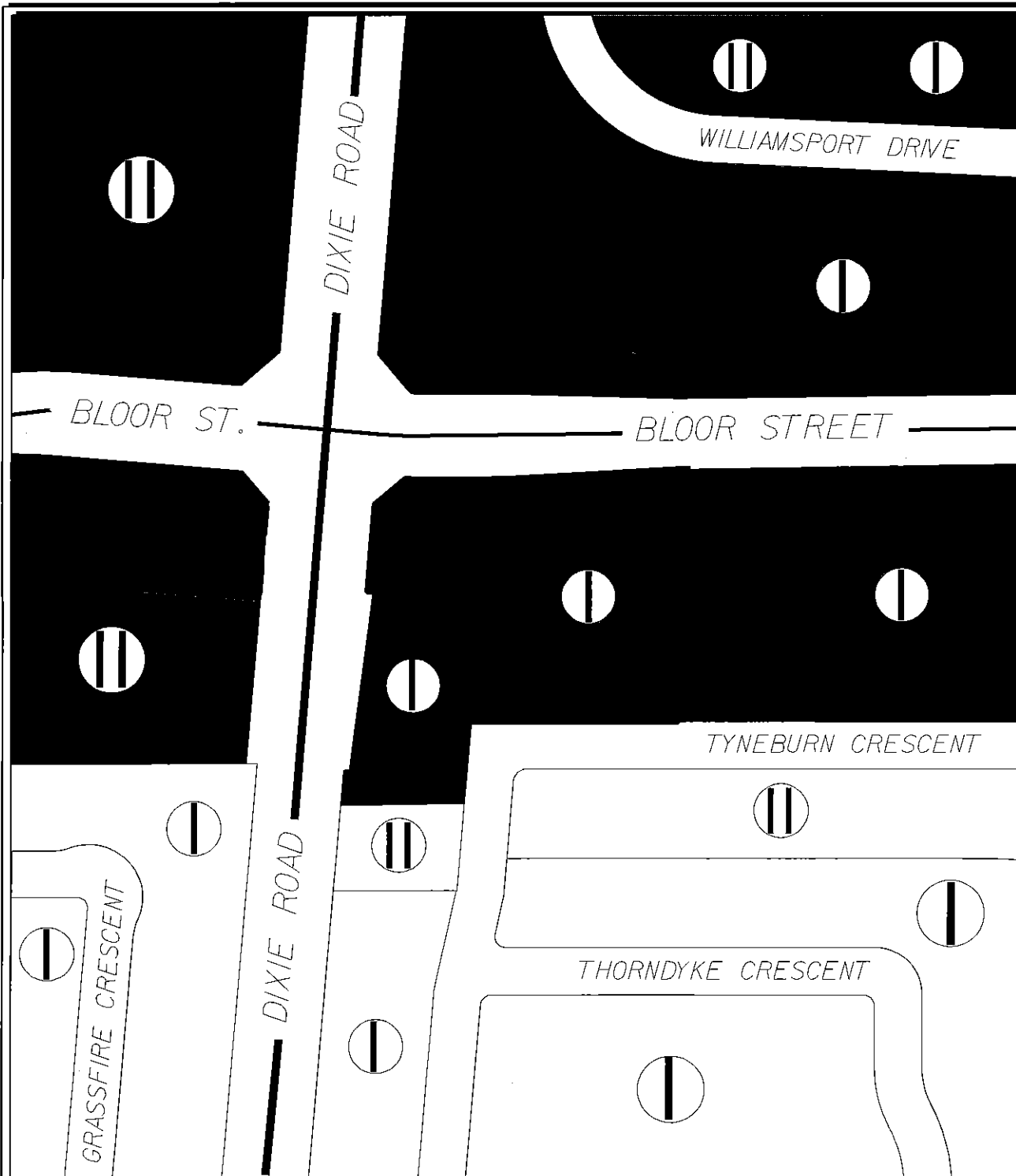
PDC DATE:
2012/09/04

DRAWN BY:
A.SHAH

APPENDIX I-2

MISSISSAUGA
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T&W, Geomatics



PART OF APPLEWOOD DISTRICT LAND USE MAP APPLEWOOD DISTRICT POLICIES OF MISSISSAUGA PLAN

LAND USE DESIGNATIONS

- Residential - Low Density I
- Residential - Low Density II
- Residential - Medium Density I
- Residential - Medium Density II
- Residential - High Density I
- Residential - High Density II
- General Retail Commercial
- Convenience Retail Commercial
- Motor Vehicle Commercial
- Office
- Public Open Space
- Private Open Space
- Greenbelt
- Utility

TRANSPORTATION LEGEND

- Arterial
- Major Collector
- Minor Collector
- Local Road
- Major Transit Corridor

Amendment No. 95 Appealed to the Ontario Municipal Board.
Delete and replace with Higher Order Transit Corridor

LAND USE LEGEND

- Node Boundary
- Cem - Cemetery
- 1996 NEP/2000 NEF Composite Noise Contours
- Planning District

Special Site Areas (See Special Site Policies)

NOTE:
ANY PART OF THE ROAD NETWORK SHOWN OUTSIDE THE CITY BOUNDARIES IS SHOWN FOR INFORMATION PURPOSES ONLY.

Note:
The 1996 Noise Exposure Projection (NEP/2000 Noise Exposure Forecast (NEF) Composite Noise Contours are shown for information purposes only and are not part of the Official Land Use Map. These contours will change from time to time as new information becomes available.
For accurate reference the composite NEP/NEF map produced by Transport Canada at a scale of 1:50 000 should be consulted.

Amendment No. 95 Appealed to the Ontario Municipal Board

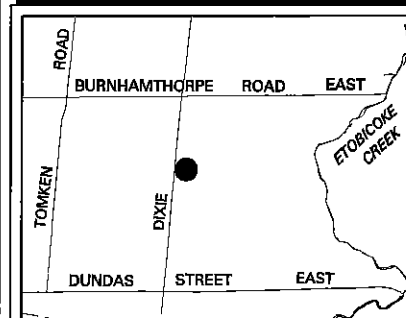


SUBJECT LANDS



SUBJECT:

TAPES INVESTMENTS



FILE NO:
OZ 11/012 W3

DWG. NO:
11012L

SCALE:
1:2500

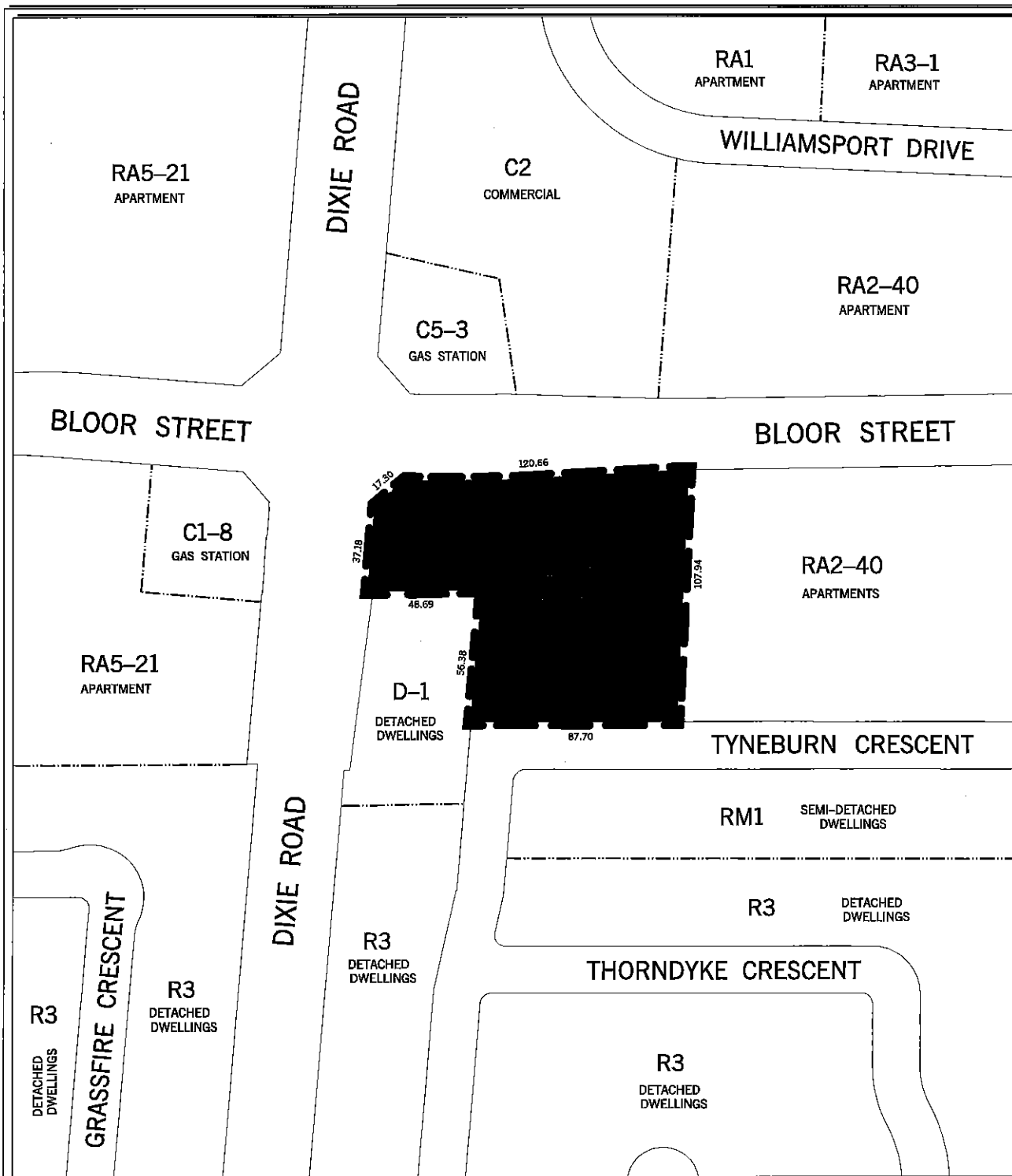
PDC DATE:
2012/09/04

DRAWN BY:
A.SHAH

APPENDIX I-3

MISSISSAUGA
Planning and Building

Produced by
T&W, Geomatics



LEGEND:



PROPOSED REZONING FROM "RA2-40" (APARTMENT DWELLINGS) TO "RM5-EXCEPTION" (STREET TOWNHOUSE DWELLINGS) TO PERMIT TEN (10) STREET TOWNHOUSE DWELLINGS.



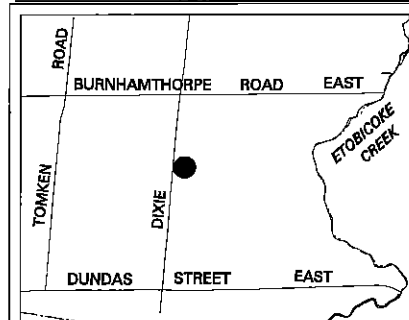
PROPOSED REZONING FROM "RA2-40" (APARTMENT DWELLINGS) TO "RA2-EXCEPTION" (APARTMENT DWELLINGS) TO MAINTAIN THE EXISTING APARTMENT BUILDING.

NOTE: EXISTING ZONING DELINEATED ON THE PLAN
PROPOSED ZONING INDICATED BY SHADING WITHIN
THE APPLICATION AREA.



SUBJECT:

TAPES INVESTMENTS



FILE NO:
OZ 11/012 W3

DWG. NO:
11012R

SCALE:
1:2500

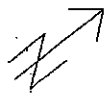
PDC DATE:
2012/09/04

DRAWN BY:
A.SHAH

APPENDIX I-4

MISSISSAUGA
Planning and Building

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T&W, Geomatics



BLOOR STREET EAST

DIXIE ROAD

APARTMENT BUILDING

Limit of Underground Parking Garage

Town House Property Line

1.8 M Acoustical Fence

NEW ZONE OF PARKING

5-M Green Buffer Area

COURTYARDS

STREET TOWNHOUSES

TYNEBURN CRESCENT

Revised Drawings
Jan. 19, 2012

Proposed Densification Project
1440 Bloor Street East, Mississauga, Ontario
Tapes Investment c/o Berkley Property Management Inc.

Peter Favot Architect Ltd. Architects & Urban Planners
Toronto, Ontario (905) 305-0655

CONCEPT PLAN

APPENDIX I-5



PROPOSED CONCEPT PLAN RESIDENTIAL DENSIFICATION

Scale 1:400

1440 Bloor Street East Mississauga, Ontario
Tapes Investment c/o Berkley Property Management Inc.

Existing

Official Plan- High Density - 1
Zoning- RA-2 (Exception-40)
RA-2 Permitted FSI - 0.8 to 1.2
Exception-40- Max for Apartment Dwelling Zone= FSI- 0.8

Proposed

To sever part of land that fronts onto Tyneburn Crescent for a
Rezoning to RM5 (Street Townhouse Dwelling Zone)

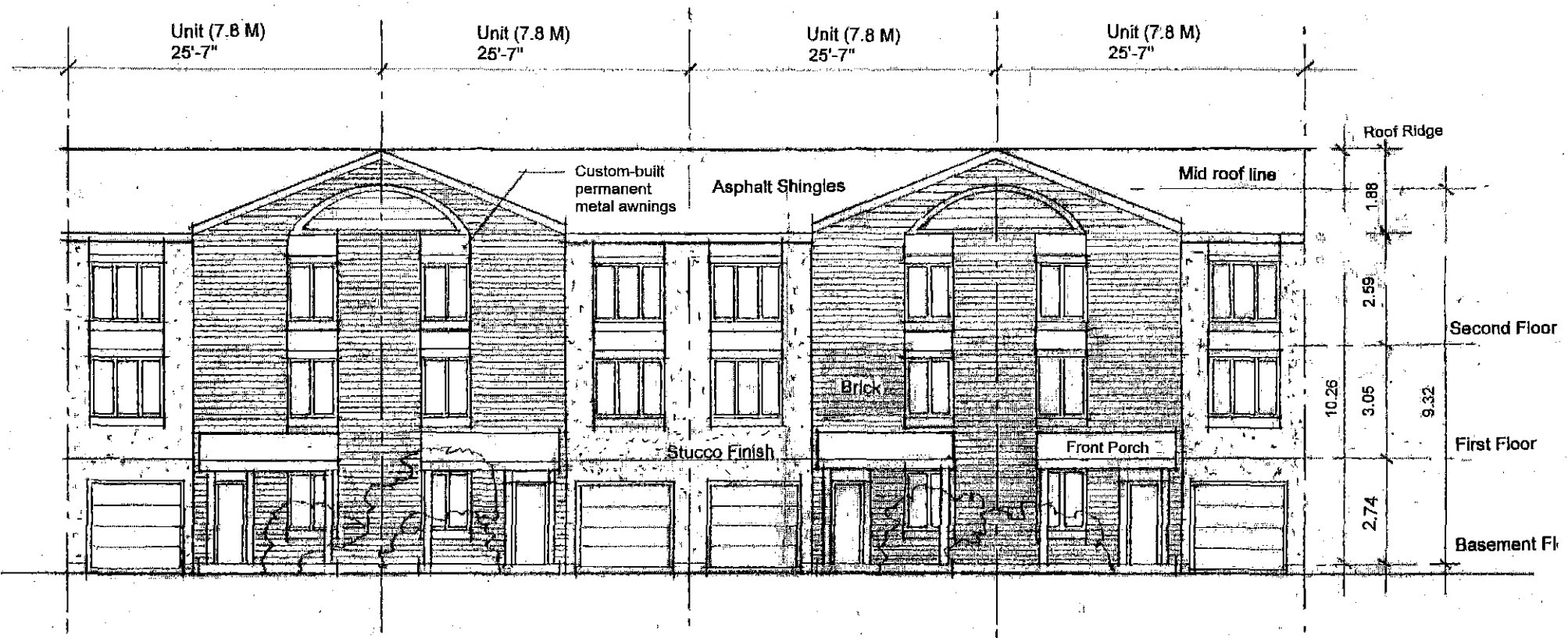
Total number of Apartment units is unchanged - 93 units

Existing Parking- Underground Parking - 42 cars
Surface Parking - **108 cars**
Total Parking - **150 cars**

Proposed Parking- No changes to 150 total, only a reconfiguration
of surface parking

Land Area- 121,840 sf (11319M2) 1.132 HA

Notes for additional data refer to separate data sheets

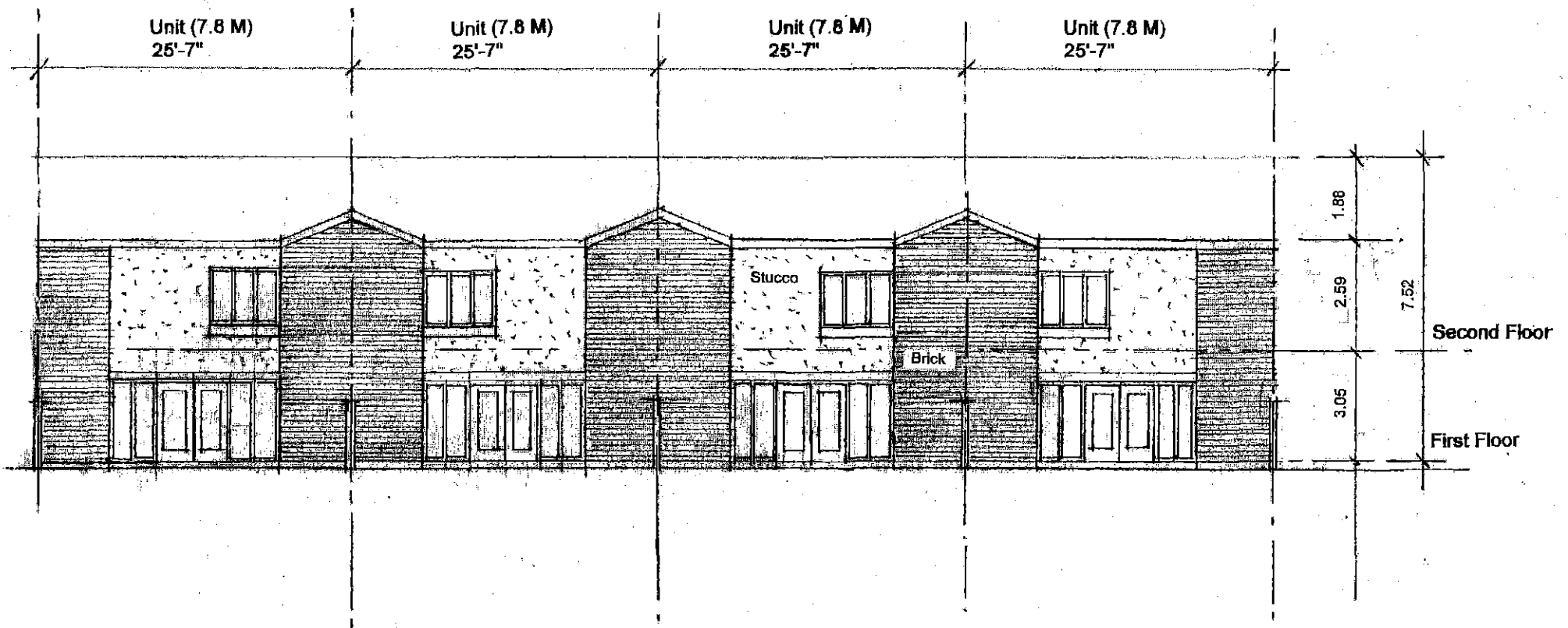


South Elevation of Street Townhousing
scale 1:

Proposed Densification Project
1440 Bloor Street East, Mississauga, Ontario
Tapes Investment c/o Berkley Property Management Inc.
Peter Favot Architect Ltd. Architects & Urban Planners
Toronto, Ontario (905) 305-0655 July 2011

Revised Drawings
Jan. 19, 2012



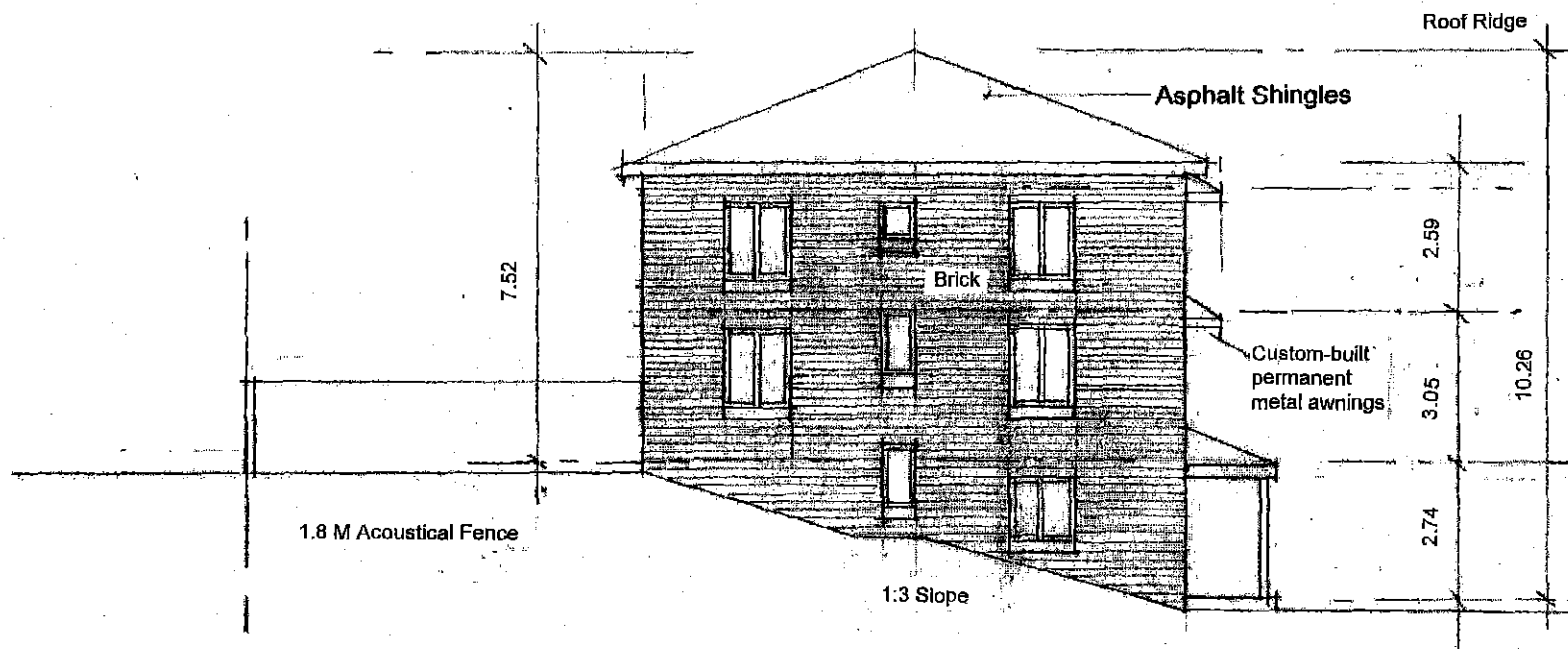


North Elevation of Street Townhousing
Scale 1:



Proposed Densification Project
1440 Bloor Street East, Mississauga, Ontario
Tapes Investment c/o Berkley Property Management Inc.
 Peter Favot Architect Ltd. Architects & Urban Planners
 Toronto, Ontario (905) 305-0655 July 2011

Revis Revised Drawings
 Jan. 19, 2012



East & West End Elevations of Street Townhousing
Scale 1:



Proposed Densification Project
1440 Bloor Street East, Mississauga, Ontario
Tapes Investment c/o Berkley Property Management Inc.
 Peter Favot Architect Ltd. Architects & Urban Planners
 Toronto, Ontario (905) 305-0655 July 2011

Revised Drawings
 Jan. 19, 2012

Tapes Investments**File: OZ 11/012 W3****Agency Comments**

The following is a summary of comments from agencies and departments regarding the application.

Agency / Comment Date	Comment
<p>Region of Peel (January 3, 2012)</p>	<p>Regional Planning staff have reviewed the noted application and provide the following comments:</p> <p>There is a 150 mm (6") diameter watermain and a 250 mm (10") diameter sanitary sewer on Tyneburn Crescent.</p> <p>The Region of Peel has received Functional Servicing Reports (FSR) prepared by Johnson Sustronk Weinstein + Associates, dated August 2011. The copies of the FSRs were sent to the Water and Wastewater division for review. A satisfactory report will not be a condition of site approval.</p> <p>At site plan stage, site servicing drawings will be required and there is a First Submission Application fee of \$150 as per fee by-law 7-2011 (all connections must conform to Regional standards and specifications and design criteria). Site servicing approvals will be required prior to building permit.</p> <p>Curbside collection will be provided by the Region of Peel.</p> <p>Once the Draft Reference Plan has been submitted, the Region would like to be circulated for comment.</p>
<p>Dufferin-Peel Catholic District School Board and the Peel District School Board (October 25, 2011 and November 15, 2011)</p>	<p>In comments, dated October 25, 2011 and November 15, 2011, both School Boards responded that they are satisfied with the current provision of educational facilities for the catchment area and, as such, the school accommodation condition as required by City of Mississauga Council Resolution 152-98 pertaining to satisfactory arrangements regarding educational facilities need not be applied for this development application.</p>

Tapes Investments

File: OZ 11/012 W3

Agency / Comment Date	Comment
	<p>In addition, if approved, both School Boards require that a clause be placed in any agreement of purchase and sale of residential lots that sufficient accommodation may not be available for all anticipated students in the neighbourhood schools and that some students may be accommodated in temporary facilities or bussed to schools outside of the area.</p>
<p>City Community Services Department - Planning, Development and Business Services Division/Park Planning Section (June 28, 2012)</p>	<p>Burnhamdale Park (P-053) is located approximately 500 m (1,640 ft.) from the subject property and includes a play site, a senior soccer field, a multi pad with a basketball hoop and two tennis courts.</p> <p>In the event that the application is approved, prior to by-law enactment, a cash contribution for street tree planting will be required. Further, prior to the issuance of building permits, cash-in-lieu for park or other public recreational purposes is required pursuant to Section 42 of the <i>Planning Act</i> (R.S.O. 1990, c.P. 13, as amended) and in accordance with City's Policies and By-laws."</p>
<p>City Transportation and Works Department (June 27, 2012)</p>	<p>The Transportation and Works Department confirmed receipt of Concept Plan, Grading Plan, Functional Servicing Report, Environmental Noise Assessment, Parking Plan, and Environmental Site Assessment Phase 1.</p> <p>Prior to the Supplementary Report proceeding, the applicant has been requested to provide revised plans that address certain grading concerns and technical discrepancies, to update the Functional Servicing Report and to submit a servicing plan that illustrates the underground works required in support of the development.</p> <p>The applicant has also been requested to update the Parking Plan to demonstrate sufficient parking within the development and to validate the Environmental Site Assessment Phase 1,</p>

Tapes Investments**File: OZ 11/012 W3**

Agency / Comment Date	Comment
	<p>dated April 4, 2012.</p> <p>Further detailed comments/conditions will be provided prior to the Supplementary Meeting, pending receipt and review of the foregoing.</p>
Other City Departments and External Agencies	<p>The following City Departments and external agencies offered no objection to this application provided that all technical matters are addressed in a satisfactory manner:</p> <p>City Community Services Department - Fire and Emergency Services Division Bell Canada Canada Post Corporation Rogers Cable Enersource Hydro Mississauga</p>
	<p>The following City Department and external agencies were circulated the application but provided no comments:</p> <p>City of Mississauga Realty Services Conseil Scolaire de District Catholique Centre-Sud Conseil Scolaire de District Centre-Sud-Ouest</p>

Tapes Investments

File: OZ 11/012 W3

School Accommodation

The Peel District School Board	The Dufferin-Peel Catholic District School Board
<ul style="list-style-type: none"> Student Yield: <ul style="list-style-type: none"> 1 Kindergarten to Grade 5 1 Grade 6 to Grade 8 1 Grade 9 to Grade 12/OAC School Accommodation: <ul style="list-style-type: none"> Brian W. Fleming P.S. <ul style="list-style-type: none"> Enrolment: 702 Capacity*: 783 Portables: 2 Glenhaven Sr. <ul style="list-style-type: none"> Enrolment: 551 Capacity: 545 Portables: 1 Glenforest S.S. <ul style="list-style-type: none"> Enrolment: 1,404 Capacity: 1,023 Portables: 12 <p>* Note: Capacity reflects the Ministry of Education rated capacity, not the Board rated capacity, resulting in the requirement of portables.</p>	<ul style="list-style-type: none"> Student Yield: <ul style="list-style-type: none"> 1 Junior Kindergarten to Grade 8 1 Grade 9 to Grade 12/OAC School Accommodation: <ul style="list-style-type: none"> St. Alfred <ul style="list-style-type: none"> Enrolment: 453 Capacity: 426 Portables: 6 Philip Pocock <ul style="list-style-type: none"> Enrolment: 1,390 Capacity: 1,257 Portables: 5

Tapes Investments

File: OZ 11/012 W3

Official Plan Policies

3.2.3.1

Residential lands will be developed to achieve a compact, orderly urban form generally characterized by lower densities in the interior of communities and higher densities along major roads and near concentrations of retail commercial, community, and transportation facilities.

3.2.3.2

High quality and innovative residential design will be promoted in a form which reinforces and enhances the local community character, respects its immediate context and creates a quality living environment. Innovative housing types and zoning standards will be encouraged. Design issues related to built form, scale, massing, orientation, parking, overshadowing, and the quantity and quality of open space will be priorities in assessing the merits of residential development. Broader urban design issues related to the creation of an urban street character, developing a sense of gateway into a community and highlighting district focal points will also be considered in assessing residential development.

3.13.5.2

Residential intensification outside intensification areas will generally occur through infilling.

3.13.5.3

Intensification outside intensification areas may be considered where the proposed development is compatible in built form and scale to surrounding development, enhances the existing or planned development and is consistent with the policies of this Plan.

3.13.6.17

Development proposals will demonstrate compatibility and integration with surrounding land uses by ensuring that an effective transition in built form is provided between areas of different development densities and scale. Transition in built form will act as a buffer between the proposed development and planned uses, and should be provided through appropriate height, massing, character, architectural design, siting, setbacks, parking, and public and private open space and amenity space.

3.13.6.20

Development should be located on public roads.

Tapes Investments

File: OZ 11/012 W3

3.18.2.6

Building, landscaping and site design will minimize the effects of noise, unattractive views, other negative impacts and will buffer adjacent land uses.

3.18.4.1

Areas of high and medium density residential buildings should manifest a broad range of building types and forms.

3.18.5.2

Buildings should address the street with main entrances facing the street, with strong pedestrian connections and landscape treatments that connect buildings to the street.

3.18.7.3

Building and site designs should create a sense of enclosure, pedestrian scale and identity. Enclosure means having built form along the street edge with heights appropriate to its context.

3.18.9.4

Building height and site design will create a gradual scale transition to adjacent buildings.

Tapes Investments

File: OZ 11/012 W3

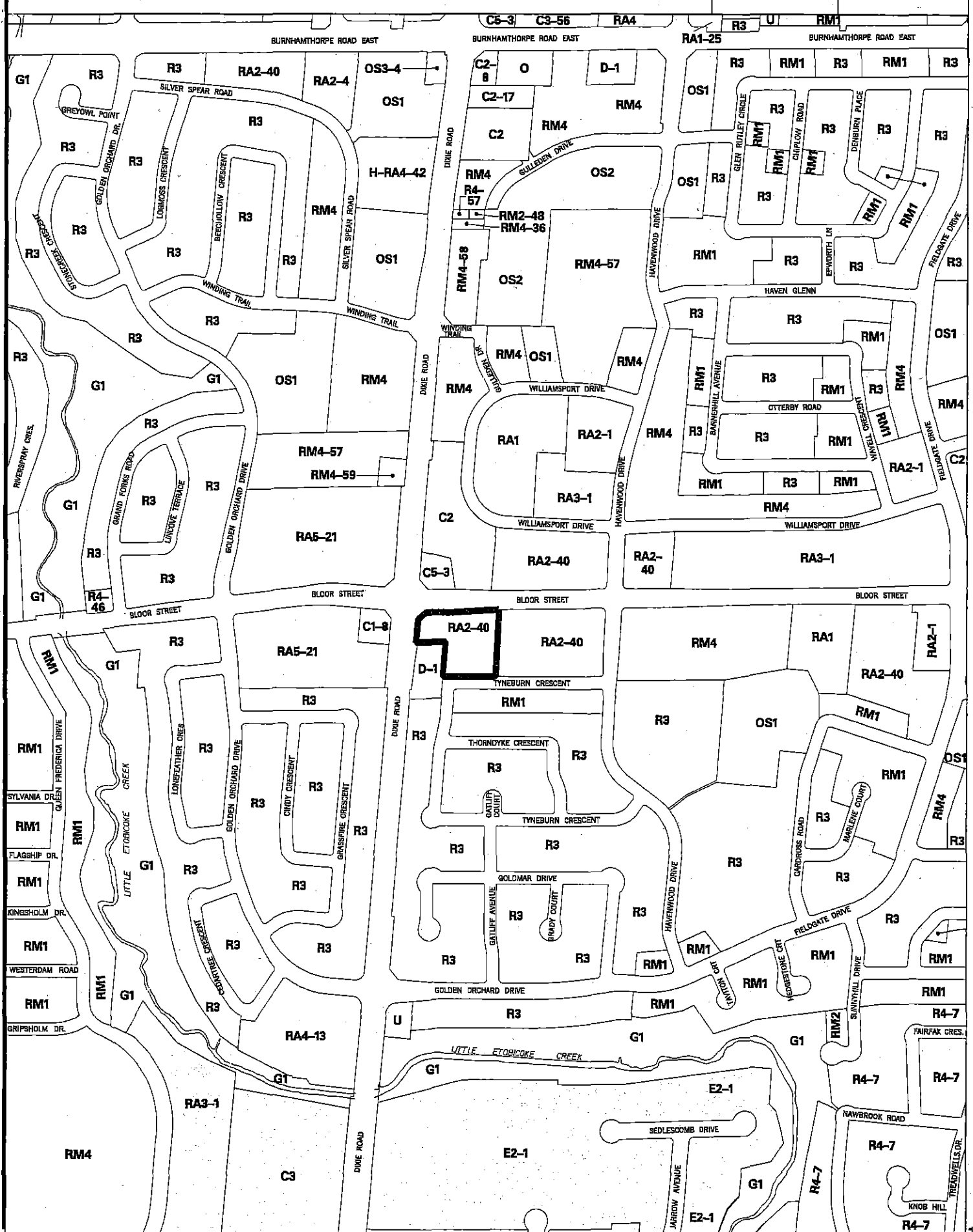
Proposed Zoning Standards**Proposal for Ten (10) Street Townhouses fronting Tyneburn Crescent**

	RM5 Zone	Proposed RM5-Exceptions	RM1 Zone (existing Semi- Detached Dwelling zone south side of Tyneburn Crescent)
Permitted Use	Street Townhouse Dwellings		Semi-Detached Dwellings and Detached Dwellings
Min. Lot Area (Interior)	200 m ² (2,153 sq. ft.)	<i>191 m² (2,056 sq. ft.)</i>	340 m ² (3,660 sq. ft.)
Min. Lot Frontage (Interior)	6.8 m (22.3 ft.)	<i>7.8 m (25.6 ft.)</i>	9 m (29.5 ft.)
Min. Front Yard	4.5 m (14.8 ft.)	<i>6.3 m (21.3 ft.)</i>	6.0 m (19.7 ft.)
Min. setback to Garage Face	6.0 m (19.7 ft.)	<i>6.5 m (21.3 ft.)</i>	6.0 m (19.7 ft.)
Min. Interior Side Yard	0.0 m (0.0 ft.) on attached side and 1.5 m (4.9 ft.) on unattached side		0.0 m (0.0 ft.) on attached side and 1.8 m (5.9 ft.) on unattached side
Min. Rear Yard	7.5 m (24.6 ft.)		7.5 m (24.6 ft.)
Max. Height	10.7 m (35.1 ft.)		10.7 m (35.1 ft.)
Min. Landscaped Area	25% of the lot area		25% of the lot area
Max. Gross Floor Area	0.75 times the lot area	<i>1.15 times the lot area</i>	n/a

Proposed Retained Apartment lands – with existing 8 storey apartment building

	Existing RA2-40 Zone	Proposed RA2-Exceptions	RA2 Base Zone
Minimum Floor Space Index	0.5	<i>0.5</i>	0.5
Maximum Floor Space Index	0.8	<i>0.94</i>	1.0

APPENDIX I-11



Tapes Investments

File: OZ 11/012 W3

Mississauga Official Plan Policies

16.1.2.5

Proposals for additional development on lands with existing apartment buildings will be subject to the following, in addition to other policies regarding medium and high density residential development in this Plan:

- a. on lands designated Residential High Density, development in addition to existing buildings will be restricted to uses permitted in the Residential Medium Density designation; and
- b. as a condition of development, the site in its entirety must meet current site plan and landscaping requirements, and existing buildings must meet current building code, fire code and property standards.

16.2.2.1

For Medium and High Density Development, new development should not exceed the height of any existing buildings on the property, and should be further limited in height so as to form a gradual transition in massing when located adjacent to low density residential development. Buildings immediately adjacent to low density housing forms should be limited to three storeys. In situations where the low density housing forms are separated from the high density development by a public road, park, utility corridor or other permanent open space feature, four to five storeys may be compatible.

5.3.5.5

Intensification within Neighbourhoods may be considered where the proposed development is compatible in built form and scale to surrounding development, enhances the existing or planned development and is consistent with the policies of this Plan.

5.3.5.6

Development will be sensitive to the existing and planned context and will include appropriate transitions in use, built form, density and scale.

9.1.3

Infill and redevelopment within Neighbourhoods will respect the existing and planned character.

Tapes Investments

File: OZ 11/012 W3

Non-Intensification Areas

9.2.2.1

Heights in excess of four storeys will be required to demonstrate that an appropriate transition in height and built form that respects the surrounding context will be achieved.

9.2.2.4

While new development need not mirror existing development, new development in Neighbourhoods will:

- a. respect existing lotting patterns;
- b. respect the continuity of front, rear and side yard setbacks;
- c. respect the scale and character of the surrounding area;
- d. minimize overshadowing and overlook on adjacent neighbours;
- e. incorporate stormwater best management practices;
- f. preserve mature high quality trees and ensure replacement of the tree canopy; and
- g. be designed to respect the existing scale, massing, character and grades of the surrounding area.

9.3.1.11

Reverse frontage lots will not be permitted, except for infill development where a street pattern has already been established.

9.5.1.2

Developments should be compatible and provide appropriate transition to existing and planned development by having regard for the following elements:

- a. Natural Areas System;
- b. natural hazards (flooding and erosion);
- c. natural and cultural heritage features;
- d. street and block patterns;
- e. the size and configuration of properties along a street, including lot frontages and areas;
- f. continuity and enhancement of **streetscapes**;
- g. the size and distribution of building mass and height;
- h. front, side and rear yards;

Tapes Investments

File: OZ 11/012 W3

- i. the orientation of buildings, structures and landscapes on a property;
- j. views, sunlight and wind conditions;
- k. the local vernacular and architectural character as represented by the rhythm, textures and building materials;
- l. privacy and overlook; and
- m. the function and use of buildings, structures and landscapes.

9.5.1.3

Site designs and buildings will create a sense of enclosure along the street edge with heights appropriate to the surrounding context.

9.5.4.1

Development proposals should enhance public streets and the open space system by creating a desirable street edge condition.

9.5.4.2

An attractive and comfortable public realm will be created through the use of landscaping, the screening of unattractive views, protection from the elements, as well as the buffering of parking, loading and storage areas.

9.5.6.1

Site layout, buildings and landscaping will be designed to promote natural surveillance and personal safety.

9.5.6.2

Active building frontages should be designed to face public spaces including entries and windows to ensure natural surveillance opportunities.

Tapes Investments

File: OZ 11/012 W3

Recommendation PDC-0051-2012

PDC-0051-2012

- "1. That the Report dated August 14, 2012, from the Commissioner of Planning and Building regarding the application to change the Zoning from "RA2-40" (Apartment Dwellings) to "RM5-Exception" (Street Townhouse Dwellings) and "RA2-Exception" (Apartment Dwellings) to permit ten (10) street townhouse dwellings fronting onto Tyneburn Crescent and maintain the existing apartment building under file OZ 11/012 W3, Tapes Investment, 1440 Bloor Street, southeast corner of Bloor Street and Dixie Road, be received for information.
2. That the following correspondences expressing concerns with respect to file OZ 11/12 W3 be received:
 - a. Email and attachment dated August 20, 2012 from Jugal Ghosh
 - b. Email dated August 31, 2012 from Robert and Margaret Eagleson
 - c. Email dated September 1, 2012 from Liem Tran
 - d. Email and attachment dated September 4, 2012 from Jugal Ghosh
 - e. Email dated September 4, 2012 from Teresa Barranca."



Corporate Report



Originator's
Files OZ 09/011 W5
T-M09004 W5

DATE: March 12, 2013

TO: Chair and Members of Planning and Development Committee
Meeting Date: April 2, 2013

FROM: Edward R. Sajecki
Commissioner of Planning and Building

SUBJECT: **Official Plan Amendment, Rezoning and Draft Plan of
Subdivision Applications**
**To permit apartments with ground related commercial uses,
and townhouses under standard and common element
condominium tenure**
5081 Hurontario Street
East side of Hurontario Street, north of Eglinton Avenue East
Owner: Summit Eglinton Inc.
Applicant: Jim Lethbridge, Lethbridge and Lawson Inc.
Bill 51

Addendum Supplementary Report **Ward 5**

RECOMMENDATION: That the Report dated March 12, 2013, from the Commissioner of Planning and Building recommending approval of the applications under Files OZ 09/011 W5 and T-M09004 W5, Summit Eglinton Inc., 5081 Hurontario Street, east side of Hurontario Street, north of Eglinton Avenue East, originally endorsed by Council on April 25, 2012, be adopted in accordance with the following:

1. That notwithstanding that subsequent to the public meeting, changes to the applications have been proposed, Council considers that the changes do not require further notice and, therefore, pursuant to the provisions of subsection 34(17) of

the *Planning Act*, R.S.O. 1990, c.P.13, as amended, any further notice regarding the proposed amendment is hereby waived.

2. That the application to change the Zoning from "D" (Development) to "RA2 - Exception" (Apartment Dwellings) and "RM5 - Exception" (Townhouse and Semi-Detached Dwellings) on a portion of the subject property described in recommendations of the report dated March 27, 2012 except for the two (2) zoning performance standards concerning townhouse and semi-detached dwellings and the request to not require a holding zone provision on Block 49 (Apartment Dwellings) of the proposed Draft Plan of Subdivision (see Appendix AS-4) described in this report, be approved subject to the applicant agreeing to satisfy all the requirements of the City and any other official agency concerned with the development.

BACKGROUND:

On April 25, 2012 City Council approved Recommendation PDC-0030-2012 to approve the subject Official Plan Amendment, Rezoning and Draft Plan of Subdivision applications under Files OZ 09/011 W5 and T-M09004 W5.

COMMENTS:

On February 22, 2013, the planning consultant representing Summit Eglinton Inc. submitted a request to:

- Increase the lot coverage of the proposed semi-detached dwellings from 45% to 48%;
- Decrease the interior side yard setback from 1.2 m (4 ft.) to 0.91 m (3 ft.) for the proposed townhouse dwellings; and
- To not require a holding zone provision for the proposed apartment dwellings on Block 49 of the Draft Plan of Subdivision (see Appendix AS-4).

The Planning and Building Department has reviewed the detailed development concepts for the proposed semi-detached and townhouse dwellings and have no concerns with these requests. It should be noted that similar zone standards have been utilized in

other semi-detached and townhouse exception zones throughout the City. In comments dated February 12, 2013, the Region of Peel advised that there is sufficient servicing capacity to accommodate the 246 apartment dwelling units on Block 49 of the Draft Plan of Subdivision. Therefore, there is no reason to impose a holding zone provision on the proposed RA2 - Exception Zone (Apartment Dwellings).

CONCLUSION:

In accordance with subsection 34(17) of the *Planning Act*, R.S.O. 1990, c.P. 13, as amended, Council is given authority to determine if further public notice is required. Since the request by the applicant is only to modify two (2) zoning performance standards and to not require a holding zone provision on one (1) apartment block, it is recommended that no further public meeting need to be held regarding the proposed changes.

The proposed zoning standards and the request to not require a holding zone provision on Block 49 of the Draft Plan of Subdivision are acceptable from a planning standpoint and should be approved. The implementing zoning by-law for the development will incorporate the recommended changes.

ATTACHMENTS:

Appendix AS-1:	Recommendation PDC-0030-2012
Appendix AS-2:	Supplementary Report with Attachments S8 to S12
Appendix AS-3:	Existing Land Use Map
Appendix AS-4:	Draft Plan of Subdivision



Edward R. Sajecki
Commissioner of Planning and Building

Prepared By: C. Rouse, Acting Manager, Development North

Summit Eglinton Inc.

Files: OZ 09/011 W5

T-M09004 W5

Recommendation PDC-0030-2012

PDC-0030-2012

- "1. That notwithstanding that subsequent to the public meeting, changes to the applications have been proposed, Council considers that the changes do not require further notice and, therefore, pursuant to the provisions of subsection 34(17) of the Planning Act, R.S.O. 1990, c.P.13, as amended, any further notice regarding the proposed amendment is hereby waived.
2. That the application to amend Mississauga Plan from "Residential - High Density II - Special Site T" to "Residential - Medium Density I - Special Site", "Residential - Medium Density II - Special Site" and "Residential - High Density II - Special Site" to permit apartments with ground related commercial and office uses, semi-detached dwellings and townhouses under standard and common element condominium tenure, be approved.
3. That the application to change the Zoning from "D" (Development) to "D" (Development), "RM5-Exception" (Street Townhouse and Semi-Detached Dwellings), "RM6-Exception" (Townhouse Dwellings on a CEC-Private Road), "H-RA2-Exception" (Apartment Dwellings) and "H-RA5-Exception" (Apartment Dwellings) to permit apartments with ground related commercial and office uses, semi-detached dwellings and townhouses under standard and common element condominium tenure, in accordance with the City supported zoning standards contained in the staff report, be approved subject to the following conditions:
 - (a) That the draft plan of subdivision be approved.
 - (b) That the applicant agree to satisfy all the requirements of the City and any other official agency concerned with the development.
 - (c) That the school accommodation condition as outlined in City of Mississauga Council Resolution 152-98 requiring that satisfactory arrangements regarding the adequate provision and distribution of educational facilities have been made between the developer/applicant and both School Boards not apply to the subject lands.
 - (d) That in accordance with Council Resolution 160-91, that a minimum of three car spaces per dwelling, including those in a garage be required onsite and a minimum of 0.25 on-street visitor parking spaces per dwelling be required for dwellings

Summit Eglinton Inc.

Files: OZ 09/011 W5

T-M09004 W5

on lots less than 12 m (39.4 ft.) of frontage for the subject development.

4. That the Plan of Subdivision under file T-M09004 W5, be recommended for approval subject to the conditions contained in Appendix S-10, as modified to incorporate semi-detached lots.
5. That the decision of Council for approval of the rezoning application be considered null and void, and a new development application be required unless a zoning by-law is passed within 36 months of the Council decision.
6. That Legal Services request the Ontario Municipal Board to make appropriate modifications to the new Mississauga Official Plan through the Board approval process to redesignate the lands from "Residential - High Density II - Special Site 7" to "Residential - Medium Density I - Special Site", "Residential - Medium Density II - Special Site" and "Residential - High Density II - Special Site".
7. That the following correspondence be received:
 - (a) Letter dated April 13, 2012 from Kurt Franklin, Vice - President, Weston Consulting Group Inc.
 - (b) Email dated April 16, 2012 from David Vo, Resident.
 - (c) Email dated April 16, 2012"



Corporate Report

Clerk's Files

Originator's

Files OZ 09/011 W5
T-M09004 W5

PDC APR 16 2012

DATE: March 27, 2012

TO: Chair and Members of Planning and Development Committee
Meeting Date: April 16, 2012

FROM: Edward R. Sajecki
Commissioner of Planning and Building

SUBJECT: **Official Plan Amendment, Rezoning and Draft Plan of Subdivision Applications**
To permit apartments with ground related commercial and office uses, and townhouses under standard and common element condominium tenure
5081 Hurontario Street
East side of Hurontario Street, north of Eglinton Avenue East
Owner: Summit Eglinton Inc.
Applicant: Jim Lethbridge, Lethbridge & Lawson Inc.
Bill 51

Supplementary Report Ward 5

RECOMMENDATION: That the Report dated March 27, 2012, from the Commissioner of Planning and Building recommending approval of the applications under Files OZ 09/011 W5 and T-M09004 W5, Summit Eglinton Inc., 5081 Hurontario Street, east side of Hurontario Street, north of Eglinton Avenue East, be adopted in accordance with the following:

1. That notwithstanding that subsequent to the public meeting, changes to the applications have been proposed, Council considers that the changes do not require further notice and, therefore, pursuant to the provisions of subsection 34(17) of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, any

further notice regarding the proposed amendment is hereby waived.

2. That the application to amend Mississauga Plan from "Residential - High Density II - Special Site 7" to "Residential - Medium Density I - Special Site", "Residential - Medium Density II - Special Site" and "Residential - High Density II - Special Site" to permit apartments with ground related commercial and office uses, and townhouses under standard and common element condominium tenure, be approved.
3. That the application to change the Zoning from "D" (Development) to "D" (Development), "RM5-Exception" (Street Townhouse and Semi-Detached Dwellings), "RM6-Exception" (Townhouse Dwellings on a CEC-Private Road), "H-RA2-Exception" (Apartment Dwellings) and "H-RA5-Exception" (Apartment Dwellings) to permit apartments with ground related commercial and office uses, and townhouses under standard and common element condominium tenure, in accordance with the City supported zoning standards contained in the staff report, be approved subject to the following conditions:
 - (a) That the draft plan of subdivision be approved.
 - (b) That the applicant agree to satisfy all the requirements of the City and any other official agency concerned with the development.
 - (c) That the school accommodation condition as outlined in City of Mississauga Council Resolution 152-98 requiring that satisfactory arrangements regarding the adequate provision and distribution of educational facilities have been made between the developer/applicant and both School Boards not apply to the subject lands.
 - (d) "That in accordance with Council Resolution 160-91, that a minimum of three car spaces per dwelling,

including those in a garage be required on-site and a minimum of 0.25 on-street visitor parking spaces per dwelling be required for dwellings on lots less than 12 m (39.4 ft.) of frontage for the subject development."

4. That the Plan of Subdivision under file T-M09004 W5, be recommended for approval subject to the conditions contained in Appendix S-10, attached to the report dated March 27, 2012, from the Commissioner of Planning and Building.
5. That the decision of Council for approval of the rezoning application be considered null and void, and a new development application be required unless a zoning by-law is passed within 36 months of the Council decision.
6. That Legal Services request the Ontario Municipal Board to make appropriate modifications to the new Mississauga Official Plan through the Board approval process to redesignate the lands from "Residential - High Density II - Special Site 7" to "Residential - Medium Density I - Special Site", "Residential - Medium Density II - Special Site" and "Residential - High Density II - Special Site".

REPORT SUMMARY:

The subject lands in their entirety are already designated for high density apartment uses. The approval of these applications will result in a concentration of density along the Hurontario Street frontage, providing for a stronger, transit supportive presence in that location. The incorporation of townhouses provides for an appropriate and improved transition to existing low density development lands to the north. The recommended cap on dwellings is in keeping with the number of dwellings that is permitted under the existing Official Plan apartment designation, for the entire parcel. The comprehensive review of the development proposal and all the studies that were required of the developer, and the modifications made to the proposal in respect of the Official Plan, has resulted in a development that:

- Strengthens the node;
- Is compatible in built form and scale to surrounding development;
- Supports transit;
- Will enhance both the existing and planned community by providing a sophisticated and well designed urban community that will positively contribute to the City's urban fabric.

To address water servicing issues associated with the proposal, the Region of Peel has recommended a Holding Symbol on all apartment lands until these matters have been resolved. It is expected that capacity in the water system will be available by 2013. The developer has agreed to phasing the development over time, which will allow for the construction of higher order transit to be more closely aligned with the build out of this community.

BACKGROUND:

Information on application background, general chronology, and changes to the development form and unit count are contained in Appendix S-1 and S-2 (Information Report).

COMMENTS:**REVISED DEVELOPMENT PROPOSAL**

The applicant is proposing to develop the lands for the following uses (see Appendix S-4 for a full statistical outline of the proposal and Appendix S-5 and S-9 for revised plan):

- Abutting Hurontario Street (Block 1/50) - Apartment dwellings in three separate towers, ranging in height from 24 storeys to 28 storeys, incorporating a maximum of 1,077 dwellings. Ground related uses include office and retail within buildings fronting onto Hurontario Street and Street C. The proposed Floor Space Index (FSI) is 6.13;
- Mid-block, abutting the southern property line (Block 3/49) - a mid-rise, six storey apartment building, containing a maximum of 246 dwellings at an FSI of 2.44;

- Along the northern property line (Block(s) 2/1-47) - a maximum of 45 three-storey on-street freehold townhouse dwellings and 2 semi-detached dwellings;
- In the southeast area of the lands (Block 4/48) - a maximum of 30 three-storey common element condominium townhouse dwellings (with the opportunity for 4 more with development to the south).

COMMUNITY ISSUES

As noted in Appendix S-1, a Community Meeting was held to advise residents of the proposal. In addition, two letters were received from adjoining landowners. A consolidated response to the comments and concerns that have been raised is attached to this report as Appendix S-12. We draw your attention to comments from the Region of Peel, who have requested that a Holding Symbol be placed on the apartment lands pending the resolution of servicing matters, and Transportation and Works who have responded to traffic concerns.

UPDATED AGENCY AND CITY DEPARTMENT COMMENTS

Updated comments have been received from City Departments and agencies dealing with school accommodation, servicing, traffic, local street network, and the consideration of future light rapid transit (LRT) along the Hurontario Street corridor. The updated comments are contained in Appendix S-6.

PLANNING COMMENTS

Official Plan

The revised proposal addresses Provincial legislation and the policies of both the Region of Peel and City of Mississauga Official Plans. The applicant has also addressed the technical issues, including traffic and land use compatibility through adjustments to the plan and allocation of units. A review of the

proposal against the policies of the Official Plan are summarized below.

Hurontario Node

The lands are located within the Hurontario Node, as established in the Official Plan. Consistent with the policies for nodes, the development proposal provides for a compact, mixed use and transit supportive development. The proposed limits on building height, FSI and dwellings will ensure that the node does not rival the downtown core.

Land Use Designations

To implement the proposal, the following amendments to the Mississauga Plan Policies for the Hurontario District are required (see Appendix S-7):

- To permit the townhouses, redesignate the lands from "Residential - High Density II - Special Site 7" to "Residential - Medium Density I - Special Site" (common element condominium townhouses) and "Residential - Medium Density II - Special Site" (on-street townhouse and semi-detached dwellings);
- For the apartment block abutting Hurontario Street, to amend the permitted FSI within a "Residential - High Density II - Special Site" designation from 2.9 to a maximum of 6.13;
- To limit the total number of apartment dwellings permitted to a maximum of 1,323 dwellings;
- To allow for a minimum of 2,750 m² (29,601 sq. ft.) and a maximum of 6,300 m² (67,815 sq. ft.) of retail commercial and office space, contained within the first three floors of the apartment buildings block fronting Hurontario Street and Street C.

The use of the lands for a high density residential purposes has already been established in the Official Plan. The transfer of

density to the Hurontario frontage, and the inclusion of townhouses within the development, allows for:

- A more appropriate concentration of density that provides for a stronger design based and transit supportive presence along Hurontario Street;
- A appropriate transition to existing lower density development lands to the north;
- Compatibility with low rise apartment and townhouse land uses to the east.

The cap on dwellings is in keeping with the number that is currently permitted under the existing Official Plan apartment designation, which is consistent with the findings of the traffic studies. Notwithstanding the transfer of density towards Hurontario Street and the addition of townhouses, the lands overall retain the maximum FSI of 2.9 that currently applies. An amendment to the permitted FSI, specific to the apartment block fronting Hurontario Street, is attributable to the development being on public roads (a more desirable condition) rather than private roads, which impacts the final FSI calculations. Minimum floor areas for retail commercial and office space are in keeping with Official Plan goals regarding mixed use communities and achieving residents and jobs density targets (people plus jobs calculations).

Concept Plan

Appendix S-2 provides a summary of the necessity for and details regarding a concept plan for the development (see page 5 under designation description and page 9). The plan has been amended to reflect the following:

- Satisfactory temporary access arrangement to Hurontario Street for Street C, as it connects with Street A;
- An interim condition for the extension of Street C (referred to on the plan as Block 57) as a one way road, pending

development to the south. This will permit development on Block 3/49;

- Revisions to Block 4/48 that will provide for the opportunity of an acceptable continuation of development on the remnant parcel at a future date, taking into account the proper alignment of a future extension of Street C.

In summary, as it applies to the review of the Special Site 7 requirements of the Official Plan, the land uses proposed are compatible with both existing and proposed surrounding land uses, and the proposed road fabric provides for acceptable ingress and egress to the roads identified.

Urban Design Policies

The following are specific design elements that demonstrate how the development is in keeping with the urban design policies of the Official Plan, summarized in Appendix S-2.

- An appropriate distribution of heights to allow the tallest buildings to front Hurontario Street, with a stepping down of heights and building scale towards existing residential development;
- An interconnected system of public roads that provides for efficient permeability and connectivity for pedestrians, cyclists and vehicles to the existing road network, and to transit service;
- Complementary zoning that provides for an appropriate transition in height and scale and allows for front building elevations and functional front entrances to address the streets (see Zoning section for details);
- Inclusion of ground floor retail and office uses, in appropriate locations, to animate the street and support transit and pedestrian activity;
- Provision for appropriate soil depth above parking structures to facilitate the growth of vegetation to maturity;

- For the high density apartments, limited surface parking and access to underground parking and service areas, which will occur mainly from a private service lane.

Transit Supportive Development

The subject lands are well situated to take advantage of a number of transit initiatives, while the road layout promotes improved access to transit services. The major transit initiative that the development will support is the proposed Light Rapid Transit (LRT) line along Hurontario Street. The Hurontario/Main Street Corridor Master Plan was approved by Council on July 7, 2010, identifying LRT as the recommended transit solution for Hurontario Street. Within the Master Plan, the subject lands are located within the Eglinton-Bristol Character Area with the nearest LRT station stop being at the intersection of Eglinton Avenue and Hurontario Street.

The City has selected a consultant team to undertake the Preliminary Design and Transit Project Assessment Process (TPAP). This work is anticipated to be complete by 2013 and will identify any additional related impacts on the subject lands. These impacts may include the need for additional land to facilitate the LRT and associated station or ancillary system requirements. Staff are, therefore, recommending that the apartment block directly abutting Hurontario Street be placed in a Holding Zone until the study has been completed (see Transportation & Works comments in Appendix S-6 and Zoning Section for details).

Criteria for Specific Official Plan Amendment Applications

The Information Report references the Mississauga Plan policies, provisions and criteria for evaluating site specific Official Plan Amendments (see page 7, Appendix S-2). Summarized below is how the proposed applications address the intent of the criteria.

Will the proposal adversely impact or destabilize the goals and objectives of the Official Plan?

As noted above, the proposal meets the goals and objectives for the Hurontario Node and the land use policies of the Official Plan. Portions of lands to the north and south are designated for high density uses but remain undeveloped. Staff are in receipt of an acceptable concept plan as per the Official Plan requirements. Approval will not adversely impact the development and functioning of these lands.

Are the lands suitable for the proposed uses, and are the land uses compatible with the surrounding lands?

The proposed development is consistent with the land use designation and policies of the Official Plan. Consideration was given for the overall massing and scale of the proposed built form, to integrate and relate appropriately with surrounding development, and the compatible use of Hurontario Street for transit usage.

Is there adequate infrastructure and community services to support the development?

The submission of technical studies in support of the applications have confirmed that the development will have limited impacts from an environmental, noise and servicing perspective. The Region of Peel has requested that a Holding Symbol be placed on all apartment lands pending the availability of adequate water and wastewater servicing capacity, which will be addressed through upcoming studies and scheduled construction programs. Matters regarding the impact of traffic are reported in the Transportation and Works Department section of Appendix S-6. Adequate levels of community services, including parks, community centres and libraries, exist in the surrounding community.

New Mississauga Official Plan

Mississauga Official Plan (2011) was adopted by City Council on September 29, 2010 and partially approved by the Region on September 22, 2011. Mississauga Official Plan (2011) has been appealed in its entirety and, as such, the existing Mississauga Plan

(2003) remains in effect. While the existing Official Plan is the plan of record against which the applications are being reviewed, regard should also be given to the new Mississauga Official Plan. Under the new Mississauga Official Plan, the subject lands are designated "Residential - High Density - Special Site". The proposed townhouse and apartment development does not conform to the land use designation contained in the new Mississauga Official Plan and associated policies, as it relates to land use and proposed density. The new Mississauga Official Plan would need to be modified to redesignate the lands to "Residential - Medium Density" and "Residential - High Density - Special Site".

The timing of the approval of the proposed site specific Official Plan Amendment may be affected by the resolution of the appeals to the new Mississauga Official Plan and any potential appeals. Accordingly, public notice under the *Planning Act* has been provided for this meeting to consider the recommendations contained in this report. Furthermore, should these applications be approved by City Council through the adoption of a site specific Official Plan Amendment to the existing Official Plan, the Ontario Municipal Board will be requested to incorporate the appropriate modifications into the new Mississauga Official Plan prior to its approval.

The proposal is in general keeping with the goals, objectives, and policies of the new Mississauga Official Plan. The one exception is the requirement in Major Nodes for a maximum building height of 25 storeys. Staff have no objection to an increase to 28 storeys, as requested by the applicant. This height limit is consistent with both the existing built form (where several buildings to the south are higher), and for what is proposed to the west in the Pinnacle development, where a height of 34 storeys has been endorsed by Council.

Zoning

The zone categories proposed for the lands are "RM5-Exception" (Street Townhouse and Semi-Detached Dwellings), "RM6-Exception" (Townhouse Dwellings on a CEC-Private Road),

"H-RA2-Exception" (Apartment Dwellings), "H-RA5-Exception" (Apartment Dwellings) and "D" (Development) (which only applies to a small remnant parcel at the southeast corner of the property). These zone categories are appropriate to accommodate the proposed development. A draft by-law prepared by staff is attached as Appendix S-8 which provides greater detail (this document may require other provisions to be consistent with Council direction and concept plans). Key elements in this document include the following:

- A range of compatible retail and office uses that will function within the first three floors of the apartment building blocks, fronting Hurontario Street and Street C;
- Caps on maximums for apartment Floor Space Index, number of apartment dwelling units, building and podium heights, and for retail and office space;
- Minimum setbacks, streetwalls and build-to-lines to provide for an appropriate relationship of the building to the street line, while prohibiting parking and laneways between the building face and street;
- Usable front doors on to Hurontario Street;
- Minimum landscape requirements;
- Parking requirements, in keeping with staff recommendations on a submitted parking study;
- Holding Symbol provisions, as discussed in the report.

On-street Townhouse Frontage Requirement

The applicant has proposed that the minimum lot frontage for a "RM5-Exception" (Street Townhouse Dwellings) zone be reduced from 6.8 m (22.3 ft.) to 5.2 m (17.1 ft.). Staff are not in favour of the decrease, and recommend that the standard not be reduced below 6.0 m (19.6 ft.). Reasons for opposition are that such development will:

- Result in a street frontage that is dominated by garage doors that is not properly balanced by other building and design features including front doors and at grade windows;

- Increase the percentage of hard surface (which cannot be adjusted to accommodate minimum driveway widths) versus an appropriate amount of green space, which in turn restricts the ability for trees to mature.

Phasing

The applicant has advised that development will be phased over time, commencing first with the townhouses and then the apartments. The Development Agreement and Site Plan Agreement (if applicable) will contain the necessary provisions regarding phasing including timing, servicing and interim conditions.

Draft Plan of Subdivision and Development Agreement

The proposed plan of subdivision is acceptable subject to certain conditions (see Appendix S-9 and S-10), and amendment to reflect the staff recommendation of 6.0 m (19.7 ft.) townhouses along the north side of Street B. The proposed road network will create several new road connections between existing roads in the area, and contribute to a framework of roads that will assist in servicing the northeast quadrant of Eglinton Avenue and Hurontario Street. Development will be subject to the completion of services and registration of the plan. In addition, both Servicing and Development Agreements will be required. Matters that may be incorporated into these agreements include the following:

- Review and certification of plans from a noise perspective;
- Submission of a satisfactory composite utility plan;
- Submission of satisfactory micro-climate and sun shadow studies, specific to each proposed building;
- Submission of plans that reflect satisfactory streetscape master plans, principal street entrances, location of exhaust vents, landscape areas, gateway features where applicable, soil depths and glazing;
- Environmental features, in keeping with the City's Green Development initiatives (see page 7 of Appendix S-2);

- The location and payment for public art, in accordance with City requirements;
- Provisions that speak to the final disposition of the remnant lands that will remain zoned "D" (Development).

Site Plan Approval

To date, only conceptual plans have been provided, to demonstrate a development form and as a basis for drafting implementing zoning. Site Plan approval will be required for all development. To address certain matters, Site Plan Agreements may be required. Items that will be considered through Site Plan Approval include the following:

- Building design, massing and materials, in particular the relationship of any structure to Hurontario Street;
- Appropriate landscaping and associated environmental features and green standards;
- Design and location of parking and loading areas, vehicular access points, and pedestrian connections;
- Building orientation and entrance location, for purposes of ensuring compliance with emergency services requirements;
- Implementation of the recommendations of the Wind Study. Revised shadow studies in accordance with the City's Revised Standards for Shadow Studies recently adopted by Council will be required of the applicant in advance of the zoning by-law moving being considered by Council.

FINANCIAL IMPACT:

Development charges will be payable in keeping with the requirements of the applicable Development Charges By-law of the City as well as financial requirements of any other official agency concerned with the development of the lands.

CONCLUSION:

In accordance with subsection 34(17) of the *Planning Act*, R.S.O. 1990, c.P. 13, as amended, Council is given authority to determine if further public notice is required. The applicant has requested to alter the development form proposed for the lands from that originally viewed at the Public Meeting. Staff are recommending that no further public meeting need be held regarding the proposed changes.

The proposed Official Plan Amendment, rezoning and draft plan of subdivision are acceptable from a planning standpoint and should be approved for the following reasons:

1. The proposal to permit townhouse and semi-detached, apartment, office and commercial development is compatible with the surrounding land uses, for reasons as outlined in the report.
2. The proposed Official Plan and zoning standards, as identified in the report, are appropriate to accommodate the requested uses for the lands.

ATTACHMENTS:


Appendix S-1: Application Background Information
Appendix S-2: Information Report
Appendix S-3: Recommendation PDC-0024-2010
Appendix S-4: Revised Application Development Statistics
Appendix S-5: Revised Concept Plan
Appendix S-6: Updated Department and Agency Comments
Appendix S-7: Revised Excerpt of Existing Land Use Map
Appendix S-8: Draft Zoning By-law
Appendix S-9: Revised Draft Plan of Subdivision
Appendix S-10: Conditions of Draft Plan Approval

Appendix S-11: Revised School Board Accommodation
Appendix S-12: Community Comment and Concerns Response



Edward R. Sajecki
Commissioner of Planning and Building

Prepared By: Robert Hughes, Development Planner

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A by-law to amend By-law Number 0225-2007, as amended.

WHEREAS pursuant to section 34 of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, the council of a local municipality may pass a zoning by-law;

NOW THEREFORE the Council of The Corporation of the City of Mississauga ENACTS as follows:

1. By-law Number 0225-2007, as amended, being a City of Mississauga Zoning By-law, is amended by adding the following Exception Table:

4.11.2.53	Exception: RM5-53	Map # 46W	By-law
In a RM5-53 zone the permitted uses and applicable regulations shall be as specified for a RM5 zone except that the following uses/regulations shall apply:			
Additional Permitted Use			
4.11.2.53.1	(1)	Semi-Detached Dwelling	
Regulations			
4.11.2.53.2	Street Townhouse Dwelling:		
(1)	Minimum lot area - interior lot	162 m ²	
(2)	Minimum lot area - corner lot	255 m ²	
(3)	Minimum lot frontage - interior lot	6.0 m	
(4)	Maximum gross floor area - residential	1.2 times the lot area	
(5)	Maximum projection of a porch or a deck, exceeding 0.61 m in height above grade at any point, from the rear wall of a dwelling	0.0 m	
(6)	Article 4.1.5.2 and/or Article 4.1.5.7 of this By-law shall apply to a porch or deck 0.61 m or less in height above grade at any point		
(7)	Maximum projection of a balcony from the rear wall of a dwelling	0.0 m	

4.11.2.53	Exception RM5-53	Map # 36W	By-law
4.11.2.53.3	A semi-detached dwelling shall comply with the RM2 zone regulations contained in Subsection 4.8.1 of this By-law except that:		
(1)	Minimum lot area - interior lot	162 m ²	
(2)	Minimum lot frontage - interior lot	6.0 m	
(3)	Maximum projection of a porch or a deck , exceeding 0.61 m in height above grade at any point, from the rear wall of a dwelling	0.0 m	
(4)	Article 4.1.5.2 and/or Article 4.1.5.7 of this By-law shall apply to a porch or deck 0.61 m or less in height above grade at any point		
(5)	Maximum projection of a balcony from the rear wall of a dwelling	0.0 m	
(6)	Maximum number of semi detached dwellings	2	

2. By-law Number 0225-2007, as amended, is further amended by adding the following Exception Table:

4.12.2.13	Exception RM6-13	Map # 36W	By-law
In a RM6-13 zone the permitted uses and applicable regulations shall be as specified for a RM6 zone except that the following uses/regulations shall apply:			
Regulations			
4.12.2.13.1	Maximum number of dwelling units on all lands zoned RM6-13	30	
4.12.2.13.2	Maximum height	13.0 m	
4.12.2.13.3	Trailer and recreational vehicle parking shall not be permitted		
4.12.2.13.4	All site development plans shall comply with Schedule RM6-13 of this Exception (See concept plan of April 16, 2012 Supplementary Report for details)		

3. By-law Number 0225-2007, as amended, is further amended by adding the following Exception Table:

4.15.3.55	Exception RA2-55	Map # 36W	By-law
In a RA2-55 zone the permitted uses and applicable regulations shall be as specified for a RA2 zone except that the following uses/regulations shall apply:			
Regulations			
4.15.3.55.1	The southerly lot line shall be deemed to be the front lot line		
4.15.3.55.2	Maximum floor space index - apartment dwelling zone	2.44	
4.15.3.55.3	Maximum number of dwelling units on all lands zoned RA2-55	246	
4.15.3.55.4	Minimum front yard	4.5 m	

4.15.3.55	Exception: RA2-55	Map # 36W	By-law
4.15.3.55.5	Minimum exterior side yard		4.5 m
4.15.3.55.6	Minimum rear yard		4.5 m
4.15.3.55.7	Minimum setback from a parking structure completely below finished grade, inclusive of external access stairwells, to a street line		0.0 m
4.15.3.55.8	Minimum landscaped area		30% of lot area
Holding Provision			
<p>The holding symbol H is to be removed from the whole or any part of the lands zoned H-RA2-55 by further amendment to Map 36W of Schedule B contained in Part 13 of this By-law, upon satisfaction of the following requirements:</p> <p>(1) Confirmation that requirements for municipal servicing (i.e. water and sanitary) have been met to the satisfaction of the Region of Peel.</p>			

4. By-law Number 0225-2007, as amended, is further amended by adding the following Exception Table:

4.15.6.44	Exception: RA5-44	Map # 36W	By-law
In a RA5-44 zone the permitted uses and applicable regulations shall be as specified for a RA5 zone except that the following uses/regulations shall apply:			
Additional Permitted Uses			
4.15.6.44.1	(1) Office (2) Medical Office - Restricted (3) Retail Store (4) Financial Institution (5) Restaurant (6) Take-out Restaurant (7) Personal Service Establishment		
Regulations			
4.15.6.44.2	The provisions of Lines 1.0 and 3.0 in Table 2.1.2.1.1 contained in Article 2.1.2.1, Subsection 2.1.14 and Article 4.1.15.1 of this By-law shall not apply		
4.15.6.44.3	For the purposes of this By-law, all lands zoned RA5-44 shall be considered one (1) lot		
4.15.6.44.4	Maximum number of dwelling units on all lands zoned RA5-44		1,077
4.15.6.44.5	The uses contained in Sentence 4.15.6.44.1 shall only be located within a building, structure or part thereof used for an apartment dwelling, long-term care dwelling, retirement dwelling , or any combination thereof		
4.15.6.44.6	Minimum total gross floor area - non-residential used for uses identified in Sentence 4.15.6.44.1, on all lands zoned RA5-44		2,750 m ²
4.15.6.44.7	Maximum total gross floor area - non-residential used for uses identified in Sentence 4.15.6.44.1, on all lands zoned RA5-44		6,300 m ²

4.15.6.44	Exception: RA5-44	Map # 36W	By-law
4.15.6.44.8	Uses contained in Clauses 4.15.6.44.1(1) and 4.15.6.44.1(2) shall not be permitted above the third storey		
4.15.6.44.9	Uses contained in Clauses 4.15.6.44.1(3) to 4.15.6.42.1(7) shall not be permitted above the second storey		
4.15.6.44.10	The lot line abutting Hurontario Street shall be deemed to be the front lot line		
4.15.6.44.11	Apartment dwelling units shall not be permitted on the first storey		
4.15.6.44.12	Indoor amenity areas accessory to an apartment dwelling, long-term care dwelling or retirement dwelling, shall not be permitted on the first storey within 10 m of the lot line abutting Hurontario Street		
4.15.6.44.13	Minimum floor space index - apartment dwelling zone on all lands zoned RA5-44	2.9	
4.15.6.44.14	Maximum floor space index - apartment dwelling zone on all lands zoned RA5-44	6.13	
4.15.6.44.15	Minimum height	3 storeys	
4.15.6.44.16	Maximum height	28 storeys	
4.15.6.44.17	Minimum height of a podium along the front lot line	3 storeys	
4.15.6.44.18	Maximum height of a podium along the front lot line	6 storeys	
4.15.6.44.19	Each building or structure shall be located up to the build-to-line identified on Schedule RA5-44 of this Exception		
4.15.6.44.20	Notwithstanding the provisions of Sentence 4.15.6.44.19, a maximum of 20% of the length of a streetwall may be set back beyond the build-to-line identified on Schedule RA5-44 of this Exception, up to 7.5 m		
4.15.6.44.21	Notwithstanding the provisions of Sentences 4.15.6.44.19 and 4.15.6.44.20, a maximum of 5% of the length of a streetwall may be set back beyond the build-to-line identified on Schedule RA5-44 of this Exception		
4.15.6.44.22	Minimum setback from the fourth floor of the exterior face of a podium to the fifth and/or sixth floors of the exterior face of a podium	2.5 m	
4.15.6.44.23	Minimum setback from the exterior face of a podium streetwall to buildings or structures, or parts thereof, located above the podium	2.5 m	
4.15.6.44.24	Minimum setback to a private road identified on Schedule RA5-42 of this Exception	3.0 m	
4.15.6.44.25	Where a building is located within 7.5 m of a street the main front entrance shall face a street		
4.15.6.44.26	Minimum above grade separation between buildings for that portion of the building above six (6) storeys	30 m	
4.15.6.44.27	Minimum exterior side yard	4.5 m	
4.15.6.44.28	Minimum interior side yard	7.0 m	
4.15.6.44.29	Minimum rear yard	4.5 m	

4.15.6.44	Exception: RA5-44	Map # 36W	By-law
4.15.6.44.30	Minimum setback from a parking structure completely below finished grade to a street line	0.0 m	
4.15.6.44.31	Minimum vertical depth	1.0 m	
4.15.6.44.32	An at-grade driveway, aisle, parking area or loading space shall not be permitted between a wall of a building or structure , or part thereof and the lot line abutting a street		
4.15.6.44.33	Minimum setback from a surface parking space to Hurontario Street	25.0 m	
4.15.6.44.34	Minimum number of resident parking spaces per one-bedroom and two-bedroom condominium apartment dwelling unit	1.1	
4.15.6.44.35	Minimum number of resident parking spaces per three-bedroom condominium apartment dwelling unit	1.2	
4.15.6.44.36	Minimum number of visitor parking spaces per condominium apartment dwelling unit	0.15	
4.15.6.44.37	For the visitor component, a shared parking arrangement may be used for the calculation of required visitor/non-residential parking in accordance with the following: the greater of 0.15 visitor spaces per unit or Parking required for all non-residential uses, except restaurant and take-out restaurant Restaurant and take-out restaurant shall not be included in the above shared parking arrangement and shall be provided in accordance with applicable regulations contained in Table 3.1.2.2 of this By-law		
4.15.6.44.38	Minimum number of parking spaces per 100 m ² GFA - non-residential for uses identified in Sentence 4.15.6.44.1, except Clauses 4.15.6.44.1(5) and 4.15.6.44.1(6)	4.3	
4.15.6.44.39	Minimum landscaped area	25 % of lot area	
4.15.6.44.40	Minimum depth of a landscaped buffer abutting a lot line	3.0 m	
4.15.6.44.41	"Podium" means the low-rise base of a building or structure located at or above established grade , that projects from the building		
4.15.6.44.42	"Height of a Podium" means the vertical distance between the established grade and the highest point of the roof surface of the podium		
4.15.6.44.43	"Vertical Depth" means the distance between the lowest grade level of the lands measured to the top of the roof membrane of a below grade parking structure		
4.15.6.44.44	For the purposes of this exception, a build-to-line means a setback at which a streetwall of a building, structure or part thereof, shall be located.		
4.15.6.44.45	All site development plans shall comply with Schedule RA5-44 of this Exception (in accordance with recommendations of April 16, 2012 Supplementary Report)		

H-RA5-44	Exception RA5-44	Map 36W	By-law
<p>Holding Provision</p> <p>The holding symbol H is to be removed from the whole or any part of the lands zoned H-RA5-44 by further amendment to Map 36W of Schedule B contained in Part 13 of this By-law, upon satisfaction of the following requirements:</p> <ol style="list-style-type: none"> <li data-bbox="462 520 966 863">(1) The identification of all land requirements in relation to the lands zoned H-RA5-44 for the development of Light Rapid Transit along Hurontario Street, to the satisfaction of the City of Mississauga, provided that such land requirements shall be determined by the City of Mississauga in accordance with the completed and approved Environmental Assessment, through the Transit Project Assessment Process for the Hurontario Light Rapid Transit Project and the corresponding amendment(s) to the official plan is implemented and in full force on or before December 31, 2018. <li data-bbox="462 877 966 1031">(2) If the land requirements are not identified on or before December 31, 2018 as set out in Paragraph (1), then, and subject to the conditions in (3) below also being satisfied, an application may be made to remove the holding "H" symbol from the lands zoned H-RA5-44. <li data-bbox="462 1045 966 1117">(3) Confirmation that requirements for municipal servicing (i.e. water and sanitary) have been met to the satisfaction of the Region of Peel. 			

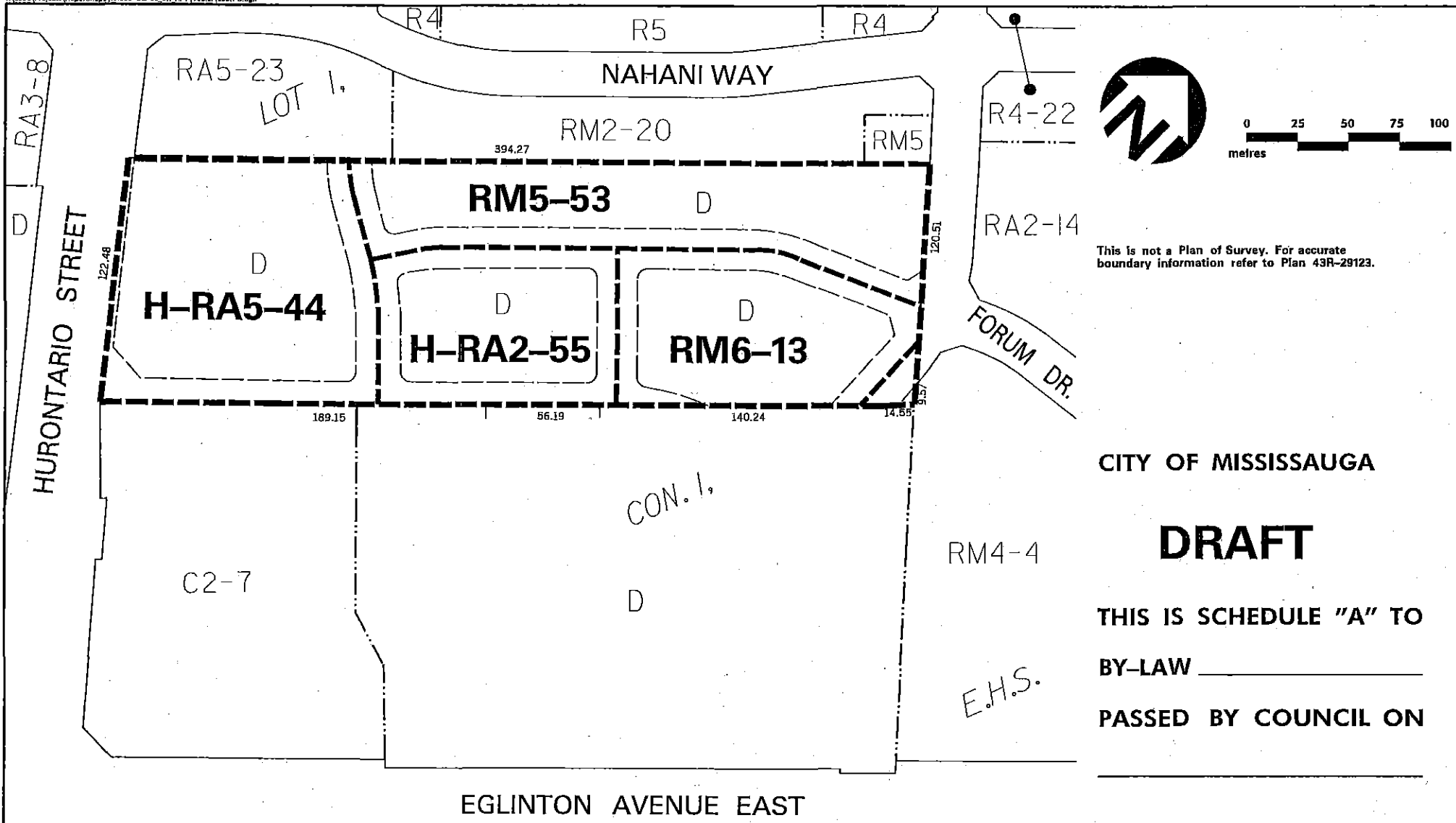
5. Map Number 36W of Schedule "B" to By-law Number 0225-2007, as amended, being a City of Mississauga Zoning By-law, is amended by changing thereon from "D" to "RM5-53", "RM6-13", "H-RA2-55" and "H-RA5-44", the zoning of Part of Lot 1, Concession 1, East of Hurontario Street, in the City of Mississauga, PROVIDED HOWEVER THAT the "RM5-53", "RM6-13", "H-RA2-55" and "H-RA5-44" zoning shall only apply to the lands which are shown on the attached Schedule "A", which is deemed to be an integral part of this By-law, outlined in the heaviest broken line with the "RM5-53", "RM6-13", "H-RA2-55" and "H-RA5-44" and "D" zoning indicated thereon.

6. This By-law shall not come into force until Mississauga Plan (Official Plan) Amendment Number _____ is in full force and effect.

ENACTED and PASSED this _____ day of _____, 2012.

MAYOR

CLERK



APPENDIX "A" TO BY-LAW NUMBER _____

Explanation of the Purpose and Effect of the By-law

This By-law amends the zoning of the property outlined on the attached Schedule "A" from "D" (Development) to "RM5-53" (Street Townhouse Dwellings), "RM6-13" (Townhouse Dwellings on a CEC-Private Road), "H-RA2-55" (Apartment Dwellings) and "H-RA5-44" (Apartment Dwellings).

"RM5-53" (Street Townhouse Dwellings) permits on-street townhouse dwellings.

"RM6-13" (Townhouse Dwellings on a CEC-Private Road) permits townhouse dwellings on a common element condominium private road.

Upon removal of the "H" provision, "H-RA2-55" (Apartment Dwellings) will permit a mid-rise apartment building.

Upon removal of the "H" provision, "H-RA5-44" (Apartment Dwellings) will permit apartment dwellings, in conjunction with retail commercial and office uses on the lower floors.

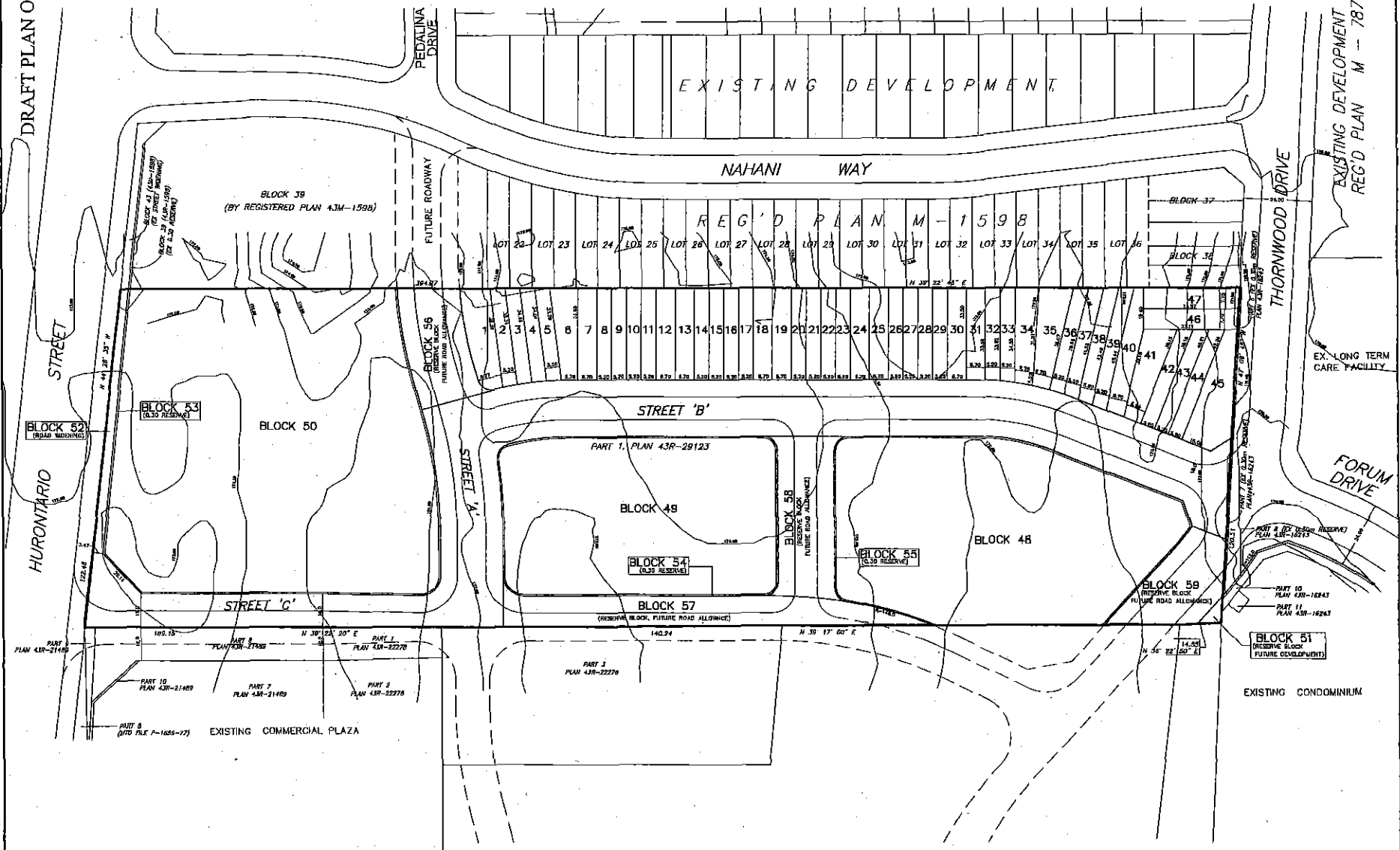
Location of Lands Affected

East side of Hurontario Street, north of Eglinton Avenue East, in the City of Mississauga, as shown on the attached Map designated as Schedule "A".

Further information regarding this By-law may be obtained from Rob Hughes of the City Planning and Building Department at 905-615-3200 ext. 5499.

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DRAFT PLAN OF SUBDIVISION





**SCHEDULE A
CONDITIONS OF APPROVAL**

FILE: T-M09004 W5

SUBJECT: Draft Plan of Subdivision
5081 Hurontario Street
Part of Lot 1, Concession 1
East side of Hurontario Street, north of Eglinton
Avenue East
City of Mississauga
Summit Eglinton Inc.

Approval of a draft plan of subdivision granted under Section 51 of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, will be valid until approval is either withdrawn or the plan is registered. Approval may be withdrawn by the Commissioner, Planning and Building Department if approval of the final plan has not been given three (3) years after the date of approval of the draft plan.

NOTE: City is "The Corporation of the City of Mississauga"
Region is "The Regional Municipality of Peel"

The City has not required either the dedication of land for park or other public recreational purposes, or a payment of money in lieu of such conveyance as a condition of subdivision draft approval authorized by Section 51.1 of the *Planning Act*, R.S.O. 1990, c.P.13 as amended. The City will require payment of cash-in-lieu for park or other public recreational purposes as a condition of development for each lot and block, prior to the issuance of building permits pursuant to Section 42(6) of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, and in accordance with the City's policies and by-laws.

- 1.0 Approval of the draft plan applies to the plan dated March 30, 2010, (revised on ____ to reflect staff recommendation of 6.0 m townhouse dwellings on north side of Street B).
- 2.0 That the owner agree, in writing, to satisfy all the requirements, financial and otherwise of the City and the Region.
- 3.0 That the applicant/owner shall enter into Servicing, Development and any other necessary agreements, satisfactory to the City, Region or any other appropriate authority, prior to ANY development within the plan. These agreements may deal with matters including, but not limited to, the following: engineering matters such as municipal services, road widenings, construction and reconstruction, signals, grading, fencing, noise mitigation, and warning clauses; financial issues, such as cash contributions, levies (development charges), land dedications or reserves, securities, or letters of credit; planning matters such as residential reserve blocks, buffer blocks, site development plan and landscape plan approvals and conservation. THE DETAILS OF THESE REQUIREMENTS ARE CONTAINED

IN COMMENTS IN RESPONSE TO THE CIRCULATION OF THE PLAN FROM AUTHORITIES, AGENCIES, AND DEPARTMENTS OF THE CITY AND REGION WHICH HAVE BEEN FORWARDED TO THE APPLICANT OR HIS CONSULTANTS, AND WHICH COMMENTS FORM PART OF THESE CONDITIONS.

- 4.0 All processing and administrative fees shall be paid prior to the registration of the plan. Such fees will be charged at prevailing rates of approved City and Regional Policies and By-laws on the day of payment.
- 5.0 The applicant/owner shall agree to convey/dedicate, gratuitously, any required road or highway widenings, 0.3 m (1 ft.) reserves, walkways, sight triangles, buffer blocks and utility or drainage easements to the satisfaction of the City, Region or other authority.
- 6.0 The applicant/owner shall provide all outstanding reports, plans or studies required by agency and departmental comments.
- 7.0 That a Zoning By-law for the development of these lands shall have been passed under Section 34 of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, and be in full force and effect prior to registration of the plan.
- 8.0 That in accordance with CPD Resolution 0121-91, that a minimum of three car spaces per dwelling, including those in a garage be required and a minimum of 0.25 visitor parking space per dwelling be required on the street for the subject development.
- 9.0 The proposed streets shall be named to the satisfaction of the City and the Region. In this regard, a list of street names shall be submitted to the City Transportation and Works Department as soon as possible after draft plan approval has been received and prior to any servicing submissions. The owner is advised to refer to the Region of Peel Street Names Index to avoid proposing street names which conflict with the approved or existing street names on the basis of duplication, spelling, pronunciation, and similar sounding.
- 10.0 Prior to final approval, the Engineer is required to submit, to the satisfaction of the Region, all engineering drawings in Micro-Station format as set out in the latest version of the Region of Peel "Development Procedure Manual".
- 11.0 Prior to final approval or preservicing, the developer will be required to monitor wells, subject to the homeowner's permission, within the zone of influence, and to submit results to the satisfaction of the Region.
- 12.0 Prior to final approval, the City shall be advised by the School Boards that satisfactory arrangements regarding the adequate provision and distribution of educational facilities have been made between the developer/applicant and the School Boards for this plan.
- 13.0 Prior to final approval, the Dufferin-Peel Catholic District School Board is to be satisfied that the applicant has agreed to include in the Development Agreement, Servicing

Agreement and all offers of purchase and sale the following warning clauses for all residential lots until the permanent school for the area has been completed:

- 13.1 Whereas, despite the best efforts of the Dufferin-Peel Catholic District School Board, sufficient accommodation may not be available for all anticipated students from the area, you are hereby notified that students may be accommodated in temporary facilities and/or bussed to a school outside of the neighbourhood, and further, that students may later be transferred to the neighbourhood school.
- 13.2 That the purchasers agree that for the purpose of transportation to school, the residents of the subdivision shall agree that children will meet the bus on roads presently in existence or at another place designated by the Board.
- 14.0 That the Development and Servicing Agreements shall contain a clause satisfactory to the Dufferin-Peel Catholic District School Board that the developer will erect and maintain signs at all major entrances to the proposed development which shall read: "Please be advised that students may be accommodated elsewhere on a temporary basis until suitable permanent pupil places, funded by the Government of Ontario, are available." These signs shall be to the School Board's specifications and at locations determined by the Board and erected prior to registration.
- 15.0 Prior to final approval, the Peel District School Board is to be satisfied that the following provision is contained in the Development Agreement, Servicing Agreement and all offers of purchase and sale for a period of five years after registration of the plan:
 - 15.1 Whereas, despite the efforts of the Peel District School Board, sufficient accommodation may not be available for all anticipated students in neighbourhood schools, you are hereby notified that some students may be accommodated in temporary facilities or bussed to schools outside of the area, according to the Board's Transportation Policy. You are advised to contact the School Accommodation Department of the Peel District School Board to determine the exact schools.
- 16.0 That the Development and Servicing Agreements shall contain a clause satisfactory to the Peel District School Board that the developer will erect and maintain signs at the entrances to the subdivision which shall advise prospective purchasers that due to present school facilities, some of the children from the subdivision may have to be accommodated in temporary facilities or bussed to schools, according to the Board's Transportation Policies. These signs shall be to the School Board's specifications and at locations determined by the Board.
- 17.0 That the owner/applicant agree to provide a temporary location at which Canada Post Corporation may locate community mailboxes during construction, until curbing and sidewalks are in place at the prescribed permanent mailbox locations.

- 18.0 Prior to final approval, confirmation be received from Canada Post Corporation that the applicant has made satisfactory arrangements for the installation of any central mail facilities required in this development.
- 19.0 Prior to preservicing and/or execution of the Servicing Agreement, the developer shall name to the satisfaction of the City Transportation and Works Department the telecommunications provider.
- 20.0 Prior to execution of the Servicing Agreement, the developer must submit in writing, evidence to the Commissioner of the City Transportation and Works Department, that satisfactory arrangements have been made with the telecommunications provider, Cable TV and Hydro for the installation of their plant in a common trench, within the prescribed location on the road allowance.
- 21.0 That prior to signing of the final plan, the Commissioner of Planning and Building is to be advised that all of the above noted conditions have been carried out to the satisfaction of the appropriate agencies and the City.

THE REQUIREMENTS OF THE CITY WILL BE EFFECTIVE FOR THIRTY-SIX (36) MONTHS FROM THE DATE THE CONDITIONS ARE APPROVED BY THE COMMISSIONER, PLANNING AND BUILDING DEPARTMENT. AFTER THIS DATE REVISED CONDITIONS WILL BE REQUIRED. NOTWITHSTANDING THE SERVICING REQUIREMENTS MENTIONED IN SCHEDULE A, CONDITIONS OF APPROVAL, THE STANDARDS IN EFFECT AT THE TIME OF REGISTRATION OF THE PLAN WILL APPLY.

Files: OZ 09/011 W5
T-M09004 W5

Summit Eglinton Inc.

Revised School Board Accommodation

The Peel District School Board	The Dufferin-Peel Catholic District School Board																																								
<ul style="list-style-type: none"> Student Yield: <table> <tr> <td>133</td><td>Kindergarten to Grade 6</td></tr> <tr> <td>66</td><td>Grade 7 to Grade 8</td></tr> <tr> <td>131</td><td>Grade 9 to Grade 12/OAC</td></tr> </table> School Accommodation: <p>Nahani Way Public School</p> <table> <tr> <td>Enrolment:</td><td>604</td></tr> <tr> <td>Capacity:</td><td>646</td></tr> <tr> <td>Portables:</td><td>0</td></tr> </table> <p>Bristol Road Senior Public School</p> <table> <tr> <td>Enrolment:</td><td>578</td></tr> <tr> <td>Capacity:</td><td>629</td></tr> <tr> <td>Portables:</td><td>0</td></tr> </table> <p>Applewood Heights High School</p> <table> <tr> <td>Enrolment:</td><td>989</td></tr> <tr> <td>Capacity:</td><td>1,284</td></tr> <tr> <td>Portables:</td><td>0</td></tr> </table> 	133	Kindergarten to Grade 6	66	Grade 7 to Grade 8	131	Grade 9 to Grade 12/OAC	Enrolment:	604	Capacity:	646	Portables:	0	Enrolment:	578	Capacity:	629	Portables:	0	Enrolment:	989	Capacity:	1,284	Portables:	0	<ul style="list-style-type: none"> Student Yield: <table> <tr> <td>31</td><td>Junior Kindergarten to Grade 8</td></tr> <tr> <td>14</td><td>Grade 9 to Grade 12/OAC</td></tr> </table> School Accommodation: <p>St. Jude Elementary School</p> <table> <tr> <td>Enrolment:</td><td>360</td></tr> <tr> <td>Capacity:</td><td>334</td></tr> <tr> <td>Portables:</td><td>0</td></tr> </table> <p>St. Francis Xavier High School</p> <table> <tr> <td>Enrolment:</td><td>2,200</td></tr> <tr> <td>Capacity:</td><td>1,500</td></tr> <tr> <td>Portables:</td><td>16</td></tr> </table> 	31	Junior Kindergarten to Grade 8	14	Grade 9 to Grade 12/OAC	Enrolment:	360	Capacity:	334	Portables:	0	Enrolment:	2,200	Capacity:	1,500	Portables:	16
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Summit Eglinton Inc.

**Files: OZ 09/011 W5
T-M09004 W5**

Community Comment and Concerns Response

The following is a summary of responses to comments and concerns received at the Community Meeting, Planning and Development Committee Meeting, and from letters received from Sheppard Brown Rosenthal (representing the Nicholas and Maria Danielak, landowners to the south) and Bratty and Partners (representing Alfonso Gallucci General Construction Limited, landowners to the north Nahani Way and Hurontario Street).

Comment

The development will result in additional traffic, which will further congest surrounding streets and intersections that are already over capacity, in particular at Hurontario Street and Eglinton Avenue.

Response

A traffic study in support of the applications was filed and reviewed by Transportation & Works staff. The conclusion of their review was that the traffic anticipated from the development can be accommodated within the existing and future road network. For additional information, see Transportation & Works comments in Appendix S-6.

Comment

Concern regarding visitor parking from the various buildings infiltrating onto nearby public roads and surrounding commercial developments, in particular given reductions in standards have been proposed.

Response

A parking study was provided that justifies the reduced parking numbers, identified in the attached draft by-law (Appendix S-8). These numbers are reflective of the urban environment proposed, and the level of transit service in the vicinity. It is not anticipated that parking will encroach onto adjacent neighbourhood streets, or to surrounding commercial lands.

Files: OZ 09/011 W5
T-M09004 W5

Summit Eglinton Inc.

Comment

Impact of building shadows on surrounding properties, in particular the proposed three storey townhouse dwellings on Block 2.

Response

The applicant had provided sun shadow studies in accordance with the previous City standards. These requirements do not necessitate the evaluation of buildings less than four storeys in height. None the less, the document did take into account the proposed three storey townhouses into consideration in its review. This document indicates there will be limited to no impact on the adjacent existing development. The City has requested that, in advance of the implementing zoning moving forward, a revised study reflecting the latest redistribution of building heights be filed in accordance with the revised standards for shadow studies approved by Council in December 2011.

Comment

Timing of construction and impacts on the surrounding neighbourhood.

Response

The applicant has advised that the first phase of the development will be the townhouse component of the development, which will include the east-west road from Hurontario Street to Thornwood Drive. Timing will be affected by the ability of the applicant to address draft plan of subdivision, site plan and building permit requirements, in addition to their own sales program. Phasing for the development will be addressed through a Development Agreement (see applicable section within report for details). Construction traffic will not be through existing residential streets, where possible.

Comment

There was concern for the number of vehicular accidents occurring in and around the intersection of Nahani Way and Hurontario Street.

Files: OZ 09/011 W5
T-M09004 W5

Summit Eglinton Inc.

Response

Transportation & Works staff have concluded that the approval of the subject application will have no bearing on accident rates at this location.

Comment

Objection to the creation of a road right-of-way directly abutting the high density lands at the immediate corner of Hurontario Street and Nahanni Way.

Response

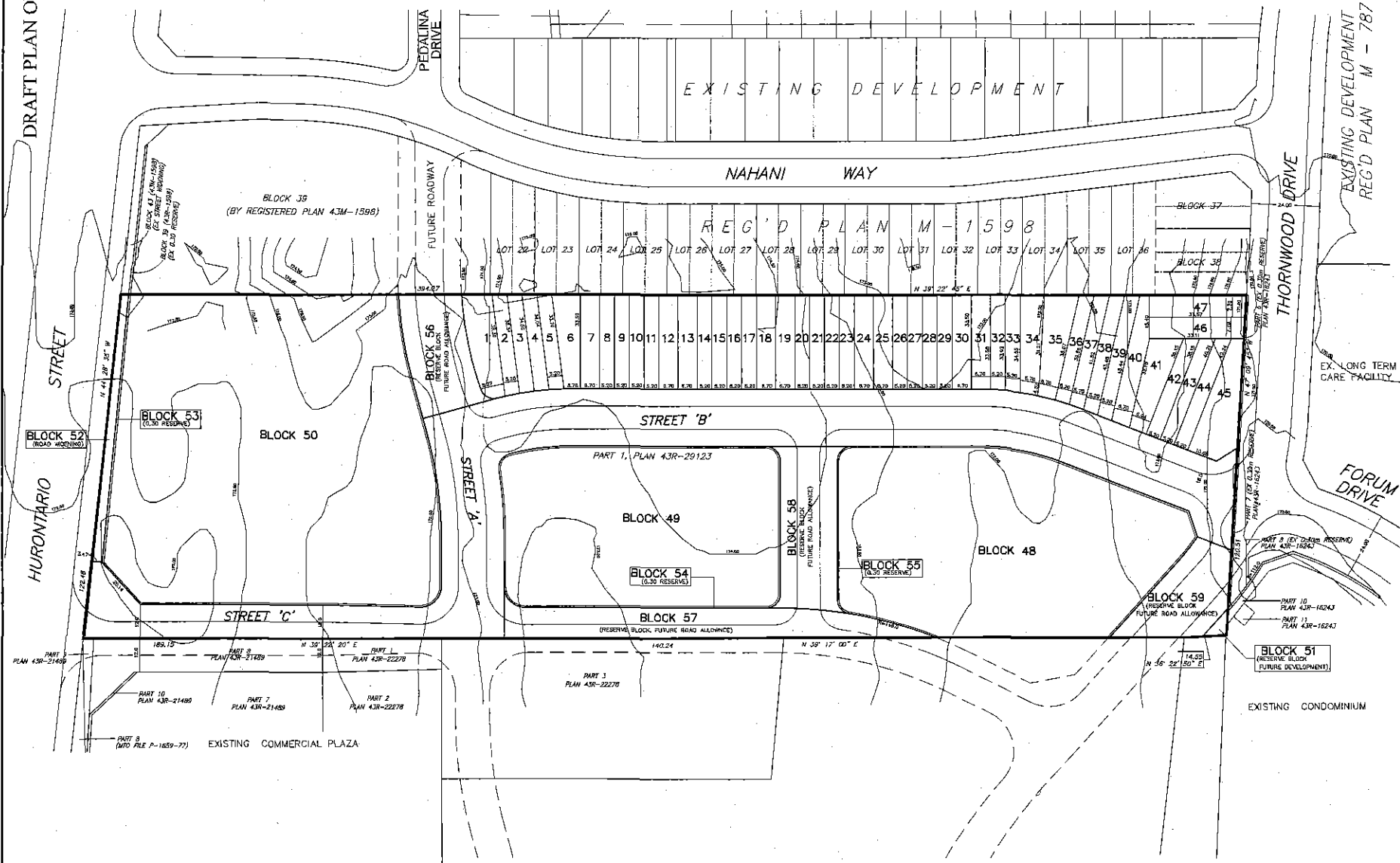
The proposal for this road connection will contribute to an improved road fabric for the area, allowing for traffic to be appropriately dispersed throughout the neighbourhood. It is not intended that the road will be constructed until development occurs to the north. If it is determined that the lands are ultimately not needed for road purposes, the lands may be appropriate for future development compatible with surrounding uses.

Comment

Unacceptable road network portrayed on submitted concept plan for lands to the south of the subject property.

Response

There are certain fixed road points that the concept plan needs to incorporate, including the extension of Thornwood Drive and its connection with Eglinton Avenue East opposite Sorrento Drive. This Major Collector road extension is identified in the Official Plan. Other routes shown provide for a permeable network of roads for the area, in conjunction with the Summit Eglinton proposal. Any road pattern would be reviewed through the submission of site specific development applications, and associated traffic study documents, for the lands.





Corporate Report

PLANNING & DEVELOPMENT COMMITTEE

APR 02 2013

Clerk's Office

Originator's

Files OZ 11/018 W5

DATE: March 12, 2013

TO: Chair and Members of Planning and Development Committee
Meeting Date: April 2, 2013

FROM: Edward R. Sajecki
Commissioner of Planning and Building

SUBJECT: **Official Plan Amendment and Rezoning Applications**
To permit a two-storey motor vehicle repair facility
Part of Lot 11, Concession 1, W.H.S, designated as Parts 1 & 2,
Plan 43R-13493
Northwest corner of Derry Road West and Hurontario Street
Owner: Antorisa Investments Ltd.
Applicant: Bousfields Inc.
Bill 51

Supplementary Report

Ward 5

RECOMMENDATION: That the Report dated March 12, 2013, from the Commissioner of Planning and Building recommending refusal of the applications under File OZ 11/018 W5, Antorisa Investments Ltd., Part of Lot 11, Concession 1, W.H.S, designated as Parts 1 & 2, Plan 43R-13493, northwest corner of Derry Road West and Hurontario Street, be adopted in accordance with the following:

1. That City Council direct the City Solicitor, representatives from the appropriate City Departments and any necessary consultants, to attend the Ontario Municipal Board hearing on the subject applications in support of the recommendations outlined in the report dated March 12, 2013 from the Commissioner of Planning and Building.

2. That City Council provide the Planning and Building Department with the authority to instruct Legal Services staff on any modifications to the position deemed necessary during the Ontario Municipal Board hearing process, however, if there is a potential for settlement, then a report shall be brought back to Council by the City Solicitor.

**REPORT
HIGHLIGHTS:**

- No revised concept plans or updated comments have been received since the public meeting on September 4, 2012;
- The applicant appealed the applications to the Ontario Municipal Board on October 18, 2012. An OMB pre-hearing has been scheduled for March 18, 2013;
- The new Mississauga Official Plan (2011) was approved by the Ontario Municipal Board on November 14, 2012, save and except for certain appeals, some of which affect the subject applications;
- The proposed official plan amendment and rezoning applications do not represent good planning, are premature and should be refused.

BACKGROUND:

A public meeting was held by the Planning and Development Committee on September 4, 2012, at which time a Planning and Building Department Information Report (Appendix S-1) was presented and received for information.

At the public meeting, the Planning and Development Committee passed Recommendation PDC-0052-2012, which was subsequently adopted by Council and is attached as Appendix S-2.

No revised plans or updated information have been received by the Planning and Building Department since the Information Report (Appendix S-1) was at Planning and Development Committee. Issues with access, grading, stormwater management, encroachments, land dedication requirements, and compatibility with the proposed Light Rail Transit Corridor along Hurontario Street are unresolved.

Further, technical documents identified in the Information Report remain outstanding and include:

- revised Stormwater Management Report;
- revised Heritage Impact Statement;
- revised Traffic Impact Review;
- Parking Utilization Study;
- validations for the Phases 1 and 2 Environmental Site Assessments dated August 2000; and
- planning rationale supporting the proposed development in consideration of the Hurontario Light Rail Transit as per the Hurontario/Main Street Corridor Master Plan adopted by Council.

At the time of preparation of the Information Report, not all City department comments had been received. Additional technical documents such as a Composite Utility Plan and Streetscape Master Plan are also required.

On October 18, 2012, the Official Plan Amendment and Rezoning Applications were appealed to the Ontario Municipal Board by the property owner, Antorisa Investments Ltd. At the time of preparation of this report, a hearing date has yet to be scheduled. A pre-hearing date has been scheduled for March 18, 2013. The purpose of this report is to receive Council's direction on the applications and the appeals.

COMMENTS:

See Appendix S-1 - Information Report prepared by the Planning and Building Department.

COMMUNITY ISSUES

Correspondence expressing objection to the applications was received by:

- email dated February 2, 2012 from Brutto Consulting on behalf of the owner of 7091 Hurontario Street (located north of the subject property on the east side of Hurontario Street, opposite Kingsway Drive) and;
- letter dated September 4, 2012 from Carl Brawley of Glen Schnarr and Associates Inc. on behalf of the owner of 7020 Hurontario Street (located immediately north of the subject property).

Issues identified are summarized below:

Comment

The proposal does not maintain the long standing intent of the Official Plan wherein the proposed vehicular repair facility uses were not contemplated or deemed to be appropriate at this Gateway location.

Comment

The proposed motor vehicle repair facility is not an appropriate land use at this intersection and does not conform with the planning policies and objectives of the Hurontario Street corridor. The application proposes eight (8) garage bay doors exposed directly to the property to the north, municipally known as 7020 Hurontario Street, which is not compatible from an urban design perspective.

Response to Comments

The above comments are also of significant concern to the Planning and Building Department. Staff's responses are contained within the Planning Comments section of this report.

PLANNING COMMENTS

The Planning and Building Department has reviewed and evaluated the materials submitted by the applicant in support of the applications and the appeals in the context of: relevant provincial policies, municipal policies, comments received from various City departments, agencies and the public, and the applicant's planning rationale.

Provincial Policy Statement (PPS)

The PPS states that "Planning authorities shall not permit development in planned corridors that could preclude or negatively affect the use of the corridor for the purpose(s) for which it is

identified" and that "a land use pattern, density and mix of uses should be promoted that minimizes the length and number of vehicle trips and supports the development of viable choices and plans for public transit".

The proposed development of a two-storey motor vehicle repair facility at or near existing and future major transit stops and stations does not take into account the planned context of Hurontario Street as an urban, vibrant, higher density transit and pedestrian-oriented street. The proposal is not consistent with the PPS.

Growth Plan for the Greater Golden Horseshoe

The Growth Plan states that "major transit stations and intensification corridors will be designated in Official Plans and will be planned to achieve: a) increased residential and employment densities that support and ensure the viability of existing and planned transit service levels; and b) a mix of residential, office, institutional, and commercial development where appropriate.

Hurontario Street has been identified as an Intensification Corridor in Mississauga Official Plan (2011), where growth is to be directed to provide higher density mixed-use development supportive of planned higher order transit along Hurontario Street. The addition of another motor vehicle oriented use at the principal intersection of Hurontario Street and Derry Road West does not support the vision for intensification corridors.

Mississauga Plan (2003)

The Official Plan Amendment application was submitted when Mississauga Plan (2003) was the, in force, Official Plan. This development proposal requires an amendment to the 2003 Mississauga Plan Policies for the Gateway Planning District. As outlined in the Information Report, Section 5.3.2.1 of Mississauga Plan provides criteria for evaluating site specific Official Plan Amendments. The criteria is outlined below, followed by a

discussion of how the proposed application does not address the intent of the criteria.

"The proposal will not adversely impact or destabilize the overall intent, goals and objectives of the Official Plan; and the development or functioning of the remaining lands which have the same designation, or neighbouring lands."

The location of the subject property is significant in terms of City image, area character and streetscape. Hurontario Street and Derry Road is a principal intersection for transit and employment growth along the Hurontario Corridor between Provincial Highways 401 and 407. At the time the OPA and rezoning applications were submitted and deemed complete (January 13, 2012), the proposed motor vehicle repair use was in conformity with the applicable "Business Employment" land use designation under the Gateway District Policies of Mississauga Plan (2003). However, an Official Plan Amendment was submitted due to the proposed two storey height of the building; whereas the applicable Special Site 2 policies require buildings at the corners of Hurontario Street and Derry Road West to be a minimum of three storeys.

The general policies of Mississauga Plan discourage Motor Vehicle Commercial uses as a single use and from locating at important intersections. While the site specific policies recognize the two existing motor vehicle service station/gas bar sites at the southeast and southwest corners of Hurontario Street and Derry Road, these uses are encouraged to be redeveloped given their prominent location. Due to the limited size of the subject property and the importance of the Hurontario Street and Derry Road intersection, land consolidation is also encouraged in the site specific policies in order to facilitate useable development parcels that allow for intensified development that would promote Hurontario Street as a major transit corridor.

"The land is suitable for the proposed uses, and compatible with existing and future uses of the surrounding lands."

While the applicant has submitted building elevations that appear to propose a three storey building with a height of approximately 10.8 m (35.3 ft.), the proposed building is, in fact, only partially two storeys in order to provide sufficient ceiling height for vehicular lifts on the ground floor. Windows are proposed on all sides of the building to give the impression of a three storey building from the street. A third storey is not proposed, and the proposed parking calculations are based on the gross floor area provided for a 756.7 sq. m (8,145.3 sq. ft.) partial two storey motor vehicle repair building.

From an urban design perspective, the Hurontario Street and Derry Road intersection is a major node that has a number of important functions, such as facilitating transit use through intensification and establishing a high quality image for the street. The applicable design guidelines outlined in the Upper Hurontario Corridor – A Design Mandate for Excellence Document (March 1996) identifies the north sector of the Hurontario Corridor as a gateway into Mississauga and "a distinctive civic boulevard having a high profile and design standard". The proposed partial two storey motor vehicle repair facility with parking located between the streetline and the front building face, visible service bays exposed to the property to the north, vehicular access points close to the intersection, insufficient building setbacks, and substandard landscaping and architectural gateway features does not satisfy the design guidelines or support the City's vision for the Hurontario Street and Derry Road intersection along the Hurontario Street corridor.

The proposed use with its significant design deficiencies will likely negatively impact the future development potential of lands with the same land use designation immediately north of the subject property, and discourage the redevelopment of the two existing motor vehicle service stations (located to the southeast and southwest) for more intensive, business employment (e.g. office)

development contemplated by the Official Plan at this high profile intersection.

"There are adequate infrastructure and community services to support the proposed development."

Vehicular access, grading, stormwater management, encroachments, land dedication requirements, and compatibility with the proposed Light Rail Transit Corridor along Hurontario Street are issues that remain outstanding. As a result, it has not been demonstrated that there is adequate infrastructure in place to support the proposed development. Notwithstanding these requirements, the proposal is not in keeping with the City's vision for the Hurontario Street corridor.

Hurontario/Main Street Corridor Master Plan Study (2010)

In July 2010, City Council endorsed the Hurontario/Main Street Corridor Master Plan Study. The Master Plan is a vision for Hurontario Street/Main Street as a Light Rail Transit Corridor that accommodates anticipated growth and transportation demands, and which complements and complies with both the Province of Ontario's Places to Grow legislation and Metrolinx's The Big Move Regional Transportation Plan. A Preliminary Design Study is now underway. A Light Rail Transit station is proposed at the intersection of Hurontario Street and Derry Road. In order to support the planned higher order transit, supportive land uses and densities are required along Hurontario Street.

Mississauga Official Plan (2011)

In 2011, the City of Mississauga adopted Mississauga Official Plan that takes a contemporary approach to land use planning in Mississauga, with a focus on integrating land use, transportation and urban design and providing for growth in locations that are supported by existing and planned infrastructure. Mississauga Official Plan was partially approved by the Ontario Municipal Board on November 14, 2012, save and except for certain appeals, some of which affect the subject applications.

The subject lands are located within the Gateway Corporate Centre. Corporate Centres represent major employment concentrations outside of the Downtown and are also considered Intensification Areas. The Gateway Corporate Centre is envisioned to be one of the premier office areas in Mississauga, with the greatest concentration of office development centered around major transit stations along the Hurontario Street Corridor, including the proposed Light Rail Transit Station at the intersection of Hurontario Street and Derry Road. The creation of office concentrations at major transit stations is critical to support the infrastructure investment in Light Rail Transit.

The subject lands continue to be designated "Business Employment" in Mississauga Official Plan (2011) but the designation no longer permits motor vehicle commercial uses as it did in Mississauga Plan (2003). The Gateway Corporate Centre, Business Employment land use policies are currently under appeal, and, as a result, the Gateway District Policies in Mississauga Plan (2003) remain in effect. Notwithstanding that the proposed motor vehicle repair facility is a permitted use under the Mississauga Plan (2003) policies, regard shall also be had for the Council endorsed Hurontario/Main Street Corridor Master Plan and the new official plan. Further, amendments to Mississauga Official Plan are proposed for the Gateway Corporate Centre Character Area to implement the findings of the Hurontario/Main Street Corridor Master Plan (2010).

Currently, lands in the Gateway Corporate Centre are generally designated "Business Employment" which permits a range of uses, some of which are land extensive and auto-dependent, such as warehousing and manufacturing. These types of uses are not supportive of the vision for Hurontario Street as a higher density mixed use corridor with Light Rail Transit. As a result, significant policy changes are proposed for the Gateway Corporate Centre Character Area, which are outlined in the Corporate Reports titled "Proposed Amendments to Mississauga Official Plan (2011) for the Gateway Corporate Centre Character Area" dated August 28, 2012 and September 25, 2012 summarized as follows:

- Identify the Hurontario Street Intensification Corridor and add policies to accommodate additional employment growth in support of the proposed Light Rail Transit system;
- Identify additional road network to allow integration of land uses within the Hurontario corridor;
- Identify major transit station locations and direct the largest concentration of density to these areas;
- Redesignate lands from "Business Employment" to "Office" to ensure the appropriate form of development occurs at the Major Transit Stations and along the frontage lands of the corridor, in support of the proposed Light Rail Transit system;
- Prohibit land extensive, auto dependant uses from fronting the corridor, including gas bars and car washes; and
- Establish a Public Realm Plan and built form standards to guide development in the Corporate Centre over the next 30-50 years.

These proposed changes and public submissions received at the statutory public meeting held on October 15, 2012 are currently under review by city staff. The recommendations are expected to be presented to City Council early fall 2013. Given the detailed draft policies developed to articulate the vision of the approved Hurontario/Main Street Corridor Master Plan (2010), and the lack of supporting studies for the proposed motor vehicle commercial use within the Gateway Corporate Centre Character Area, consideration of the subject applications is premature.

Policy Summary

The proposed partial two storey motor vehicle repair building with eight loading bays, visible parking, insufficient building setbacks, insufficient landscaped buffers, and frontage onto a major transit corridor does not support the goals and objectives of Mississauga Plan (2003), Mississauga Official Plan (2011) or the Hurontario/Main Street Corridor Master Plan (2010). Further, the proposal negatively impacts the future development of neighbouring properties that have the same land use designation.

Zoning

The existing "D" (Development) zoning is proposed to be amended to "E2-Exception" (Employment) to permit a Motor Vehicle Repair Facility - Restricted with exceptions for the reduced front yard and exterior side yard setbacks, reduced depth of landscaped buffers along all property lines, and a reduction in the amount of required parking. These exceptions are based on the concept plan dated October 5, 2011, which is attached as Appendix S-3 with the requested zone exceptions detailed in Appendix S-4. The concept plan in Appendix I-5 and proposed zoning standards outlined in Appendix I-9 within the Information Report were based upon an earlier dated plan, which was also submitted with the development applications. There are slight differences between the plans including the amount of parking proposed and the depth of the westerly landscaped buffer. The applicant has confirmed that it is the most recent plan that should be used.

While a built form which is urban in character with respect to reduced setbacks to the street is proposed, a 0.3 m (0.98 ft.) front and exterior side yard setbacks does not allow for a high standard of private realm streetscape design. Instead, it results in a parking space for persons with disabilities and the walkway to the main entrance of the building encroaching onto the City-owned right-of-ways, such that only one tree is proposed on private property along Hurontario Street. Significant landscaped buffer reductions are proposed on all sides of the property. The proposed landscaped buffer depths vary from 0.3 m (0.98 ft.) along the majority of the Derry Road West and Hurontario Street frontages, 1.5 m (4.92 ft.) along the westerly property line to the Derry West Cemetery, and 0.89 m (2.93 ft.) along the majority of the north property line, which does not allow for mitigation of visual impacts of the proposed service bays and parking lot onto the abutting development parcel to the north. Further, a site deficiency of 10 parking spaces, including 1 space for persons with disabilities is proposed for a site where off-street parking along Derry Road West and Hurontario Street is not an alternative.

The proposed partial two-storey motor vehicle repair facility requires exceptions to the "E2" (Employment) base zone, and would result in adverse impacts to the streetscape, abutting properties and the overall functionality of the site. Further, as lands at major intersections within the north sector of Hurontario Street, including Hurontario Street and Derry Road, are proposed to be redesignated to "office" in the amendments to Mississauga Official Plan (2011), the corresponding zoning would be "E1 – Exception" (Employment in Nodes). Review of the "E1" regulations, which are more restrictive than the "E2" regulations in terms of building setbacks, further demonstrates that the proposed setbacks and landscaped buffers are not appropriate.

FINANCIAL IMPACT:

Development charges will be payable in keeping with the requirements of the applicable Development Charges By-law of the City as well as financial requirements of any other official agency concerned with the development of the lands.

CONCLUSION:

It has not been demonstrated that the proposed Official Plan Amendment and Rezoning are acceptable from a planning standpoint and, therefore, the application should not be approved for the following reasons:

1. The development as proposed does not support the overall intent, goals and objectives of Mississauga Plan (2003) or Mississauga Official Plan;
2. The proposed zoning standards are not appropriate to accommodate the requested use as encroachments will be required, and insufficient landscaping and parking are proposed for a property that is significant in terms of city image, area character and streetscape;
3. The proposed development is considered premature given the extensive policy review being undertaken for the Gateway Corporate Centre Character Area;

4. It has not been demonstrated that the proposed use is compatible with the Upper Hurontario Corridor design guidelines or the Hurontario/Main Street Corridor Master Plan Study;
5. Numerous outstanding technical concerns have not been addressed at the time of the preparation of this report.

ATTACHMENTS:

Appendix S-1: Information Report
Appendix S-2: Recommendation PDC-0052-2012
Appendix S-3: Concept Plan
Appendix S-4: Revised Proposed Zoning Standards



Edward R. Sajecki
Commissioner of Planning and Building

Prepared By: Stephanie Segreti, Development Planner



Corporate Report

PDC SEP 04 2012

Clerk's Files APPENDIX S-1

Originator's
Files OZ 11/018 W5

DATE: August 14, 2012

TO: Chair and Members of Planning and Development Committee
Meeting Date: September 4, 2012

FROM: Edward R. Sajecki
Commissioner of Planning and Building

SUBJECT: **Information Report**
Official Plan Amendment and Rezoning Applications
To permit a two storey motor vehicle repair facility
Part of Lot 11, Concession 1, W.H.S., designated as Parts 1 & 2,
Plan 43R-13493
Northwest corner of Derry Road West and Hurontario Street
Owner: Antorisa Investments Inc.
Applicant: Bousfields Inc.
Bill 51

Public Meeting **Ward 5**

RECOMMENDATION: That the Report dated August 14, 2012, from the Commissioner of Planning and Building regarding the applications to amend Mississauga Plan from "Business Employment - Special Site 2" to "Business Employment - Special Site" and to change the Zoning from "D" (Development) to "E2 - Exception" (Employment), to permit a two storey motor vehicle repair facility under file OZ 11/018 W5, Antorisa Investments Inc., Part of Lot 11, Concession 1, W.H.S., designated as Parts 1 & 2, Plan 43R - 13493, be received for information.

**REPORT
HIGHLIGHTS:**

- Applications made to permit a two (2) storey motor vehicle repair facility (Active Green+Ross).
- Mississauga Plan policies permit a motor vehicle repair facility on the site; however, the new Mississauga Official Plan does not permit the use.
- Prior to the Supplementary Report, matters to be addressed include: the appropriateness of the proposed motor vehicle repair facility use for the site given the objectives for high-order office along Hurontario Street; the height and design of the building given the urban design objectives for Hurontario Street; and vehicular access concerns to the site.

BACKGROUND:

The above-noted applications have been circulated for technical comments. The purpose of this report is to provide preliminary information on the applications and to seek comments from the surrounding community.

COMMENTS:

Details of the proposal are as follows:

Development Proposal	
Applications submitted:	December 6, 2011 (Received) January 13, 2012 (Deemed Complete)
Height:	10.8 m (35.3 ft.)
Gross Floor Area:	756.7 m ² (8,145 sq. ft.)
Lot Coverage:	31.3%
Floor Space Index:	0.46
Landscaped Area:	10.4%
Parking Required:	33 spaces (2 required for persons with disabilities)
Parking Provided:	23 spaces (1 provided for persons with disabilities)
Supporting Documents:	Planning Justification Report Traffic Impact Review Building Initiatives Green Development Standards

Development Proposal	
	Arborist Report Heritage Impact Statement Stormwater Management Report Concurrence with Stage 1 & 2 Archaeological Assessment Memo

Site Characteristics	
Frontage:	26.0 m (85.3 ft.) on Hurontario Street
Depth:	52.1 m (170.9 ft.)
Net Lot Area:	1 637 m ² (17,621 sq. ft.)
Existing Use:	Vacant

Green Development Initiatives

The applicant has identified that green development initiatives will be addressed through the installation of permeable pavers where possible, the planting of new trees and native vegetation, the provision of bicycle parking in a weather-protected area and properly shielded exterior light fixtures. Additional information is provided in Appendices I-1 to I-9.

Neighbourhood Context

The subject property is located just south of the City of Brampton boundary and Highway 407. The site currently sits vacant with only a commercial billboard located on the lands. Information regarding the history of the site is found in Appendix I-1.

The surrounding land uses are described as follows:

- North: Vacant lands as well as an eight (8) storey office building, home to the Region of Peel offices, further north.
- East: A one storey restaurant (Grill One) and truck stop across Hurontario Street.
- South: A gas station (Husky) with vacant land further south across Derry Road West.
- West: Derry West Cemetery with the Mississauga Convention Centre further west.

Current Mississauga Plan Designation and Policies for Gateway (May 5, 2003)

"Business Employment" which permits an integrated mix of business activities that operate mainly within enclosed buildings, including, among others, industrial/manufacturing uses, offices, research and development, community uses, financial institutions, hotels, all types of restaurants, motor vehicle rental facilities and motor vehicle commercial uses. A motor vehicle repair facility is classified as a motor vehicle commercial use.

The site is also subject to the Gateway District Policies which are intended to encourage prestige development, accommodating a mix of manufacturing, distribution, research and development and office uses to take advantage of the system of highways and major roads and proximity to the airport. The Special Site 2 provisions of the Gateway District also apply to the four corners of Hurontario Street and Derry Road East/Derry Road West and Hurontario Street and Courtneypark Drive East/Courtneypark Drive West, as well as the Hurontario Street Corridor Development Policies (See Appendix I-8).

There are other policies in the Official Plan which also are applicable in the review of these applications including Urban Design policies (see Appendix I-8).

The proposed motor vehicle repair facility use is in conformity with the "Business Employment" land use designation, however, an Official Plan Amendment is required as the building has a proposed height of only two (2) storeys, whereas the Gateway District Special Site 2 policies require buildings at the corner of Hurontario Street and Derry Road East/Derry Road West to be a minimum of three (3) storeys.

Criteria for Site Specific Official Plan Amendments

Section 5.3.2 of Mississauga Plan contains criteria which requires an applicant to submit satisfactory planning reports to demonstrate the rationale for the proposed amendment as follows:

- the proposal would not adversely impact or destabilize the following: the overall intent, goals and objectives of the Official Plan; and the development and functioning of the remaining lands which have the same designation, or neighbouring lands;
- the lands are suitable for the proposed uses, and compatible with existing and future uses of surrounding lands;
- there is adequate infrastructure and community services to support the proposed development.

Proposed Official Plan Designation and Policies

"Business Employment - Special Site", to permit a two (2) storey motor vehicle repair facility.

Mississauga Official Plan (2011)

Mississauga Official Plan (2011) was adopted by City Council on September 29, 2010 and partially approved by the Region on September 22, 2011. Mississauga Official Plan (2011) has been appealed in its entirety; therefore, the existing Mississauga Plan (2003) remains in effect. While the existing Mississauga Plan (2003) is the plan of record against which the application is being reviewed, regard should also be given to the new Mississauga Official Plan (2011).

The new Mississauga Official Plan designates the subject lands as "Business Employment" which permits a broad range of employment type uses such as manufacturing, office and service uses, including restaurants. The "Business Employment" designation does not permit motor vehicle commercial uses. The subject lands are also located within the Gateway Corporate Centre, which is intended to serve as one of four prominent Corporate Centres within the City of Mississauga. The site is subject to the Special Site 1 provisions of the Gateway Corporate Centre, which apply to the four corners of Hurontario Street and Derry Road East/Derry Road West and Hurontario Street and

Courtneypark Drive East/Courtneypark Drive West (see Appendix I-8).

An amendment to the Mississauga Official Plan (2011) will be required to permit the proposed two (2) storey motor vehicle repair facility.

Existing Zoning

"D" (**Development**), which recognizes vacant lands not yet developed and/or permits the use that legally existed on the date of passing of By-law 0225-2007, until such time as the lands are rezoned in conformity with Mississauga Plan, in appropriate locations throughout the City. It permits a building or structure legally existing on the date of passing of this By-law and the existing legal uses of such building or structure.

Proposed Zoning By-law Amendment

"E2-Exception" (**Employment**), to permit a motor vehicle repair facility.

Details of the proposed exceptions to the "E2-Exception" (Employment) zone category are provided in Appendix I-9.

An amendment to the Mississauga Official Plan (2011) will be required should the appeals against the Plan be resolved prior to consideration of the supplementary report. Should an amendment be required, the lands should be zoned "E1-Exception" (Employment) as the corresponding zone category for lands designated Employment within a Corporate Centre is E1.

COMMUNITY ISSUES

No community meetings were held for the subject applications. A written submission was received by the Planning and Building Department on behalf of an adjacent land owner expressing concern over the motor vehicle repair facility proposed for the site

as the application is not in keeping with the intent of the Official Plan.

DEVELOPMENT ISSUES

Agency comments are summarized in Appendix I-7. Based on the comments received and the applicable Mississauga Plan policies the following matters will have to be reviewed:

Built Form

Policies in Mississauga Plan and Mississauga Official Plan require buildings in this area to be a minimum of three (3) storeys. The applicant is proposing a two (2) storey building that has the physical height of a typical three storey building at 10.8 m (35.3 ft.). It needs to be determined whether the proposed number of storeys, physical massing and location of the building is appropriate for the subject site given the requirement for any building to have prominence at this corner. Regard will also be given to the design guidelines as outlined in the *Upper Hurontario Corridor – A design mandate for excellence* document.

Streetscape

Staff will review the design of the proposed building to ensure that an appropriate main street storefront appearance and transparent façade is provided. The landscaping, lighting, screening of the parking lot and pedestrian connections will also be reviewed to protect for a pedestrian oriented main street along Hurontario Street.

Vehicular Access from Derry Road West

The City of Mississauga is undertaking the Hurontario Main Street Study. The land requirements for the study need to be determined, as potential road widenings or other land dedications may impact the subject site and the proposed development. The Region of Peel has indicated that it will not support a vehicular access point to the site on Derry Road West until it can be determined that a

westbound bus stop, as close to the Hurontario Street/Derry Road West intersection as possible, will not be prevented as a result of the subject proposal and any land dedication requirements identified through the Hurontario study.

Parking Utilization

A parking utilization study has not been submitted but will be required to properly review the proposed parking deficiency.

OTHER INFORMATION

Development Requirements

In conjunction with the proposed development, there are other matters which may require the applicant to enter into appropriate agreements with the City.

FINANCIAL IMPACT:

Development charges will be payable in keeping with the requirements of the applicable Development Charges By-law of the City as well as financial requirements of any other official agency concerned with the development of the lands.

CONCLUSION:

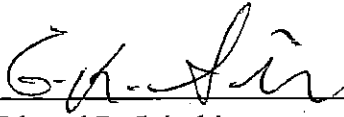
Most agency and City department comments have been received and after the public meeting has been held and all issues are resolved, the Planning and Building Department will be in a position to make a recommendation regarding these applications.

ATTACHMENTS:

- Appendix I-1: Site History
- Appendix I-2: Aerial Photograph
- Appendix I-3: Excerpt of Gateway District Land Use Map
- Appendix I-4: Excerpt of Existing Land Use Map
- Appendix I-5: Concept Plan
- Appendix I-6: Elevations
- Appendix I-7: Agency Comments
- Appendix I-8: Mississauga Plan Policies

Appendix I-9: Proposed Zoning Standards


Appendix I-10: General Context Map



Edward R. Sajecki

Commissioner of Planning and Building

Prepared By: Jeff Markowiak, Development Planner

 R:\PLAN\DEVCONTROL\GROUP\WPDATA\PDC1\OZ11018 Info Report.cr.jm.fw.so.doc

Antorisa Investments Inc.

File: OZ 11/018 W5

Site History

- May 5, 2003 – The Gateway District Policies and Land Use Map are approved by the Region of Peel, designating the lands as Business Employment.
- June 20, 2007 – Zoning By-law 0225-2007 came into force zoning the subject lands "D" (Development).
- December 1, 2009 – Mississauga Plan 40 came into effect, adding further policies and urban design principles to the Gateway District Policies.



LEGEND:



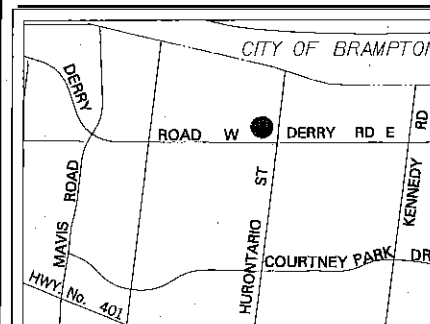
SUBJECT LANDS

DATE OF AERIAL PHOTO: 03 2011



SUBJECT:

ANTORISA INVESTMENTS LTD.



FILE NO:
OZ 11/018 W5

DWG. NO:
11018A

SCALE:
1:2500

PDC DATE:
2012 09 04

DRAWN BY:
K. PROKOP

APPENDIX 1-2

MISSISSAUGA
Planning and Building

Produced by
T&W, Geomatics



PART OF GATEWAY DISTRICT LAND USE MAP GATEWAY DISTRICT POLICIES OF MISSISSAUGA PLAN

LAND USE DESIGNATIONS

- General Retail Commercial
- Motor Vehicle Commercial
- Business Employment
- Industrial
- Public Open Space
- Private Open Space
- Greenbelt
- Parkway Belt West
- Utility

TRANSPORTATION LEGEND

- Provincial Highway and Interchange
- Arterial
- Major Collector
- Major Collector (Scenic Route)
- Future Major Collector (conceptual)
- Minor Collector
- Local Road
- Major Transit Corridor

LAND USE LEGEND

- Regulatory Floodplain
- LBPIA Operating Area Boundary - See Aircraft Noise Policies
Note: In Gateway, all lands are within the LBPIA Operating Area
- Node Boundary
- Cem - Cemetery
- Existing Stormwater Management Facility
- Proposed Stormwater Management Facility
- Special Site Areas (See Special Site Policies)

SUBJECT LANDS

Amendment No. 95 Appealed to the Ontario Municipal Board

Appealed to the Ontario Municipal Board

Amendment No. 95 Appealed to the Ontario Municipal Board

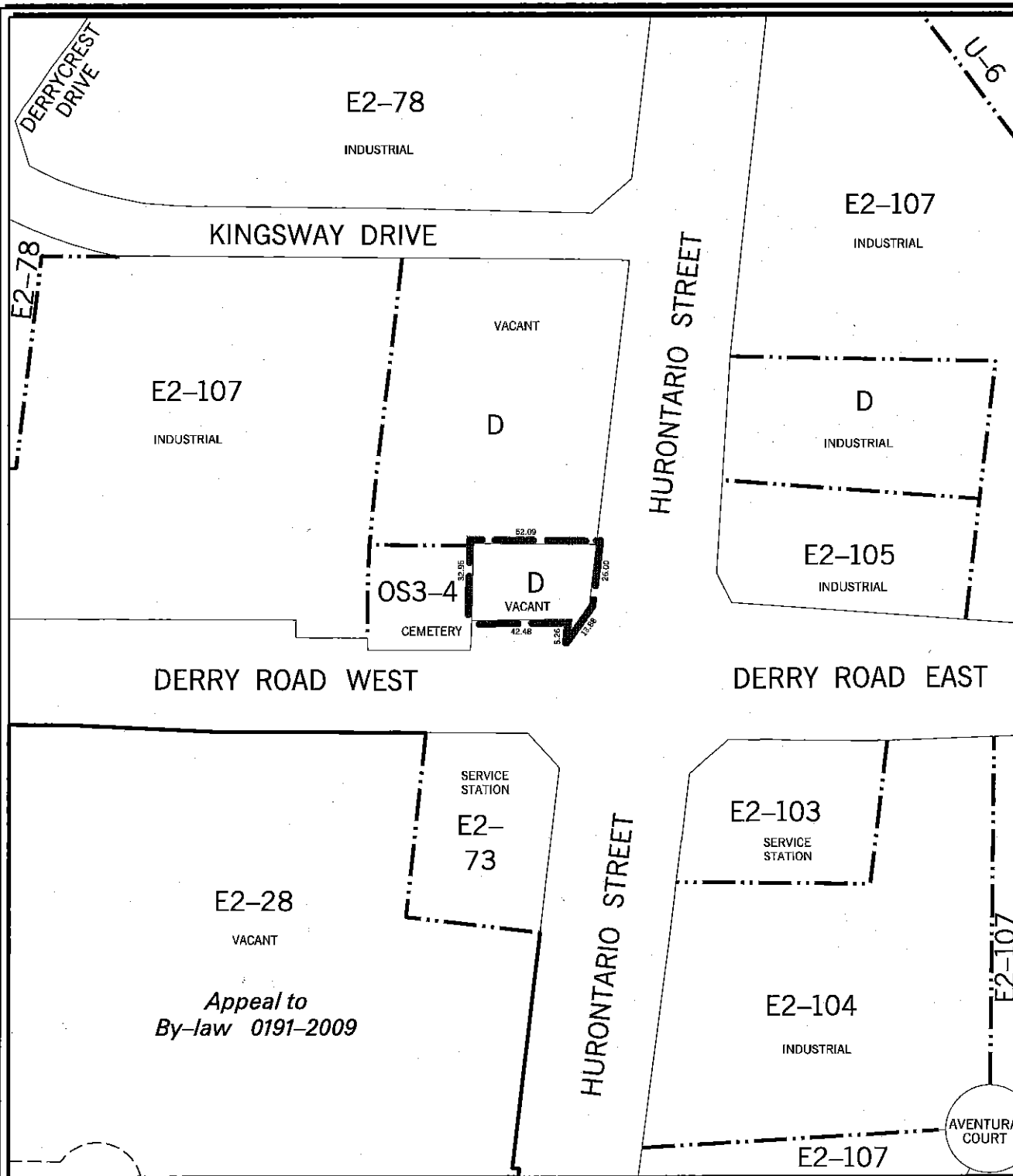
Delays and replace with Higher Order Transit Corridor

1996 NER/2000 NEF Composite Noise Contours

Planning District

SUBJECT:
ANTORISA INVESTMENTS LTD.

FILE NO: OZ 11/018 W5	APPENDIX I-3
DWG. NO: 11018R	
SCALE: 1:2500	
PDC DATE: 2012 09 04	
DRAWN BY: K. PROKOP	



LEGEND:

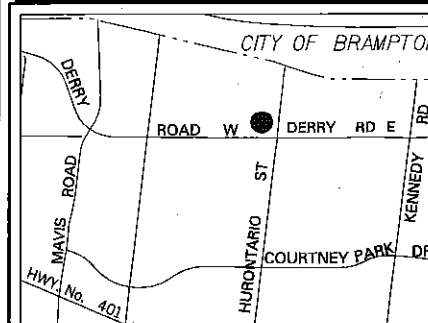


NOTE: EXISTING ZONING DELINEATED ON THE PLAN
PROPOSED ZONING INDICATED BY SHADING WITHIN
THE APPLICATION AREA.
THIS IS NOT A PLAN OF SURVEY.



SUBJECT:

ANTORISA INVESTMENTS LTD.



FILE NO:

OZ 11/018 W5

DWG. NO:

11018R

SCALE:

1:2500

PDC DATE:

2012 09 04

DRAWN BY:

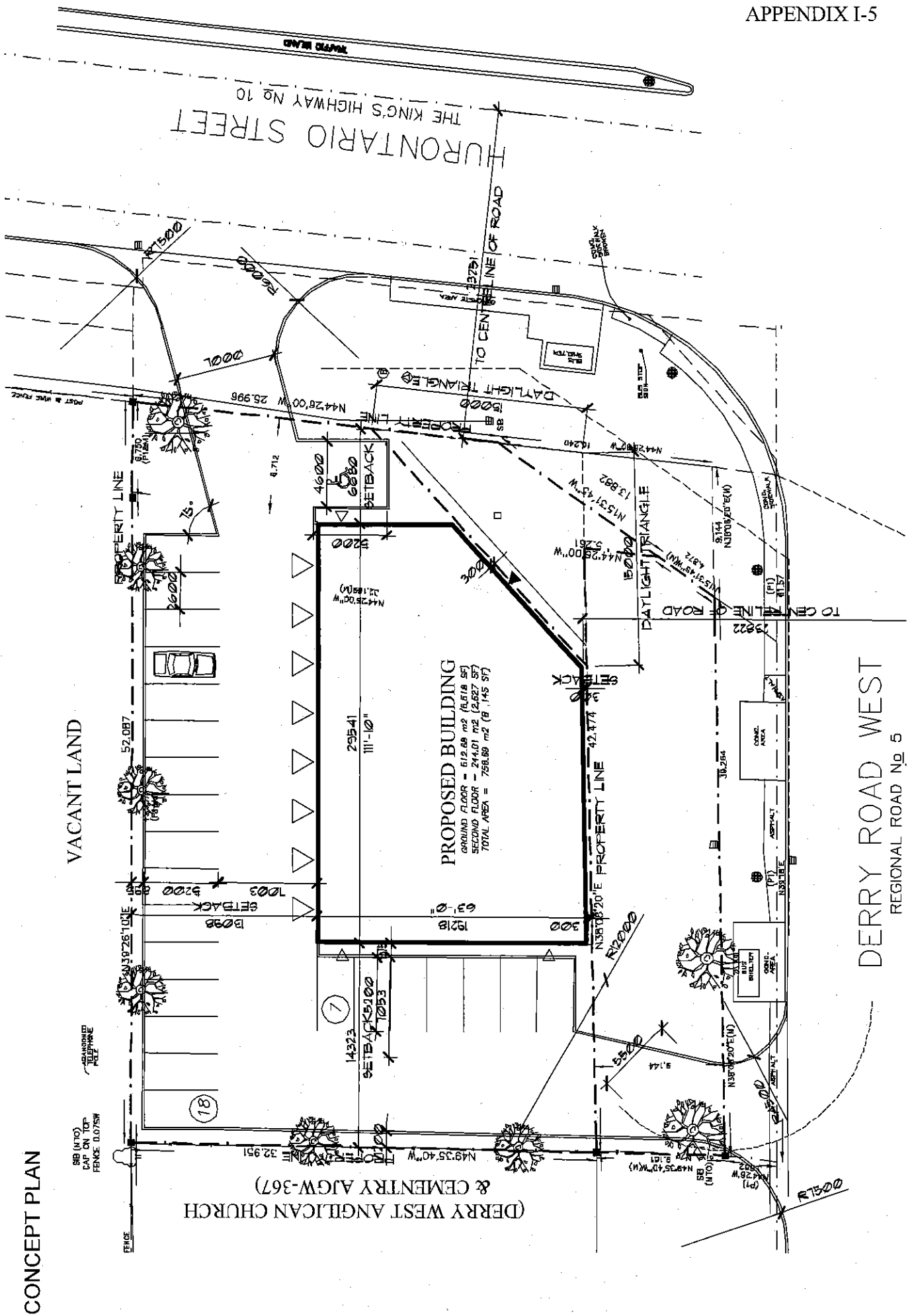
K. PROKOP

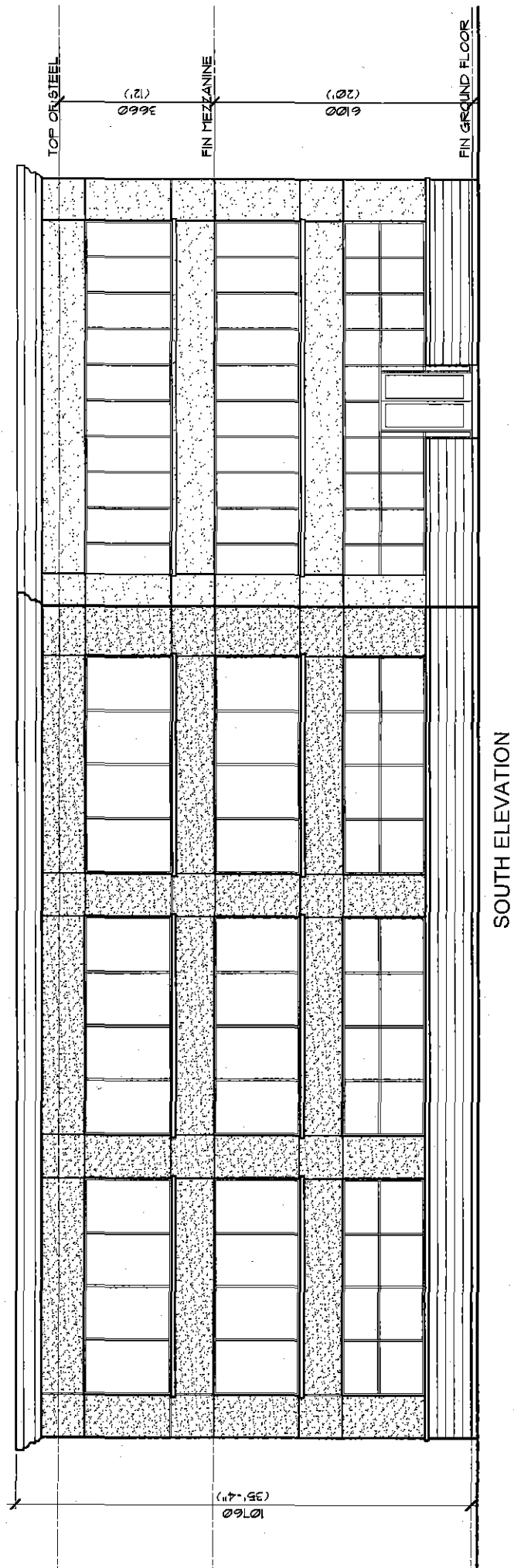
APPENDIX I-4

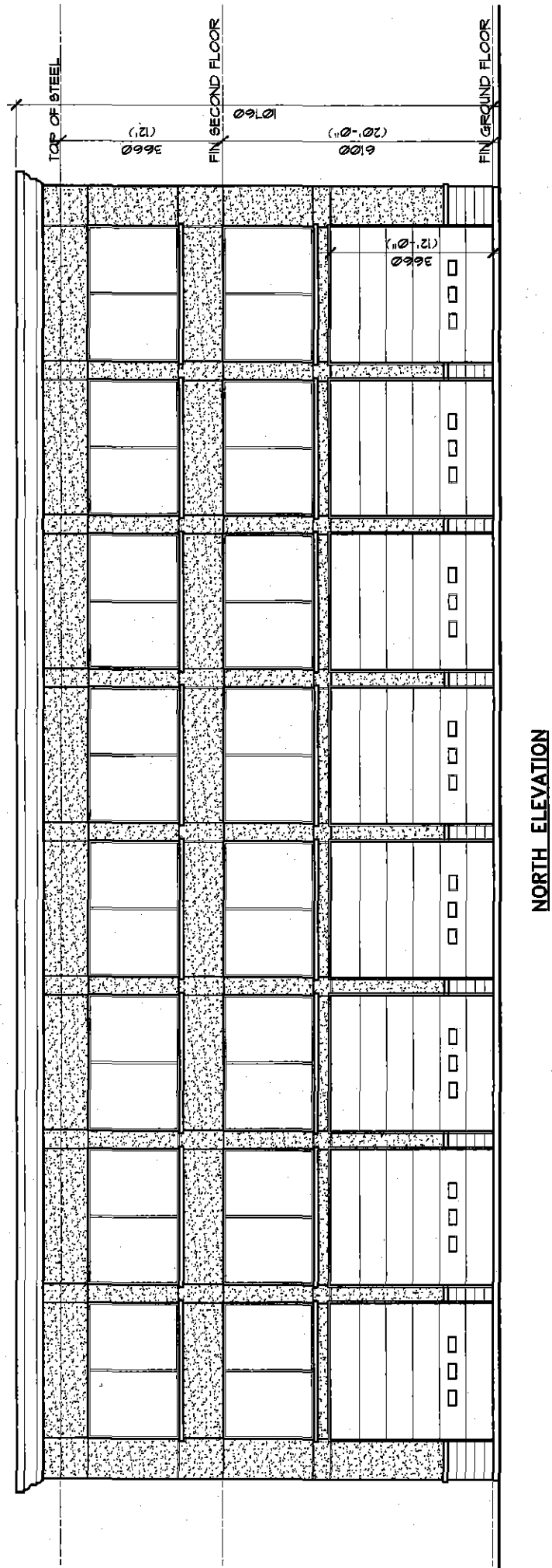


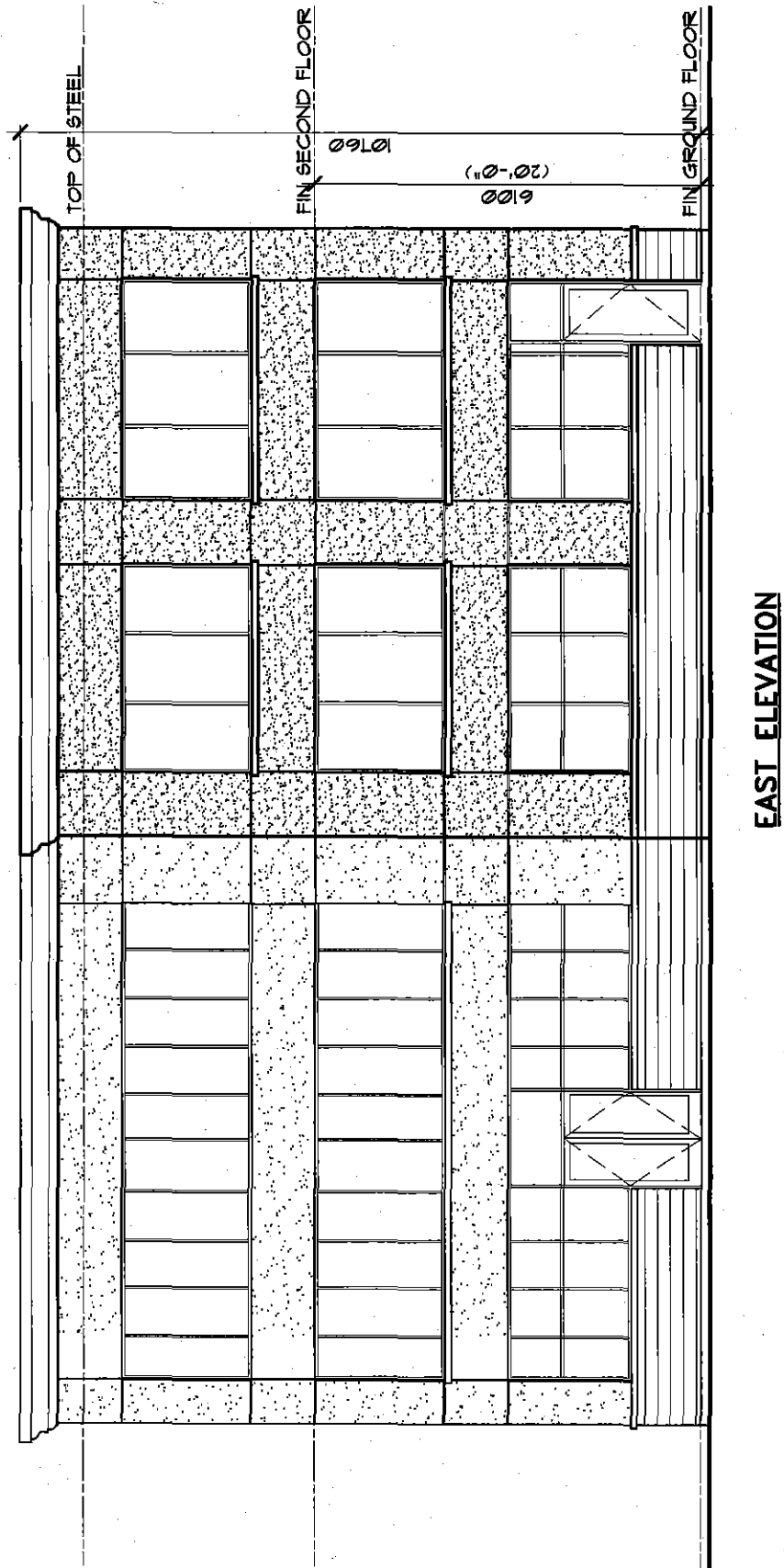
MISSISSAUGA
Planning and Building

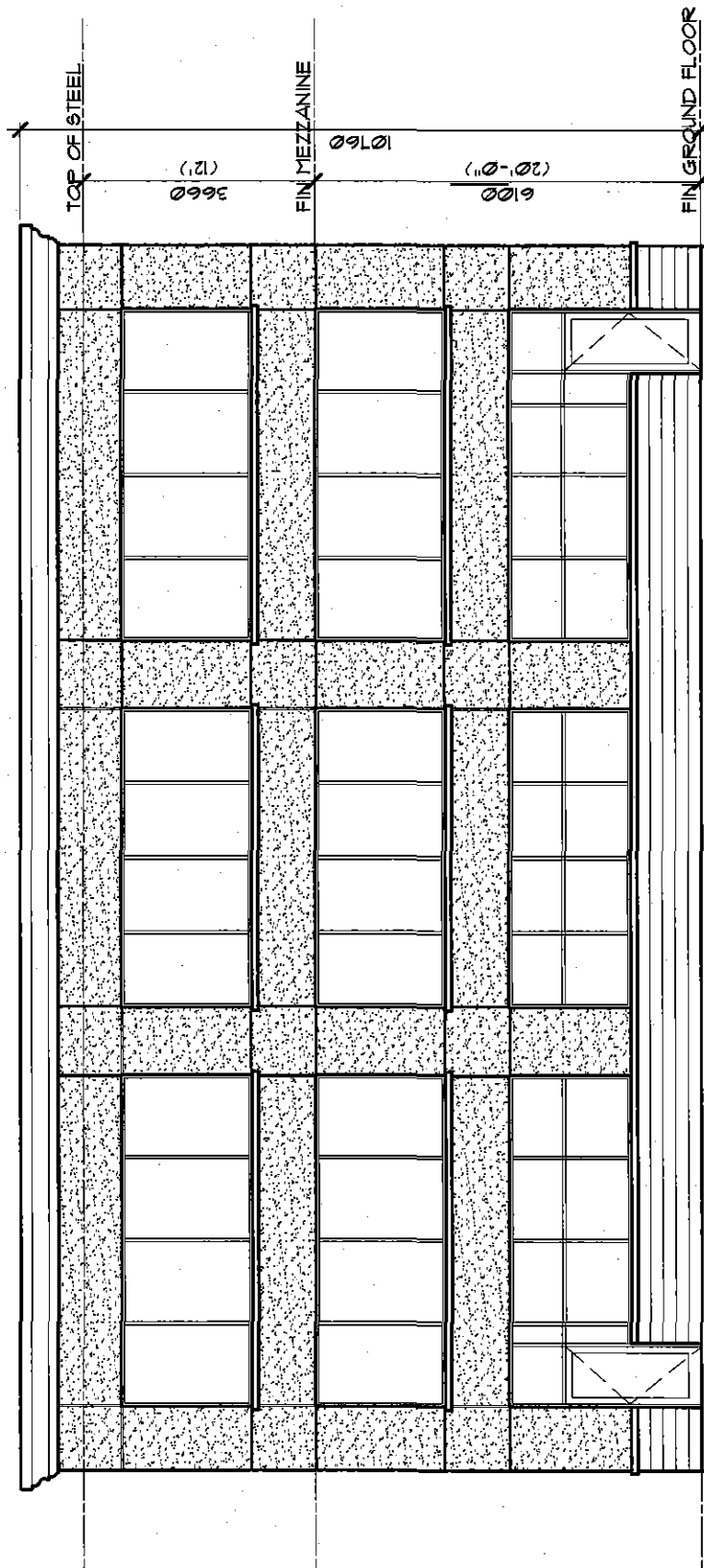
***Produced by
T&W, Geomatics***











WEST ELEVATION

Antorisa Investments Inc.

File: OZ 11/018 W5

Agency Comments

The following is a summary of comments from agencies and departments regarding these applications.

Agency / Comment Date	Comment
Region of Peel (April 25, 2012)	<p>The Traffic Engineering section has reviewed a Traffic Impact Review memorandum prepared by LEA Consulting Ltd. dated September 13, 2011. The intersection of Derry Road West and Hurontario Street has an approved Environmental Assessment which requires additional auxiliary lanes (specifically dual left turns and right turn lanes with channels in all directions) that will result in a reduced tangent curb line along Derry Road West and, consequently, affect the feasibility of a Derry Road West access point.</p> <p>Further, until such time as land requirements required to accommodate the Hurontario Main Street Study have been determined, and that any associated increase or reduction of curb frontage along Derry Road West can accommodate a vehicular entrance/exit at the westerly limits of the property without preventing the option for a westbound bus stop as close to the intersection as possible, the Region will not support a vehicular access point on Derry Road West. Should this proposal proceed with a site plan application, the Region will require a scoped traffic impact study including, but not limited to, a revised functional design assessing the feasibility of the Derry Road West access based on known property impacts at that time.</p> <p>The applicant is encouraged to pursue reciprocal access easements with properties to the north to gain access to the surrounding road network.</p> <p>The Region of Peel will be undertaking intersection improvements at Derry Road West and Hurontario Street. The Owner/Developer will be required to gratuitously convey additional lands above and beyond the Official Plan requirements to accommodate the intersection improvement works, including temporary and permanent easements.</p>

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Agency / Comment Date	Comment
	<p>The Region will require a daylight triangle and reserve at the intersection of Derry Road West at Hurontario Street.</p> <p>An existing 750 mm (30") diameter watermain is located on Derry Road and an existing 400 mm (16") diameter watermain is located on Hurontario Street. There is no existing municipal sanitary sewer to service this site. The closest existing sanitary sewer is a 250 mm (10") diameter sanitary sewer located on Kingsway Drive.</p> <p>A Storm water Management Report is required for our review to determine the affect of the proposal on the existing structures and drainage along the existing regional right-of-way.</p>
<p>City Community Services Department – Planning, Development and Business Services Division/Park Planning Section (March 7, 2012)</p>	<p>This Section notes that the subject property is adjacent to municipally owned Derry West Cemetery (P-407). As such, satisfactory arrangements regarding matters such as grading, tree preservation, hoarding and securities shall be made. Further, this Section notes that should these applications be approved, prior to the issuance of building permits, payment of cash-in-lieu of parkland dedication is required pursuant to Section 42 of the <i>Planning Act</i> (R.S.O.1990, c.P.13, as amended) and in accordance with the City's Policies and By-laws.</p>
<p>City Community Services Department – Culture Division (January 26, 2012)</p>	<p>The adjacent property, Derry West Cemetery, is designated under the <i>Ontario Heritage Act</i>. Accordingly, a Heritage Impact Statement is required. The submitted statement does not meet the terms of reference provided.</p>
<p>City Transportation and Works Department (May 8, 2012)</p>	<p>The Transportation and Works Department confirms receipt of a Site Plan, Stormwater Management Report, Site Servicing and Grading Plan, Traffic Impact Review and Environmental Site Assessment Phases 1 and 2.</p> <p>The applicant has been requested to provide a planning rationale letter indicating how the proposed development supports the proposed Hurontario Light Rail Transit as per the Hurontario/Main Street Corridor Master Plan adopted by Council.</p>

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Agency / Comment Date	Comment
	<p>Prior to the Supplementary Report meeting, the applicant has been requested to revise the plans to address certain grading concerns and encroachment issues.</p> <p>The applicant has also been requested to update the Stormwater Management Report, the Traffic Impact Review, and validate the Environmental Site Assessment Phases 1 and 2, dated August 2000.</p> <p>Further detailed comments/conditions will be provided prior to the Supplementary Report meeting pending receipt and review of the foregoing.</p>
City Arborist (February 24, 2012)	<p>The willow tree on the abutting Cemetery lands is adjacent to a very low lying parcel of land which is prone to wet conditions. The large willow is an asset in terms of water absorption. It is advisable, due to the tree's health and water absorption capabilities, to retain and prune this tree at the Developer's expense.</p>
Other City Departments and External Agencies	<p>The following City Departments and external agencies offered no objection to these applications provided that all technical matters are addressed in a satisfactory manner:</p> <p>City Community Services Department – Fire and Emergency City Economic Development Office Enersource Canada Post Rogers Cable</p>
	<p>The following external agencies were circulated the applications but provided no comments:</p> <p>Bell Canada City of Brampton Enbridge Greater Toronto Airport Authority (GTAA)</p>

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Mississauga Plan Policies

Gateway District – Special Site 2

4.15.6.3 The site is also subject to the Special Site 2 provisions of the Gateway District, which apply to the four corners of Hurontario Street and Derry Road East/Derry Road West and Hurontario Street and Courtneypark Drive East/Courtneypark Drive West. Notwithstanding the Business Employment designation and the Urban Design Policies in Section 4.15.3.2, the following additional policies will apply to lands located within Special Site 2:

- a. existing motor vehicle service station/gas bar sites at the southeast and southwest corners of Derry Road East/Derry Road West and Hurontario Street are recognized, but are encouraged to be redeveloped for other permitted uses;
- b. expansion of the existing motor vehicle service station/gas bar sites at the southeast and southwest corners of Derry Road East/Derry Road West and Hurontario Street will be permitted. As part of the expansion of the existing gas bar at the southeast corner of Derry Road East and Hurontario Street, a car wash will also be permitted.

The reconstruction or alteration of the existing car wash at the south-east corner of Hurontario Street and Derry Road East may be permitted if the proposal results in a visual or functional improvement of the site which achieves the intent and policies of the Gateway District Policies;

- c. accessory retail commercial uses will generally be limited to a maximum of 30% of the total Gross Floor Area (GFA).

Free-standing accessory retail commercial uses will not be permitted. Accessory retail commercial uses must be contained within the same building as the principal use;

- d. assembly of lands at the Hurontario/Derry intersection is encouraged;
- e. prior to development of the lands at the Hurontario/Derry intersection, an internal access concept will be prepared to the satisfaction of the Transportation and Works Department;
- f. these lands represent the principal intersections along the Hurontario corridor north of Provincial Highway 401 (Derry Road East/Derry Road West and Courtneypark Drive

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File: OZ 11/018 W5

East/Courtneypark Drive West). Development abutting the intersections should highlight these locations as focal points within the streetscape, given their high profile and visibility.

In addition to the Urban Design Policies in Section 4.15.3.2, these lands will be subject to the following:

- built form at the corners of the intersections should have prominence, occupy a majority of the streetline and be a minimum of three (3) storeys. The reconstruction of the service stations at the south east and south west corners of Hurontario Street and Derry Road East/West for motor vehicle commercial purposes may be permitted if it results in an improvement of the site by meeting the spirit and intent of this Plan by providing, for example, the massing, height and built form of a two (2) storey mezzanine building.
 - buildings with minimal frontal setbacks with active street-oriented elevations, main front doors and fenestration integrated with the streetscape;
- g. regard will be given to the design guidelines as outlined in the urban design manual entitled Upper Hurontario Corridor – a Design Mandate for Excellence during the processing of development applications.

Gateway District – Hurontario Street Corridor Development Policies

4.15.3.2 The purpose of these policies is to promote high quality urban design and built form. These policies are also intended to reinforce and enhance the image of Hurontario Street as the main north-south corridor through the City.

- a. Encourage a high quality urban design in the built form which is distinctive and urban in character, and which contributes to the identity of Hurontario Street as a principal City thoroughfare.
- b. Encourage a high standard of public and private realm streetscape design that is coordinated and comprehensive which includes street furniture, public art, building forecourts, open space, bus shelters, tree planting, and the sensitive location of utilities.
- c. Ensure buildings are street-related with pedestrian entrances, active building elevations, and fenestration forming an integrated link between the building and the sidewalk.

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- d. Encourage the development of a unique Hurontario Street character, and enhance its image through the creation of streetscape design, prominent intersections built form features, an integrated public and private realm and gateway features.
- e. Orient the most active and architecturally detailed building facaded to the public street by use of main entrances and a large percentage of fenestration addressing the streetscape.
- f. Locate parking facilities at the rear and/or side of buildings instead of between the front of the building and the public street.
- g. Design buildings with sufficient height, mass and width of street frontage to define and frame the street.
- h. Complete the road system to improve cyclist and pedestrian movement, vehicular and servicing access, and to create usable and accessible development parcels.
- i. Integrate the principal and the accessory uses, within individual buildings.
- j. Encourage the continued development of varied and innovative prestige buildings.
- k. Encourage development that provides a safe and convenient pedestrian environment that promotes the use of Hurontario Street as a major transit corridor.
- l. Minimize building setbacks from the streetline(s) while balancing continuous landscaping between the building and the street and pedestrian linkages to the public sidewalk.
- m. Encourage the appropriate transition of built form between buildings.
- n. Provide for safe, pleasant and convenient pedestrian movement from the public sidewalk and on-site parking area to the principal building entrance(s).
- o. Discourage the fragmentation of land parcels that will inhibit the eventual development of employment uses. Encourage land consolidation, in particular at the principal intersections to facilitate useable development parcels.

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File: OZ 11/018 W5

- p. Priority will be given to pedestrian movement when accommodating both pedestrian and vehicular traffic. Design efficient parking facilities to avoid circuitous routes and dead end aisles.
- q. Encourage built form (outside the gateway and main intersection areas) to incorporate a high level of physical continuity, cohesion and linkage between buildings, from block to block, and from street to street.
- r. Create a sense of prominence at the intersections of Hurontario Street, in addition to those subject to Special Site Policies, by integrating features such as, tall, more distinctive buildings located close to the street, unique landscape and streetscape treatment, elevated and distinguished rooflines.
- s. Internalize, screen and minimize visual impacts of the service and loading facilities from the streetscape(s), public view, pedestrian walkways, and abutting uses.
- t. The submission of a concept plan will be required for all development applications to demonstrate how the urban design policies will be implemented.
- u. Development applications will also have regard for the urban design guidelines in the urban design manual entitled Upper Hurontario Corridor – A Design Mandate for Excellence.

Mississauga Official Plan (2011) Policies

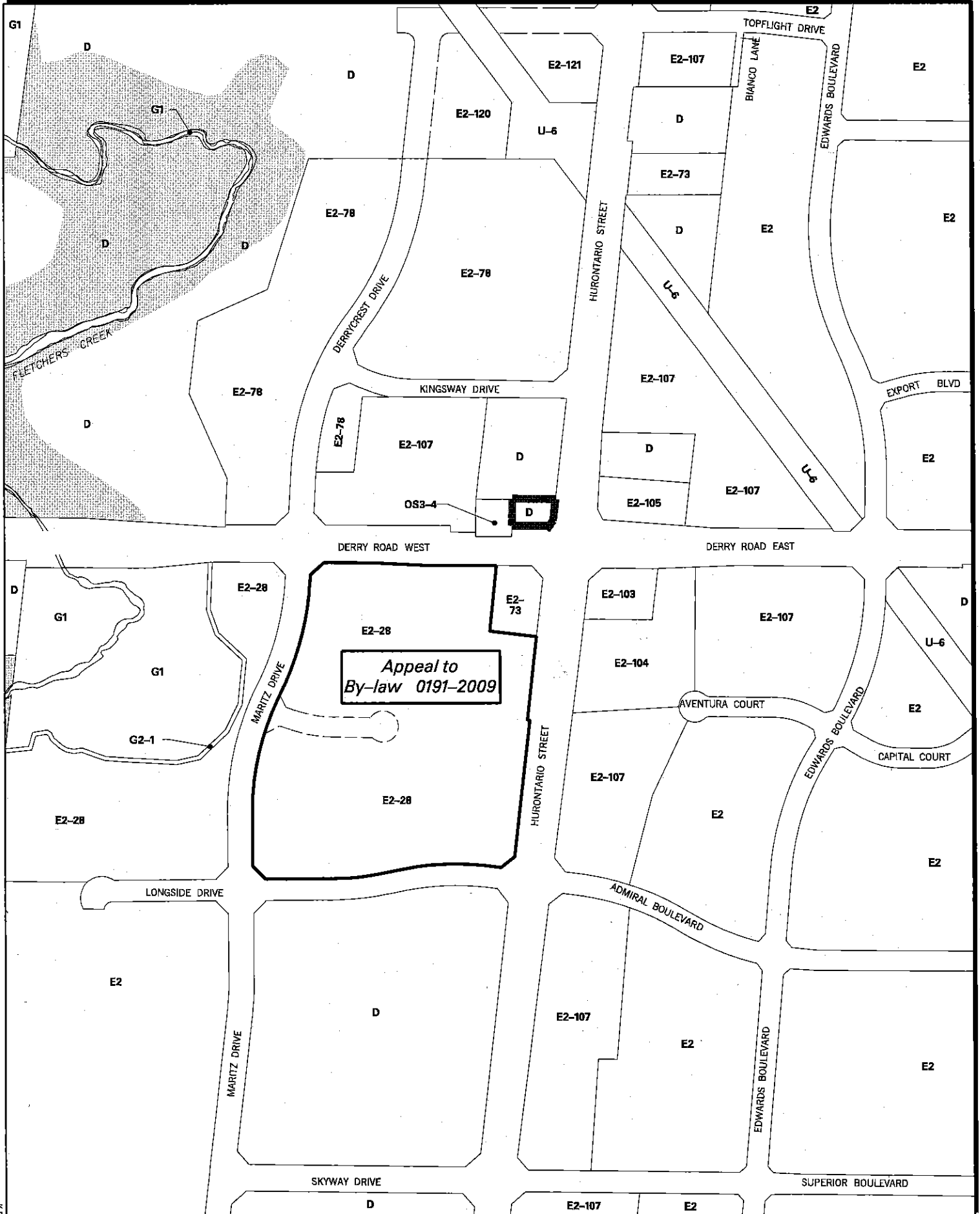
The language for the Special Site 2 and Urban Design Policies of the Gateway District in the Mississauga Plan, as outlined above, have been carried forward into the new Mississauga Official Plan under the Gateway Corporate – Special Site 1 policies (15.3.3.1) and Urban Design Policies (15.3.1).

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File: OZ 11/018 W5

Proposed Zoning Standards – "E2-Exception" (Employment)

	Required Zoning By-law Standard	Proposed Standard
Parking	33 spaces (2 designated for persons with disabilities)	23 spaces (1 designated for persons with disabilities)
Minimum Front Yard Setback	7.5 m (24.6 ft.)	0 m (0 ft.)
Minimum depth of a landscape buffer measured from any other lot line	4.5 m (14.8 ft.)	1.5 m (4.9 ft.)



Antorisa Investments Ltd.

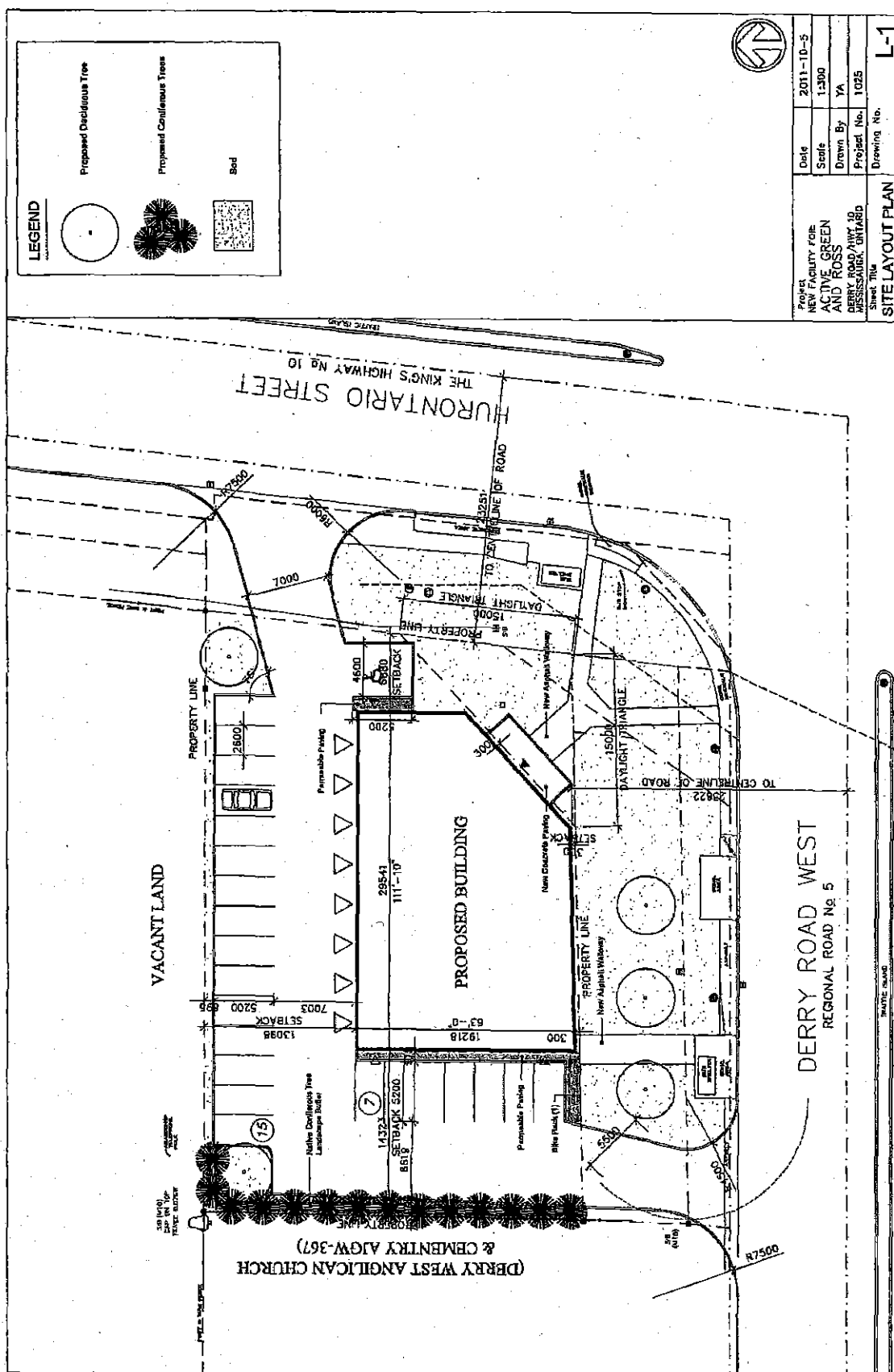
File: OZ 11/018 W5

Recommendation PDC-0052-2012

PDC-0052-2012

1. That the Report dated August 14, 2012, from the Commissioner of Planning and Building regarding the applications to amend Mississauga Plan from 'Business Employment - Special Site 2' to 'Business Employment - Special Site' and to change the Zoning from 'D' (Development) to 'E2 - Exception' (Employment), to permit a two storey motor vehicle repair facility under file OZ 11/018 W5, Antorisa Investments Inc., Part of Lot 11, Concession 1, W.H.S., designated as Parts 1 & 2, Plan 43R - 13493, be received for information.
2. That the correspondences expressing concern with respect to file OZ 11/018 W5 be received:
 - a. Email and attachments dated February 2, 2012 from Claudio Brutto
 - b. Letter dated September 4, 2012 from Carl Brawley, Glen Schnarr and Associates Inc.

Project NEW FACILITY FOR ACTIVE GREEN AND ROSS CREEK ROAD/AVENUE MISSISSAUGA, ONTARIO	Date 2011-10-5 Scale 1:300 Drawn By YA Project No. 1025 Drawing No.
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Antorisa Investments Ltd.

File: OZ 11/018 W5

Revised Proposed Zoning Standards – "E2-Exception" (Employment)

	Required Zoning By-law Standard	Proposed Standard
Parking	33 spaces (2 designated for persons with disabilities)	23 spaces (1 designated for persons with disabilities)
Minimum Front Yard Setback	7.5 m (24.6 ft.)	0.3 m (0.98 ft.)
Minimum Exterior Side Yard Setback	7.5 m (24.6 ft.)	0.3 m (0.98 ft.)
Minimum depth of a landscape buffer measured from a lot line that is a street line (Huronario Street)	4.5 m (14.8 ft.)	0.3 m (0.98 ft.)
(Derry Road)	4.5 m (14.8 ft.)	0.3 m (0.98 ft.)
Minimum depth of a landscape buffer measured from any other lot line (westerly side)	4.5 m (14.8 ft.)	1.5 m (4.9 ft.)
(north side)	4.5 m (14.8 ft.)	1.5 m (4.9 ft.)