

Heritage Advisory Committee

Date

2016/01/12

Time

9:30 AM

Location

Civic Centre, Council Chamber,
300 City Centre Drive, Mississauga, Ontario, L5B 3C1 Ontario

Members

Carolyn Parrish	Councillor - Ward 5
George Carlson	Councillor - Ward 11 (Chair)
Michael Battaglia	Citizen Member
Beth Bjarnason	Citizen Member
Robert Cutmore	Citizen Member
David Dodaro	Citizen Member
Lindsay Graves	Citizen Member
James Holmes	Citizen Member
Rick Mateljan	Citizen Member (Vice Chair)
Cameron McCuaig	Citizen Member
Paul McGuigan	Citizen Member
Matthew Wilkinson	Citizen Member

NOTE: To support corporate waste reduction efforts the large appendices in this agenda will not be printed. The appendices can be viewed at:
<http://www.mississauga.ca/portal/cityhall/heritageadvisory.ca>.

Contact

Mumtaz Alikhan, Legislative Coordinator, Legislative Services
905-615-3200 ext. 5425
mumtaz.alikhan@mississauga.ca

Find it online

<http://www.mississauga.ca/portal/cityhall/councilcommittees>

1. CALL TO ORDER
2. APPROVAL OF AGENDA
3. DECLARATION OF CONFLICT OF INTEREST
4. MINUTES OF PREVIOUS MEETING
5. DEPUTATIONS
6. MATTERS TO BE CONSIDERED
 - 6.1. Proposed Heritage Designation of 1130-40 Clarkson Road North (Ward 2)
 - 6.2. Request to Demolish a Heritage Listed Property: 49 Queen Street South (Ward 11)
 - 6.3. Heritage Advisory Committee and Related Staff Milestones: 2015 Year in Review
7. SUBCOMMITTEE UPDATES
 - 7.1. Report of the Meadowvale Village Heritage Conservation District Advisory Sub-Committee of January 5, 2016 (APPROVAL)
 - 7.2. Heritage Designation Sub-Committee
 - 7.3. Public Awareness Sub-Committee
8. INFORMATION ITEMS
9. OTHER BUSINESS
10. DATE OF NEXT MEETING – February 9, 2016 in Council Chamber, 2nd Floor, Civic Centre
11. ADJOURNMENT

City of Mississauga
Draft Minutes



Heritage Advisory Committee

Date

November 17, 2015

Time

9:30 a.m.

Location

Council Chamber

Members Present

Councillor George Carlson, Ward 11 (Chair)
Rick Mateljan, Citizen Member (Vice-Chair)
Councillor Carolyn Parrish, Ward 5
Michael Battaglia, Citizen Member
Elizabeth Bjarnason, Citizen Member
Robert Cutmore, Citizen Member
David Dodaro, Citizen Member
Lindsay Graves, Citizen Member
James Holmes, Citizen Member
Cameron McCuaig, Citizen Member
Paul McGuigan, Citizen Member

Members Absent

Matthew N. Wilkinson, Citizen Member

Staff Present

Mark Warrack, Manager, Culture and Heritage Planning
Paula Wubbenhorst, Senior Heritage Coordinator, Culture Division
Cecilia Nin Hernandez, Heritage Coordinator, Culture Division
Mumtaz Alikhan, Legislative Coordinator

Find it online

<http://www.mississauga.ca/portal/cityhall/heritageadvisory>

1. CALL TO ORDER – 9:30am

2. APPROVAL OF AGENDA

The Chair advised of an additional item on the agenda under Items for Information with respect to Committee of Adjustment applications.

APPROVED (L. Graves)

3. DECLARATIONS OF CONFLICT OF INTEREST

R. Mateljan expressed a conflict with Item 6.5 on the agenda as his company is involved in this matter.

4. APPROVAL OF PREVIOUS MINUTES

4.1 Minutes of Meeting held on September 15, 2015.

APPROVED (D. Dodaro)

5. DEPUTATIONS

5.1 Trail Development within Sawmill Valley Trail - J. Cranstone, MMM Group Ltd.

Jay Cranstone, Senior Landscape Architect, MMM Group Ltd., gave a PowerPoint overview of the Trail Development within Sawmill Valley Trail including the background of the Project, the existing conditions and the preferred route celebrating the natural and cultural features of the site.

In response to the Committee's question regarding the provision of parking, Mr. Cranstone noted that there is street parking permitted on the far side of the Sawmill Valley to access the area as well providing mapping to show generally where the site is.

The Committee thanked Mr. Cranstone for his informative deputation and agreed that it is important to preserve the site.

RECOMMENDATION

HAC-0058-2015

That the PowerPoint Presentation from Jay Cranstone, MMM Group Ltd., with respect to Trail Development within Sawmill Valley Trail to the Heritage Advisory Committee on November 17, 2015, be received for information.

RECEIVED (J. Holmes)

5.2 Barbertown Bridge over the Credit River - Michael Gusche, Project Manager, Park Development

Michael Gusche, Project Manager, Park Development, reviewed the very deteriorated condition of the Barbertown Bridge over the Credit River and noted three options that are available for the structure: rehabilitation, replacement or removal. He stated that from a heritage point of view, keeping the existing bridge and installing a self-supported deck is considered a preferred option as recommended by the Project's Environmental Assessment. Mr. Gusche advised that the next steps in the process, is to compile a report to submit to the Ministry of Environment of all the feedback received. A further update will be provided to the Committee.

The Committee noted its appreciation for the maintenance of the original structure.

RECOMMENDATION

HAC-0059-2015

That the PowerPoint Presentation from Michael Gusche, Project Manager, Park Development, Community Services, with respect to the Project's Environmental Assessment's recommended preferred option to preserve the Barbertown Bridge over the Credit River to the Heritage Advisory Committee, dated November 17, 2015, be received.

RECEIVED (Councillor C. Parrish)

5.3 Museums and Heritage Planning Strategic Plan Update - Marcus Letourneau, Letourneau Heritage Consulting

Marcus Letourneau, Heritage Planner, Letourneau Heritage Consulting, provided a PowerPoint briefing on preliminary directions and concepts of the Strategic Plan for Mississauga Museums and Heritage Planning Group. He outlined the purpose of the project is to help guide Mississauga's heritage framework and an innovative approach that will include heritage planning and museum functions. Mr. Letourneau noted that a number of focus group sessions have been held and what has been heard is that "Heritage" is a large, inclusive, abstract and multi-layered concept, but it's not well understood and respected. Amongst the preliminary directions for consideration impacting the City's Heritage Policies and Directions is to identify key themes that are important to Mississauga's Heritage.

A copy of the of Mr. Letourneau's presentation will be emailed to the Committee as requested.

Councillor Carlson stated that this is an important discourse on the future of Heritage in Mississauga in its interpretation, implementation, funding and new direction. He thanked Mr. Letourneau for his informative presentation.

RECOMMENDATION

HAC-0060-2015

That the PowerPoint Update from Marcus Letourneau, Letourneau Heritage Consulting, with respect to the Museums and Heritage Planning Strategic Plan to the Heritage Advisory Committee, dated November 17, 2015, be received.

RECEIVED (C. McCuaig)

6. **MATTERS TO BE CONSIDERED**

16.1 **Proposed Heritage Designation Amendment, Timothy Street House, 41 Mill Street (Ward 11)**

Corporate Report from Commissioner of Community Services dated October 27, 2015.

RECOMMENDATION

HAC-0061-2015

1. That the Designation Bylaw 521-77 designating the property known as the Timothy Street House located at 41 Mill Street (Ward 11) be amended, per Section 30.1(1) of the *Ontario Heritage Act*, for its physical/design, historical/associative, and contextual value, and that the appropriate City officials be authorized and directed to take the necessary action to give effect thereto.
2. That if there are objections to the amendment of Designation Bylaw 521-77, City Council direct the City Clerk to refer the matter to the Conservation Review Board.

APPROVED (Councillor C. Parrish)

6.2 **Request to Demolish a Heritage Listed Property – 1190 Mona Road (Ward 1)**

Corporate Report from the Commissioner of Community Services dated October 29, 2015:

RECOMMENDATION

HAC-0062-2015

That the property at 1190 Mona Road (Ward 1), which is listed on the City's Heritage Register, is not worthy of heritage designation and consequently, that the owner's request to demolish proceed through the applicable process.

APPROVED (P. McGuigan)

- 6.3 Request to Alter a Designated Property – 906 Whittier Crescent (Ward 2)
Corporate Report from the Commissioner of Community Services dated October 29, 2015:

RECOMMENDATION

HAC-0063-2015

That the property at 906 Whittier Crescent (Ward 2), which is listed on the City's Heritage Register, is not worthy of heritage designation and consequently, that the owner's request to demolish proceed through the applicable process.

APPROVED (L. Greaves)

- 6.4 Request to Demolish a Heritage Listed Property – 3056 McNaughton Avenue (Ward 5)
Corporate Report from the Commissioner of Community Services dated October 29, 2015.

RECOMMENDATION

HAC-0064-2015

That the property at 3056 McNaughton Avenue (Ward 5), which is listed on the City's Heritage Register, is not worthy of heritage designation and consequently, that the owner's request to demolish proceed through the applicable process from the Planning Department.

APPROVED (Councillor C. Parrish)

R. Mateljan left the meeting for Item 6.5 below due to a conflict of interest.

- 6.5 Request to Demolish a Heritage Listed Property – 25 Queen Street South (Ward 11)
Corporate Report from the Commissioner of Community Services dated October 29, 2015.

RECOMMENDATION

HAC-0065-2015

That the property at 25 Queen Street South (Ward 11), which is listed on the City's Heritage Register, is not worthy of heritage designation and consequently, that the owner's request to demolish proceed through the applicable process.

APPROVED (Councillor C. Parrish)

At this point, R. Mateljan returned to the meeting.

6.6 2016 Heritage Advisory Committee Meeting Dates

Memorandum from Mumtaz Alikhan, Legislative Coordinator, dated October 27, 2015 entitled *2016 Heritage Advisory Committee Meeting Dates*.

RECOMMENDATION

HAC-0066-2015

That the Memorandum dated October 27, 2015 from Mumtaz Alikhan, Legislative Coordinator, entitled *2016 Heritage Advisory Committee Meeting Dates*, be received for information.

RECEIVED (J. Holmes)

7. SUBCOMMITTEE UPDATES

7.1 Heritage Designation Subcommittee

C. McCuaig reported verbally that the Subcommittee has met and provided input to staff and to the Consultants conducting the Heritage and Museums Planning Strategic Plan.

7.2 Public Awareness Subcommittee

There was no update from the Subcommittee.

9. INFORMATION ITEMS

(a) Committee of Adjustment Applications within the Old Port Credit Village HCD: 42 Front Street South and 43 John Street South

Memorandum dated November 16, 2015 from Cecilia Nin Hernandez, Heritage Coordinator.

RECOMMENDATION

That the Memorandum dated November 16, 2015 from Cecilia Nin Hernandez, Heritage Coordinator, with respect to the Committee of Adjustment Applications within the Old Port Credit Village Heritage Conservation District for properties located at 42 Front Street South and 43 John Street South, be received for information.

Received (D. Dodaro)

OTHER BUSINESS

DATE OF NEXT MEETING – Tuesday, January 12, 2016 at 9:30 a.m., Council Chamber

ADJOURNMENT – 10:52 am

City of Mississauga

Corporate Report



Date: October 29, 2015 To: Chair and Members of Heritage Advisory Committee From: Paul Mitcham, P. Eng, MBA, Commissioner of Community Services	Originator's files:
	Meeting date: 2015/11/17

Subject

Proposed Heritage Designation 1130-40 Clarkson Road North (Ward 2)

Recommendation

1. That the property at 1130-40 Clarkson Road North be designated under the *Ontario Heritage Act* for its physical/design, historical/associative and contextual value and that the appropriate City officials be authorized and directed to take the necessary action to give effect thereto.
2. That, if there are objections to the designation, City Council direct the City Clerk to refer the matter to the Conservation Review Board.

Background

The subject property contains three structures. From north to south, these include the William Clarkson House, the former Post Office and the Clarkson store. Village namesake Warren Clarkson built the original store here in the nineteenth century. His son William built the existing mid-nineteenth century house. The general store included postal services from 1875 until 1947 when Henry Gerhardt, a subsequent owner, built the separate post office building. The village of Clarkson originated around this nucleus.

The property is currently listed on the City's Heritage Register. The Culture Division commissioned Underman McPhail Associates to conduct a Cultural Heritage Evaluation Report to determine if the property merits designation under the *Ontario Heritage Act*. The report is attached as Appendix 1. It includes a full history of the property.

Comments

To merit designation under the *Ontario Heritage Act*, a property must meet the criteria for determining cultural heritage value or interest, Regulation 9/06. A property must have

physical/design, historical/associative and/or contextual value to merit designation. (The full regulation is included in Appendix E of the attachment.)

After a full analysis, Unterman McPhail concludes that the property meets these criteria as follows (in summary):

The property has physical/design value as the store is a rare example of rural commercial stores of its age in the municipality and the house is “representative of a vernacular style dwelling of wood frame construction with Gothic Revival characteristics.”

The property has historical/associative value because it has “direct associations with the historical theme of the settlement of the former Toronto Township and the village of Clarkson.” It yields information that “contributes to an understanding of the evolution of the cultural landscape associated with the history of the rural village of Clarkson.” It is a “physical reminder of the history and presence of the historical settlement of Clarkson.”

The property has contextual value because it is “important in maintaining the physical evidence of the historical and rural community of Clarkson in the surrounding urban landscape.” The property is “important in defining, maintaining and supporting the character of Clarkson as individual buildings and as a discrete cultural heritage landscape that is physically, functionally and historically linked to its surroundings.” The property is a landmark as it is “a visual reminder of the historical centre of the village of Clarkson.”

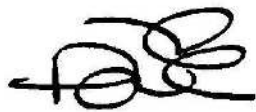
As such, Heritage Planning staff recommend that the property be designated under the *Ontario Heritage Act* as per the Statement of Cultural Heritage Value, which includes heritage attributes to be protected, included in the report.

Conclusion

The subject property is the birthplace of Clarkson, one of Mississauga’s founding villages. The property has physical/design, historical/associative and contextual value. As such, it merits cultural heritage designation under the *Ontario Heritage Act*.

Attachments

Appendix 1: Cultural Heritage Evaluation Report



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: P. Wubbenhorst, Senior Heritage Coordinator

**CULTURAL HERITAGE EVALUATION REPORT
1130-1140 CLARKSON ROAD NORTH
MISSISSAUGA, ONTARIO**



**Clarkson Road North
[Mississauga Library System
Historic Images Gallery, PH3490].**

**Prepared for:
Culture Division
City of Mississauga**

October 2015

Prepared by:



**Cultural Heritage Evaluation Report
1130-1140 Clarkson Road North
Mississauga, Ontario**

**Prepared for:
Culture Division
City of Mississauga**

October 2015

**Prepared by:
Unterman McPhail Associates
Heritage Resource Management Consultants
540 Runnymede Road
Toronto, ON M6S 2Z7
Tel: 416-766-7333**

Executive Summary

The subject property is located on Lot 67, Plan G-13, formerly part of Lot 28, Concession 2 SDS, Toronto Township at the municipal address of at 1130-1140 Clarkson Road North, City of Mississauga. Both the municipal addresses of 1130 and 1140 Clarkson Road North are listed on the City's Heritage Register, but not designated under the OHA.

The subject property, in its entirety, functions as a physical, historical and contextual reminder that clearly identifies the historical community core of the former Clarkson village.

This Cultural Heritage Evaluation Report (CHER) has satisfied the heritage evaluation criteria of 'Ontario Regulation 9/06.' It recommends the subject property at 1130-1140 Clarkson Road North, known as the Clarkson Property, be municipally designated, in its entirety, under Part IV the Ontario Heritage Act for its demonstrated design/physical value; its historical/associative value, and its contextual value.

TABLE OF CONTENTS

	Page
Executive Summary	
1.0 INTRODUCTION	1
1.1 Purpose of Report	1
1.2 Location and Property Description	1
1.3 Methodology and Report Format	4
2.0 HISTORICAL SUMMARY	4
2.1 Toronto Township	4
2.2 Village of Clarkson	6
2.3 1130-1140 Clarkson Road North	9
2.3.1 Clarkson Family (1819-1901 and 1907-1919)	9
2.3.2 George Gooderham Sr. (1900-1907)	14
2.3.3 Edith Maud Clarkson (1907-1919)	15
2.3.4 Harold U. Hare (1921-1925) and Jessie Jamieson (1925-1944)	16
2.3.5 W.T. McCord and Carload Groceteria (1926-1937)	17
2.3.6 1937 to 1978	18
2.3.7 1978 to Present	19
3.0 DESCRIPTION OF THE CULTURAL HERITAGE LANDSCAPE	19
3.1 Introduction	19
3.2 Area Context	19
3.2.2 Adjacent Heritage Properties	23
3.3 Site Description	24
4.0 DESCRIPTION OF THE BUILT HERITAGE RESOURCES	25
4.1 Introduction	25
4.2 Clarkson Store, Exterior	26
4.2.1 Modifications	27
4.3 Former Post Office Building, Exterior	30
4.3.1 Modifications	31
4.4 William Clarkson House, Exterior	31
4.4.1 Modifications	32
4.5 Comparative Analysis	33
5.0 EVALUATION UNDER “CRITERIA FOR DETERMINING CULTURAL HERITAGE VALUE OR INTEREST”, ‘ONTARIO REGULATION 9/06’	34
5.1 Introduction	34
5.2 Design/Physical Value	34
5.3 Historical/Associative Value	36
5.4 Contextual Value	39
5.5 Summary	42

TABLE OF CONTENTS **(Continued)**

	Page
6.0 STATEMENT OF CULTURAL HERITAGE VALUE UNDER ‘ONTARIO REGULATION 9/06’	42
7.0 RECOMMENDATIONS	44
 SOURCES	
Appendix A: Building Chronology: Clarkson Store, Former Post Office Building & William Clarkson House	
Appendix B: Historical Maps, Photographs and Plan	
Appendix C: Cultural Heritage Landscape Photographs	
Appendix D: Built Heritage Resource Photographs	
Appendix E: “Criteria for Determining Cultural Heritage Value or Interest,” Ontario Heritage Act, ‘Ontario Resolution 9/06’	

LIST OF FIGURES

		Page
Cover	Clarkson Road North [Mississauga Library System, Historic Images Gallery, PH3490, undated].	
Figure 1.	A map of the location of the subject property at 1130-1140 Clarkson Road North, City of Mississauga [Google Maps 2015].	1
Figure 2.	A plan showing the location of 1130-1140 Clarkson Road North to the north of Lakeshore Road West within the City of Mississauga [Culture Division, City of Mississauga].	2
Figure 3.	An aerial view showing the location of the subject property at 1130-1140 Clarkson Road North [City of Mississauga].	3
Figure 4.	A portion of Plan G-13 shows the outline of the Clarkson Store and the William Clarkson House in 1913 [Peel Land Registry Office, Plan of Subdivision of Part Lots 28, 29 and 30, Con. II, S.D.S., Twp. of Toronto, Co. Peel, Speights and van Nostrand, O.L. Surveyors, Toronto, May 15, 1913].	15
Figure 5.	An aerial view of the subject property at 1130-1140 Clarkson Road North and nearby properties listed on the City's Municipal Heritage Inventory [Google Maps 2015].	20
Figure 6.	A pre-1906 view northwest to the Clarkson Store showing the former railway station on the north side of the tracks [MLS, Historic Images Gallery, BA0155, undated].	22
Figure 7.	A view northwest from the railway tracks to the Clarkson Store to the Clarkson Store and the former Post Office Building [July 2015].	22
Figure 8.	A streetscape view looking south on Clarkson Road North to the Clarkson Store (right), the railway crossing, and the former Alex Durie Store and Clarkson Basket Factory building site to the south of the railway tracks (left) [MLS, Historic Images Gallery, HA0039, 1910].	22
Figure 9.	A streetscape view south on Clarkson Road North to the William Clarkson House, the view northwest from the railway tracks to the Clarkson Store and the former Post Office Building and the store (right) and the railway crossing [July 2015].	22
Figure 10.	A streetscape view to the Clarkson Store (left) located immediately north of the railway line with the former Alex Durie store and Clarkson Basket Factory site (right) located at 1119 Clarkson Road North [MLS, Historic Images Gallery, PH3490, undated].	22
Figure 11	A streetscape view north on Clarkson Road North with the Clarkson Store (left) north of the railway line and (right) the former Alex Durie Store and Clarkson Basket Factory at 1119 and Auld Butchers at 1109 Clarkson Road North to the south of the railway [July 2015].	22
Figure 12.	A streetscape view southwest on Clarkson Road North to Carload's Groceteria [Clarkson Store] showing the former Post Office Building with a flat roof building to the north angled parking in front [MLS, Historic Images Gallery, BA0075, undated].	23

LIST OF FIGURES (Continued)

	Page
Figure 13. A view southwest on Clarkson Road North showing the addition of a gable roof to the former Post Office Building and the angle parking area in front of the store and post office [July 2015].	23
Figure 14. An aerial view shows the subject property at 1130-1140 Clarkson Road North [GoogleMaps 2015].	25
Figure 15. A view showing the front (east) elevation of the Clarkson Store [July 2015].	26
Figure 16. A view of the Clarkson Store including the post office prior to the 1906-1910 renovations, [MLS, Historic Images Gallery, HA0040, undated].	28
Figure 17. A view of northwest to the Clarkson store in 1904 showing its proximity to the railway station platform [MLS, Historic Images Gallery].	28
Figure 18. Edith Clarkson and Cora Clarkson in front of the Clarkson Store [MLS, Historic Images Gallery, HA0041, undated].	29
Figure 19. A view of the E.M. Clarkson Store and Post Office after the 1906-1910 renovations [Frost Postcard collection, 1991.028.315, PAMA, photo PN2014_04299].	29
Figure 20. A view of the interior of the E.M. Clarkson Store and Post Office circa 1910 [MLS, Historic Images Gallery, BA0062, 1910].	29
Figure 21. A view of the interior view of the Clarkson Store and post office [MLS, Historic Images Gallery, BA0061, 1910].	29
Figure 22. The front elevation of the Carload Groceteria [Clarkson Store] on Clarkson Road North [MLS, Historic Gallery, BA0074, undated].	29
Figure 23. The front (east) elevation of the Clarkson Store [Culture Division, Heritage Services Files, 1980s].	29
Figure 24. A view of the front (east) elevation of the former Post Office building located between the Clarkson Store and the William Clarkson House [July 2015].	30
Figure 25. The front (east) elevation of the William Clarkson House [July 2015].	31
Figure 26. An early 20 th century view of the front (east) elevation of the William Clarkson House [MLS, Historic Image Gallery, BA0154, undated].	32
Figure 27. The front (east) elevation of the William Clarkson House [MLS, Historic Images Gallery, J771, William Clarkson House, Clarkson, 1980].	32
Figure 28. The front (east) elevation of the William Clarkson House [Heritage Mississauga, 2005].	32
Figure 29. A view to the north elevation of the William Clarkson House [September 2014].	32

1.0 INTRODUCTION

1.1 Purpose of Report

The City of Mississauga, Community Services Department, Culture Division, has requested the evaluation criteria under 'Ontario Regulation 9/06' be applied in the preparation of a Cultural Heritage Evaluation Report (CHER) for the property located at the municipal address of 1130-1140 Clarkson Street North. The CHER is to provide the City with a Statement of Cultural Heritage Value for consideration in municipally designating the subject property under Part IV of the *Ontario Heritage Act* (OHA).

Currently, the municipal addresses of 1130 and 1140 Clarkson Road North are listed on the City's Municipal Heritage Register as cultural heritage resources. Both municipal addresses include the following three buildings,

- the Clarkson Store,
- the former Post Office Building, and
- the William Clarkson House.

1.2 Location and Description

The legal description of the subject property is Part Lot 67, Plan G13, formerly the westerly Part of Lot 28, Concession 2 SDS (South of Dundas Street). It is located at 1130-1140 Clarkson Street North, north of Lakeshore Road West on the northwest corner of the CN/GO Transit rail corridor (**Figures 1, 2 and 3**).

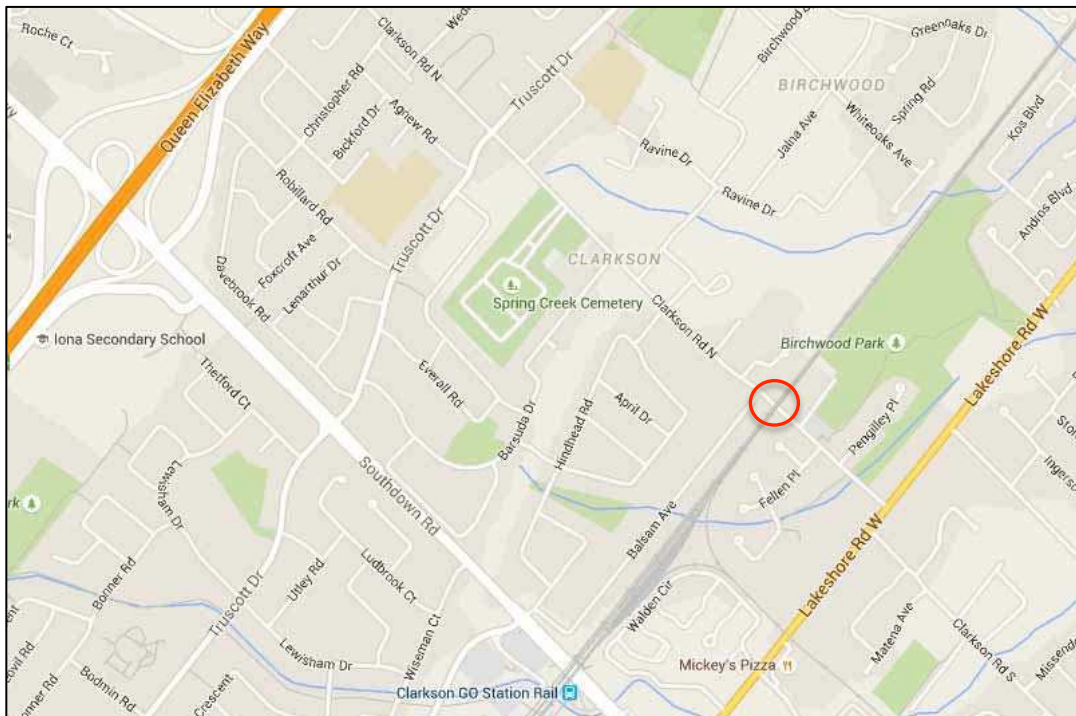


Figure 1. A map of the location of the subject property at 1130-1140 Clarkson Road North, City of Mississauga [Google Maps 2015].

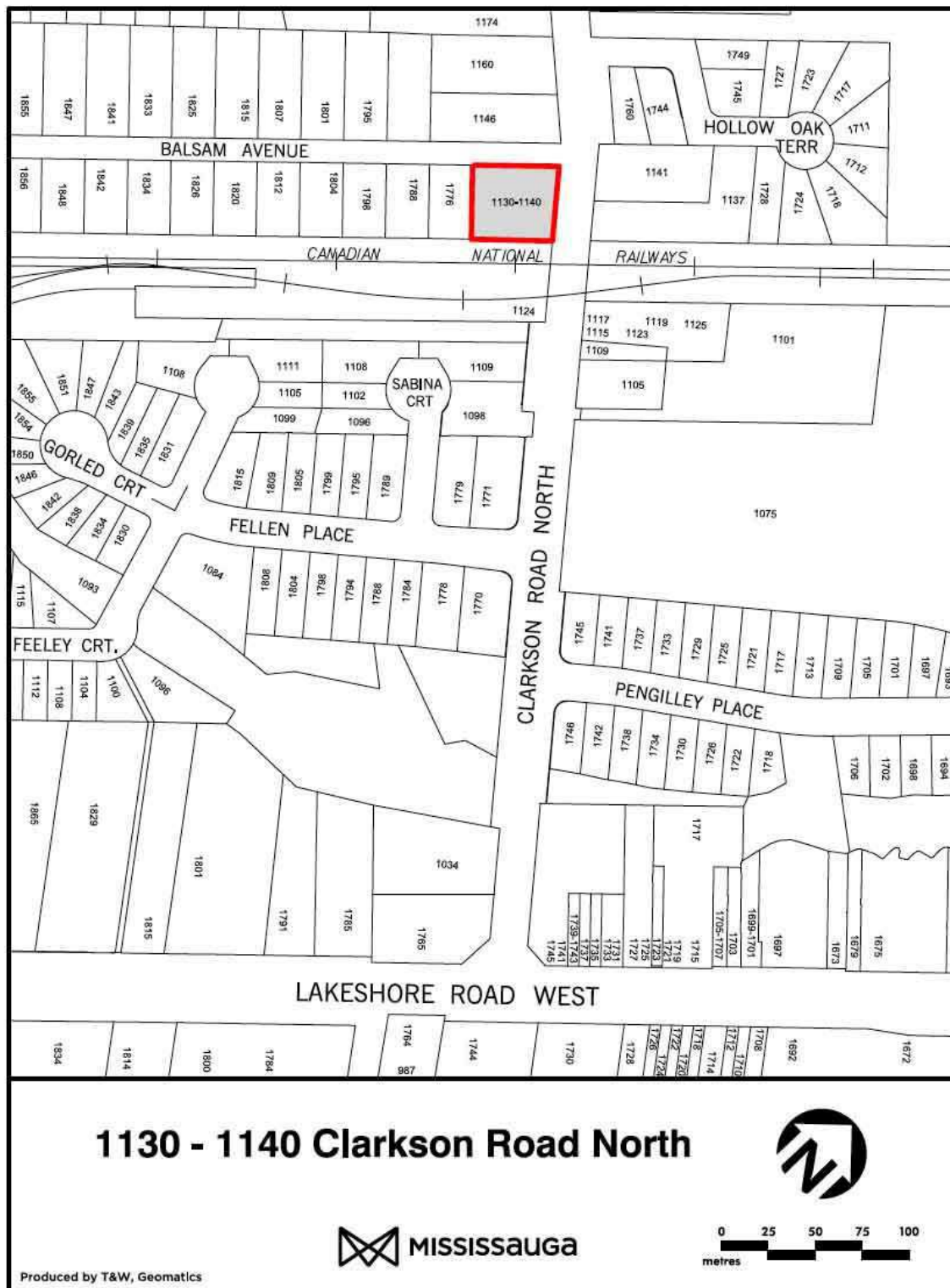


Figure 2. A plan showing the of 1130-1140 Clarkson Road North to the north of Lakeshore Road West [Culture Division, City of Mississauga].



Figure 3. An aerial view showing the location of 1130-1140 Clarkson Road North [City of Mississauga].

1.3 Methodology and Report Format

The “Criteria for Determining Cultural Heritage Value or Interest” set out in the ‘Ontario Regulation (O. Reg.) 9/06’ under the *Ontario Heritage Act* (OHA) has been used to determine the cultural heritage value or interest of the subject property for this CHER. The criteria are contained in *Appendix C*.

Section 1 of this CHER includes an introduction. A summary of the historical development of the subject property is found in Section 2. The cultural heritage landscape and built heritage resource descriptions are provided in Sections 3 and 4, respectively. The evaluation process is located in Section 5, while the ‘O.Reg. 9/06’ Statement of Cultural Heritage Value is contained in Section 6. Mitigation recommendations are presented in Section 7. Historical maps and photographs are included in Appendix A. Photographs of the cultural heritage landscape and the built heritage resources are found in Appendix B and Appendix C, respectively. Appendix D contains ‘O. Reg. 9/06’.

2.0 HISTORICAL SUMMARY

2.1 Toronto Township

Administratively, Toronto Township was located in the District of Nassau before becoming part of the Home District in 1791. It was formed and named on August 2, 1805, when officials from York purchased an area extending from Burlington Bay to the Etobicoke Creek from the New Credit First Nation (Mississaugas). With this “First Purchase”, the Mississaugas retained some fishing rights and one mile of land on either side of the Credit River. This area became known as the Old Survey. Samuel Wilmot completed the survey of the southern half, or the “Old Survey,” of Toronto Township in 1806. The area was opened up for Euro-Canadian settlement, principally United Empire Loyalists, who had emigrated from the United States after the American War of Independence and received land grants from the British Government for their service and loyalty. The northern area, or “New Survey”, of Toronto Township was acquired by the British government in 1818 and surveyed in 1819.

The principal transportation roads in Toronto Township were opened in the first part of the 19th century. Dundas Street, also known as the Governor’s Road, was opened in the early 19th century as a principal military route between York and London. Lakeshore Road, also opened in the early 19th century, was a principal transportation route along the northern shore of Lake Ontario providing a link between York (Toronto) and settlements to its west. Hurontario Street, which divided the township into east and west parts, was opened soon after the township survey and served as an important north to south transportation route for the settlers in Toronto Township and further north. As settlement progressed, other township roads were opened until a grid like pattern of local roads provided access to all parts of the township.

Smith's *Canadian Gazetteer* (1846) describes Toronto Township as follows:

*"This is one of the best settled townships in the Home District: it contains a large portion of very excellent land, and a number of well cultivated farms. For from two to three miles from the lake the land is light and sandy, and the timber principally pine; afterwards, it becomes rolling, and the timber the best kinds of hardwood. The Rivers, Credit and Etobicoke both run through the township... There are four grist and twenty-one saw mills in the township. Population in 1842: 5,377."*¹

Tremaine's Map (1859) shows the southern part of the township in the "Old Survey" had developed into an agricultural landscape with established hamlets and villages and a local road system. The Hamilton & Toronto Railway, part of the Great Western Railway, was constructed through the southern part of the Toronto Township in the mid-1850s with a station at Clarkson's store. The Grand Trunk Railway took over the line in 1882. By the 1870s, Toronto Township comprised agricultural land and fruit growing orchards outside of the numerous hamlets and villages. The south portion of the Toronto Township map in the *Illustrated Historical Atlas* (1877) shows a well-developed agricultural landscape dotted with numerous farmsteads, orchards, schools, churches, villages including Clarkson and an extensive local road system. In 1877, the township was described as follows:

*"Where were dense wildernesses and howling of the wild beasts are now beautifully cultivated farms and almost palatial residences. Toronto Township contains a large portion of arable land, the greater portion of which is rolling. The soil varies in quality, some portions of it being sandy loam, while others are stiff loam and clay... The River Credit runs through the western portion of the Township, and has proved a great source of wealth to its inhabitants, as it is not only a good watering stream, but there are endless mill privileges the whole length of the river, which has been largely utilized in this Township..."*²

At the end of the 19th century, the southern part of Toronto Township was a prosperous agricultural area specializing in fruit and vegetable gardens that supplied the more urban areas in the region.

The Grand Trunk Railway became the Canadian National Railway in 1923. Lakeshore Road remained a key route in Toronto Township, and served as the principal road from Toronto to Oakville, Burlington and Hamilton. It became the Toronto-Hamilton Highway and then Provincial Highway 2 in 1917. It continued as the main transportation route between Toronto and Hamilton until the opening of the Queen Elizabeth Way (QEW) in 1939.

¹ Wm. H. Smith, *Smith's Canadian Gazetteer* (Toronto: H. & W. Rowsell, 1846) 192-193.

² *Illustrated Historical Atlas of the County of Peel, Ont.* (Toronto: Walker & Miles, 1877) 60.

With the exception of the villages of Port Credit and Streetsville, the Township of Toronto was amalgamated in 1968 to form the Town of Mississauga. In 1974, when the town became a city, Port Credit and Streetsville were annexed by Mississauga. The City of Mississauga experienced its greatest growth in the 1980s and 1990s.

2.2 Village of Clarkson

Thomas Merigold acquired Lot 29, Concession 3 SDS, in 1809 and Lot 3, Concession 4 SDS, in 1818. The area was referred to as Merigold's Point for several years. Merigold's son-in-law Benjamin Monger bought Lot 30, Concession 2, in 1811. Monger hailed from Dutchess County, New York.

Land records indicate Peter Hess received the Crown Patent for all 200-acres of Lot 29, Concession 2 SDS, in 1815 and sold 120 acres to Malcolm Wright in the same year. His heir, Archibald Wright, sold part of Lot 29, Concession 2 SDS, to Warren Clarkson in February 1819. Clarkson built a house on the property. The Crown Patent for the 200-acres comprising Lot 28, Concession 2 SDS, was given to Frederick H. Jarvis in 1833; Jarvis sold 150 acres to Edgar Neave in the same year. Neave then sold 50 acres of land to Warren Clarkson in August 1835, on which Clarkson built his store.

From about the mid-1830s stagecoaches from Toronto travelled to Springfield, later Erindale, along Dundas Street and then south to the Lakeshore Road along a trail that became Clarkson Road. By 1835, Warren Clarkson owned land on Lots 28, 29, Concession 2 SDS, on either side of the wagon trail running north from Lakeshore Road. He had built a home c1819 on Lot 29, followed by a small store on the trail just north of his home on the westerly edge of Lot 28 c1835, providing the nucleus for the future village of Clarkson. The trail served as a stagecoach route and the store as a stagecoach stop. In 1845, Warren Clarkson and Captain James Harris donated land to open a given road along the wagon route. The road was named after Clarkson's store. Since Clarkson refused to move his store to accommodate for a straight right-of-way north from Lakeshore Road, the road jogged at the store.³ Traffic increased along the road in the late 1840s and in 1850, the Toronto Township Council officially opened it as a township road, now Clarkson Road.⁴

The Springcreek Cemetery was established c1827 on one acre of land on the northeast corner of Lot 20, Concession 2 SDS, on the west side of the trail that became the route for Clarkson Road. Its current municipal address is 1390 Clarkson Road North. The cemetery was originally named the Chambers Springcreek Grave Ground. Most of the Clarkson settlers are buried in this cemetery. In 1849, John Chambers sold the one acre cemetery property to Warren Clarkson and the cemetery trustees. Warren Clarkson and Nathaniel Hemphill were early cemetery trustees. The original name for the cemetery was used until the early 20th century; by 1907, the property was referred to as the

³ Kathleen A. Hicks, *Clarkson and Its Many Corners* (Mississauga, Ontario: Mississauga Library System, 2003) 53.

⁴ *Ibid.*, 52.

Clarkson Cemetery in the board records. It then became the Clarkson Springcreek Cemetery.⁵

In November 1853, the Hamilton & Toronto Railway Company, later the Great Western Railway, bought six acres of Warren Clarkson's property for a right-of-way, railway station and siding space. A small railway station, built on the north side of the line behind the Clarkson Store, was named Clarkson's. The first train travelled through the area in December 1855. The first train arrived at the Clarkson depot in December 1855.

Tremaine's Map (1859) shows the railway corridor through the Clarkson property (**Appendix A**). In the same year, Captain Edward Sutherland started a strawberry farm and industry at Clarkson, using the train to ship his produce. Clarkson became known as the strawberry capital of Ontario.⁶ The train service allowed the local fruit growing business to eventually expand into a packing, storing and shipping enterprise centred on the Clarkson's station in the late 19th century.

While the Clarkson family was farming and operating a store in the area, several upper-middle class homes were built in and around the village of Clarkson in the mid 19th century including Benares House.⁷ Edgar Neave began the construction of a house at Benares in 1836 before selling the extensive property and his unfinished house to Captain James and Elizabeth Harris. Benares House suffered a fire in 1855 and was rebuilt in 1857 at the current address of 1503 Clarkson Road North. Captain Harris' youngest son, Arthur, married Mary Magrath, the granddaughter of Reverend James Magrath of Erindale. The Harris family would have been acquainted with Warren and Susan Clarkson. A noted Clarkson resident from 1924 to 1928 was author Mazo de la Roche. As a summer neighbour of the Sayers/Harris family of Benares, she resided in "Trail Cottage" where she wrote her award-winning, best-selling novel *Jalna* about the fictional Whiteoaks family.

William Clarkson, who took over the management of the Clarkson Store from his father, built a house for his family, now the William Clarkson House, beside his store. Local history has suggested a mid 1860s construction date; however, it may have been constructed later in 1870s or around 1882. The *Illustrated Atlas* (1877) shows the postal station at Clarkson, which had opened in 1875 in the Clarkson Store (**Appendix A**). William's brother Henry Shook Clarkson also built a house on Clarkson Road North near the family store and house in the 19th century. By the end of the 19th century, Clarkson's village included, but was not limited to, the Clarkson Store and post office, the William Clarkson House, the house built by Warren and Susan Clarkson and a house owned by Henry Clarkson and Clarkson's railway station.

⁵ Ibid. 18.

⁶ Ibid., 61.

⁷ Built in 1857, Benares House is associated with the early settlement of the north shore of Lake Ontario and is one of the earliest settled sites in the village of Clarkson's Corner. Municipally designated under the OHA and protected by an OHT conservation easement, the historic museum reflects the way the Harris family of Clarkson lived in 1918. Considered to be the inspiration for the *Jalna* novels written by Mazo de la Roche, it is also the site of an Ontario Heritage Trust plaque.

In 1907, a larger railway station was built on the south side of the railway tracks to replace the earlier and smaller station on the north side. Alex Durie leased property from the Henry Clarkson family on the east side of Clarkson Road North south of the railway tracks in 1908. He moved his family into the building closest to the tracks and opened a store selling meat, groceries and fruit and vegetables. Durie delivered groceries locally starting in 1909 and his store became a neighbourhood meeting place for the local farmers. In 1915, Durie took over the telephone management in the village from Edith Clarkson and the switchboard was moved to his store.⁸ In 1923, he bought the store property from Mildred [Clarkson] Michell⁹, now 1119 Clarkson Road North. Durie sold the store to George Battersby in 1928; Charles Terry bought the store in 1932 from the Durie estate and the building became part of the Izatt's Basket Factory, also known as the Clarkson Basket Factory.

In 1913, Durie leased his other building to The Merchants Bank and a branch office was opened on Clarkson Road North on November 2, 1913. The bank became part of the Bank of Montreal in 1922; the branch office was closed in 1938. In 1940, Phillip Mitchell, husband of Mildred Clarkson, sold the building to James Pengilley; he sold it to Douglas Auld, who opened up Auld's Butcher Shop.¹⁰ The store, located at 1109 Clarkson Road North, closed in 2015 after 68 years of operation.

In 1936, the Clarkson Fire Insurance Plan shows the nucleus of the Clarkson community on Clarkson Road North and the railway tracks. It included the Clarkson Store and William Clarkson House at 49 (1130) and 53 (1140) Clarkson Road North, respectively, and Edith Clarkson House at 25 (1060) Clarkson Road North. The Clarkson/Paisley House was noted as 52 (1141) Clarkson Road North. The plan also shows several houses on Balsam Avenue including the current residence at 1776 Balsam Avenue. The railway station and the premises of the Harold U. Hare Coal Co. (now demolished) at located at 27 Clarkson Road North south of the railway line.

The Clarkson Basket Factory was found at 32 (1119) Clarkson Road North on the east side of the street with a barbershop in 30 (1119) Clarkson Road North and the Bank of Montreal at 26 (1109) Clarkson Road North. A "gatehouse" (now demolished) is indicated on the east side of the road at the railway tracks. To the south of the railway tracks number of houses are shown at the intersection of Clarkson Road and Lakeshore Road, noted as Highway 2. As well commercial development had occurred further to the south on Lakeshore Road West. The Clarkson United Church is shown on the southeast corner of Clarkson Road South and Lakeshore Road West.

In 1936, Major John and Blanche Barnett purchased the Warren and Susan Clarkson property situated on the west side of Clarkson Road North south of the railway tracks. In 1980, the Barnett estate sold the remaining seventeen acres of land associated with the early 19th century farmstead for a residential subdivision. The Clarkson-Barnett House

⁸ Hicks, 111.

⁹ Ibid., 90.

¹⁰ Ibid.

was incorporated into the subdivision and still stands within the development on its original site, facing onto Clarkson Road, at 1084 Feeley Court.

After a major fire in 1941, the former Durie Store at 1119 Clarkson Road North was substantially rebuilt and converted to residential use.¹¹ Currently, the building contains apartments and commercial offices. Sometime in the late 1940s, Harold Hare constructed a new building between the Clarkson Store and the William Clarkson House to house the post office. In 1955, a new post office building was erected on the west side of Clarkson Road North at Balsam Avenue. This building served as a post office until 1962. It was then sold to Clarkson Holdings. Since 1995 it housed the Toronto Ability School.¹²

The Clarkson railway station was destroyed by fire in 1962.¹³ In the same year, the railway station name was changed from Clarkson's to Clarkson.¹⁴

2.3 1130 to 1140 Clarkson Road North

The following sections provide a summary history of the subject property and its three buildings.

2.3.1 Clarkson Family (1819-1901 and 1907-1919)

The Clarkson family owned the subject property for about one hundred years from 1819 to 1919, with a small break between 1901-1907.

Warren Clarkson and Susan Shook

In 1808, a 15 year-old Warren Clarkson travelled to Upper Canada with his elder brother Joshua to work for a family friend. Warren Clarkson was born in 1793 in the Town of Pegasus, Dutchess County, Thile, New York, as the son of Richard Rouse Clarkson, an immigrant from Cheshire, England. After arriving in Upper Canada, the Clarkson brothers met Benjamin Monger in 1809. Monger was the son-in-law of the early Toronto Township settler, and United Empire Loyalist, Thomas Merigold. Monger encouraged the brothers to come to Toronto Township to work for the Merigolds. Joshua Clarkson, who was 12 years older than Warren, brought his family and stayed two years before moving to Markham Township.¹⁵ Warren Clarkson remained, and on August 4, 1816, he married Susan (Susannah) Shook, the daughter of Henry and Rosanna Shook of Trafalgar Township.¹⁶ The Shook family had moved from Reinbeck, Dutchess County, New York to Upper Canada in 1808. Warren and Susan Clarkson's first son George was born two years later in 1818.

¹¹ Ibid., 91.

¹² Ibid., 78.

¹³ Ibid., 59.

¹⁴ *A History of Peel County: To Mark Its Centenary 1867-1967* (The Corporation of the County of Peel November 1967) 273.

¹⁵ Hicks, xv.

¹⁶ Hicks, xv and 80.

Clarkson bought land from Archibald Wright in 1819 and proceeded to build a four-room timber house on Lot 29, Concession 2 SDS.¹⁷ A daughter Charlotte was born in 1822. In 1825, with the assistance of his brother Joshua, Warren built a second family house on his property. A second daughter, Lovina, was born in 1827, followed by two sons, William Warren in 1830, and Henry in 1834. A third daughter Isabella, born in 1837, died at three years of age.

Warren Clarkson immersed himself in his local community. He was instrumental in the establishment of the Chamber Springcreek Grave Ground, later Springcreek Cemetery, and remained as a trustee and chairman of the board for 50 years until 1873. In 1835, Clarkson built the first store in the area. It served as a waiting area for stagecoach travellers on Dundas Street south from Erindale. In 1837, two years after establishing the store at Clarkson's Corner, history recounts Clarkson built a trading post in the Port Credit harbour area on Lake Ontario. He sold the Port Credit store in 1852.¹⁸

The Census Return (1851) records 57-year old William [Warren] Clarkson as a farmer living with his wife Susan, their children George, William, Henry and Charlotte, and a servant, Elizabeth Shook in a single household. William Clarkson, age 22-years, was noted as a "clark" [clerk], possibly he was working in the family store. The same census does not record a shop/store on Clarkson's 140-acre property on Lot 29, Concession 2.

Susan Clarkson died in 1853 and is buried in Springcreek Cemetery. On November 20, 1855, Warren Clarkson married Mary Ann Kirkus, a widow from Richmond Hill. The Census Return (1871) notes Warren and Mary Clarkson lived together in a household without others. At that time, Clarkson owned a total of 102 acres of land including four village lots and two houses. In 1873, Warren and Mary Clarkson moved to Oakville, Ontario. Warren Clarkson died at 89 years of age on September 11, 1882.¹⁹ He is buried in Springcreek Cemetery at 1390 Clarkson Road North, Mississauga.

As well as other bequests, the last will and testament of Warren Clarkson stipulated his land holdings in the Clarkson area were to be divided between his two surviving sons, Henry and William. Henry Shook Clarkson received 100 acres of land comprising the southerly halves of the southerly halves of Lots 28, 29 and 30, Concession 2 SDS, except for that property already sold to the railway and for a public highway and a portion of Lot 28 already received by Henry, and the 25 acres of Lots 28, 29 and 30, Concession 2 SDS Inherited by his brother William Clarkson.²⁰ An annuity paid from Warren Clarkson's estate to his wife Mary Ann. it was released upon her death.²¹

¹⁷ The Clarkson-Barnett House property was developed as a residential subdivision and the house remains on its original site, facing Clarkson Road within the modern subdivision at 1084 Feeley Court. This house is one of the oldest structures still standing in Mississauga.

¹⁸ Hicks, xvi.

¹⁹ AO, Ontario, Canada, Deaths, 1869-1938 and Deaths Overseas, 1939-1947, MS 935, Schedule C, County of Halton, Division of Oakville, #006410, Warren Clarkson, Date of Death September 11, 1882.

²⁰ Culture Division, *Draft Clarkson General Store, Post Office and William Clarkson House, 1130-1140 Clarkson Road North, Cultural Heritage Assessment* (October 2014). Peel Land Registry Office, Instrument 3712, Warren Clarkson's Last Will & Testament, 15 May 1877, dated 25 September 1882.

²¹ Ibid.

The land inherited by William Clarkson was described in the last will and testament of Warren Clarkson as follows:

All and singular that certain parcel or tract of land and premises situate in the township of Toronto in the county of Peel and the province of Ontario and being composed of parts of Lots numbers 28, 29, and 30 in the 2nd concession South of Dundas Street in the said Township of Toronto containing by admeasurement 25 acres more or less and situated in the North Westerly side of the Toronto and Hamilton Railway (now the Grand Trunk Railway and known as Clarkson's Flag Station and said 25 acres bounded on the North by the lands of one William Summerset Bowbeer [sic], on the South by the said Railway, on the East by the Public highway and extending westerly to another public highway, being all the lands on the north westerly side of the said railway formerly owned by the said Warren Clarkson.²²

William Warren Clarkson and Lorenda Hemphill

William Warren Clarkson, son of Warren and Susan Clarkson, was born in 1830. Local history states he moved to Ohio for a few years to avoid legal problems and debt charges in the mid 1850s.²³ William married Lorenda Hemphill, the daughter of Nathaniel and Hannah Hemphill before 1857/58 when their daughter Helene Bertha was born in Toronto Township, Ontario.²⁴ Bertha was followed by siblings Warren Frank Clarkson, born in the United States in 1861 and Edith Maud in 1868 and Cora Vale in 1874, both born in Toronto Township.

Tremaine's Map (1859) shows one building on the subject property, presumably the store. (**Appendix B**). Local history recounts William Clarkson took over the management of his father's Clarkson store in the mid-1860s and then built a family home beside it.

The Census Return (1861) does not have a record of William and Lorenda Clarkson as a household in Toronto Township because they were living in the United States at that time. Local history suggests the William Clarkson House was built soon after William took over the management of the family store in the mid 1860s; however, this construction date has not been verified.²⁵ Certainly, the Clarksons were living in Toronto Township by March 1868 when their daughter Edith was born.

²² Culture Division, *Draft Clarkson General Store, Post Office and William Clarkson House, 1130-1140 Clarkson Road North, Cultural Heritage Assessment* (October 2014); and, Peel Land Registry Office, Instrument #7439, 28 January 1891, between William Warren Clarkson et ux and The Bristol and West of England Canadian Land Mortgage and Investment Company Ltd.

²³ Heritage Mississauga. "Recommendation for Designation on the Heritage Registry, 1130-1132 and 1140 Clarkson Road North (Clarkson Store and William Clarkson House", built 1865-1894 (July 15, 2005) 1.

²⁴ AO. Ontario, Canada, Marriages, 1801-1928, County of Peel, Division of Toronto, #009131, Helena Bertha Clarkson, March 6, 1888.

²⁵ Toronto Township Assessment Rolls for 1860s are not available as a source for a building date.

The Census Return (1871) notes William Clarkson as a trader by profession. He lived with his wife Lorenda and their three children, Bertha, Frank and Edith. No further information on William's real estate holdings and a family dwelling are noted in other census schedules. The Toronto Township map in the *Illustrated Historical Atlas* (1878) shows the initials WWC for William Warren Clarkson and one building, believed to be the store, on the subject property at that time (**Appendix B**). The William Clarkson House may have been built later in the late 1870s, or around 1882 when Clarkson inherited the property from his father. The architectural style of the William Clarkson House supports a construction date from the 1860s to the early 1880s.

In 1874, William Clarkson's profession was noted as "peddler" on the birth certificate of his daughter Cora.²⁶ By 1875, he was the first postmaster for the Clarkson's post office opened in June 1875.²⁷ The Census Return (1881) notes the 51 year-old Clarkson as a postmaster and store owner. He lived with his wife Lorenda and their three daughters, Helena Bertha, Edith Maud and Cora Vale Clarkson, possibly in the William Clarkson House. In 1882, he inherited the 25 acres of Lot 28, Concession 2 SDS, which included the Clarkson Store with the post office, from his father's estate.

The Census Return (1891) indicates William and Lorenda Clarkson and three of their children, Frank, Edith and Cora, lived in a two storey, eight room wood construction house, the existing William Clarkson House. William's occupation was noted as "farmer," perhaps indicating his son had taken on duties at the family store. In the same year, land records indicate William Clarkson mortgaged the property to The Bristol and West of England Canadian Land Mortgage and Investment Company Ltd., (hereinafter The Bristol and West of England Mortgage Co.). The mortgage instrument references "buildings" on the property.²⁸

William W. Clarkson died at his home on Clarkson Road North in 1894. He served as local postmaster until his death, with his official resignation date recorded as March 15, 1894.²⁹ He is buried in the Springcreek Cemetery at 1390 Clarkson Road North, Mississauga. It seems reasonable to assume Lorenda Clarkson continued to live in the family home on Clarkson Road North with her two daughters, Edith and Cora, and her son Frank until she died on March 24, 1897, at 61 years of age. She is buried with her husband in Springcreek Cemetery.³⁰

²⁶ AO. Registrations of Births and Stillbirths – 1869-1913. MS 929. Schedule A – Births, County of Peel, Toronto Township, #016369, Cora Vale Clarkson, November 13, 1874.

²⁷ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario). Access: --<<http://www.bac-lac.gc.ca/eng/discover/postal-heritage-philately/post-offices-postmasters/Pages/item.aspx?IdNumber=15501&>> (July 2015).

²⁸ Culture Division, *Draft Clarkson General Store, Post Office and William Clarkson House, 1130-1140 Clarkson Road North, Cultural Heritage Assessment* (October 2014) 9.

²⁹ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario).

³⁰ Ontario CanadianHeadstones.com. Springcreek Cemetery, Mississauga. Lorenda Clarkson and William W. Clarkson. Access: <<http://www.canadianheadstones.com/on/cemetery.php?cemID=532&pg=7>> (July 2015).

Helena Bertha Clarkson

Helena Bertha Clarkson was born in Toronto Township in c1857/58. She probably lived with her parents and siblings in the William Clarkson House until her marriage to Amos Shearer in 1888. She then moved to Alnwick Township, Northumberland County. She died in 1898.³¹

Warren “Frank” Clarkson

Warren “Frank” Clarkson, the son of William and Lorenda Hemphill, was born in Wilson, New York, in 1861.³² He returned to the village of Clarkson as a young child when his father moved back from the United States to take over the operation of the family store, sometime in the mid-1860s. In 1891, the Census Return indicates he lived in the family home on Clarkson Road North. A local directory for 1896 notes W.F. Clarkson operated a retail business in Clarkson, Ontario, and that he was the owner of property on Lot 28, Concession 2 SDS.³³

Frank Clarkson took over the postmaster duties for the Clarkson’s Post Office in May 1894 after the resignation of his father.³⁴ Three years later, on August 11, 1897, at age 36 years, he married Emma Chamberlain (1864-1939).³⁵ The Clarksons had one daughter, Marie (1900-1922). It is reasonable to suggest Frank and his wife lived in the William Clarkson House after their marriage until they moved away in 1901.

Land records indicate George Gooderham Sr., assumed the Bristol and West of England Company mortgage on the Clarkson property, which had been taken out in 1891.³⁶ In December 1900, George Gooderham Sr., acquired ownership of the subject property from Warren F. Clarkson.³⁷

Frank Clarkson resigned as postmaster on March 25, 1901.³⁸ The Census Return (1901) indicates Frank Clarkson and his family moved to Norfolk County soon after his resignation as postmaster. Frank Clarkson died in Norfolk County on February 15, 1904,

³¹ Ancestry.ca. Clarkson Family Tree. Helena Bertha Clarkson.

Access: --<<http://trees.ancestry.ca/tree/73661735/person/40279600310>> (July 2015).

³² AO, Ontario, Canada, Marriages, 1801-1928, Schedule B- Marriages, County of York, Toronto. #002239, Warren Frank Clarkson, August 11, 1897.

³³ *The Union Publishing Co.’s (of Ingersoll) Farmer’s and Business Directory for the Counties of Dufferin, Peel and York, 1896*, Vol. X (Ingersoll, Ontario: Union Publishing Co.’s of Ingersoll, 1896) 82 and A189.

³⁴ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario).

³⁵ AO, Ontario, Canada, Marriages, 1801-1928, Schedule B- Marriages, County of York, Toronto. #002239, Warren Frank Clarkson, August 11, 1897.

³⁶ Peel Land Registry Office. Toronto Township, Lot 28, Concession 2 SDS, Instrument #10358, between The Bristol and West of England Canadian Land Mortgage and Investment Company Ltd. and George Gooderham, dated 31 October 1900; and, and Culture Division, *Draft Clarkson General Store, Post Office and William Clarkson House, 1130-1140 Clarkson Road North, Cultural Heritage Assessment* (October 2014).

³⁷ Ibid, Instrument #10366, dated 10 Dec 1900, between Warren F. Clarkson et ux and George Gooderham.

³⁸ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario).

at 42 years of age³⁹ and is buried in the Springcreek Cemetery at 1390 Clarkson Road North, Mississauga.⁴⁰

Edith Maud Clarkson

Edith Maud Clarkson, the daughter of William Clarkson and Lorenda Hemphill, was born in Toronto Township on March 29, 1868. She was educated locally and probably worked in the family store as a young adult. After the death of her parents, she continued to live in the William Clarkson House on the subject property until about 1910. She became the Clarkson postmistress in 1901 and operated the store and post office until just before her death in 1919. Edith Maud Clarkson is buried in Springcreek Cemetery at 1390 Clarkson Road North, Mississauga.⁴¹ (*See Section 2.3.3*).

Cora Clarkson

Cora Clarkson, the daughter of William Clarkson and Lorenda Hemphill, was born in Toronto Township on November 13, 1874.⁴² She lived in the William Clarkson House with her family, and may have worked in the family business prior to her marriage to Robert C. Shook in December 1903.⁴³ The couple operated the Shook family farmstead in Toronto Township for many years. Cora Vale Clarkson Shook died on June 18, 1923, and is buried in the Springcreek Cemetery at 1390 Clarkson Road North, Mississauga.

2.3.2 George Gooderham Sr. (1900-1907)

George Horace Gooderham Sr., was the son of William Gooderham and Harriet Herring of Toronto. In the early 1830s, William Gooderham had co-founded with his brother-in-law James Worts a business that eventually became Gooderham and Worts, the largest distiller of alcoholic beverages in Canada.⁴⁴

George H. Gooderham acquired 360 acres of land in Toronto Township, Lots 31 and 32, Concessions 3 and 4 SDS, in 1893. He established the Gooderham Estate, a farm that produced large amounts of fruits and vegetables, which were shipped to the Toronto markets from the Clarkson train station.⁴⁵ Land records indicate Gooderham acquired

³⁹ AO, Ontario, Canada, Deaths, 1869-1938 and Deaths Overseas, 1939-1947, MS 935, Schedule C, County of Norfolk, #019208, Frank Clarkson, Date of Death February 15, 1904.

⁴⁰ Ontario CanadianHeadstones.com. Springcreek Cemetery, Mississauga. Warren F. Clarkson. Access: <<http://www.canadianheadstones.com/on/cemetery.php?cemID=532&pg=7>> (July 2015).

⁴¹ Ontario CanadianHeadstones.com. Springcreek Cemetery, Mississauga. Edith Maud Clarkson. Access: <<http://www.canadianheadstones.com/on/cemetery.php?cemID=532&pg=7>> (July 2015).

⁴² AO. Registrations of Births and Stillbirths – 1869-1913. MS 929. Schedule A – Births, County of Peel, Toronto Township, #016369, Cora Vale Clarkson, November 13, 1874

⁴³ Hicks, 71.

⁴⁴ The former Gooderham and Worts manufacturing facilities in the City of Toronto form part of the historic area known as the Distillery District.

⁴⁵ Hicks, 87.

2.3.3 Edith Maud Clarkson (1901-1919)

[illegible]

Figure 4. A portion of Plan G-13, shows the outline of the Clarkson Store and the William Clarkson House in 1913 [Peel Land Registry Office, Plan of Subdivision of Part Lots 28, 29 and 30, Con. II, S.D.S., Twp. of Toronto, Co. Peel, Speights and van Nostrand, O.L. Surveyors, Toronto, May 15, 1913].

Sometime between 1906 and 1910, the 19th century Clarkson Store (**Figures 6, 16 and 18**) was significantly altered. A second floor and a north addition for the post office were added transforming the original storey-and-a-half front gable store into a two-and-a-half

⁴⁶ Peel Land Registry Office. Toronto Township, Lot 28, Concession 2 SDS, Instrument #10358, between The Bristol and West of England Canadian Land Mortgage and Investment Company Ltd. and George Gooderham, dated 31 October 1900; and, Culture Division, *Draft Clarkson General Store, Post Office and William Clarkson House, 1130-1140 Clarkson Road North, Cultural Heritage Assessment* (October 2014).

⁴⁷ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario).

⁴⁸ Hicks, 111.

⁴⁹ *Ontario commercial yearbook and gazetteer for 1906*, 420.

storey, “L-shaped” structure (**Figure 19**). Plan G-13 shows the altered form of the store. Edith Clarkson probably carried out the store renovations (**Figure 4**).

The Census Return (1911) notes Edith Clarkson, a single woman, was employed as a general merchant, lived with a housekeeper and a labourer on Lot 29, Concession 2 SDS. Edith built a new house at 1060 Clarkson Road North beside the William Clarkson House around 1910. After moving to her new house, Edith appears to have rented the house to tenants.

The official resignation of Edith Clarkson as Clarkson’s postmaster was registered on November 30, 1919, a day after her death.⁵⁰ She had served in the position for twenty-nine years. Land records indicate the estate of Edith M. Clarkson sold the subject property with the store, post office and house to Harold U. Hare in August of 1921.

2.3.4 Harold U. Hare (1921-1925) and Jessie Jamieson (1925-1944)

Harold “Harry” Urquhart Hare was born on November 19, 1884, the son of William J. Hare and Louisa Grafton of Port Credit.⁵¹ He married Margaret Ellen Hedge on December 8, 1909.⁵² Hare became Clarkson’s postmaster on January 1, 1920.⁵³ It is likely Hare had leased the Clarkson Store and William Clarkson House prior to his purchase of the store, its contents and surrounding lands from the Clarkson estate on August 15, 1921.⁵⁴

The Census Return (1921) indicates merchant Harry Hare, his wife Margaret Hare and their two sons Harold and Gerald, along with his niece Margaret Hammand who was noted as a clerk by occupation, lived together, and presumably in the William Clarkson House. Hare and his family occupied the William Clarkson House from about 1920 to 1925.

Land records indicate Jessie Jamieson purchased the subject property from Harold Hare on August 10, 1925. In exchange Hare acquired Jessie Jamieson’s existing house and property at 1191 Stavebank Road.⁵⁵ Hare was operating the Harold U. Hare Coal Co., on the west south side of Clarkson Road North just south of the railway tracks on Clarkson

⁵⁰ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario).

⁵¹ AO. Registrations of Births and Stillbirths – 1869-1913. *MS 929, Schedule A- Births, County of Peel, Division of Toronto Township, #028646, Harold U. Hare*, November 19, 1884.

⁵² Ibid. Ontario, Canada, Marriages, 1801-1928, County of Peel, Division of Toronto, #016673, Harold U. Hare and Margaret Ellen Hedge, December 8, 1909.

⁵³ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario).

⁵⁴ Peel Land Registry Office. Toronto Township, Lot 28, Concession 2 SDS, Instrument #21213, registered September 12, 1921, between Edith Clarkson Estate and Harold U. Hare; and, Culture Division, *Draft Clarkson General Store, Post Office and William Clarkson House, 1130-1140 Clarkson Road North, Cultural Heritage Assessment* (October 2014).

⁵⁵ Ibid., Township of Toronto, Plan G-13, Lot 67, Lots 28, 29, Concession 2 SDS, Instrument #26512, registered August 17, 1925 exchange of lands between Harold U. Hare and Jessie Jamieson.

Road North.⁵⁶ Harold U. Hare died on February 7, 1958,⁵⁷ and is buried in the Springcreek Cemetery at 1390 Clarkson Road North.

Jesse Jamieson was the owner of the subject property from 1925 to 1944. During this period, Jamieson appears to have leased the Clarkson Store as well as the William Clarkson House.

2.3.5 W.T. McCord and Carload Groceteria (1926-1937)

William Thomas McCord was born in Antrim, Ireland, in 1899, the son of William James McCord and Martha Blackadder.⁵⁸ It appears the 21 year-old McCord emigrated to Canada in May 1920, travelling on the ship *Megantic*.⁵⁹ He married Amanda G. Quin in Toronto on May 27, 1925.⁶⁰ The McCords probably moved to Clarkson soon after their marriage. He was appointed the Clarkson Postmaster on February 1926⁶¹ and appears to have leased the Clarkson Store from Jesse Jamieson. The McCord family may have lived in the William Clarkson House at this time. The Clarkson Store was a “Red & White” before being named the McCord Bros. By May 1934, it was known as W.T. McCord. In August 1 of 1934, McCord had transformed the store to the “Carload Groceteria.”

On August 22, 1934, the *Port Credit Weekly* announced in a Clarkson news item that Mr. W.T. McCord intended to open a new Carload Groceteria at Clarkson in the store, which has operated for some years.⁶² The grand opening of the Carload Groceteria in Clarkson was held on August 30, 1934. An advertisement for the opening (**Figure 4**) on August 29, 1934, noted,

*The increasing popularity of the “groceteria type” store and the economies affected by this system of merchandising have resulted in changing our store (W.T. McCord) from a service grocery to a CARLOAD GROCETERIA. We are confident that the residents of Clarkson and district will approve of this change and find both pleasure and profit in shopping at our new Groceteria.*⁶³

A report of the store opening in the local newspaper a week after the opening event described the day.

⁵⁶ Clarkson Fire Insurance Plan (1936).

⁵⁷ *Find A Grave*. Find A Grave. Harold Urquart Hare, Springcreek Cemetery, Mississauga, Ontario. Access: --<<http://www.findagrave.com/cgi-bin/fg.cgi?page=gr&GRid=124600996&ref=acom>> (July 2015).

⁵⁸ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario).

⁵⁹ LAC. *Passenger Lists, 1865–1935*. May 23, 1920 and *Form 30A Ocean Arrivals (Individual Manifests), 1919-1924, William Thomas McCord*.

⁶⁰ AO. Ontario, Canada, Marriages, 1801-1928, Toronto, Ontario, #003304, William Thomas McCord, May 27, 1925.

⁶¹ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario).

⁶² “Clarkson”, *The Port Credit News*, August 22, 1934, 2

⁶³ *Clarkson’s Newest Shopping Centre Opens Thursday, August 30th*. *The Port Credit News*, August 29, 1934.

The opening of the new Carload Groceteria in Clarkson last week was a big affair. In fact, one of the big events in the history of Clarkson. From the time that Mr. McCord opened the door at 2 p.m. on Thursday, there was a constant stream of customers, and this continued throughout the entire day and also on Friday and Saturday. The values were outstanding and the customers were delighted with the new service. Wednesday will also be a big day in Clarkson when a Weiner Roast and Dance is to be staged by the enterprising proprietor of the Carload Groceteria.”⁶⁴

The Clarkson Fire Insurance Plan (1936) shows the Clarkson Store as a rectangular shaped, two storey building of wood frame construction with an exterior cladding.⁶⁵ The roof was clad in metal at the front and wood shingles at the rear. A one storey wood frame addition was located on the northwest corner of the building. Three outbuilding are located to the west (rear) of the store. The William Clarkson House was shown as a two storey wood frame building with a one storey rear addition, both the front and the addition had wood shingle roofs.

William T. McCord resigned as Clarkson’s postmaster in October 1937.⁶⁶ He moved away from Clarkson, and after a lingering illness, died on August 6, 1955, in Barrie, Ontario.⁶⁷

2.3.6 1937 to 1978

Henry “Harry” W. Gerhardt assumed the store’s management in 1937, leasing it for a number of years from Jesse Jamieson. Gerhardt served as the Acting Postmaster of the Clarkson’s Post Office for a short period of time in 1937. David Terry was appointed the postmaster on November 10, 1937⁶⁸ while Gerhardt managed the Clarkson Store. The *Port Credit Weekly* noted in 1941 that the Carload Groceteria supplied coffee and sandwiches for the firemen who had worked on the basket factory fire across the street.⁶⁹ Land records indicate Gerhardt bought the subject property on Lot 67 from Jessie Jamieson in April 1944.⁷⁰ David Terry left his position as postmaster in July 1945 and Harry Gerhardt resumed the duties.⁷¹ Sometime c1947, it appears Gerhardt built a two-

⁶⁴ “Clarkson”, *Port Credit News* (September 4, 1934) 2.

⁶⁵ The Fire Insurance Plan (1936) indicates the store was of wood frame construction with a veneer of either stone, concrete or concrete blocks. This may be a reference to the metal cladding on the building.

⁶⁶ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario).

⁶⁷ Parkin, Mrs. O., “From Toronto Township’s West End, Clarkson Comments”, *Port Credit News* (August 6, 1955).

⁶⁸ *Port Credit Weekly* Dec 31, 1939. Although Jessie Jamieson still owned the store at this time Harry W. Gerhardt is listed as the store proprietor of the Carload Groceteria.

⁶⁹ Hicks, 76.

⁷⁰ Peel Land Registry Office, Township of Toronto, Plan G-13, Lot 67, Lots 28, 29, Concession 2 SDS.; and, Culture Division, *Draft Clarkson General Store, Post Office and William Clarkson House, 1130-1140 Clarkson Road North, Cultural Heritage Assessment* (October 2014). The first reference to the Carload Groceteria was on Land Registry abstracts in 1944 when Gerhardt purchased the store.

⁷¹ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario).

storey flat roof building between the Clarkson Store and William Clarkson House as a separate post office building.⁷²

According to local history, John Bodley took over the operation of the Carload Groceteria in 1947. It appears Gerhardt managed the post office operations. Bodley changed the nature of the Clarkson Store to strictly a grocery business and as a result, the business declined. Bodley left the store in 1952 and James Tait took over the store management.⁷³

In 1955, the postal service constructed a new post office building at 1146 Clarkson Road North on the west side to the north side of Balsam Avenue to replace the building on the subject property between the Clarkson store and house. This ended the 80 years of postal service on the subject property. Gerhardt resigned as postmaster in 1957.⁷⁴

2.3.7 1978 to Present

Land records indicated the executors of the estate of Harry W. Gerhardt's sold the property at 1130-1140 Clarkson Road North to Leo Jablonski and Douglas A. Elliott on February 9, 1978.⁷⁵ The property was resold a year later in 1979 to William and Dora Jung and Arnold and Laura Mark. Under the Jung and Mark ownership, the Clarkson Pet Food and Supplies was located in the south part of the store and the Normandy House Canadian Crafts in the north part of the Clarkson Store (**Figure 23**). In 1986, Naseem and Shahida Kaukab bought the subject property.

3.0 DESCRIPTION OF THE CULTURAL HERITAGE LANDSCAPE

3.1 Introduction

Unterman McPhail Associates completed a survey from the public roadway area in the vicinity of the subject property of 1130-1140 Clarkson Road North in July 2015. Digital images of the site and buildings presented in this report were completed at that time unless otherwise stated and are included in **Appendix C**.

3.2 Area Context

Warren Clarkson, one of the original settlers of the area, and after whom Clarkson village is named, built the first general store on this site circa 1835. It was around this nucleus that the village developed. Forty years later, in 1875, the first Clarkson's Post Office was opened on the Clarkson Property. This significant event meant the local residents no longer had to travel to Erindale to retrieve their mail.

⁷² Hicks, 78.

⁷³ Hicks, 78.

⁷⁴ LAC. Postal Heritage and Philately, Post Offices and Postmasters, Clarkson, Peel (Ontario).

⁷⁵ Land Abstract, Peel County, Toronto Township, Plan G-13, Lots 28, 29, Concession 2 SDS.

Today, Clarkson Road North is a two lane paved road that runs north of the intersection at Lakeshore Road West. From Lakeshore Road West to the railway tracks the road exhibits urban characteristics such as curbs and sidewalks. There is on-street parking on the east and west sides. The CN/GO rail line runs east to west across Clarkson Road North (**Figure 7**) on the southern boundary of the subject property at 1130-1140 Clarkson Road North. There is an at-grade railway crossing on the street. The railway line was built through the property of Warren Clarkson in 1855. GO Transit trains regularly travel on this line providing access to Toronto to the east and Hamilton to the west.



Figure 5. An aerial view of the subject property at 1130-1140 Clarkson Road North and nearby properties listed on the City's Municipal Heritage Inventory [Google Maps 2015].

On the east side of Clarkson Road North south of the railway tracks, Birchwood Park is located at 1101 Clarkson Road North. An entrance off Clarkson Road North provides access to a large parking lot. Storage buildings associated with the former Clarkson Basket Factory (**Photo: Appendix C**) shown on the Fire Insurance Plan of 1936 also have the municipal address of 1101 Clarkson Road North. Located behind the Durie building property access is from a driveway on the east side of Clarkson Road North along the north side of Birchwood Park. Currently, the building is in various commercial use, including a fitness centre, a yoga and Pilates studio and a bicycle business. Currently, the properties located at 1101 to 1125 Clarkson Road North are a subject of redevelopment discussions with the City.

The property at 1105 Clarkson Road North, currently vacant, was the site of a service station. The building located at 1109 Clarkson Road North is currently vacant (**Photo: Appendix C**). The Merchant Bank branch office occupied this building from 1913-1922,

a Bank of Montreal branch office from 1922-1938 and the Auld Butcher Shop from 1947-2015. It is a listed heritage property on the City's Municipal Heritage Register.

The neighbouring building at 1119-1125 Clarkson Road North was the Alex Durie general store from 1908 to 1932 (**Photo: Appendix C**). It was then used as a basket factory before becoming apartments in 1941. The first Clarkson Village library once occupied the second floor of the building. It is a listed heritage property on the City's Municipal Heritage Register.

To the south of the railway tracks on the west side, mid-to-late 20th century redevelopment, including the Clarkson Village Dental Clinic at 1098 Clarkson Road North, has occurred from Lakeshore Road West to the vacant lot at the tracks that once held the Clarkson Railway Station. This redevelopment on the west side includes Feeley Court, which was once the Warren Clarkson property. The Clarkson/Barnett House, built c1819, is located within the development at 1084 Feeley Court. It is a municipally listed property.

North of the railway tracks Clarkson Road North continues as a two lane paved road, but has a paved brick area in its centre (**Figures 9 and 11**). There is on-street parking on the east and west sides immediately north of the railway tracks. The angle parking area in front of the subject property at 1130 and 1140 Clarkson Road North is still apparent in the roadscape (**Figures 12 and 13**). There is a sidewalk on the west side of the road.

William's daughter Edith Clarkson subdivided the family property in the early 20th century as shown on Plan G-13, Lot 67 (**Figure 5**). Part of the subdivision plan was the creation of Balsam Avenue running westward from Clarkson Road North immediately north of the William Clarkson House (**Photo: Appendix C**). The Cox House was built in 1920 at 1776 Balsam Avenue adjacent to the William Clarkson House (**Photo: Appendix C**). It is still in residential use and a listed heritage property on the City's Municipal Heritage Register. Edith Clarkson built a new home at 1160 Clarkson Road North (**Photo: Appendix C**). It is still in residential use and is a listed heritage property on the City's Municipal Heritage Register. In 1955, a new post office building was erected at 1146 Clarkson Road North. The site is currently used by a private school and day care centre.

To the north of the Edith Clarkson House, there is twentieth century residential development. Low stone walls, identified on the City's Cultural Heritage Inventory (F-SLF-1), are located in front of the properties found at 1182, 1188, and 1190 Clarkson Road North. These stonewalls are identified as significant cultural feature on Clarkson Road North with respect to its contribution to the visual diversity and the character of the streetscape. Further to the north is the Springcreek Cemetery at 1390 Clarkson Road North, one of the oldest active cemeteries in the Region of Peel. Many early settlers of the Clarkson area, including members of the Clarkson family, are buried in this cemetery.

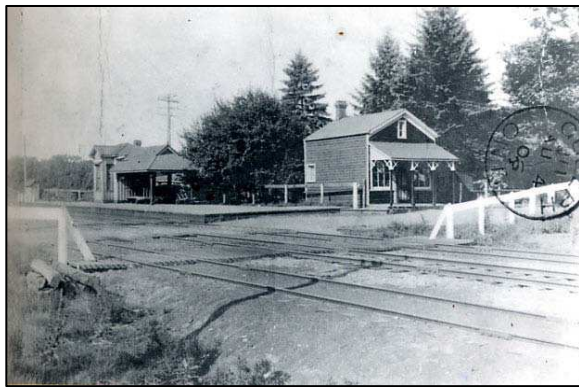


Figure 6. A pre-1906 view northwest to the Clarkson Store showing the former railway station on the north side of the tracks [MLS, Historic Images Gallery, BA0155, undated].



Figure 7. A view northwest from the railway tracks to the Clarkson Store to the Clarkson Store and the former Post Office Building [July 2015].



Figure 8. A streetscape view looking south on Clarkson Road North to the Clarkson Store (right), the railway crossing, and the former Alex Durie Store and Clarkson Basket Factory site to the south of the railway tracks (left) [MLS, Historic Images Gallery, HA0039, 1910].



Figure 9. A streetscape view south on Clarkson Road North to the William Clarkson House, the former Post Office Building and the store (right) and the railway crossing [July 2015].



Figure 10. A streetscape view to the Clarkson Store (left) located immediately north of the railway line with the former Alex Durie store and Clarkson Basket Factory site (right) located at 1119 Clarkson Road North [MLS, Historic Images Gallery, PH3490, undated].



Figure 11. A streetscape view north on Clarkson Road North with the Clarkson Store (left) north of the railway line and (right) the former Alex Durie Store and Clarkson Basket Factory at 1119 and Auld Butchers at 1109 Clarkson Road North to the south of the railway [July 2015].



Figure 12. A streetscape view southwest on Clarkson Road North to Carload's Groceteria [Clarkson Store] showing the former Post Office Building with a flat roof building to the north angled parking in front [MLS, Historic Images Gallery, BA0075, undated].



Figure 13. A view southwest on Clarkson Road North showing the addition of a gable roof to the former Post Office Building and the angle parking area in front of the store and post office [July 2015].

Across from the subject property and immediately north of the railway tracks on the east side of Clarkson Road North, there is a vacant lot and then the Clarkson Paisley House located at 1141 Clarkson Street North, (*Photo: Appendix C*). Now in commercial and residential use, The Clarkson-Paisley House, once the location of the Henry Clarkson residence, is a listed heritage property on the City's Municipal Heritage Register. In the public right-of-way in front of the property, there is a commemorative stone dedicated to "Clarkson Village 1808-2008". It was erected by the Clarkson 1808 Celebration Committee as a lasting commemoration of the Committee's celebration of Clarkson's 200th anniversary (*Photo: Appendix C*). The commemoration specifically mentions Warren Clarkson and his general store and the post office. Oak Tree Park is located within a small fenced area on the east side of the road immediately north of the Clarkson Paisley House. It contains one of the oldest trees in Mississauga. Access to the park is on Hollow Oak Terrace. St. Christopher's Catholic Church and School is located to the north of Hollow Oak Terrace. Much further to the north on Clarkson Street North is Benares House, a provincially significant and municipally designated property.

Although the commercial core of Clarkson had begun to shift south to Lakeshore Road West and Clarkson Road in the mid 20th century, the historic heart of the community is still represented on Clarkson Road North by the a distinct collection of 19th and early 20th century buildings such as the Clarkson Store, the William Clarkson House, the post office buildings, the former Durie Store, the former Clarkson Basket Factory buildings and the Auld Butcher Shop, the Edith Clarkson House, the Clarkson Paisley House and the Cox Property on Balsam Avenue.

3.2.2 Adjacent Heritage Properties

The following properties located in the vicinity of the subject property are included on the City's Municipal Heritage Register as listed heritage resources:

- 1109 Clarkson Road North (former Merchant Bank/Auld Butcher Shop) (*Photo: Appendix C*);

- 1115, 1117, 1119, 1123, 1125 Clarkson Road North (former Alex Durie Store) **(Photo: Appendix C)**;
- 1141 Clarkson Road North (Clarkson Paisley House) **(Photo: Appendix C)**;
- 1160 Clarkson Road North (Edith Clarkson House) **(Photo: Appendix C)**;
- 1182, 1188, 1190 Clarkson Road North (Low Stone Walls), and included in the City's *Cultural Heritage Inventory* as item F-SLF-1;
- 1176 Balsam Avenue (Cox Residence) **(Photo: Appendix C)**; and
- 1084 Feeley Court (Clarkson-Barnett House).

The Clarkson Paisley House at 1141 Clarkson Road North on the former Henry Clarkson property, the Edith Clarkson House at 1160 Clarkson Road North and the Clarkson-Barnett House at 1084 Feeley Court, which is the c1819 home of Warren Clarkson, all have historical associations with the Clarkson family, and therefore, the subject property at 1130-1140 Clarkson Road North.

Furthermore, Heritage Mississauga has produced the pamphlet *A Heritage Tour Clarkson est. 1808* that features all of the above noted properties as being landmark properties of Clarkson.

3.3 Site Description

The subject property at 1130-140 Clarkson Road North is bound by the CN/GO Rail line on the south, Balsam Avenue to the north, Clarkson Road North on the east and the property boundary with 1176 Balsam Avenue on the west **(Figure 14)**.

The subject property and its three buildings, the store, the post office and the house, are physically, functionally and historically linked to their surroundings. The subject property, in its entirety, serves as a visual reminder that this was once the location of the commercial core of Clarkson's village.

Warren Clarkson acquired the subject property in the early 19th century. Clarkson was one of the original settlers of the area and after whom Clarkson village was named. The grouping of buildings on the property includes the mid 19th century Clarkson Store **(Figure 15)**, the former Post Office Building **(Figure 24)** built in the late 1940s, and the 19th century William Clarkson House **(Figure 25)**.

The CN/GO Transit rail line is located on the south boundary of the property. The Clarkson Store, which is the southernmost building on the subject site, is located beside the rail line. It faces onto Clarkson Road North. In 1875, the first post office in Clarkson was opened inside the store. Historical photographs indicate the Clarkson Store was situated close to the front of the lot adjacent to the road **(Figures 16 and 18)**. In the 20th century, angle parking was installed in front of the Clarkson Store for customers. The extra road width, which now accommodates parallel parking, is still evident **(Figures 13 and 14)**. A sidewalk runs north to south between the road and the subject property.



Figure 14. An aerial view shows the subject property at 1130-1140 Clarkson Road North [GoogleMaps, 2015].

The former Post Office Building located between the Clarkson Store and William Clarkson House was built in the late 1940s. It faces onto Clarkson Road North. The William Clarkson House located on the north side of the former Post Office Building also faces onto Clarkson Road North at Balsam Avenue. Unlike the Clarkson Store and the former Post Office Building, the William Clarkson House is set back from the road to provide front and side yard space. To the rear of the buildings there is a yard that extends westward to the property line with 1176 Balsam Avenue.

4.0 DESCRIPTION OF THE BUILT HERITAGE RESOURCES

4.1 Introduction

Unterman McPhail Associates completed a survey of the subject property at 1130-1140 Clarkson Road North and its three buildings, the Clarkson Store, the former Post Office Building and the William Clarkson House, in July 2015 from the public roadway. There was no permission to enter the subject property. Exterior renovation work was ongoing during the site visit. Digital images of the site and buildings presented in this report were completed at that time, unless otherwise stated and are included in *Appendix D*.

4.2 Clarkson Store, Exterior



Figure 15. A view showing the front (east) elevation of the Clarkson Store [July 2015].

Warren Clarkson built a general store on the subject property at 1130-1140 Clarkson Road North in 1835. It was probably a log building. No known physical evidence of this building exists on-site today. A simply designed, one-and-a-half storey, wood frame building with a front gable roof appears to have replaced the 1835 structure, possibly in the 1860s (**Figures 6, 16 and 17**). This store was probably clad in horizontal wood siding, likely clapboard, when built. Its front gable roof design was similar to many small rural enterprises of its time period.

Before 1906-1910, historical photographs indicate the exterior walls of the Clarkson Store had been clad in a metal sheet siding with an embossed pattern. A front porch, typical of 19th century commercial buildings, with shed roof supported on wood posts and brackets, spanned the full width of the front elevation. At some point wood lattice had been added to the north and south ends of the porch roof on the store. The ground floor of the front elevation was divided into three bays, two storefront windows flanking a centre store entrance. A wooden step provided access to the wood plank floor of the porch on the south side. The upper gable has a single window opening. The south elevation of the building facing the railway tracks had a single window opening on ground floor in the southeast corner.

The Clarkson Store was significantly altered c1906-1910 (*See Section 4.2.1*).

Currently, as of July 2015, the front (east) elevation of the Clarkson Store has a gable roof with a wide overhang and a pediment on the south end and a side gable roof extending to the north. The ground floor, which originally had two storefronts, now has three entrances and two modern store entrances with associated flanking modern store windows and a middle entrance to the second floor, within the storefront area. An area for signage has been left above the door and window openings. The area below the windows has been refaced with applied sheets of a stone facing material. The second floor has three evenly spaced window openings with modern window units and a modern facsimile of a board and batten siding. The former round-headed window opening in the gable has been removed.

The adjacent building obscures the north elevation. The rear (west) elevation has a two storey addition. A vehicle obscured the ground floor during the site visit; the second floor has two window openings while the end gable has a single round head window opening. The south elevation is two and-a-half storeys high with a gable roof and end pediment. The exterior cladding on the front (east) elevation is a modern facsimile of a board and batten siding.

The south elevation of the store has a door opening and a new single window unit in an existing opening on the southwest corner. There are four new window openings on the second floor. The exterior cladding is a modern facsimile of a board and batten siding.

4.2.1 Modifications

The Clarkson Store is located on its original site. The first one and-a-half storey wood frame store on the property, as shown in historical photographs (*Figures 16 and 17*), had a front gable with a rectangular window opening. It was significantly altered from its original modest design sometime between 1906 and 1910. A second floor was added to the building as well as an addition on the north side transforming it in to a two-and-a-half storey, “L-shaped” structure with a front gable roof and a storefront (*Figure 18*). The design of the early 20th century renovations is still evident today despite more recent renovations in 2015 (*Figure 15*).

In 2005, a Heritage Mississauga report described the Clarkson Store as follows,

The store is “L” shaped in plan and is 2 ½ storeys with a low pitch gable roof, Classical Revival in style. There are pediments on the east-front, west- rear and north façades. The store has been boarded up but according an architectural assessment done on the property in 1989, the: first floor is divided into two stores, each having a central door flanked by display windows. The door on the left stands in a recess decorated by two thin Corinthian pillars. The second storey is defined by a moulded wooden entablature over the store windows. There are three over the store windows. There are three rectangular sash windows on the second floor, and a small rectangular window in a round-headed opening in the gable. The south façade has a

door on the first floor and three sash windows on the second. At the rear are a door on the first floor, two sash windows on the second and a small round-headed window in the gable. There is also a rectangular sash window on the north side inside the "L". The building is covered with a brick pattern pressed metal, exposed on the northern façade and otherwise covered with asphalt siding. The cornice is decorated throughout with patterned moulding. The wide soffits are panelled.

The pressed metal brick pattern siding was popular in the early 20th century, it is believed that the siding was applied between 1900-1920, after the building was constructed, covering 1x10 –inch horizontal boards.⁷⁶

Although currently undergoing extensive exterior renovations, the Clarkson Store retains the height, scale, shape, form and massing and commercial storefront of the earlier c1906-1910 renovations. The overhanging eaves have been reclad and the patterned moulding described in 2005 has been removed. The pressed metal siding has been either removed or covered with new siding.

On the front elevation, the round-headed window in the front gable added in the c1906-10 renovations and (**Figures 18, 21 and 23**) has been removed and a new entrance has been inserted between the two storefronts. The south elevation has modern horizontal siding, and new double window units on the second floor and a new double window on the southwest corner of the ground floor beside an entrance door. A view of the ground floor of the west (rear) elevation was hidden by a parked vehicle during the site visit. There is no information on the interior alterations to the building.



Figure 16. A view of the Clarkson Store and Post Office prior to the 1906-1910 renovations, [MLS, Historic Images Gallery, HA0040, undated].

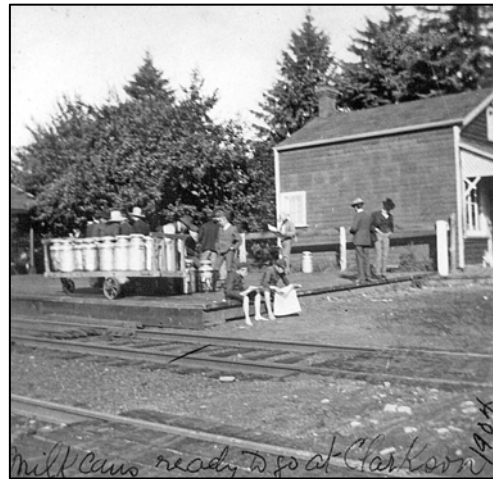


Figure 17. A view of northwest to the Clarkson store in 1904 showing its proximity to the railway station platform [MLS, Historic Images Gallery].

⁷⁶ Heritage Mississauga. "Recommendation for Designation on the Heritage Registry, 1130-1132 and 1140 Clarkson Road North, (Clarkson Store and William Clarkson House)", built 1865-1894 (July 15, 2005) 3.



Figure 18. Edith Clarkson and Cora Clarkson in front of the Clarkson Store [MLS, Historic Images Gallery, HA0041, undated].



Figure 19. A view of the E.M. Clarkson Store including the post office after the 1906-1910 renovations [Frost Postcard collection, 1991.028.315, PAMA, photo PN2014_04299].



Figure 20. A view of the interior of the E.M. Clarkson Store and Post Office circa 1910 [MLS, Historic Images Gallery, BA0062, 1910].



Figure 21. A view of the interior of the Clarkson Store and post office [MLS, Historic Images Gallery, BA0061, 1910].



Figure 22. The front elevation of the Carload Groceteria [Clarkson Store] on Clarkson Road North [MLS, Historic Gallery, BA0074, undated].



Figure 23. The front (east) elevation of the Clarkson Store [Culture Division, Heritage Services Files, 1980s].

4.3 Former Post Office Building, Exterior



Figure 24. A view of the front (east) elevation of the former Post Office Building located between the Clarkson Store and the William Clarkson House [July 2015].

The former Post Office Building appears to have been built in the late 1940s. When built, it was two-storeys high with a flat roof (**Figure 24**). Commercial windows, a centre door and a signboard above distinguished the ground floor. The second floor had two single window openings.

In 2005, a Heritage Mississauga report described the former Post Office Building as,

The brick building situated between the Clarkson store and William Clarkson's house is not included in the request for designation. The building lacks a definitive style, however, the absence of decoration and the simple utilitarian nature of the building would likely date the structure in the early 20th century. The building was used as a post office beginning in 1947.⁷⁷

⁷⁷ Ibid., 4.

4.3.1 Modifications

Heritage Mississauga (2005) described the building as a brick structure.⁷⁸ Currently, the building has been reclad. The recent renovations have altered the storefront windows and entrance and the second floor window units. The flat roofline has been replaced by a gable front roof with large side dormers. A two and-a-half storey rear addition with a gable roof has been added. There is no information on interior alterations to the building.

4.4 William Clarkson House, Exterior



Figure 25. The front (east) elevation of the William Clarkson House [July 2015].

The exact construction date of the William Clarkson House has not been determined. It may be as early as the mid-1860s, as suggested by local history, or possibly from the end of the 1870s or around 1882 (**Figure 25**). Plan G-13 (1913) shows the building with what may be a wraparound verandah on the south as well as a larger rear (west) extension than currently exists (**Figure 4**). A third building, now demolished, is shown to the rear (west) part of the property behind the house, perhaps a small barn or a driveshed, which may have been accessed by Balsam Avenue.

The William Clarkson House is a representative example of a vernacular style dwelling of wood frame construction with Gothic Revival characteristics. It has a steeply pitched cross, one and-a half story, gable roof, a rectangular floor plan, symmetrical arrangement

⁷⁸ Ibid., 2.

of openings on the principal elevations and a front verandah, which may have wrapped around the south side when built. There is a one-storey rear wing. Original detailing included clapboard siding with cornerboards and door and window openings with triangular shaped heads on the front elevation facing the road.



Figure 26. An early 20th century view of the front (east) elevation of the William Clarkson House [MLS, Historic Image Gallery, BA0154, undated].



Figure 27. The front (east) elevation of the William Clarkson House [MLS, Historic Images Gallery, J771, William Clarkson House, Clarkson, 1980].



Figure 28. The front (east) elevation of the William Clarkson House [Heritage Mississauga, 2005].



Figure 29. A view to the north elevation of the William Clarkson House [September 2014].

4.4.1 Modifications

The William Clarkson House is located on its original site. Although it has undergone some exterior alterations since constructed, and change to the exterior trim and cladding is currently in progress, it retains the original height, scale, shape, form and massing of its 19th century design (*Figures 26 and 27*).

In 2005, Heritage Mississauga report described the exterior of the house (*Figures 28 and 29*) as follows,

The Clarkson House is a one-and-a-half storey Victorian Gothic with an “L-shaped” floor plan. A single storey later addition was built on the rear, with a hipped roof. The

original house was a cross gable roof, the front gable of the house faces Clarkson Road North. The peak has a hammer beam fretwork decoration. This is new as it is not present in the older picture. The front veranda extends beyond the south wall indicating that it may have been a wraparound veranda, however, the side portion is gone. This may also specify that the two sets of bay windows on the south facade are an addition. A wide reed moulding surrounds the front door. The original exterior covering is unknown as it was covered by pressed steel siding in a brick pattern, which was also present on the store, this siding was available in the late 19th century but became popular in the early 20th century. The new plywood covering conceals the majority of the pressed metal sheathing.

and,

Recent renovations to the house include dark yellow plywood siding with vertical laths, resembling board and batten, as well as blue window trim. The window trim is respectful of the heritage of the home. The trim detail at the top of the windows comes to a slight point in the centre. This feature looks similar to a picture of the Clarkson house in the Barnett Scrapbook. Two chimneys visible in the older photograph are no longer present from the exterior.⁷⁹

There is no information on interior alterations to the building.

4.5 Comparative Analysis

The Clarkson Store is one of a limited number of examples of rural commercial buildings remaining from the 19th century in the City of Mississauga. Therefore, although altered on the exterior, the Clarkson Store is of heritage interest and value.

A limited number of purpose-built post office buildings were constructed in Toronto Township, now the City of Mississauga. There will be even fewer in the future. Therefore, although altered on the exterior, the former Post Office Building is of heritage interest and value as an example of a heritage resource that will diminish over time.

The William Clarkson House is a representative example of a 19th century vernacular style dwelling of wood frame construction with Gothic Revival characteristics. Examples of rural houses from this time period remaining in the City of Mississauga are a limited heritage resource. Therefore, although there have been some alterations to the exterior of the building, the William Clarkson House is of heritage interest and value.

⁷⁹ Ibid., 3-4.

5.0 EVALUATION OF UNDER “CRITERIA FOR DETERMINING CULTURAL HERITAGE VALUE OR INTEREST”, ‘ONTARIO REGULATION 9/06’

5.1 Introduction

The municipal addresses of 1130 and 1140 Clarkson Road North are listed on the City’s Heritage Register, but not designated under the OHA.

The “Criteria for Determining Cultural Heritage Value or Interest” set out in the ‘O. Reg 9/06,’ has been applied to the subject property to determine its cultural heritage value or interest for municipal designation under the OHA.

5.2 Design/Physical Value

Design or Physical Value of Property	
i. Rare, unique, representative or early example of a style, type, expression, material or construction method.	Yes
ii. Displays a high degree of craftsmanship or artistic merit.	No
iii. Demonstrates a high degree of technical or scientific achievement.	No

i. Representative of a style, type, expression, material or construction method

The physical/design value of the property relates specifically to the Clarkson Store and the William Clarkson House (*See comments below for each individual building on-site*).

Clarkson Store

This building started as a mid-19th century, one and-a-half storey, wood frame building. It was substantially altered c1906-1910 with the addition of a second storey. The early 20th century alterations are considered to be of sufficient age as to form an integral part of the history of the building. Although more recent exterior alterations have been completed, the store building retains its 1906-1910 height, scale, shape, form and massing and a commercial storefront. Dating to the mid-1860s, the Clarkson store on the subject property is considered to be a representative example of a vernacular rural commercial building of frame construction. Few examples of rural commercial stores of this age remain in the municipality.

Former Post Office Building

This modestly designed building dates to the late 1940s. Originally, it was designed as a two-storey flat roof, brick veneer structure with a commercial storefront. It has been significantly altered from its original appearance with the addition of a gable roof and rear additions.

William Clarkson House

This modest mid 19th century house is a vernacular example of the Gothic Revival architectural style that was popular in Ontario in the mid to late 19th century. It is one and-a-half storeys with a front/cross gable roof, rectangular floor plan, symmetrical arrangement of openings on the principal facades and a entry verandah. Originally, it had clapboard siding with cornerboards and door and window openings with triangular shaped heads on the front elevation. Although the house has undergone numerous exterior alterations, including, but not limited to, the installation of new siding, the addition of two roof dormers, the removal of the original chimneys and modifications to the door and window openings, it retains its dominant form and elements of its original design character.

Dating to the mid-to-latter part of the 19th century, the William Clarkson House is considered to be a representative example of a vernacular style dwelling of wood frame construction with Gothic Revival characteristics.

ii. Displays a high degree of craftsmanship or artistic merit

Clarkson Store

The residence is not considered to display a high degree of craftsmanship or artistic merit. However, its longevity attests to some degree of craftsmanship.

Former Post Office Building

The building is not considered to display a high degree of craftsmanship or artistic merit.

William Clarkson House

The residence is not considered to display a high degree of craftsmanship or artistic merit. However, its longevity attests to some degree of craftsmanship.

iii. Demonstrates technical or scientific achievement

Clarkson Store

No aspects of technical or scientific merit were identified for the building.

Former Post Office Building

Same comment as Clarkson Store above.

William Clarkson House

Same comment as Clarkson Store above.

5.3 Historical/Associative Value

Historical or Associative Value of Property	
i. Has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community.	Yes
ii. Yields, or has the potential to yield, information that contributes to an understanding of a community or culture.	Yes
iii. Demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.	No

i. Direct associations with a theme

The subject property is concluded to have direct associations with the historical theme of the settlement of the former Toronto Township and the village of Clarkson, which now lies within the boundaries of the City of Mississauga, and the Clarkson family, a significant family associated with the historical development of the area and village (*See comments below for the individual buildings on-site*).

Clarkson Store

The property at 1130-1140 Clarkson Road North is associated with the historical theme of the early 19th century settlement of the former Toronto Township in Peel County and the rural village of Clarkson. The store is illustrative of the typical subdivision in the early 19th century of land located along important roadways for commercial development. Located on Clarkson Road north of Lakeshore Road, the land was particularly attractive for development in the early history of the township. Warren Clarkson acquired ownership in 1835 and built a store on the site. Members of the Clarkson family were associated with the store ownership/management until 1919. The subject property has been the site of a store serving the area and village from 1835 to the present, over 180 years of business. The building also housed the Clarkson's Post Office from 1875 to the late 1950s, over 75 years of postal service.

Heritage Mississauga has recognized the historical importance of the history of Clarkson and the associative history of the Clarkson Store through its inclusion in its pamphlet *A Heritage Tour Clarkson est. 1808* that features all of the above noted properties as being landmark properties of Clarkson.

Former Post Office Building

The property at 1130-1140 Clarkson Road North is associated with the historical theme of the early 19th century settlement of the former Toronto Township in Peel County and the rural village of Clarkson. The Clarkson store became the site of the first Clarkson's Post Office in 1875. Early post offices were typically housed in stores since they were the centre of a community and the store owner/manager was typically appointed the postmaster/mistress like the Clarkson family members. Members of the Clarkson family were associated with the post office from 1875 to 1919. The post office remained in the Clarkson Store until the late 1940s when a new post office building was erected between

the store and residence. Clarkson's post office was located on the subject property from 1875 to the late 1950s, over 75 years of postal service.

William Clarkson House

The property at 1130-1140 Clarkson Road North is associated with the historical theme of the early 19th century settlement of the former Toronto Township in Peel County and the rural village of Clarkson. Warren Clarkson acquired ownership of the subject property in 1835 and built a store. His son William W. Clarkson took over the management of the family business in the mid 1860s and then built a residence beside the family store and post office, possibly in the late 1870s/early 1880s. Members of the Clarkson family resided in the house for over thirty years.

Heritage Mississauga has recognized the historical importance of the history of Clarkson and the associative history of the William Clarkson House through its inclusion in its pamphlet *A Heritage Tour Clarkson est. 1808* that features all of the above noted properties as being landmark properties of Clarkson.

ii. Understanding of a community or culture

The subject property at 1130-1140 Clarkson Road North contributes to an understanding of the evolution of the cultural landscape associated with the history of the rural village of Clarkson in the geographic Township of Toronto. Clarkson Road was named after the Clarkson family, who were early Toronto Township and area settlers. Various members of the Clarkson family lived near the subject property, including the original owners, Warren and Susannah Clarkson, to the south and their son Henry Clarkson in a house across the road from the family store, and a daughter, Edith Clarkson, who built a house just north of the subject property in 1913 at 1160 Clarkson Road North. Other early and prominent area settlers/residents to use the Clarkson store and railway station included the Harris family of Benares fame.

As the Clarkson's station developed into a busy shipping point for local fruit and produce, particularly strawberries, other businesses were established in the area of the store in the early 20th century, including, but not limited to, Alex Durie's store, the Merchants Bank, a basket factory and a coal Company. A post office for Clarkson remained open on Clarkson Road North until the late 1950s.

The subject property, located at 1130-1140 Clarkson Road North, set on a subdivided lot close to the roadway, remains as a physical reminder of the history and presence of the historical settlement of Clarkson within the current City of Mississauga and demonstrates the important contribution and service of the Clarkson family to the community (***See comments below for the individual buildings on-site.***)

Clarkson Store

Warren Clarkson built a house in 1819 on Lot 29, Concession SDS. The north to south running Clarkson Road was a significant early transportation route in the township and county. It was a well-travelled roadway regularly used for stagecoach conveyance by the

mid 1830s, attracted by the presence of the Clarkson store just north of Lakeshore Road as a stopping point. The Clarkson store formed the nucleus of the burgeoning small rural community in the mid 19th century. The railway station and post office were established as a result of the importance of the store in the area. In 1853, Warren Clarkson sold part of his land for a railway and Clarkson's Station was opened adjacent to the store. The Clarkson's Post Office was opened in the store in 1875. William W. Clarkson built a house beside his store in the late 1870s/early 1880s. A store has been located at this site since 1835.

Former Post Office Building

The Clarkson's Post Office was opened in the Clarkson store on the subject property in 1875. A new post office for Clarkson was built beside the store on the subject property in the late 1940s. It remained open on Clarkson Road North until the late 1950s. The Clarkson Store and the former Post Office Building on the subject property, together with the post office building constructed off-site at 1146 Clarkson Road, just north of the subject property, clearly illustrate the evolution of the postal history of Clarkson's village.

William Clarkson House

William W. Clarkson built a house beside his store in the late 1870s/early 1880s. Edith Clarkson subdivided her property into Plan G-13 in 1913 resulting in further residential development in Clarkson's village including her house at 1160 Clarkson Road North, just north of the family store and house on the subject property, and houses on Balsam Avenue.

iii. Designer/Builder

The designer/builders of the Clarkson Store, the former Post Office Building and the William Clarkson Store do not reflect the work of a builder of significance to the community (*See comments below for the individual buildings on-site*).

Clarkson Store

Warren Clarkson, an early settler in the area and Toronto Township, built the store and the post office. His son William Clarkson, a general store owner and postmaster for Clarkson, is believed to be the builder of the house beside his store and post office building.

Former Post Office Building

This building is believed to have been built for Harry Hare in the late 1940s by an undetermined builder.

William Clarkson House

William W. Clarkson, son of Warren Clarkson, and the manager of the Clarkson store from the mid-1860s to 1894 is credited with being the builder of this house. As the son of an early settler in the area and Toronto Township, William Clarkson served as the local

postmaster from 1875 to 1894 and as an important contributor and figure in the community a general store owner and postmaster for Clarkson.

5.4 Contextual Value

Contextual Value of Property	
i. Is important in defining, maintaining, or supporting the character of an area.	Yes
ii. Is physically, functionally, visually or historically linked to its surroundings.	Yes
iii. Is a landmark.	Yes

i. Character

Rural hamlets or villages on main roads, such as Clarkson, were once common in Ontario's landscape and provided important services that supported the local population. The rural hamlet supported, at a minimum, a nucleus of a general store, a few residences, perhaps a church, and occasionally a postal station for the area. The urban development of the latter part of the 20th century in Toronto Township has surrounded and incorporated the 19th century rural village of Clarkson and contributed to the diminishment of its earlier bucolic surroundings and associated historical fabric.

The subject property at 1130-1140 Clarkson Road North contains three buildings that are important in maintaining the physical evidence of the historical and rural community of Clarkson in the surrounding urban landscape (*See comments below for the individual buildings on-site*).

Clarkson Store

The Clarkson Store demonstrates the historical fabric of the former rural village of Clarkson.

Former Post Office Building

Same comments as the Clarkson Store as above.

William Clarkson House

Same comments as the Clarkson Store as above.

ii. Linkages

The Clarkson Store forms an important part of the narrative of the history of Clarkson. The buildings on the subject property at 1130-1140 Clarkson Road North are located on land subdivided in 1913 from the original farm lot acquired by Warren Clarkson in the early 1800s. The original location of the Clarkson Store close to the edge of Clarkson Road North has been retained. Clarkson Road North was an historically important north to south transportation route in Toronto Township. The early 20th century size and configuration of Lot 67, Plan G-13, on which the subject property is located, has been

maintained between railway line and Balsam Avenue. Located beside the railway line, the history of the subject property is closely linked to its history, which is important to 19th century development of Clarkson's village. The Clarkson Store has been in commercial use throughout its history. The former Post Office Building has been in either public or commercial use since built in the late 1940s and the William Clarkson House has been used for residential purposes for most of its history.

The subject property is the focal point in the former village of Clarkson that connects together historically and visually the adjacent railway track and other associated village properties including Edith Clarkson House at 1160 Clarkson Road, the former Post Office Building at 1146 Clarkson Road, the former Merchant's Bank/Bank of Montreal building at 1109 Clarkson Road North, the Alex Durie Store and Clarkson Basket Factory at 1119 Clarkson Road North, the Clarkson-Paisley House at 1141 Clarkson Road North, the Clarkson Barnett House at 1084 Feely Court and the Springcreek Cemetery at 1390 Clarkson Road North. As such, the subject property, with its three buildings, has contextual value because it is physically, functionally, visually or historically linked to its surroundings and it maintains and supports the character of Clarkson and it is a landmark.

In recognition of the Clarkson family, the road passing in front of the general store and post office, the train station, and the village itself were all named Clarkson. The local GO Transit station still bears the family name. Furthermore, together the Clarkson Store, the former Post Office Building and the William Clarkson House have contextual value as significant elements in a discrete group of buildings that form a cultural heritage landscape on the subject property.

It is concluded the Clarkson Store, former Post Office Building and the William Clarkson House on the subject property at 1130-1140 Clarkson Road North are important in defining, maintaining and supporting the character of Clarkson as individual buildings and as a discrete cultural heritage landscape that is physically, functionally, visually and historically linked to its surroundings, and which form a local landmark (*See comments below for the individual buildings on-site*).

Clarkson Store

The Clarkson Store served as a community-meeting place and serves as an anchor building and a focal point for the three on-site buildings at 1130-1140 Clarkson Road North. It is located prominently on the south end of the property beside the railway track. The store is intricately linked with the history of the Clarkson family, the former Post Office Building and the William Clarkson House, the history of the adjacent railway and Clarkson's village.

Former Post Office Building

The subject property served as a community-meeting place as the location of Clarkson's post office from 1875 to the late 1940s, first in the family store, and then in the late 1940s to the late 1950s in the former Post Office Building. The building is historically linked with the Clarkson Store and the William Clarkson House and Clarkson.

William Clarkson House

The William Clarkson House is closely linked with the history of the Clarkson family, the Clarkson Store and Clarkson's village. It served as the residence for the store owner/manger and, at times, the postmaster.

iii. Landmark

Rural hamlets on main roads, such as Clarkson, were once common in Ontario's landscape and provided important services that supported the local population.

It is concluded the subject property at 1130-1140 Clarkson Road North, including the Clarkson Store, the former Post Office Building and the William Clarkson House, is of landmark value to residents and travellers, as a visual reminder of the historical centre of the village of Clarkson. *(See comments below for the individual buildings on-site).*

Clarkson Store

Views to the subject property and associated buildings are afforded from Clarkson Road North from the north and south. The wood frame store, with the former post office and the one and-a-half storey frame house, is located in proximity to the roadway and is an important visual element in the roadscape. The built form is considered to be visually prominent and distinctive in the context of the area. The roadway is well-travelled and the building is well-known in the area. Heritage Mississauga has included the Clarkson Store in a pamphlet profiling significant heritage buildings in the Clarkson area, thus recognizing its landmark status.

Former Post Office Building

Views to the subject property and the associated buildings are afforded from Clarkson Road North from the north and south. The former Post Office Building, located between the Clarkson Store and the William Clarkson House, forms an integral part of the important historical and visual element, i.e., the Clarkson Store, the former Post Office Building and the William Clarkson House, in the roadscape. The built form is considered to be visually prominent in the context of the subject property and the area. The roadway is well-travelled and the former Post Office Building is well-known in the area.

William Clarkson House

Views to the subject property and associated buildings are afforded from Clarkson Road North from the north and south. The wood frame house, with the former Post Office Building and the Clarkson Store, is located in proximity to the roadway and is an important visual element in the roadscape. The built form is considered to be visually prominent and distinctive in the context of the area. The roadway is well-travelled and the building is well-known in the area. Heritage Mississauga has included the Clarkson Store in a pamphlet profiling significant heritage buildings in the Clarkson area, thus recognizing its landmark status.

5.5 Summary

It has been determined through the application of the “Criteria for Determining Cultural Heritage Value or Interest” under ‘O. Reg 9/06,’ that the subject property at 1130-1140 Clarkson Road North, including the three buildings referred to as the Clarkson Store, the former Post Office Building and the William Clarkson House, is of cultural heritage value or interest due to its physical or design value, historical or associative value and contextual value.

6.0 STATEMENT OF CULTURAL HERITAGE VALUE UNDER ‘ONTARIO REGULATION 9/06’

The subject property is located on Lot 67, Plan G-13, formerly part of Lot 28, Concession 2 SDS, Toronto Township at the municipal address of at 1130-1140 Clarkson Road North, City of Mississauga. Both the municipal addresses of 1130 and 1140 Clarkson Road North are listed on the City’s Heritage Register, but not designated under the OHA.

The subject property, in its entirety, functions as a physical, historical and contextual reminder this was once the principal location of the Clarkson’s village commercial core from the 19th century onwards.

Statement of Cultural Heritage Values

Design/Physical Value

The subject property located at 1130-1140 Clarkson Road North in the City of Mississauga is an important and significant cultural heritage property that has design, associative and contextual values.

The design/physical value of the property relates specifically to the Clarkson Store and the William Clarkson House.

The Clarkson Store was substantially from its 19th century form c1906-1910. These early 20th century alterations are considered to be of sufficient age as to form an integral part of the history of the building. Although more recent exterior alterations have been completed, the store building still retains its 1906-1910 height, scale, shape, form and massing and a commercial storefront of an early 20th century rural store. Examples of rural commercial stores of this age are considered to be a diminished resource in the City.

The William Clarkson House is a vernacular, mid 19th century, wood frame residence with characteristics of the Gothic Revival style. When built, it was one and one-half storeys high with a front/cross gable roof, rectangular floor plan, symmetrical arrangement of openings on the principal facades and a wrap-around verandah. Exterior details included clapboard siding with cornerboards and door and window openings with triangular shaped heads on the front elevation. Despite later alterations to the exterior

including the installation of new siding, the addition of two roof dormers, removal of the original chimneys and some modifications to the door and window openings, the building retains its dominant form and exterior elements of its original design character.

Historical/Associative Value

The property has historical value for its associations with an important settlement family that contributed significantly to the settlement of Clarkson and Toronto Township in the 19th century. In recognition of the significance of the Clarkson store to the community, the road passing in front of the general store, and later post office, the train station, and the village were all named Clarkson. The local GO Transit train station still bears the family name.

The Clarkson family was fundamental to the development of the local community. Warren Clarkson was one of the first settlers in this area of Toronto Township. His decision to locate the first general store in the area, the construction of the railway beside the store, which ensured the community's economic viability, and the location of the first postal service in the store represent and illustrate important social and development periods of the community's history.

The property was the site of the first post office in Clarkson, an event that meant local residents no longer had to travel to Erindale to retrieve their mail. The post office was located in the Clarkson Store, the focal point of the community. It was located on the subject property for 80 years from 1875 to 1955. A new, separate post office building was constructed in the late 1940s beside the store and house.

Contextual Value

The subject property, in its entirety, has been identified as having contextual value relating to the historical, visual, physical and functional links between the property, its buildings and the surrounding neighbourhood that once formed the nucleus of Clarkson's Corners, later Clarkson, people and activities that were significant to the community and its ability to yield information that contributes to an understanding of the community. The property was the site of Warren Clarkson's store in 1835. The store became the nucleus of the historic settlement community and in 1875, the location of the first post office to serve Clarkson.

The three buildings on the subject property are positioned on the north side of the railway tracks facing onto and set close to Clarkson Road North, an important transportation north to south route both historically and currently. This setting of the subject property is valued for maintaining the 19th and early 20th century character of the historic village core of Clarkson village. The original Clarkson land holdings were subdivided as Plan G-13 in 1913 and the buildings were placed on Lot 67. The configuration of Lot 67 has not been subdivided since 1913 and is clearly delineated in the environment between the railway tracks to the south and Balsam Avenue to the north.

The property with its buildings is the pivot point for historically, visually and contextually linking the adjacent properties associated with the Clarkson family and the village. The Clarkson Store on the south end beside the railway track and the William Clarkson House on the north end of the property at Balsam Avenue are clearly visible in the streetscape. The subject property at 1130-1140 Clarkson Road North is considered to be a physical landmark in the community.

The heritage attributes of the subject property located at 1130-1140 Clarkson Road North include, but are not limited to the following listed.

Heritage Attributes

- The placement and orientation of the buildings, the Clarkson Store, the former Post Office Building and the William Clarkson House, in their original positions on the west side of Clarkson Road North to the north at the railway tracks with the principal elevations facing onto the said road.
- The integrity of Lot 67, Plan G-13 on which the three buildings are located.
- Its pivotal position in the historic core of Clarkson as an historical, visual and contextual link adjacent to the neighbourhood properties associated with the Clarkson family, former commercial, industrial and residential buildings that were clustered around the store and the location of the former Clarkson's railway station.
- The wood frame construction, the scale, height, form, massing, gable roofline, original window and door openings and the commercial storefront of the Clarkson Store.
- The wood frame construction, the scale, height, form, massing, gable roofline, original window and door openings, and verandah of the William Clarkson House.
- The north and south views to the subject property and its three buildings from Clarkson Road North.

7.0 RECOMMENDATIONS

The subject property at No. 1130-1140 Clarkson Road North, which includes the three buildings known as the Clarkson Store, the former Post Office Building and the William Clarkson House, meets the evaluation criteria of under 'O. Reg. 9/06,' for design/physical value, historical/associative value and contextual value.

Therefore, it is recommended the municipal property known as 1130-1140 Clarkson Road North in the City of Mississauga, in its entirety, should be designated under Part IV of the OHA for its design, historical/associative and contextual value.

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Image BA0062, Clarkson Post Office, Interior, 1910.
Image BA0074, Carload Groceteria, undated.
Image BA0075, Carload Groceterias, undated.
Image BA0154, William Clarkson House, undated.
Image BA0155, Clarkson Store and Railway Station, c1906.
Image BA0156, Edith and Cora Clarkson, undated.
Image HA0039, Alex Durie's Store and the Edith Clarkson Store, Clarkson, 1910.,
undated.
Image HA0040, Cora Clarkson sitting in front of the William Clarkson General Store
and Post Office, undated.
Image HA0041, Clarkson Store, Clarkson, undated.
Image HA0042, Clarkson Railway Station, undated.
Image J771, William Clarkson House, Clarkson, 1980.
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Contact

Paula Wubbenhorst, Senior Heritage Coordinator, City of Mississauga.

Elaine Eigl, Heritage Coordinator Research, City of Mississauga.

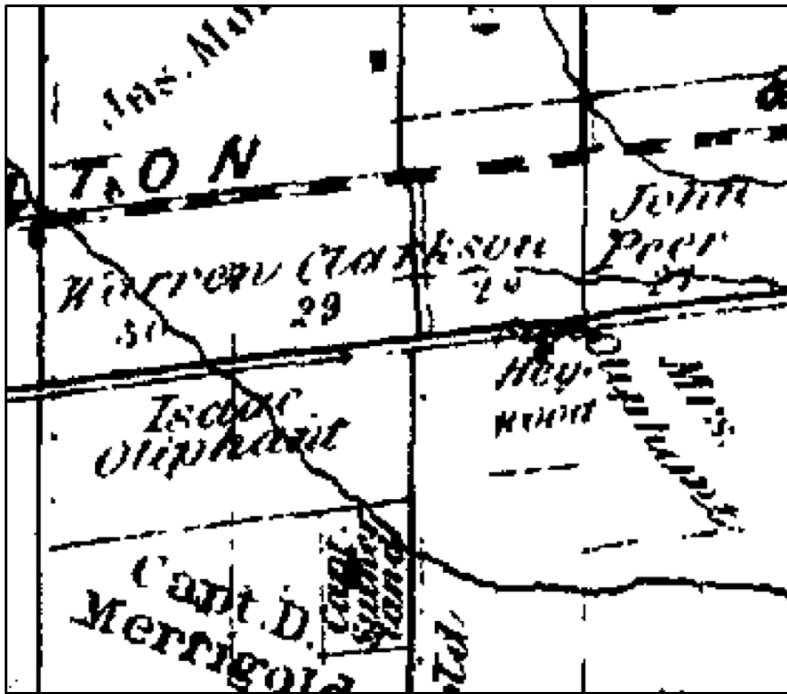
**Appendix A:
Building Chronology:
Clarkson Store,
Former Post Office Building &
William Clarkson House**

1130 Clarkson Road North – Clarkson Store	
1835	Construction of first store on Clarkson Road North, then a trail, by Warren Clarkson on his property.
1859	<i>Tremaine Map</i> does not show a building in the location of the store; however, this map did not always represent all buildings.
Mid-to-late 1860s	Possible construction date by William Warren Clarkson of the 1 ½ storey, front gable roof, wood frame store as shown in historical photographs.
1871	William Warren Clarkson referred to as a trader in the Census Return, presumably a reference to the store.
1873-74	Lynch Directory notes William Clarkson as a “peddler”.
June 1875	Clarkson’s post office opened in the store; William Warren Clarkson appointed as first postmaster.
1877	The Toronto Township map in the <i>Illustrated Historical Atlas</i> (1877) shows one building on the subject property, believed to be the store.
1906-1910	Substantial alterations to the 19 th century building with the addition of a 2 nd floor on the original building, a north addition creating an “L” Plan and a separate interior area for the post office. Edith Clarkson probably carried out the work.
1913	Plan G-13 shows the store with its current “L” shape plan.
1934	Renovations to the storefront and interior completed for the opening of the Carload Groceteria; completed by W.T. McCord.
1951	Post office removed from the store when a separate building constructed on north side of the store; believed to have been completed by Harry U. Hare.
1980s	Two separate businesses operating in the building.
2014-15	Substantial renovations to the exterior and interior of the building by present owner.

1132 Clarkson Road North – Former Post Office Building	
Late 1940’s	Separate post office building constructed on north side of the store.
Circa 2014	Substantial renovations to the exterior and interior of the building by present owner.

1140 Clarkson Road North – William Clarkson House	
Mid-to-late 19 th century	Construction of the 1 ½ storey, front gable roof, wood frame house, believed to have undertaken by William Warren Clarkson. The house may have been built after William W. Clarkson inherited the store property from his father in 1882.
1877	The Toronto Township map in the <i>Illustrated Historical Atlas</i> (1877) shows one building on the subject property, believed to be the store, not the house.
1913	Plan G-13 shows the house with a larger rear addition and possibly a wrap-around verandah on the south side and now demolished building to the west of the house.
2014	Substantial renovations to the exterior and interior of the building by present owner.

**Appendix B:
Historical Maps
Photographs
and Plan**



Warren Clarkson's property in the Clarkson area in the late 1850s [Tremaine's Map of the County of Peel, Canada West. Toronto: George C. Tremaine, 1859].



The William Clarkson property on Lots 28, 29, and 30, Concession 2 SDS, and the village of Clarkson in the late 1870s as shown in 1877. Note the building is shown in the location of the current Clarkson Store and marked as the property of W.W.C. for William Warren Clarkson [Illustrated Historical Atlas, 1877].



Early 20th century topographic map showing the community of Clarkson [NTS, Brampton, 30 M/12 1909].



An early 20th century topographic map shows the community of Clarkson [NTS, Brampton, 30 M/12, 1922].



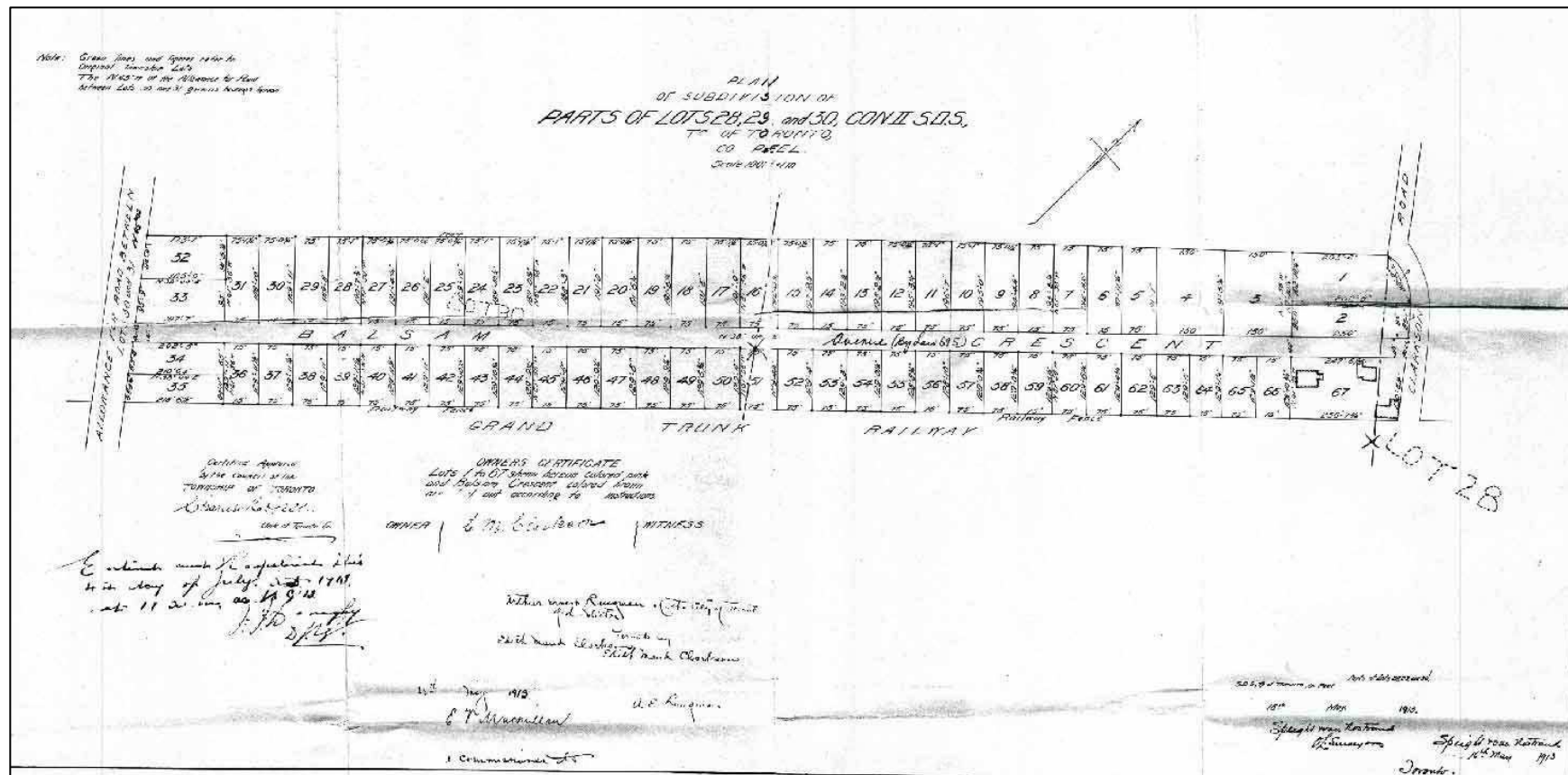
Late 20th century topographic map shows the urban environment of the community of Clarkson within the City of Mississauga [NTS, Brampton, 30 M/12, 1994].



This photograph shows Edith Clarkson and her sister Cora Clarkson, undated [Image BA0156, Mississauga Library System, Historic Images Gallery].



Clarkson's Railway station on the north side of the tracks [MLS, Historic Images Gallery, HA0414, undated].



Plan G-13, Plan of Subdivision of Parts Lot 28, 29 and 30, Con. II, S.D.S., Twp. Of Toronto, Co. Peel, Speights and van Nostrand, O.L. Surveyors, Toronto, May 15, 1913.



This photograph shows the storefront of the McCord Store in Clarkson [MLS, Historic Images Gallery, MC0414, undated].

Clarkson's Newest Shopping Centre
OPENS
THURSDAY, AUGUST 30TH
AT 2 P.M.

The increasing popularity of the "grocery type" store and the economies effected by this system of merchandising have resulted in changing our store (W. T. McCord) from a service grocery to a CARLOAD GROCETERIA. We are confident that the residents of Clarkson and district will approve of this change and find both pleasure and profit in shopping at our new Groceteria.

Community Weiner Roast & Dance Sept. 5th

To celebrate the opening of this new CARLOAD GROCETERIA a community Weiner Roast and Dance is to be held on WEDNESDAY EVENING, SEPTEMBER 5th. Come and bring your friends.

TICKETS FREE TO OPENING DAY CUSTOMERS
AUGUST 30th

Tickets for dancing and refreshments will be given to all opening day customers. Be on hand early, THURSDAY, August 30th, and secure your tickets. Store opens at 2 p.m.

DELIVERY SERVICE: Delivery is available on all orders for the sum of 5c per order.

SHOPPING BAGS — FREE SAMPLES and GIFTS

Some of Our Many GROCERY SPECIALS

Sugar, 10 lbs.	53c	Shortening, 2 lbs.	17c
Lard, per lb.	11c	Black Fly Insecticide, 8 oz. tin	29c
White Sponge Flour, 7½ lbs.	23c	Whole Mixed Pickling Spice, 1 lb.	19c
Hillcrest Pears, No. 4's, 20's	25c	Olives, 1 oz. Queen	15c
Pure Light Honey, 2½ lbs.	2 for 19c	Olives, 11 oz. Queen	19c
C. & B. Baked Chicken	23c	Olives, 8 oz. Stuffed	19c
Chief Ready Meals	2 for 21c	Sugar Crisp Corn Flakes, 3 for	19c
Maxwell House Coffee, 1½ lbs.	35c	Scribbles (Corked) 180 pages	3c
Yonges, Speltz or Cider, per gal.	35c	Lead Pencil	3 for 5c
Kipper Snacks	6 for 25c	Kak's Castle Soap	4 for 19c
Waxed Paper, 15 sheet roll	2 for 5c	St. William's Plain Jam, 32 oz.	21c
Waxed Paper, 40 ft. roll	10c	Schick's Tea, 5 lb.	30c

MEAT PRICES
MEATS AT CARLOAD PRICES
PRICES CUT BUT QUALITY MAINTAINED

BEEF	
Round Roast of Beef	lb. 11c
Round Steak Roast or Sliced	lb. 14c
Prime Rib Roast of Beef	lb. 12c
Shoulder Roast of Beef	lb. 10c
Pot Roast	lb. 8c
Brisket	lb. 5c
Lean Hamming Steak	3 lbs. 19c
Steering Beef	3 lbs. 19c
LAMB	
Choice Spring Lamb Legs	lb. 18c
Short Loin	lb. 19c
Forelegs	lb. 12c
Steering Lamb	lb. 10c
VEAL	
Leg of Veal	lb. 19c
Veal Chop	lb. 17c
Half Fillet of Veal	lb. 13c
Home-made Sausages	3 lbs. 25c

WATCH THURSDAY'S PAPER FOR BUTTER PRICES

An opening announcement of the Carload Groceteria in the local newspaper [*Port Credit News*, August 29, 1934].



Clarkson Store, front (east) elevation [Heritage Mississauga, 2005].



Clarkson Store, front (east) elevation [Heritage Mississauga, 2005].



William Clarkson House, front (east) elevation [Heritage Mississauga, 2005].



William Clarkson House, northeast corner [Heritage Mississauga, 2005].

Appendix C: Cultural Heritage Landscape Photographs



View north on Clarkson Road North from Birchwood Park. Note the William Clarkson Store is visible in the streetscape.



View north on Clarkson Road North from Auld's Butchers at 1109, a municipally listed property. Note the Clarkson Store and former Post Office Building are visible in the streetscape.



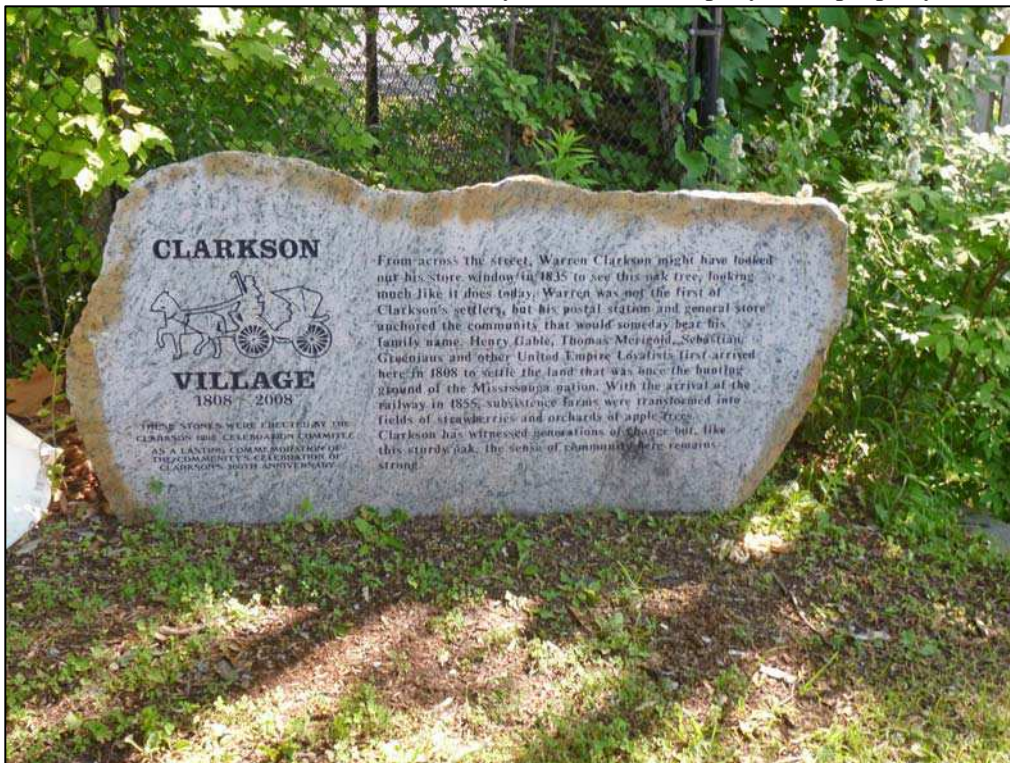
1119 Clarkson Road North, a municipally listed property.



Former Clarkson Basket Factory buildings behind 1119 Clarkson Road North.



1141 Clarkson Road North, Clarkson-Paisley House, a municipally listed property.



Commemorative stone located in front of 1141 Clarkson Road North.



View west from Clarkson Road North to Balsam Avenue with the William Clarkson House on the left and a former Post Office Building, now in educational use, on the right.



Cox House, 1176 Balsam Avenue, a municipally listed property.



Edith Clarkson House at 1160 Road North, a municipally listed property.



View south on Clarkson Road North to 1130-1140 at Balsam Avenue.

Appendix D:
Built Heritage Resource Photographs



East elevation (east) of the Clarkson Store (left) and former Post Office Building (right) [July 2015].



North elevation of the former Post Office Building with additions to rear [July 2015].



Rear (west) elevation s of the William Clarkson House (left), former Post Office Building and Clarkson Store (right) [July 2015].



South elevation (east) of the Clarkson Store [July 2015].



Front (east) elevation of the William Clarkson House [July 2015].



Front (east) elevation of the William Clarkson House [July 2015].



North and east elevations of the William Clarkson House [July 2015].



Rear (east) elevations of the William Clarkson House [July 2015].

Appendix E:
“Criteria for Determining Cultural Heritage Value or
Interest”
Ontario Heritage Act, ‘Ontario Regulation 9/06’

Criteria for Determining Cultural Heritage Value or Interest

Ontario Heritage Act ONTARIO REGULATION 9/06

CRITERIA FOR DETERMINING CULTURAL HERITAGE VALUE OR INTEREST

Criteria

1. (1) The criteria set out in subsection (2) are prescribed for the purposes of clause 29 (1) (a) of the Act. O. Reg. 9/06, s. 1 (1).

(2) A property may be designated under section 29 of the Act if it meets one or more of the following criteria for determining whether it is of cultural heritage value or interest:

1. The property has design value or physical value because it,
 - i. is a rare, unique, representative or early example of a style, type, expression, material or construction method,
 - ii. displays a high degree of craftsmanship or artistic merit, or
 - iii. demonstrates a high degree of technical or scientific achievement.
2. The property has historical value or associative value because it,
 - i. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,
 - ii. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or
 - iii. demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.
3. The property has contextual value because it,
 - i. is important in defining, maintaining or supporting the character of an area,
 - ii. is physically, functionally, visually or historically linked to its surroundings, or
 - iii. is a landmark. O. Reg. 9/06, s. 1 (2).

Transition

2. This Regulation does not apply in respect of a property if notice of intention to designate it was given under subsection 29 (1.1) of the Act on or before January 24, 2006. O. Reg. 9/06, s. 2.

City of Mississauga

Corporate Report



Date: December 17, 2015 To: Chair and Members of Heritage Advisory Committee From: Paul Mitcham, P. Eng, MBA, Commissioner of Community Services	Originator's files:
	Meeting date: 2016/01/12

Subject

Request to Demolish a Heritage Listed Property: 49 Queen Street South (Ward 11)

Recommendation

1. That the property at 49 Queen Street South, which is listed on the City's Heritage Register, is not worthy of heritage designation, and consequently, that the owner's request to demolish proceed through the applicable process.

Background

The owner received permission to demolish the existing structure at the subject property in June 17, 2014. (The corresponding report is attached as Appendix 1. Please note the Heritage Impact Assessment is available upon request due to changes in the application of FOI legislation) The redevelopment has not yet occurred. In April 2014, the heritage permit by-law was updated with a year expiry clause. As such, this item is again before the Heritage Advisory Committee. The addendum to the Heritage Impact Assessment provides updated drawings for the proposed infill development subject to other City approvals for information purposes.

Comments

The property continues to have no cultural heritage value. As such, the demolition should be approved.

Financial Impact

There is no financial impact.

Conclusion

The proposal to demolish 49 Queen Street South is before the Committee due to an expiry clause in the heritage permit by-law. The property does not merit designation under the *Ontario Heritage Act*. As such, the

proposed demolition should be allowed to proceed. The addendum in Attachment 2 shows updated drawings depicting the proposed infill development subject to other City approvals. The addendum is included for information purposes.

Attachments

Appendix 1: June 17, 2014 Corporate Report

Appendix 2: Addendum to HIA



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: C. Nin Hernandez, Heritage Coordinator



Corporate Report

Clerk's Files

Originator's
Files

DATE: May 20, 2014

TO: Chair and Members of the Heritage Advisory Committee
Meeting Date: June 17, 2014

FROM: Paul A. Mitcham, P. Eng., MBA
Commissioner of Community Services

SUBJECT: Request to Demolish a Listed Property Within a Cultural
Landscape
49 Queen Street South
(Ward 11)

RECOMMENDATION: That the property located at 49 Queen Street South, which is listed on the City's Heritage Register as part of the Streetsville Core Cultural Landscape, is not worthy of heritage designation, and consequently, that the owner's request to demolish the structures be approved and that the appropriate City officials be authorized and directed to take the necessary action to give effect thereto, as described in the Corporate Report dated May 20, 2014 from the Commissioner of Community Services.

BACKGROUND: The subject property was Listed on the City's Heritage Register in 2005 as part of the Streetsville Core Cultural Landscape. This area is recognized as a significant cultural landscape because it retains a portfolio of heritage buildings of a consistent scale and portrays a period landscape of a small village with a commercial core element.

In July 2011, the property owner submitted Site Plan application SPI 11/129, in support of a new application to remove the existing single detached building and to replace it with a new two storey commercial office building. The subject property is adjacent to a Designated property located at 47 Queen Street South. A second property,

adjacent and located to the rear, located at 5 Ellen Street, is also proposed to be demolished as part of this Site Plan Application. However, because it is not listed on the City's Heritage Register, it is not subject to a Heritage Permit for demolition. The Heritage Impact Statement prepared by Joan Burt Architect is attached as Appendix 1. Landscaping and urban design matters will be reviewed as part of the Site Plan review process to ensure the project respects the character of the surrounding Cultural Landscape and adjacent heritage Designated property.

COMMENTS:

Section 27. (3) of the *Ontario Heritage Act* states that "structures or buildings on property listed on the City's Heritage Register cannot be demolished without 60 days' notice to Council. This allows Council time to review the property's cultural heritage value and to determine if the property merits designation, as set out under Regulation 9/06 of the *Ontario Heritage Act*. In order to merit designation, one or more of the following three criteria must be satisfied:

1. The property has design value or physical value;
2. The property has historical value or associative value;
3. The property has contextual value.

In addition, Section 27. (5) of the *Ontario Heritage Act*, states that Council may require the applicant to submit plans in support of a demolition application for a property included on the city's Heritage Register.

Furthermore, because this subject property is located adjacent to a Designated heritage property, located at 47 Queen Street South, 7.4.1.12 of the Mississauga Official Plan states that: "The proponent of any construction, development, or property alteration that might adversely affect a listed or designated cultural heritage resource or which is proposed adjacent to a cultural heritage resource will be required to submit a Heritage Impact Statement, prepared to the satisfaction of the City and other appropriate authorities having jurisdiction." Heritage Planning staff requested that the heritage consultant address any negative impacts to the cultural heritage attributes of the proposal to the Designated property.

The Heritage Impact Statement concludes the property at 49 Queen Street South is not worthy of heritage Designation under Regulation 9/06 of the *Ontario Heritage Act*. The existing structure does not illustrate a style, trend or pattern; have any direct association with an important person or event; illustrate an important phase in the city's social or physical development; nor does it illustrate the work of an important designer. It is Heritage Planning's opinion that the subject property does not warrant heritage conservation.

FINANCIAL IMPACT: There is no financial impact.

CONCLUSION: The property owner of 49 Queen Street South has requested permission to demolish a structure on a property within a Cultural Landscape listed on the City's Heritage Register. The subject property is not worthy of designation and the request for demolition should, therefore, be recommended for approval.

ATTACHMENTS: Appendix 1: Heritage Impact Statement by Joan Burt Architect



Paul A. Mitcham, P. Eng., MBA
Commissioner of Community Services

Prepared By: Laura Waldie, A/Senior Heritage Coordinator

RTD Business Centre

Mixed Use Development

49 Queen St. S & 5 Ellen St., Streetsville, Mississauga

5 E L L E N S T R E E T

Addendum to Heritage Impact Statement (Feb 2014)

Prepaid by: RALLY Architects/ Rafid Kustou

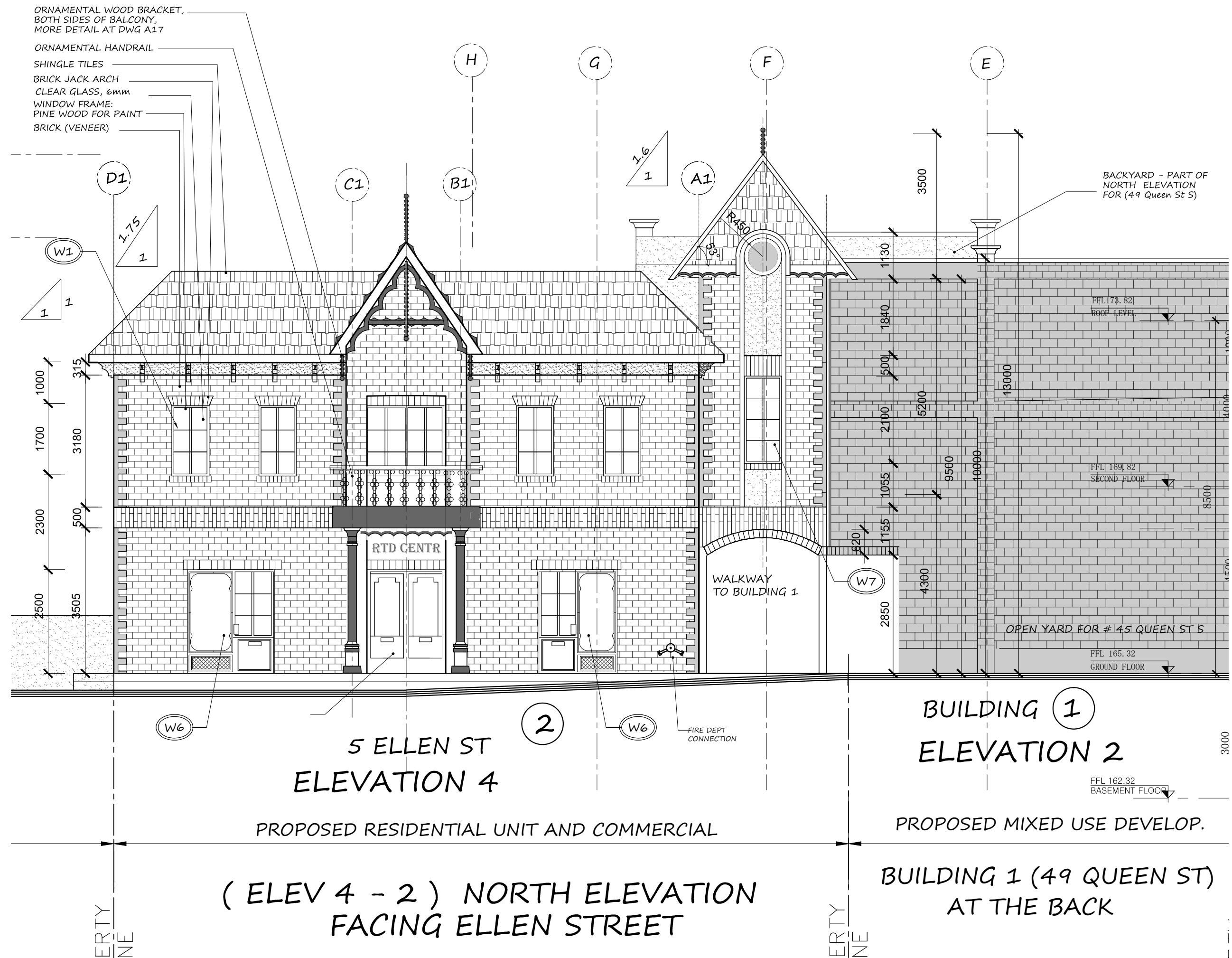
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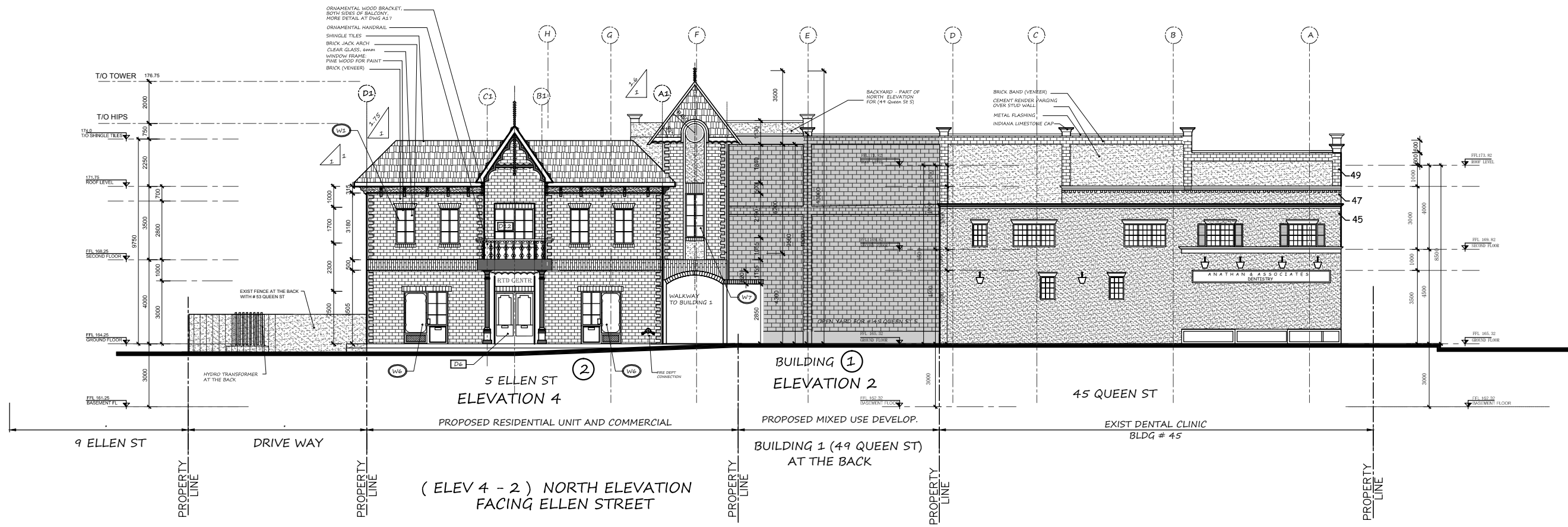
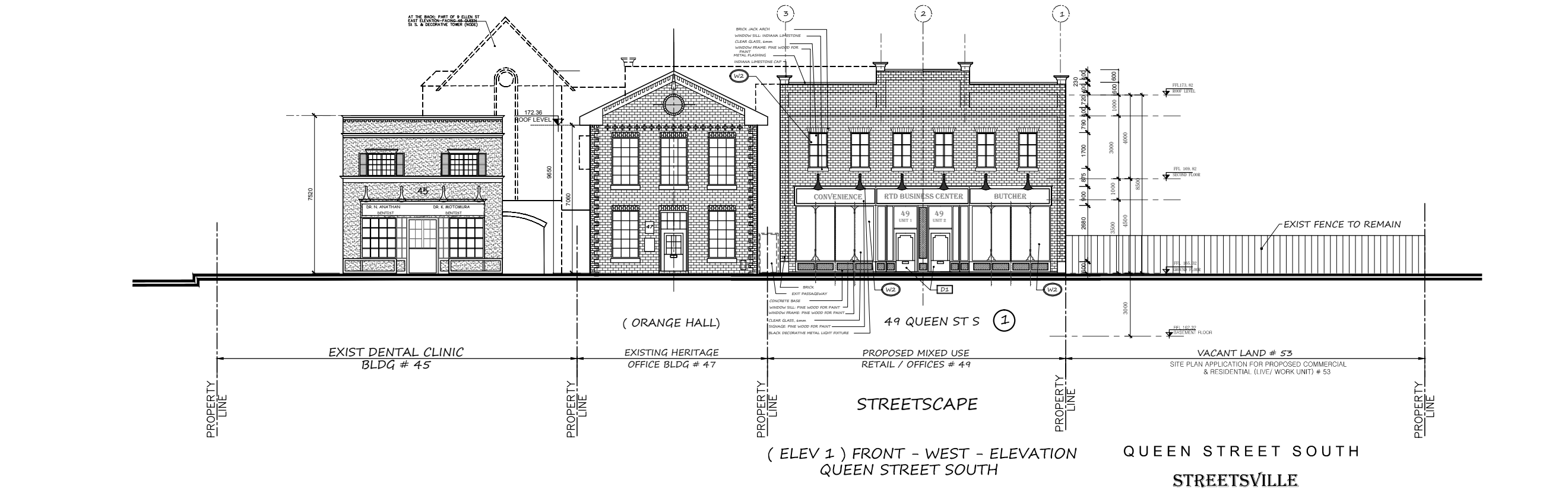


5 ELLEN STREET ELEVATIONS / DETAILS

ARCHITECTURAL DRAWINGS SITE PLAN - PLANS

LANDSCAPE DRAWING SITE PLAN





TRUE NORTH

PROJECT NORTH

KEY MAP

SITE

STREETSVILLE

PROJECT BRIEF

2 STOREYS MIXED USE DEVELOPMENT, ON -'S' SHAPE LOT, FACING 2 STREETS "THROUGH LOT", BESIDE A HERITAGE DESIGNATED "ORANGE HALL", SHARING RIGHT OF WAY & IN STREETSVILLE VILLAGE-MAINSTREET CHARACTER AREA

SURVEYORS REAL PROPERTY REPORT

PART OF LOTS 112 & 113
ACCORDING TO HYDE & RUTLEDGE'S PLAN
OF THE VILLAGE OF STREETSVILLE
REFERRED TO AS PLAN No. STR-2
CITY OF MISSISSAUGA
REGIONAL MUNICIPALITY OF PEEL

GENERAL NOTES

THIS DRAWING IS THE PROPERTY OF RALLY Architects. IT SHALL NOT BE USED OR REPRODUCED WITHOUT PRIOR PERMISSION. DRAWINGS SHALL NOT BE SCALED. CONTRACTORS TO VERIFY ALL DIMENSIONS AND CONDITIONS ON SITE AND REPORT ANY DISCREPANCIES TO THE ARCHITECT PRIOR TO COMMENCING WORK.

THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION PURPOSES UNLESS INDICATED AS "ISSUED FOR CONSTRUCTION".

IT IS CONTRARY TO THE ONTARIO BUILDING CODE ACT TO COMMENCE CONSTRUCTION PRIOR TO RECEIPT OF A VALID BUILDING PERMIT.

NOTES

1 THIS PROPOSED DEVELOPMENT IS CONSIDERED ONE BUILDING / ONE BUILDING PERMIT APPLICATION, WITH 2 ADDRESSES. THIS MATTER WAS DISCUSSED EARLIER (MAY 2015) WITH BUILDING DEPT., FIRE PREVENTION & FIRE SAFETY DEPT. AND PROPERTY & GEOMATICS DEPT.

2 FOR MATTER OF COORDINATION, WE WILL CALL: PORTION #1: (49 QUEEN ST S) IS THE MAIN BLDG. MIXED USE (COMMERCIAL-OFFICES) PORTION #2: (5 ELLEN ST) IS THE SMALL BUILDING RESIDENTIAL UNIT AT THE 2ND FL. & COMMERCIAL AT GR. FL.

3 ALL SIGNS SHOWN ON PROPOSED BUILDING ELEVATIONS (EXCEPT BARBER FREE SIGNAGE AND FIRE DEPT SIGN) NEED TO HAVE A SEPARATE PERMIT AND TO FOLLOW MISSISSAUGA SIGN BY-LAW.

ABBREVIATION

EXIST: EXISTING	UG: UNDER GROUND	TC: TOP OF CURB
CL: CENTRE LINE	SP: SITE PLAN	TW: TOP OF WALL
M: METER	ST: STEEL	TD: TOP OF
MM: MILLIMETER	SM: SIMILAR	UG: UNDER SIDE
MIN: MINIMUM	TYP: TYPICAL	

RAFIID KUSTOU
RALLY Architects

TEL: 416 454 9764 rallyarch@gmail.com

Sign: Date: NOV 20 2015

Copyright: RALLY

PROJECT TITLE: RTD CENTRE
2 STOREYS NEW BUILDING
MIXED USE DEVELOPMENT

OWNER: 2112836 ONTARIO INC

ADDRESS: 49 QUEEN STREET SOUTH & 5 ELLEN ST,
STREETSVILLE, MISSISSAUGA, ON, L5M 1K5

DRAWING TITLE: ELEVATIONS 1

REVISION:

REV	DATE	DISCUSSION
1	30-05-2015	BUILDING PERMIT APPLICATION
2	05-11-2015	REVISION 4 - SITE PLAN APPLICATION
3	20-11-2015	REV 1 - BLDG PERMIT APP - STATUS REPORT SEPT 22 2015

SCALE: 1/100

PROJECT # R3-2015

DATE: SEP 09 13

DRAWN BY: RAFID K

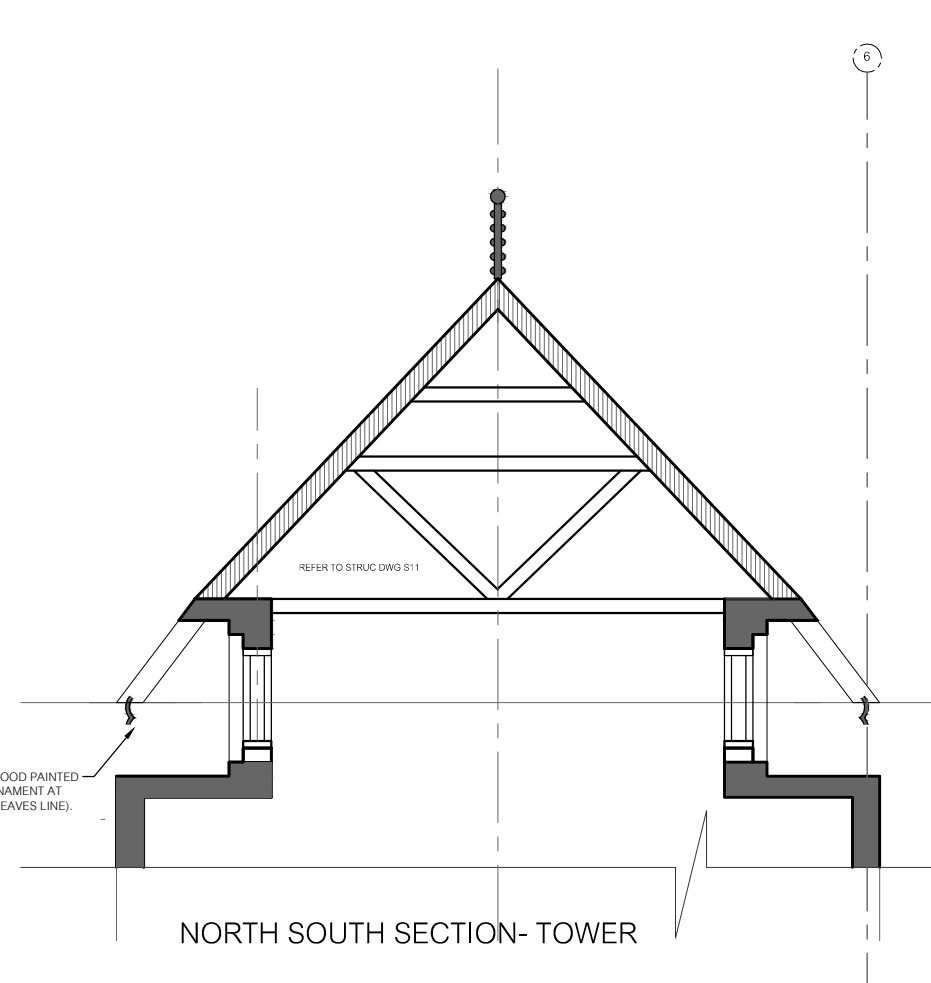
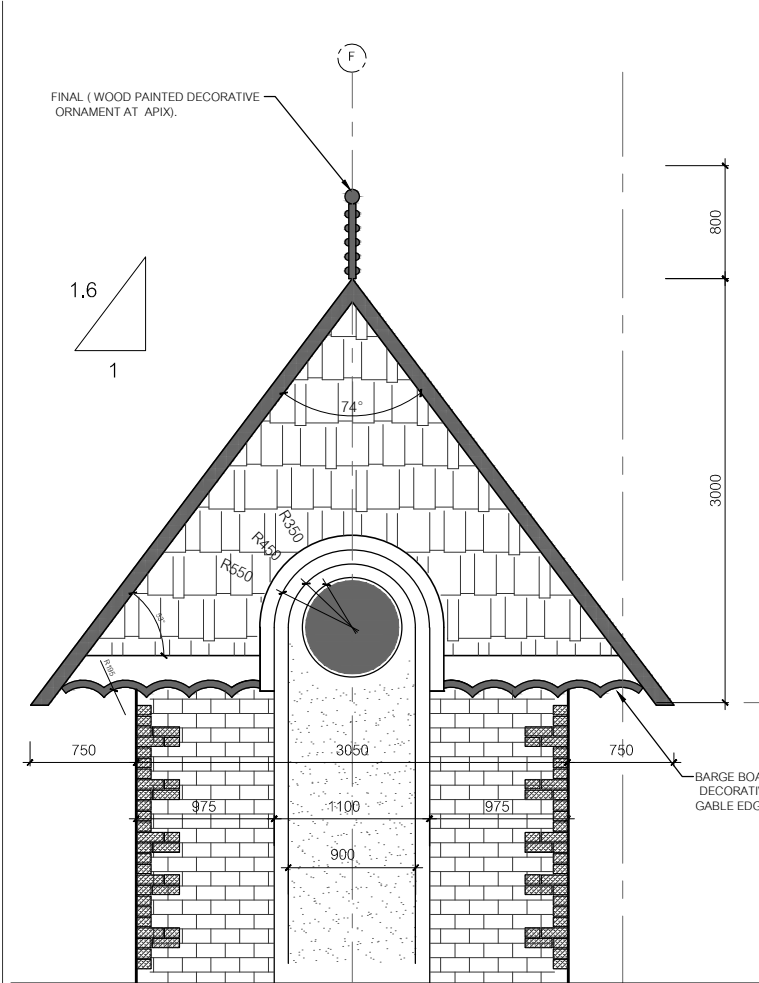
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SITE PLAN APP # SP 11/129

REV NO: 3

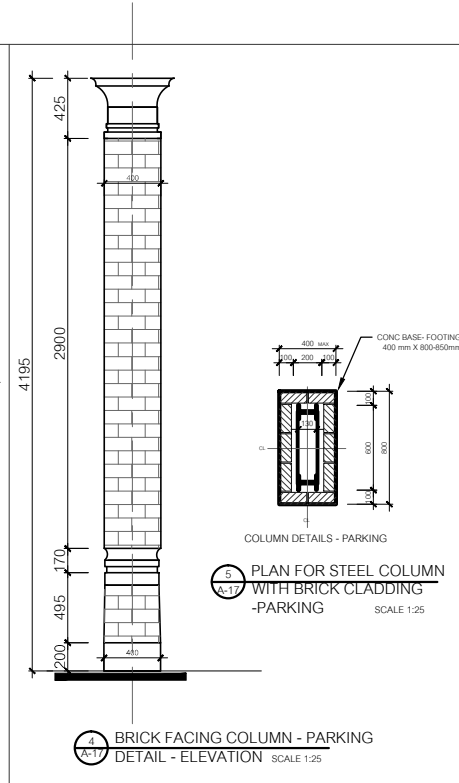
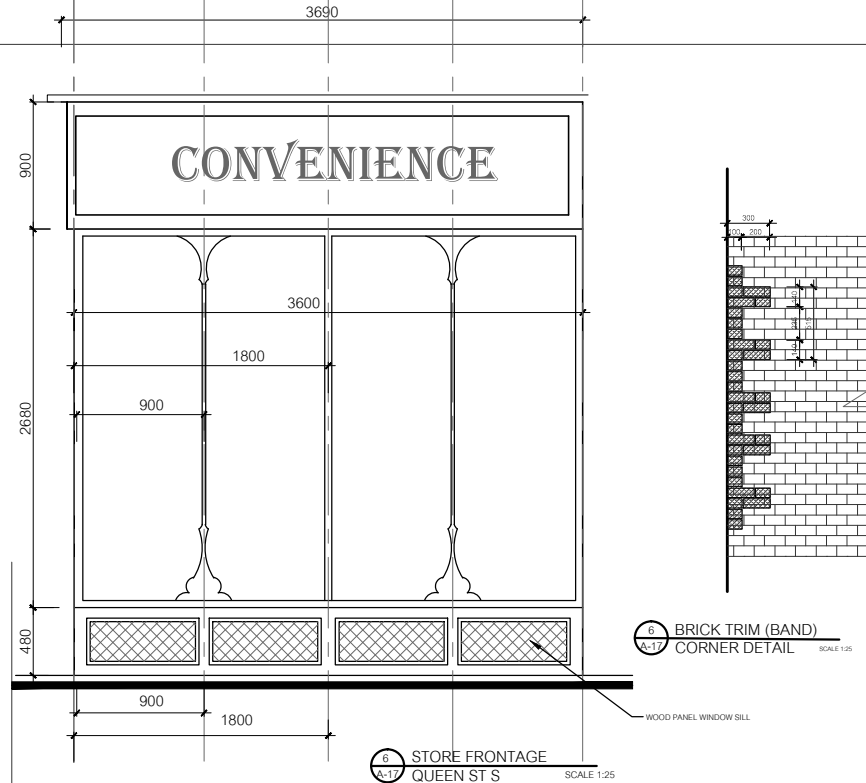
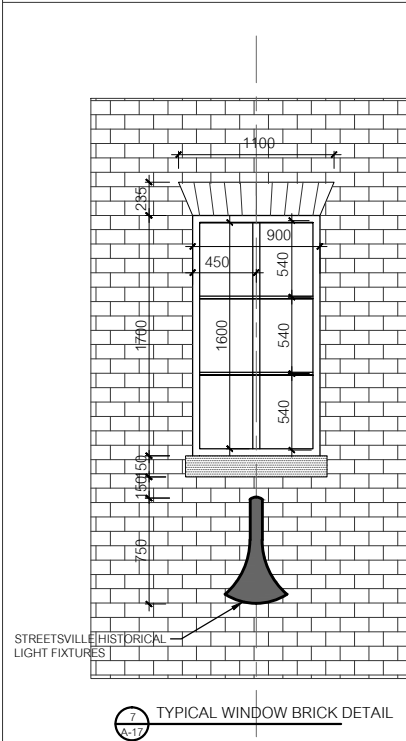
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Printed Dec 02 15



1. TOWER TOP DETAIL
5 ELLEN ST
SCALE 1:25

3. TOWER TOP SECTION
5 ELLEN ST
SCALE 1:25



TRUE NORTH

PROJECT NORTH

KEY MAP

SITE

STREETVILLE

PROJECT BRIEF

SURVEYORS REAL PROPERTY REPORT

GENERAL NOTES

NOTES

ABBREVIATION

RAFID KUSTOU

RALLY Architects

TEL: 416 494 9764 rallyarch@gmail.com

Sign:

Date: NOV 20 2015

Copyright: RALLY

RALLY

ONTARIO ASSOCIATION OF ARCHITECTS

RAFID KUSTOU

LICENCE: 7198

PROJECT TITLE:

OWNER:

ADDRESS:

DRAWING TITLE:

REVISION:

SCALE:

PROJECT #:

DATE:

DRAWN BY:

CHECKED BY:

SITE PLAN APP #:

BLDG PERMIT APP #:

REV NO:

DWG NO:

RTD CENTRE

2 STOREYS NEW BUILDING

MIXED USE DEVELOPMENT

2112836 ONTARIO INC

49 QUEEN STREET SOUTH & 5 ELLEN ST,

STREETVILLE, MISSISSAUGA, ON, L5M 1K5

ARCHITECTURAL DETAILS

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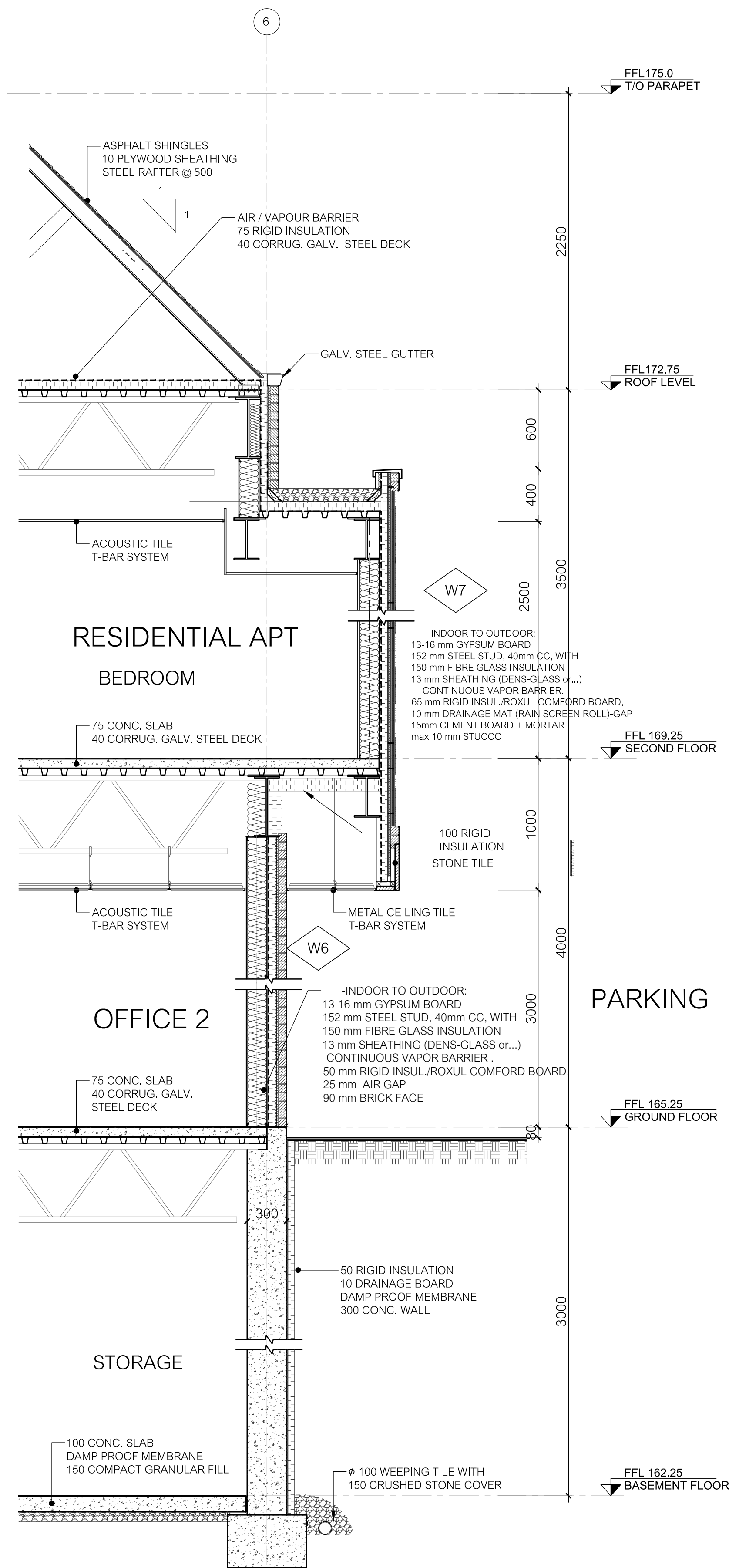
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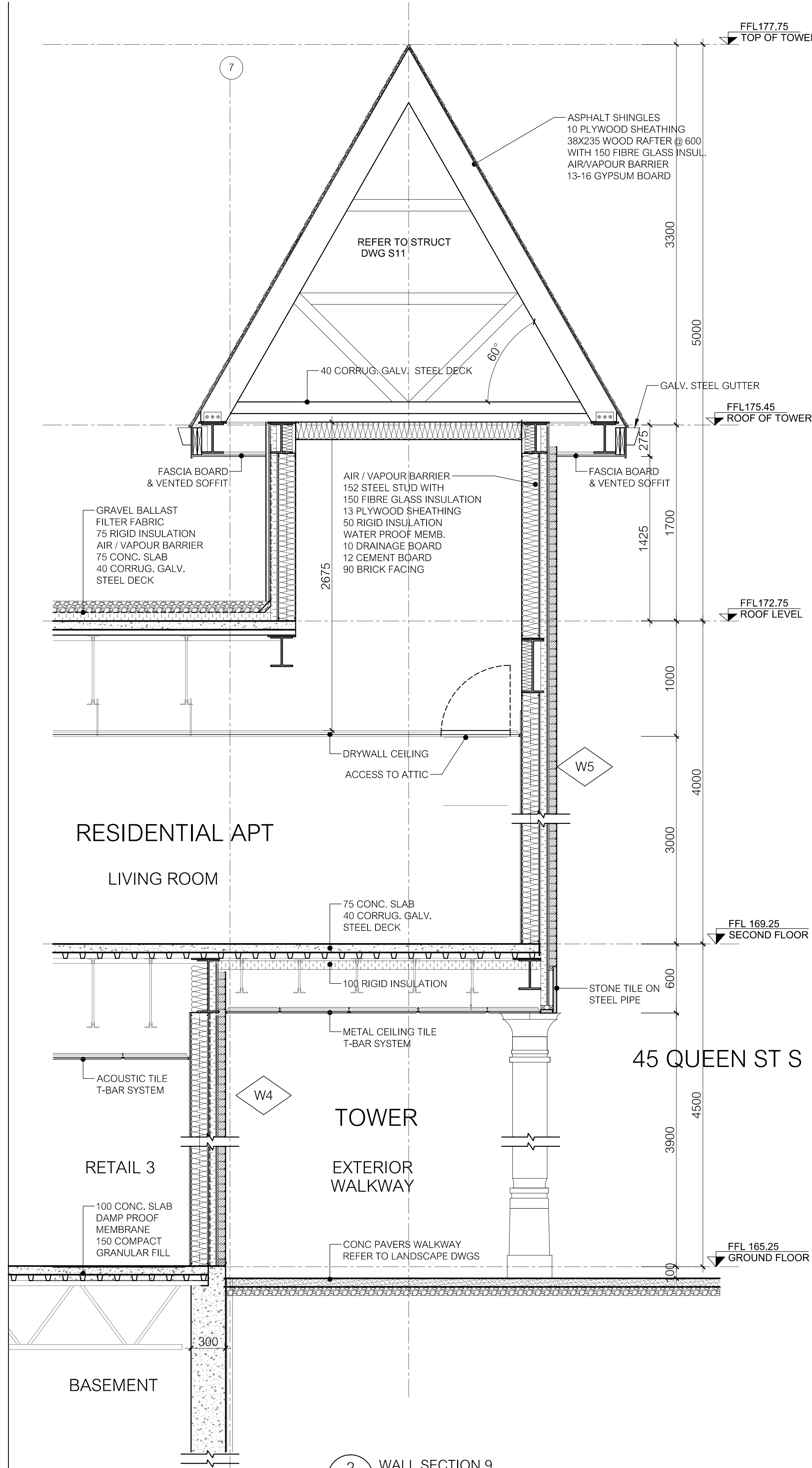
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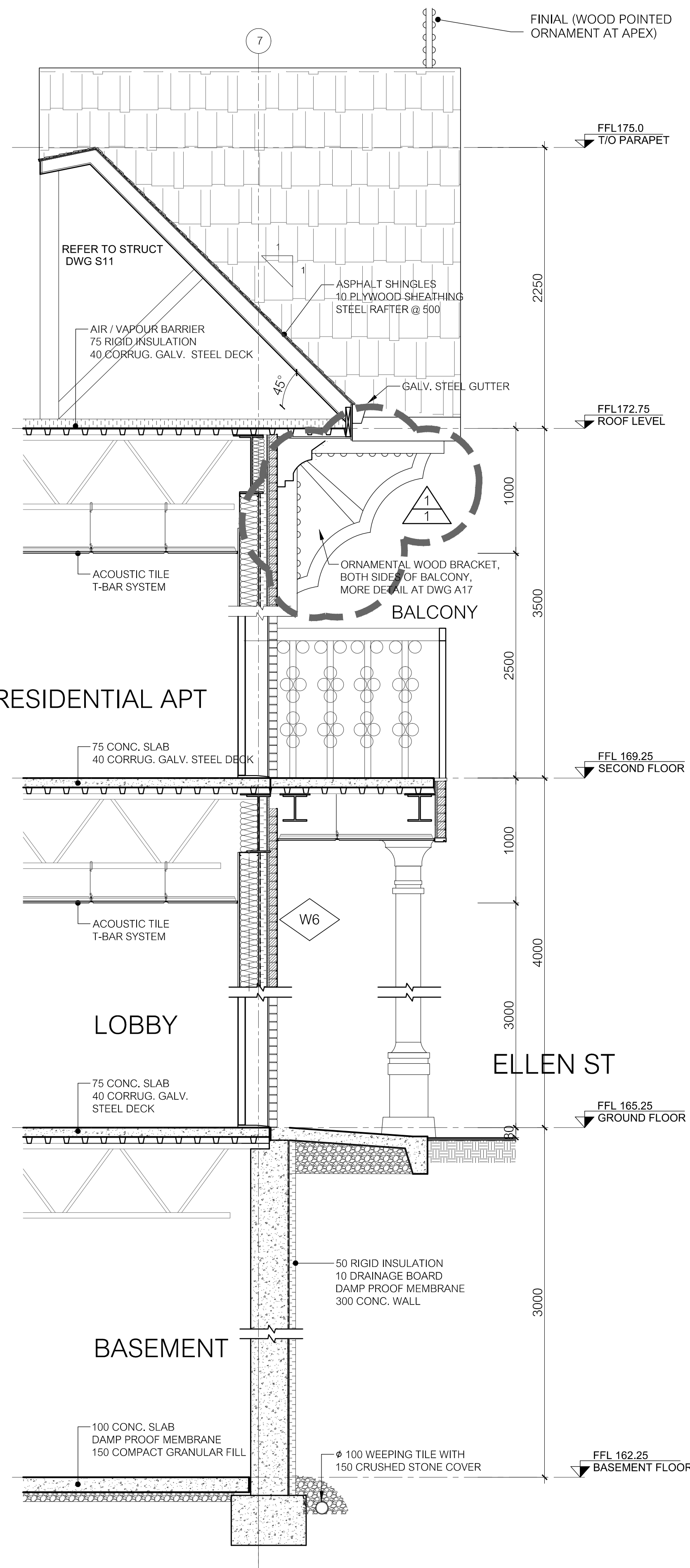
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3 WALL SECTION 10
A-15 SCALE 1:25 BUILDING 2



2 WALL SECTION 9
A-15 SCALE 1:25 BUILDING 2



1 WALL SECTION 8
A-15 SCALE 1:25 BUILDING 2

TRUE NORTH

PROJECT NORTH

ARCH. ST

9 ELLEN ST

RIGHT OF WAY

RTD

63

57

53

49

47

45

QUEEN STREET SOUTH

STREETSVILLE

PROJECT BRIEF

2 STOREYS MIXED USE DEVELOPMENT, ON -T- SHAPE LOT, FACING 2 STREETS "THROUGH LOT", BESIDE A HERITAGE DESIGNATED "ORANGE HALL", SHARING RIGHT OF WAY & IN STREETSVILLE VILLAGE-MAINSTREET CHARACTER AREA

SURVEYORS REAL PROPERTY REPORT

PART OF LOTS 112 & 113
ACCORDING TO HYDE & RUTLEDGE'S PLAN
OF THE VILLAGE OF STREETSVILLE
REFERRED TO AS PLAN No. STR-2
CITY OF MISSISSAUGA
REGIONAL MUNICIPALITY OF PEEL

GENERAL NOTES

THIS DRAWING IS THE PROPERTY OF RALLY Architects.
IT SHALL NOT BE USED OR REPRODUCED WITHOUT PRIOR PERMISSION
(DRAWINGS SHALL NOT BE SCALED) CONTRACTORS TO VERIFY ALL
DIMENSIONS AND CONDITIONS ON SITE AND REPORT ANY
DISCREPANCIES TO THE ARCHITECT PRIOR TO COMMENCING WORK.
THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION PURPOSE
UNLESS INDICATED AS ISSUED FOR CONSTRUCTION
IT IS CONTRARY TO THE ONTARIO BUILDING CODE ACT TO COMMENCE
CONSTRUCTION PRIOR TO RECEIPT OF A VALID BUILDING PERMIT

NOTES:

1 THIS PROPOSED DEVELOPMENT IS CONSIDERED ONE BUILDING / ONE BUILDING PERMIT APPLICATION, WITH 2 ADDRESSES. THIS MATTER WAS DISCUSSED
FAHUR (MARK 5015) WITH: BUILDING DEPT., FIRE PREVENTION & FIRE SAFETY DEPT
AND PROPERTY & GEOMATICS DEPT.

2 FOR MATTER OF COORDINATION, WE WILL CALL
PORTION # 1 (49 QUEEN ST S) IS THE MAIN BLDG: MIXED USE (COMMERCIAL OFFICES)
PORTION # 2 (15 ELLEN ST) IS THE SMALL BUILDING: RESIDENTIAL APT AT THE 2ND F.
ALL BUILDING ELEVATIONS (EXCEPT BARRIER FREE
SIGNAGE AND FIRE DEPT SIGN) NEED TO HAVE: A SEPARATE PERMIT AND TO
FOLLOW: MISSISSAUGA SIGN BY-LAW

ABBREVIATION

EXIST: EXISTING
CL: CENTRE LINE
m: METER
mm: MILLIMETER
MIN: MINIMUM

UG: UNDER GROUND
SP: SITE PLAN
ST: STEEL
SM: SIMILAR
TYP: TYPICAL

TC: TOP OF CURB
TW: TOP OF WALL
T/O: TOP OF
US: UNDER SIDE

RAPID KUSTOU
RALLY Architects

TEL: 416 494 9764 rallyarch@gmail.com
SIN: NOV 20 2015
Copyright: RALLY

RAPID KUSTOU
LICENCE
7196

PROJECT TITLE:

RTD CENTRE
2 STOREYS NEW BUILDING
MIXED USE DEVELOPMENT

OWNER:

2112836 ONTARIO INC

ADDRESS:

49 QUEEN STREET SOUTH & 5 ELLEN ST,
STREETSVILLE, MISSISSAUGA, ON, L5M 1K5

DRAWING TITLE

WALL SECTIONS - 3

REVISION:

REV DATE DISCRPTION

1 30-06-15 BUILDING PERMIT APPLICATION

2 20-11-2015 REV 1 - BLDG PERMIT APP - STATUS REPORT SEPT 23 2015

SCALE: 1/25

PROJECT # R3-2013

DRAWN BY: RAPID K

SITE PLAN APP # SP 11/120

DATE: SEP 09 13

CHECKED BY: RAPID K

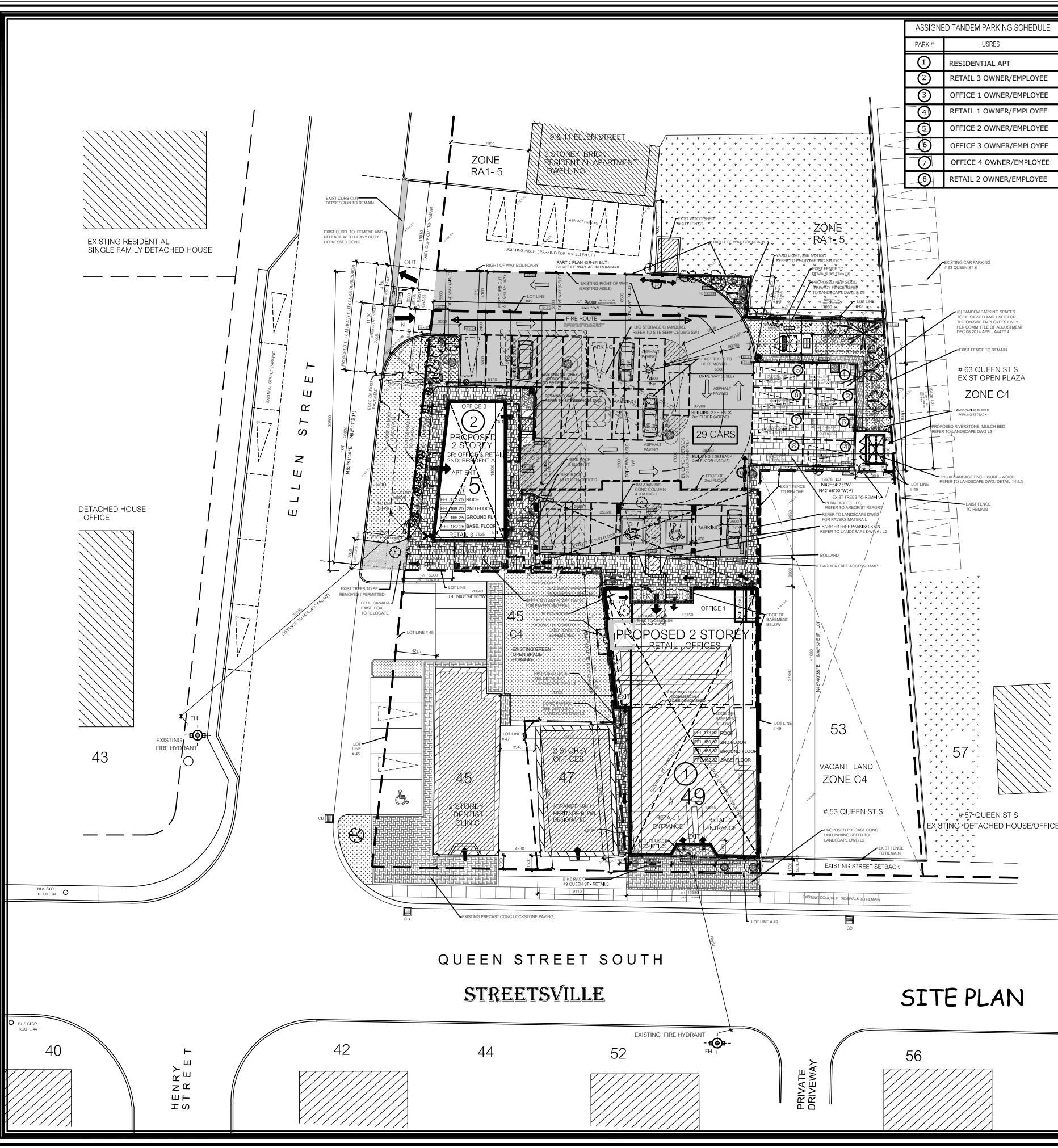
BLDG PERMIT APP # 5402

REV NO. 2

DWG NO. A15

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ASSIGNED TANDEM PARKING SCHEDULE	
PARK #	USRES
1	RESIDENTIAL APT
2	RETAIL 3 OWNER/EMPLOYEE
3	OFFICE 1 OWNER/EMPLOYEE
4	RETAIL 1 OWNER/EMPLOYEE
5	OFFICE 2 OWNER/EMPLOYEE
6	OFFICE 3 OWNER/EMPLOYEE
7	OFFICE 4 OWNER/EMPLOYEE
8	RETAIL 2 OWNER/EMPLOYEE

SITE PLAN APPLICATION # SP 11/129 W11 < > BUILDING PERMIT APPLICATION # 6402

R T D CENTER - MIXED USE DEVELOPMENT

49 QUEEN ST SOUTH & #5 ELLEN ST,
STREETSVILLE, MISSISSAUGA

PROJECT BRIEF
NEW CONSTRUCTION: MIXED USED, 2 STOREYS WITH PARTIAL BASEMENT.
BUILDING 1: OFFICES & COMMERCIAL/RETAILS, BUILDING 2: RESIDENT. APT OVER COMMERCIAL/RETAIL.
ADJACENT TO EXISTING HERITAGE DESIGNATED BUILDING (ORANGE HALL): # 47 QUEEN ST S.
The property forms part of the Streetsville Village Core Cultural Landscape and the Mississauga Road
Scenic Route Cultural Landscape

SITE STATISTICS (BOTH BUILDINGS)		1 & 2	ZONE C4	BUILDING 1 (49 QUEEN ST) MIXED USE	BUILDING 2 (5 ELLEN ST) COMMERCIAL - APT
LOT TYPE	IRREGULAR	1076.5 m ²	1076.5 m ²	1076.5 m ²	1076.5 m ²
LOT AREA	1076.5 m ²	1076.5 m ²	1076.5 m ²	1076.5 m ²	1076.5 m ²
LOT FRONTAGE	QUEEN ST 13.58 m	13.58 m	13.58 m	13.58 m	13.58 m
GROUND FLOOR BUILDING AREA	473 m ²	473 m ²	473 m ²	473 m ²	473 m ²
SECOND FLOOR BUILDING AREA	750 m ²	750 m ²	750 m ²	750 m ²	750 m ²
TOTAL BUILDING AREA	1223 m ²	1223 m ²	1223 m ²	1223 m ²	1223 m ²
TOTAL BASEMENT AREA	215 m ²	215 m ²	215 m ²	215 m ²	215 m ²
TOTAL COVERED AREA	1438 m ²	1438 m ²	1438 m ²	1438 m ²	1438 m ²
LANDSCAPED AREA	160 m ²	160 m ²	160 m ²	160 m ²	160 m ²
BUILDING HEIGHT	BY-LAW 3 STOREYS	3 STOREYS	3 STOREYS	3 STOREYS	3 STOREYS

CAR PARKING CALCULATION

REQUIRED CAR PARKING, ZONING BY-LAW (C4 ZONE):
RETAIL STORE - ZONE C4: 4 CARS / 100 m² (3.1.2.2 - 41.2)
OFFICES: 3.2 CARS / 100 m² (3.1.2.2 - 31.1)
RESIDENTIAL UNIT ABOVE COMMERCIAL: 1.25 CART (3.1.2.1 - 8)
HANDI-CAPED REQUIRE 2 CARS (3.1.3.1, FROM 26 to 50 NEED 2)
LOADING SPACE IS NOT REQUIRED (MAX 250 m²) (3.1.4.2 - 3.1.4.3)
PART 3: 3.1.2.3 - MIXED USE DEVELOPMENT SHARED PARKING FORMULA
PART 3: 3.1.1.4 - NUMERIC FRACTION (LESS THAN 0.5)
CAR SPOT SIZE: 5.26m x 2.2m, WITH 6.5m DRIVEWAY WIDTH (VARIANCE)

CAR PARKING REQUIREMENT DEPEND ON GFA NON RESIDENTIAL AREA
OFFICE GFA NON RESIDENT. = 250 m² (250 / 100 = 2.5 (M4) = 10 CARS
OFFICE GFA NON RESIDENT. = 635 m² (635 / 100 = 6.35 (X3.2) = 20.3 CARS
1 BEDROOM RESIDENTIAL APT = 1.25 CARS
TOTAL CAR PARKING REQUIRED FOR BUILDING 1+2 = 31.5 CARS
FOLLOW: MIXED USED SHARED PARKING FORMULA = 29.4 CARS
CAR PARKING PROVIDED = 29 CARS (NUMBER IN BRACKET)

COMMITTEE OF ADJUSTMENT HAD APPROVED THE VARIANCE OF:
8 TANDEM PARKING SPOTS TO BE ASSIGNED (RESERVED) FOR USERS and TO BE INCLUDED IN MIXED USED SHARED PARKING FORMULA
GFA NON RESID. FOR RETAILS INCLUDE RETAILS AREA ONLY (NO PUBLIC CORRIDORS), ALL RETAILS HAVE DIRECT ACCESS FROM STREETS.
GFA NON RESID. FOR OFFICES INCLUDE OFFICES AREA AND (GROUND & SECOND FLOORS CORRIDORS).

LEGEND

- EXISTING BUILDING TO BE DEMOLISHED & REMOVED
- PROPOSED BUILDING
- ASPHALT PAVING
- ENTRANCE / EXIT TO BUILDINGS
- SOD
- CONC. PAVERS - WALKWAY
- PERMEABLE PAVERS - PARKING
- BARRIER FREE CAR PARKING
- BARRIER FREE PARK SIGN (DETAILS AT LANDSCAPE DWG)
- PAINTED LINES - YELLOW NO PARKING ALLOWED
- EXISTING TREES TO REMOVE REF. TO ARBORIST REPORT
- EXISTING FIRE HYDRANT
- PROPOSED YARD LIGHT REF. TO PHOTOMETRIC PLAN
- PROPOSED CATCH BASIN / MANHOLE
- PROPOSED ELEVATION (REFER TO SITE GRADING DWGS)
- EXISTING ELEVATION (REFER TO LAND SURVEY)
- FIRE DEPARTMENT SIGNAGE FIRE ROUTE, NO PARKING
- FIRE DEPARTMENT (SIAMSE) PUMPER CONNECTION

Planning and Building Department Standard Notes:

i) "I hereby certify that this drawing conforms in all respects to the site development Plans as approved by the City of Mississauga under file number: SP 11/129 W11"

ii) "The City of Mississauga requires that all working drawings submitted to the Building Division as part of an application for the issuance of a building permit shall be certified by the architect or engineer as being in conformity with the site development plan as approved by the City of Mississauga."

iii) "All exterior lighting will be directed onto the site and will not infringe upon the adjacent properties."

iv) "All roof top mechanical units shall be screened from view by the applicant."

v) "Parking spaces reserved for people with disabilities must be identified by a sign, installed at the applicant's expense, in accordance with the design specifications of Schedules 2 & 3 of By-law 001-2009."

vi) "The applicant will be responsible for ensuring that all plans conform to Transport Canada's restrictions."

vii) "Grades will be met within a 33% maximum slope at the property lines and within the site."

viii) "All damaged areas are to be reinstated with top soil and sod prior to the release of securities."

ix) "Signage shown on the site development plans is for information purposes only. All signs will be subject to the provisions of Sign By-law 0054-2002, as amended, and a separate sign application will be required through the Building Division."

Landscaping Dev. & Design Department Standard Notes:

"The Applicant is responsible for ensuring that tree protection hoarding is maintained throughout all phases of demolition and construction in the location and condition as approved by the Planning and Building Department. No materials (building materials, soil, etc.) may be stockpiled within the area of hoarding. Failure to maintain the hoarding as originally approved or the storage of materials within the hoarding will be cause for the Tree Preservation Letter of Credit to be held for two (2) years following completion of all site works."

Development and Engineering:

Note: "The Applicant will be required to contact all Utilities Companies to obtain all required locates Prior to the installation of Hoarding within the Municipality Right of Way."

Note: "All Asphalt and base materials of the existing Driveway being removed are to be reinstated with topsoil and sod."

Note: "If the final course of Asphalt paving is delayed, install a temporary lift of Asphalt at ramps or curb cuts to provide barrier-free access."

Note: "The structure design of any retaining wall over 0.60m in height or any retaining wall located on a property line, is to be shown on the Site Grading Plan for this project, and is to be approved by the Consulting Engineer for the project."

Note: "Prior to construction taking place for any hoarding within the Municipality Right-of-Way, P.U.C.C. approval and road occupancy permit shall have been issued."

Note: "Prior to any construction taking place, structurally certified overhead hoarding/public way protection in accordance with O. Reg. 213/91, Section 64 located within the right-of-way and/or public lands, hoarding adjacent to existing properties to protect from construction activity, and all required hoarding in accordance with the Ontario Occupational Health and Safety Act regulation for construction projects must be erected, inspected and certified by professional Engineer and maintained throughout all phases of construction."

The Applicant will be responsible for the cost of any utility relocations necessitated by the Site Plan.

Any fencing adjacent to Municipal Lands is to be located 15 cm (6.0 in) inside the Property Line.

Transportation and work:

i) "Drive ways on the Municipal Right Of Way shall be paved by the applicant."

ii) At the Entrances of the Site, the Municipal Sidewalk and Curb shall be continuous through the Driveway.

iii) All surface drainage shall be Self-Contained and collected and discharged at Location to be approved prior to Issuance of Building Permit.

iv) The Driveway Grade will be compatible with future Sidewalk and Curb depression will be provided for each Entrance.

v) All proposed curbing at the Entrances to the Site to Stop at the Property line or at the Municipal Sidewalk.

vi) All excesses excavated material will be removed from the site.

Fire Department:

i) "The Fire Route shall be designed to support a load of not less than 11,363 KG/AXLE and have a change in gradient of not more than (1) in (12.5) over a minimum distance of 15m. Fire Route shall be designated as per By-Law 1036-81 as amended."

ii) Grades will be met within a 33% Max slope at the property lines and within the Site.

URBAN DESIGN:

i) "The Structure Design of any Retaining Wall over 0.60 m in height or any Retaining Wall located on Property Line, is to be shown on the Site Grading Plan of this project and to be approved by the Consultant Engineer of the Project"

TRUE NORTH PROJECT NORTH

KEY MAP

STREETSVILLE

PROJECT BRIEF

2 STOREYS MIXED USE DEVELOPMENT, WITH 'T' SHAPE LOT, PACING 2 STREETS THROUGH LOT, BESIDE A HERITAGE DESIGNATED "ORANGE HALL", SHARING RIGHT OF WAY & IN STREETSVILLE VILLAGE-MAIN STREET CHARACTER AREA

GENERAL NOTES

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IT IS CONTRARY TO THE ONTARIO BUILDING CODE ACT TO COMMENCE CONSTRUCTION PRIOR TO RECEIPT OF A VALID BUILDING PERMIT.

NOTES:

"THIS PROPOSED DEVELOPMENT IS CONSIDERED ONE BUILDING / ONE BUILDING PERMIT APPLICATION, WITH 2 ADDRESSES. THIS MATTER WAS DISCUSSED EARLIER (MAR 2013) WITH BUILDING DEPT., FIRE PREVENTION & FIRE SAFETY DEPT AND PROPERTY & GEOMATICS DEPT."

FOR MATTER OF COORDINATION, WE WILL CALL:

PORTION # 1: (49 QUEEN ST S) IS THE MAIN BLDG. MIXED USE (COMMERCIAL-OFFICES)

PORTION # 2: (5 ELLEN ST) IS THE SMALL BUILDING. RESIDENTIAL APT. AT THE 2ND FL. & COMMERCIAL AT GR. FL.

COMMITTEE OF ADJUSTMENT DECISION - DEC 11 2014. APPLICATION FILE # A-44714-4.

(8) TANDEM PARKING SPACES BE SIGNED AND USED FOR THE ON-SITE EMPLOYEES ONLY.

ABBREVIATION	
EXIST. EXISTING	TC: TOP OF CURB
CL: CENTRE LINE	SP: SITE PLAN
M: METER	ST: STEEL
MM: MILLIMETER	SIM: SIMILAR
MIN: MINIMUM	TYP: TYPICAL
	US: UNDER SIDE

RAFID KUSTOU RALLY Architects

ADDRESS: 3-1 CHESTER BLVD. TORONTO, M9W 2B5, CANADA
TEL: 416 494 9754 ralfyarch@gmail.com

Signature: *RAFID KUSTOU*
Date: NOV 20 2015
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CHARTERED ASSOCIATION OF ARCHITECTS OF ONTARIO

RAFID KUSTOU LICENCE 7195

PROJECT TITLE: **RTD CENTRE**

2 STOREYS NEW BUILDING MIXED USE DEVELOPMENT

OWNER: 2112836 ONTARIO INC

ADDRESS: 49 QUEEN STREET SOUTH & 5 ELLEN ST, STREETSVILLE, MISSISSAUGA, ON, L6M 1K5

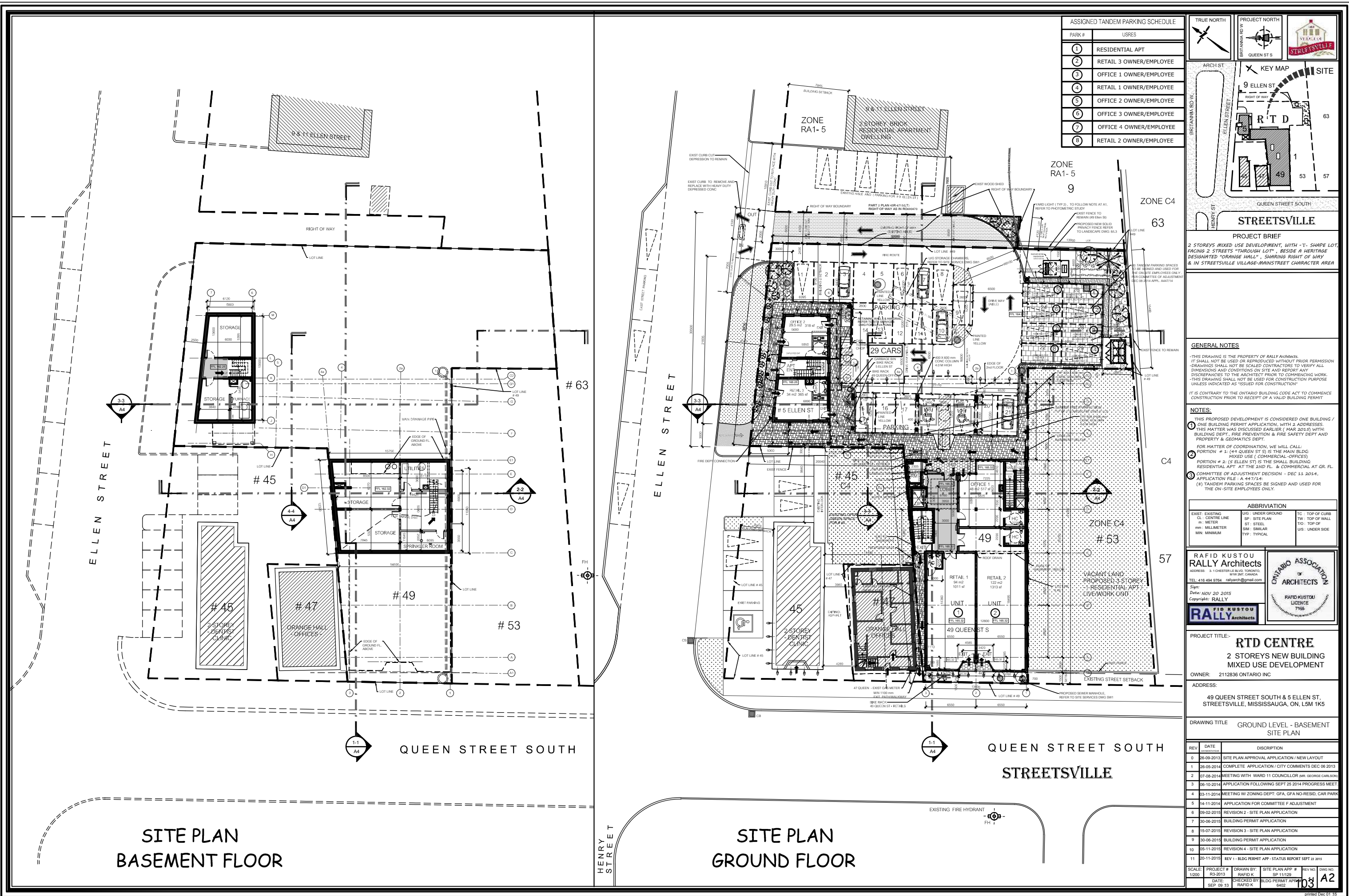
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1	26-05-2014	COMPLETE APPLICATION / CITY COMMENTS DEC 06 2013	
2	07-08-2014	MEETING WITH WARD 11 COUNCILLOR MR. GEORGE CARLSON	
3	06-10-2014	APPLICATION FOLLOWING SEPT 25 2014 PROGRESS MEET	
4	03-11-2014	MEETING W/ ZONING DEPT: GFA, GFA NO-RESID, CAR PARK	
5	14-11-2014	APPLICATION FOR COMMITTEE F ADJUSTMENT	
6	09-02-2015	REVISION 2 - SITE PLAN APPLICATION	
7	30-06-2015	BUILDING PERMIT APPLICATION	
8	15-07-2015	REVISION 3 - SITE PLAN APPLICATION	
9	30-06-2015	BUILDING PERMIT APPLICATION	
10	05-11-2015	REVISION 4 - SITE PLAN APPLICATION	
11	20-11-2015	REV 1 - BLDG PERMIT APP - STATUS REPORT SEPT 23 2015	

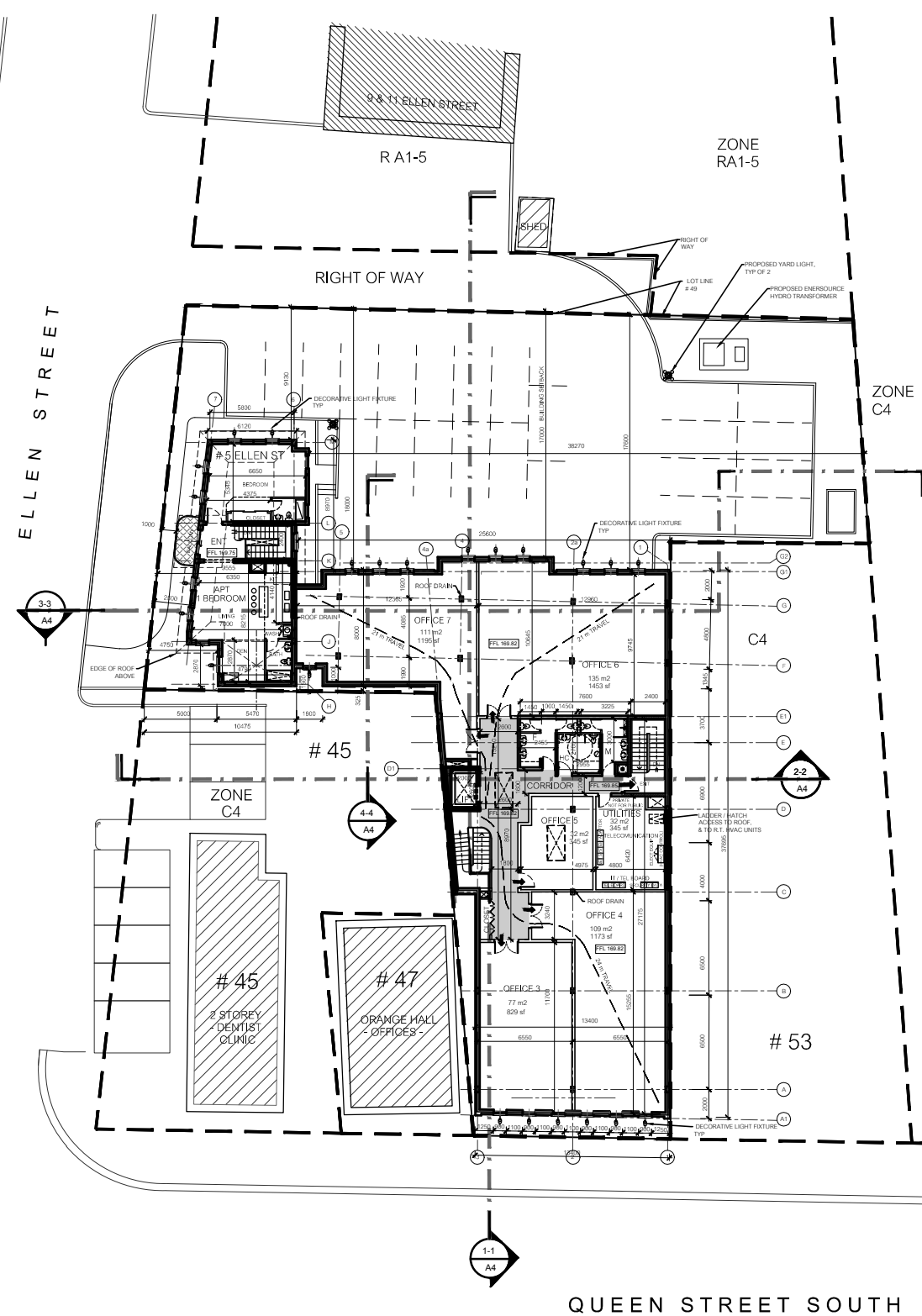
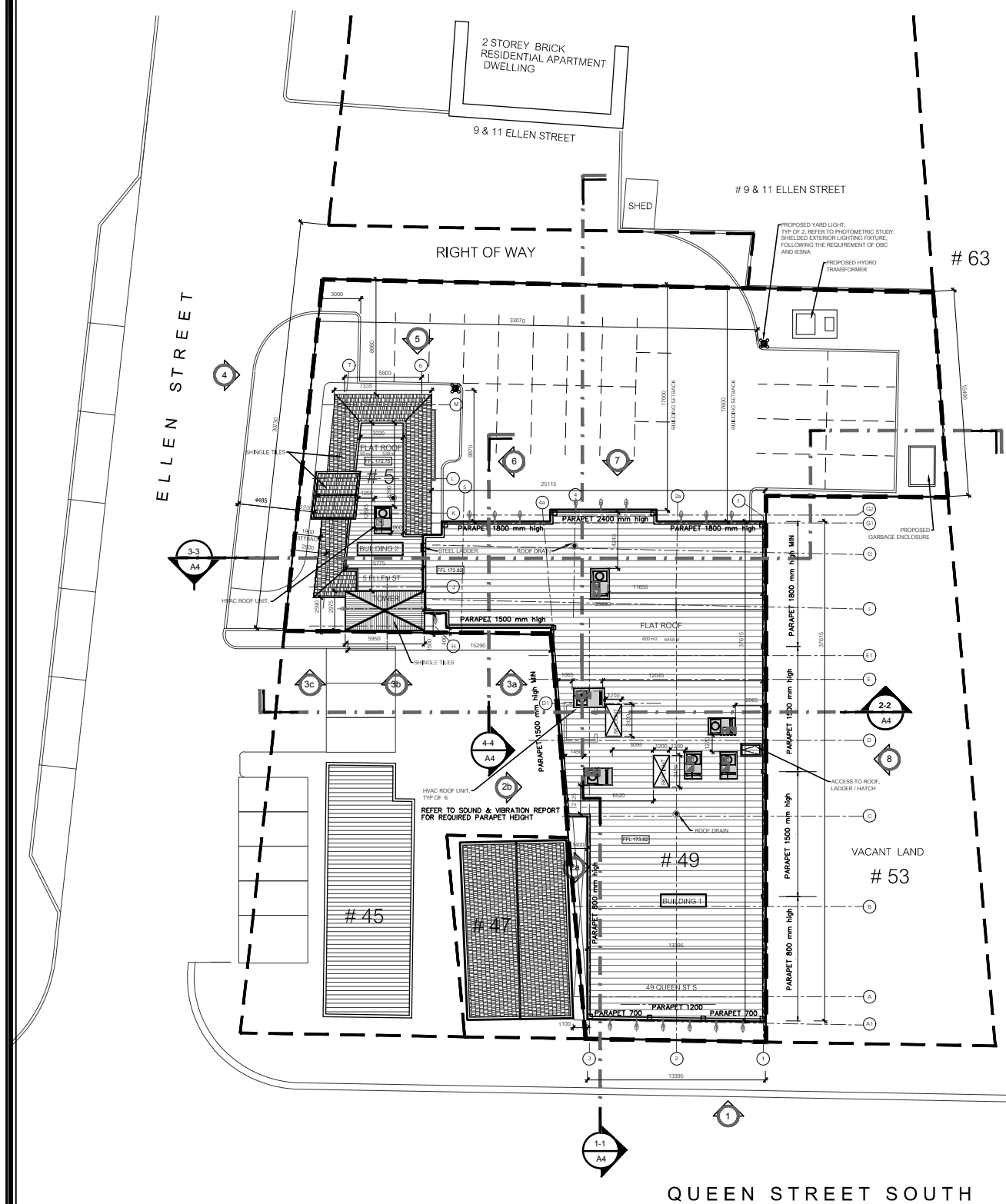
SCALE: 1/200 PROJECT # R3-2015 DRAWN BY: RAFID K. SITE PLAN APP # SP 11/129 REV NO. 11 DWG NO. 6402


DATE: SEP 09 13 CHECKED BY: BLDG PERMIT APP # 6402

OWNER APPROVAL: M. MAROUN DIAB (2112836 ON INC), TEL: 416 565 1722, SIGN


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TRUE NORTH



PROJECT NORTH




STREETSVILLE

BRITANNIA RD. W.

ARCH ST

ELLEN STREET

HENRY ST



KEY MAP

9 ELLEN ST

RIGHT OF WAY

R T D

63

45 47 49 53 57

QUEEN STREET SOUTH

SITE

STREETSVILLE

PROJECT BRIEF

2 STOREYS MIXED USE DEVELOPMENT, WITH "T" SHAPE LOT, FACING 2 STREETS "THROUGH LOT", BESIDE A HERITAGE DESIGNATED "ORANGE HALL", SHARING RIGHT OF WAY & IN STREETSVILLE VILLAGE-MAIN STREET CHARACTER AREA

GENERAL NOTES

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-DRAWINGS SHALL NOT BE SCALED CONTRACTORS TO VERIFY ALL DIMENSIONS AND CONDITIONS ON SITE AND REPORT ANY DISCREPANCIES TO THE ARCHITECT PRIOR TO COMMENCING WORK.

-THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION PURPOSE UNLESS INDICATED AS "ISSUED FOR CONSTRUCTION"

IT IS CONTRARY TO THE ONTARIO BUILDING CODE ACT TO COMMENCE CONSTRUCTION PRIOR TO RECEIPT OF A VALID BUILDING PERMIT

NOTES:

THIS PROPOSED DEVELOPMENT IS CONSIDERED ONE BUILDING / ONE BUILDING PERMIT APPLICATION, WITH 2 ADDRESSES:

① PORTION # 1: (45 QUEEN ST S) IS THE MAIN BLDG. MIXED USE (COMMERCIAL-OFFICES)

② PORTION # 2: (5 ELLEN ST) IS THE SMALL BUILDING. RESIDENTIAL APT. AT THE 2ND FL. & COMMERCIAL AT GR. FL.

③ COMMITTEE OF ADJUSTMENT DECISION - DEC 31 2014, APPLICATION FILE # A 447/14:

(8) TANDEN PARKING SPACES BE SIGNED AND USED FOR THE ON-SITE EMPLOYEES ONLY.

ABBREVIATION			
EXIST: EXISTING	UG: UNDER GROUND	TC: TOP OF CURB	TW: TOP OF WALL
CL: CENTRE LINE	SP: SITE PLAN	TO: TOP OF	TO: TOP OF
M: METER	ST: STEEL	US: SIMILAR	US: UNDER SIDE
MM: MILLIMETER	TYP: TYPICAL		

RAFID KUSTOU
RALLY Architects


ADDRESS: 3 DUNDAS STREET EAST, SUITE 200
M5B 2B7 CANADA

TEL: 416 454 9754 rallyarch@gmail.com

Sign:
Date: NOV 20 2015

CHECKED: RALLY

FID KUSTOU
RALLY Architects



ONARIO ASSOCIATION
OF
ARCHITECTS

RAFID KUSTOU
LICENSEE
7155

PROJECT TITLE:-

RTD CENTRE

**2 STOREYS NEW BUILDING
MIXED USE DEVELOPMENT**

OWNER: 2112836 ONTARIO INC

ADDRESS:

49 QUEEN STREET SOUTH & 5 ELLEN ST,
STREETSVILLE, MISSISSAUGA, ON, L5M 1K5

DRAWING TITLE

2ND FLOOR - ROOF PLAN SITE PLAN

REV	DATE	DISCRPTION
0	26-09-2013	SITE PLAN APPROVAL APPLICATION / NEW LAYOUT
1	26-05-2014	COMPLETE APPLICATION / CITY COMMENTS DEC 06 2013
2	07-08-2014	MEETING WITH WARD 11 COUNCILLOR (MR. GEORGE CARLSON)
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4	03-11-2014	MEETING W/ ZONING DEPT. GFA, N/A NO-RESID. CAR PARK
5	14-11-2014	APPLICATION FOR COMMITTEE F ADJUSTMENT
6	29-02-2015	REVISION 2: SITE PLAN APPLICATION
7	30-06-2015	BUILDING PERMIT APPLICATION
8	15-07-2015	REVISION 3: SITE PLAN APPLICATION
9	30-06-2015	BUILDING PERMIT APPLICATION
10	05-11-2015	REVISION 4: SITE PLAN APPLICATION
11	20-11-2015	REV 1 - BLDG PERMIT APP - STATUS REPORT SEPT 23 2015

SCALE: PROJECT R3-2013
1/200

DATE: SEP 09 13

DRAWN BY: RAFID K

CHECKED BY: BLDG PERMIT APP 6402

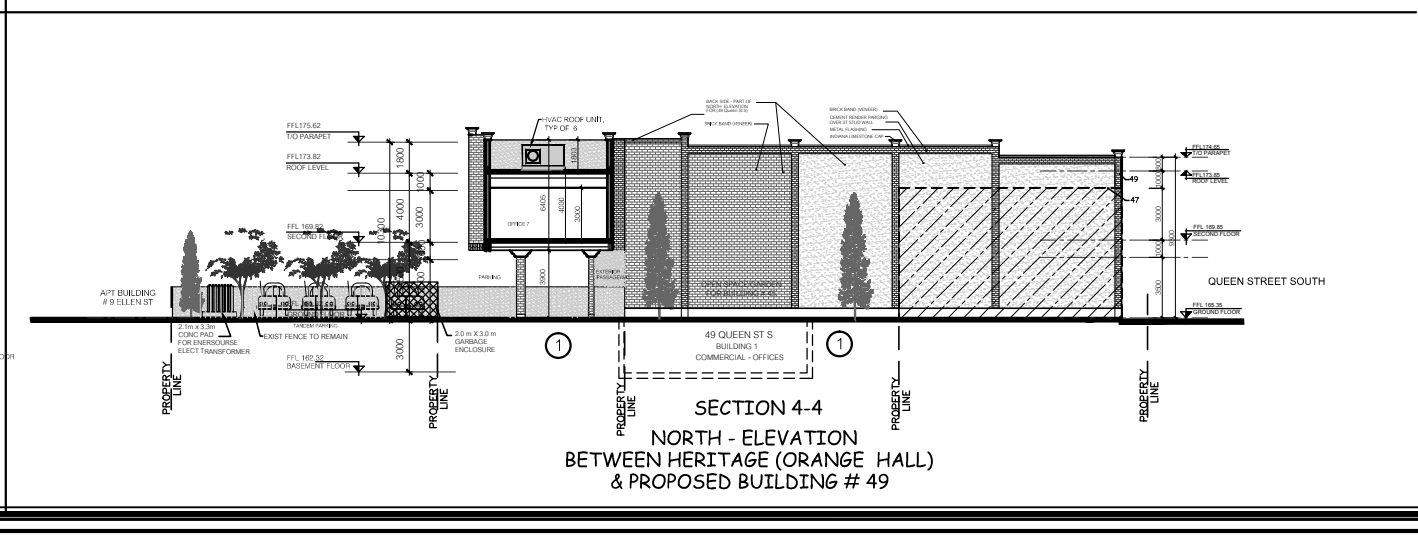
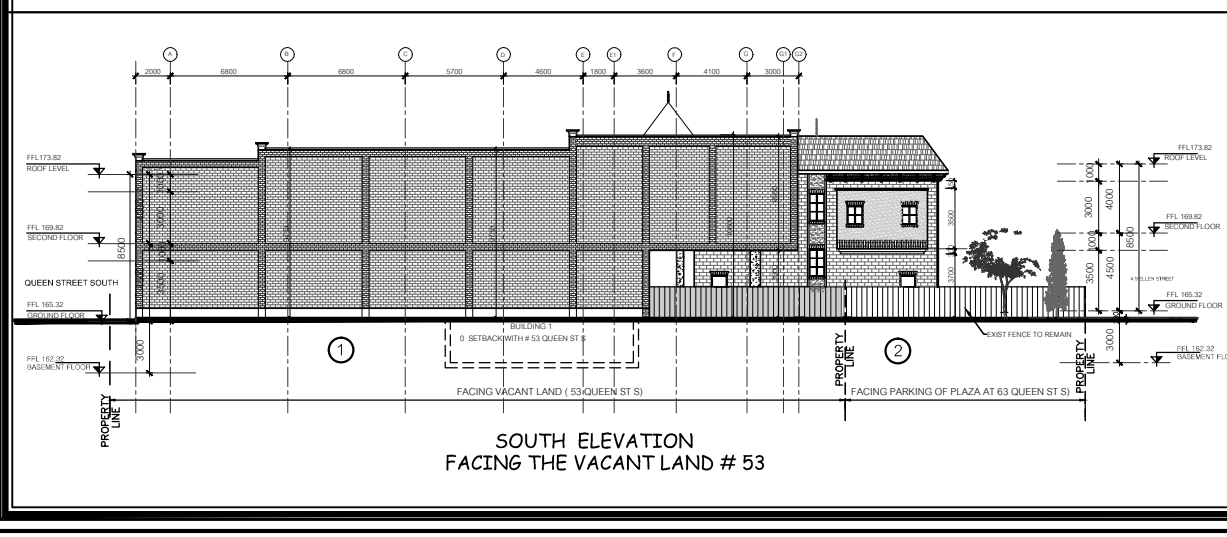
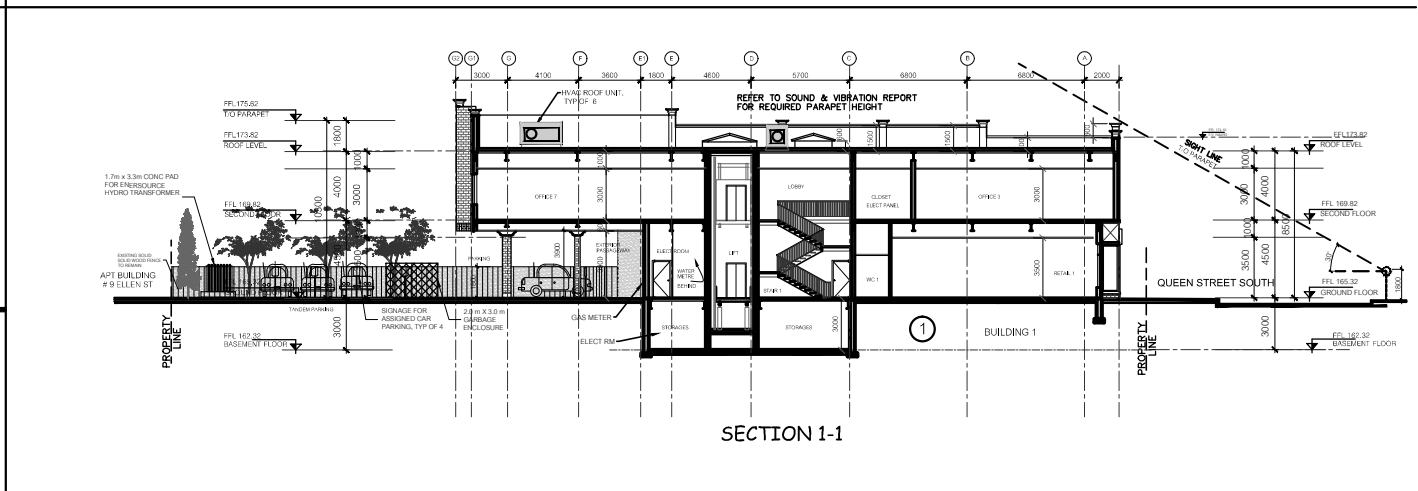
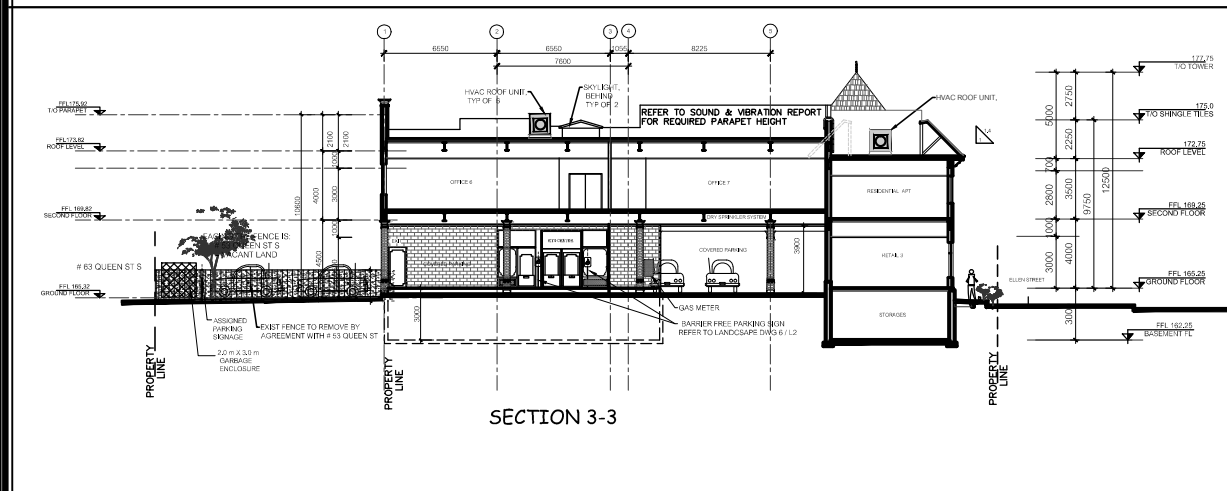
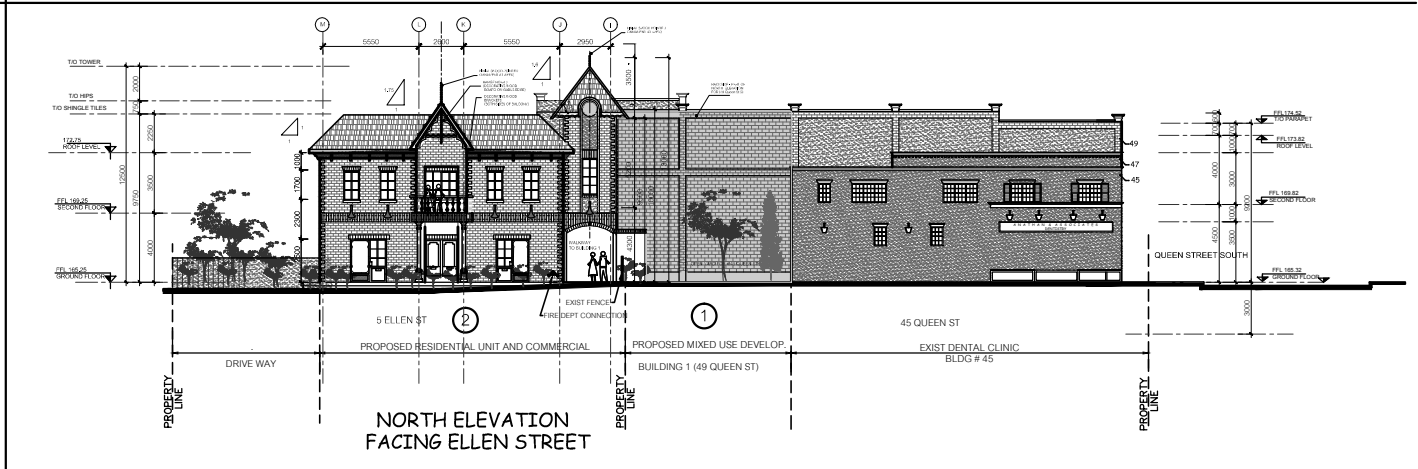
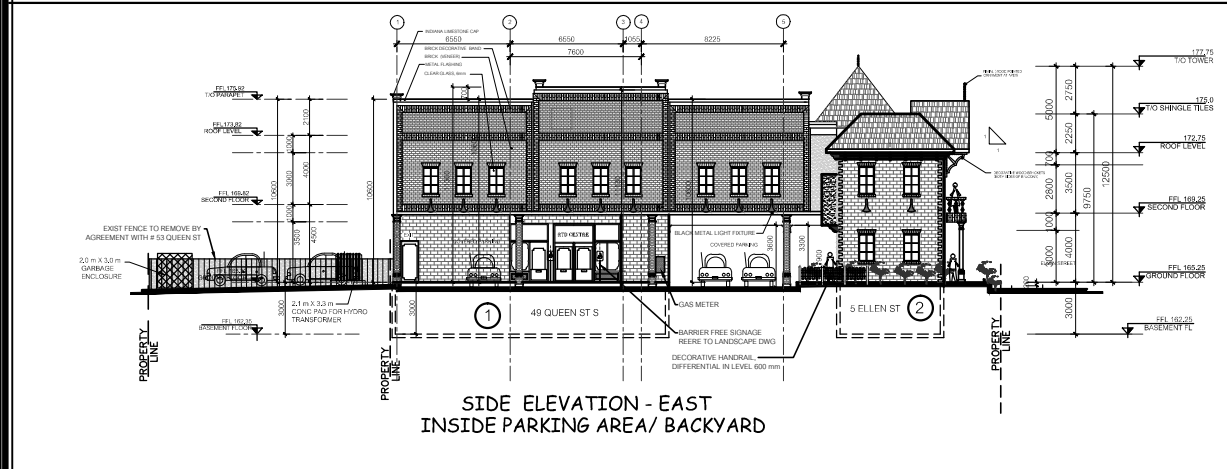
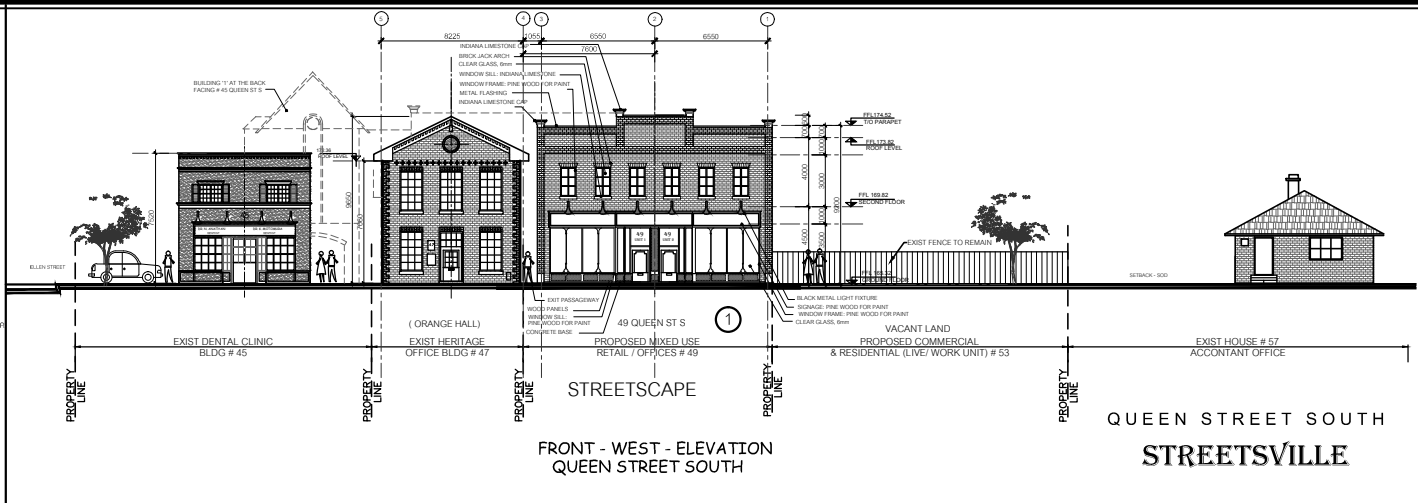
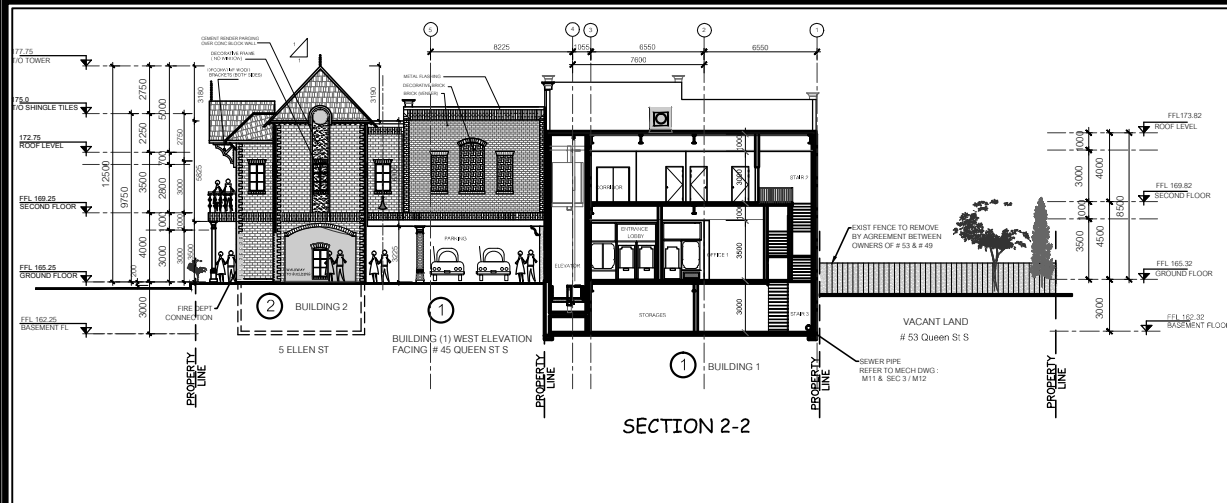
SITE PLAN APP # SP 11/125

REV NO: 1

DWG NO: A3

Rev 1

created: SEP 09 13



STREETSVILLE

PROJECT BRIEF

2 STOREYS MIXED USE DEVELOPMENT, WITH 'T' SHAPE LOT, FACING 2 STREETS THROUGH LOT, BESIDE A HERITAGE DESIGNATED 'ORANGE HALL', SHARING RIGHT OF WAY & IN STREETSVILLE VILLAGE-MAINSTREET CHARACTER AREA

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IT IS CONTRARY TO THE ONTARIO BUILDING CODE ACT TO COMMENCE CONSTRUCTION PRIOR TO RECEIPT OF A VALID BUILDING PERMIT.

NOTES:

THIS PROPOSED DEVELOPMENT IS CONSIDERED ONE BUILDING / ONE BUILDING PERMIT APPLICATION, WITH 2 ADDRESSES. THIS MATTER WAS DISCUSSED EARLIER (MAR 2015) WITH: BUILDING DEPT., FIRE PREVENTION & FIRE SAFETY DEPT AND PROPERTY & GEOMATICS DEPT.

FOR MATTER OF COORDINATION, WE WILL CALL: MIXED USE (COMMERCIAL-OFFICES) PORTION # 2: (5 ELLEN ST) IS THE SMALL BUILDING; RESIDENTIAL APT. AT THE 2ND FL. & COMMERCIAL AT GR. FL. COMMITTEE OF ADJUSTMENT DECISION - DEC 11 2014, APPLICATION FILE # A-44734-1.

(8) TANDEM PARKING SPACES BE SIGNED AND USED FOR THE ON-SITE EMPLOYEES ONLY.

ABBREVIATION

EXIST: EXISTING	UG: UNDER GROUND	TC: TOP OF CURB
CL: CENTRE LINE	SP: SITE PLAN	TW: TOP OF WALL
m: METER	ST: STEEL	TOT: TOP OF
mm: MILLIMETER	SIM: SIMILAR	US: UNDER SIDE
MIN: MINIMUM	TYP: TYPICAL	

RAFID KUSTOU RALLY Architects

ADDRESS: 3-1 CHESTER BLVD, TORONTO, M9W 5B7, CANADA
TEL: 416 494 9754 | rallyarch@gmail.com

Sign: NOV 20 2015
Copyright: RALLY

ONTARIO ASSOCIATION OF ARCHITECTS
RAFID KUSTOU LICENCE 7195

PROJECT TITLE:
RTD CENTRE

2 STOREYS NEW BUILDING MIXED USE DEVELOPMENT

OWNER: 2112836 ONTARIO INC

ADDRESS:
49 QUEEN STREET SOUTH & 5 ELLEN ST, STREETSVILLE, MISSISSAUGA, ON, L5M 1K5

DRAWING TITLE
ELEVATIONS - SECTIONS

REV	DATE	DESCRIPTION
0	26-09-2013	SITE PLAN APPROVAL APPLICATION / NEW LAYOUT
1	26-05-2014	COMPLETE APPLICATION / CITY COMMENTS DEC 06 2013
2	07-08-2014	MEETING WITH WARD 11 COUNCILLOR MR. GEORGE CARLSON
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9	30-06-2015	BUILDING PERMIT APPLICATION
10	05-11-2015	REVISION 4 - SITE PLAN APPLICATION
11	20-11-2015	REV 1 - BLDG PERMIT APP - STATUS REPORT SEPT 23 2015

SCALE: PROJECT # R3-2013, DRAWN BY: RAFID K., SITE PLAN APP # SP-11129, DATE: SEP 09 13, CHECKED BY: BLDG PERMIT APP # 6402, DWG NO. 105 A4

printed Dec 01 15

RTD Centre: 49 Queen St S and 5 Ellen St
Streetsville, Mississauga, Ontario, L5M 1K5

Addendum to Heritage Impact Assessment (Feb 2014)
Evaluation for the building: 5 Ellen St

Prepaid by: RALLY Architects / Rafid Kustou

General Information had changed:

1st page: to the Reader:

Property Owner:

Project Architect and Project Coordinator:

RALLY Architects

Contact: Rafid Kustou – Principal, B.SC.ENG.ARCH.,OAA

Tel: 416 494 9764, email:rallyarch@gmail.com

Construction Manager:

KIB Building Restoration INC-

Contact: Mr. Anthony Monardo

Tel: 905 614 1400, Fax 905 614 0003

Applications at the City of Mississauga:

- *Site Plan Application: SP 11 / 129*
- *Building Permit Application: BP 3 NEW 15 6402*

This Addendum to the HIS report is prepaid by:

RALLY Architects, Contact: Rafid Kustou

Tel: 416 494 9764 , email:rallyarch@gmail.com

Notes - Changes:

- Orion Pacific Engineering is No longer the Project Manager: RALLY Architect/ Rafid Kustou is the Project Coordinator for both Site Plan Approval & Building Permit Application.
- KIB is acting as the Construction Manager and they had started the coordination for: Demolish Exist. buildings, Road Occupancy Permit and the Schedule of Construction work.
- The 2nd Floor of building (2) at Ellen St is a Residential Apartment for the Owner and will not be a Live/Work unit any more.

This Report/Addendum is following the same numbering and sequences for the Items had been addressed in the original Report, changes to each item as below:

2.0: Present Site Description, Survey of Property (Page 3):

The 2 lands of the Site: 49 Queen St S and 5 Ellen St had been merged to one Land, New revised Survey dated Oct 2014 has indicated that (enclosed), and the Report from Land Registry office (Bin Abstract No is included in this Addendum).

- The Project now has one Property, with 2 attached Buildings (one structure).
- One building Permit application with 2 Addresses: 49 Queen St S and 5 Ellen St.

4.3: Adjacent Heritage Building (Orange Hall) Designated Building.

Additional Information

- Orange Hall is for the same Owner of 49 Queen St S (2112836 ON. Inc.)
- The revised Site Plan shows an Exterior Passageway from 49 Queen St S to Queen St S.. This walkway is sharing between both properties.

5.0: Applicable Policies and Documents Reviewed:

- 5.3: Historic Streetsville Design Guidelines (*City Of Mississauga / July 2011*).
- 5.4: Chapter 14-Community Nodes (*Mississauga Official Plan - Part 3 / Sept 26 2013*)
- Culture Landscape Inventory, (*City Of Mississauga, Community Services / Jan 2005*)

6.0: Revised Drawings:

- 15: Site Plan – Site Statistics
- 16: Ground Floor & Basement Floor (Site Plan)
- 17: Second Floor & Roof Plan (Site Plan)
- 18: Elevations & Sections (Site Plan)
- 22: North Elevation -- Ellen Street (Streetscape) .
- 23: South Elevation -- Facing 53 Queen St S..
- 24: East Elevation --Right of Way
- 24 A: New Colored Elevations, Wall Sections and Details.

7.0: Comments on the Proposed Development – [Impact of 5 Ellen St](#)

Progress:

Originally, according Item (7 - Comments) at the original Heritage Impact Assessment: The second building (2) was totally separated and located at the dead-end (south-east) corner, refer to previous design drawings at pages 15 to 19 at the previous Report.

Site Plan Layout had changed in Sept 2014 because of the Planners of the City preferred to hide the Car Parking from Ellen St's pedestrians and to keep the Residential Characteristic of Ellen St. This decision affects and reduced the Total No of Car Parking spots, and this coordination ended with:

Committee of Adjustment decision on Dec 11 2014 had approved the current layout with the small building in-front facing Ellen St (with 2.5 m front setback) and 8 Tandem Car Parking spots at the back (in Dead-end Corner). Refer to Committee of Adjustment decision - enclosed.

7.1: General Description of the Proposed Development /Project Brief:

Existing Difficulties – the Project's Conditions are: 2 Storeys Mixed Use Development, on a T shape property, facing 2 Streets "Through Lot" beside a Heritage Designated Building (Orange Hall), in Streetsville Village and Sharing Right of Way as the only main Vehicle Access from Ellen St.

According Streetsville Heritage Guidelines, Heritage Characters are:

- 49 Queen St S is part of (Main Street Character) Area, and 5 Ellen St is part of (Areas of Transaction).

This proposed Development is considered as one Building / one Building Permit Application with two Addresses. For matter of Coordination, we call:

Building (1): the larger portion of the building at 49 Queen St S: Main building adjacent to Orange Hall. Mixed use development (Commercial – Offices): Ground Floor has 2 Retails with direct access from Queen St S, one small Office facing the Car Parking, and 5 Offices at the Second Floor.

Building (2): the small building at 5 Ellen St.: with the Residential Apt at the 2nd floor (for the Owner) and one small office and one Retail at the ground Floor.

Vehicle Access is not preferred from Queen St S per Streetsville Heritage Guidelines. The only access is from Ellen Street, at the same location of the existing Right of Way. Proposed building is Sprinklered, then Fire Route is not required.

7.2: Character and Design of building 2 at 5 Ellen St.

General Description of the proposed development at 5 Ellen St:

According Page 6: 5 Ellen St is not remarkable, and we got the Demolish Permit.

- The current elevations (Facades) were discussed and are following all the comments of Planning Dept and Urban Designer during the last 3 Site Plan Status Reports (Refer to Marked-up PDFs at the end of Report).

The proposed Mass & Facade for small building 2 on Ellen St has a harmony with Streetsville neighborhood per the Items below:

Building Setback

The Site Plan Layout was approved through the Process of Committee of Adjustment (Dec 11 2014). There are many restrictions for this Unique Property. Car Parking spots is the major Design's issue to maximize building area. The current Layout had satisfied all required conditions of the City of Mississauga and Urban design with 29 Car Parking , the Small building 5 Ellen is facing the street and hiding the Car Parking.

The Partial Building of the project (5 Ellen St) has 2.50 m setback from Ellen St : 1.00 m is Landscaped area and 1.5 m walkway around the frontage.

The adjacent Dentist Office building (# 45 Ellen St) has 4.20 m setback from Ellen with Front car parking (Municipal Right of Way is added to parking Spots).

9 Ellen St existing Residential Apt building has around 8.00 m front setback (as a sod / garden) and car parking on side of Right of Way sharing with this project.

Ellen St has no Pedestrian walkway on the south side curb facing our project.

Community Node

- Following on Mississauga Official Plan - Part 3 , 14-1 Community Node:
 - A Tower as a featured Node is created on Ellen St to be as: **Landmark Node** to Streetsville village North Sector (area in Transaction), and gives an easy direction for public to the Walkway and to the main Entrance to the Building 49 Queen St S.
- 3 Dimensional colored presentation/Rendering are enclosed.

Building Height and Massing (Streetscape):

C4 Zoning By-Law allows a maximum height of 12.5 m for flat roof and 16.0 m for Sloped Roof.

5 Ellen St has 2 Storeys building with Central Entrance as an access to one bedroom Apt at the Second Floor, and 1 Retails & 1 Office facing Ellen St.

The proposed decorative Tower between 5 Ellen St and Existing 45 Queen St. gives a good Transition and masses changes between 5 Ellen St and 49 Queen St S.

Building 2 (5 Ellen St) Height is 9.750 m and the Tower is 12.5 m high. Building (1) 49 Queen St S is around 11.0 m high.

45 Queen St S (Dental Office) at the West is a 2 storeys – 9.00 m building height similar to the existing Residential Apt at 9 Ellen St., beside the Right of Way.

Style and Material

- Proposed elevations have Residential Style with Shingle Tiles. The Façade Design is Similar to the Existing Building (52 Queen St) in front of 49 Queen St S. Many Heritage Features are added:
 - Muntin Bars (divider Bars hold panes of glass in windows and doors).
 - Wood Finial (pointed ornament at Apex).
 - Barge board (decorative board on a gable edge)
 - Facia Board (covering the joint between the top of wall and projecting Eaves).
 - Trim Frame and Fenestration (edge of windows and colored brick corners/trims).
 - Typical Streetsville decorative Heritage Light Fixtures are proposed around all facades.

Rhythm and Proportion

The mass has residential Style, similar to the scale of Residential Houses in Streetsville village and Ellen Street.

- Proposed Widows had the same size and proportions of the Facade at 49 Queen St S and 47 Queen St S (Orange Hall).

Fenestration and Entrance

- Windows Rhythm, Trim and Size are similar to the Façade of 49 Queen St S. Typical windows are proposed around building 1+2 facades, (20 typical windows).
- All Facades has a Brick Facing around, Similar to proposed Building 1 / 49 Queen St S. including Fenestration/Trims with contrast in colors.

Color

Contrast in colors for Brick and Trims for Facades/corners of the Building 5 Ellen St. Different colors for the shingle Tiles for the Tower from 5 Ellen St Roof. Refer to colored elevation at the end of the Report.

Rear and Side Elevation

Rear and side Facades of 5 Ellen St have the same quality and features as the Front Elevation. Fenestration, Brick Facades and decorative Trims and Muntin bars for Windows and Doors.

Landscaping

Original Report did not include Landscape drawings Review. Landscape Plan is enclosed. There are few Landscape Elements around 5 Ellen St (building 2): A walkway around building 2 with permeable brick interlocking. Landscaping and shrubs in front of Ellen Street's setback and between the building and the Driveway which gives a Residential Scale for the Streetscape. Also the walkway between Heritage Orange Hall and 49 Queen St has a brick interlocking - traditional material in Streetsville.

Fence

New boundary fence to be added between the Tandem Parking and 9 Ellen St Residential Apt. The Fence will stay between the parking area and the Existing Plaza at 63 Queen St S. We prefer to remove the Fence between the exterior walkway and the exiting Garden/parking of 45 Queen Street, to explore our project to Queen St S.

There is new Development for the vacant Land at 53 Queen Street South. We had many meetings together in last year. We had agreed to remove the Fence between our Parking Area & their parking at the Back, there is different in grading between the 2 lots (600 mm).

Awning

No awning is used in 5 Ellen St (same for 49 Queen St S)

Signs

One Facia Sign is used at the base of Balcony at the front Facade.

Car Parking

29 Car Parking Spots are behind Building 2 and Building 1. Only one Vehicle access from 5 Ellen St (sharing Right of Way). Low Traffic with dead-end Circulation. This Layout was approved by the Committee of Adjustment. 5 Ellen St Building is hiding the parking from streetscape per Planning Dept requirements.

Culture Landscape

The design for both buildings: 49 Queen St S and 5 Ellen St with decorative Brick Façade and creation of a Node (Heritage Tower) are following the Culture Landscape of City Of Mississauga in General and Streetsville Village particularly to give sense of Streetsville Heritage & Tradition. Also have many Cultural Features which give Harmony with Streetsville Village, including: Fenestrations, Trims, Finial, and heritage light fixtures.

8: Conclusion:

The original Heritage Impact Assistance had a comprehensive Feasibility Study for the Project & the Site, including Streetscape and heritage evaluation for Queen St S and Ellen St.. 5 Ellen St is not Adjacent to Orange Hall Heritage Designated (47 Queen St). The Layout and Facades were reviewed carefully by the Architect, Urban Planner and The City Councillor (Mr George Carlson) at the early stage of the Preliminary design of the project. And is following the Historical Streetsville Guidelines. (Ellen Streetscape is the *Areas in Transection*).

This Report is the evaluation of proposed façade for Ellen Streetscape and 5 Ellen St. The proposed design for the Facade is following existing façade of new 2 Storeys Apartment building at 52 Queen St with minor changes to locate 2 Retails at the Ground floor with direct access from Ellen St. – Refer to the photos at the end of the Report.

Proposed Streetscape is following Chapter 14-Community Nodes City of Mississauga, Streetsville Heritage Guidelines. And the comments (Mark-up) of the Urban Designer / Planning and building Dept during many revisions of Site Plan Application.

Site Plan Layout and building Facades comply with Culture Landscape Inventory for the City of Mississauga. Proposed 5 Ellen St will be a good sample for *Area in Transaction* at the North of Streetsville Historical Village.

9: Project Applications History:

- Heritage Impact Assessment (comprehensive Evaluation Report) was submitted on March 2014, approved on June 17 2014.
- For the Benefits of the Neighbourhood, Ward 11 Councillor (Mr George Carlson) and Planning & Development Design Dept advised to relocate the small Building to the Front, facing Ellen St to hide the car parking. (August 2014)
- Committee of Adjustment decision on Dec 11 2014 approved the current Site Plan Layout.
- Demolish Permit approved by the Heritage Dept on July 03 2014
- Demolish Permit received for existing buildings: 49 Queen St S and 5 Ellen St on Oct 2014
- Site Plan Revision 2 was submitted on Feb 27 2015, and then Rev 3 on May 26 2015
- Latest Site Plan Application Rev 4: July 14 2015 (SW 11 / 129), Status Report Received from the City on Oct 01 2015
- Building Permit Application : June 30 2015 (# 6402).
- Site Plan Application Rev 4: Nov 09 2015

10: Attached Documents:

- Committee of Adjustment Decision: Dec 11 2014
- Current Land Survey and Bin Abstract No (merging the 2 Lands to one).
- Colored Elevations and 3 Dimention Presentation rendering.
- Chapter 14-Community Nodes (Mississauga Official Plan - Part 3) – Streetsville
- Comments from the Urban Designer during Site Plan Approval process to improve the Façade, Mass and the Elevations for 5 Ellen St..
- Samples of recent Residential project in Streetsville Heritage Village.
- Architectural drawings and Details for 5 Ellen St: A1, A2, A3, A4, A6, A7, A8, A9, A15, A17

Sources:

- Heritage Impact Assistance (Joan Burt Architects - Feb 2014)
- Chapter 14-Community Nodes (Mississauga Official Plan - Part 3).
- Historic Streetsville Design Guidelines (Development & Design Division/ City of Mississauga)
- Urban Planner comments and Mark-up drawings, Building & Planning Dept.
- Site Plan Application Status Report (Apr 17 & Oct 01 2015) City of Mississauga.

Rafid Kustou / RALLY Architects

Dec 02 2015

RALLY Architects, Arch Design & Building Permit, #3, 1 Chester Le Blvd, Tor onto, M1W 2M7, Canada,
RTD BUSINEES CENTRE 49 Queen St S

Tel: 1 416 494 9764, E:Rallyarch@gmail.com
November 20 2015

Qualification



RALLY Architects / Rafid Kustou

RALLY Architects – Architectural Design, Feasibility Study and Building Permit Application, Toronto

Architectural Services & Experience in Heritage projects

RTD Centre: 49 Queen St, Streetsville, Mississauga.

2012 - present

- 2 Storeys Mixed Use Development - Building Area 1223 m2 (13165 sf) & \$4,000,000 construction fees. Scope includes: Feasibility Study, Site Plan Layout, Site Plan Approval, Committee of Adjustment and Building Permit Application. Design Development and construction Documents for this project. Create the Schedule of Work and coordinate the Scope & Progress for All Engineers. Coordinate with all departments at the City of Mississauga & Community Services Department (Heritage Dept.). Following on all the restrictions and requirements for Streetsville Heritage Village.

I had helped the Heritage Consultant (Ms Joan Burt) in all stages of Heritage Impact Assessment including: As-built, professional photographs, searching historical info about Streetsville Heritage Village & information about Heritage Orange Hall (47 Queen St S) and Worked closely on the design of Front Elevation on Queen St S with full AutoCAD production.

Site Measurement and As-built (Exterior Façade, Fenestration, Heritage elements and interior partitions /Layout for Orange Hall at 47 Queen St S.(refer to attached file)

NORR Limited, Toronto

July 2005 – Jan. 2009

Job Captain,

- Union Station Design Development study (2nd Platform & Concourse Improvements) Nov 2007 Member of team to develop a preliminary study/Report for the existing building & preliminary design / sections for the addition of a mezzanine floor & alternation, coordinating with prelim structure dwgs, at design development stages including site visits.

NORR Limited, CVS Pharmacy – Milwaukee –Wisconsin, USA

Apr. - May 2006

- Joined the survey team for CVS Pharmacy Stores alternations through many travels to different cities around Milwaukee. Team is responsible to complete the survey/As-Built for around 30 (15000 SF each) / 2 days per store, the survey included: Site measurement, Photos, locate existing furniture & shelves in retails, finishes schedule and On-site CAD production for quick coordination.

Bregman + Hamann Architects, Toronto
Zeidler Grinnel Partnership Architects, Toronto

Joint venture

Oct.2002 – May 2003

Architectural Technologist

- I was part of a professional team for producing working drawing, details and construction documents for (Fallsview) **Casino Niagara** project. I worked on the details of the Façade of existing Heritage (Hydro building) which is attached to the new project. Coordinate the Architectural drawings with Shop dwgs and engineering drawings. CAD Production some details for the proposed Steel Frame to support the Heritage Façade from inside the building.
- Worked on the Site Plan and Design Development, preparing CAD drawings for different buildings for the project of **Xiamen National Accounting College** (China). Helped for coordination for the new construction with the existing Heritage building at this college

Conseco-International, British planning & Consultants, Abu Dhabi - U.A.E Feb. 2000 – Feb. 2001

Architect - Job Captain

Samalia Heritage Student Campus in Samalia Island – Abu Dhabi.

- Prepared full sets of architectural drawings for new two storey Residential Project in Abu Dhabi (Total built-up area: 4000m². Total cost: \$3,000,000.00). To create new building with Traditional Heritage Style.
- Coordinate full set of all the drawings including landscaping, structural & Electromechanical.
- Contract Admin and site visits/reports.
- Create Heritage Items Library and CAD production for many Heritage and Traditional details.

Affiliation

- **OAA** (Ontario Association of Architects) – Architect.
- **TSA** (Member of Toronto Society of Architects).
- **SOF** (Member of U.A.E. Society of Engineering -since 1999).

Community & Volunteer Services

- Member of OAA volunteers team for Doors Open Toronto for 8 years. Usually help at OAA headquarter, other Heritage Building/locations: Ontario Legislative Building (Queen's Park), Distillery Historic District and Toronto Railway Museum & Steam whistle Brewing.
- Member of Mar Barsaumo Church new development Committee for the new design & construction of Mar Barsaumo new Traditional Orthodox church/ Markham 2007-2010. Part of team to establish: Design Program, Budget, Schedule of work and study/research for traditions/Orthodox Heritage style.
- I had joined the Heritage Tour for Streetsville village by BIA (Business Improvement Association).

Technical Experience

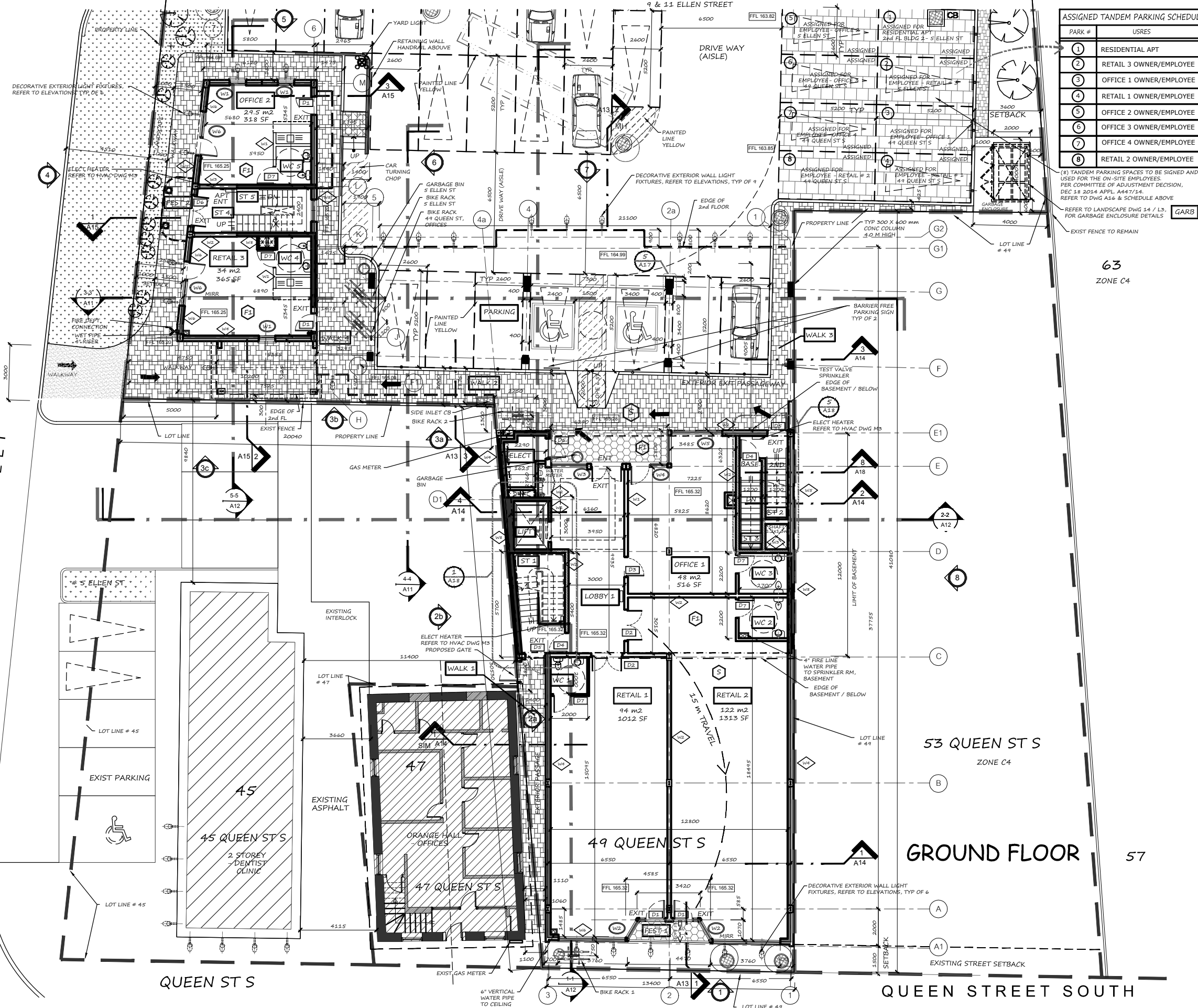
- Advanced technical knowledge in preparing working drawings/details & engineering system Coordination for Heritage Projects.
- Good Experience in producing As-Built drawings and Site Measurement for Heritage buildings
- Good practice in Ontario Building Code and National Building Code.
- Twenty years of experience in AutoCAD (R2006 to 2015). 2 years of experience with Arch Revit 2010.
- Variety of overseas experience with International Architectural & Engineering Consulting.

Samples of drawings and are available

Contact Information**Rafid Kustou B.SC.ENG.ARCH., OAA – Principal / RALLY Architects**Unit 3, 1 Chester Le Blvd.,
Toronto, Ontario, M1W 2M7E-mail: Rallyarch@gmail.com
Office (416) 494 9764 – Cel (416) 880 9764

ROOM NAME	
RETAIL #	RETAIL STORE
LOBBY #	MAIN LOBBY
OFFICE #	OFFICE
WC #	WASHROOM
WALK #	EXTERIOR COVERED WALKWAY
PARKING	COVERD PARKING
FEST 2	EXTERIOR FESTIBULE
GARB	GARBAGE ROOM
ELECT	ELECTRICAL ROOM
ST #	STAIRCASE
LIFT	H.C. PASSENGER ELEVATOR

ELLEN STREET



ASSIGNED TANDEM PARKING SCHEDULE	
PARK #	USERS
1	RESIDENTIAL APT
2	RETAIL 3 OWNER/EMPLOYEE
3	OFFICE 1 OWNER/EMPLOYEE
4	RETAIL 1 OWNER/EMPLOYEE
5	OFFICE 2 OWNER/EMPLOYEE
6	OFFICE 3 OWNER/EMPLOYEE
7	OFFICE 4 OWNER/EMPLOYEE
8	RETAIL 2 OWNER/EMPLOYEE

(8) TANDEM PARKING SPACES TO BE SIGNED AND USED FOR THE ON-SITE EMPLOYEES. PER COMMITTEE OF ADJUSTMENT DECISION, DEC 18 2014 APPL. A447/14. REFER TO DWG A16 & SCHEDULE ABOVE FOR GARBAGE ENCLOSURE DETAILS.

STREETSVILLE

PROJECT BRIEF

2 STOREYS MIXED USE DEVELOPMENT, ON "I" SHAPE LOT, FACING 2 STREETS "THROUGH LOT", BESIDE A HERITAGE DESIGNATED "ORANGE HALL", SHARING RIGHT OF WAY & IN STREETSVILLE VILLAGE-MAINSTREET CHARACTER AREA

SURVEYORS REAL PROPERTY REPORT

PART OF LOTS 112 & 113
ACCORDING TO HYDE & RUTLEDGE'S PLAN
OF THE VILLAGE OF STREETSVILLE
REFERRED TO AS PLAN No. STR-2
CITY OF MISSISSAUGA
REGIONAL MUNICIPALITY OF PEELE

GENERAL NOTES

THIS DRAWING IS THE PROPERTY OF RALLY Architects. IT SHALL NOT BE USED OR REPRODUCED WITHOUT PRIOR PERMISSION. DRAWINGS SHALL NOT BE SCALED. CONTRACTORS TO VERIFY ALL DIMENSIONS AND CONDITIONS ON SITE AND REPORT ANY DISCREPANCIES TO THE ARCHITECT PRIOR TO COMMENCING WORK. THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION PURPOSES UNLESS INDICATED AS "ISSUED FOR CONSTRUCTION".

IT IS CONTRARY TO THE ONTARIO BUILDING CODE ACT TO COMMENCE CONSTRUCTION PRIOR TO RECEIPT OF A VALID BUILDING PERMIT.

NOTES:

- THIS PROPOSED DEVELOPMENT IS CONSIDERED ONE BUILDING / ONE BUILDING PERMIT APPLICATION. WITH 2 ADDRESSES. THIS MATTER WAS DISCUSSED EARLIER (MAY 2015) WITH BUILDING DEPT., FIRE PREVENTION & FIRE SAFETY DEPT. AND PROPERTY & ZONING DEPT.
- FOR MATTER OF COORDINATION, WE WILL CALL PORTION #1: (49 QUEEN ST S) IS THE MAIN BLDG MIXED USE (COMMERCIAL-OFFICES) PORTION #2: (5 ELLEN ST) IS THE SMALL BUILDING RESIDENTIAL APT AT THE 2ND FL. & COMMERCIAL AT GR. FL.
- ALL SIGNS SHOWN ON PROPOSED BUILDING ELEVATIONS (EXCEPT BARRIER FREE SIGNAGE AND FIRE FIGHT SIGN) NEED TO HAVE A SEPARATE PERMIT AND TO FOLLOW MISSISSAUGA SIGN BY-LAW.

ABBREVIATION

EXIST: EXISTING	UG: UNDER GROUND	TC: TOP OF CURB
CL: CENTRE LINE	SP: SITE PLAN	TW: TOP OF WALL
M: METER	ST: STEEL	TD: TOP OF
mm: MILLIMETER	SIM: SIMILAR	UG: UNDER SIDE
MIN: MINIMUM	TYP: TYPICAL	

RAFIK KUSTOU
RALLY Architects

TEL: 416 454 9764 rallyarch@gmail.com

Sign: _____

Date: NOV 20 2015

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RAFIK KUSTOU
RALLY Architects

ONTARIO ASSOCIATION OF ARCHITECTS

RAFIK KUSTOU
 LICENSE
 7196

PROJECT TITLE:
RTD CENTRE
2 STOREYS NEW BUILDING
MIXED USE DEVELOPMENT

OWNER: 2112836 ONTARIO INC

ADDRESS:
 49 QUEEN STREET SOUTH & 5 ELLEN ST,
 STREETSVILLE, MISSISSAUGA, ON, L5M 1K5

DRAWING TITLE:
GROUND FLOOR PLAN

REVISION:

REV	DATE	DISCUSSION
1	30-05-2015	BUILDING PERMIT APPLICATION
2	05-11-2015	REVISION 4 - SITE PLAN APPLICATION
3	20-11-2015	REV 1 - BLDG PERMIT APP - STATUS REPORT SEPT 22 2015

SCALE: 1/100

PROJECT # R3-2013

DRAWN BY: RAFIK K

SITE PLAN APP # SP 11/129

REV NO. 3

DWG NO. A6

DATE: SEP 09 23

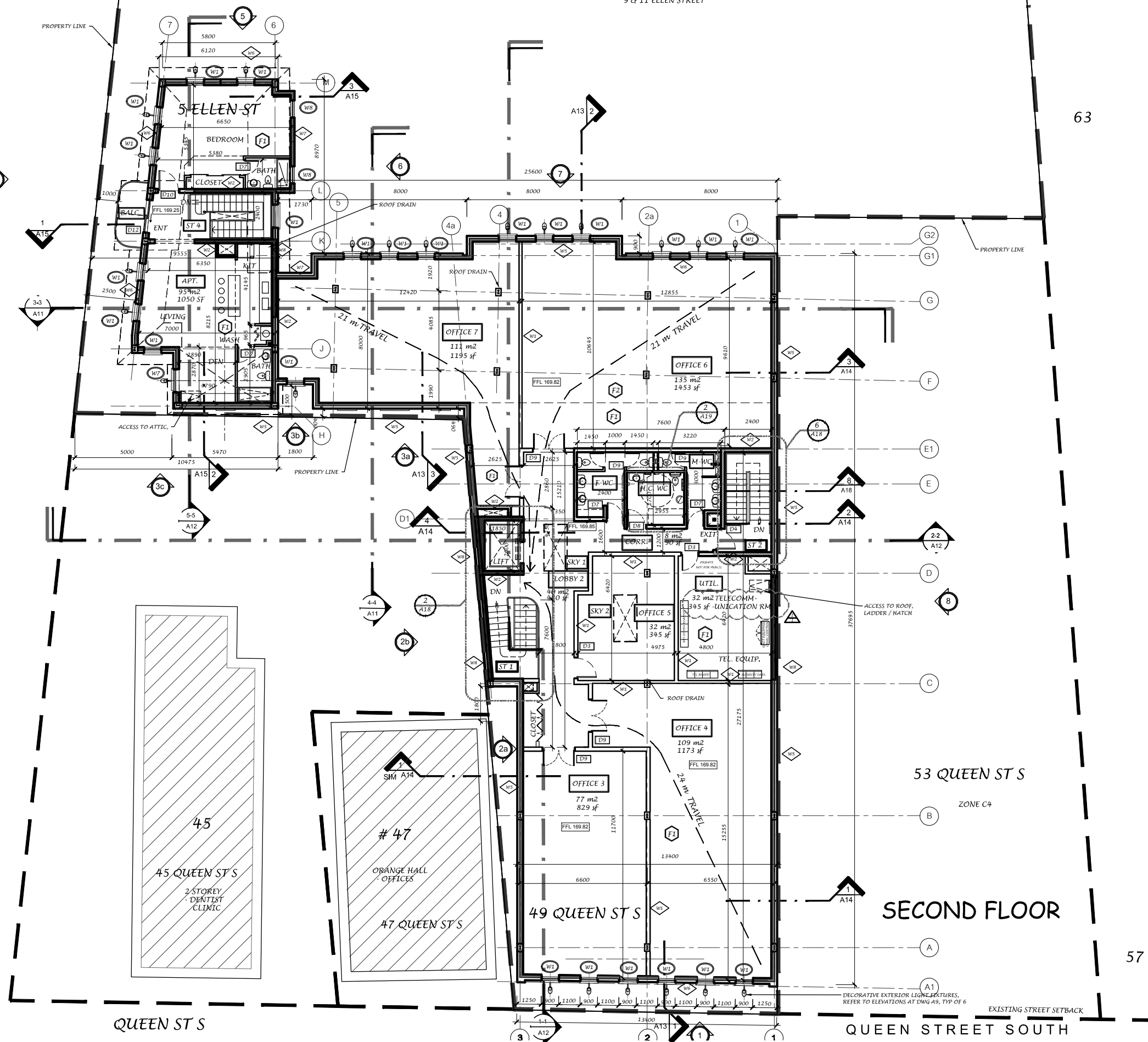
CHECKED BY: RAFIK K

BLDG PERMIT APP # 6402

Printed Dec 02 15

ROOM NAME	
OFFICE #	OFFICE
LOBBY #	MAIN LOBBY
APT.	1 BEDROOM RESIDENTIAL APARTMENT
M-WC	MALE/FEMALE WASHROOM
HC-WC	UNIVERSAL WASHROOM
CORR.	CORRIDOR
BALC.	BALCONY
UTIL.	UTILITIES
SKY.	SKYLIGHT
ST #	STAIRCASE
LIFT	H.C. PASSENGER ELEVATOR

ELLEN STREET



SECOND FLOOR

STREETSVILLE

PROJECT BRIEF

2 STOREYS MIXED USE DEVELOPMENT, ON -V- SHAPE LOT, FACING 2 STREETS "THROUGH LOT", BESIDE A HERITAGE DESIGNATED "ORANGE HALL", SHARING RIGHT OF WAY & IN STREETSVILLE VILLAGE-MAINSTREET CHARACTER AREA

SURVEYORS REAL PROPERTY REPORT

PART OF LOTS 112 & 113
 ACCORDING TO HYDE & RUTLEDGE'S PLAN
 REFERRED TO AS PLAN No. STR-2
 CITY OF MISSISSAUGA
 REGIONAL MUNICIPALITY OF PEEL

GENERAL NOTES

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NOTES:

1. THIS PROPOSED DEVELOPMENT IS CONSIDERED ONE BUILDING / ONE BUILDING PERMIT APPLICATION, WITH 2 ADDRESSES. THIS MATTER WAS DISCUSSED EARLIER (MAY 2015) WITH BUILDING DEPT., FIRE PREVENTION & FIRE SAFETY DEPT. AND PROPERTY & GEOMATICS DEPT.

2. FOR MATTER OF COORDINATION, WE WILL CALL: PORTION #1: (49 QUEEN ST S) IS THE MAIN BLDG. MIXED USE (COMMERCIAL-OFFICES) PORTION #2: (5 ELLEN ST) IS THE SMALL BUILDING RESIDENTIAL APT. AT THE 2ND FL. & COMMERCIAL AT GR. FL.

3. ALL SIGNS SHOWN ON PROPOSED BUILDING ELEVATIONS (EXCEPT BARBER FREE SIGNAGE AND FIRE SIGN) NEED TO HAVE A SEPARATE PERMIT AND TO FOLLOW MISSISSAUGA SIGN BY-LAW.

ABBREVIATION

EXIST: EXISTING	CL: CENTRE LINE	US: UNDER GROUND	TC: TOP OF CURB
CL: CENTRE LINE	ST: STEEL	SP: SITE PLAN	TW: TOP OF WALL
m: METER	mm: MILLIMETER	SM: SIMILAR	TO: TOP OF
MIN: MINIMUM	TYP: TYPICAL	US: UNDER SIDE	

RAFID KUSTOU
RALLY Architects

TEL: 416 454 9764 rallyarch@gmail.com

Sign: _____

Date: NOV 20 2015

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 7198

PROJECT TITLE: **RTD CENTRE**
 2 STOREYS NEW BUILDING
 MIXED USE DEVELOPMENT

OWNER: 2112836 ONTARIO INC

ADDRESS:
 49 QUEEN STREET SOUTH & 5 ELLEN ST,
 STREETSVILLE, MISSISSAUGA, ON, L5M 1K5

DRAWING TITLE
SECOND FLOOR PLAN

REVISION:

REV	DATE	DISCUSSION
1	30-06-2015	BUILDING PERMIT APPLICATION
2	05-11-2015	REVISION 4 - SITE PLAN APPLICATION
3	20-11-2015	REV 1 - BLDG PERMIT APP - STATUS REPORT SEPT 22 2015

SCALE: 1/100	PROJECT # R3-2015	DRAWN BY: RAFID K	SITE PLAN APP # SP 11/129	REV NO: 3	DWG NO: A7
DATE: SEP 09 23	CHECKED BY: BLDG PERMIT APP # 6402				

printed Dec 02 15

URBAN DESIGNER'S COMMENTS FOR
STATUS REPORTS - 2015

MISSISSAUGA OFFICIAL PLAN
PART 3 - COMMUNITY NODES

COMMITTEE OF ADJUSTMENT
DECISION - DEC 11 2014

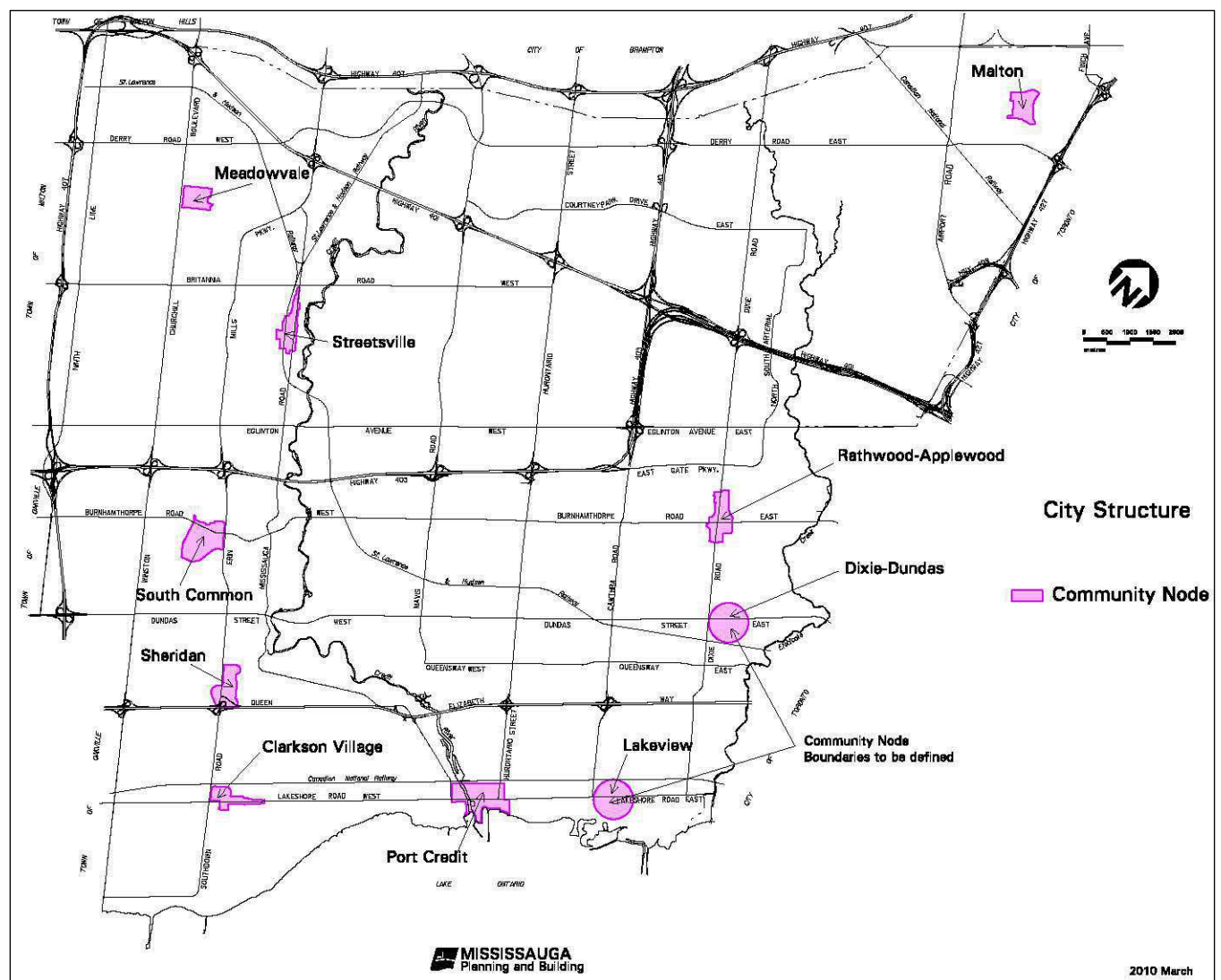
14 Community Nodes

14.1 Introduction

There are ten Community Nodes in Mississauga:

- Clarkson Village;
- Dixie-Dundas;
- South Common;
- Lakeview;
- Malton;
- Meadowvale;
- Port Credit;
- Rathwood-Applewood;
- Sheridan; and
- Streetsville.

This section identifies the modifications to the General Land Use designations in Chapter 11 that apply to all Community Nodes.



14.1.1 General

14.1.1.1 Proponents of development applications within a Community Node may be required to demonstrate how the new development contributes to the achievement of the residents and jobs density target and the population to employment ratio.

14.1.1.2 For lands within a Community Node a minimum building height of two storeys to a maximum building height of four storeys will apply unless Character Area policies specify alternative building height requirements or until such time as alternative building heights are determined through the review of Character Area policies.

14.1.1.3 Proposals for heights less than two storey, more than four storeys or different than established in the Character Area policies will only be considered where it can be demonstrated to the City's satisfaction, that:

- a. an appropriate transition in heights that respects the surrounding context will be achieved;
- b. the development proposal enhances the existing or planned development;
- c. the City Structure hierarchy is maintained; and
- d. the development proposal is consistent with the policies of this Plan.

14.1.2 Residential

14.1.2.1 The Residential Low Density I and Residential Low Density II designations will not be permitted, except for lands designated Low Density I and Residential Low Density II at the time this Plan comes into effect.

14.1.2.2 Notwithstanding the Residential Medium Density policies of this Plan, the following additional uses will be permitted:

- a. low-rise apartment dwellings.

14.1.2.3 Proposals for additional development on lands with existing apartment buildings will, as a condition of development, demonstrate that the site in its entirety meets current site plan and landscaping requirements, and existing buildings meet current building code, fire code and property standards.

14.1.3 Mixed Use

14.1.3.1 Notwithstanding the Mixed Use policies of this Plan, the following uses will not be permitted, except uses existing at the time this Plan comes into effect:

- a. Permitted uses with drive-through facilities.

14.1.4 Office

14.1.4.1 Notwithstanding the Office policies of this Plan, the following additional uses will be permitted:

- a. Post secondary educational facilities.

14.1.5 Motor Vehicle Commercial

14.1.5.1 The Motor Vehicle Commercial designation will not be permitted, except for lands designated Motor Vehicle Commercial at the time this Plan comes into effect.

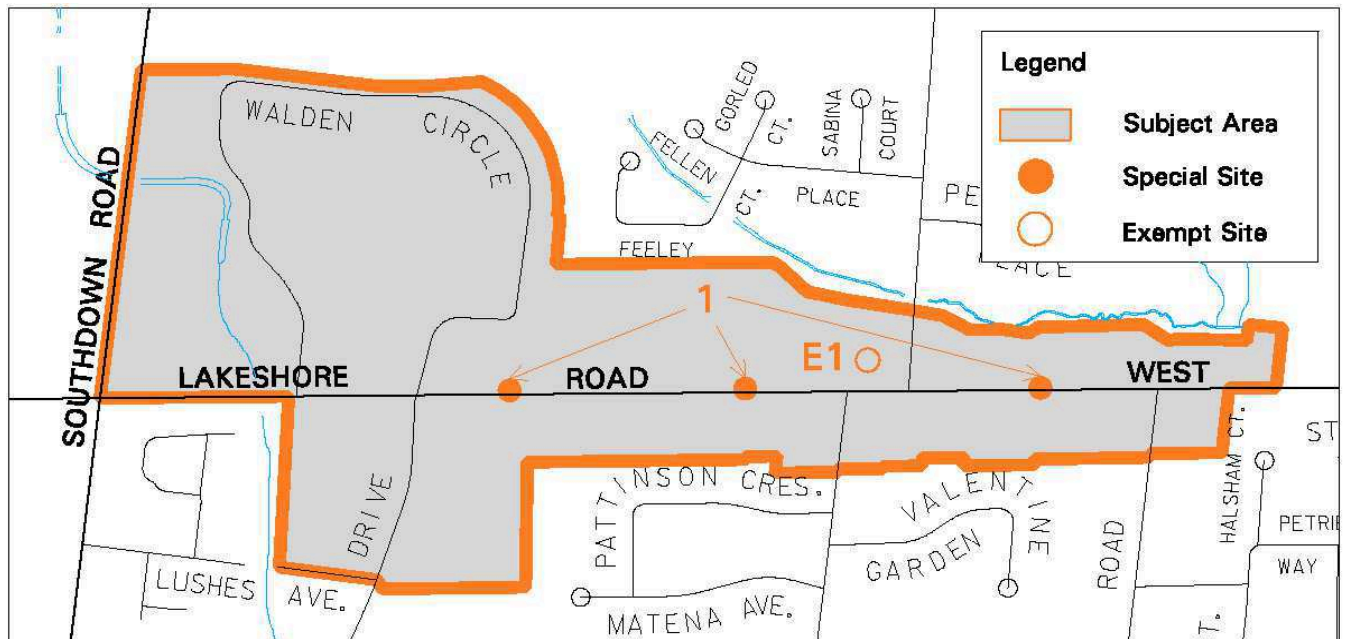
14.1.6 Business Employment

14.1.6.1 The Business Employment designation will not be permitted, except lands designated Business Employment at the time this Plan comes into effect.

14.1.7 Industrial

14.1.7.1 The Industrial designation will not be permitted.

14.2 Clarkson Village



Clarkson Village Community Node Character Area

14.2.1 Urban Design Policies

Community Identity and Focus

14.2.1.1 The Clarkson Village Community Node will be the focus of activity for surrounding Neighbourhoods, combining residential uses, cultural activities, shopping, dining, commerce and recreation

14.2.1.2 Developments should be compatible with and enhance the character of Clarkson-Lorne Park as a diverse established community by integrating with the surrounding area.

14.2.1.3 Development should be designed to reflect and enhance the Clarkson Village Mixed Use area **streetscape**.

14.2.2 Transportation

14.2.2.1 No major changes to Clarkson Road will be undertaken except minor channelization, reconstruction, bus-bay construction, and improvements of a similar nature.

14.2.2.2 Notwithstanding the classification of Clarkson Road, this road will be limited to no more than two through lanes.

14.2.2.3 Mississauga will encourage and promote better utilization of the existing parking inventory within the Clarkson Village Mixed Use area through discussions with the members of the Clarkson Village Business Improvement Area.

14.2.3 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

14.2.3.1 Site 1



14.2.3.1.1 The lands identified as Special Site 1 are located on the north and south sides of Lakeshore Road West between Walden Circle and Meadowwood Road.

14.2.3.1.2 Notwithstanding the Residential Low Density I, Residential High Density and Mixed Use designations, the following additional policies will apply.

14.2.3.1.3 Encourage pedestrian walkways and elements along the edge of Turtle Creek to connect to the parking areas and the shopping street to encourage the visual enjoyment and surveillance of this natural creek feature.

14.2.3.1.4 Encourage the development of symbolic gateways to define entry to and exit from Clarkson Village.

14.2.3.1.5 Encourage more on-street parking and promote the introduction of well-lit on-site parking to be located at the rear of buildings with clear visibility and sight lines through to Lakeshore Road West.

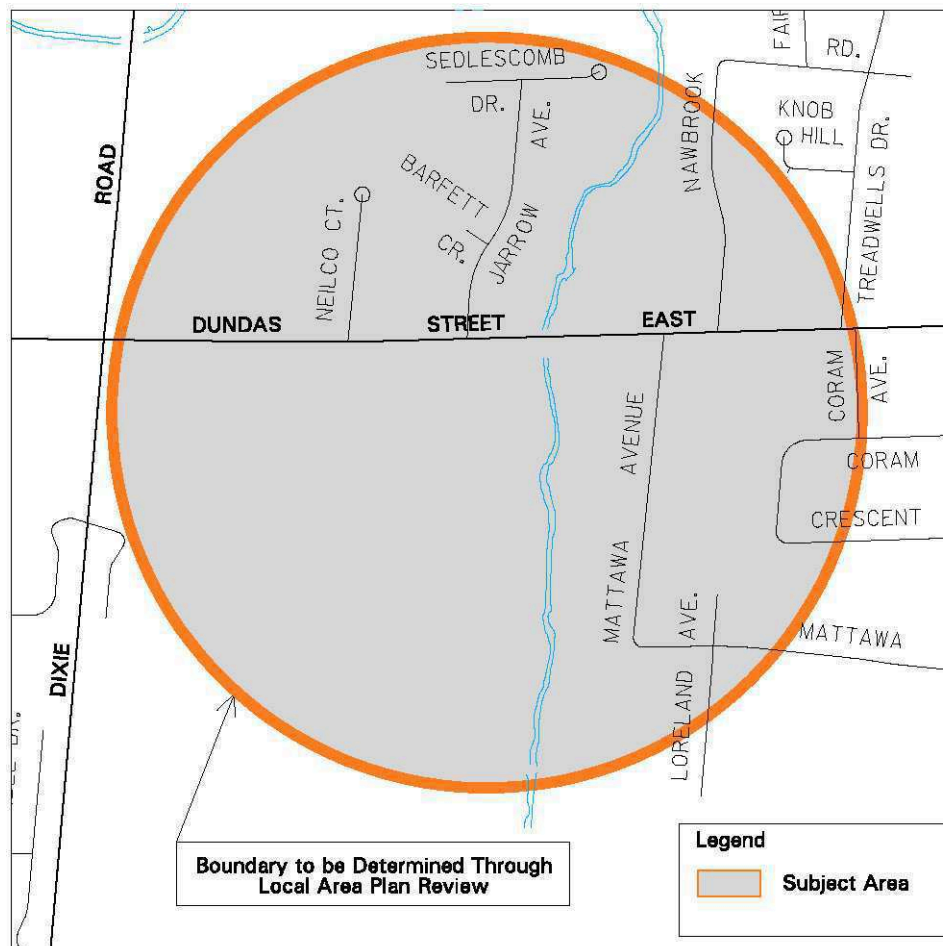
14.2.3.1.6 Encourage two to three storey, zero street lot line, mixed use development.

14.2.3.1.7 For lands identified as Area A, a business, professional or administrative office will also be permitted provided that it is designed to be compatible with the abutting residential area.

14.2.3.1.8 For lands identified as Area B, a retirement dwelling with a maximum building height of eight storeys will be permitted.

14.3 Dixie-Dundas

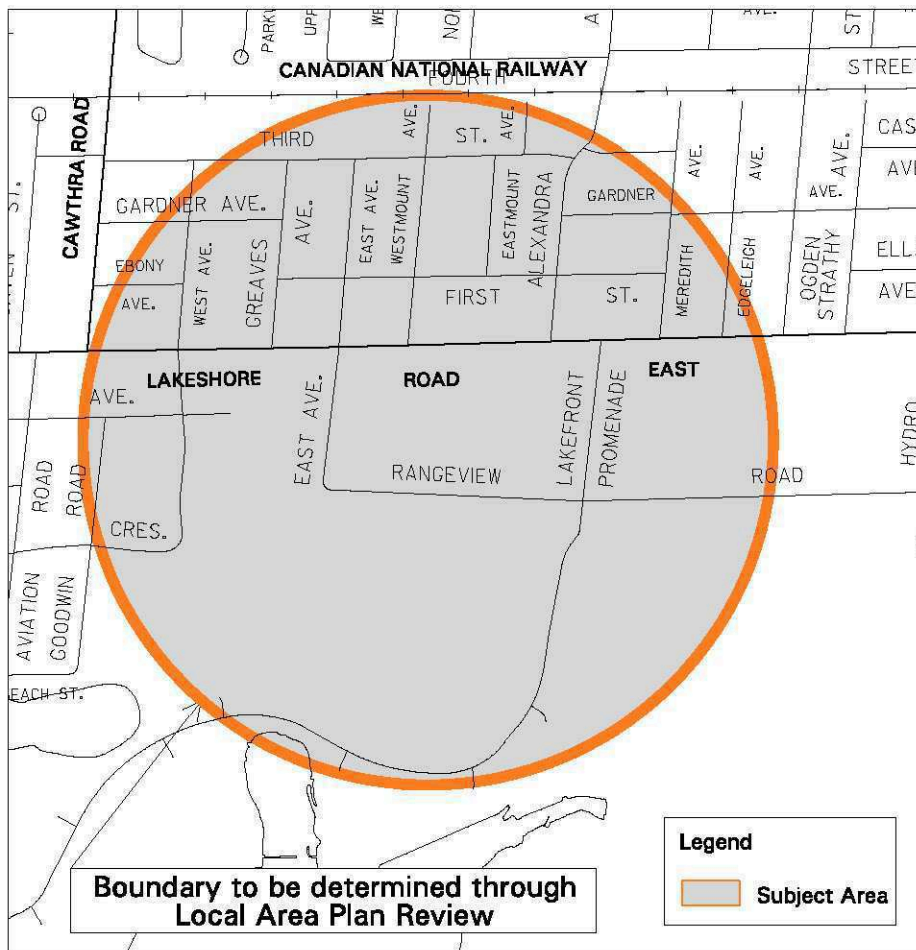
The boundary of and Official Plan policies for lands within the Dixie-Dundas Community Node will be determined through a Local Area Plan review. Until such time the Local Area Plan review is completed, the Official Plan policies are contained in the Dixie Employment Area Character Area Policies.



Dixie-Dundas Community Node Character Area

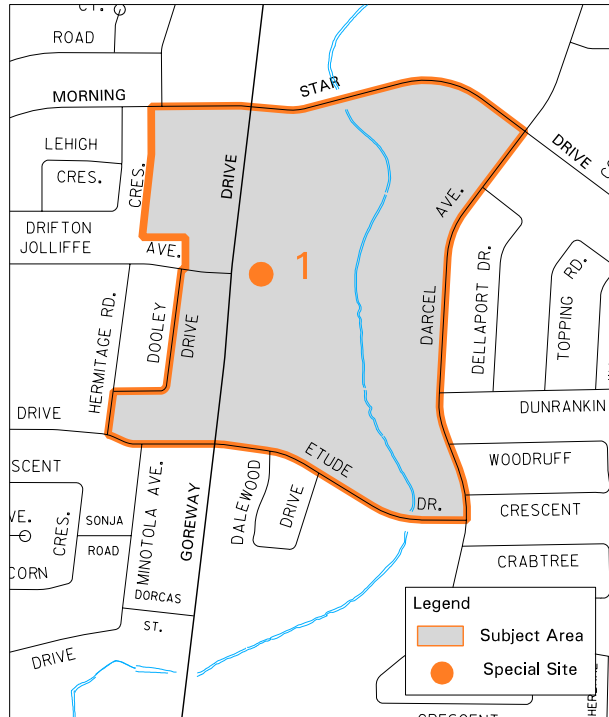
14.4 Lakeview

Official Plan policies for lands within the Lakeview Community Node are contained in the Lakeview Local Area Plan under separate cover. The boundary of the Lakeview Community Node will be determined through a Local Area Plan review.



Lakeview Community Node Character Area

14.5 Malton



Malton Community Node Character Area

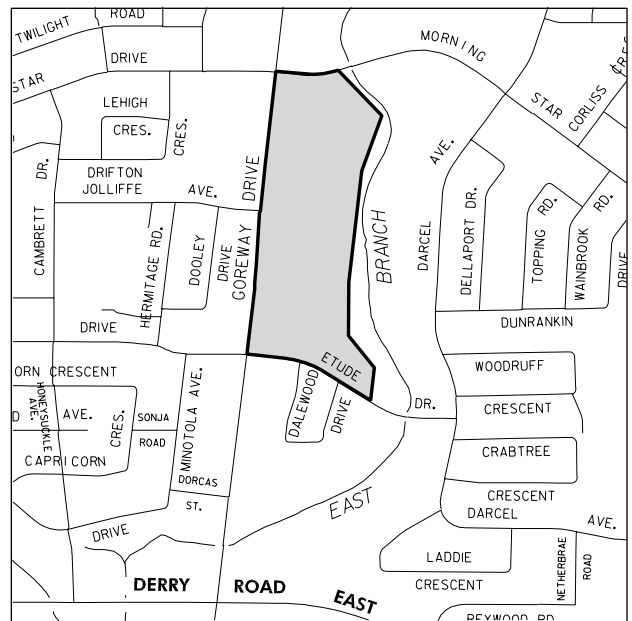
14.5.1 Urban Design Policies

14.5.1.1 During the evaluation of any development proposal the provision of open space connections that link commercial developments with public parks and community infrastructure through use of walkways, underpasses, bridges, **streetscape** development, and enhancement of pedestrian and cyclist access to greenbelt lands, should be encouraged wherever possible.

14.5.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

14.5.2.1 Site 1



14.5.2.1.1 The lands identified as Special Site 1 are located on the east side of Goreway Drive between Morningstar Drive and Etude Drive.

14.5.2.1.2 Notwithstanding the Mixed Use designation of this Plan, the following additional policies will apply.

Pedestrian Linkages

14.5.2.1.3 Pedestrian connections to the commercial uses along Goreway Drive are vital to their role in the community. The following pedestrian links should be maintained and/or improved as part of any development application:

- open space walkway linkages to the following parks: Elmcreek Park, Malton Greenway and Derry Greenway;
- mid-block connection to Jolliffe Avenue;

- c. linkages to the Malton Community Centre, the Transit Terminal and Westwood Secondary School (emphasizing safety).

14.5.2.1.4 Where pedestrian linkages from the Greenbelt and Residential Community exist, they are to be strengthened.

Focal Points

14.5.2.1.5 A focal point on the north side of Westwood Mall, should be developed:

- a. to strengthen the pedestrian connection from the corner of Goreway Drive and Morning Star Drive and from the Transit Terminal to Westwood Mall;
- b. to improve the building massing edge treatment, parking lot circulation, and **streetscape** development at the south-east corner of Goreway Drive and Morning Star Drive.

14.5.2.1.6 The main entrance to Westwood Mall from Goreway Drive, opposite Jolliffe Avenue, should be improved to contribute to the identity of the site through the use of built form, landscaping, directional signs and/or lighting.

Parking Areas

14.5.2.1.7 Parking areas around Westwood Mall should be clearly defined to facilitate the safe movement of pedestrians, cyclists and vehicles through the site. Parking areas should be improved by ensuring adequate sight lines, incorporating landscape islands that define vehicular routes throughout the site, and providing sidewalk and lighting.

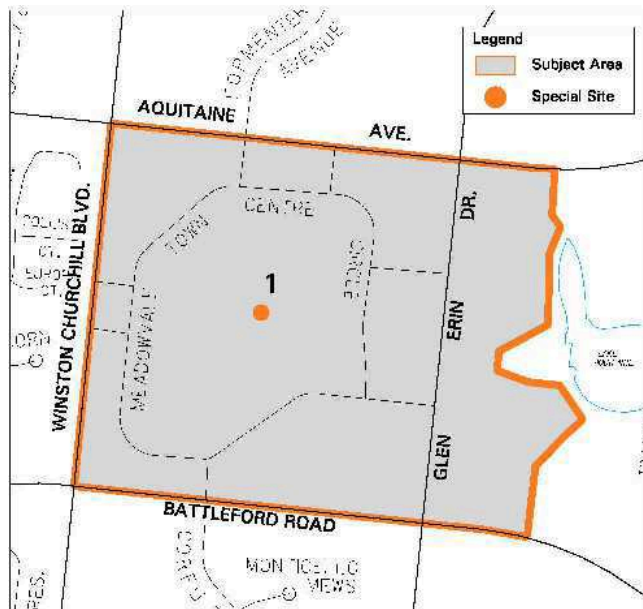
Building and Massing

14.5.2.1.8 Should additional development occur along the Goreway Drive frontage the following items should be addressed:

- a. no parking or driveway areas should be provided between the buildings and the street line;

- b. blank walls should be avoided along the street in favour of windows, building entrances and architectural detailing;
- c. service, loading, and garbage storage areas should be located on the internal side of the development, away from public streets, and should be screened from view by means of built form and landscaping;
- d. all building entrances should be clearly articulated and linked to pedestrian walkway connections.

14.6 Meadowvale



Meadowvale Community Node Character Area

14.6.1 Land Use

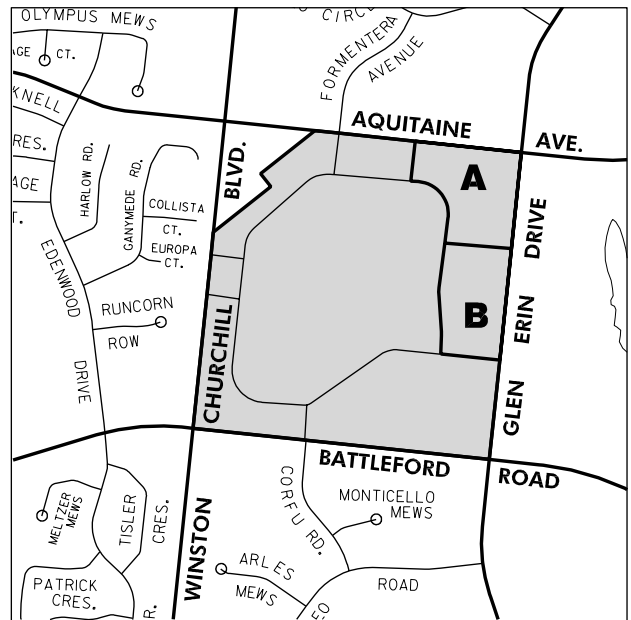
14.6.1.1 In addition to the general Residential Medium and High Density development policies of this Plan, the following additional policies will apply:

- in order to create acceptable built form transitions, buildings should be limited in height when adjacent to low density residential neighbourhoods;
- buildings immediately adjacent to low density housing forms should be limited to three storeys; and
- in situations where the low density housing forms are separated from high density development by a public road or other permanent open space feature, a height of four to five storeys may be compatible.

14.6.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

14.6.2.1 Site 1



14.6.2.1.1 The lands identified as Special Site 1 are bounded by Aquitaine Avenue to the north, Glen Erin Drive to the east, Battleford Road to the south and Winston Churchill Boulevard to the west.

14.6.2.1.2 Notwithstanding the provisions of the Mixed Use, Residential Medium Density and Residential High Density designations, the following additional policies will apply.

14.6.2.1.3 Area A is located at the southwest corner of Aquitaine Avenue and Glen Erin Drive. Notwithstanding the provisions of the Residential Medium Density designation, only townhouses and semi-detached dwellings with a maximum Gross Floor Area (GFA) of 0.6 times the lot area in the range of 34.6 to 44.5 units per **net residential hectare** are permitted.

14.6.2.1.4 Area B is located between Aquitaine Avenue and Battleford Road on the west side of Glen Erin Drive. Notwithstanding the provisions of

the Residential Medium Density designation, only townhouses with a maximum Gross Floor Area (GFA) of 0.63 times the lot area in the range of 34.6 to 44.5 units **net residential hectare** are permitted.

Pedestrian Linkages

14.6.2.1.5 Pedestrian connections to the Town Centre are vital to its role in the community. The following pedestrian links should be maintained and/or improved as part of any development application:

- a. open space walkway linkages to the following parks: Lake Aquitaine, Hunter's Green and Settler's Green;
- b. mid-block connections to the area west of Winston Churchill Boulevard between Battleford Road and Aquitaine Avenue;
- c. linkages to the Meadowvale Community Centre, medical offices and the transit terminal;
- d. diagonal connections to the site from the northeast corner of Battleford Road and Winston Churchill Boulevard.

14.6.2.1.6 A strong connection between the bicycle route on Winston Churchill Boulevard and the Town Centre should be provided.

Focal Areas

14.6.2.1.7 A focal area on the east side of the Town Centre between the existing transit terminal, medical offices, church campus and Town Centre tower should be developed to strengthen the space as an outdoor amenity with seating, landscaping, lighting or staging areas; additional building massing should contribute to the central courtyard area by framing the interior space.

14.6.2.1.8 One main entrance to the Town Centre from Winston Churchill Boulevard should be developed to strengthen the site entry and contribute to the identity of the site through the use of built form, landscaping, directional signs or lighting.

Ring Road and Parking Areas

14.6.2.1.9 The internal ring road and parking areas around the Town Centre should be clearly defined to facilitate the safe movement of pedestrians and vehicles through the site; definition of the ring road **streetscape** should be improved by ensuring adequate sight lines, incorporating landscape islands that define parking areas, and providing sidewalks and lighting.

Building Massing

14.6.2.1.10 Development along the public road frontages should address the following:

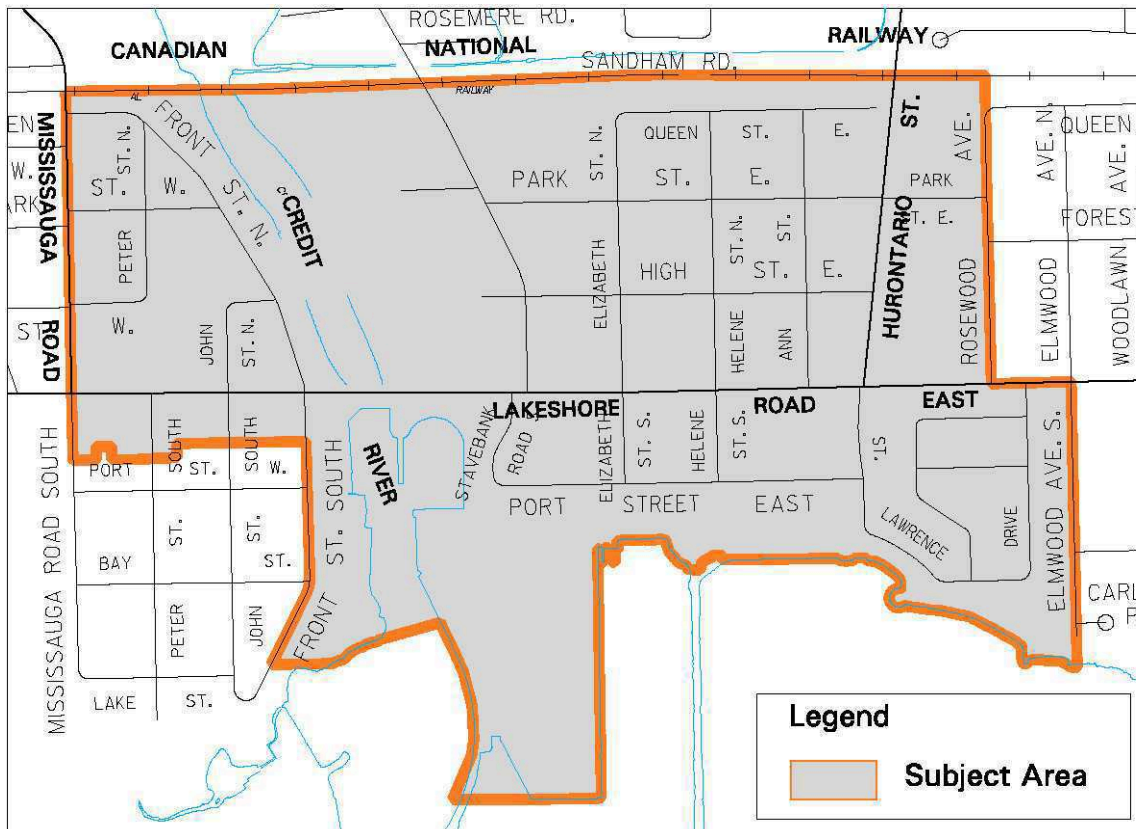
- a. no parking or driveway areas should be provided between the buildings and the street line;
- b. blank walls should be avoided along the street in favour of fenestration, building entrances and architectural detailing;
- c. service, loading and garbage storage areas should be located on the internal side of the development away from public streets and screened from view by means of built form and landscaping
- d. all building entrances should be clearly articulated and linked to pedestrian walkway connections.

Streetscape

14.6.2.1.11 Landscape treatment of the Winston Churchill frontage of the Town Centre should reinforce the treatment within the municipal boulevard.

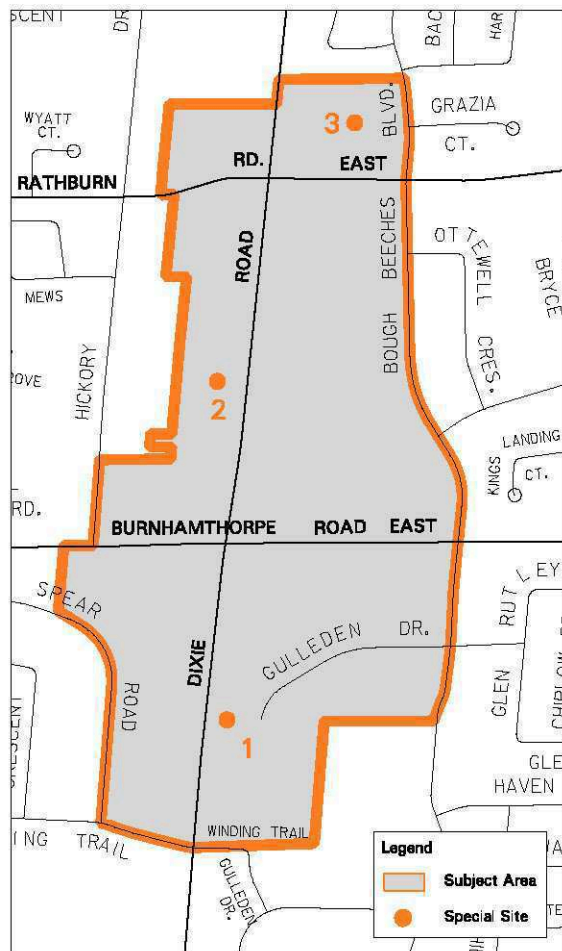
14.7 Port Credit

Official Plan policies for lands within the Port Credit Community Node are contained in the Port Credit Local Area Plan under separate cover.



Port Credit Community Node Character Area

14.8 Rathwood-Applewood



**Rathwood-Applewood Community Node
Character Area**

14.8.1 Land Use

14.8.1.1 For lands designated Residential Medium or High Density that are south of Burnhamthorpe Road, the following additional policies will apply:

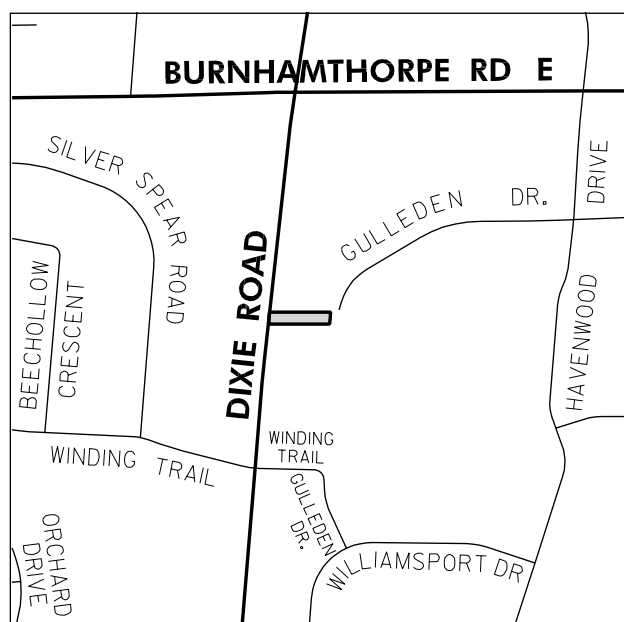
- new development should not exceed the height of any existing buildings on the property, and should be further limited in height so as to form a gradual transition in massing when located adjacent to low density residential development;
- buildings immediately adjacent to low density housing forms should be limited to three storeys; and

- in situations where the low density housing forms are separated from the high density development by a public road, park, utility corridor or other permanent open space feature, four to five storeys may be compatible.

14.8.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

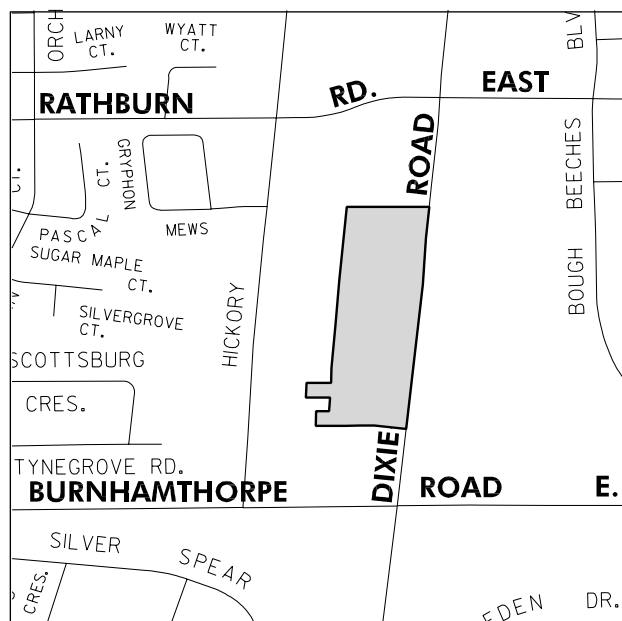
14.8.2.1 Site 1



14.8.2.1.1 The lands identified as Special Site 1 are located on the east side of Dixie Road, south of Burnhamthorpe Road East.

14.8.2.1.2 Notwithstanding the provisions of the Residential Medium Density designation, detached and semi-detached dwellings may also be permitted.

14.8.2.2 Site 2



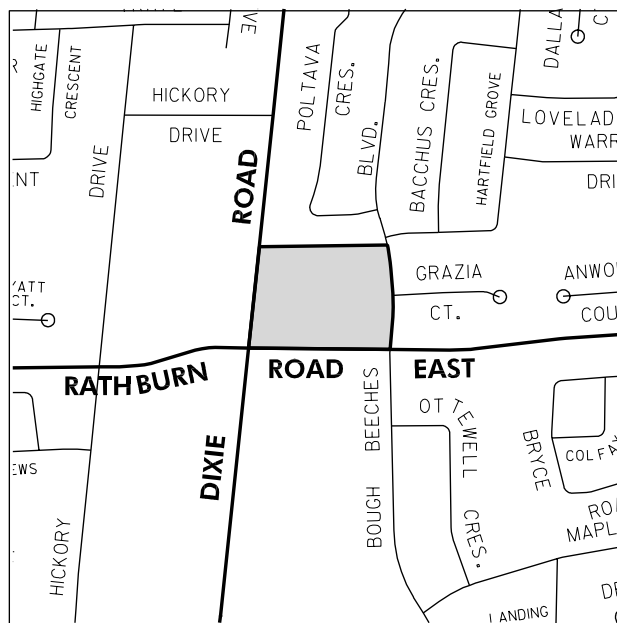
14.8.2.2.1 The lands identified as Special Site 2 are located west of Dixie Road and south of Rathburn Road East.

14.8.2.2.2 Notwithstanding the provisions of the Residential High Density and Convenience Commercial designations, the following additional policies will apply:

- a. a concept plan for all or part of this site may be required and will address, among other matters, the following:
 - compatibility of building form and scale with existing surrounding land uses;
 - convenient pedestrian access through the site to nearby transit service on Dixie Road and Burnhamthorpe Road East;
 - traffic generated will not adversely affect the transportation system;
 - acceptable ingress and egress, off-street parking, landscaping and buffering;
 - preservation of all mature trees and other significant natural features;

- b. a concept plan will be required to address, to the satisfaction of the City of Mississauga and the Region of Peel, matters relating to transportation and access onto Dixie Road;
- c. the redevelopment of this site will minimize access points to Dixie Road to preserve the integrity of Dixie Road as a major arterial roadway;
- d. the redevelopment of this site will allow for the construction of a centrally located street which is parallel to Hickory Drive and Dixie Road. The proposed street will connect to Dixie Road and align with the Rockwood Mall entrance on Dixie Road.

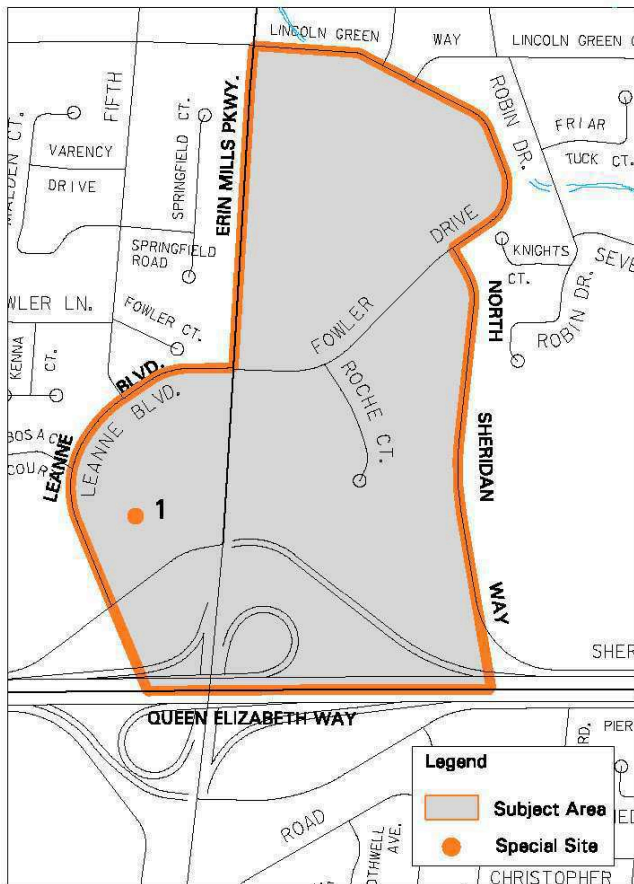
14.8.2.3 Site 3



14.8.23.1 The lands identified as Special Site 3 are located north of Rathburn Road East and east of Dixie Road.

14.8.23.2 Notwithstanding the provisions of the Residential High Density designation, townhouse dwellings will be permitted. The maximum Floor Space Index permitted for all development shall be 2.56.

14.9 Sheridan



Sheridan Community Node Character Area

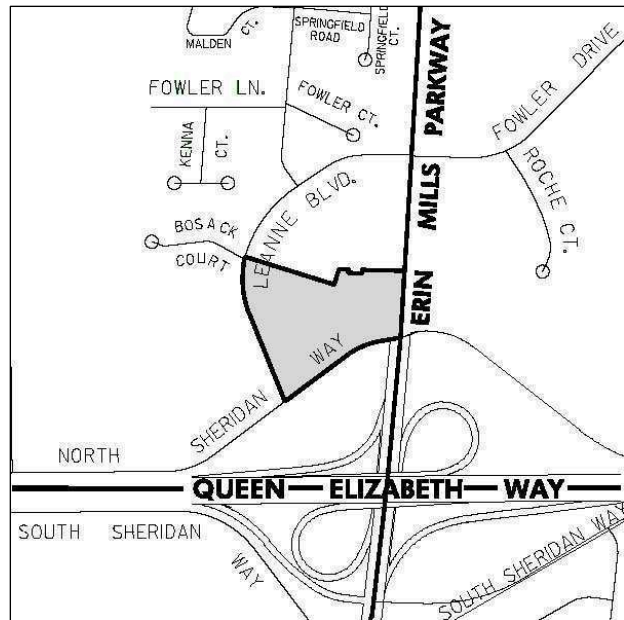
14.9.1 Land Use

14.9.1.1 For lands designated Mixed Use, existing Major office will be permitted.

14.9.2 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

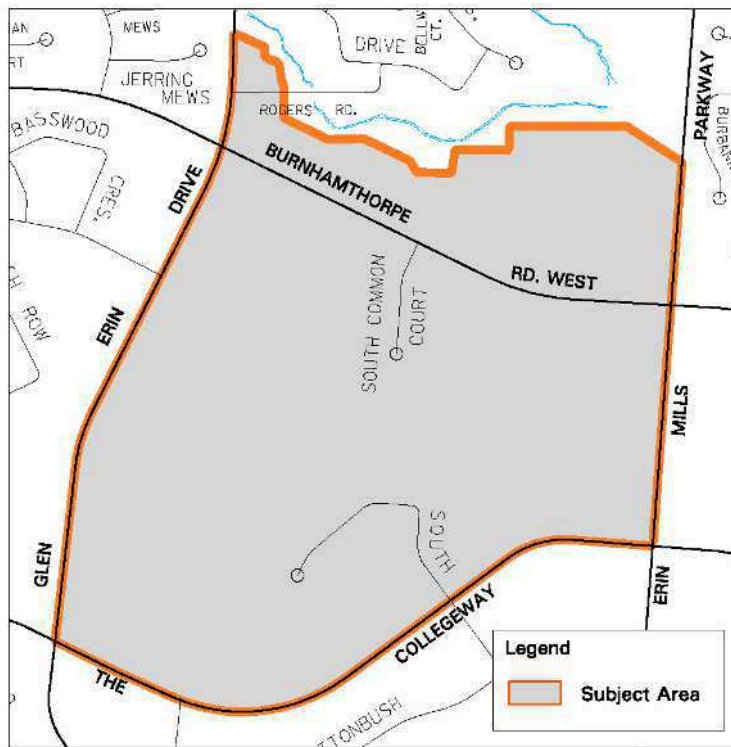
14.9.2.1 Site 1



14.9.2.1.1 The lands identified as Special Site 1 are located on the north side of North Sheridan Way, between Erin Mills Parkway and Leanne Boulevard.

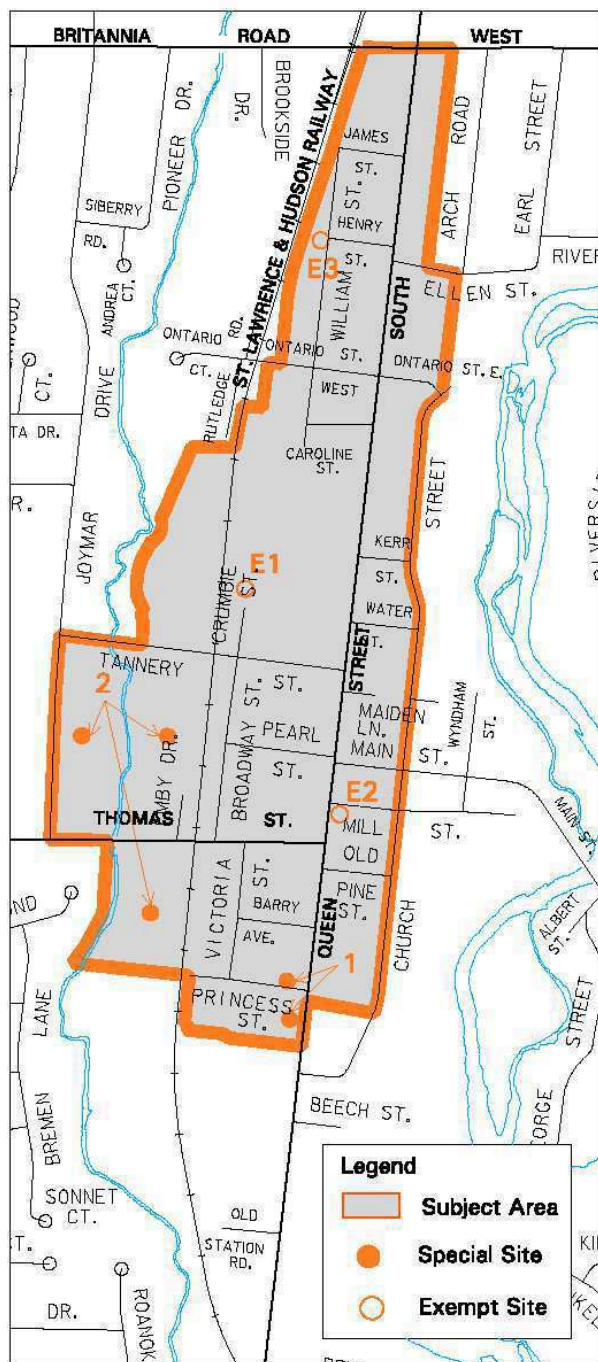
14.9.2.1.2 Notwithstanding the provisions of the Office designation, a long term health-care facility and overnight accommodations will also be permitted. Limited commercial uses may be considered.

14.10 South Common



South Common Community Node Character Area

14.11 Streetsville



Streetsville Community Node Character Area

14.11.1 Urban Design Policies

Community Identity and Focus

14.11.1.1 Development will be compatible with and enhance the village character of Streetsville as a distinct established community by integrating with the surrounding area.

14.11.1.2 Development with a high level of urban design, pedestrian amenity, landscaping and compact built form will be encouraged to create a strong sense of place and reinforce the role of the Community Node as the centre of activity for the surrounding community.

14.11.1.3 Queen Street South will remain the focus of the commercial core within the Streetsville Node.

14.11.1.4 The development of symbolic gateways to define entry to and exit from the Streetsville Node will be encouraged.

Historic Character

14.11.1.5 Alterations to heritage structures, including building repairs and additions, and/or alterations to existing vegetation which is part of the cultural heritage landscape, will be in keeping with the original character of the heritage resources to be preserved.

14.11.1.6 Designs for new buildings and additions will enhance the historic character and heritage context of the Streetsville Node through appropriate height, massing, architectural pattern, proportions, set back and general appearance.

14.11.1.7 Commercial signs will reflect the historic village character in the Streetsville Community Node. Hand painted wooden signs, projecting signs as permitted by municipal by-laws and other signs which reflect late nineteenth or early twentieth century practices will be encouraged.

14.11.1.8 The established residential character of the areas generally located along Queen Street South, south of Barry Avenue, will be maintained through appropriate building masses, setbacks,

intensive landscaping, **streetscapes** with many mature trees, and a regular street grid pattern.

14.11.1.9 The following will apply to lands designated Mixed Use:

- a. development with a mix of residential and office uses on upper floors and street-related commercial uses in closely spaced storefronts lining the street, will be encouraged along the principal streets in the Streetsville Community Node to promote an active pedestrian environment;
- b. new buildings will be at least two storeys but not more than three storeys in height. Building additions will not be more than three storeys in height and will be generally harmonious in style and massing with the buildings to which they are attached;
- c. the apparent height of new buildings will be reduced through massing and design;
- d. new development will be encouraged to reflect the original lot pattern and setback dimensions of surrounding properties. The front setback of new buildings should match the setback of adjacent buildings so as to create a uniform street wall. Where the building setbacks on either side are not equal, the lesser setback should be used;
- e. integrated parking opportunities, which may include rear lane access to combined parking areas and the development of private pay parking lots, will be encouraged where appropriate;
- f. development which enhances the **streetscape** through landscaping and the provision of pedestrian amenities will be encouraged along the principal streets within the Streetsville Community Node.

Public Realm

14.11.1.10 The placement of parking areas to the rear of buildings and the consolidation of driveways will be encouraged along principal street frontages

in the Streetsville Community Node to increase the area available for planting and public amenity.

14.11.1.11 The rear facades of developments which back onto places accessible to the public, including parking areas or laneways, should be designed to provide a level of detail, fenestration, and direct access appropriate to their role as secondary frontages.

14.11.2 Land Use

14.11.2.1 Notwithstanding the Residential Low Density I policies of this Plan, the Residential Low Density I designation permits only detached dwellings.

14.11.2.2 For lands designated Residential High Density a maximum building height of seven storeys is permitted.

14.11.2.3 In addition to the uses permitted by the Land Use Policies of this Plan, bed and breakfast establishments are permitted provided that they front upon Queen Street South, Main Street, Thomas Street or Church Street.

14.11.3 Heritage

14.11.3.1 Heritage resources will include those properties listed on the City's Heritage Register, but will not be restricted to the list.

14.11.3.2 Mississauga will encourage the investigation of the Streetsville historic core area as an area to be examined for future designation as a Heritage Conservation District in accordance with the Ontario Heritage Act.

14.11.4 Stormwater Management

14.11.4.1 A stormwater drainage report will be required to ensure that the existing drainage system has the capability to convey the increase storm flow from redevelopment in accordance with current City drainage standards.

14.11.5 Transportation

14.11.5.1 The maximum pavement width of Queen Street South, from Britannia Road West to the St. Lawrence and Hudson Railway, will not exceed a basic two lane cross-section, except for bus bays, on street parking lanes, turning lanes at intersections and bicycle and pedestrian routes.

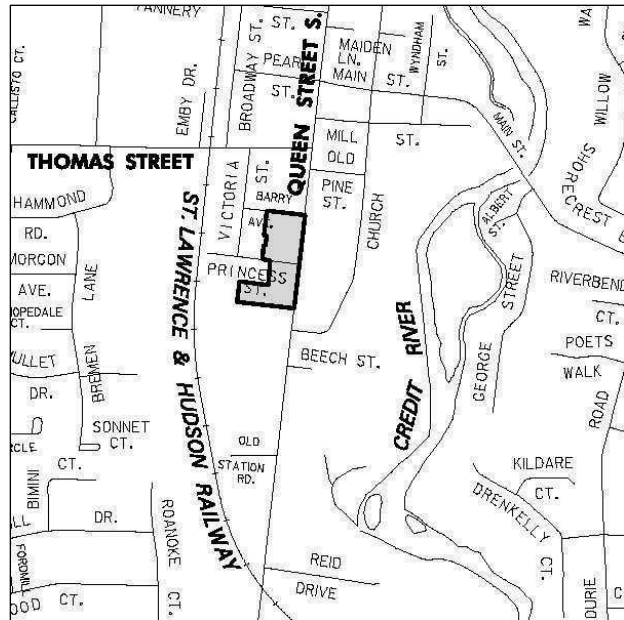
14.11.5.2 Main Street between Queen Street South and Church Street will not be widened to accommodate additional through traffic lanes. Minor modifications to this roadway for on-street parking, safety or maintenance will be permitted, however, major modifications which would have an adverse effect on the character of the roadway, will require an amendment to this Plan.

14.11.5.3 The City will continue to encourage and promote better utilization of the existing and future parking inventory through discussions with the members of the Streetsville Business Improvement Area (BIA), with a goal of developing a self sufficient parking operation through measures such as parking charges.

14.11.6 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

14.11.6.1 Site 1



14.11.6.1.1 In addition to the uses permitted by their Residential Low Density I and II designations, the lands identified as Special Site 1 which front on Queen Street South, may also be used for offices.

14.11.6.1.2 Any office conversion should maintain a residential appearance in keeping with the existing scale, materials, and character of the existing dwellings in the immediate area. The existing houses should be preserved if at all possible, while the interior floor plan may be altered for office use.

14.11.6.1.3 Any additions and alterations of existing buildings should be sensitive to the existing vernacular and heritage village theme, and should be largely confined to the rear of the property.

14.11.6.1.4 Any additions, alteration, conversion or redevelopment should maintain the existing front yard setback, with the front yard used for landscaping.

14.11.6.1.5 Sufficient on-site parking, which will consist of only surface parking, as required by the Zoning By-law, should be provided in the rear yard only at grade without removal of existing trees, except at the discretion of the City arborist.

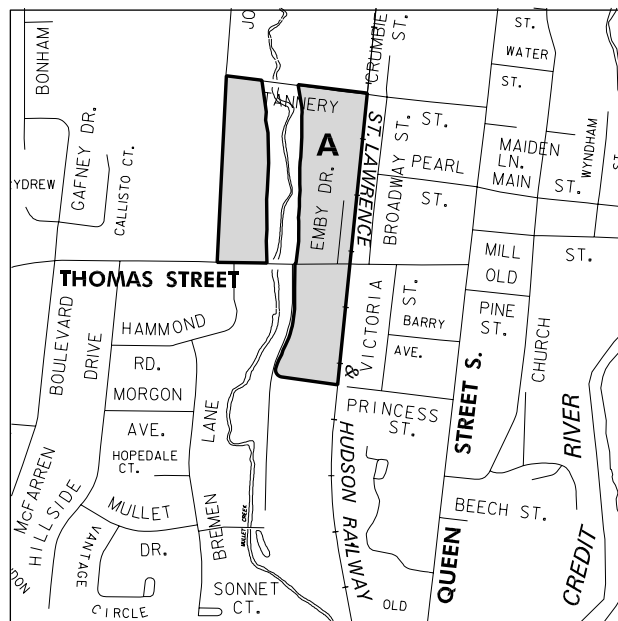
14.11.6.1.6 Vehicular entrances should be combined to minimize the number of access points on Queen Street South.

14.11.6.1.7 Minimal signage will be permitted. The design of such signage and external lighting should be compatible with the residential character of the area.

14.11.6.1.8 Existing lot sizes should be retained.

14.11.6.1.9 Rear yard drainage will be provided to the satisfaction of the City.

14.11.6.2 Site 2



14.11.6.2.1 The lands identified as Special Site 2 are located south of Tannery Street, north of Old Station Road, east of Joymar Drive and west of the St. Lawrence and Hudson Railway.

14.11.6.2.2 Notwithstanding the Residential High Density, Residential Medium Density and Greenbelt

designations the following additional policies will apply:

- the determination of the area suitable for redevelopment will have regard for the extent of the "regulatory storm" floodplain and the erosion hazards associated with Mullet Creek, whichever is greater. The extent of areas required for conservation purposes will be determined to the satisfaction of Credit Valley Conservation and the City;
- building forms should consist of low profile buildings ranging in height from three storeys near Mullet Creek to six storeys near the railway tracks;
- redevelopment will include provisions for a community park and a multi-use recreational trail. The trail will be located adjacent to the east side of the Mullet Creek valley, from Tannery Street to the existing trail crossing Mullet Creek, south of Thomas Street. The location and size of the community park will be determined through the preparation of a parkland concept plan.

14.11.6.2.3 Redevelopment of Area A should include provision for a public road connecting Thomas Street and Tannery Street west of the St. Lawrence and Hudson Railway right-of-way.

COMMITTEE OF ADJUSTMENT

IN THE MATTER OF SECTION 45(1) OR (2)
of The Planning Act R.S.O. 1990, c.P.13, as amended

- and -

IN THE MATTER OF ZONING BY-LAW 0225-2007
as amended

- and -

IN THE MATTER OF AN APPLICATION BY

2112836 ONTARIO INC

on Thursday December 11, 2014

2112836 Ontario Inc is the owner of part of lot 112 & 113, Registered Plan STR-2, located and known as 49 Queen Street South & 5 Ellen Street, zoned C4 - Commercial. The applicant requests the Committee to authorize a minor variance to permit development of the subject property as a mixed use development proposing:

1. a total of eight (8) tandem parking spaces; whereas By-law 0225-2007, as amended, does not permit tandem parking spaces in this instance;
2. to permit the eight (8) tandem parking spaces to be reserved for specific users and to calculate the required parking using the mixed use development shared parking formula as identified in Article 3.1.2.3 of the Zoning By-law; whereas By-law 0225-2007, as amended, requires that all required parking spaces must be accessible to all users participating in the shared parking arrangement and may not be reserved for specific users;
3. a minimum on-site aisle width of 2.40 m (7.87 ft.) and off-site aisle width of 4.10 m (13.45 ft.) for eight (8) parking spaces; whereas By-law 0225-2007, as amended, requires a minimum on-site aisle width of 7.00 m (22.97 ft.) in this instance; and,
4. a minimum aisle width of 6.50 m (21.33 ft.) for the remaining parking spaces; whereas By-law 0225-2007, as amended, requires a minimum aisle width of 7.00 m (22.97 ft.) in this instance.

Mr. R. Kustou, a representative of the authorized agent, attended and presented the application to permit the development of the property to contain two mixed use buildings together with parking at the interior of the site. Mr. Kustou advised the Committee that the subject property was uniquely shaped with frontages onto both Queen Street South and Ellen Street. He noted that parking would be provided at the interior of the site and access from Ellen Street. Mr. Kustou explained that the shape of the property and the parking requirement of the Zoning By-law required for a unique approach for the provision of parking spaces. He explained that a number of tandem parking spaces would be provided and noted that these spaces would be used for the business owners of the site. He suggested that this was an appropriate arrangement as it would allow the remaining spaces to be provided for visitors. Mr. Kustou noted that some of the drive aisles would be deficient in with as a result of the narrow interior of the property. He confirmed that the aisles would remain functional and that emergency vehicles would be able to access the interior of the site.

The Committee reviewed the information and plans submitted with the application.



MISSISSAUGA

File: "A" 447/14
WARD 11

The City of Mississauga Planning and Building Department commented as follows (December 5, 2014):

1.0 RECOMMENDATION

The Planning and Building has no objection to the requested variances, subject to the condition outlined below.

2.0 BACKGROUND

Mississauga Official Plan

Character Area: Streetsville Community Node
Designation: Mixed Use

Zoning By-law 0225-2007

Zoning: "C4", Mainstreet Commercial

3.0 OTHER APPLICATIONS

☒ *Site Plan* File: SP 11-129 W11 - Satisfactory

4.0 COMMENTS

Based on a review of the Site Plan application, the variances as requested are correct.

We note that the subject property is uniquely shaped. The provision of parking and aisle widths has been sufficiently evaluated through the Site Plan process, and we advise that we are satisfied with the parking layout. Therefore, we have no objection to the requested variances. However, we recommend that the following condition be implemented:

1) eight (8) tandem parking spaces be signed and used for the on-site employees."

The City of Mississauga Transportation and Works Department commented as follows (December 3, 2014):

"We note for Committee's information that the City is currently processing a Site Plan Application for this property, Reference SP 11/129. Transportation and Works Department concerns/requirements for this property will be addressed through the Site Plan Process."

The Region of Peel, Environment, Transportation and Planning Services, commented as follows (December 5, 2014):

"This property is within the vicinity of the Streetsville Landfill Site, previously used for the disposal of waste and catalogued by the M.O.E as #7074. Closed in 1957, there is no evidence to suggest the presence of methane gas or leachate.

This property is also within the vicinity of DH1, an inactive landfill located south of Britannia, west of Queen, and Canada Brick, an inactive landfill located north of Britannia, between Erin Mills and Queen St. The exact boundaries are unknown."

No other persons expressed any interest in the application.

The Committee after considering the submissions put forward by Mr. Kustou and having reviewed the plans and comments received, is satisfied that the request is desirable for the appropriate further development of the subject property.

The Committee is satisfied that the general intent and purpose of the Zoning By-law and the Official Plan will be maintained in this instance.

The Committee is of the opinion that the requested variance is minor in nature in this instance.

Accordingly, the Committee resolves to authorize and grant the request as presented subject to the following condition:

1. The eight (8) tandem parking spaces be signed and used for the on-site employees only.

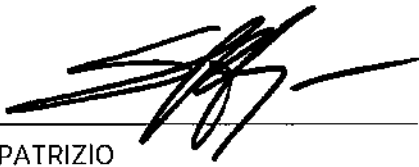
MOVED BY: J. Robinson SECONDED BY: R. Bennett CARRIED

Application Approved on condition as stated.

Dated at the City of Mississauga on December 18, 2014.


THIS DECISION IS SUBJECT TO APPEAL TO THE ONTARIO MUNICIPAL BOARD BY FILING WITH THE SECRETARY-TREASURER OF THE COMMITTEE OF ADJUSTMENT A WRITTEN NOTIFICATION, GIVING REASONS FOR THE APPEAL, ACCOMPANIED WITH THE PRESCRIBED FEE ON OR BEFORE JANUARY 7, 2015.

Date of mailing is December 22, 2014.


S. PATRIZIO


D. GEORGE (CHAIR)


R. BENNETT

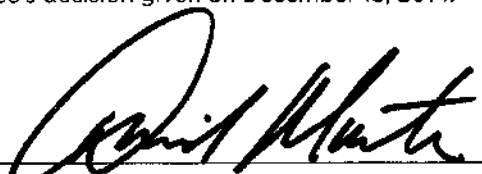

J. THOMAS


D. KENNEDY

ABSENT
L. DAHONICK


J. ROBINSON

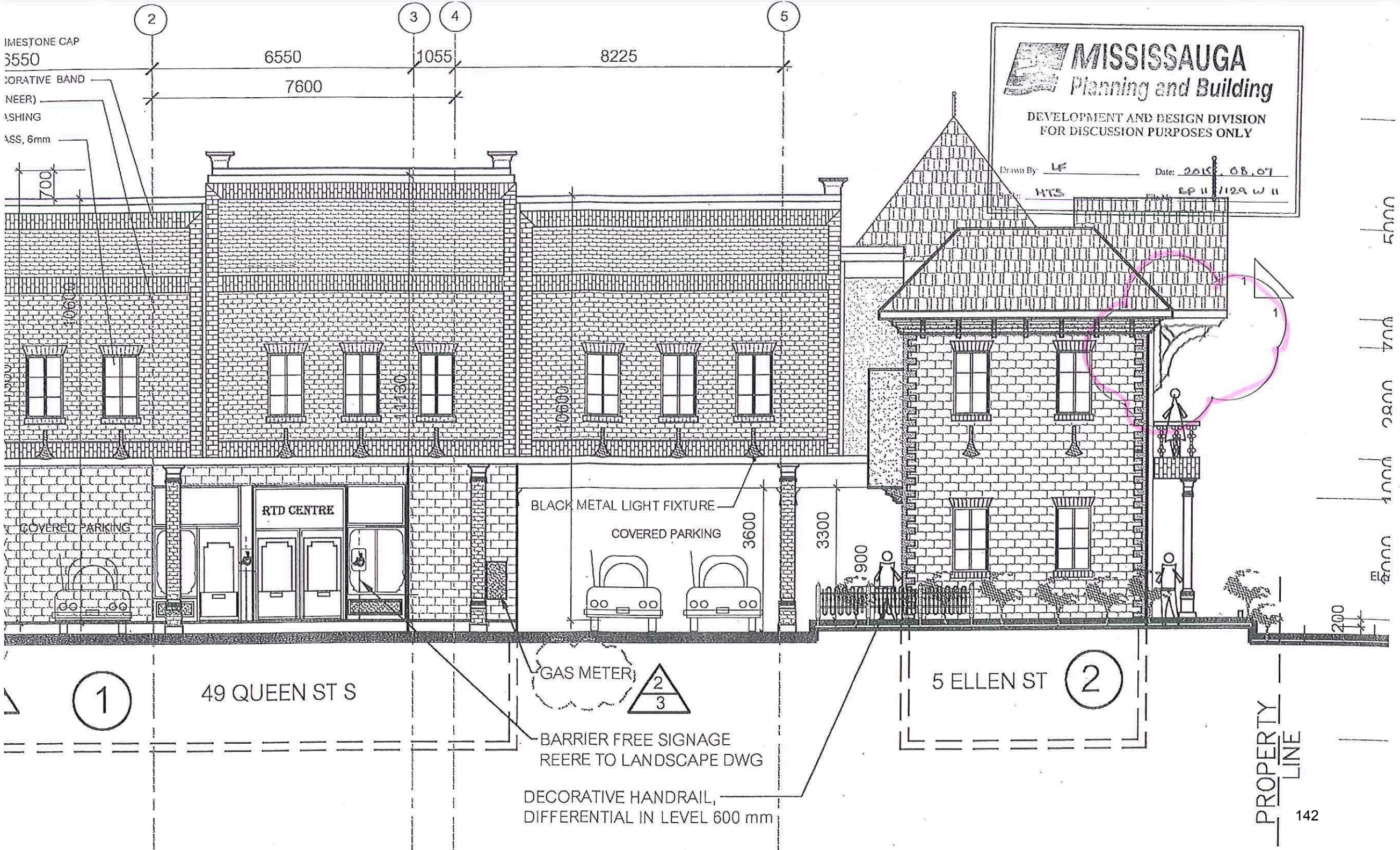
I certify this to be a true copy of the Committee's decision given on December 18, 2014.


DAVID L. MARTIN, SECRETARY-TREASURER

A copy of Section 45 of the Planning Act, as amended, is attached.

NOTES:

- A Development Charge may be payable prior to the issuance of a Building Permit.
- Further approvals from the City of Mississauga may be required i.e. a Building Permit, a Zoning Certificate, a License, etc.



IMESTONE CAP
3550
DECORATIVE BAND
(NEER)
FLASHING
ASS, 6mm

6550

7600

1055

8225

700

1130

6600

3600

3300

900

200

RTD CENTRE

BLACK METAL LIGHT FIXTURE

COVERED PARKING

GAS METER

BARRIER FREE SIGNAGE
REERE TO LANDSCAPE DWG

DECORATIVE HANDRAIL,
DIFFERENTIAL IN LEVEL 600 mm

49 QUEEN ST S

5 ELLEN ST

PROPERTY
LINE

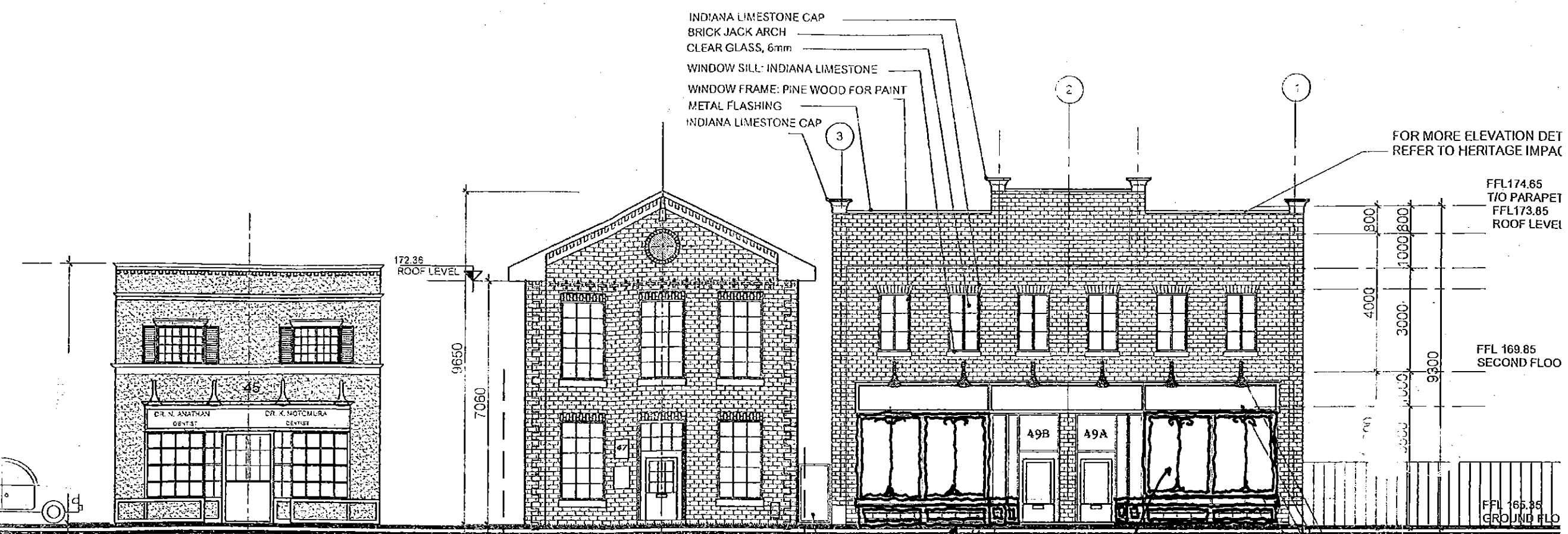


MISSISSAUGA

Planning and Building

DEVELOPMENT AND DESIGN DIVISION
FOR DISCUSSION PURPOSES ONLY

Drawn By: LE Date: 2015.08.07
Scale: HTS Plot # SP 11/1129 W 11



EXIST DENTAL CLINIC
BLDG # 45

(ORANGE HALL)
EXIST HERITAGE
OFFICE BLDG # 47

PROPOSED MIXED USE
RETAIL/OFFICES/LIVE-WORK UNIT # 49

PROPOSED
& RESIDENTIAL



MISSISSAUGA
Planning and Building

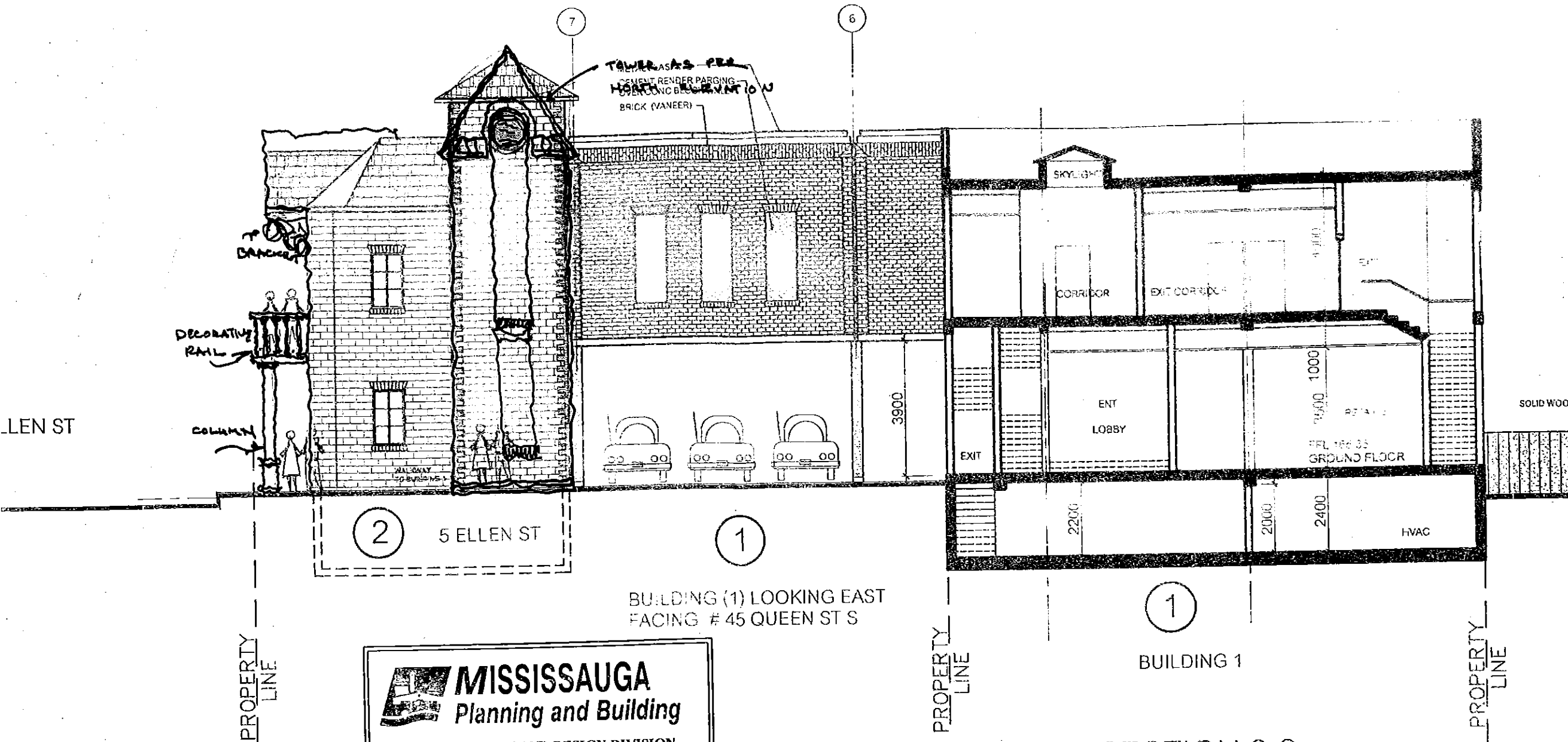
DEVELOPMENT AND DESIGN DIVISION
FOR DISCUSSION PURPOSES ONLY


Drawn By: LF Date: 2015.10.28

Scale: NTS File No: SP 11/129 W11

STREETSCAPE

FRONT - WEST - ELEVATION
QUEEN STREET SOUTH





MISSISSAUGA

Planning and Building

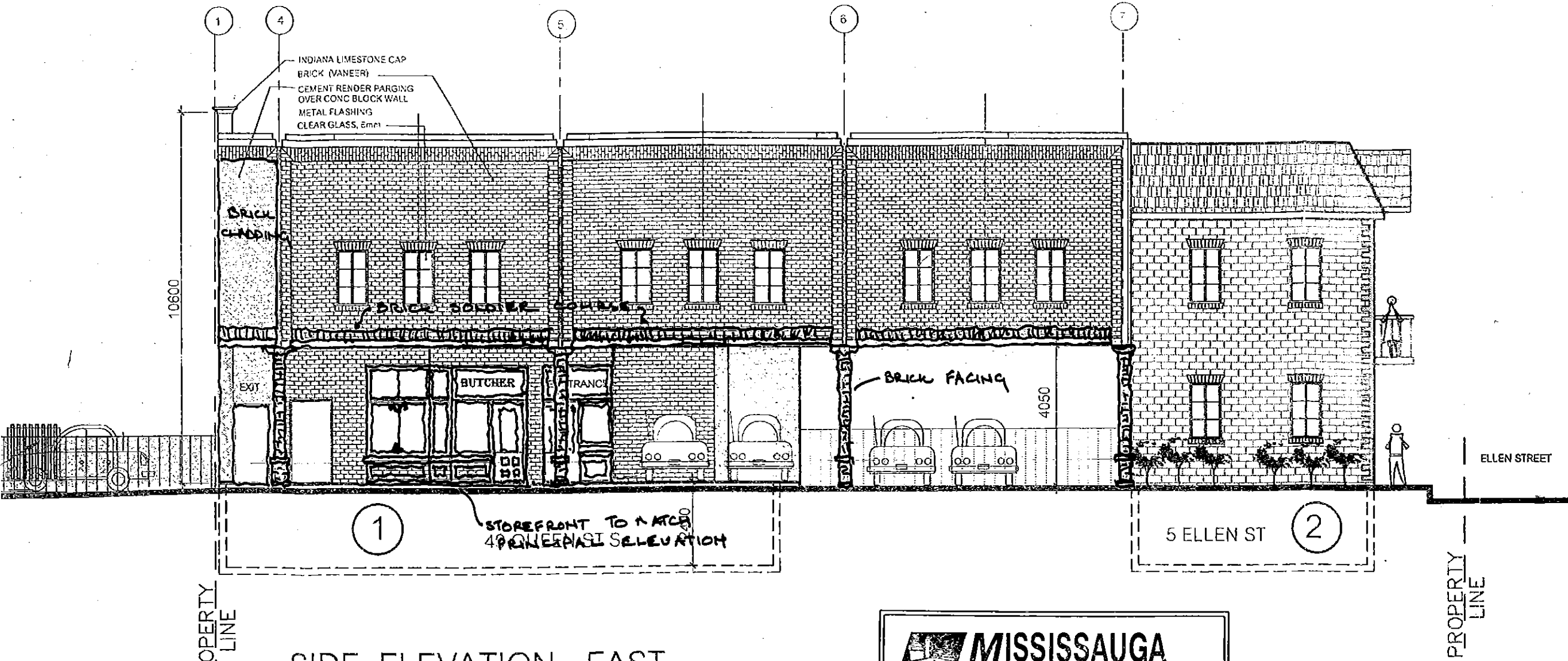
DEVELOPMENT AND DESIGN DIVISION
FOR DISCUSSION PURPOSES ONLY

Drawn By: LF

Scale: NTS

Date: 2015. 10. 28

File No: SP 11/129 W11



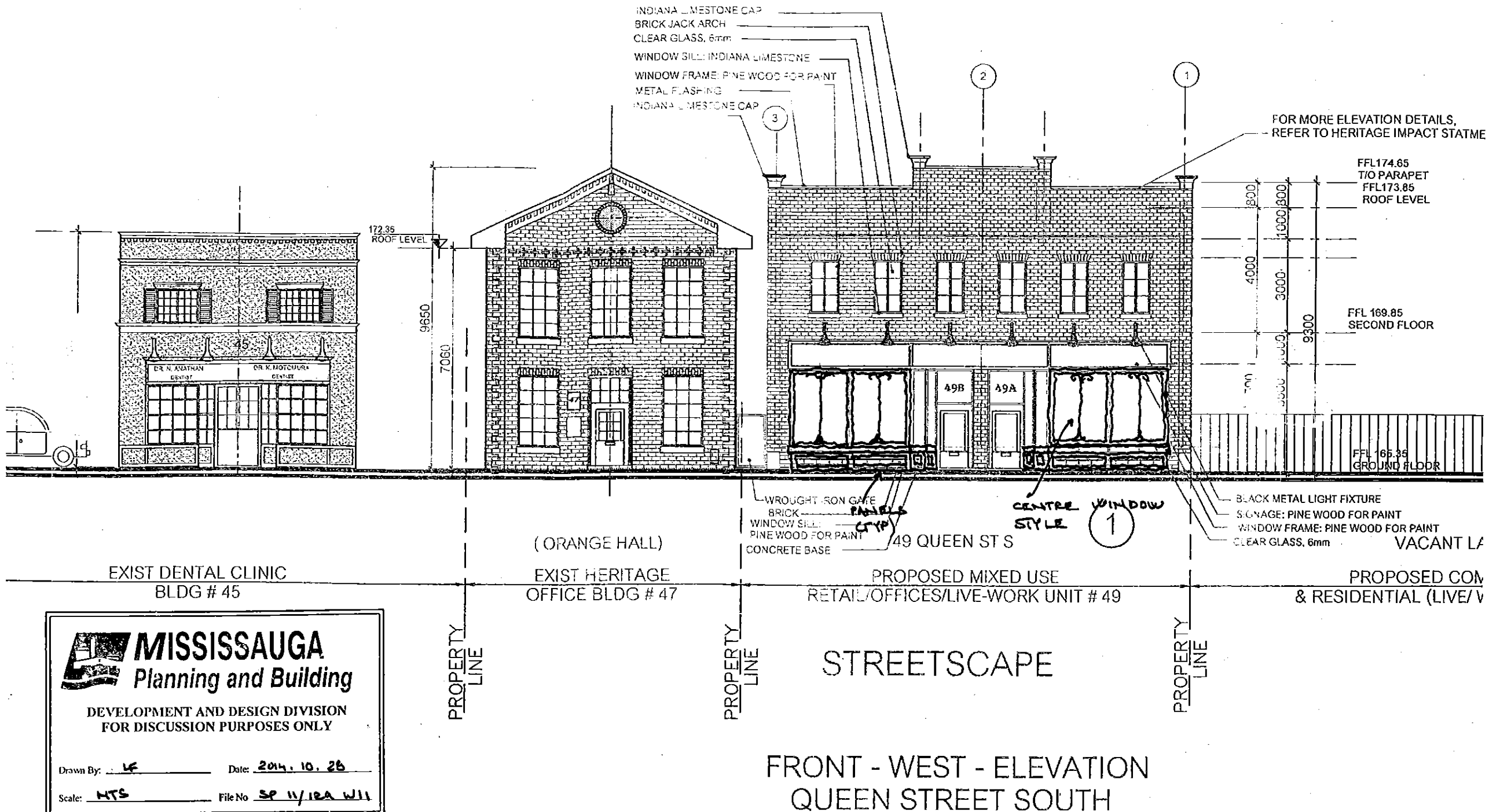
SIDE ELEVATION - EAST
INSIDE PARKING LOT

MISSISSAUGA
Planning and Building

DEVELOPMENT AND DESIGN DIVISION
FOR DISCUSSION PURPOSES ONLY

Drawn By: LF Date: 2014.10.28

Scale: NTS File No: SP 11/129 W11



NEW LAND SURVEY

BIN ABSTRACT NO
(MERGE TO ONE LAND)

(RIGHT OF WAY AGREEMENT)
WALKWAY BETWEEN
47 AND 49 QUEEN ST S

5 ELLEN ST

COLORED FRONT ELEVATION

3 DIMENSIONAL PRESENTATION RENDERING

SAMPLES OF EXISTING BUILDINGS
52 & 62 QUEEN ST S, STREETSVILLE
ORANGE HALL (47 QUEEN ST S)

PART 2:
LOCATION OF BUILDING No. 49 ENCLOSED ON THE
SOUTHEAST LIMIT OF PART 1 PLAN 43R-6715, AS
SHOWN IN PART 1. THE EAVES ON BUILDING No. 49
AND THE EAVES ON BUILDING No. 47 ENCLOSED ON
THE NORTHWEST LIMIT OF PART 1 PLAN 43R-6715, AS
SHOWN ON PART 1.
NO EASEMENTS ARE RECITED IN THE PARCEL REGISTER.
RIGHT-OF-WAY AS SHOWN ON PART 1.
THIS SURVEY IS MADE FOR ORION PACIFIC ENGINEERING
INC.

BEARING NOTE:
BEARINGS SHOWN HEREON ARE ASTROMONIC
AND ARE DERIVED FROM THE NORTHEAST LIMIT
OF QUEEN STREET AS SHOWN ON PLAN
43R-6715 HAVING A BEARING OF N 42° 20' W.

BENCHMARK NOTE:
ELEVATIONS SHOWN HEREON ARE DERIVED
FROM CITY OF MISSISSAUGA BENCHMARK
No. 773 HAVING A PUBLISHED ELEVATION
OF 163.653 METRES.

SCALE - 1:250

0 2 4 6 8 10 20

H. PILLER CORPORATION LTD.

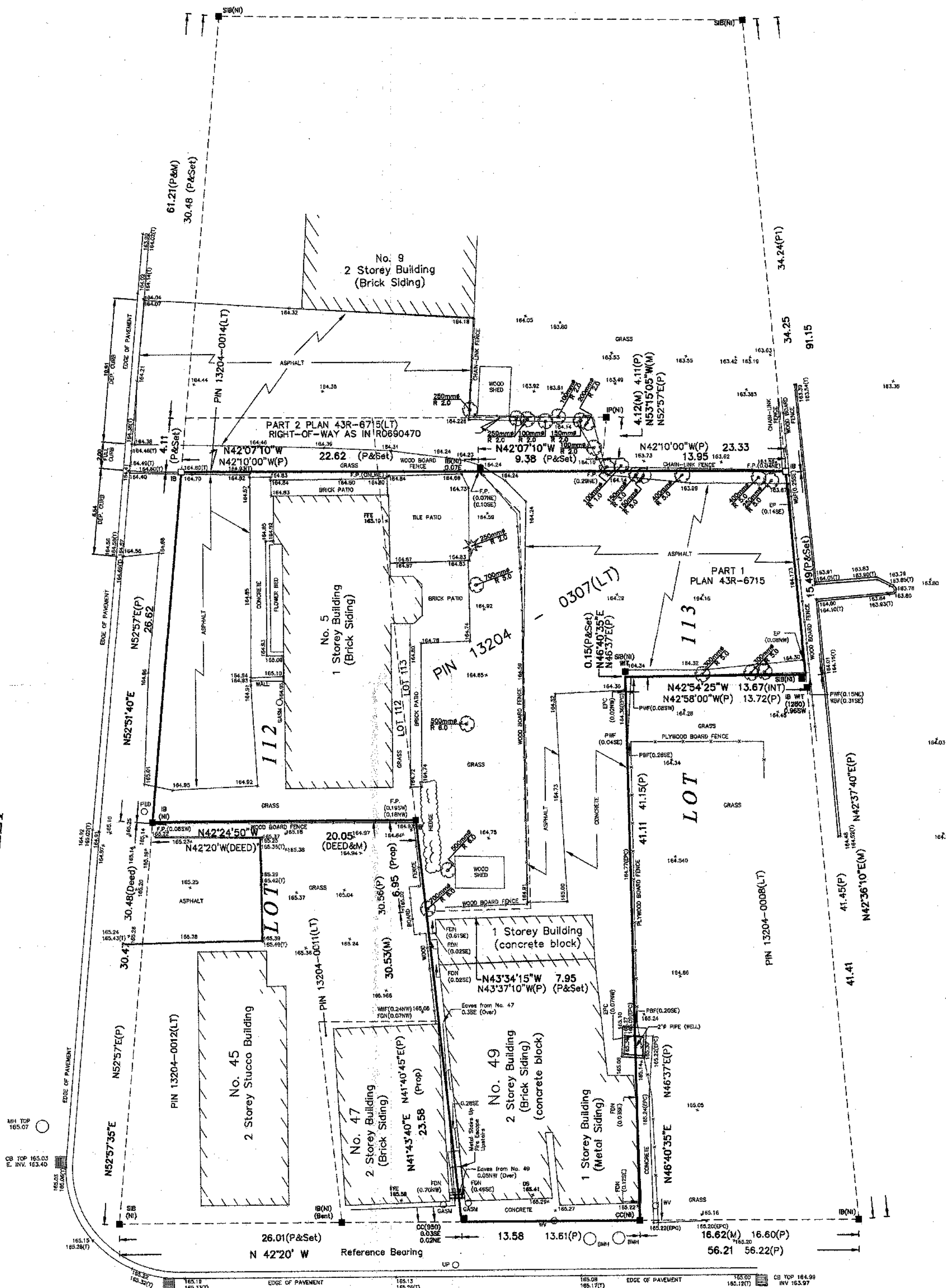
METRIC:
DISTANCES SHOWN ON THIS PLAN ARE IN METRES AND
CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

SURVEYORS REAL PROPERTY REPORT

PART 1:

PART OF LOTS 112 & 113
ACCORDING TO HYDE & RUTLEDGE'S PLAN
OF THE VILLAGE OF STREETSVILLE
REFERRED TO AS PLAN No. STR-2
CITY OF MISSISSAUGA
REGIONAL MUNICIPALITY OF PEEL

ELLEN STREET



QUEEN STREET

(FORMERLY KNOWN AS KING STREET ON REGISTERED PLAN STR-2)
(ROAD ALLOWANCE BETWEEN CONCESSIONS 4&5, W.H.S.)

PIN 13204-0001(LT)

LEGEND:

■	DENOTES MONUMENT FOUND	FFE	DENOTES FINISHED FLOOR ELEVATION
SIB	DENOTES STANDARD IRON BAR	DS	DENOTES DOOR SILL
IB	DENOTES IRON BAR	CB	DENOTES CATCH BASIN
IP	DENOTES IRON PIPE	MH	DENOTES MAN HOLE
CC	DENOTES CUT CROSS	BMH	DENOTES BELL MAN HOLE
OP	DENOTES CONCRETE PIN	GASM	DENOTES GAS METER
(P)	DENOTES PLAN 43R-6715	WV	DENOTES WATER VALVE
(P1)	DENOTES SURVEY PLAN BY TARASICK, McMillan LTD. (FEB. 25TH, 1987)	UP	DENOTES UTILITY POLE
(M)	DENOTES MEASURED	PED	DENOTES BELL PEDESTAL
(INT)	DENOTES INTERSECTION	F.P.	DENOTES FENCE POST
(PROP)	DENOTES PROPORTION	PBF	DENOTES PLYWOOD BOARD FENCE
WT	DENOTES WITNESS	PWF	DENOTES POST WIRE FENCE
(NI)	DENOTES NOT IDENTIFIED	WBF	DENOTES WOOD BOARD FENCE
(1280)	DENOTES ANTON KIKAS	FDN	DENOTES BUILDING FOUNDATION
W.H.S.	DENOTES WEST OF HURONTARIO STREET	(T)	DENOTES TOP OF CURB
N.E.W.S	DENOTES NORTH, EAST, WEST & SOUTH RESPECTIVELY	EP	DENOTES EDGE OF PAVEMENT
		EPC	DENOTES EDGE OF CONCRETE PAVING
		○	DENOTES DECIDUOUS TREE
		✱	DENOTES CONIFEROUS TREE
		⊙	DENOTES TRUNK DIAMETER AND SPREAD RADIUS

SURVEYOR'S CERTIFICATE:

- I CERTIFY THAT:
1. This survey and plan are correct and in accordance with the Surveys Act and the Surveyors Act and the regulations made under them;
 2. The survey was completed on the 20th day of December, 2010.
 3. Additional fieldwork and updated plan as of March 10th, 2014

NOVEMBER 28, 2014

DATE

HELMUT PILLER

Ontario Land Surveyor

REF. No. 10-21-3220-02

H. Piller Corporation Ltd.

TORONTO

416 - 488 - 1174

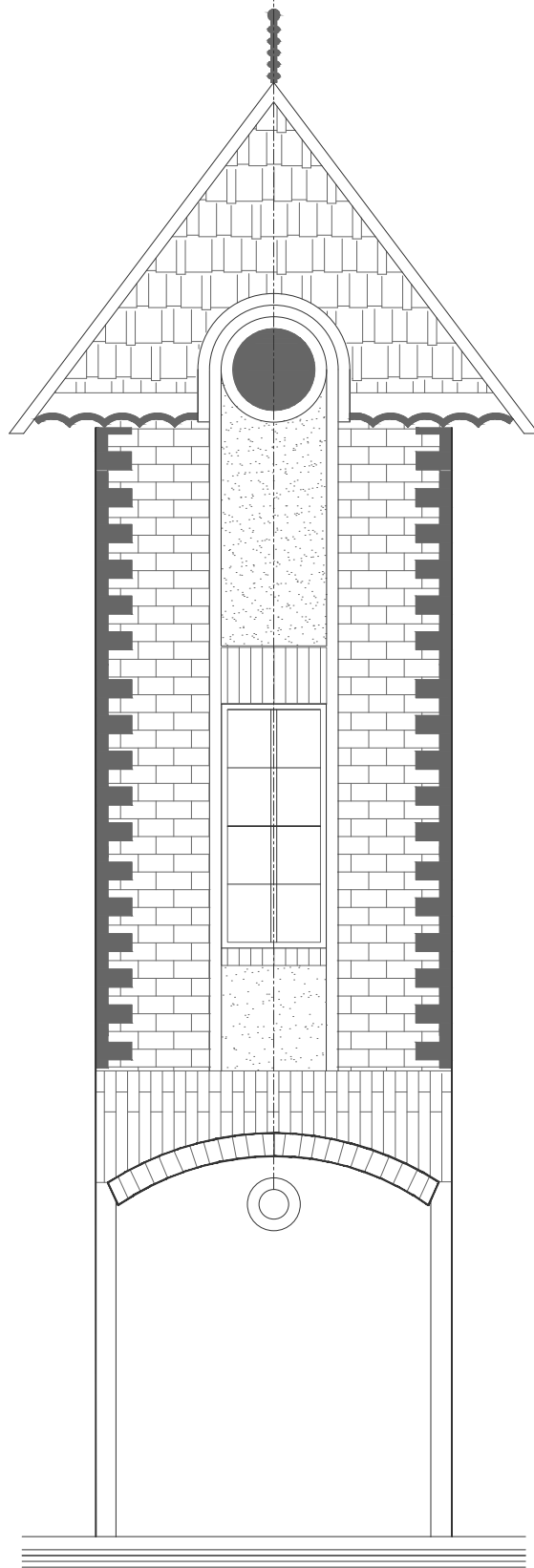
CANADA

DRAWN BY:
F.C.

CHECKED BY:
C.T.

DATE:
NOV. 27, 2014

REFERENCE No.:



RTD CENTRE

5 ELLEN ST, STREETSVILLE

RALLY ARCHITECTS









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09.06.2013 09:46

155

30.11.2014 15:29



City of Mississauga Corporate Report



Date: December 17, 2015 To: Chair and Members of Heritage Advisory Committee From: Paul Mitcham, P. Eng, MBA, Commissioner of Community Services	Originator's files:
	Meeting date: 2016/01/12

Subject

Heritage Advisory Committee and Related Staff Milestones: 2015 Year in Review

Recommendation

That the Corporate Report dated December 17, 2015 from the Commissioner of Community Services, entitled "Heritage Advisory Committee and Related Staff Milestones: 2015 Year in Review," be received for information.

Background

Council established the Heritage Advisory Committee (HAC), then known as the Local Architectural Conservation Advisory Committee, in 1976. The Heritage Advisory Committee advises Council on matters relating to the identification, conservation and preservation of Mississauga's cultural heritage property.

Comments

Over the last year, HAC has achieved the following, which the City recognizes and appreciates of this volunteer committee:

Heritage Permit Application Review

A total of 37 heritage permit applications were processed in 2015. 18 of these were redevelopment applications in the City's cultural landscapes.

In addition, Heritage staff analyze, evaluate and comment on every Official Plan Amendment, Rezoning and Plan of Subdivision application, as well as Site Plan and Committee of Adjustment applications that pertain to heritage properties.

Heritage Grant Program

Sixteen grants were approved in 2015; the recipients are as follows:

1011 Old Derry Road	\$3,493.00
1074 Old Derry Road	\$3,600.00
1155 Willow Lane	\$7,200.00
1352 Lakeshore Road East	\$1,374.00
141 Lakeshore Road East	\$3,600.00
15 Mississauga Road South	\$813.00
264 Queen Street South	\$3,560.00
27 Mill Street	\$1,080.00
32 Front Street South	\$7,110.00
34 Thomas Street	\$2,268.00
41 Bay Street	\$3,600.00
42 John Street South	\$3,600.00
7005 Pond Street	\$7,200.00
7059 Second Line West	\$3,600.00
7076 Old Mill Lane	\$7,200.00
913 Sangster Avenue	\$4,841.00
	\$64,139.00

The remaining \$10,861.00 will be transferred to the Arts Reserve and will be used towards the maintenance of heritage properties.

Designations under the Ontario Heritage Act:

The following properties were designated under the *Ontario Heritage Act*:

- 307 Queen Street South
- 915 North Service Road
- 41 Mill Street (Amendment)

Conservation Review Board

The following property was the subject of a Conservation Review Board hearing:

- 2625 Hammond Road

Heritage Easement Agreement

Staff drafted a heritage easement agreement for the following property:

- 41 Mill Street

Additional Notable 2015 Staff and Committee Initiatives:

- “Intro to Property Research” presentation, Heritage Day at the Grange, Heritage Mississauga
- Referral of City Plaque Process to City Manager's Office
- Joint Peel Heritage Advisory Committees Meeting
- Heritage Priority Setting Meeting
- Meadowvale Village Historic Art Walking Tour (includes brochure update)
- Doors Open Display
- Museums & Heritage Planning Strategic Plan

Heritage Planning staff core workload also includes:

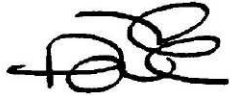
- Responding to numerous inquiries re: the 3500+ properties included on the City's Heritage Register
- Provide clearance to building permit office on heritage property permit proposals that do not require a heritage permit
- Contribute to City planning policies, visioning studies, strategic and master plans
- Serve as team member on multiple City planning projects with a potential heritage component; includes park/City asset projects and environmental assessments and more
- Work with by-law enforcement and prosecutions staff, if necessary, on contraventions related to heritage properties
- Advance preparation and/or managing recommendations/decisions related to Conservation Review Board and Ontario Municipal Board hearings

Financial Impact

Not applicable

Conclusion

In 2015, the Heritage Advisory Committee advised Council on numerous heritage conservation initiatives. Heritage Planning staff recommend that the Corporate Report entitled "Heritage Advisory Committee and elated Staff Milestones: 2015 Year in Review," be received for information.



Paul Mitcham, P. Eng, MBA, Commissioner of Community Services

Prepared by: P. Wubbenhorst, Senior Heritage Coordinator

City of Mississauga
Minutes (DRAFT)



Meadowvale Heritage Conservation District Advisory Sub-Committee

Date

January 5, 2016

Time

1:30 p.m.

Location

Meadowvale Village Hall, 6970 Second Line West, Mississauga

Members

Jim Holmes, Citizen Member
Brian Carmody, Citizen Member
Gord MacKinnon, Citizen Member
John McAskin, Citizen Member
David Moir, Citizen Member
Colleen Newmarch, Citizen Member
Terry Wilson, Citizen Member
Greg Young, Citizen Member

In Attendance

George Carlson, Councillor Ward 11 – *ex-officio*

Members Absent

Janet Clewes, Citizen Member
David Dodaro, HAC Representative
Paul McGuigan, HAC Representative

Staff Present

Paula Wubbenhorst, Senior Heritage Coordinator, Culture Division
Cecilia Nin Hernandez, Heritage Coordinator, Culture Division
Mumtaz Alikhan, Legislative Coordinator, Legislative Services Division

Find it online

<http://www.mississauga.ca/portal/cityhall/heritageadvisory>

CALL TO ORDER – 1:30pm

Mumtaz Alikhan, Legislative Coordinator, Office of the City Clerk, called the meeting to order.

1. Appointment of Chair

Ms. Alikhan called for nominations for the position of Chair of the Meadowvale Heritage Conservation District Advisory Sub-Committee (Sub-Committee) for the period ending November 2018.

T. Wilson nominated Jim Holmes for the role of Chair, seconded by Brian Carmody.

RECOMMENDATION

0001/2016

That Jim Holmes be appointed Chair of the Meadowvale Heritage Conservation District Advisory Sub-Committee for the period ending November 2018, or until a successor is appointed.

Approved (T. Wilson)

2. Appointment of Vice-Chair

Ms. Alikhan called for nominations for the position of Vice-Chair of the Meadowvale Heritage Conservation District Advisory Sub-Committee (Sub-Committee) for the period ending November 2018.

B. Carmody nominated T. Wilson for the role of Vice-Chair of the Sub-Committee, seconded by Colleen Newmarch.

RECOMMENDATION

0002/2016

That Terry Wilson be appointed Vice Chair of the Meadowvale Heritage Conservation District Advisory Sub-Committee for the period ending November 2018, or until a successor is appointed.

Approved (B. Carmody)

APPROVAL OF AGENDA

Approved (C. Newmarch)

DECLARATIONS OF CONFLICT OF INTEREST - Nil

APPROVAL OF PREVIOUS MINUTES - Nil

DEPUTATIONS - Nil

MATTERS TO BE CONSIDERED

1. Approval of Terms of Reference

RECOMMENDATION

0003/2016

That the Meadowvale Heritage Conservation District Advisory Sub-Committee Terms of Reference be approved as presented.

Approved (G. Young)

2. Revised Request to Alter 1059 Old Derry Road

Memorandum dated December 16, 2015 from Paula Wubbenhorst, Senior Heritage Coordinator, Culture Division.

Ms. Wubbenhorst, Senior Heritage Coordinator, and Mr. Lew D'Souza, Owner, reviewed the revised request to alter the property located at 1059 Old Derry Road. It was noted that there would be a partial demolition but the exterior elevations will be identical to those presented to the Heritage Advisory Committee in 2015.

RECOMMENDATION

0004/2016

That the Revised Request to Alter 1059 Old Derry Road, as presented in the Memorandum dated December 16, 2015 from Paula Wubbenhorst, Senior Heritage Coordinator, Culture Division, be approved.

Approved (G. MacKinnon)

OTHER BUSINESS

1. 7025 Pond St. - Revised Garage Construction and Relocation

R. Mateljan, Strickland Mateljan Design Associates, reviewed the need for the relocation of the garage on the property located at 7025 Pond Street. The Sub-Committee did not object to the proposed relocation.

RECOMMENDATION

0005/2016

That the Meadowvale Village Heritage Conservation District Advisory Sub-Committee had no objection to the proposed garage relocation at 7025 Pond Street and that this matter be referred to staff for review and be reported directly to the Heritage Advisory Committee.

Approved (C. Newmarch)

DATE OF NEXT MEETING - Tuesday, January 26, 2016 at 1:30 a.m., Meadowvale Village Hall, 6970 Second Line West if required.

ADJOURNMENT – 2:05 p.m.