

AGENDA



HERITAGE ADVISORY COMMITTEE

THE CORPORATION OF THE CITY OF MISSISSAUGA

TUESDAY, MAY 20, 2014 – 9:30 A.M.

COUNCIL CHAMBER

SECOND FLOOR, CIVIC CENTRE

300 CITY CENTRE DRIVE, MISSISSAUGA, ONTARIO, L5B 3C1

www.mississauga.ca

Members

Councillor George Carlson, Ward 11 (CHAIR)

Councillor Jim Tovey, Ward 1 (VICE-CHAIR)

Robert Cutmore, Citizen Member

David Dodaro, Citizen Member

Mohammad N. Haque, Citizen Member

James Holmes, Citizen Member

Rick Mateljan, Citizen Member

Cameron McCuaig, Citizen Member

Michael Spaziani, Citizen Member

Michelle Walmsley, Citizen Member

Matthew N. Wilkinson, Citizen Member

NOTE: Heritage Advisory Committee Members are encouraged to visit the properties listed on agendas prior to Committee meetings in order to gain information and context.

CONTACT PERSON: Mumtaz Alikhan, Legislative Coordinator, Office of the City Clerk

Telephone Number: 905-615-3200, ext. 5425; Fax Number: 905-615-4181

Email Address: mumtaz.alikhan@mississauga.ca

CALL TO ORDERAPPROVAL OF AGENDADECLARATIONS OF CONFLICT OF INTERESTDEPUTATIONSMATTERS TO BE CONSIDERED

1. Approval of Minutes of Previous Meeting held on April 22, 2014
2. Demolition of a Designated Part V Structure – 1050 Old Derry Road, Meadowvale Village Heritage Conservation District (Ward 11)
Corporate Report dated April 28, 2014 from the Commissioner of Community Services with respect to the demolition of a Designated Part V Structure – 1050 Old Derry Road in the Meadowvale Village Heritage Conservation District (Ward 11).

RECOMMENDATION FOR APPROVAL

That the property owner's request to demolish the structures on the property located at 1050 Old Derry Road, which is Designated under Part V of the Ontario Heritage Act as part of the Meadowvale Village Heritage Conservation District be denied, as described in the Corporate Report dated April 28, 2014, from the Commissioner of Community Services.

3. Request to Demolish Structures on a Heritage Listed Property - 6432 Ninth Line (Ward 10)
Corporate Report dated April 23, 2014 from the Commissioner of Community Services with respect to a request to demolish structures on a heritage listed property at 6432 Ninth Line.

RECOMMENDATION FOR APPROVAL

That the barn and outbuilding structures located on the property at 6432 Ninth Line, which is individually listed on the City's Heritage Register, is not worthy of heritage designation, and consequently, that the owner's request to demolish the structures be approved pending the following conditions as described in the Corporate Report dated April 23, 2014 from the Commissioner of Community Services:

1. The dwelling structure is retained and,
 2. That solid wood board hoarding be installed at a distance of five (5) metres around the perimeter of the dwelling structure.
- (Ward 10)

4. Request to Demolish a Heritage Listed Property Within a Cultural Landscape, 3110 Merritt Avenue (Ward 5)

Corporate Report dated April 12, 2014 from the Commissioner of Community Services with respect to a request to demolish a Heritage Listed Property Within a Cultural Landscape at 3110 Merritt Avenue (Ward 5).

RECOMMENDATION FOR APPROVAL

That the property at 3110 Merritt Avenue, which is listed on the City's Heritage Register as part of the War Time Housing (Malton) Cultural Landscape, is not worthy of heritage designation, and consequently, that the owner's request to demolish the structure be approved and the appropriate City officials be authorized and directed to take the necessary action to give effect hereto, as described in the Corporate Report dated April 12, 2014 from the Commissioner of Community Services (Ward 5).

5. Designated Heritage Property Grants 2014

Corporate Report dated April 25, 2014 from the Commissioner of Community Services, with respect to the Heritage Property Grant Program requests.

RECOMMENDATION FOR APPROVAL

That the Heritage Property Grant Program requests be approved as outlined in the report from the Commissioner of Community Services, dated April 25, 2014.

6. Caledon Heritage Foundation 4th Annual Heritage Bus Tour - DIRECTION REQUIRED

Invitation to the Caledon Heritage Foundation 4th Annual Heritage Bus Tour at 9:00 a.m. on May 29, 2014 to Churches/Sites/Cemeteries.

7. Ontario Medal for Good Citizenship

Invitation from the Minister of Citizenship and Immigration dated April 2014 to nominate a deserving citizen for the Ontario Medal for Good Citizenship for receipt.

8. Notice of Commencement – Detail Design – GWP 2163-10-00 - Queen Elizabeth Way (QEW) and Highway 403 Structural Rehabilitation and Replacements from Trafalgar Road to Winston Churchill Boulevard

Ministry of Transportation (MTO) Notice of Commencement of a Detail Design for the rehabilitation and/or replacement of bridge structures for receipt.

9. Transfer of Microfilm Land Registry Records to Thunder Bay, Ontario

Letter dated May 12, 2014 from Service Ontario noting that there are no plans to transfer Microfilm Land Registry Records to Thunder Bay, Ontario, for receipt.

10. Monthly Update Memorandum from Heritage Planning
Memorandum dated April 28, 2014 from Laura Waldie, Heritage Coordinator – Planning providing a monthly update from Heritage Coordinators for receipt.
11. Status of Outstanding Issues Chart from the Heritage Advisory Committee
Chart dated May 8, 2014 prepared by Sacha Smith, Legislative Coordinator with respect to the status of outstanding issues from the Heritage Advisory Committee for receipt.
12. SUBCOMMITTEE UPDATES FROM CHAIRS

Heritage Designation Subcommittee

Public Awareness Subcommittee

DATE OF NEXT MEETING – Tuesday, June 17, 2014 at 9:30 a.m., Council Chamber

OTHER BUSINESS

ADJOURNMENT

MINUTES



HERITAGE ADVISORY COMMITTEE

THE CORPORATION OF THE CITY OF MISSISSAUGA

TUESDAY, APRIL 22, 2014 – 9:34 A.M.

COUNCIL CHAMBER

SECOND FLOOR, CIVIC CENTRE

300 CITY CENTRE DRIVE, MISSISSAUGA, ONTARIO, L5B 3C1

www.mississauga.ca

MEMBERS PRESENT:

Councillor George Carlson, Ward 11 (CHAIR)
Councillor Jim Tovey, Ward 1 (VICE-CHAIR)
Robert Cutmore, Citizen Member
David Dodaro, Citizen Member
Mohammad N. Haque, Citizen Member
James Holmes, Citizen Member
Rick Mateljan, Citizen Member
Cameron McCuaig, Citizen Member
Michael Spaziani, Citizen Member
Matthew N. Wilkinson, Citizen Member
Michelle Walmsley, Citizen Member (arrived at 10:02 a.m.)

MEMBER ABSENT:

Nil

STAFF PRESENT:

Elaine Eigl, Heritage Coordinator
Laura Waldie, Heritage Coordinator
Sacha Smith, Legislative Coordinator

CALL TO ORDER – 9:34 A.M.

APPROVAL OF AGENDA

Approved (Councillor J. Tovey)

DECLARATIONS OF CONFLICT OF INTEREST

Rick Mateljan declared a conflict of interest with Item 3 as the firm he is employed with worked on the project.

DEPUTATIONS

- A. John Ariyo, Supervisor, Research and Projects and Michael Tunney, Cultural Research Coordinator with respect to Cultural Resource Map and Heritage Strategic Plan.

Mr. Ariyo spoke to the Heritage Strategic Plan and explained that the goal is to develop of 5-year strategic plan for heritage that will guide the delivery of heritage services in Mississauga. He outlined the objectives, expected outcomes, project scope and community engagement. Mr. Ariyo further explained the timelines and noted that development would take place between October 2014 - October 2015 and spoke to the Committee's role in developing the Strategy.

Robert Cutmore noted that some real estate agents and companies are not as aware of heritage elements as they should be and suggested that they be involved as a stakeholder for the Heritage Strategic Plan.

Matthew Wilkinson spoke to a working plan that was developed by Heritage Advisory Committee, Heritage Mississauga, Mississauga Library and Museums of Mississauga Advisory Committee to clarify the roles of each organization. However, the roles need to be advertised to the public and coming together to work on the Strategy may help to accomplish this.

Cameron McCuaig enquired about an interim plan until the Heritage Strategic Plan is completed. Laura Waldie, Heritage, Special Projects advised that the *Ontario Heritage Act* would guide the Committee.

Mr. Tunney explained the Cultural Resource Map and the type of data it provides. The next steps are to upload revised information, evaluate options for the maintenance and support of the Map for 2015 and beyond, conduct stakeholder engagement (including HAC) on proposed enhancements, finalize business requirements for proposed enhancements and implement site enhancements.

Cameron McCuaig enquired if there have been any changes to the original map since its implementation. Mr. Tunney explained that some of the comments that were received were incorporated if they were feasible, otherwise there have been no infrastructure changes to the Map. Mr. McCuaig suggested that the plaques and markers across the City and the Urban Design Award winners/nominees be considered for the Map.

Councillor Tovey enquired if events such as Doors Open and Carassauga are included on the Map. Mr. Tunney advised that there is one marker for the festival. He further explained that the map serves as a planning tool and not a visitor's map.

Rick Mateljan enquired about the uses of the Map. Mr. Tunney explained that data is captured on the first page of the site, once the user moves to the Map no further data is collected. He noted that this matter would be brought to the vendor and addressed moving forward.

RECOMMENDATION

HAC-0017-2014

That the deputation by John Ariyo, Supervisor, Research and Projects and Michael Tunney, Cultural Research Coordinator with respect to Cultural Mapping and Heritage be received for information.

Received (Councillor J. Tovey)

Michelle Walmsley joined the meeting at 10:02 am.

MATTERS CONSIDERED

1. Approval of Minutes of Previous Meeting

Minutes of the meeting held March 18, 2014.

Approved (Cameron McCuaig)

2. Request to Demolish a Structure on a Heritage Listed Property within a Cultural Landscape – 6545 Creditview Road, the “Harris Farm” (Ward 11)

Corporate Report dated March 20, 2014 from the Commissioner of Community Services with respect to a request to demolish a structure on a heritage listed property within a Cultural Landscape at 6545 Creditview Road, the “Harris Farm”.

Matthew Wilkinson enquired about the time duration between the existing and the replacement of the new bridge. Mark Howard, Supervisor, Long Range Planning advised that the work would take between 4-6 months for completion. Mr. Wilkinson expressed concern about the duration of time to not have access to the property. Mr. Howard advised that the property would be monitored by ADT and Corporate Security. In the event an alarm does occur, staff have made arrangements with the property owners to the north for security to gain access.

Robert Cutmore enquired about consultation with Credit Valley Conservation (CVC) and the Ministry of Natural Resources because it is a critical area for fish. Mr. Howard advised that the construction window works around the fish habitat and migration, therefore the work will take place during the right time of the year.

RECOMMENDATION

HAC-0018-2014

That the owner's request to demolish and replace the bridge spanning the Credit River on the property located at 6545 Creditview Road, which is listed on the City's Heritage Register as part of the Credit River Corridor Cultural Landscape, be approved and that the appropriate City officials be authorized and directed to take the necessary action to give effect thereto, as described in the Corporate Report dated March 20, 2014 from the Commissioner of Community Services.

Approved (R. Cutmore)

At this time Rick Mateljan departed the meeting area as he declared a conflict of interest with Item 3.

3. Alteration to a Listed Heritage Property – 1276 Clarkson Road North (Ward 2)

Corporate Report dated March 17, 2014 from the Commissioner of Community Services with respect to an alteration to a listed heritage property at 1276 Clarkson Road North.

RECOMMENDATION

HAC-0019-2014

That the owner's request to demolish the two car garage and to make alterations to the Listed Heritage property located at 1276 Clarkson Road North be approved and that the appropriate City officials be authorized and directed to take the necessary action to give effect thereto as described in the Corporate Report dated March 17, 2014 from the Commissioner of Community Services.

Approved (C. McCuaig)

4. Alteration of a Designated Part V Property Meadowvale Village Heritage Conservation District – 1074 Old Derry Road (Ward 11)

Corporate Report dated March 20, 2014 from the Commissioner of Community Services with respect to an alteration of a Designate Part V Property Meadowvale Village Heritage Conservation District at 1074 Old Derry Road.

Jim Holmes noted that the garage design is simple and the applicant has been cooperative in addressing any concerns.

RECOMMENDATION

HAC-0020-2014

That the owner's request to make alterations to the Designated Heritage property located at 1074 Old Derry Road in the Meadowvale Village Heritage Conservation District by constructing a stand-alone, one-storey, two-car garage be approved and that the appropriate City officials be authorized and directed to take the necessary action to give effect thereto, as described in the Corporate Report dated March 20, 2014 from the Commissioner of Community Services.

Approved (J. Holmes)

5. Status of Outstanding Issues Chart from the Heritage Advisory Committee

RECOMMENDATION

HAC-0021-2014

That the chart dated March 28, 2014 from Sacha Smith, Legislative Coordinator with respect to the status of outstanding issues from the Heritage Advisory Committee be received for information.

Received (M. Spaziani)

6. Monthly Update Memorandum from Heritage Planning

Memorandum dated March 28, 2014 from Laura Waldie, Heritage Coordinator – Planning providing a monthly update from Heritage Coordinators.

Received (Councillor J. Tovey)

7. SUBCOMMITTEE UPDATES FROM CHAIRS

There were no updates from the subcommittees.

OTHER BUSINESS

Laura Waldie spoke with respect to the transfer of Microfilm Land Registry Records to Thunder Bay, Ontario. She noted that the project is on hold while the process is being reviewed to see how Service Ontario can work around it. She further noted that Service Ontario received a lot of complaints from heritage planners and researchers on how the change would affect their work. Councillor Tovey thanked Matthew Wilkinson for bringing this matter to the Committee's attention.

Matthew Wilkinson spoke to Heritage Mississauga's "The Credits" Award Program and noted the deadline for nominations. The invitation to the Heritage Advisory Committee would be on the next agenda.

Mr. Wilkinson advised that the Culture Café evenings would start on May 14, 2014 in the C Café and the theme is Mississauga Legends Row which will be facilitated by Ron Duquette. In June, Michael Spaziani will be speaking to building Mississauga 40 years in architecture.

Robert Cutmore spoke to history walks in Port Credit that would take place on April 26, 2014 and May 24, 2014 recounting World War I. The walks start at 10:00 am at the lighthouse in Port Credit. Mr. Cutmore further noted that there has been a lot of interest and there is a possibility that the walks could take place monthly.

Michelle Walmsley enquired about discussion at the last meeting regarding an off-site meeting. Councillor Carlson advised that it would take place following this meeting.

DATE OF NEXT MEETING – Tuesday, May 20, 2014 at 9:30 a.m., Council Chamber

ADJOURNMENT - 10:20 A.M.



Corporate Report

2-1

Clerk's Files

Originator's
Files

DATE: April 28, 2014

TO: Chair and Members of the Heritage Advisory Committee
Meeting Date: May 20, 2014

FROM: Paul A. Mitcham, P. Eng, MBA
Commissioner of Community Services

SUBJECT: **Demolition of a Designated Part V Structure**
1050 Old Derry Road
Meadowvale Village Heritage Conservation District (Ward 11)

RECOMMENDATION: That the property owner's request to demolish the structures on the property located at 1050 Old Derry Road, which is Designated under Part V of the Ontario Heritage Act as part of the Meadowvale Village Heritage Conservation District be denied, as described in the Corporate Report dated April 28, 2014, from the Commissioner of Community Services.

BACKGROUND: The subject property is located within the Meadowvale Village Heritage Conservation District under By-law 453-80. The subject property supports a single storey, brick bungalow and a single car detached garage of the same materials.

The subject property was part of the original 200 acre parcel of land granted to John Simpson, the founder of Meadowvale Village. By 1865, Simpson began selling off portions of his land including the subject property. Significant owners of the subject property included James Rowcliffe, who set up a blacksmith shop on the property in 1894. James Albert Lambe, the grandson of John Simpson, acquired the property in 1938 and built the existing residence.

The property owner has submitted a Heritage Property Permit Application and a Heritage Impact Statement prepared by Paul Oberst, attached as Appendix 1. To date a Site Plan Approval application has

not been filed with the City's Planning and Building department.

COMMENTS:

In 2003, the Conservation Principles and Design Guidelines for the Meadowvale Village Heritage Conservation District were approved. This document states the purpose of the Heritage Conservation District is to preserve the existing buildings and village appearance. Additionally, it states "no existing buildings should be demolished" However, Section 42.(1) of the Ontario Heritage Act (OHA) provides Council the authority to consider demolition. Specifically, the OHA states "no owner of a property located within a heritage conservation district shall erect, demolish or remove any building or structure on the property without a heritage permit".

Typically, in consideration of a Heritage Property Permit Application, staff would refer to the property's statement of significance. Unfortunately, the Conservation Principles and Design Guidelines for the Meadowvale Village Heritage Conservation District does not include this for the subject property. This is due to the fact that the document pre-dates changes to the OHA which required such statements for each property within the Heritage Conservation District Plan. Therefore, in the absence of a specific definition of heritage attributes for this subject property within the 2003 document, staff have undertaken the research. The following provides an assessment of the impact of the proposed demolition in the context of this research.

The existing structure contributes to the village as a good example of a one-storey residential bungalow of the 1930s. It is the only structure in Meadowvale Village of this time period to have been constructed of brick veneer. The positioning of the house on the lot is contextually significant and contributes to the streetscape and green space to the west and rear of the property. The dwelling was situated in relation to the ridge to the east and the slope to the west to help retain the original topography of the landscape.

This main structure represents the only dwelling known to have existed on this site, which was once part of the nineteenth century commercial core of Meadowvale Village. In the early twentieth century, the economy of Meadowvale Village began to decline with a number of businesses closing along Old Derry Road and being redeveloped into residential properties. The subject property represents that shift in economy and is the first known dwelling to inhabit this site, making its cultural heritage value to the twentieth

century development of Meadowvale Village significant and worthy of retention.

Based on this assessment, it is staff's opinion the subject property has cultural heritage value. Moreover, it is staff's opinion that a more appropriate approach to managing change within the Village and in particular, to the subject property, would be through a sensitive alteration and/or addition to the existing structure. The Conservation Principles and Design Guidelines for the Meadowvale Village Heritage Conservation District support this approach and specifically refer to the principle of alteration and additions to heritage properties which include:

- Preserving what exists;
- Make changes cautiously and knowledgeably; and
- Not misrepresent history.

Therefore, Heritage Planning staff recommends the application for demolition be denied.

FINANCIAL IMPACT: There is no financial impact.

CONCLUSION: The property owner of 1050 Old Derry Road has requested permission to demolish a structure located within the Meadowvale Village HCD. Heritage Planning staff recommend the owner's request for demolition be denied.

ATTACHMENTS: Appendix 1: Heritage Impact Statement by Paul Oberst



Paul A. Mitcham, P. Eng, MBA
Commissioner of Community Services

Prepared By: Laura Waldie, A/Senior Heritage Coordinator, Planning

Heritage Impact Statement
1050 Old Derry Road, Meadowvale Village
City of Mississauga



Existing House, view from the southwest. Photo by author.

Paul Oberst, B. Arch., OAA, CAHP
Paul Oberst Architect
April 2014

Engagement:

I am an architect licensed in Ontario, and a professional member of the Canadian Association of Heritage Professionals (CAHP). I was engaged by the owners to produce a heritage impact statement regarding the property at 1050 Old Derry Road, in Meadowvale Village in the City of Mississauga. I am also engaged to advise the project architect, Chris Wallace.

Contacts:

Heritage Consultant-	Paul Oberst Architect	416-504-6497
	oberst@bellnet.ca	
Owner-	Current Owner	416-898-3077
	neil@rogers.com	
Architect-	Christopher Wallace Architect	905-753-1122
	chris@cwallacearchitect.com	

Table of Contents

1. The Proposal	3
2. Location	3
3. Historical Back Ground	4
4. Chain of Ownership	8
5. Current Development Controls	11
6. Existing House Description	13
7. Construction	15
8. Dating the House	15
9. HCD Inventory Description	17
10. To Conserve the District Character	18
11. Contribution of the Property to District Character	19
12. Consultant's Assessment	20
13. Development Alternatives	22
14. Conclusions	30
11. Bibliography	32

Appendices

Project Conservation Principles

Consultant's CV

Project drawings and data

1. The Proposal

It is proposed to renovate the existing house at 1050 Old Derry Road in the Meadowvale Village Heritage Conservation District and to construct additions that will be sympathetic to the architectural and landscape character of the District.

The owner's original intent was to remove the existing house and construct a new sympathetic building. This idea was not well received by the Village Heritage Committee, and the proposal is now as stated above. See section below on alternatives.

2. Location

The property is located on the south side of Old Derry Road in Meadowvale Village in the City of Mississauga. The eastern lot line is roughly opposite the intersection of Pond Street and Old Derry Road. The property is described as Concession 3 West of Hurontario Street East Part Lot 10, and it bears the Roll Number of 21-05-040-098-23600-0000.

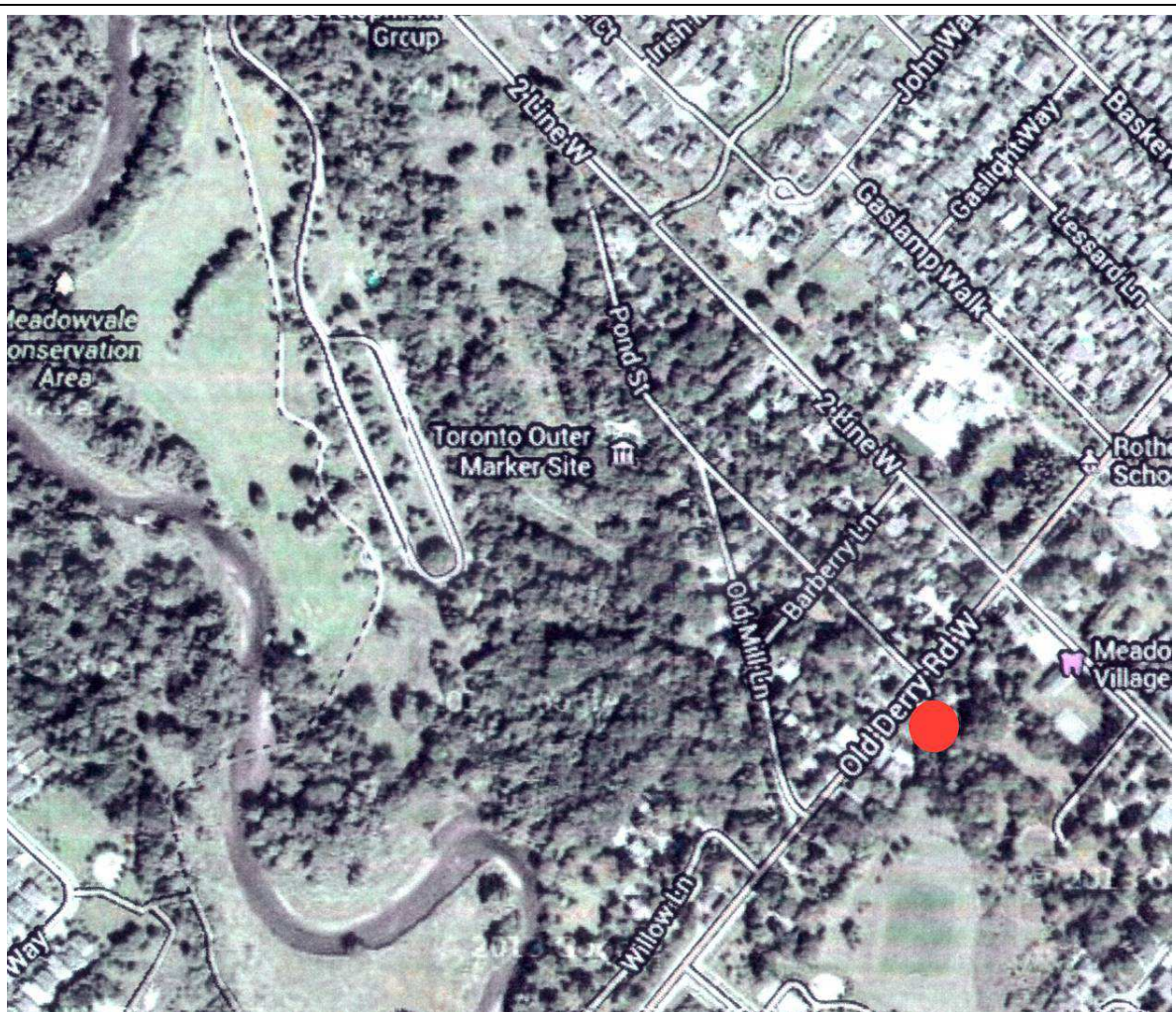


Figure 1. Aerial photo. Location of the house is indicated with the red dot.

3. Historical Background

3.1 Overview: Meadowvale Village is a historic settlement near the northern boundary of Mississauga, about 3 km west of Hurontario Street. It is located on the banks of the Credit River, at the intersection of two original concession roads, Second Line Road and the Sideroad, between Lots 10 & 11, now called Old Derry Road. Farming settlement began in 1819 with the arrival of John Beatty. As in many early Ontario settlements, the proximity of a road to a watercourse provided the opportunity for development of a water-powered milling industry. In 1838, John Simpson built the first successful sawmill in Meadowvale, a few years after James Crawford's attempted mill had faltered. Milling became the economic engine of the village, and milling activity continued for more than a century.

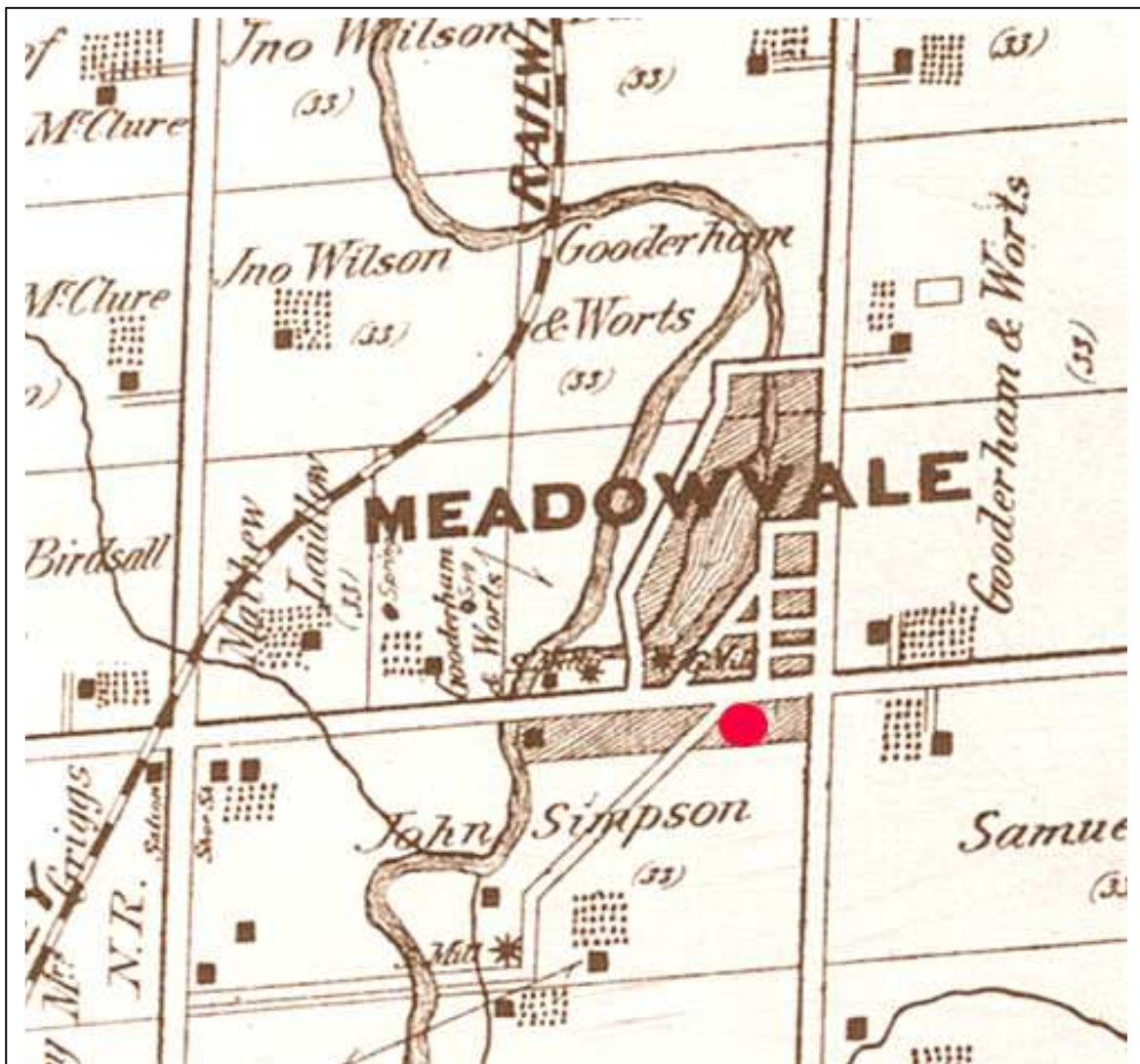


Figure 2. From 1877 Peel County Atlas. Houses on farms are shown individually, but within the village, the settled area is shaded, without showing individual buildings. Red dot is approximate site of the subject property. Two sawmills and a grist mill are shown on this map.

3.2 Historical background: There is extensive material on many aspects of the Village's history in the Phase 1 Report (Background) of the HCD Plan Review (Phase 1 Report). It is not necessary to repeat all 50-plus pages here. I will focus my attention on the persons and activities that bear on Old Derry Road generally, and the subject property in particular.

People: Several persons noted as significant to Meadowvale's history in the Phase 1 Report or in the Property Inventory appear in the title abstracts connected to the subject property. Refer to Section 4, Chain of Ownership, later in this document. The first person of historic interest is the John Simpson who built the first successful mill, as mentioned above. John Simpson is considered to be the true founder of Meadowvale Village, due to his efforts to bring new settlers to the area to work in his sawmill or as forestry workers to cut down the trees he processed. He is connected to the subject property as the owner of the entire 200 acres of lot 10, on the south side of Old Derry Road between the Second and Third Line Roads. Starting in the 1860s, Simpson began selling off his lands east of the Credit River. So the subject property, at ¼ acre, is connected to Meadowvale's milling pioneer by occupying about 1/10 of 1% of the Simpson farm lot.

The second significant figures are Thomas Graham and his wife, Mary Jane Graham, who was John Simpson's daughter. In around 1862, Simpson gave a 2-acre parcel to Thomas Graham, and built the house that is still standing at 1020 Old Derry Road as their wedding present. This parcel apparently did not include the subject property, because Thomas Graham doesn't appear in the title chain until 1886. His widow Mary Jane Graham sold it in 1903. In 1888, the Grahams moved to the Gooderham Mansion at 929 Old Derry Road. Before they moved, the severed lots from their property and sold them to a number of purchasers. Part of the severed land became the site of James Rowcliffe's blacksmith shop in 1894. His son, Albert, took over the shop in 1897, and lived "directly across the road" at 1043 Old Derry Road.

The third significant owner is Francis J. Jackson, a prominent local businessman, politician and farmer. He inherited his father's house at 1200 Old Derry Road in 1912, and it's associated farmland, and began to assemble all of the lots between Nos. 1020 and 1200 on the south side of Old Derry Road. He purchased the subject property in 1912.

Activities: Today, the only businesses on Old Derry Road are the garage and pet boutique operating out of the old Texaco station immediately west of the subject property. But the road was once the commercial and industrial heart of the village, and was home to several stores and a number of industries. It was a very different streetscape than it is today.

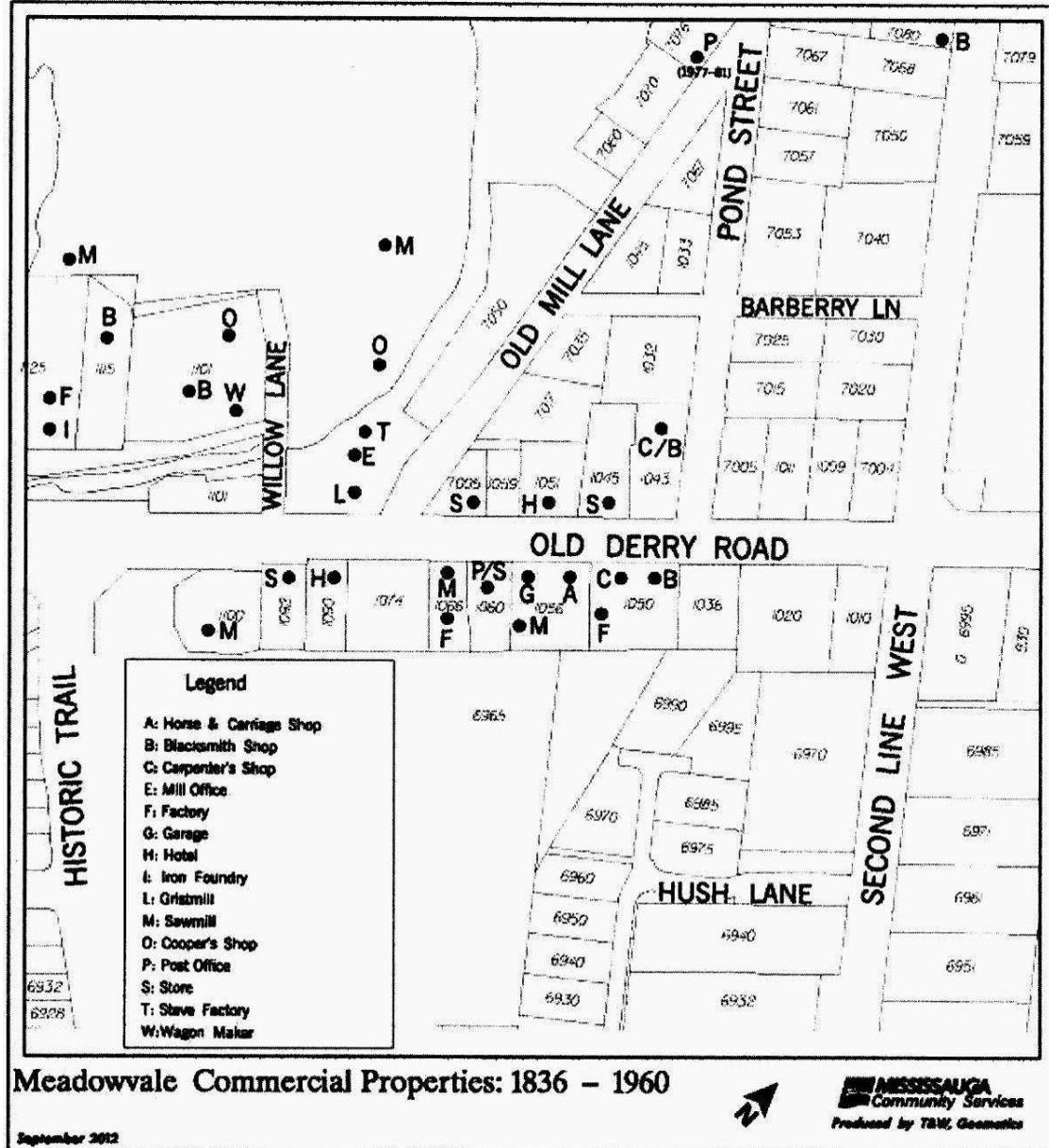


Figure 3. Map from the Phase 1 Report, showing the historical commercial character of Old Derry Road. The subject property is shown to have contained a blacksmith shop, a carpenter's shop, and an unspecified factory. It appears from the narratives that the blacksmith shop was the one operated by James Rowcliffe, and then his son Albert.



Figure 4. Looking west on Old Derry Road, with the 2-storey porch of the Commercial Hotel visible at the right. The commercial and industrial buildings on the south side of the road are evident. Photo provided by Heritage Mississauga.



Figure 5. A similar view from a bit later, showing the new sidewalk which was laid around 1894. Note the well-worn foot-track across the street. This may have been trod by the Rowcliffes going between their blacksmith shop and their home across the street. Taken from near the west lot line of the subject property. Photo provided by Heritage Mississauga.

4. Chain of Ownership. The Bristow Survey of 1856 laid out “House and Park Lots” in the village north of Old Derry Road. Each of these parcels were given separate entries in the Land Titles Abstract. South of Old Derry Road, John Simpson’s farm remained undivided, so that subsequent severances all appear in the Titles abstract as “part of Lot 10, 3rd Concession West of Horontario Street. As a result every transaction, mortgage, and mechanics lien on all subsequent lots are listed together chronologically in the Abstracts. Following the ownership of the ¼ acre parcel at 1050 Old Derry Road was to be a formidable task.

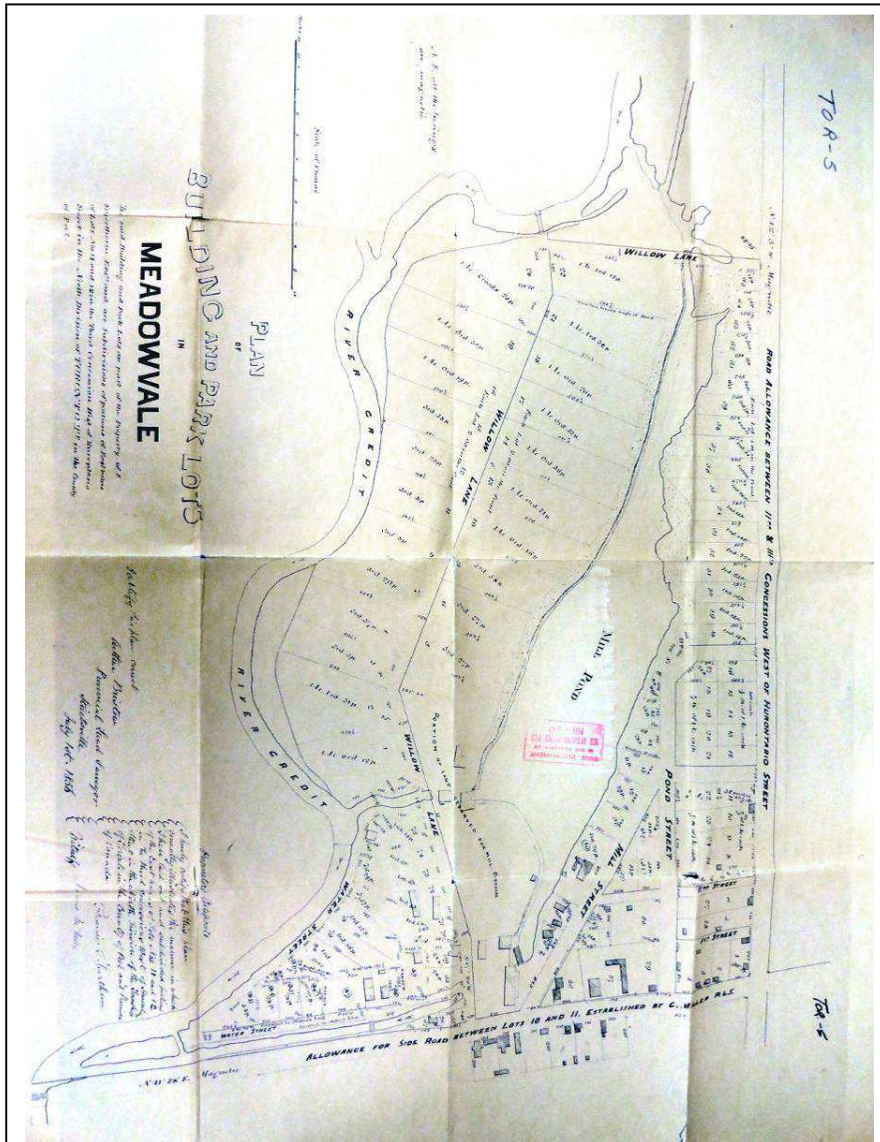


Figure 6. The Bristow Survey, 1856. Although lots and buildings are shown on the south side of Old Derry Road, they are not on separately enumerated parcels. Photograph provided by Heritage Mississauga.

The following images are from the Abstracts of Title in the Peel Archives. The relevant transactions are highlighted.

LOT No. 10				3 CONCESSION <i>West</i>				
No. of INSTRUMENT	INSTRUMENT	ITS DATE	DATE OF REGISTRY	GRANTOR	GRANTEE	QUANTITY OF LAND	CONSIDERATION OR AMOUNT OF MORTGAGE	REMARKS
	Patent	27 April 1824		The Crown	Caran Richards	200 acres		
5576	Leff	1 Dec 1826	25 Apr 1826	Caran Richards	Jane Brown	"		
7204	P.S.	3 May 1828	24 Feb 1830	Andrew Brown et ux	Matthew Dawson	"	£75-	
1783	LoR	1 Dec 1837	24 Nov 1839	William Bell et al	John Simpson	"	£89-	
22589	M	30 Apr 1844	3 May 1844	John Simpson et ux	Jonas Hardy	W 2	£39-	
927	"	5 Sept 1852	6 Sept 1852	George Bell et ux	William Hardy	1 Road	£35-	pt 82
1031	P.S.	24 Oct 1852	25 Oct 1852	John Simpson et ux	Jonathan Robinson	1 "	£25-	"
1025	"	5 Sept 1852	" "	John Simpson et ux	George Bell	1 Road	£60-	"
1062 1/2	D.M.	24 Oct 1853	8 Nov 1853	Jonas Hardy	John Simpson	W 2		22589
1998	P.S.	24 Oct 1853	1 Oct 1853	John Simpson et ux	George Bell	Pl 2 1/2	£30-	
2514	"	" "	10 May 1856	John Simpson et ux	Matthew Ludlow	2 Roads	£60-	
2517	"	10 May 1856	" "	George Bell et ux	Matthew Ludlow	2 "	£30-	
4196	"	10 Aug 1857	10 Aug 1857	Jonathan Robinson et ux	Christopher Chyne	1 "	£200-	

Figure 7. The first four transactions include the entire 200 acres of Lot 10, ending with John Simpson's purchase in 1937. The fifth transaction is of a 2 rood (1/4 acre) parcel.

Figure 7. The first four transactions include the entire 200 acres of Lot 10, ending with John Simpson's purchase in 1937. The fifth transaction is of a 2 rood (1/4 acre) parcel.

3589	B.A.S.	22 Feb 1882	7 Mar 1882	Thomas Graham	Thomas Elliott	150.00	Part E
3700	B.A.S.	2 Jul 1879	27 Jul 1882	Sarah Eldridge et al	Thomas O'Shaughnessy	310.00	1 rood
3701	B.A.S.	25 Jul 1882	27 Jul 1882	T. O'Shaughnessy et ux	T. Sibbald	2000.00	Part E
3702	B.A.S.	3 Jul 1882	27 Jul 1882	T. O'Shaughnessy et ux	John Henry	400.00	Part E
3703	MORTGAGE	21 Jul 1882	27 Jul 1882	F. Sibbald et ux	T. O'Shaughnessy	1500.00	Part E
5101	MORTGAGE	2 May 1884	14 May 1884	John Henry et ux	Thomas Graham	250.00	Part E
5200	MORTGAGE	1 Mar 1884	19 Nov 1884	Francis Sibbald et ux	Jonas Hardy	500.00	1/10 ac.
5201	MORTGAGE	13 Nov 1884	19 Nov 1884	Jonas Hardy	Mary E. Saunders	300.00	1/10 ac.
5365	B.A.S.	10 Apr 1885	23 Apr 1885	Thomas Elliott et ux	Thomas Graham	450.00	Part E
5458	MORTGAGE	5 Nov 1885	6 Nov 1885	Frank Sibbald et ux	Thomas Duggan	310.00	Part E and O.L.
5604	B.A.S.	23 Dec 1884	17 Mar 1886	Francis Sibbald et al	Albert Lamb	1300.00	Part E
5832	B.A.S.	29 Oct 1886	1 Nov 1886	John Henry et ux	Thomas Graham	217.50	Part E
6421	W.O.	19 Mar 1888	20 Mar 1888	Hardy vs. Campbell	Francis Sibbald		1/10

Figure 8. The next three transactions, ending with Thomas Graham's purchase in 1886.

Township of Toronto.									
From Page 500 Book B		Lot No. 10		in the Third Range		Con. West N. 1st			
NO. OF DEEDS	DATE	TO	FROM	GRANTOR	GRANTEE	QUANTITY OF LAND	CONSIDERATION	REMARKS	
11267	11 Feb. 1902	19 Feb. 1902	James Henry & Thomas H. Deegan	James H. Deegan	1/10	1/10	1/10	1/10	1/10
11268	7 Nov. 1902		James Henry & Thomas H. Deegan	James H. Deegan	1/10	1/10	1/10	1/10	1/10
11291	28 Nov. 1903	1 Dec. 1903	Mary Jane Graham	Jennette Hayes	1/10	1/10	1/10	1/10	1/10
11292	15 Dec. 1903	2 Feb. 1904	James Jackson & Mary Jane Graham	James Jackson	1/10	1/10	1/10	1/10	1/10

Figure 9. Mary Jane Graham's sale to Jennette Hayes.

13971	10 Feb. 1910	7 May 1910	William Francis et al.	Francis J. Jackson	1/10	1/10	1/10	1/10	1/10
14016	10 Feb. 1910	21 Feb. 1910	Francis J. Jackson	John J. Jackson	1/10	1/10	1/10	1/10	1/10
14017	10 Feb. 1910	21 Feb. 1910	Francis J. Jackson	John J. Jackson	1/10	1/10	1/10	1/10	1/10
14018	10 Feb. 1910	21 Feb. 1910	Francis J. Jackson	John J. Jackson	1/10	1/10	1/10	1/10	1/10
14019	10 Feb. 1910	21 Feb. 1910	Francis J. Jackson	John J. Jackson	1/10	1/10	1/10	1/10	1/10

Figure 10. Sale to Francis J. Jackson in 1912.

201	25 Feb. 1921	28 Feb. 1921	William Brown et al.	Henry Brimble	2	1000	1000	1000	1000
202	25 Feb. 1921	28 Feb. 1921	William Brown et al.	Henry Brimble	2	1000	1000	1000	1000
203	25 Feb. 1921	28 Feb. 1921	William Brown et al.	Henry Brimble	2	1000	1000	1000	1000
204	25 Feb. 1921	28 Feb. 1921	William Brown et al.	Henry Brimble	2	1000	1000	1000	1000
205	25 Feb. 1921	28 Feb. 1921	William Brown et al.	Henry Brimble	2	1000	1000	1000	1000

Figure 11. Sale to Alfred M. Hall in 1921 & sale to William Lambe later the same year.

38584	25 Jan. 1938	5 Aug. 1938	Hydro. Electric Power Com.	Francis J. Jackson	1/10	1/10	1/10	1/10	1/10
38585	25 Jan. 1938	5 Aug. 1938	Hydro. Electric Power Com.	Francis J. Jackson	1/10	1/10	1/10	1/10	1/10
38586	25 Jan. 1938	5 Aug. 1938	Hydro. Electric Power Com.	Francis J. Jackson	1/10	1/10	1/10	1/10	1/10
38587	25 Jan. 1938	5 Aug. 1938	Hydro. Electric Power Com.	Francis J. Jackson	1/10	1/10	1/10	1/10	1/10
38588	25 Jan. 1938	5 Aug. 1938	Hydro. Electric Power Com.	Francis J. Jackson	1/10	1/10	1/10	1/10	1/10

Figure 12. From William Lambe to his son James A. Lambe in 1938.

41224	10 Oct. 1941	20 Oct. 1941	Minerva S. Little	Lulu Reeve	1/10	1/10	1/10	1/10	1/10
41530	4 Feb. 1942	12 Feb. 1942	Wm. T. Little	Lulu Reeve	1/10	1/10	1/10	1/10	1/10
41709	25 Apr. 1942	2 May 1942	Jas. A. Lambe et al.	Lulu Reeve & John T. Reeve	1/10	1/10	1/10	1/10	1/10

Figure 13. Sale to Lulu Reeve and John T. Reeve in 1942.

Figure14. Transactions in 1944 and 1945: Reeve to Hale to McLaren.

Figure15. Sale from the McLaren estate to Mary E. Rea and Gordon W. Rea.

No.	Instrument	Date	Grantor	Grantee	Quantity
RO1045301	Transfer	17Aug1993	Mary E. Rea	Mary E. Rea	Part
RO1066962	Transfer	1Jun1994	Mary E. Rea Estate	Jason Holmes	Part

5. Current Development Controls

a) The Mississauga Official Plan designates the land use of the subject property as Residential Low Density I. This designation allows Detached Dwellings, Semi-detached Dwellings, and Duplex Dwellings. The adjacent lot at 1060 Old Derry Road is designated Convenience Commercial.

1050 Old Derry Road, Meadowvale Village 11

Under the heading of Site Development Standards

16.17.1.26. The development of properties within the Heritage Conservation District and the Village Precinct will be subject to site plan control. In the case of the Heritage Conservation District, development of properties will require the approval of the local Architectural Conservation Advisory Committee (LACAC) in accordance with the *Meadowvale Village Heritage Conservation District Plan*.

Under the heading of Buildings and Spaces

16.17.1.28 Buildings and Spaces Policies apply to all lands within the Meadowvale Village Character Area. In applying the following policies, the effect of buildings and spaces on the surrounding environment should be considered equally with the function and aesthetic appeal of the site itself:

- a. Sites within the Heritage Conservation District will be subject to the policies of the *Meadowvale Village Heritage Conservation District Plan* and *Conservation Principles and Design Guidelines for the Meadowvale Village Heritage Conservation District*.

5.2 Zoning Bylaw 0225-2007, enacting by-law BL 0308/11

The subject property is zoned R1-32. R1 designation allows only detached dwellings. Among the requirements of this designation, interior lots must have minimum frontage of 22.5m and a minimum area of 750 sq m. Coverage is limited to 25%.

Additionally, in an R1-32 zone the following uses/regulations shall apply:

Regulations

4.2.2.32.1 The regulations of Sentence 3.1.1.7.1 of this By-law shall not Apply (this concerns surface treatment of driveways).

4.2.2.32.2 A flat roof shall not be permitted.

4.2.2.32.3 Maximum height - highest ridge: sloped roof=7.0 m.

4.2.2.32.4 An attached garage shall not be permitted.

4.2.2.32.5 Maximum projection of the front garage face of a detached garage beyond any portion of the first floor front wall or exterior side wall=0.0m.

6. Existing House Description

The house is located about 8m back from the front lot line, near the centre of a large, well-treed lot.

The house is a brick one-storey hipped roof bungalow with a projecting front-gabled porch which occupies the west half of the façade. The gable pediment is in wood above the brick, expressed as a lintel about 25cm deep, with a board and batten infill above. Bargeboards are plain. Soffits are chamfered wood boards.

The main wing has a triplet window of 3 over 1 double hung windows. This window type is found in all of the existing original openings. The porch door is offset to the east, with one window on the left, and two on the right. There are 5 steps, without a landing, leading to the porch door from a sidewalk at local grade.

The entry from the porch into the house is offset to the east side of the porch—you step to the side as you walk through the porch. This entry door is the only original door in the house, 3/3 lights at the top over 3 vertical panels.

There is a modern side door near the rear of the west wall, at local grade, opening onto the basement stair below the ground floor level. This is probably in an original opening. A new French door has been installed toward the west side of the rear wall. Grade is higher at the back, so it is only 3 steps above local grade.



Figure 16. Front (north) elevation.



Figure 17. Rear and east elevations.
The steep topography is evident.

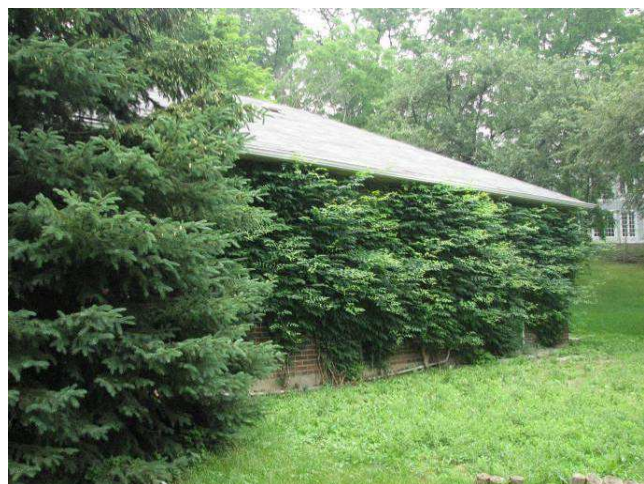


Figure 18. West elevation.

The interior of the house has been much altered. Walls have been removed to create a more open plan than what would have been the original layout. New closets, with double width doors have been created in place of what would have been much smaller ones. All interior trim, including doors, door and window casings and baseboards have been replaced with modern items.



Figure 19. Door from porch to house.
This is typical of the arts-and-crafts style.



Figure 20. The interior is much altered. New large closets with double width doors have been installed. This is a mirrored door, reflecting the front windows behind.

7. Construction

Construction details are visible in the basement. The foundation walls are of concrete block, up to near grade, with double wythe solid brick walls above. This technique allows the facades to present a brick exterior all the way down to grade. Floor joists are planed dimension timber, not the rough-sawn lumber found in earlier construction. There is a steel support beam running north-south in the centre of the house, with one intermediate support. It is an unusual item in that it is a section of railway track. It's a fairly insubstantial beam for the span and loads involved. The rail bears the maker's name and date: Carnegie 1895.

8. Dating the House.

8.1 Documentary Evidence

I haven't been able to find any firm evidence of the date of construction. There are no maps that show it suddenly appearing where there was previously vacant land. The Meadowvale HDC Plan Inventory dates it "c.1930". I spoke with staff at the local office of MPAC. They found a "card" describing the house and its construction in some detail, and noting 1930 as a construction date. The card itself is not dated, and the actual historic tax rolls are no longer in existence. It's impossible to know the provenance of that date assignment—it might have been a guess on the part of an assessor. The Title Abstracts in the Peel County Archives put values on most transactions, although when William T. Lambe sold the property to his son, in 1938, the value was listed as "\$1+NLA". This is certainly after the house was built—so it isn't relevant. Previous sales show values as follows:

1912-to Francis J Jackson-\$ 425

1921-to Alfred M. Hall- \$2500

1921-to William T. Lambe-\$2500



Figure 21. Foundation wall: Block below, brick above.



Figure 22. Floor joists are planed, not rough-sawn.



Figure 23. Maker's name and date on the railway track beam..

1938-to James A Lambe- \$1+NLA

(This kind of valuation is not uncommon in family transactions.)

1942-to the Reeves- \$4000

8.2 Physical Evidence

The physical dating evidence includes:

The use of dimension lumber in the visible floor joists and attic rafters. The use of dimension lumber became common after World War I, and national dimension standards in the U.S. were first established in 1924, according to *History of Lumber Yard Size Standards*, published by the U.S. Forestry Service in 1964.

The use of rug brick in the exterior walls. Rug, or tapestry brick came into vogue around 1910, according to *The Annual Report of the Ontario Department of Mines, Vol 39, 1930*.

The use of concrete block in the foundation. In 1919, the Concrete Block Manufacturers Association was established. In that year, U.S. production was about 50 million blocks, and it reached nearly 400 million by 1928.

The construction materials suggest that the house dates from the 1920s or later.

The house design reflects the Arts-and Crafts style—called American Craftsman in the U.S—as evidenced in the 3/1 double hung windows with a tall lower sash, and the design of the front door. This style was in its heyday from about 1915 to 1930. The 1927 newspapers used as insulation, noted in the Inventory, are likely a good indication of the construction date.

The preponderance of physical evidence places the date of construction from 1925 to 1930, suggesting it was built when William T. Lambe owned the property.

9. Meadowvale Heritage Conservation District Plan Inventory, 2013 Draft

The 2013 Inventory entry for the property is provided below, with my commentary:

Background:

This property sits on land once owned by John Simpson. Little is known about this current property except for the fact that this was believed to have been the site of a wagon wheel shop. Land Registry abstracts indicate that William Lambe acquired this property in 1921. He sold the lot to his son James Albert Lambe in 1938 and it is believed that William built the house for James and his family. James lived here until 1942. Renovations conducted inside the house in about 2005 produced newspapers dating to 1927 used in the walls for insulation. Although this is not definitive proof of the house's construction, as paper was kept and used for future use such as insulation, it is generally believed the house was built close to 1930.

Comment: The newspapers are probably the best evidence we have for a date of construction. See Section 8, above.

Description:

A modest one storey red brick veneer structure with a projecting bay entrance at the front façade. The wood windows are original three-over-one pattern. The house has a modest scale, shape and form on a large lot.

Heritage Attributes:

The modest scale, shape, massing and form of a one storey structure of original brick veneer

Original wood windows in a three-over-one pattern

Its location in relation to Old Derry Road and open views to the front and side facades within generous side and rear yards of open green space.

Comment: The form of the building, and the 3/1 windows are attributes of the house, but they are not common to the heritage character of Meadowvale Village. The location and views could also be maintained by a new building.

Statement of Significance:

The property at 1050 Old Derry Road has historical association with the early development of Meadowvale Village, and the current structure with the mid-20th century Village development. The building contributes to the Village character with its modest scale, size, shape, massing, form and materials within a context of generous side and rear yards of open green space.

Comment: The association of the property with the early development of Meadowvale Village relates to land ownership only. John Simpson once owned the land, along with 199-3/4 additional acres. That association will not be changed by a new building. Although the existing bungalow is associated with later Village development, its form, shape, massing and materials are not consistent with the heritage character of Meadowvale Village.

10. To Conserve the District Character

The Ministry of Tourism Culture and Sport describes the function of Heritage Conservation Districts in the first section of its Heritage Toolkit book *Heritage Conservation Districts: A Guide to Designation Under the Ontario Heritage Act*:

District designation enables the council of a municipality to manage and guide future change in the district, through the adoption of a district plan with policies and guidelines for conservation, protection and enhancement of the area's special character. (emphasis added.)

In other words, the Ministry recognizes that districts change. The aim is not to conserve every element in the area, but to conserve the area's overall character.

It has been common, therefore, for Heritage Conservation District Plans to make a statement about what that character is. Since the 2005 amendments to the Ontario Heritage Act, Plans have been required to contain "a statement explaining the cultural heritage value or interest of the heritage conservation district". The Toolkit describes this statement as follows:

The statement of cultural heritage value describes the heritage values that contribute to the special identity and character of the district that should be protected. A clear statement will help to promote understanding of the values and attributes and will assist decision-makers in ensuring that future changes and interventions contribute to, rather than detract from, the character of the area. Statements should be brief and succinct and should relate specifically to the identified values and attributes of the district.

The 2003 *Conservation Principles and Design Guidelines for the Meadowvale Village Heritage Conservation District*, which is the currently operating Plan, describes the character of the Village:

The historic village of Meadowvale is situated on land first worked by Irish immigrants in 1819. Loggers were attracted to the stands of white pine, and by 1836, the village boasted a sawmill and 250 people. The town reached its greatest size just 30 years later when the Toronto-base distillery firm Gooderham and Worts purchased land, timber and mills to produce whisky barrels. The mill, millpond, and raceway gave shape to the town; though the mill is long gone, the historic road patterns and many period houses remain. Meadowvale derives its fundamental character from that era. (emphasis added.)

The Phase I Report-Background (Draft), 2012—not yet operating—includes a description of heritage attributes that expands on this:

- Description of heritage attributes
- a significant location, adjacent to the Credit River, in a cultural heritage landscape of integrated natural and cultural heritage elements within the river's low floodplain to the gentle sloping ridge.
- the ecological feature of the floodplain meadow on the Credit River which has existed for hundreds of years
- a land pattern that retains the layout and plan of lots since the mid nineteenth century

- a spatial organization of narrow soft landscaped streets with no shoulder, mature trees and a visual relationship that blends from public to private space among front and side yards void of privacy fencing
- the long term tradition of streetscapes with no formalized parking, sidewalks (except on Old Derry Road), basic signage and limited modest lighting
- a consistency of building types, modest in architectural detail, vernacular style and size, reflecting the nineteenth century development of a milling village
- a common use of stacked plank construction with exterior stucco finish or wood siding, one-and-a-half storeys and limited use of brick
- structures of compatible size, shape, form and style, although not necessarily of historic significance, contribute to the overall character of the village
- visual identity of rural character roadway entry points to the village from the west on Old Derry Road and from the north along Second Line West, and the open green space of Old Ridge Park to the south
- individual properties of particular character and significance are identified in “The Meadowvale Village Heritage Conservation District Plan Review List of Properties”.

Both of these documents are clear that the character of Meadowvale Village, the character that is to be conserved, was established by its 19th century development. On the ground, it is significant that the south side of Old Derry Road was not included in the Bristow Survey, and that its development history differed from that of the village to the north. In the stretch of about 200 metres between the Graham-Pearson House at No. 1020, and the house at No. 1074 there are no existing 19th century buildings at all, the oldest being the old store at No. 1060, now residential, which was built in 1915.

11. Contribution of the Subject Property to the District Character

11.1 Meadowvale HCD Plan Inventory

The 2013 Inventory entry for the property is reproduced below:

Background:

This property sits on land once owned by John Simpson. Little is known about this current property except for the fact that this was believed to have been the site of a wagon wheel shop. Land Registry abstracts indicate that William Lambe acquired this property in 1921. He sold the lot to his son James Albert Lambe in 1938 and it is believed that William built the house for James and his family. James lived here until 1942. Renovations conducted inside the house in about 2005 produced newspapers dating to 1927 used in the walls for insulation. Although this is not definitive proof of the house’s construction, as paper was kept and used for future use such as insulation, it is generally believed the house was built close to 1930.

Description:

A modest one storey red brick veneer structure with a projecting bay entrance at the front façade. The wood windows are original three-over-one pattern. The house has a modest scale, shape and form on a large lot.

Heritage Attributes:

The modest scale, shape, massing and form of a one storey structure of original brick veneer
Original wood windows in a three-over-one pattern.

Its location in relation to Old Derry Road and open views to the front and side facades within generous side and rear yards of open green space.

Statement of Significance:

The property at 1050 Old Derry Road has historical association with the early development of Meadowvale Village, and the current structure with the mid-20th century Village development. The building contributes to the Village character with its modest scale, size, shape, massing, form and materials within a context of generous side and rear yards of open green space.

12.1 Consultant's Assessment

12.1.1 Contribution to District Character

The first question that needs to be addressed is: What contribution does the existing building make to the heritage character of the Meadowvale Village Heritage Conservation District?

The first Heritage Attribute listed in the Inventory cites the scale, shape, massing and form of the house. It should be noted that neither the form, height, nor material of the house reflect the characteristics cited in the heritage attributes listed in the Phase I document: "exterior stucco finish or wood siding, one-and-a-half storeys"

The second Heritage Attribute listed in the Inventory refers to the 3-over-one windows. These are not typical of the defining 19th century character of the Village, but are distinctly 20th century types. They are not shown as an appropriate window in the *Conservation Principles and Design Guidelines for the Meadowvale Heritage Conservation District 2003*.

The final Heritage Attribute listed in the Inventory refers to location, views, and side and rear yards. These are site plan characteristics, and do not adhere to the building itself.

The Statement of Significance in the Inventory includes historical association with the early development of Meadowvale Village. This association relates to land ownership only, as the current building was not in existence then. John Simpson once owned the land, along with 199-3/4 additional acres. That association will not be changed by a new building.

The building does have association with mid-20th century development, and its scale, size, shape, massing, form, materials and site plan contributes to the Village's current character as described in the Phase I document:

- structures of compatible size, shape, form and style, although not necessarily of historic significance, contribute to the overall character of the village

In other words, the existing bungalow contributes to the Village character, but does not help to define it. Some late 20th century and early 21st century houses meet that standard: 1009 and 1011 Old Derry Road; 7057, 7135, 7143, and 7155 Pond Street; 7040, 7068, and 7080 Second Line West; and 1101 and 1115 Willow Lane. All of these are described in their Inventory Statements of Significance as contributing to, or being compatible with, the Village character by virtue of their design, form, scale, and/or lot characteristics.

12.1.2 Worthiness for Part IV Designation

A second question that might be addressed is: “Does the property at 1050 Old Derry Road merit designation under Part IV of the Ontario Heritage Act?” Ontario Regulation 9/06 sets out the criteria for designation, referenced in Section 29(1)(a) of the *Ontario Heritage Act* as a requirement for designation under Part IV of the Act.

The Regulation states that “A property may be designated under section 29 of the Act if it meets one or more of the following criteria for determining whether it is of cultural heritage value or interest:”

1. The property has design value or physical value because it,
 - i. is a rare, unique, representative or early example of a style, type, expression, material or construction method,
 - ii. displays a high degree of craftsmanship or artistic merit, or
 - iii. demonstrates a high degree of technical or scientific achievement.
2. The property has historical value or associative value because it,
 - i. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,
 - ii. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or
 - iii. demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.
3. The property has contextual value because it,
 - i. is important in defining, maintaining or supporting the character of an area,
 - ii. is physically, functionally, visually or historically linked to its surroundings, or
 - iii. is a landmark. O. Reg. 9/06, s. 1 (2).

My evaluation of the subject property, on the basis of these criteria follows:

1. i, The existing bungalow is not a rare, unique, or early example of its style, type, expression, material or construction method. As with any building, it is representative of something, but this is not a criterion to be applied lightly. The monster home looming over the back yard of the subject property is representative of monster homes. That doesn't make it worthy of designation under Part IV of the Act.

1. ii, The craftsmanship or artistic merit of the bungalow is not high.

1. iii, There is no demonstration of technical or scientific achievement in the building.

2. i, There is no direct association with a theme, event, belief, activity, organization or institution that is significant to the Meadowvale community. By the apparent date of construction, the building is associated with the person of William Lambe, who owned the property between 1921 and 1938. William ran a series of Village stores, was Village Postmaster, and a Justice of the Peace, and at one time owned the Commercial Hotel. There is no documentation that William Lambe ever lived in the house at 1050 Old Derry Road. The

2013 draft Property Inventory says that it is believed that William built the house for his son James, but this is not documented. In 1938 he sold the property to James, who sold it in turn 3 ½ years later. James Lambe was not as significant to the village history as his father or his grandfather before him.

William Lambe's historical role in the Village is well commemorated by his final shop building at 1060 Old Derry Road and the Commercial Hotel. His connection with the bungalow at 1050 Old Derry does not reflect his public roles, and does not meet the threshold for designation, in my professional opinion.

2. ii, The bungalow does not yield or have potential to yield significant information about the community or its culture.

2. iii, There is no identified architect, artist, builder, designer, or theorist.

3. i, The identified character of the area is based on its 19th century development. The existing building does not help define, nor maintain that character. It might be said to support it, mainly due to its scale and landscape characteristics, but the same is can be said of a large number of recent new building as well.

3. ii, The physical connections to the surroundings are weak. The form, materials, and design details would not be permitted by the Meadowvale Village HCD Plan. The original topography has been altered through grading.

3. iii, The building is not a landmark, and is scarcely visible from the street.

In my professional opinion, and based on the criteria in Ontario Regulation 9/06, the property at 1050 Old Derry Road is not a candidate for designation under Part IV of the *Ontario Heritage Act*.

12.1.3 Summary of Heritage Value of Existing Condition

The heritage value of the existing house is not entirely clear-cut. It is not of sufficient value to be designated under Part IV of the Ontario Heritage Act. It does not share the character-defining elements described in the Meadowvale Village HCD Plans, old or new. However it does represent the transition of the Village from its Victorian origins to its modern condition, and is contemporary with some significant buildings, such as the store. Its value to the heritage character of the District relies mostly on its scale and the streetscape aspects of planting and the spatial qualities of the site plan.

13. Development Alternatives

There has been a lengthy design process in consultation with the Village Heritage Committee. It is worth providing a basic narration.

13.1 Previous Approved Proposal

Jason Holmes, the immediately prior owner, received approval for additions and alterations to west and rear of the existing house in 2003. The proposal roughly doubled the area of the house. The front elevation of the proposal is shown later in this document, for comparison.

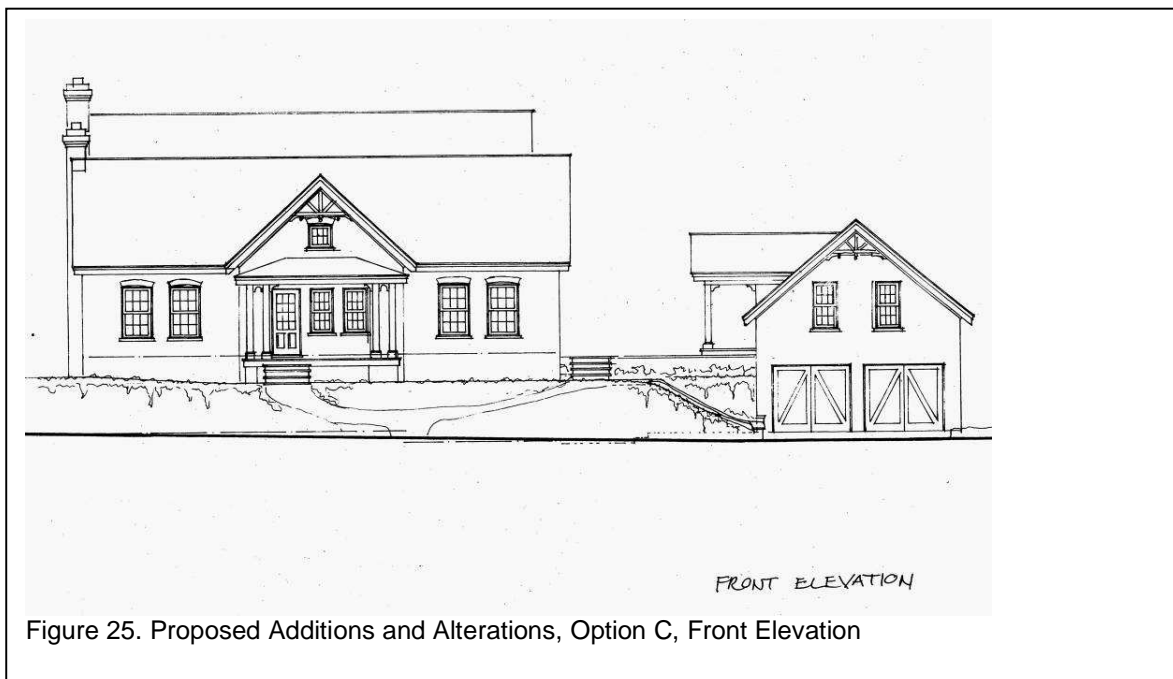
13.1 First Proposals

As outlined above, the Owner's original intention was removal of the existing house, and construction of a new sympathetic building, designed to be similar in form and massing to existing historical buildings with rear additions and to existing new buildings of a similar form. Drawings for two new-build options and one addition and alteration option were prepared for a first meeting with the Village Heritage Committee on 16 August 2013 at the home of the Chair, Jim Holmes.

Prior to showing any drawings, I made an oral presentation of my heritage impact investigation and conclusions. There were no objections from the committee regarding my conclusions, as outlined in Section 12.1.3, above.

The architect, Chris Wallace, presented drawings of the design options. Options A and B, both newly-constructed houses, were not well received by the committee. As a result, they are not shown here.

Option C, consisting of substantial alteration and additions to the existing house was viewed more favourably. A front elevation is shown below. Further discussion led the owner and the architect to believe that a new house, designed to be similar to the addition and alteration proposal of Option C, would be acceptable to the committee. On that basis they prepared designs for a second meeting with the Village committee.



13.2 Second, New Building

On the basis of an apparently favourable response to Option C, the architect and client prepared revisions for a second meeting with the Village committee, which was held at the Meadowvale Community Hall on 15 November 2013. The architect brought a revised proposal, called Option K, the front elevation of which is shown below. Before the drawings were presented, Elaine Eigl, the City's Heritage Co-ordinator, stated that she could not support any demolition proposal, and that the project should be done as an addition and alteration to the existing house. The owners felt that they had no choice but to proceed on that basis, and they instructed the architect to begin work on such a proposal.



Figure 26. New-build proposal from 15 November 2013. Made moot before it was presented.

13.3 Current Proposal Scheme 1, Additions and Alterations to Existing

A revised proposal, involving additions and alterations to the existing house was presented to a third meeting of the Village committee, held at the home of the Chair, on 10 January 2014. The proposal was well-received by the committee, and there were subsequent comments from the City's Heritage Co-ordinator, a number of which were incorporated in further minor revisions. Drawings included in the body of this report are those that bear on the public-realm aspects of the proposal. The architect's full drawing set and area and building material data are found in an appendix.



1050 OLD DERRY ROAD, MEADOWVALE VILLAGE : PROPOSED RENOVATION/ADDITION- FINAL SCHEME 1 - NORTH ELEVATION
CHRISTOPHER WALLACE ARCHITECT Scale 1 : 75 April 9, 2014

Figure 27. Renovation proposal, April 2014. Front Elevation



1050 OLD DERRY ROAD, MEADOWVALE VILLAGE : PROPOSED RENOVATION/ADDITION- FINAL SCHEME 1 - WEST ELEVATION
CHRISTOPHER WALLACE ARCHITECT Scale 1 : 75 April 9, 2014

Figure 28. Renovation proposal, April 2014. West Elevation

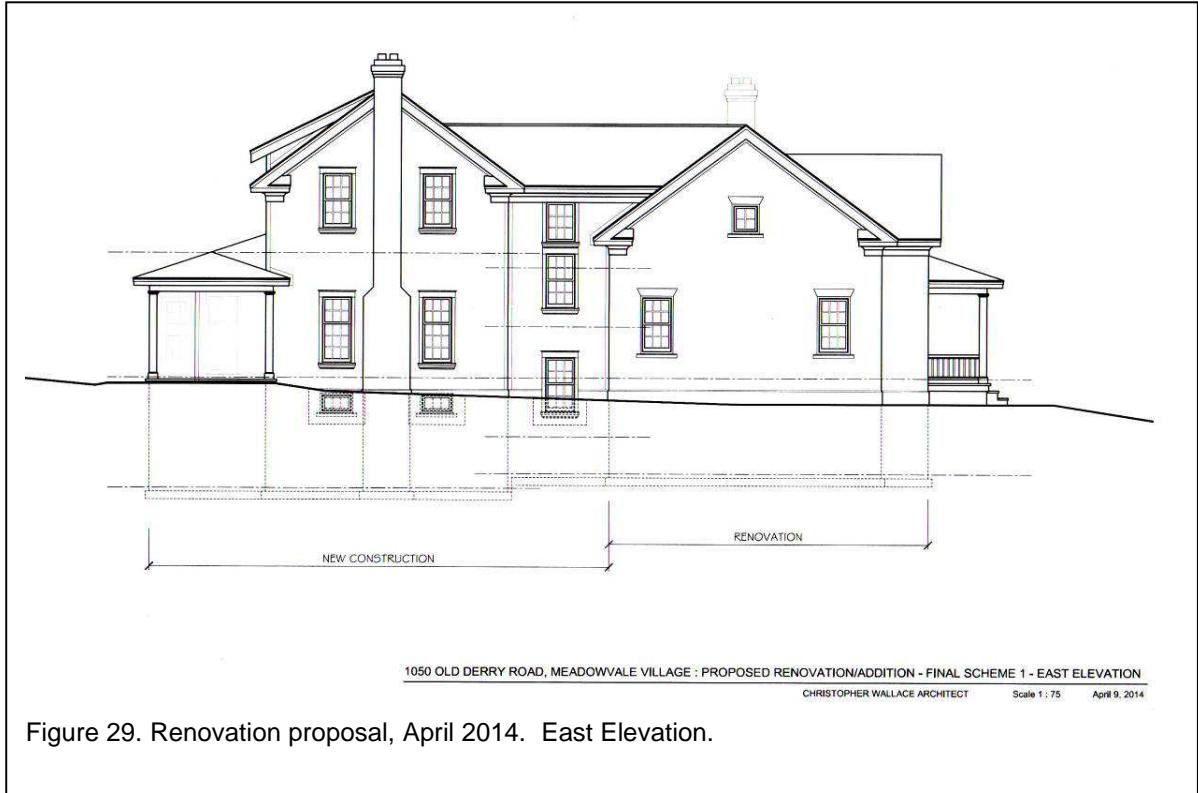


Figure 29. Renovation proposal, April 2014. East Elevation.

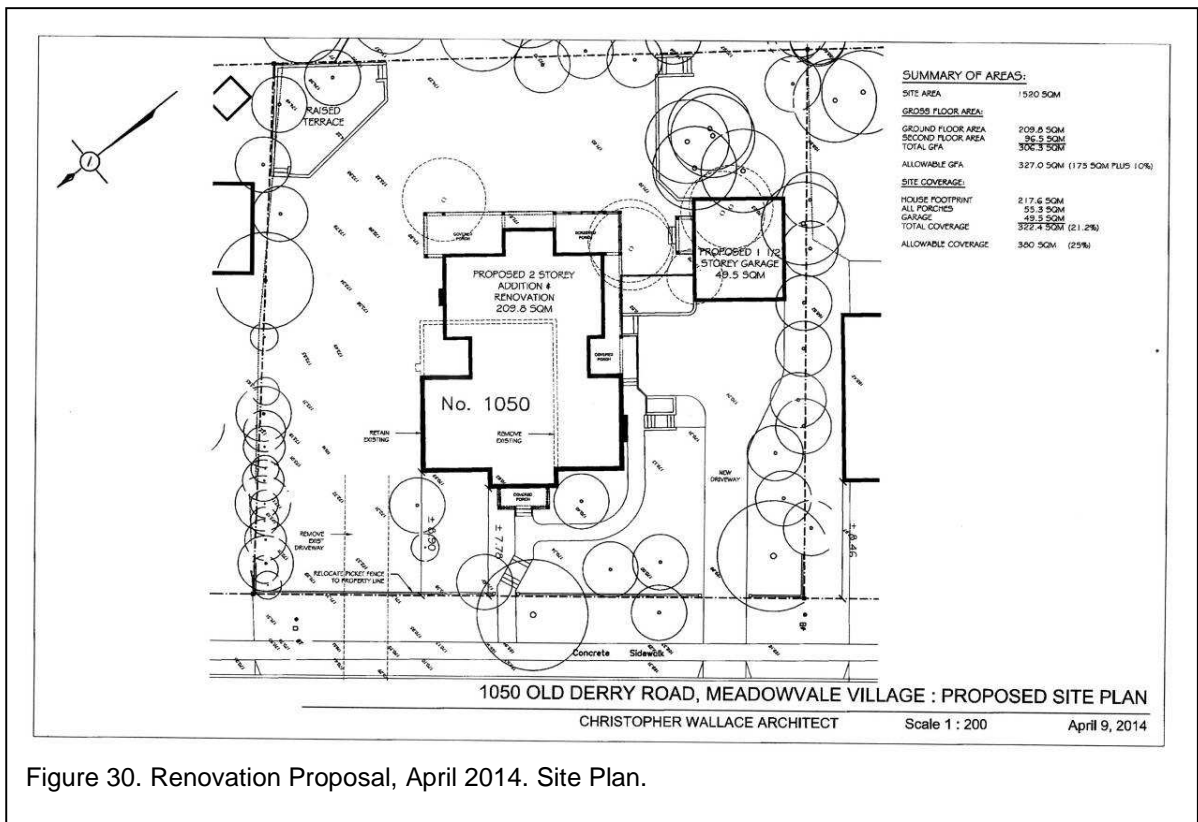


Figure 30. Renovation Proposal, April 2014. Site Plan.



Figure 31. Renovation proposal, April 2014. Streetscape site plan. Open spaces between buildings form a consistent pattern.

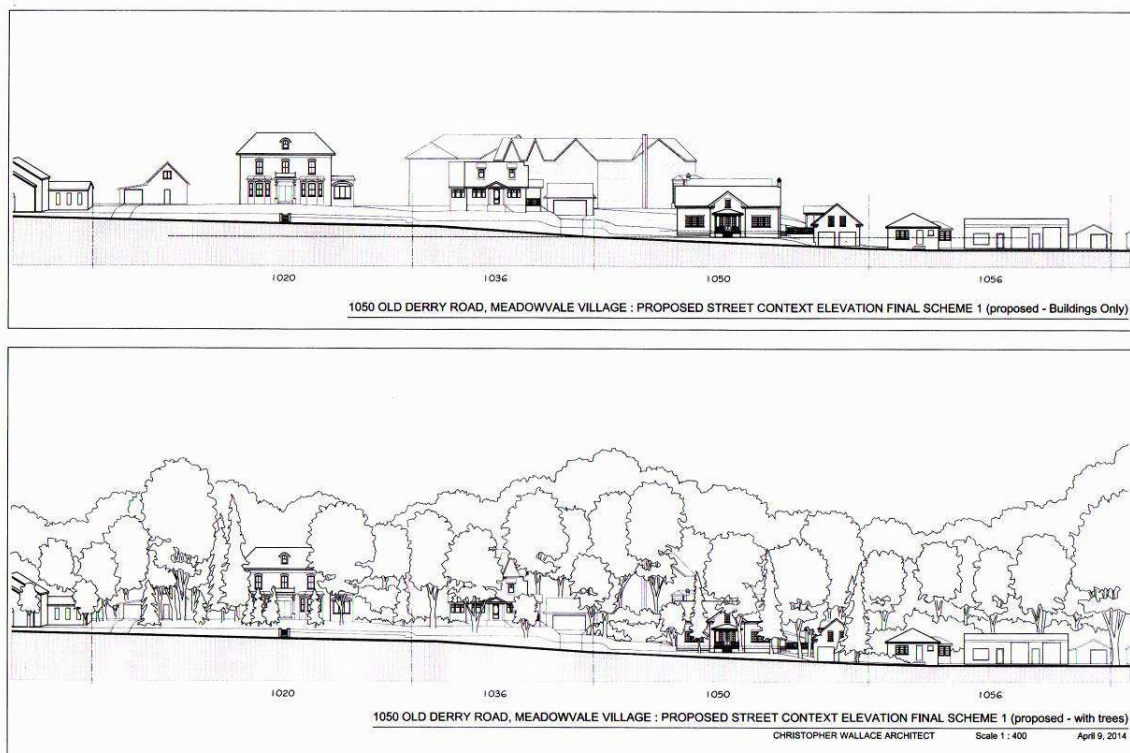
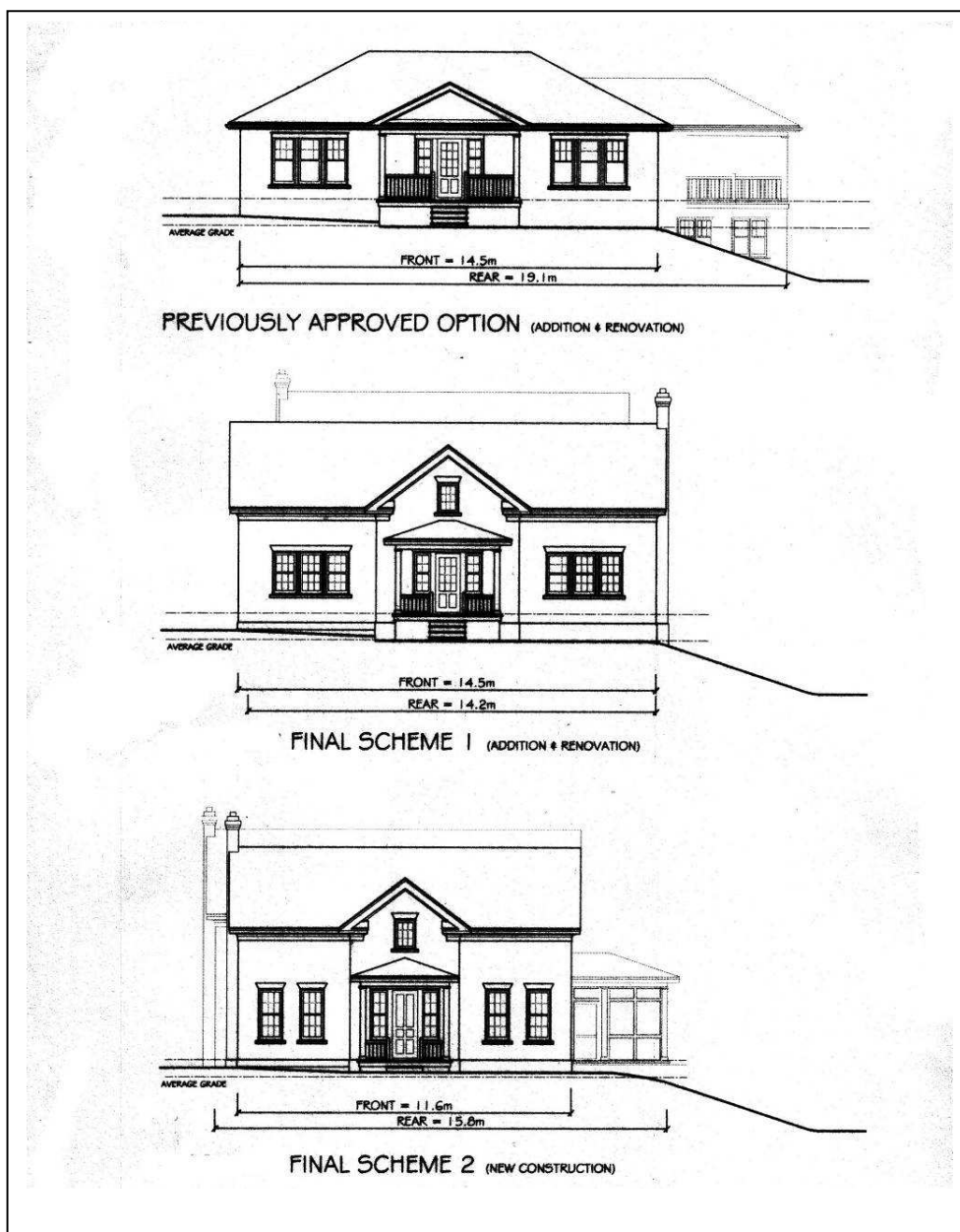


Figure 32. Renovation proposal, April 2014. Streetscape views, without trees (above) and with trees (below). Monster homes behind are shown in the top view.

13.4 Current Proposal Scheme 2, New Construction

The owner and architect have also prepared a proposal for a new building, based on the design of Option 1, but taking advantage of the liberty afforded by not dealing with the existing plan and footprint. As a result, the width of the front façade is reduced by 2.9 metres, making it close to the average width of historic houses in the District. The west side yard is substantially increased, and the east side yard is reduced, which provides improved screening of the HUSH monster homes to the rear. Drawings included in the body of this report are those that bear on the public-realm aspects of the proposal. The architect's full drawing set and area and building material data are found in an appendix. The first drawing, below, shows comparative front elevations of the addition and alteration approved in 2003, and the two current proposals, Options 1 and 2. The decreased width and increased west side yard of Option 2 is readily visible.



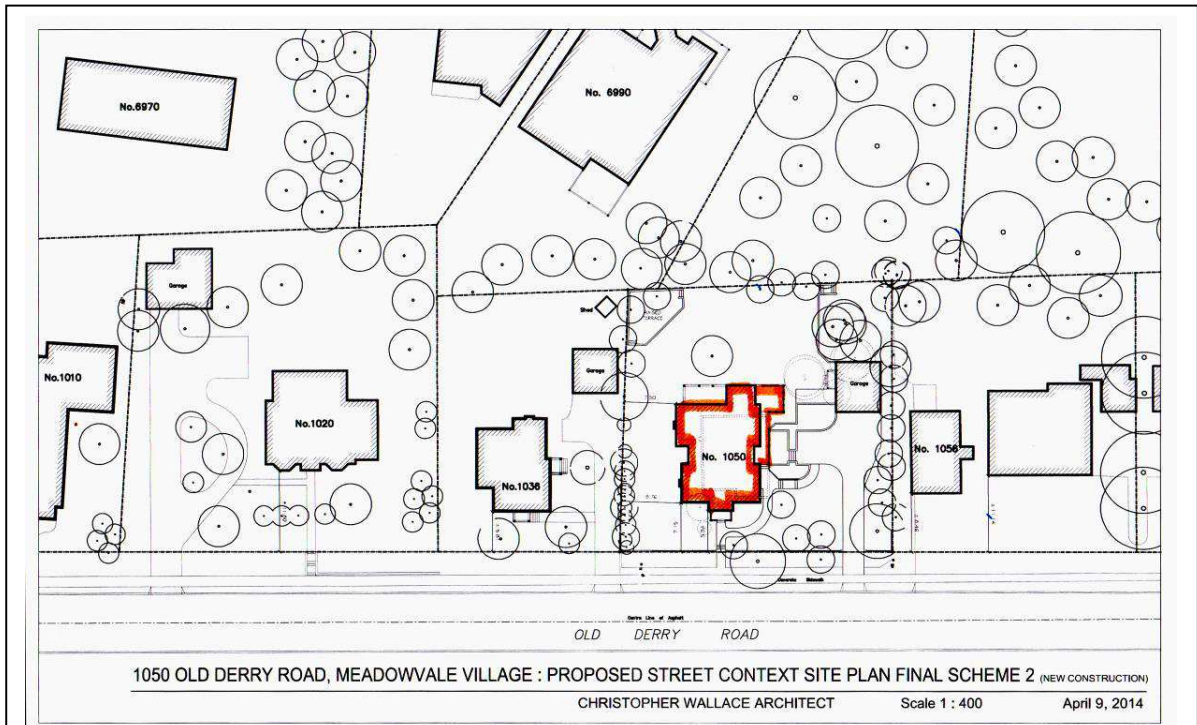


Figure 31. New-build proposal, April 2014. Streetscape site plan. Open spaces between buildings form a consistent pattern.



Figure 32. Comparison of Scheme 1 (renovation) above, with Scheme 2 (new-build) below.

14. Conclusions

14.1 The Village Character Conserved

A Heritage Conservation District is a place that is distinguished from its surroundings by virtue of its heritage character. The purpose of District designation is not to prevent change, but to enable municipalities to “manage and guide future change...for the conservation, protection, and enhancement of the area’s special character,” as stated on the opening page of the Toolkit book on Heritage Conservation Districts.

The 2003 and 2012 Meadowvale Village HCD documents each describe the character of the village. Both of these documents are clear that the character of Meadowvale Village, the character that is to be conserved, was established by its 19th century development. It is fair to say that the Meadowvale Village HCD has achieved this goal of managing change while preserving its special character.

14.2 Special Character of Old Derry Road

The character of Old Derry Road, and its history, are distinct from that of the area to the north, where the lots and buildings shown on the Bristow Survey largely survive in place. Old Derry Road is also anomalous to the Village character described in the 2012 Guidelines, in having curbs and a sidewalk, as well as fenced front yards at the subject property and the Hotel across the street.

In the stretch of about 200 metres between the Graham-Pearson House at No. 1020, and the house at No. 1074 there are no existing 19th century buildings at all, and the lot layouts are more recent. As seen in Figures 4 and 5, the 19th century uses were commercial and industrial buildings of a fairly roughshod character, and the subsequent changes have generally improved the fit with the old residential Village.

14.3 The Proposed Development at 1050 Old Derry Road

In my professional opinion, the proposed development for the property at 1050 Old Derry Road in the Meadowvale Village Heritage Conservation District merits approval. It conserves the heritage character of the District, in the context of its location on Old Derry Road.

- The spatial character as viewed from the street, with generous side yards, is conserved.
- The garage is set well back, and will be screened the public realm by the house and west lot line planting, except when looking directly down the driveway.
- The character of the landscaping is conserved, with retention of all front yard trees. The front fencing, also found on the Hotel across the street, is a part of the character of Old Derry Road and is to be conserved.
- The scale and form of the proposal is in keeping with what is found in the District—both 19th century homes with rear additions, and new homes that mimic that form. The front element is 1 ½ storeys and of a width similar to many historical houses in the District
- The architectural style and materiality of the proposal is compatible with historic styles found in the District.

- The proposal produces a sustainable use as a modern single-family dwelling.
- The lengthy consultation with the Village committee merits respect.

14.4 A Note on Non-heritage Buildings in Heritage Conservation Districts

As noted above, the purpose of a Heritage Conservation District is to manage change so that it enhances, rather than detracts from, the heritage character of a District. Generally speaking, identified heritage buildings will be preserved, and any additions or alterations to them will be sympathetic to their design. Non-heritage buildings may be replaced with sympathetic buildings, or may have additions or alterations.

In the nine HCD Plans I've produced with Phillip Carter, owners have been given two approaches for additions and alterations to non-heritage buildings, at their option:

- 1) Contemporary Alteration, that makes changes consistent with the original style, and
- 2) Historical Conversion, where the alterations convert the appearance to one consistent with the character of heritage buildings in the District. The 2003 proposal is an example of the first method, and the presented Scheme 1 is an example of the second method. It converts the architectural style of the building from a 20th century bungalow to a 19th century 1 ½ storey house.

Scheme 2 is a new building that derives from the design of Scheme 1, arrived at after lengthy consultation with the Village committee, and differing from it in ways that benefit the heritage character of the streetscape, in my professional opinion.

- The front façade is narrower, making it more consistent with the average heritage house in the District.
- The west side yard is more generous, preserving the existing setback to that side.
- The east side yard is slightly narrower, and with the rear extension will provide better masking of the large HUSH houses to the rear.
- These benefits derive from the ability to break free of the existing footprint that new construction provides. In my opinion, it makes sense to take the opportunity.
- In my professional opinion, Scheme 2 is consistent with the conservation of the heritage character of the District.

15. Bibliography

City of Mississauga: Conservation Principles and Design Guidelines for the Meadowvale Village Heritage Conservation District 2003

City of Mississauga: Meadowvale Village Heritage Conservation District Plan, Property Inventory, Appendix A, 2013 Draft

City of Mississauga: Phase 1 Report Background (Draft) Meadowvale Village Heritage Conservation District Plan Review, 2012

City of Mississauga: Meadowvale Village Heritage Conservation District Plan Review, Draft Design Guidelines and Policies, 2013

Ontario: Ontario Heritage Act, R.S.O. 1990, Chapter O. 18, as amended.

Ontario Ministry of Housing and Municipal Affairs. Provincial Policy Statement 2005, Toronto: 2005

Ontario Ministry of Tourism, Culture and Sport: Ontario Heritage Toolkit, Heritage Conservation Districts, Queen's Printer for Ontario, 2006.

Parks Canada. Standards and Guidelines for the Conservation of Historic Places in Canada, second edition. Queen's Printer, 2010

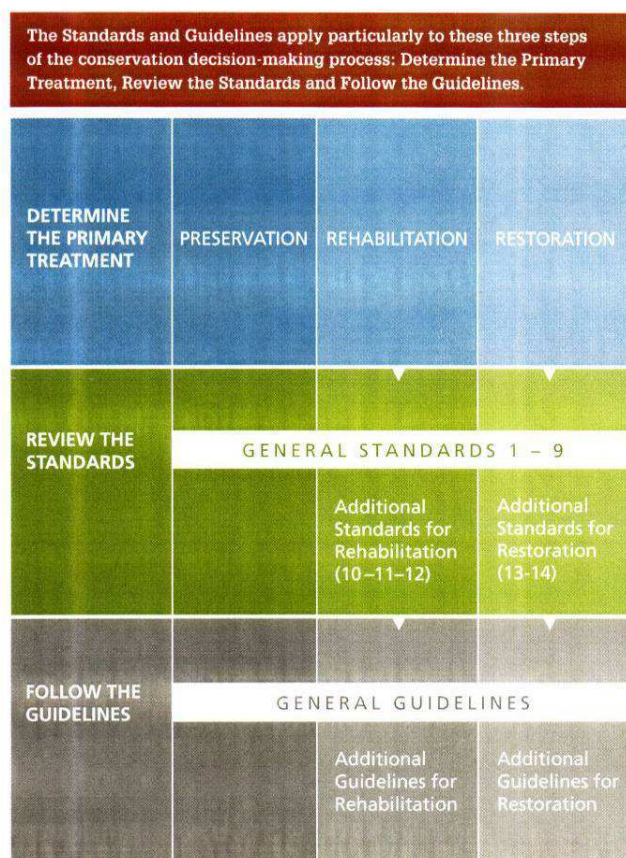
Ontario Department of Mines, Annual Report, Vol 39, 1930:

Smith, L. W. and L. W. WOOD, History of Lumber Yard Size Standards, Forest Products Laboratory, Forest Service, U.S. Department of Agriculture, Sept 1964

Appendix: Project Conservation Principles

The conservation approach for the Additions and Alterations to 1050 Old Derry Road relies on *Standards and Guidelines for the Conservation of Historic Places in Canada*, published by Parks Canada—hereinafter referred to as *Standards and Guidelines*. Briefly stated, it provides guidance for planning and executing conservation projects on identified historic places.

The chart below, from the introduction to the document, shows how it is to be used.



Identify Heritage Value and Character-Defining Elements:

As described in Sections 6 through 12, above, the existing house at 1050 Old Derry Road has minimal individual heritage value. It does not meet the threshold for designation under the Part IV of the Ontario Heritage Act. It does not reflect the defining architectural styles described in the Heritage Conservation Plans. However it does represent the transition of the Village from its Victorian origins to its modern manifestation, and is contemporary with some significant buildings, such as the store. Its modest scale and its rich landscaping are in keeping with, and contribute to, the historic character of the Village. The visual scale of the house and the quality of the landscaping are the Character-Defining Elements that are worthy of conservation.

Maintain or Select and Appropriate and Sustainable Use:

Maintenance of use as a single-family dwelling is sustainable within the context of the Meadowvale Village Heritage Conservation District.

Determine the Primary Treatment:

The project is to upgrade and enlarge a single-family dwelling to make it suitable for continuing and sustainable use. The primary treatment will therefore be considered a Rehabilitation project, under the terms of the *Standards and Guidelines*.

Review Standards:

Within the context of the Meadowvale Village Heritage Conservation District, the proposal conserves the Character-Defining Elements of the subject property. In maintaining the visual building scale, respecting historic building styles, and conserving significant landscape elements, the proposal will sustainably conserve and enhance the heritage character of the District

Balancing Other Considerations:

The second edition of the Standards and Guidelines has removed the Balancing Other Considerations section. It is still worth noting that upgrading the existing 90-year old house will provide long-term benefits in terms of both energy efficiency, and longevity. The most important factor in conservation is finding a sustainable and long-term use.

PAUL OBERST, OAA, B.Arch, CAHP
CURRICULUM VITAE

EDUCATION

1970 B. ARCH (WITH DISTINCTION) University of Michigan

PROFESSIONAL HISTORY

1993 – Present	Paul Oberst Architect, Principal
1995-Present	Consultant to: Phillip H. Carter Architect
1993-1995	Consultant to: R. E. Barnett Architect
1989 - 1993	Designer, Gordon Cheney Architect Inc.
1984 - 1989	Paul Oberst Design, Principal
1981-1984	Designer Lloyd Alter Architect
1973-1981	Major Works Building, Principal

AWARDS

City of Toronto Architecture and Urban Design Awards, 2000. Honourable Mention in Adaptive Re-use, a new category created by the jury for Kensington Lofts.

Canadian Institute of Planners, 2005. Neighbourhood Planning, Honourable Mention for Aurora HCD

Heritage Toronto William H. Greer Award of Excellence, 2013. For Victoria Lofts

SELECTED PROJECT EXPERIENCE:



The Beverley Street project preserved a large Victorian row of 16 houses, maintaining their original use as single-family dwellings. It was nominated for an Ontario Renews Award.

HERITAGE PROJECTS

For Lloyd Alter Architect

Contact Lloyd Alter, 416-656-8683

Beverley Street Row, Toronto,
Renovation and preservation, 1982

This project was part of the redevelopment of a largely vacant city block. The developer chose to preserve this 16-house Victorian row, an enlightened attitude for the time.

Mr. Oberst worked on several of the houses in the project, with responsibilities including design, construction documents, and field review .

McCabe Houses, 174-178 St. George Street, Toronto
restoration for adaptive re-use, 1982

Mr. Oberst assisted in working drawings and field review.

For Lloyd Alter Architect

Fulton-Vanderburgh House, Richmond Hill,
exterior restoration, 1984

This project was part of a development agreement for farmland south of Richmond Hill. CAPHC member David Fayle was the LACAC liaison.

Mr. Oberst handled the project, having full responsibility for design, construction documents, and field review.



The Fulton-Vanderburgh House in Richmond Hill, after its restoration. Built around 1810, this is the oldest house in York Region



Woodstock Public Library. Phillip Carter's project combined sensitive alterations and an addition with the restoration of one of Ontario's finest Carnegie libraries.

For Phillip H. Carter Architect and Planner
Contact Phillip Carter, 416-504-6497
Woodstock Public Library,
Restoration, addition, and renovations, 1996

Mr. Oberst assisted in the production of working drawings and wrote the specifications.

Port Hope Public Library, restoration, addition and renovations, 2000

Mr. Oberst wrote the specifications.



Setting back the third-floor addition allowed the restored bank building to retain its street presence, and maintain the detail significance of the cornice and entry-bay decoration. Preservation Services provided oversight for work under the façade improvement program.

For Paul Oberst Architect

The Dominion Bank

2945 Dundas Street W., Toronto

Restoration, addition, and renovation, 2002

This 1915 bank by John M. Lyle Architect was converted to a commercial residential building with a penthouse addition, set back 2.3m from the building line, and following the curve of the façade.

The original structure was restored under a local façade improvement program, including cleaning and installation of replacement 1-over-1 double hung windows on the second floor.

Medland Lofts

2925 Dundas Street W., Toronto

Restoration, addition, and renovation, 2005

This Art Deco building was in extreme disrepair following an uncompleted renovation. The completed project provided 10 residential and 3 commercial condominium units. It contributes to the revitalization of the Junction commercial area.



This building has a set-back addition similar to the one at the Dominion Bank across the street. In this case the penthouse has a Moderne design, reflecting the Art Deco style of the original building.



Balconies behind the original arches double the window area to meet the requirements of residential use, without cutting new openings in the historic masonry structure.

For Paul Oberst Architect

Victoria Lofts

152 Annette Street, Toronto

Residential Conversion,
Occupied 2011

The 1890 Victoria-Royce Presbyterian Church was designed by Knox and Elliot, who were also the architects for the Confederation Life building on Yonge Street. In 2005, the parish ceased operation, no longer having sufficient members to maintain this large and important heritage building.

The project preserves and restore the building envelope and many of the interior features, and will provide 34 residential condominiums.

Significant elements that were not used in the project, like the 1908 Casavant organ, and the enormous stained glass windows have been preserved intact in new homes at other churches.

This project received the William H. Greer Award of Excellence at the Heritage Toronto Awards 2013.

HERITAGE DISTRICTS

In association with Phillip H. Carter Architect and Planner

Collingwood Downtown Heritage Conservation District Study and Plan, 2001-2002

Kleinburg-Nashville Heritage Conservation District Study and Plan, 2002-2003

Old Burlington Village Heritage Conservation District Study, 2004-2005. Resulted in our Urban Design Guidelines for the downtown.

Northeast Old Aurora Heritage Conservation District Study and Plan, 2005-2006. Received Honourable Mention (2nd place nationally) in the Neighbourhood Plans category—Canadian Institute of Planning, 2007.

Village of Maple Heritage Conservation District Study and Plan, 2006-2007.

Buttonville Heritage Conservation District Study and Plan, underway.

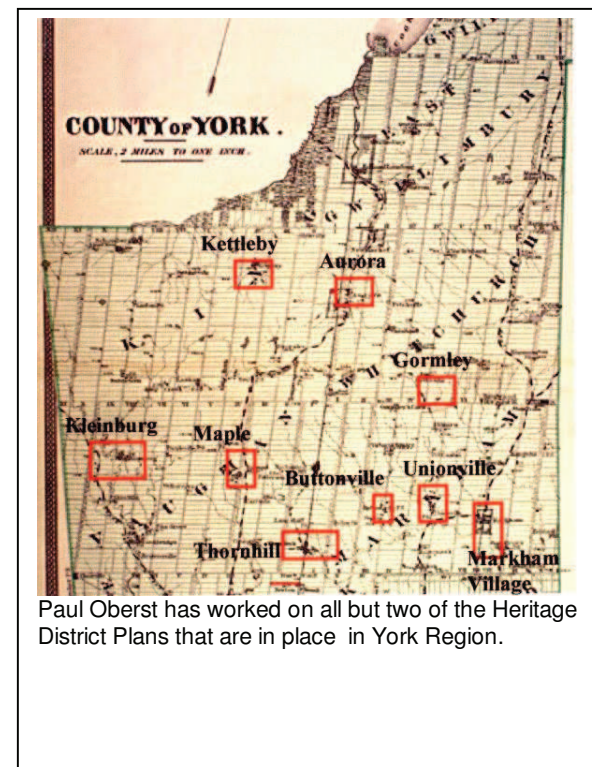
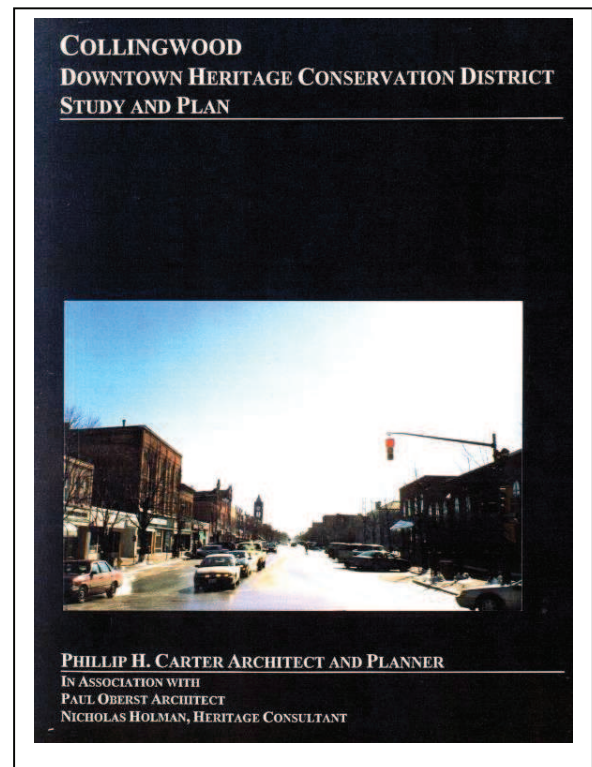
Thornhill Markham Heritage Conservation District Study and Plan, 2007.

Thornhill Vaughan Heritage Conservation District Study and Plan, 2007.

Gormley Heritage Conservation District Study and Plan, 2008

Kettleby Heritage Conservation District Study and Plan, suspended by Council.

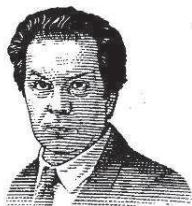
Pickering Village Heritage Conservation District Study and Plan, 2013 Council.



Paul Oberst has worked on all but two of the Heritage District Plans that are in place in York Region.

Talk of preserving heritage a façade

Zoning change to Spadina will raze historic buildings



JOE FIORITO
City columnist

Walk slowly up Spadina, from Dundas to College; it doesn't take much time. Get yourself some barbecued duck, a bowl of pho, a bag of star fruit and keep walking. Look closely at the buildings as you walk.

Under decades of neglect and

cheap signage you will see a streetscape of handsome bay windows, latticed brick and old stone cornices — the remnants of Victorian Toronto.

Now listen carefully: Hidden in the noise of traffic, there is another, softer sound — landlords rubbing their hands, lawyers drafting agreements, wrecking crews licking their chops.

Final arguments over a zoning change to Spadina Avenue have just been heard by the Ontario Municipal Board; if the change is approved, those old buildings may topple like dominoes.

To boil it down: Certain developers want a change in zoning to allow an increase in the density of the street, which would make the narrow little properties on Spadina more valuable as real estate, which in turn would make it easy for somebody with the cash to bundle up several of the narrow lots into larger parcels for redevelopment — i.e., demolition.

City council has approved the

change, in spite of staff recommendations to the contrary. Perhaps council believes a denser, post-development Spadina will still look like Spadina, only better, healthier, more vibrant; and perhaps one day the fire hydrants will be filled with cherry Kool-Aid.

It could happen, but it ain't likely.

I went to the offices of the OMB last Friday, to listen as the opposition made its case.

I was won over.

Architect and neighbourhood resident Paul Oberst led the arguments by calling a planner for the city, Suzanne Pringle. She explained that if the density along that strip of Spadina were increased, it would make it easy to assemble small lots. However, in order to achieve the proposed density, it would be necessary to demolish what's there and rebuild.

Next, heritage expert Marcia Cuthbert testified that there are a number of architecturally significant buildings along this stretch of Spadina that ought to be considered historic properties. They have not yet been designated as such, partly because the heritage board is overworked, and partly because of delays caused by amalgamation.

See SPADINA on Page A17

Better safe than sorry?

SPADINA

Continued from Page A16

Among the more notable buildings are: the Standard, a Yiddish theatre that morphed into the Victory Burlesque which in turn became the Golden Harvest Chinese Theatre, at 285 Spadina; Grossman's Tavern, at 377; the house where Red Emma Goldman lay in state upon her death, at 346; and Broadway Hall, once used by the Women's Christian Temperance Union, at 450 Spadina.

Margie Zeidler, one of the few developers in town who preserves heritage buildings for a living, noted that many of the buildings on this stretch of the street were architecturally outstanding, and in reasonable

shape. She said that the economics of preservation were generally equivalent to the economics of demolition.

Cathy Nasmith, another heritage expert, noted that there were whole blocks of Spadina worth preserving — including one stretch of 11 Victorian buildings in a row. When challenged by a lawyer who said that façades could easily be preserved as a part of redevelopment, she observed succinctly, "If there is a heritage scale, with demolition at one end and renovation at the other, I'd say saving a façade is one step up from demolition." In any case, there's no way to enforce the preservation of façades.

And that is a rather brutal summary of a day's worth of arguments and cross-examinations. A couple of questions:

Why is it that when you knock a building down, what goes up in its place is generally uglier? Why don't we force developers to pay a price for zoning changes that give them profits they don't earn? Why, when it comes to Toronto's irreplaceable architectural heritage, don't we take the position that it's better to be safe than sorry? And why don't we follow the example of certain, um, world-class cities — London, Paris, Edinburgh — and encourage preservation?

I'm not one of those guys who thinks old is intrinsically good; neither am I certain that we should erase, deface, or reface our common heritage for the sake of a few bucks.

It's worth noting that the OMB rarely rules against a decision taken by city council, but it has happened. Yes, and the water in the hydrants could run sweet and red.

National Post

CITIZEN ADVOCACY

Mr. Oberst was the "Party", before the Ontario Municipal Board, opposing an application for rezoning and Official Plan Amendment on Spadina Avenue in Toronto in 2001. Rezoning threatened 113 heritage properties on one kilometre of street frontage.

He organized and presented the case to the OMB, with the assistance of residents and many heritage activists.

Joe Fiorito's column, to the left, provides a succinct narration.

Mr. Oberst continues to work on heritage issues in the neighbourhood, being involved in the designation of Kensington Market as a National Historic Site, and the preservation of the historic parish of Saint Stephen-in-the-Fields.

Contact:

Catherine Nasmith
416-598-4144



OTHER ARCHITECTURAL WORK

RESIDENTIAL

Kensington Market Lofts

Condominium Conversion, George Brown College Kensington Campus, \$13,000,000

Design partner, in joint venture with R.E. Barnett Architect.

At the Toronto Architecture and Urban Design Awards 2000 the jury created the new category of 'Adaptive Re-use' to recognize this project and the Roundhouse. Since it was a new category, we received an honourable mention rather than an award.

St John's Lofts

Condominium Conversion, 1 St. John's Road, Toronto, \$1,000,000

Design partner, in joint venture with R.E. Barnett Architect

COMMERCIAL

Retail/Apartment Building, 80 Kensington Avenue, Toronto, \$400,000
Designer for Paul Oberst Architect

Kings Tower, 393 King Street West Toronto, 12 Storey mixed use building, \$10M
Designer for Gordon Cheney Architect Inc

Office Building, 2026 Yonge Street Toronto, 3 Storey mixed use building, \$3M
Designer for Lloyd Alter Architect

THEATRE WORK

Set designer, *A Ride Across Lake Constance*, by Peter Handke
New Theatre, Toronto 1975

Set and Costume designer, *The Curse of the Starving Class*, by Sam Shepard
New Theatre, Toronto, 1979

COMMUNITY WORK

Kensington Market Working Group

-Board Member

1994-97 & 2000-2001.

-Secretary 1994-97.

Kensington Market Action Committee,

-Board Member 2001-2002.

WRITINGS

Founding Editor of *A.S.*

A student architecture journal

University of Michigan, 1968-70

Founding Co-editor of *FILE Magazine*

Toronto, 1972

Originator and author of

Rear Elevation essay series

Toronto Society of Architects Journal, 1994-1996

Author of articles and reviews in:

Globe & Mail,
NOW magazine
File magazine

PUBLICATION OF WORK

Kensington Market Lofts is listed in: *East/West: A Guide to Where People Live In Downtown Toronto*

Edited by Nancy Byrtus, Mark Fram, Michael McClelland. Toronto: Coach House Books, 2000

Class Acts, by John Ota, Toronto Star, May 20, 2001, describes a Kensington unit in the old elementary school.

Urban Arcadia, By Merike Weiler,

City & Country Home, April 1990

Customizing your Condo, by Kathleen M. Smith

Canadian House and Home, October 1989

A Place of Your Own, by Charles Oberdorf and Mechtilde Hoppenrath,

Homemaker's Magazine, November 1980

The Invention of Queen Street West, by Debra Sharpe

The Globe & Mail *Fanfare* section, January 10, 1980

Alternatives, by Charles Oberdorf and Mechtilde Hoppenrath,

Homemaker's Magazine, April 1979

Various accounts, reviews and/or photographs of heritage work, furniture designs, theatre design work, and exhibitions.

FINAL SCHEME – NEW CONSTRUCTION : SUMMARY OF AREAS

SITE AREA	1520 SQM	
<u>GROSS FLOOR AREA:</u>		
GROUND FLOOR AREA	186.8 SQM	
SECOND FLOOR AREA	<u>138.1 SQM</u>	
TOTAL GFA	324.9 SQM	
ALLOWABLE GFA	327.0 SQM	(175 SQM PLUS 10%)
<u>SITE COVERAGE:</u>		
HOUSE FOOTPRINT	186.8 SQM	
ALL PORCHES	51.04 SQM	
GARAGE	<u>49.5 SQM</u>	
TOTAL COVERAGE	287.3 SQM	(18.9%)
ALLOWABLE COVERAGE	380 SQM	(25%)

1050 OLD DERRY ROAD: MATERIALS AND COLOURS

Front Wing:

- walls – burnt clay red brick (selected to match village precedents like the church and the Graham house)
- chimney – matching red clay brick
- foundation walls – limestone
- doors – solid wood, contrasting colour
- window sash – light cream solid wood
- window casings, eaves & soffit, porch trim – dark olive wood

Rear Wing and Linking Wing:

- walls – light cream solid wood siding
- chimney – matching red clay brick
- foundation walls – limestone
- doors – light cream solid wood
- window sash – light cream wood
- window casings, eaves & soffit, porch trim – dark olive wood

Roof:

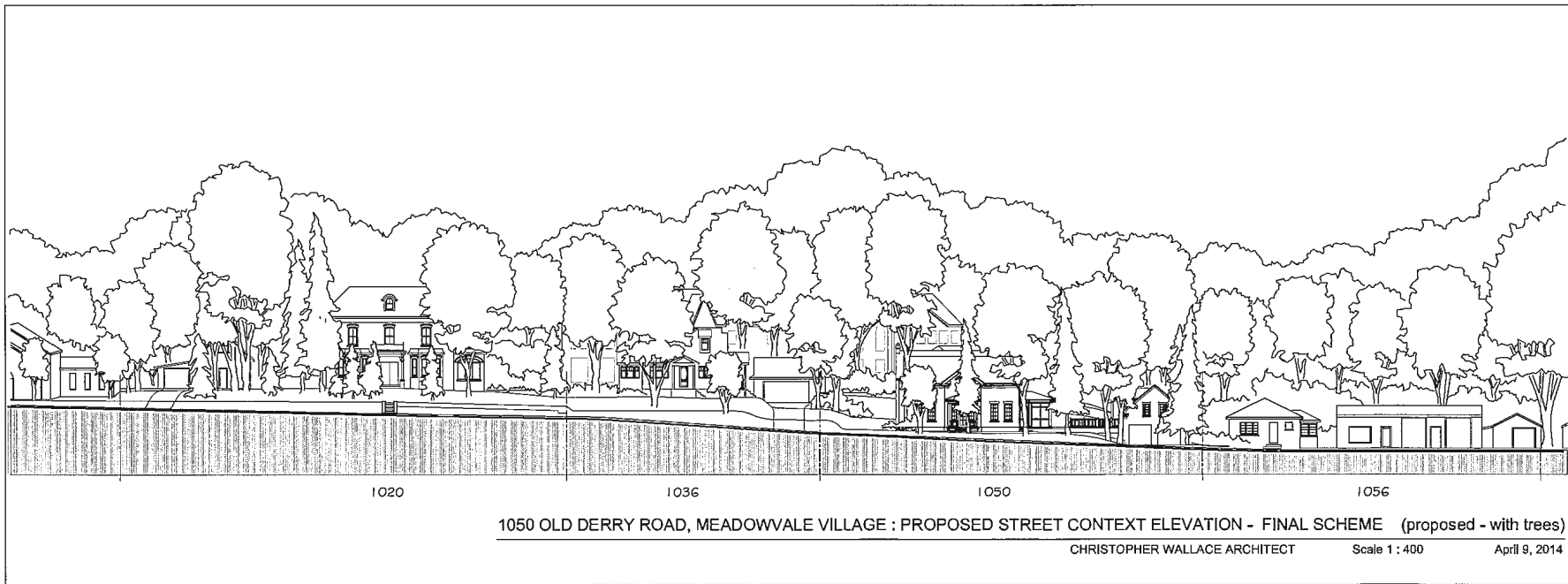
- Dark gray premium asphalt shingles

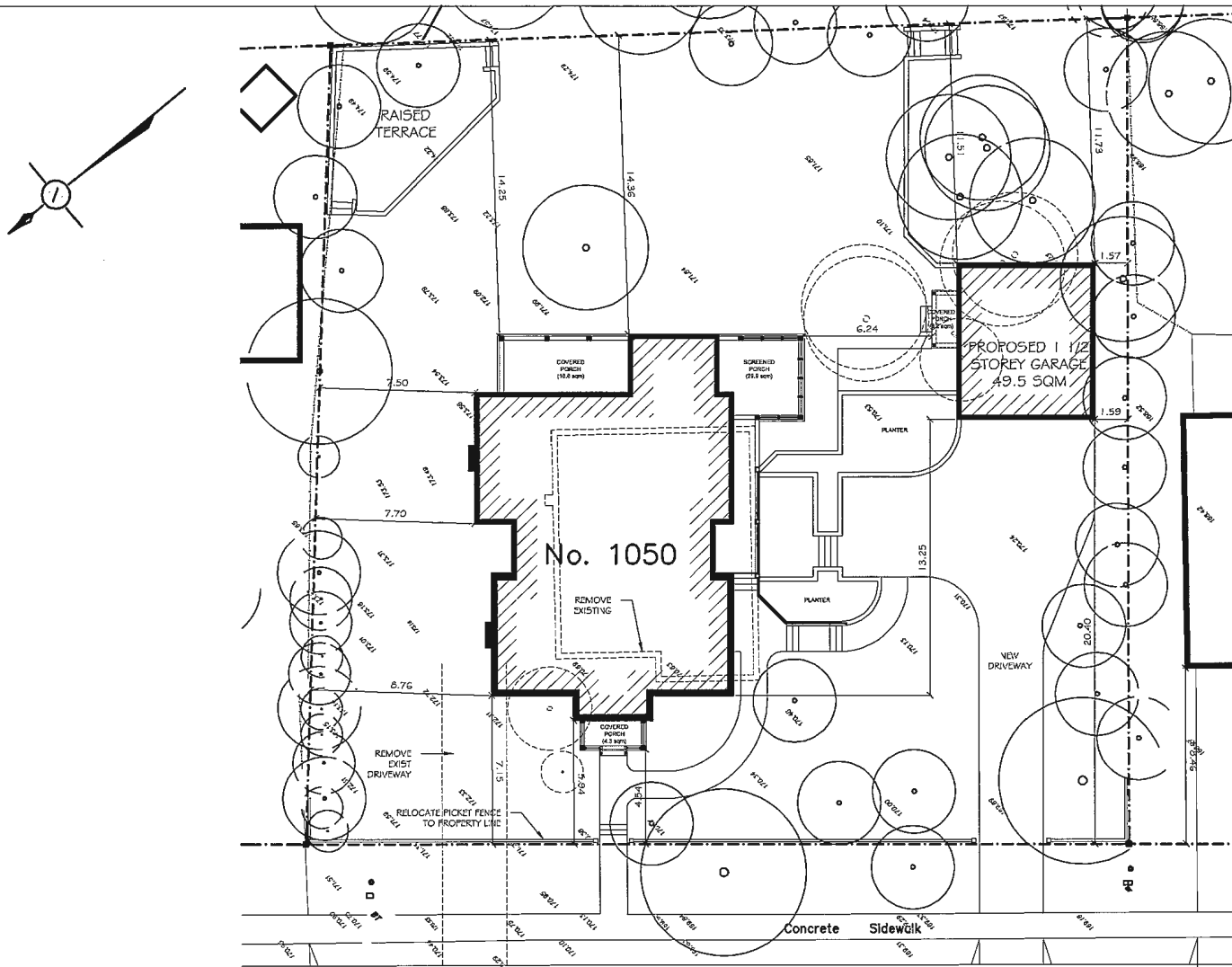
HERITAGE ADVISORY COMMITTEE SUBMISSION

FINAL SCHEME (NEW CONSTRUCTION) FOR : 1050 OLD DERRY ROAD, MEADOWVALE VILLAGE

CHRISTOPHER WALLACE ARCHITECT

April 9, 2014





SUMMARY OF AREAS:

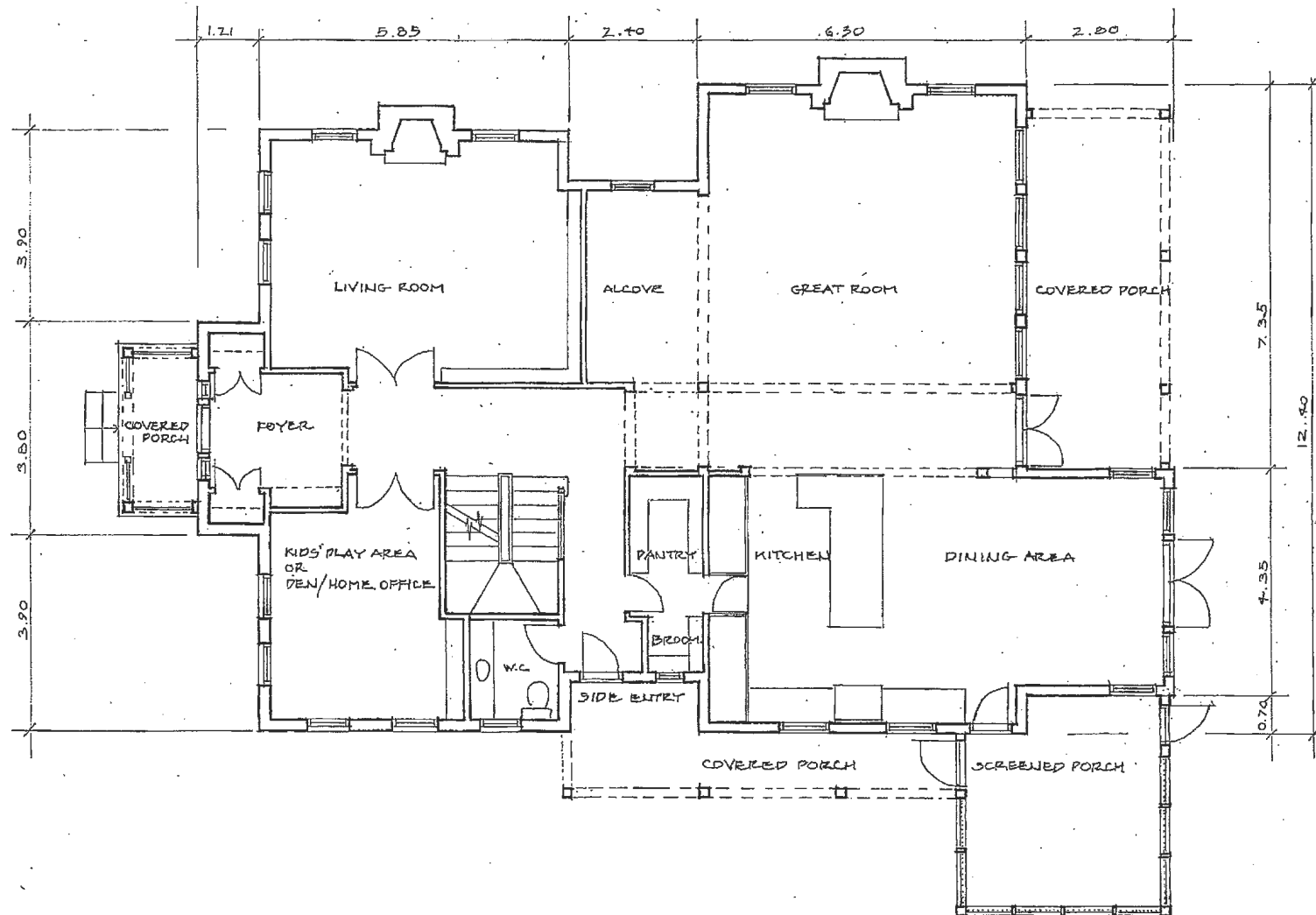
SITE AREA	1520 SQM
GROSS FLOOR AREA:	
GROUND FLOOR AREA	186.8 SQM
SECOND FLOOR AREA	138.1 SQM
TOTAL GFA	324.9 SQM
ALLOWABLE GFA	327.0 SQM (175 SQM PLUS 10%)
SITE COVERAGE:	
HOUSE FOOTPRINT	186.8 SQM
ALL PORCHES	51.04 SQM
GARAGE	49.5 SQM
TOTAL COVERAGE	287.3 SQM (18.9%)
ALLOWABLE COVERAGE	380 SQM (25%)

1050 OLD DERRY ROAD, MEADOWVALE VILLAGE : PROPOSED SITE PLAN - FINAL SCHEME (NEW CONSTRUCTION)

CHRISTOPHER WALLACE ARCHITECT

Scale 1 : 200

April 9, 2014

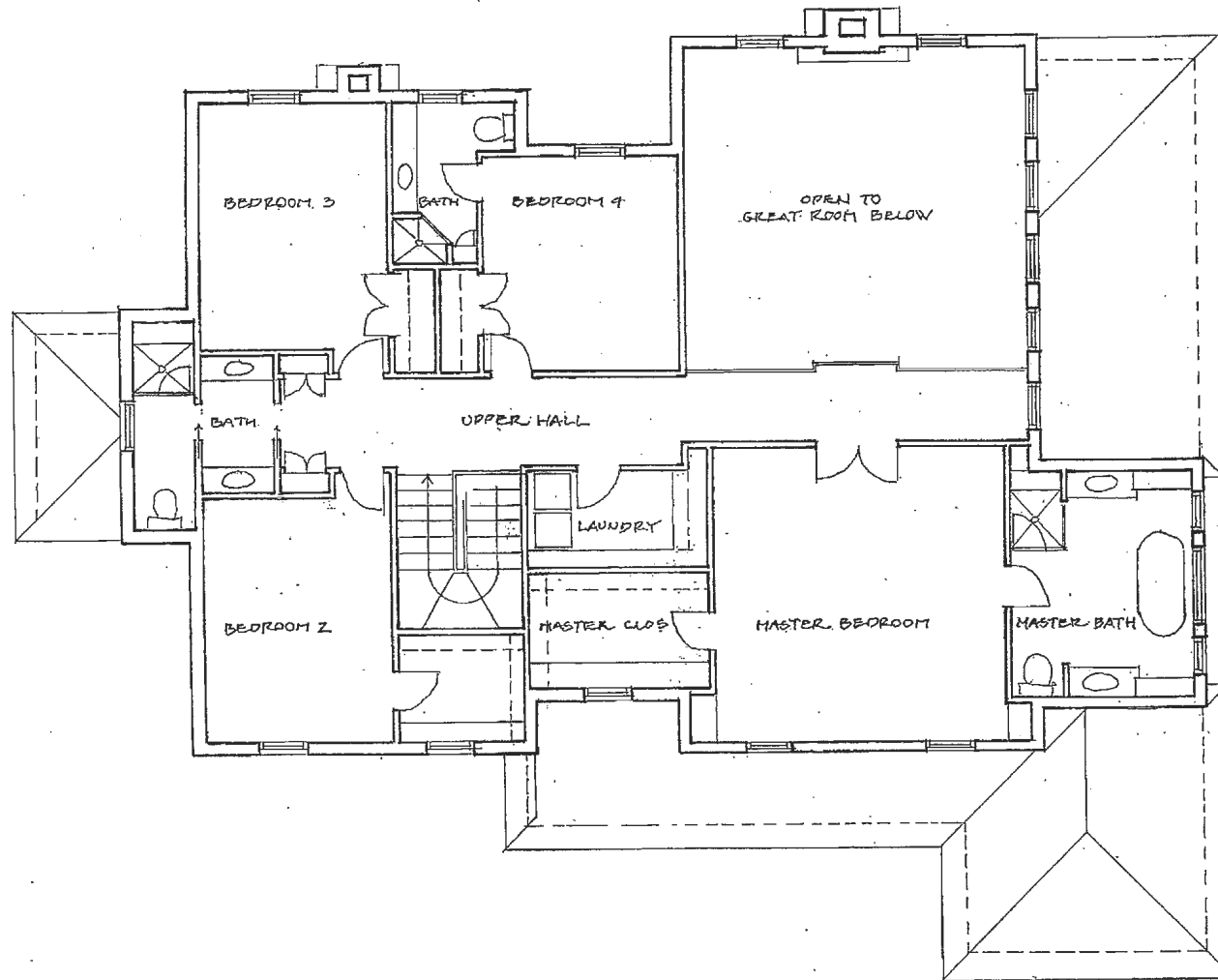


1050 OLD DERRY ROAD, MEADOWVALE VILLAGE: PROPOSED NEW CONSTRUCTION FINAL SCHEME – GROUND FLOOR PLAN

CHRISTOPHER WALLACE ARCHITECT

Scale 1:75

April 9, 2014

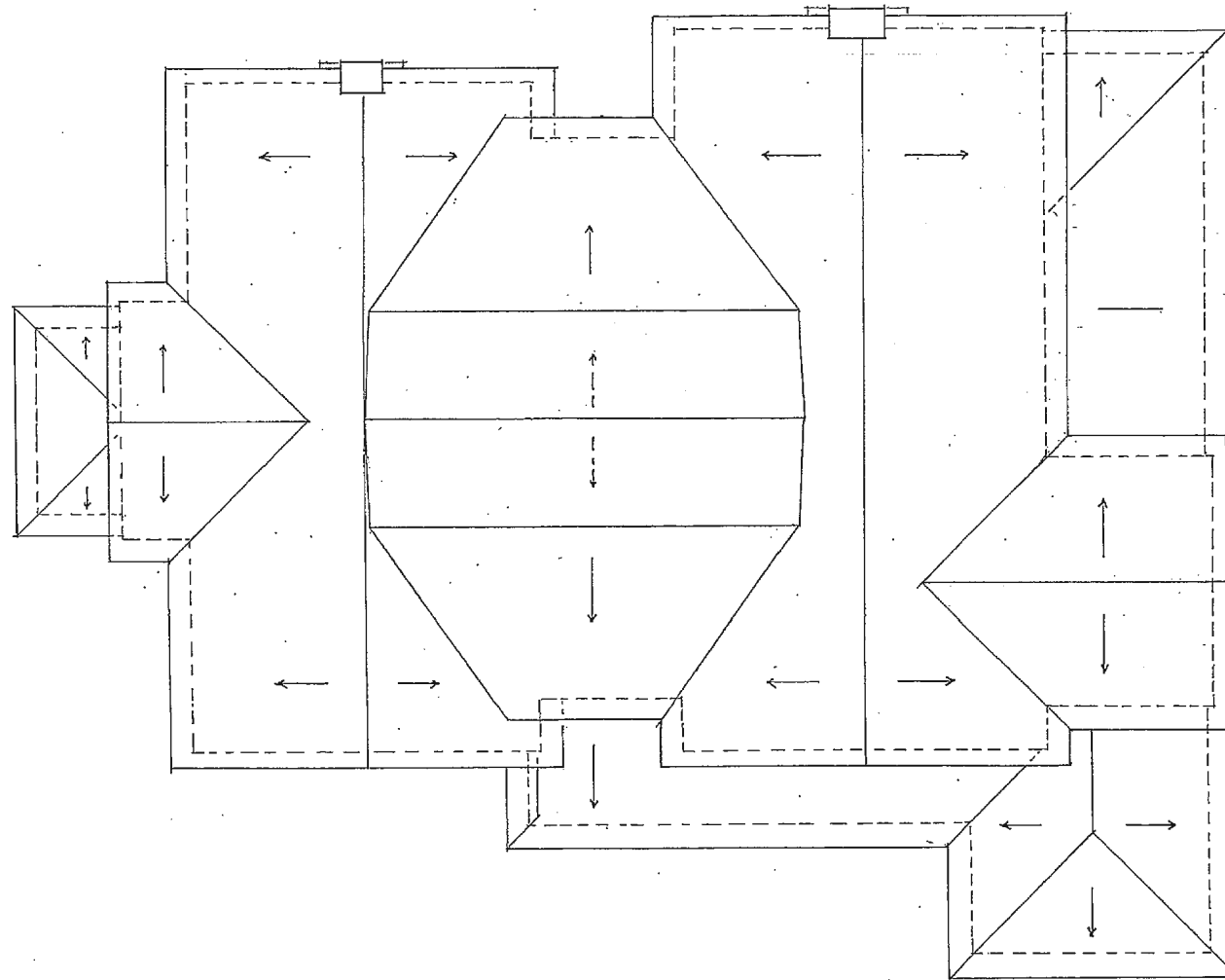


1050 OLD DERRY ROAD, MEADOWVALE VILLAGE: PROPOSED NEW CONSTRUCTION FINAL SCHEME - SECOND FLOOR PLAN

CHRISTOPHER WALLACE ARCHITECT

Scale 1:75

April 9, 2014

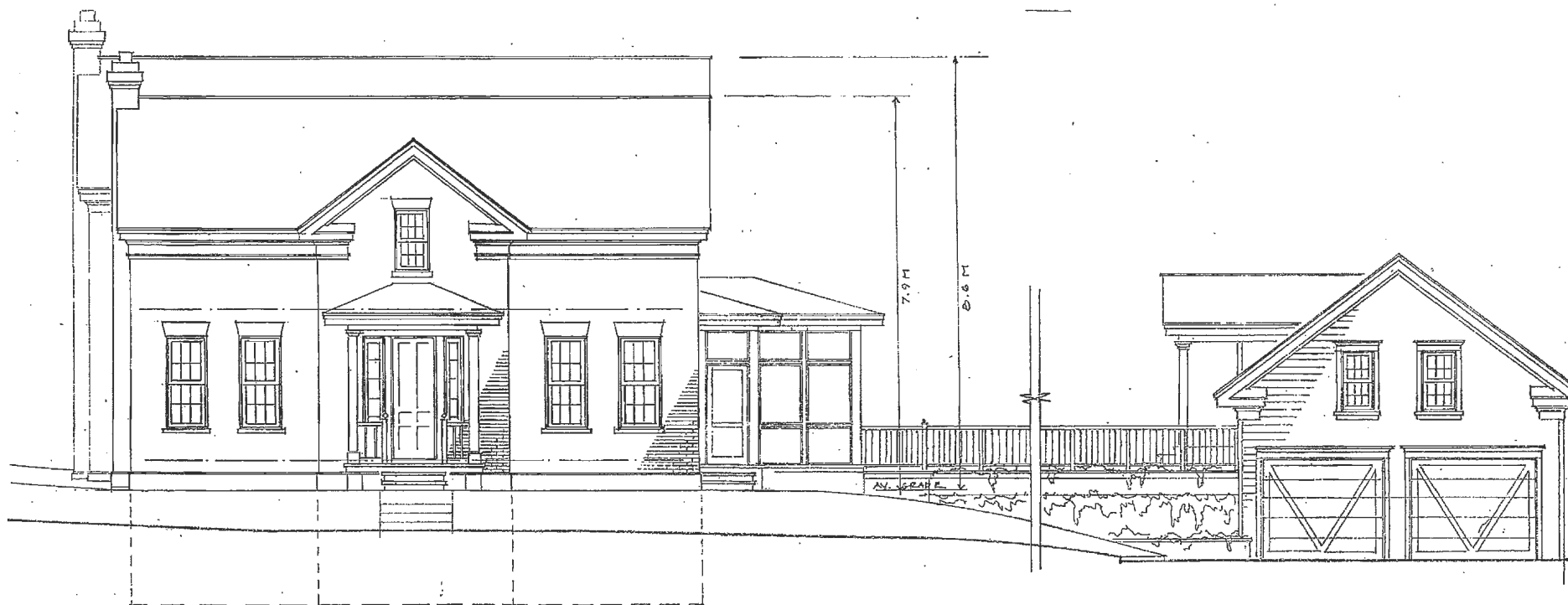


1050 OLD DERRY ROAD, MEADOWVALE VILLAGE: PROPOSED NEW CONSTRUCTION FINAL SCHEME – ROOF PLAN

CHRISTOPHER WALLACE ARCHITECT

Scale 1:75

April 9, 2014



1050 OLD DERRY ROAD, MEADOWVALE VILLAGE: PROPOSED NEW CONSTRUCTION FINAL SCHEME — FRONT (NORTH) ELEVATION

CHRISTOPHER WALLACE ARCHITECT

Scale 1:75

April 9, 2014

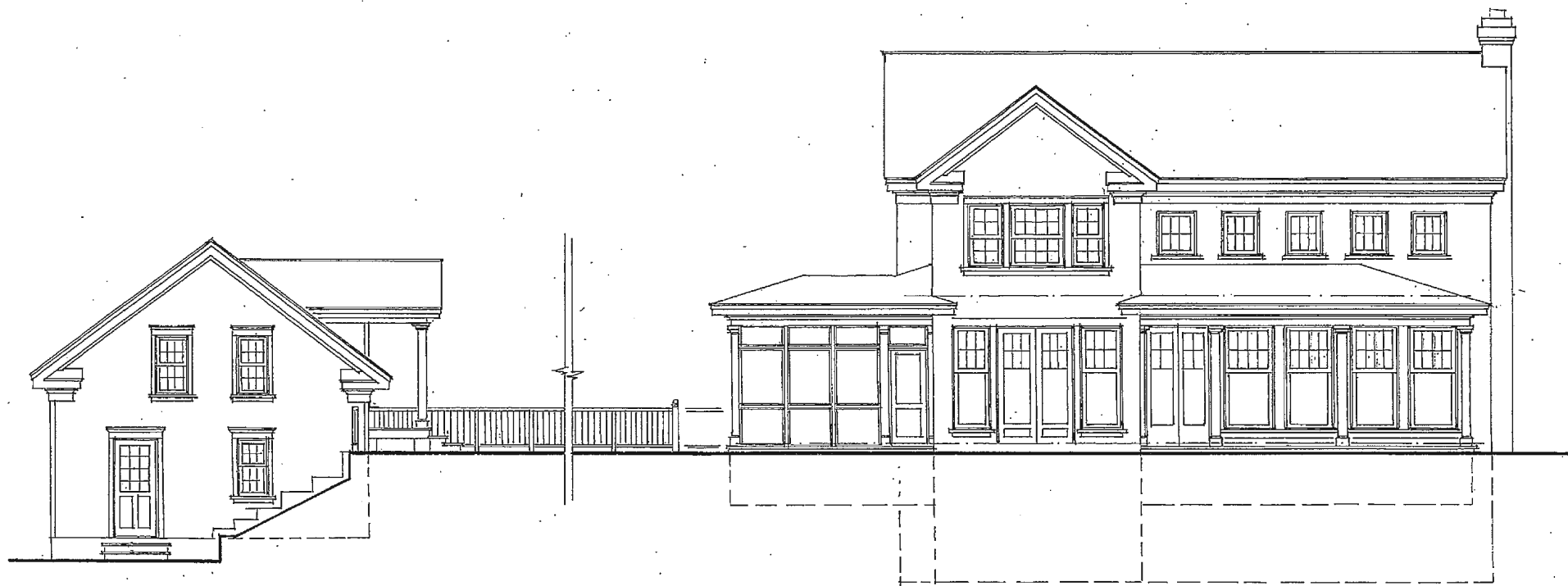


1050 OLD DERRY ROAD, MEADOWVALE VILLAGE: PROPOSED NEW CONSTRUCTION FINAL SCHEME – WEST SIDE ELEVATION

CHRISTOPHER WALLACE ARCHITECT

Scale 1:75

April 9, 2014



1050 OLD DERRY ROAD, MEADOWVALE VILLAGE: PROPOSED NEW CONSTRUCTION FINAL SCHEME — REAR (SOUTH) ELEVATION

CHRISTOPHER WALLACE ARCHITECT

Scale 1:75

April 9, 2014

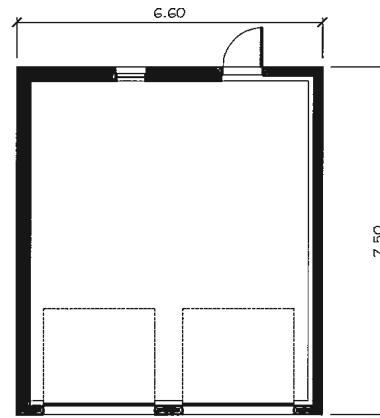


1050 OLD DERRY ROAD, MEADOWVALE VILLAGE: PROPOSED NEW CONSTRUCTION FINAL SCHEME — EAST SIDE ELEVATION

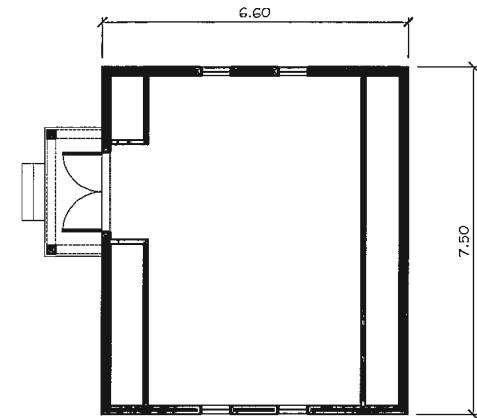
CHRISTOPHER WALLACE ARCHITECT

Scale 1:75

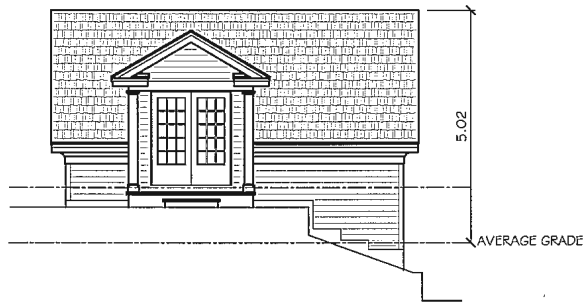
April 9, 2014



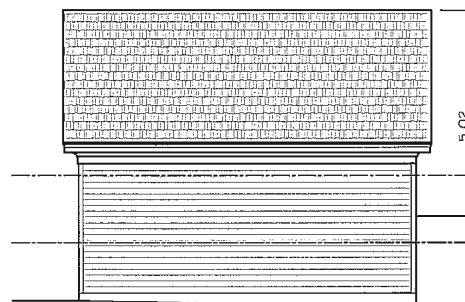
LOWER LEVEL PLAN



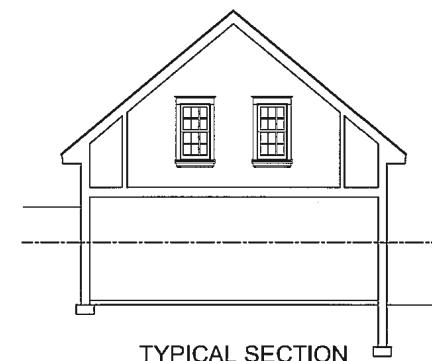
UPPER LEVEL PLAN



EAST ELEVATION



WEST ELEVATION



TYPICAL SECTION

1050 OLD DERRY ROAD, MEADOWVALE VILLAGE : PROPOSED GARAGE & LOFT

CHRISTOPHER WALLACE ARCHITECT

Scale 1 : 100

April 9, 2014



Corporate Report

3-1

Clerk's Files

Originator's
Files

DATE: April 23, 2014

TO: Chair and Members of the Heritage Advisory Committee
Meeting Date: May 20, 2014

FROM: Paul A. Mitcham, P. Eng., MBA
Commissioner of Community Services

SUBJECT: **Request to Demolish Structures on a Heritage Listed Property
6432 Ninth Line
(Ward 10)**

RECOMMENDATION: That the barn and outbuilding structures located on the property at 6432 Ninth Line, which is individually listed on the City's Heritage Register, is not worthy of heritage designation, and consequently, that the owner's request to demolish the structures be approved pending the following conditions as described in the Corporate Report dated April 23, 2014 from the Commissioner of Community Services:

1. The dwelling structure is retained and,
2. That solid wood board hoarding be installed at a distance of five (5) metres around the perimeter of the dwelling structure.

BACKGROUND: The subject property was individually Listed on the Town of Milton's Heritage Register in 2010 as part of the Ninth Line Corridor land transfer to the City of Mississauga. The subject lands were added to the City's Heritage Register. The subject property was part of an original 100 acre land parcel granted in 1840 to farmer Christopher Row. It remained an active farm until the late 1960s where the property was eventually sold off in parcels to various owners. The remaining parcel of land containing the subject property was sold to the current owners in 2007.

In 2007, a demolition permit for the outbuildings was issued to the property owners by the Town of Milton, but demolition did not take place. In 2010, when the subject property became part of the City of Mississauga, the owners submitted a heritage property permit application and Heritage Impact Statement prepared by Archaeological Research Associates Limited requesting demolition of all structures as part of a future Plan of Subdivision application for redevelopment. This application did not proceed further and was eventually cancelled. The property owner has decided to renew this process and has applied for a new demolition permit for the removal of the barn and two garage outbuildings citing their deteriorated condition. The owner has indicated that the house is to be retained. Attached is the Heritage Impact Statement by Archaeological Research Associates Ltd (Appendix 1) and the concept development plan (Appendix 2) from 2010. There has been no submission of a Site Plan or Plan of Subdivision application to Planning and Building to date.

COMMENTS:

Section 27. (3) of the *Ontario Heritage Act* states that “structures or buildings on property listed on the City’s Heritage Register cannot be demolished without 60 days’ notice to Council. This allows Council time to review the property’s cultural heritage value and to determine if the property merits designation, as set out under Regulation 9/06 of the *Ontario Heritage Act*. In order to merit designation, one or more of the following three criteria must be satisfied:

1. The property has design value or physical value;
2. The property has historical value or associative value;
3. The property has contextual value.

Furthermore, Section 27. (5) of the *Ontario Heritage Act*, states that Council may require the applicant to submit plans in support of a demolition application for a property included on the city’s Heritage Register. Concept Plans were submitted in 2010 as part of the Heritage Impact Statement submission.

The Heritage Impact Statement concludes that the garage and barn structures are not worthy of heritage designation under Regulation 9/06 of the *Ontario Heritage Act*. They do not illustrate a style, trend or pattern; have any direct association with an important person or

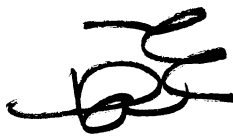
event; illustrate an important phase in the city's social or physical development; nor does it illustrate the work of an important designer.

Staff do not concur with the findings in the HIS with respect to the residential structure not having heritage value. Further investigation of the dwelling should be conducted when the property enters the Site Plan Approval process. The proposed redevelopment of the subject property will be required to go through the Site Plan approval process where Heritage Planning staff will be circulated for formal comment on any proposed new construction or site alterations.

FINANCIAL IMPACT: There is no financial impact.

CONCLUSION: The property owner of 6432 Ninth Line has requested permission to demolish a barn and two garage structures on an individually Listed property on the City's Heritage Register. Heritage Planning staff recommend the property owner's application for demolition of these structures, as noted in the Heritage Impact Assessment, on the conditions that the dwelling structure be retained and that solid wood board hoarding be installed at a distance of five (5) metres around the perimeter of the dwelling structure.

ATTACHMENTS: Appendix 1: Heritage Impact Statement
Appendix 2: Concept development plan



Paul A. Mitcham, P. Eng., MBA
Commissioner of Community Services

Prepared by: Laura Waldie, Acting Heritage Coordinator, Planning



97 Gatewood Road, Kitchener, ON N2M 4E3
Tel: (519) 835-4427
Fax: (519) 954-4797
248 Ruby St., Midland, ON L4R 2L4
Tel: (705) 526-9518
Fax: (705) 526-4541

**Heritage Impact Statement
Scappichio Property
6432 Ninth Line
City of Mississauga, Region of Peel
(Formerly Town of Milton, in the Regional Municipality of Halton), Ontario**

Prepared for
Mattamy Development Corporation
2360 Bristol Circle, Oakville, ON L6H 6M5
Tel: (905) 829-6998 Fax: (905) 829-7958

By
Rosi Zirger, M.A.
Archaeological Research Associates Ltd.
97 Gatewood Road, Kitchener, On N2E 3W3
Tel: (519) 835-4427 Fax: (519) 884-8853

July 2010

TABLE OF CONTENTS

1.0	Introduction	1
2.0	Location	1
3.0	Methodology	3
4.0	Historical Context	4
5.0	Site History	6
6.0	Site Analysis and Evaluation	8
6.1	<i>Dwelling</i>	10
6.2	<i>Utility Building #1</i>	10
6.3	<i>Utility Shed #2</i>	10
6.4	<i>Barn</i>	10
7.0	Recommendation for Heritage Designation	21
8.0	Description of Proposed Development	22
9.0	Consideration of Design Alternatives and Mitigation	24
10.0	Summary Statement and Conservation Recommendations	24
11.0	References Cited	26
12.0	Professional Qualifications	27

List of Figures

Figure 1: Study Area within Southern Ontario	2
Figure 2: Location of Subject Property on Halton-Peel Boundary	2
Figure 3: Detail of Walker & Miles <i>Historical Atlas of the Township of Trafalgar</i> (1877)	6
Figure 4: Aerial Photograph of Subject Property (2010)	8
Figure 5: Site Plan of Subject Property (2010)	9
Figure 6: Subject Property within Proposed Development	23

List of Plates

Plate 1: View North West from Ninth Line	11
Plate 2: View South West from Ninth Line	12
Plate 3: East (Front) Elevation of Dwelling	12
Plate 4: South Elevation of Dwelling	13
Plate 5: North Elevation of Dwelling	13
Plate 6: Detail of East (Front) Elevation	14
Plate 7: View of Interior-Livingroom facing south	14
Plate 8: View of Kitchen facing West	15
Plate 9: View of North West Room-1st floor	15
Plate 10: View of Upper Hall facing East	16
Plate 11: South & East Elevations of Utility Building #1	16
Plate 12: South Elevation of Utility Shed #2	17
Plate 13: North & West Elevations of Utility Shed #2	17
Plate 14: North & East Elevations of Barn	18
Plate 15: South & East Elevations of Barn	18
Plate 16: West Elevation of Barn & View of Square Storage Bin	19
Plate 17: Detail North West Corner of Barn- Deteriorated Foundation & Board	19
Plate 18: Interior of Barn-Lower Level	20
Plate 19: Interior of Barn-Lower Level	20

PROJECT TEAM

Project Manager: P.J. Racher, M.A., CAHP

Researchers: P.J. Racher, P. Hoskins, R. Zirger

Field Work: P.J. Racher, R. Zirger

Photographer: P.J. Racher, R. Zirger

Report Preparation: P.J. Racher, R. Zirger

Graphics: P. Hoskins, P.J. Racher

ACKNOWLEDGEMENTS

The preparation of this document owes much to the generous and invaluable assistance of the following individuals:

Paula Wubbenhorst, Senior Heritage Co-ordinator, City of Mississauga

Michelle Finn, Information Co-ordinator, Halton Region Museum

Chris Marvell, Local History Librarian, Milton Public Library

1.0 Introduction

Under a contract awarded in May of 2010, **Archaeological Research Associates Ltd. (ARA)** conducted a Heritage Impact Assessment (HIA) of the property at 6432 Ninth Line, in the City of Mississauga, Ontario. The assessment was carried out under contract to **Mattamy Development Corporation** in advance of an application for subdivision.

The property located at 6432 Ninth Line was, until recently, located within the boundaries of the Town of Milton. On January 1st, 2010 lands situated east of Highway 407, including the subject property, were annexed to the City of Mississauga. The property does not have a Heritage Designation, but it is listed in the Heritage Inventory for the City of Mississauga.

This assessment was carried out in accordance with the provisions of the *Ontario Heritage Act* (A.S.O. 1990), the *Provincial Policy Statement* (2005) and the *Heritage Impact Statements Terms of Reference* (City of Mississauga 2009).

2.0 Location

The subject of this study is a remnant farmstead located at 6432 Ninth Line in the City of Mississauga, formerly in the Town of Milton, Ontario (Figure 1-2). In July of 2010 the property was comprised of a parcel of land measuring approximately 12 acres, and included a 1 1/2-storey stucco dwelling with a frame garage attached, a large concrete block utility building, a small frame shed, and a large gable-roofed barn with a square storage bin (silo) at the rear.

The subject property is bounded on the east by the Ninth Line, on the north and south by agricultural land, and on the west by lands that form part of Highway 407.

Given the orientation of the subject property and the structures within it, the northwest direction will be treated as north in order to facilitate resource and photograph descriptions.

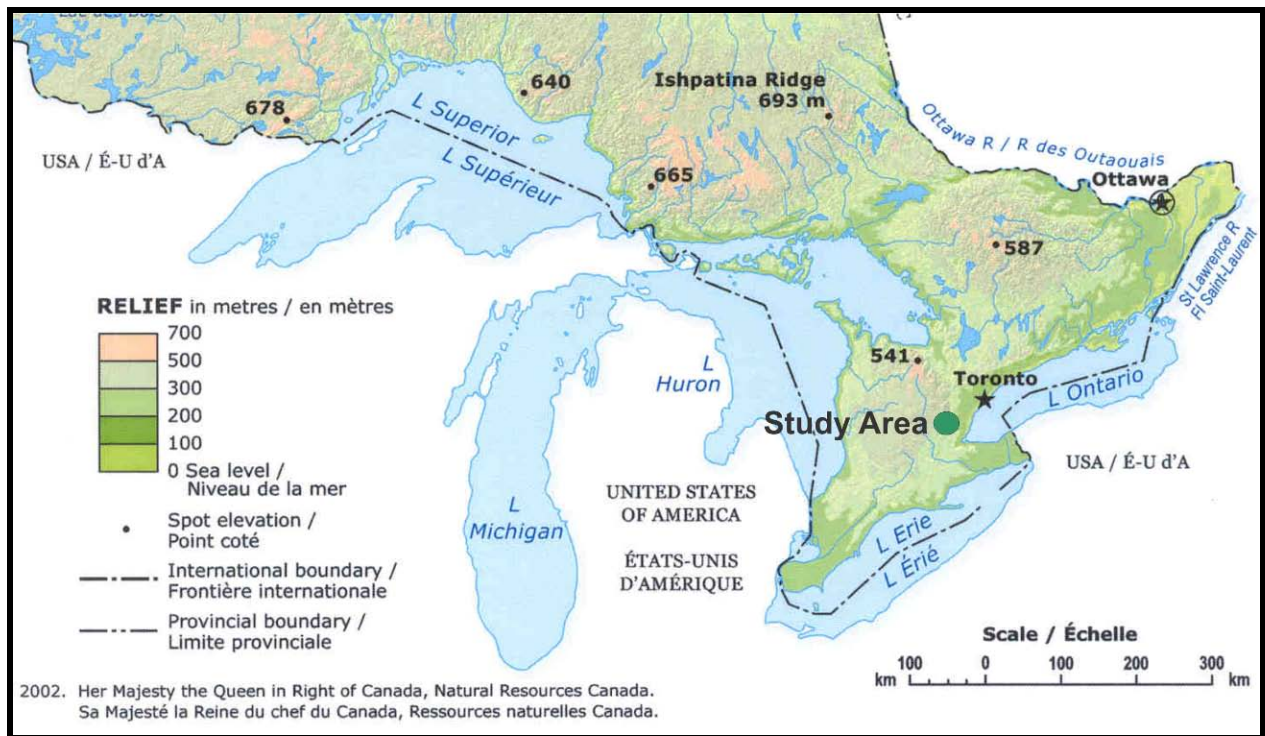


Figure 1: Study Area within Southern Ontario

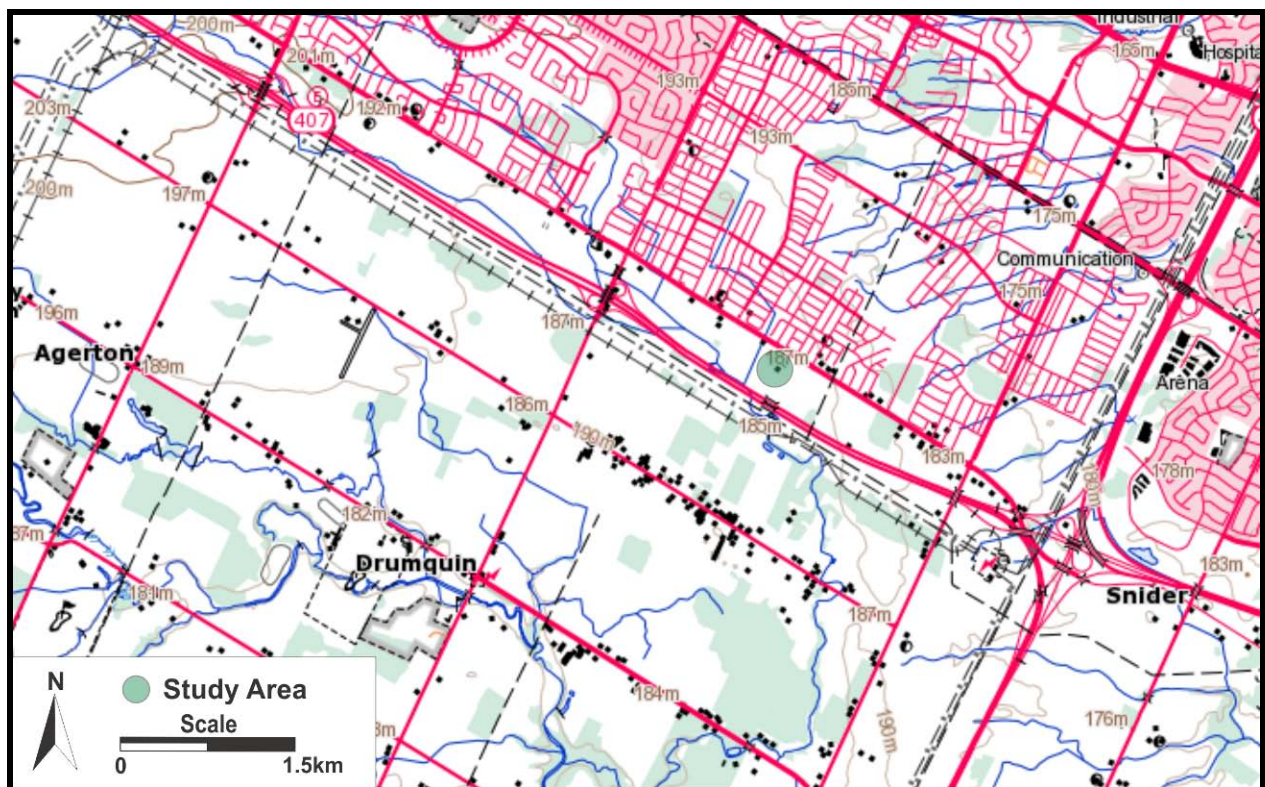


Figure 2: Location of Subject Property on Halton-Peel Boundary

3.0 Methodology

Data collection consisted of background research and systematic photographic documentation through a field survey of the study area. Background information was gathered from available historical and aerial maps, land records and other records located at the local archives, the land registry and/or local history collections at the public library. The Ministry of Culture's Ontario Heritage Properties Database was consulted as well as local Municipal Planning Department to determine if the property is a recognized, designated or protected cultural heritage property.

This report is based on the following definitions and concepts:

Cultural Heritage Resources:

- an umbrella term that includes cultural heritage landscapes and/or individual built heritage resources that have cultural heritage value or interest. A rolling age of 40 years is generally accepted for the preliminary identification of cultural heritage resources. However, it carries the understanding that this does not necessarily exclude resources less than forty years that demonstrate heritage value or design significance, nor does it necessarily include all cultural heritage resources more than 40 years old.

Built Heritage Resource (BH):

- “one or more *significant* buildings, structures, monuments, installations or remains associated with architectural, cultural, social, political, economic or military history and identified as being important to a community. These resources may be identified through designation or heritage conservation easement under the Ontario Heritage Act, or listed by local, provincial or federal jurisdictions” (Provincial Policy Statement 2005).

Cultural Heritage Landscape (CHL):

- “a defined geographical area of heritage significance which has been modified by human activities and is valued by a community. A landscape involves a grouping(s) of individual heritage features such as structures, spaces, archaeological sites and natural elements, which together form a significant type of heritage form, distinctive from that of its constituent elements or parts. Examples may include but are not limited to, heritage conservation districts designated under the Ontario Heritage Act; and villages, parks, gardens, battlefields, main streets and neighborhoods, cemeteries, trailways and industrial complexes of cultural heritage value” (Provincial Policy Statement 2005).

Cultural Heritage Value or Interest (CHVI):

- “the aesthetic, historic, scientific, cultural, social or spiritual importance or significance for past, present or future generations. The *heritage value* of a *historic place* is embodied in its character-defining materials, forms, location, spatial configurations, uses and cultural associations or meanings” (*Standards and Guidelines for the Conservation of Historic Places in Canada* 2003).

This document is supported by the guidelines and policies provided by the following:

- the **Ontario Heritage Act** (R.S.O. 1990)
- the **Ontario Planning Act** (R.S.O. 1990)
- the **Provincial Policy Statement** (Ministry of Municipal Affairs and Housing 2005)
- **Standards and Guidelines for the Conservation of Historic Places in Canada** (2003)
- **Heritage Impact Statement Terms of Reference** (City of Mississauga 2009)

Evaluation and recommendations are based on the criteria established under Ontario Regulation 9/06 of the Ontario Heritage Act. A property must have the potential to meet at least one of the criteria to be considered to have heritage significance. These criteria fall into three categories: design or physical value, historical or associative value and contextual value. All the properties in the study area that had the potential to be cultural assets were evaluated against these criteria.

4.0 Historical Context

The subject property is a remnant of a 100-acre farm originally comprised of the eastern half of Lot 8, Concession 9, New Survey, in the former Geographic Township of Trafalgar, County of Halton, Ontario. The following is a brief description of the immediate vicinity of the 19th century farm.

The present study area falls within the original boundaries of Home District of Upper Canada created by the Constitutional Act of 1792. The County of Halton was formed in 1853 with the Township of Trafalgar being one of its original townships. Since then, a number of changes in jurisdiction have occurred. In 1962, jurisdiction of the subject land fell within the boundaries of the Town of Oakville. With the establishment of Regional Government in 1974, the subject property fell within the limits of the Town of Milton in the Regional Municipality of Halton. Under the same policy, land on the east side of Ninth Line was annexed to the Region of Peel. On January 1, 2010 portions of the Town of Milton lying east of the Highway 407, and including the study area, were annexed to the City of Mississauga, Region of Peel.

The survey and settlement of the northern portion of the Township of Trafalgar commenced following the 1819 purchase by the British Crown of land from the Mississauga First Nation. The tract was established as part of the “New Survey”, which was based on a grid and concession pattern distinct from the county’s “Old Survey” in the southern portion. The 200-acre lots of the “New Survey” were laid out using the double front system and were typically granted in square 100 acre parcels; a configuration intended to facilitate farming and provide maximum access to transportation corridors.

Although the settlement of Trafalgar Township began as early as 1807, it was in the years following the 1820’s that saw settlements grow and the economy mature. In 1817, the population of the township was 548 with only one grist mill and four saw mills (Walker & Miles 1877:59). By 1850, the population of the township had increased to 4,513. The greatest change in settlements patterns came following the arrival of the railway in the 1850’s (Halton Heritage Study 1981). The railway nearest the present study area was the Credit Valley Railway, built in 1856. It was located north of present-day Derry Road (Halton Heritage Study 1981, Map 4)

The construction of roads in the early 19th century was instrumental in transporting both settlers and goods. Seventh Line (commonly known as Trafalgar Road) was an early and important transportation route in Ontario. By 1841 Seventh Line was extended to present day Owen Sound. The 19th Century communities along Seventh Line would have been the nearest local service centres for the farming families of Ninth Line. In 1877, Drumquin, (see Figure 3) the crossroads community located on Seventh Line at Britannia Road included a post office, a store, a blacksmith shop and an inn (Walker & Miles 1877:81). A little further north on Seventh Line at Derry Road was Agerton (later known as Auburn), which in 1877 included a blacksmith shop and temperance house. The hamlet of Hornby was located on the next sideroad north. Although Drumquin and Auburn have all but disappeared, Hornby remains the postal district for the subject property. The largest 19th Century service centres nearest the present study area were Streetsville, in the former County of Peel, located approximately 3.2 kilometres east, and Milton located about 16 kilometres to the west.

Ninth Line retained its rural agriculture character until the 1990's, when residential developments began to appear on the east side of Ninth Line. A more profound change to the rural character which once defined the area occurred with the construction of Highway 407 to the west. This required the purchase of large tracts of farmland, and resulted in the isolation of several homesteads on Ninth Line from their acreages.

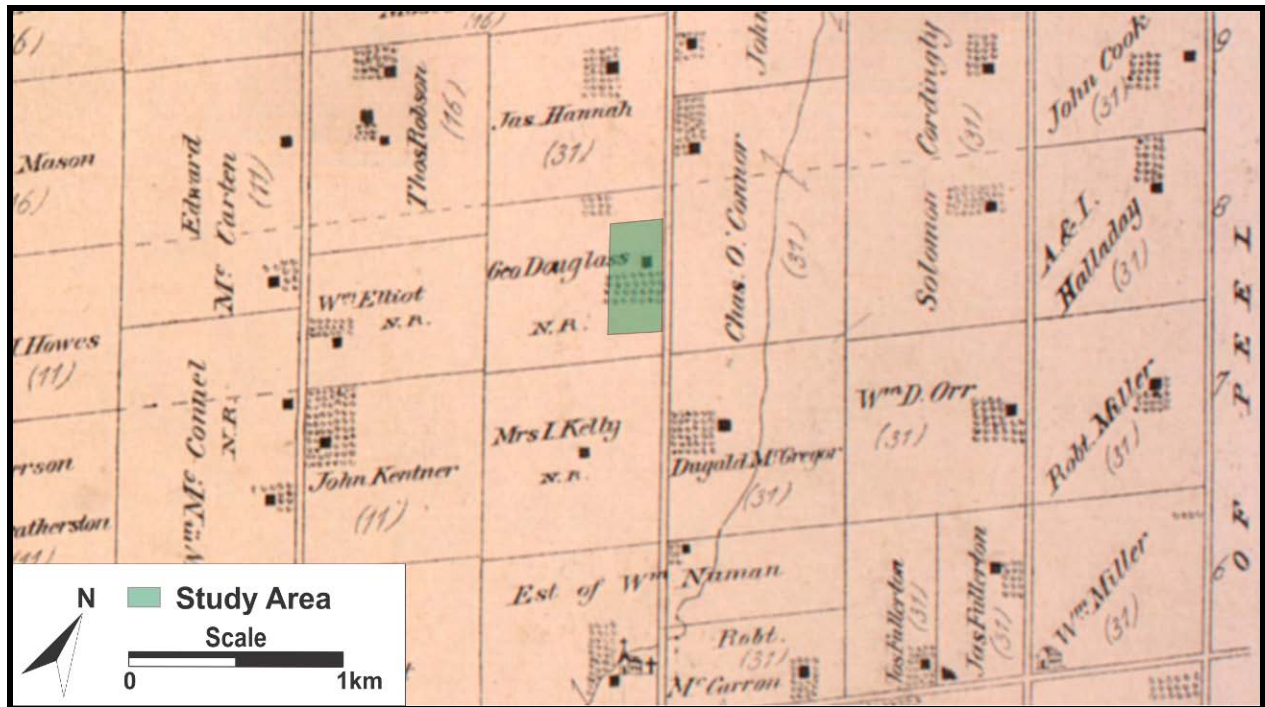


Figure 3: Detail of Walker & Miles *Historical Atlas of the Township of Trafalgar* (1877)

5.0 Site History

The following site history is based on a thorough search of Land Registry Office Records, as well as Census and Cemetery Records, local newspapers and other archival materials located at the Milton Public Library, the Halton Region Museum or accessed online.

Early Owners (1840-1870)

The first registered owner of the subject property was Christopher Row, who received a Crown Patent on November 11, 1840, of 100 acres on the East half of a Clergy Reserve on Lot 8 Concession 9, New Survey. Row and his wife Mary owned the property for about 10 years before selling it to William Watson in March 1851 for £300 (Inst #9/1851).

William Watson and his wife Elizabeth owned the 100-acre farm for about 10 years, from their purchase in 1851 until William's death around 1861. Beginning in 1858, seven years after he purchased the property, Watson mortgaged the property five times. All the mortgages were paid off and discharged before his death. Nineteenth century mortgage financing was often an indication of money being raised for construction.

In 1862, William Watson's widow, Elizabeth sold the 100-acre property, left to her by her husband's will, to Edward Coyne for \$4,000 (Inst #11/1862). Within a year, in 1863, Coyne transferred the property to Daniel Sturdy, a gentleman of Dorset County, in England, formerly of Toronto (Inst #160/1863). It appears that the transfer was to secure a pledge or other

performances between the parties. Whatever their agreement, by July 1866, Coyne had defaulted and the property vested in Daniel Sturdy, who in turn sold the property to Archibald McCurdy for \$1,500 (Inst #37 & 38 /1866). Archibald McCurdy owned the property for four years, from July 1866 until November 1870, during which time he raised \$2,200 secured by two mortgages.

George Douglas (1870 -1899)

In November 1870 McCurdy and his wife Mary Anne sold the property to George Douglas for \$2,700 (Inst 503/1870). The 1877 *Historical Atlas for the Township of Trafalgar* shows a house and orchard on the East half of Lot 8 Concession 9, which was then owned and occupied by George Douglas. Douglas and his family remained on the property for about 30 years until 1899 (Inst #7397/1899). The 1891 Census Record lists members of the household as George Douglas (51), his wife Mary (41), and their children Peter (21), George (14), Nellie (12) and Nora (8) (Div 2 Pg 39,Line 20-25).

William McCarron (1899-1951)

In November 1899 William J. McCarron, an Irish Catholic, purchased the 100-acre property farm from George Douglas for \$6,450 (Inst #7397/1899). During his fifty years in Trafalgar Township, William McCarron acquired additional farm property in Concession 10 (on the east side of Ninth Line) and raised a large family. The 1911 Census Record lists the occupants of the house on Lot 8, Concession 9 as William J. McCarron (39, b.1872), his wife Maria (29, b. 1881), and six children, Irene (10, b.1900), Christina (9, b.1902), William (7, b.1903), May (6, b.1905), Roy (4, b.1907) and Arthur (1, b. 1909). Another three children were born after 1911.

When William Joseph McCarron died in May 1951, his Will and Probate named nine children. He left cash bequests to his daughters and the farm property to his sons (Probate #6979GR/1951). By his will, he left his son, Cecil McCarron, his “home farm” on the East half of Lot 8, Concession 9, he left 40 acres of the east half of Lot 9, Concession 10 to his son Roy, and the remaining 60 acres of East half Lot 9 Concession 10 to his son Frank.

Cecil McCarron (1951-1987)

Cecil McCarron retained his family’s farm until 1967, when he sold the south 50 acres for \$52,500 (Inst #230372/1967). In 1976 McCarron, divided his remaining property again, this time selling the west half (approximately 23.5 acres) to the Ministry of Government Services. (Inst. #435185/1976). Part of this land was later incorporated into the present alignment of Highway 407.

Cecil retained the remainder of the McCarron family farm (approximately 12.6 acres), including all the buildings, until his death in 1987. It appears that Cecil McCarron left no next of kin (Inst #679521/1987). After his death in 1987, the property was purchased by Francesco, Hedwig, Michele & Candida Scapicchio, Francesco & Antonietta Fraschini, and Gino and Linda

DelleDonne, who retained ownership until 2007 when the property was purchased by Derry Britannia Developments Inc. for the proposed development.



Figure 4: Aerial Photograph of Subject Property (2010)

6.0 Site Analysis and Evaluation

The following photographs and site description were collected on a site visit on June 23rd, 2010. In June 2010 the subject property consisted of approximately 12 acres of land (based on LRO Records), and included a dwelling with an attached garage, a large concrete block utility building, a frame shed, and a large barn on a concrete block foundation with a small metal storage bin to the rear of the barn (see Figures 4 & 5). The dwelling is located near the present alignment of Ninth Line. The other farm and/or utility buildings are located behind (west) of the dwelling. The buildings are accessed via a short driveway from Ninth Line. Agricultural fields, under cultivation at the time of this site visit, surround the buildings. There are no trees, shrubbery, or other vegetation near the farm or house yards. The subject property does not have the appearance of a rural agricultural landscape, let alone one that might have its roots in the 19th century.

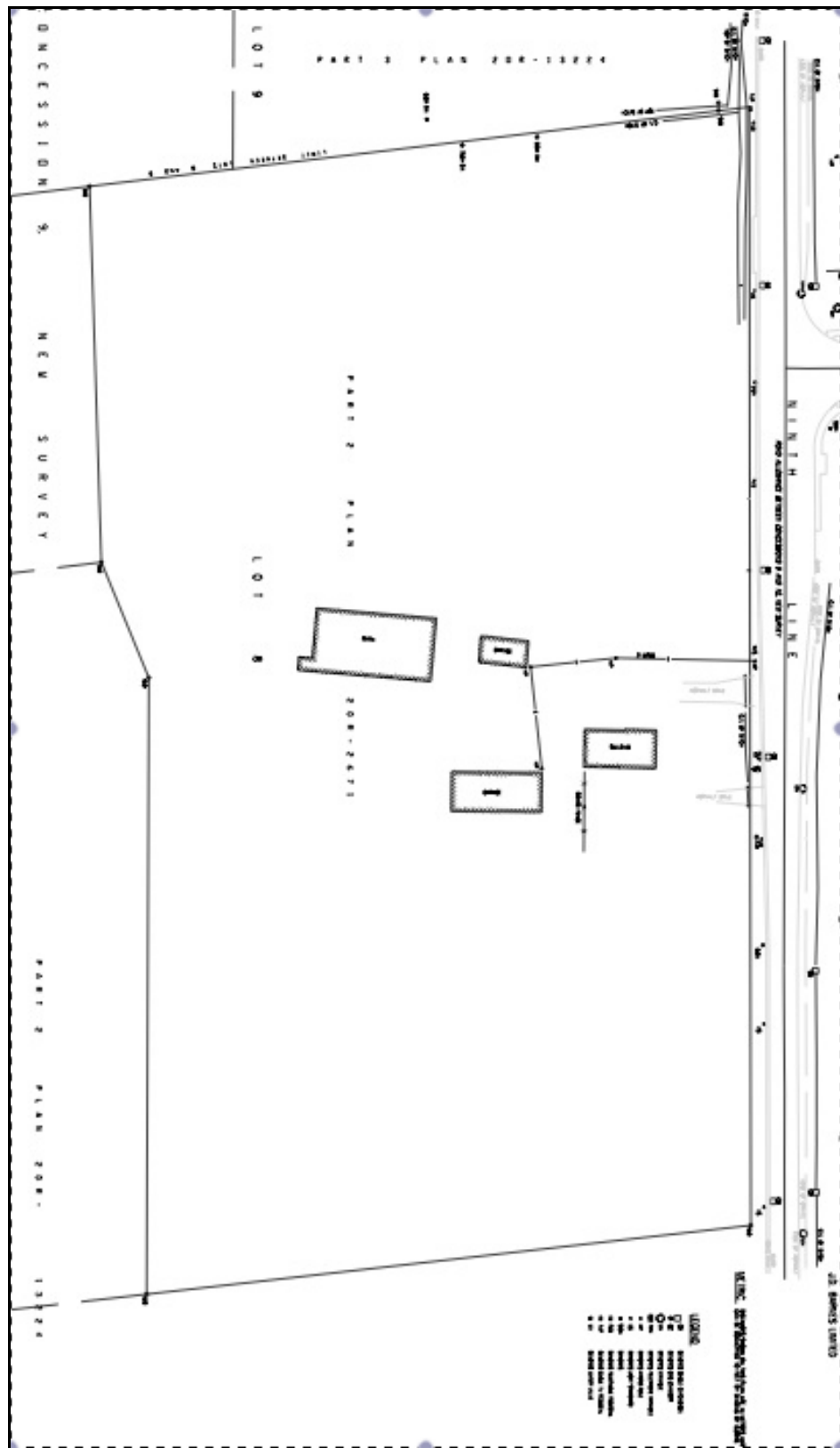


Figure 5: Site Plan of Subject Property (2010)

6.1 Dwelling

The dwelling is a 1-1/2 storey stucco over red brick structure, with a hipped roof clad in asphalt shingles (see Plates 1-6). No determination was made as to the type of foundation; however, there is no basement under the dwelling (Per Comm. with Tenant July 2010). The front (east) façade has a small center gable similar to the vernacular Ontario Cottage style popular in the mid-19th century. The only exterior details that might suggest early construction are stone windowsills and the remnant of a wooden architrave around the front (east) door (see Plate 6). All the doors and windows are mid to late 20th century replacements. The balanced center-hall floor plan and window placement are similar to those found in 19th century homes (Macrae & Adamson 1967:232). However, no interior details indicate early construction. The dwelling is in poor condition, and has no Cultural Heritage Value or Interest (CHVI).

The frame garage attached to the rear of the house is a mid to late 20th century addition.

6.2 Utility Building #1

This large concrete block utility building immediately to the west of the house was likely constructed in the late 20th century (see Plate 11). The building is in excellent condition, but it has no particular CHVI.

6.3 Utility Shed #2

A vertical board, gable-roofed shed is located north of the dwelling near the barn (see Plate 12 & 13). This shed was likely built to store farm machinery. Its vertical board cladding has been covered on the north and west sides with corrugated metal sheeting, as has the roof. The interior shows original roof boards still in place, 2 x 4 trusses and large squared beams, which support the roof. The exact date of construction is unknown, however, the frame construction is typical of mid-twentieth century. The shed is in fair condition, but it has no CHVI.

6.4 Barn

This large gable-roofed barn at the west end of the driveway dominates the farm complex (see Plate 14 & 17). Its original board and batten exterior, visible on the north and west sides has been covered over on the south and east sides with metal sheeting. The gable roof is clad with corrugated metal sheeting. The entire barn has been raised onto a new concrete block foundation, and a 2-storey flat-roofed addition has been added on the east side. The interior of the lower level shows trees and joists running across squared timber beams support the upper floor (see Plate 18 & 19). The beams are supported on steel jacks and concrete.

At the time of the site visit in June of 2010, there was no access to the upper level of the barn either from the interior or the exterior. The only visible exterior openings were on the south side, but they were not accessible from the ground. Without inspecting the upper level and the overall

structure it is difficult to estimate the date of its construction. The barn has most recently been used to store car and truck tires. The tenant has vacated and presumably all the tires have been removed. The square, metal-clad storage bin, a silo type structure located at the south west corner of the barn, was also not open for access.

The date of construction for the barn is unknown, though it seems likely that it dates to the mid-twentieth century. The barn is in good condition, primarily because of the addition of the new concrete block foundation. However, it has only limited CHVI.



Plate 1: View North West from Ninth Line



Plate 2: View South West from Ninth Line



Plate 3: East (Front) Elevation of Dwelling



Plate 4: South Elevation of Dwelling



Plate 5: North Elevation of Dwelling



Plate 6: Detail of East (Front) Elevation

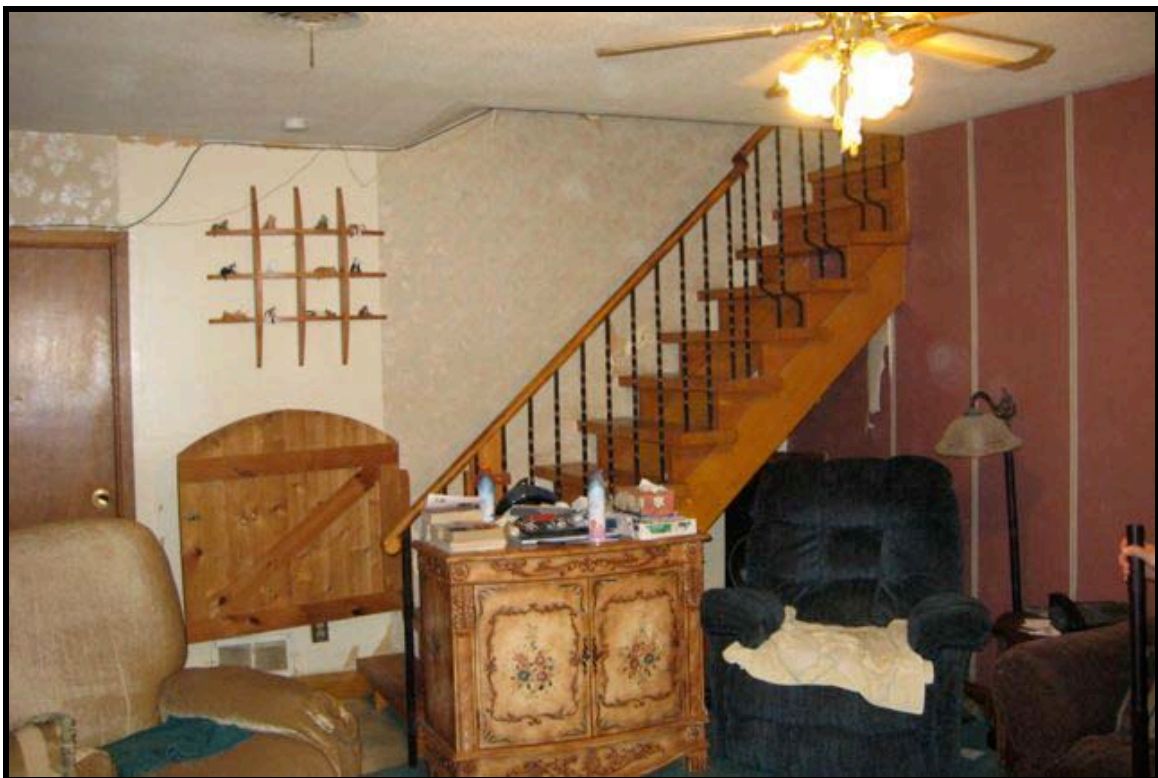


Plate 7: View of Interior-Livingroom facing south



Plate 8: View of Kitchen facing West



Plate 9: View of North West Room-1st floor



Plate 10: View of Upper Hall facing East



Plate 11: South & East Elevations of Utility Building #1

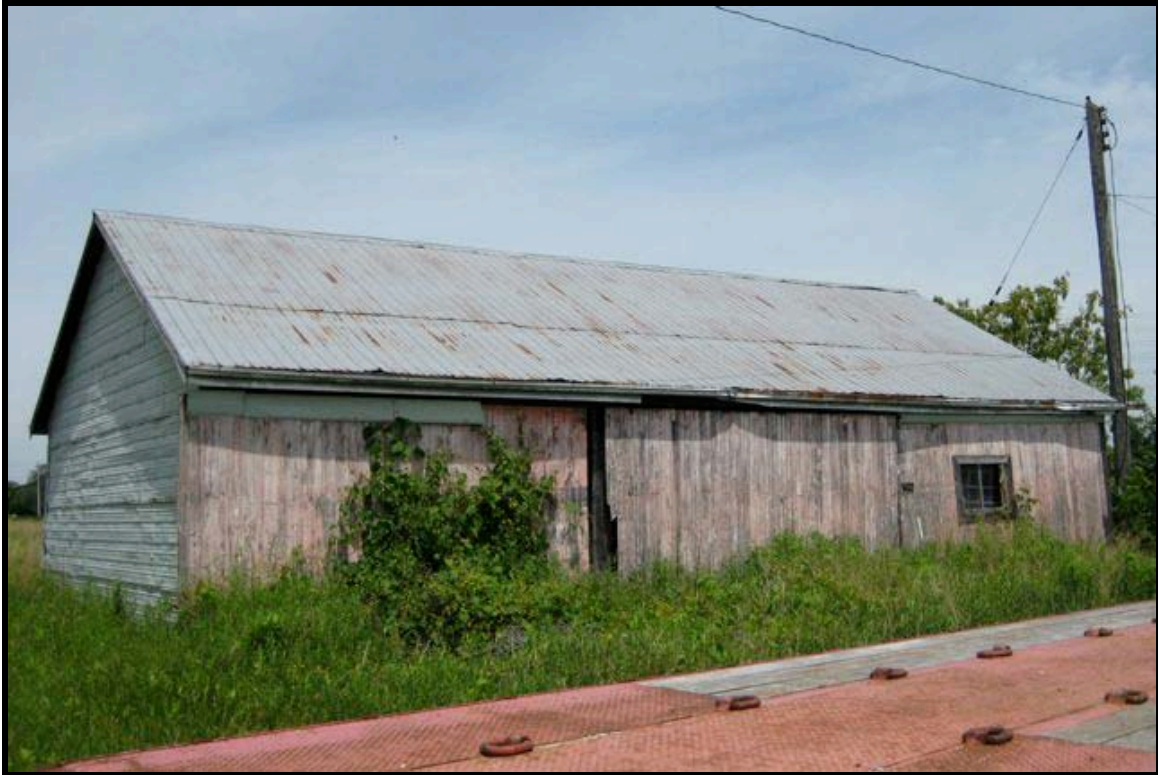


Plate 12: South Elevation of Utility Shed #2



Plate 13: North & West Elevations of Utility Shed #2



Plate 14: North & East Elevations of Barn



Plate 15: South & East Elevations of Barn



Plate 16: West Elevation of Barn & View of Square Storage Bin



Plate 17: Detail North West Corner of Barn- Deteriorated Foundation & Board



Plate 18: Interior of Barn-Lower Level



Plate 19: Interior of Barn-Lower Level

7.0 Recommendation for Heritage Designation

Ontario Regulation 9/06 of the *Ontario Heritage Act* establishes the criteria for determining Cultural Heritage Value of Heritage Resources. A property must have the potential to meet at least one of the criteria to be considered to have heritage significance. These criteria fall into three categories: design or physical value, historical or associative value and contextual value. The following considers and evaluates the subject property against these criteria.

The property has design value or physical value because it,		
i	is a rare, unique, representative or early example of a style, type, expression, material or construction method	None
ii	displays a high degree of craftsmanship or artistic merit	None
iii	demonstrates a high degree of technical or scientific achievement	None
The property has historical value or associative value because it,		
i	has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community,	None
ii	yields or has the potential to yield, information that contributes to an understanding of a community or culture	None
iii	demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community	None
The property has contextual value because it,		
i	is important in defining, maintaining or supporting the character of an area	This property does not maintain or support the rural character that once defined the Ninth Line
ii	is physically, functionally, visually or historically linked to its surroundings	The buildings on the property do not appear to be linked to their surroundings.
iii	is a landmark	The property is not a landmark

Based on the above criteria the subject property has no Cultural Heritage Value or Interest (CHVI). Accordingly, the subject property has no attributes that would warrant Heritage Designation under the Ontario Heritage Act.

8.0 Description of Proposed Development

The proposed development is part of an overall re-development of the land between Ninth Line and Highway 407. The City of Mississauga's "District Land Use Index" indicates that the subject property is intended as a "residential district" (for more information see <http://www.mississauga.ca/file/COM/mpdist2010.swf>). However, the City of Mississauga's Official Plan, marks the land situated between Highway 407 and Ninth Line, including the subject property, as a "Special Study Area", suggesting that its proposed land use has yet to be determined.

The proposed development calls for the subject property to be developed for residential use and requires the demolition and/or removal of all buildings from the subject property (see Figure 6).



Figure 6: Subject Property within Proposed Development

9.0 Consideration of Design Alternatives and Mitigation

The following is a consideration of generally accepted mitigation options or alternatives as each applies to the subject property. The proposed development calls for the demolition and/or removal of all buildings from the site. No other design options have been presented or proposed at this time, however the following considers possible alternatives.

Retention: This option provides for cultural heritage resources to be retained in their original location, but allowing for the possibility of adaptive re-use. None of the existing buildings warrant retention based on heritage significance.

Relocation: This option allows for a cultural heritage resource to be moved within or beyond the subject property provided an appropriate context is maintained. This option is based on the assumption that the cultural heritage resource is moved to retain its heritage integrity and value. None of the existing buildings warrant relocation based on heritage significance.

Ruinification: This option allows for the exterior of the structure to be maintained on the site. None of the structures on the property exhibit any features that would warrant ruinification.

Symbolic Conservation: This option allows for the recovery of unique heritage components and incorporation into the new construction in remembrance of the past. There are no structural components that warrant symbolic conservation.

Salvage and Documentation: This option allows the demolition of the structures, ensuring thorough documentation of the property is carried out. None of the existing buildings have structural components, such as interior trim or brick that warrant salvage.

10.0 Summary Statement and Conservation Recommendations

The subject property does not exhibit any attributes that can be considered to be of Cultural Heritage Value or Interest (CHVI). The property is a remnant of early 19th Century farmstead. Historically, two families were associated with the property for a significant length of time. George Douglas, a Presbyterian farmer of Scottish descent owned and farmed the property from 1870 until 1899. The McCarrons, an Irish Catholic family, owned the property for nearly 90 years. William McCarron owned the farm from 1899 until his death in 1951, and then his son Cecil McCarron from 1951 until his death in 1987. Historical research has not determined any contribution by either the Douglas or the McCarron families that would warrant local historical significance.

The property does not have a Heritage Designation, but it is listed on the City of Mississauga's Heritage Inventory. However, the alterations and renovations made to the house over the years have destroyed any CHVI it may once have had. None of the other structures on the subject property appear to have any CHVI.

The proposed development calls for the demolition and /or removal of all structures from the subject property for a residential housing development. At this time no other design alternatives have been proposed.

Mitigation measures were examined against the impacts on the structures on the property. The existing structures within the subject property do not exhibit any cultural heritage attributes that might warrant retention.

Accordingly, it is recommended that demolition of all structures on the subject property be allowed, and that the proposed development be allowed to proceed without further heritage conditions.

11.0 References Cited

Land Records, County of Halton Land Registry Office

Landplan Collaborative Inc. et al

2005 **Cultural Landscape Inventory for the City of Mississauga.** Study
Commissioned by the City of Mississauga.

Macrae, Marion & Anthony Adamson

1967 **The Ancestral Roof: Domestic Architecture of Upper Canada.** Toronto; Clarke, Irwin
& Co.

Ministry of Culture

2005 **Ontario Heritage Act**

Ministry of Municipal Affairs and Housing

1990 **Ontario Planning Act**

2005 **Provincial Policy Statement.**

Parks Canada

2003 **Standards and Guidelines for the Conservation of Historic Places in Canada.**
Toronto: Parks Canada.

Regional Municipality of Halton

1981 **Halton Heritage Study.** Regional Municipality of Halton Planning & Development
Department

Walker & Miles

1877 **Illustrated Historical Atlas of the County of Halton.** Toronto: Walker and Miles.

Newspapers (microfilmed) **Canadian Champion**

Additional Historic information accessed online at:

Halinet (Halton Information Network) www.halinet.on.ca

12.0 Professional Qualifications

Curriculum Vitae

**Paul J. Racher, 4456 Cedar Springs Road, RR#1 Burlington
Ontario L7R 3X4 (519) 835-4427**

Education

- 1992-1997 Ph.D. Programme, Department of Anthropology, University of Toronto.
Supervisors: E.B. Banning and B. Shroeder.
- 1989-1992 M.A., Dep't of Anthropology, McMaster University, Hamilton, Ontario. Thesis entitled:
The Archaeologist's 'Indian': Narrativity and Representation in Archaeological Discourse.
- 1985-1989 Honours B.A., Wilfrid Laurier University, Waterloo, Ontario.
Major: Prehistoric Archaeology.

Professional Memberships and Accreditations

- Current Professional Member of the Canadian Association of Professional Heritage Consultants
(**CAPHC**). Volunteer on the ethics committee.
- Member of the Ontario Archaeological Society (**OAS**). Volunteer on the Professional
Committee.
- RAQS registered with MTO.

Publications

- 1990 “Scary Tales – Narrativity and Representation in Archaeological Discourse” **Nexus: The Canadian Student Journal of Anthropology**. McMaster University, Hamilton.
- 1993 “The Tales We Tell – The Iroquois as ‘Savage’ in Ontario Archaeology” **Vis a Vis: Explorations in Anthropology**. University of Toronto, Toronto.
- 1995 **A Biophysical Model for Prehistoric Archaeological Sites in Southern Ontario**. Co-authored with Penny M. Young, Malcolm R. Horne, Colin D. Varley, and Andrew J. Clish. The Research and Development Branch, MTO.
- 2006 “Up from the Muck: Towards a Truly Professional Archaeology in Ontario” **Arch Notes**. July/August Issue.

Conference Papers

- 1990 (with C. Varley & P. Ramsden) “East Meets West – The Mythological and Social Transformations of Space amongst the Early Historic Iroquois of Ontario”. Presented at the 1990 Chacmool Conference, University of Calgary.
- 1991 “The Iroquois of Archaeology – Narrativity and Representation in Ontario Archaeology” Presented at the 1991 conference of the Northeastern Anthropological Association, Waterloo, ON.

- 1997 (with E.B. Banning) "Sampling theory and microrefuse analysis: Neolithic house floors in Wadi Ziqlab, Jordan" Presented at the 1997 SAA meetings, Nashville, TN.
- 2006 "Up from the Muck: Towards a Truly Professional Archaeology in Ontario" Presented at the 2006 meetings of the Canadian Archaeological Association, Toronto, ON.

Scholarly Talks

- 1991 "Black Horse Corners" A lecture presented to archaeology students at Conestoga College, Waterloo, Ontario.
- 2008 "Ethics in Consulting Archaeology" A lecture presented to the first Aboriginal Monitor Training Workshop, Ohsweken, Six Nations Territory, Ontario
- 2009 "Ethics in Cultural Resource Management" A lecture presented to the 2nd Aboriginal Monitor Training Workshop, Ohsweken, Six Nations Territory, Ontario
- 2009 "The Archaeology of the Grand River Watershed" A lecture presented to the Ministry of the Environment at the request of the Six Nations Eco Centre, Ohsweken, Ontario
- 2009 "Heritage Consulting in Ontario" A lecture presented to students of the heritage planning programme at the University of Waterloo.

Work Experience

- 2000-Current **Project Manager / Principal Investigator, Archaeological Research Associates Ltd.**
Supervised field research and wrote reports for an archaeological consulting firm. In the past nine field seasons, I have served as Field Director for Stage 1-4 work on dozens of projects varying in size from Early Archaic campsites to Iroquoian Villages.
- 2008 **Part-Time Faculty, Wilfrid Laurier University.**
Lecturer for Cultural Resource Management course (AR 336). In charge of all teaching, coursework, and student evaluations.
- 1995 **Field Archaeologist, University of Toronto.**
Served as an supervisor on a multinational archaeological project in northern Jordan.
- 1992-1995 **Teaching Assistant, University of Toronto.**
Responsible for teaching and organizing weekly tutorials for a number of courses.
- 1991-1994 **Part-Time Faculty, Wilfrid Laurier University.**
Lectured for several courses in anthropology. Held complete responsibility for all teaching, coursework, and student evaluations.
- 1992-1996 **Partner in Consulting Company: Cultural Management Associates Incorporated.**
Supervised several archaeological contracts in Southern Ontario. Participated in a major (now published) archaeological potential modeling project for MTO.
- 1989-1991 **Partner in Consulting Company: Cultural Resource Consultants.**
Managed the financial affairs of a consulting firm whilst supervising the completion of Several contracts performed for heritage parks in central Ontario.

- 1988-1991 **Principal Investigator/Project Director: Archaeological Research Associates Ltd.**
Oversaw the completion of large contracts, wrote reports, and was responsible for ensuring that contracts were completed within budget.
- 1988 **Assistant Director of Excavations: St. Marie Among the Hurons, Midland, Ontario.**
Duties included crew supervision, mapping, report writing and photography.
- 1986-1987 **Archaeological Crew Person: Archaeological Research Associates Ltd.**
Participated in background research, survey, and excavation on a number of Archaeological sites across Ontario.

Selected Contracts Managed

Built Heritage and Cultural Heritage Landscape Contracts Managed

- 2009 **Chatham-Kent Wind Power Project**
Carried out a Heritage Impact Assessment (HIA) for a large wind power project in southwestern Ontario.
- 2009 **The Mattawishkwia River Bridge - Hearst**
Carried out a CHER for a bridge to be impacted by Highway improvements.
Client: Stantec.
- 2009 **Mavis Road Widening – Brampton**
Carried out a cultural landscape study of lands to be impacted by the proposed widening of the Mavis Road alignment. Client: TMHC.
- 2008 **9th Line – Milton**
Supervised the completion of a built heritage inventory and a heritage impact statement along historic 9th Line in Milton. Client: Mattamy Homes.
- 2008 **Highway 26 – Meaford to Thornbury**
Updated built heritage inventory for project area and supervised the heritage impact statement (HIS) for a property to be affected by road widening. Client: Stantec.
- 2007 **Highway 401 Improvements – Woodstock**
Supervised the completion of two CHER studies for bridges to be impacted by improvements to Highway 401. Client: Stantec.
- 2006 **Bridgeport Bridge – Kitchener**
Supervised the completion of a built heritage study on the historic Bridgeport Bridge in Kitchener, Ontario. Client: Stantec.

CURRICULUM VITAE

ROSI J. ZIRGER, M.A.

2 Village Drive, RR #3, Niagara on the Lake, ON L0S 1J0

EDUCATION / TRAINING

Willowbank School of Restoration Arts, Queenston, ON
(Diploma in Heritage Conservation -2009)

University of Toronto, Toronto, ON (M.A. 1997)

University of Toronto, Toronto, ON (B.A. 1994)

Centennial College, Toronto, ON (Law Clerk Certificate 1977)

PROFESSIONAL EXPERIENCE IN HERITAGE CONSERVATION

Principal Rosi Zirger, Heritage Conservation Consultant
 -Research in local & architectural history
 -Analysis of historic buildings

Built Heritage & Historic Research Specialist
 Archaeological Research Associates Ltd., Waterloo, ON

Researcher Heritage Welland Committee, Welland, ON
 -Historical Research & Architectural Analysis,

SELECTED PROJECTS AS A HERITAGE CONSULTANT

Built Heritage and Cultural Heritage Landscape Inventory, Port Dover-Nanticoke Wind Project,
Haldimand-Norfolk, ON (Archaeological Research Associates Ltd. 2010)

Built Heritage and Cultural Heritage Landscape Assessment, Dixie Road, Caledon, ON
(Archaeological Research Associates Ltd. 2010)

Built Heritage and Cultural Heritage Landscape Assessment, Steeles Avenue & Airport Road,
Brampton, ON (Archaeological Research Associates Ltd. 2010)

Conservation Plan, St. Mark's Rectory, Niagara on the Lake, ON (2009)

Conservation Plan, London Psychiatric Hospital, London, ON (contributor - Julian Smith &
Assoc., Architect 2008)

Conservation Plan, St. Thomas Psychiatric Hospital, St. Thomas, ON (contributor - Julian Smith
& Assoc., Architect 2008)

Historical Report, Grand Victorian, Niagara on the Lake, ON (commissioned by owner 2008)

Existing Conditions Report, The Gardener's Cottage, Dundurn Castle NHS, Hamilton, ON (Contributor – Craig Sims, Built Heritage Consultant 2008)

Historic Property Study & Management Plan, Stark House, Thorold, ON (contributor – Julian Smith, Architect 2007)









Corporate Report

Clerk's Files

Originator's
Files

DATE: April 12, 2014

TO: Chair and Members of the Heritage Advisory Committee
Meeting Date: May 20, 2014

FROM: Paul A. Mitcham, P. Eng., MBA
Commissioner of Community Services

SUBJECT: **Request to Demolish a Heritage Listed Property Within a Cultural Landscape - 3110 Merritt Avenue (Ward 5)**

RECOMMENDATION: That the property at 3110 Merritt Avenue, which is listed on the City's Heritage Register as part of the War Time Housing (Malton) Cultural Landscape, is not worthy of heritage designation, and consequently, that the owner's request to demolish the structure be approved and the appropriate City officials be authorized and directed to take the necessary action to give effect thereto, as described in the Corporate Report dated April 12, 2014 from the Commissioner of Community Services.

BACKGROUND: The property owner has applied to Heritage Planning to demolish the existing structure and build a two storey replacement structure. The subject property was Listed on the City's Heritage Register in 2005 as part of the War Time Housing (Malton) Cultural Landscape. This cultural landscape is noted for the consistent scale of built features; the direct association with an important person or event and the important phase in Mississauga's social or physical development.

The original Crown Grantee for Lot 11, Concession 7 was King's College (presently University of Toronto), which received a two-hundred (200) acre parcel in 1808. One hundred (100) acres of the original 200 acre parcel was sold to Alexander McDonald in 1842,

remaining in the McDonald family until 1890 when Thomas Codlin purchased 95 acres of the west half of Lot 11. Codlin retained ownership of most of this parcel until 1942 when it was sold to, or expropriated by, Wartime Housing Ltd. Soon after Wartime Housing Ltd. acquired the property, the lands set aside for a wartime housing subdivision were surveyed and developed.

This planned subdivision is located opposite the northeast corner of Pearson International Airport. The neighbourhood is close to where the original Malton Terminal was located and remains close to the present airplane manufacturing and service industry. Although some of the original houses have been altered many still retain characteristics typical of the period.

COMMENTS:

Section 27. (3) of the *Ontario Heritage Act* states that structures or buildings on property listed on the City's Heritage Register cannot be demolished without 60 days' notice to Council. This allows Council time to review the property's cultural heritage value and to determine if the property merits designation, as set out under Regulation 9/06 of the *Ontario Heritage Act*. In order to merit designation, one of the three following criteria must be satisfied:

1. The property has design value or physical value;
2. The property has historical value or associative value;
3. The property has contextual value.

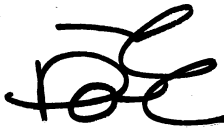
Furthermore, Section 27. (5) of the *Ontario Heritage Act* states that Council may require the applicant to submit plans in support of a demolition application for a property included on the city's Heritage Register. Plans for the replacement dwelling have been included in the submitted Heritage Impact Statement from Gillespie Heritage Consulting (Appendix 1). This area of Mississauga is not subject to Site Plan Control.

The author of the Heritage Impact Statement concludes the house at 3110 Merritt Avenue is not worthy of heritage designation under Regulation 9/06 of the *Ontario Heritage Act* based on its individual architectural, historical significance or contextual value. Heritage Planning staff have reviewed the Heritage Impact Statement, and concur with this opinion.

FINANCIAL IMPACT: There is no financial impact.

CONCLUSION: The property owner of 3110 Merritt Avenue has requested permission to demolish a structure on a property listed within a Cultural Landscape on the City's Heritage Register. The subject property comprises one of many homes built in the Victory Housing style and does not hold any particular historical, architectural or contextual interest which would warrant heritage designation under Part IV of the *Ontario Heritage Act*. As such, the request for demolition should be recommended for approval.

ATTACHMENTS: Appendix 1: Heritage Impact Statement by Gillespie Heritage Consulting



Paul A. Mitcham, P. Eng., MBA
Commissioner of Community Services

Prepared By: Laura Waldie, Heritage Coordinator - Planning

Heritage Impact Statement

3110 Merritt Avenue

Malton Victory Village Cultural Landscape
City of Mississauga

24 March 2014



Gillespie Heritage Consulting

2 Mayfair Court, Dundas, ON L9H 3P2 905 627 8607

CONTENTS

1	INTRODUCTION	1
1.1	Intent of Heritage Impact Statement for 3110 Merritt Avenue	1
1.2	Background on the Malton Victory Village Cultural Landscape.....	2
2	HISTORICAL AND ARCHITECTURAL BACKGROUND.....	2
2.1	Malton, its Airport and Related Industries	2
2.2	Wartime Housing in Canada.....	4
2.3	Wartime Housing Ltd. Land Acquisition and Victory Village.....	8
2.4	Victory Village since WWII	9
3	3110 MERRITT AVENUE: SETTING AND SITE DESCRIPTION	10
3.1	Present Setting and Cultural Background	10
3.2	Site Description	11
4	3110 MERRITT AVENUE: HISTORY, ARCHITECTURE, AND CULTURAL HERITAGE VALUE	12
4.1	Chain of Ownership and Building History	12
4.2	Historical Associations.....	13
4.3	Cultural Heritage Value	15
5	PROPOSED NEW RESIDENCE.....	16
5.1	Replacement Dwellings in the Malton Victory Village Cultural Landscape	16
5.2	Description of Proposed New Residence and Landscaping	17
5.3	Design Evaluation	17
6	CONCLUSIONS AND RECOMMENDATIONS.....	19
6.1	Existing Dwelling	19
6.2	Proposed New Residence.....	20
6.3	Recommendations	20
7	SOURCES, CONTACTS, SITE VISITS AND QUALIFICATIONS.....	21
7.1	Sources	21
7.2	Contacts.....	22

7.3	Site Visits	22
7.4	Qualifications of the Author	23
8	ILLUSTRATIONS	24
9	SITE VISIT PHOTOGRAPHS.....	52
9.1	Setting	52
9.2	3110 Merritt Avenue.....	60
APPENDIX A: Chain of Ownership.....		70

1 INTRODUCTION

1.1 Intent of Heritage Impact Statement for 3110 Merritt Avenue

Figure 1; cover illustration

The property at 3110 Merritt Avenue was purchased by the present owners, Mohammed and Rafika Manjra, in November 2011.¹ It is situated in a planned wartime subdivision located in the community of Malton in the north-east corner of the City of Mississauga. The lot is occupied by a small post-war bungalow which is currently rented. The owners plan to demolish this dwelling and replace it with a larger two-storey residence to be occupied by their own family.

The property is located in an area identified as a significant “cultural landscape” (residential category) in the *Cultural Landscape Inventory for the City of Mississauga*. In this report, the area is referred to as War Time Housing (Malton) in the Residential Landscape category, which includes 13 residential areas.² It is also referred to as the Malton Victory Housing Cultural Landscape but the term preferred by the author of this report and used henceforth is the *Malton Victory Village Cultural Landscape*, given that the entire planned subdivision known as Victory Village comprises the cultural landscape. All properties located within its boundaries (similarly to other cultural landscapes throughout Mississauga) have subsequently been added to the City’s Heritage Register. Accordingly, Heritage staff requires that a Heritage Impact Statement be prepared by a qualified heritage consultant for the substantial alteration/enlargement of an existing dwelling or its total replacement.³

This Heritage Impact Statement adheres to the *Cultural Landscape Heritage Impact Statement Terms of Reference* prepared by the Community Services Department of the City of Mississauga in June 2012. Its completion and acceptance by Heritage staff is a condition of obtaining a Building Permit. In contrast to other residential cultural landscapes, such as Mineola West and Lorne Park, this area is not subject to Site Plan Control. For properties located in designated Site Plan Control areas, Site Plan approval must be obtained before a Building Permit is issued and designs for replacement dwellings are evaluated in accordance with the City’s *Design Guidelines and Site Plan Requirements [for] New Dwellings, Replacement Housing and Additions* (April 2007). While these guidelines would be applicable to properties within the Malton Victory Village Cultural Landscape, they cannot technically be enforced through any planning

¹ The property is legally registered in the name of Rafika Manjra but all communication for the purpose of this Heritage Impact Statement has been with her husband Mohammed and their daughter Shirin.

² Landplan Collaborative Ltd., *Cultural Landscape Inventory* (January 2005); available on the CM website: www5.mississauga.ca/pdfs/Cultural_Landscape_Inventory_Jan05.pdf > L-RES-5 War Time Housing (Malton). All properties located in one of the approximately 60 *cultural landscapes* are listed on the City’s Heritage Register regardless of individual architectural / historic interest. Cultural landscapes and features include historic settlements; agricultural, industrial, urban, residential, civic and natural areas; parks; scenic views; scenic roadways; bridges; and wall formations.

³ Under the provisions of Section 27 (1.2) of the Ontario Heritage Act, a 60-day delay of demolition is imposed once the Heritage Impact Statement has been approved by Heritage staff.

process. Moreover, the Malton District Policies of Mississauga Plan (Section 4.19) provide no policies that specifically address the Malton Victory Village Cultural Landscape.

1.2 Background on the Malton Victory Village Cultural Landscape

Figure 3; Figure 4

The Malton Victory Village Cultural Landscape is located on the east side of Airport Road north of Derry Road. The original subdivision, which became known as “Victory Village”, and the present-day cultural landscape with the same boundaries, encompasses sections of Victory Crescent, McNaughton Avenue, Churchill Avenue, Merritt Avenue, Etude Drive and Lancaster Avenue. It comprises a fairly homogeneous residential subdivision of wartime and post-war housing consisting largely of 1 to 1 ½ storey frame houses with medium to steep-pitched, side-gabled roofs and central doorways. However, this character has been increasingly threatened by the incremental intrusion of larger two-storey suburban dwellings into the area, which can be discouraged but not prevented without imposing additional planning controls.

As described in the *Cultural Landscape Inventory*, Section L-RES-5:

This planned subdivision is located opposite the north-east corner of Pearson International Airport. The neighbourhood is close to where the original Malton Terminal was located and remains close to the present airplane manufacturing and service industry. Although some of the original houses have been altered with newer porches, dormers, raised basements and garages, many retain characteristics typical of the period with 1 to 1 roof pitches, central front doors, picture windowed living rooms to one side, kitchen and eating areas on the opposite side and bedrooms and bathrooms to the rear. According to local sources, one in four of the houses was moved from Bramalea Road when the airport was expanded in 1950. The relocated houses and lots sold for \$2,500.00 each. The street names in the area, including Churchill Avenue and Victory Crescent, act as reminders that this area was developed during the post-war period [and also the war period as later described]. Its significance lies in the fact that it retains a number of post-war houses which represent some of the first mass produced housing in the GTA.

2 HISTORICAL AND ARCHITECTURAL BACKGROUND

2.1 Malton, its Airport and Related Industries

Figure 5; Figure 6; Figure 7

Malton originated as a small farming community, centred on the north-south boundary between Toronto Gore and Toronto Townships (now Airport Road). Malton was ceded to Toronto Township in 1952, and then incorporated into the Town of Mississauga in 1967, and finally the City of Mississauga in 1974.

One of the earliest and most influential settlers was Richard Halliday, a native of Malton in Yorkshire, England, and blacksmith who arrived in 1819. The village of Malton developed around the “four corners” of what is now the intersection of Derry and Airport roads, and by 1850 the village consisted of a general store, a cobbler’s shop, a small hotel and blacksmith’s shop. The arrival of the Grand Trunk Railway in 1854 provided local farmers with easier access

to markets and contributed significantly to the development of Malton as a major grain handling and export centre. Malton was awarded the county seat in 1859, which it held for one year, and was incorporated as a police village in 1914.

In 1937 Malton was chosen as the site for a new international airport for the Trans-Canada Airlines (the precursor of Air Canada). 13 farms south of Derry Road were purchased in 1937-8 by the City of Toronto and farm buildings levelled for the construction of runways and a terminal building. With the outbreak of WWII, the new airport also became the base for the Commonwealth Air Training Plan where aviation bombing practices were held. In 1938, the National Steel Car Company of Montreal opened an aircraft factory on lands to the east of the airport and south of Derry Road. The factory, which first manufactured the Avro Anson and Westland Lysander, brought hundreds of employees to Malton. In 1941 or 42, the plant was taken over by the federal government as part of the war effort for use as a centre for allied aircraft production and the company was then renamed Victory Aircraft Ltd. With a workforce of 10,000, housing was immediately needed for many of its workers and families. This resulted in the construction of a nearby subdivision to the east of Airport Road and north of the plant, known as Victory Village.

At the end of the war, the assets of Victory Aircraft Ltd. were sold to A.V. Roe Canada Ltd., a subsidiary of the British Hawker Siddely Group, the largest conglomerate of aeronautical manufacturers in the world. In 1949, this company began manufacturing the C-102 “Jetliner”, North America’s first jet passenger plane and CF100 “Canuk” fighter jets for the Royal Canadian Air Force. A.V. Roe is best known for the development of the CF105 Arrow fighter jet, the “Avro Arrow”, which was to have been the most advanced of its kind in the world. By the end of the 1950s an extensive industrial area had developed east of the airport and south of Derry Road, which included A.V. Roe Canada and Orenda Engines Ltd. (originally a division of A.V. Roe but now owned by Magellan Aerospace Corporation). Unfortunately for Malton, the production of this supersonic fighter jet, unveiled in October 1957, was short-lived.⁴ Production was abruptly terminated in February 1959 by Prime Minister John Diefenbaker, largely as a cost-saving measure, and the company was sold in the 1960s to the American company, McDonnell-Douglas. This company, in turn, was taken over by Boeing, which demolished most of the original Victory Aircraft buildings between 2003 and 2004. A major expansion of Malton Airport in 1963 resulted in its reopening by Prime Minister Lester B. Pearson in 1964 and its renaming as Lester B. Pearson International Airport.⁵

⁴ Although the Arrows manufactured in Malton were production models rather than prototypes, the Arrow program was terminated before the aircraft entered service with the RCAF.

⁵ The research for this section was undertaken for the Heritage Impact Statement for 7157 Lancaster Avenue and re-used in this report: sources included: Kathleen Hicks, *Malton: Farms to Flying*, Part 3, 1900-1950; Heritage Mississauga’s website (www.heritagemississauga.com/page/Malton) and other sources cited in that report (footnote 5).

2.2 Wartime Housing in Canada

2.2.1 History

The first national housing legislation was introduced during the Great Depression, beginning with the Dominion Housing Act of 1935, which provided \$20 million in loans and helped finance 4,900 housing units over 3 years. The Government of Canada continued to be involved in housing during the Second World War. On January 1st, 1940, Wartime Housing Ltd. was established as a Crown corporation by the Department of Munitions and Supply to address this crisis. Its mission was to acquire tracts of land in many communities and build non-profit, subsidized rental housing for workers employed in industries supporting the war effort. By 1940, a war-related industrial boom had created overcrowded conditions and serious housing shortages, which in turn, disrupted industrial production. In 1940 some defence industries, such as Small Arms Ltd. in Long Branch, responded by constructing temporary housing for employees close to their plants. Between 1941 and 1947, approximately 32,000 rental units (mostly single-family dwellings) were erected across the country to accommodate munitions workers, servicemen's families and, after the war, returning veterans. In 1944, Wartime Housing Ltd. moved beyond its original strict mandate to providing accommodation for defence-industry workers because of the threat of evictions and homelessness for the families of soldiers fighting overseas. To address this problem, a Veterans' Housing Program was created to provide affordable housing for returning veterans. The National Housing Act of 1946 created Central Mortgage and Housing Corporation (CMHC) and the following year a large proportion of the housing built for Wartime Housing Ltd. was transferred to CMHC. In 1949 the remaining assets of Wartime Housing Ltd. were taken over by CMHC, which then proceeded to register all wartime and post-war surveys as legal plans of subdivision with the intent of selling off the individual lots and houses. Once a plan of subdivision was registered in the local registry office, lots could be sold to private owners but this occurred gradually over time, depending on the tenants' circumstances. Existing tenants would be offered the first right of purchase but if a tenant turned down the offer, then the lot with house was advertised on the free market. Gradually divested of its initial role as landowner and landlord, CMHC evolved a mandate which now includes the provision of mortgage loan insurance and mortgage-backed securities and the development of housing policy and programs.⁶

2.2.2 Character of the Wartime Subdivisions

The wartime subdivisions were originally intended to provide temporary housing, with the federal government pledging to remove them soon after the war. However, it was soon realized that these new planned residential neighbourhoods could provide permanent, low-cost housing for working class families as well as communal amenities. The establishment of a tenant relations department encouraged the development of community centres, which served a wide range of civic functions. Typically, they provided space for prenatal clinics, libraries,

⁶ The research for this section was also undertaken for the Heritage Impact Statement for 7157 Lancaster Avenue (with sources cited in footnote 6) and re-used in this report.

garden clubs, cooking and sewing classes, first aid courses, supervised children's play, youth organizations, home improvement associations, and community councils.

The uniqueness of wartime housing subdivisions stems not from the style or construction of the individual houses but their *raison d'être*, as a response to the unique housing needs created during WWII and their planned nature based on City Beautiful principles. Characteristic features included interesting street layouts with boulevards, crescents, cul-de-sacs, and curved roadways. Streetscapes were homogenous but not uniform with a mix of house forms based on standard Wartime Housing Ltd. plans for modest one and 1 ½ storey dwellings. Roadways were surfaced with gravel, wood board sidewalks and walkways and trees were planted. These subdivisions typically also included park reserves for outdoor recreational use and community centres, and possibly also an elementary school. The lots were relatively large compared to the size of the houses which combined with the boulevards and park spaces created an overall feeling of spaciousness.

As these wartime subdivisions evolved into their present-day appearance, alterations and additions were progressively made to the original dwellings, roadways were paved, concrete sidewalks were laid and trees matured. Compared to the barren character of the new subdivisions, today these neighborhoods are often enhanced by an abundance of mature deciduous and coniferous trees on boulevards and in public parks.

2.2.3 House Design and Construction

Figure 8 to Figure 14

For the first time in Canada, during World War II dwellings were constructed on a large scale of prefabricated components that could be quickly assembled on site by relatively unskilled labourers, thereby adopting the mass-production techniques of wartime industries.⁷

Two sources provide detailed information on the construction of wartime dwellings: "Wartime Housing", a short documentary produced by the National Film Board in 1943 and an article entitled "Wartime Housing and Architectural Change, 1942-1992," published in 1995.⁸ The documentary, which promotes the work of Wartime Housing Ltd, provides valuable insight into the standardized, prefabrication techniques employed to expedite construction of urgently needed housing for industry workers. Blueprints of standard designs were prepared by architects employed by the company and distributed to local contractors. Built house forms represented many variations of two basic models of frame construction: a single storey and a 1 ½ storey: H1 (24' square with a living room, two bedrooms, kitchen and bathroom on one floor); H2 (same plan reversed); and H12 (a slightly larger version of H1: 24' x 28') with an

⁷ A general overview of Canadian wartime housing and its significance is provided by John Blumenson, *Ontario Architecture: A Guide to Styles and Building Terms, 1784 to the Present* (Fitzhenry & Whiteside: 1990), Chapter 24: Victory Housing (1940-50), pp. 219-223; and Thomas Wicks, "Wartime Housing", blog posted on the Spacing Toronto website, October 2007: <http://spacingtoronto.ca/2007/12/12/wartime-housing>

⁸ "Wartime Housing", National Film Board documentary by Graham McInnes, 1943; available online at www.nfb.ca/film/wartime_housing; Annmarie Adams and Pieter Sijpkens, "Wartime Housing and Architectural Change, 1942-1992." (see SOURCES: Section 7.1.2.)

additional two bedrooms in the attic area provided by a steep-pitched roof. Roofs took the following forms: hipped with a low pitch, side or front gabled with medium or steep pitches, all with very shallow eaves. The different floor plans generated both symmetrical and asymmetrical facades. The 1 ½ storey models had steeply pitched gable roofs with attic space for two additional bedrooms but dormers were omitted as a cost and labour saving measure. Overall both the one and 1 ½ storey designs represent simplified interpretations of the Colonial Revival style, with the 1 ½ storey model often referred to by historians as Cape Cod Colonial. The characteristic multi-paned vertical sash window design chosen for wartime housing was a typical feature of this style.

According to the NFB documentary, teams of specialized tradesmen (carpenters, plumbers, electricians, etc.) worked in an assembly like manner in three shifts, enabling an entire dwelling to be erected in less than 36 hours. However, it is known that there was a problem then as today with “jerry builders”; hence, many wartime subdivision may have been built up to speed but not to the standards expected by Wartime Housing Ltd.

As the dwellings were intended to be temporary and dismantled after the war, the first plans did not include basements, except where necessitated by harsh winter conditions, as was the case in central Canada. **(Figure 11)** As documented in the NFB film, the first site work was the erection of a temporary structure to mill the lumber and prefabricate floor, wall and roof sections, and doors and windows, etc. The first step in the construction of an individual dwelling was to bore holes several feet deep to insert creosoted cedar posts or less commonly concrete posts poured on site. Floor beams (sill plates) were then attached to the row of levelled posts (two or three rows depending on the depth of the house). A typical floor, wall, ceiling or roof section consisted of framing with a backing of wood planks or possibly plywood. Floor sections with outer boards covered with tar paper were first attached to the sill plates. Wall sections were then erected, nailed to the sill plates and bolted together. Next the ceiling sections were hoisted up and into place on the wall sections and bolted together and to the wall sections. Finally two triangular sections were erected at each of the two gable ends and then the rectangular sections were raised into place and attached to the gable sections. At this stage, the exterior house framing was complete and work could proceed on the exterior finishes and the installation of window units and doorways. Rolls of tar paper were nailed to the wood sheathing and the outer cladding nailed on. The most common siding materials were clapboard, wood shingles, or composite shingles.⁹ Window units comprising a wood frame with a multi-paned vertical sash window were entirely prefabricated in several standard sizes (including painting). It is assumed that, given the extremes of Canadian climate, the wartime dwellings were provided with storm windows. Doorway frames were similarly prefabricated and installed without the doors, which were subsequently hinged to the frames. Door designs typically featured panelling with multi-paned glazing in the upper panes. The 1943 NFB documentary also shows that storm doors may also have been provided for extra weather protection. Once the roof sheathing was covered with asphalt shingles the dwelling was now

⁹ It is assumed that the term “composite shingles” refers to ones made of asbestos-cement, a mixture of portland cement reinforced with asbestos fibers. It was commonly used as a siding material on wartime and post-war housing for economical and practical reasons.

weathertight. A prefabricated “fuel box” was added to the rear facade to provide coal storage for a coal-fired stove, vented by a pipe through the roof. A final finishing task was painting the exterior cladding, shown in the NFB film to be done with a spray gun.

Interior work began by insulating the floor sections with rock wool, laying the subfloor and the installation of electric outlets and wiring. Interior wall sections were similarly insulated with rock wool stuffed into the spaces between the studs; tar paper was then stapled to the studs and the finish material consisting sheets of pressed wood or gypsum boards nailed to the framing. These prefabricated panels, the precursor of modern drywall, were much more time and cost effective than traditional lath and plaster.¹⁰ Flooring typically consisted of narrow strip tongue-and-groove hardwood in the living and bedroom areas and linoleum or asphalt tile in the kitchen and bathroom. Baseboards and trim were cut to size on site from specially milled dimensioned lumber. Interior doors in wartime and early post-war housing were a solid panelled type.¹¹ Wartime dwellings were all provided with interior plumbing: every plan included a bathroom, with a toilet, sink and bathtub and a kitchen counter with a sink. By the 1940s, electric stoves were widely available for cooking. Dwellings with no basements were equipped with a heating stove, also referred to as a “space heater”, which was located centrally on the main floor as indicated by a central chimney or metal vent pipe.¹²

2.2.4 Alterations and Additions since the 1940s

Alterations, upgrades, and additions often began soon after the tenants or prospective buyers took possession of the previously rented dwellings. Some documentation indicates that CMHC was supposed to construct basements under the wartime dwellings prior to their sale but it is not clear to what extent that happened. It is known that many were sold without this amenity. According to the article “Wartime Housing and Architectural Change”, CMHC installed partial basements in the houses in the Montreal’s St. Laurent wartime neighbourhood prior to their sale from 1964 on.¹³ As most residents would have preferred full basements to provide additional living space, the task fell upon each new owner with help from neighbours to jack up and support the dwelling, while the ground below was excavated deeper. A full basement, in both wartime and postwar dwellings, was built of poured concrete or concrete block walls resting on concrete footings, with a poured concrete floor. With a full basement, the ground floor heating stoves could be replaced with a coal-fired boiler distributing heat through radiators and greatly improving comfort levels in winter. Home heating systems were again improved in the 1950s with the availability of oil-burning furnaces. During the 1950s heating systems underwent more improvements with the installation of oil-burning furnaces and additional insulation. Aluminum storm doors and windows were also commonly added to

¹⁰ According to a 2000 CMHC publication on post-war 1 ½ storey houses, early post-war models had interior walls finished in lath and plaster, which was gradually replaced by drywall in the 1950s. (CMHC, *Renovating Distinctive Homes – 1 ½ Storey Post-War Homes*, p. 8)

¹¹ CMHC, op. cit., pp. 8-9.

¹² According to the previously cited article “Wartime Housing and Architectural Change”, these space heaters were found to be inadequate by most residents of wartime houses in the St. Laurent neighbourhood of Montreal. (p. 18)

¹³ Ibid. p. 23.

further improve insulation value. The original asphalt shingles were replaced or covered with new asphalt-shingles, roofs were also extended with wider soffits and the installation of eavestroughing and downspouts added to better drain water away from the foundation walls. During this decade, when car ownership became increasingly common, owners built free-standing garages in the rear yards or added a carport or garage to the side of the house.

In the course of the next few decades, common alterations included rear additions, new front and back porches, covering of original siding with aluminum and later vinyl siding, replacement of original windows with vinyl-clad, thermopane units; and replacement of original wood doors with insulated doors made of steel or fibreglass. These changes have invariably altered the exterior appearance of the original dwellings, to a lesser or greater degree and sometimes almost beyond recognition. However, unless the house has been substantially enlarged and renovated, the original interior wall partitions and doorways are usually still largely intact. Common alterations include layers added to wall and floor surfaces, such as wallpaper, wood panelling, vinyl tile, laminate flooring, etc. Kitchen and bathroom plumbing fixtures have been replaced, and in some cases, more than once. Small energy-efficient gas furnaces have replaced older furnaces in basements or been installed on the main floor if there is no basement.

2.3 Wartime Housing Ltd. Land Acquisition and Victory Village

Figure 15 to Figure 18

In the course of 1942, Wartime Housing Ltd. proceeded to purchase or expropriate parcels of farmland owned by Fred Codlin, on the east side of 6th line (now Airport Road) north of the Malton Side Road (Derry Road). In 1939, Fred Codlin had partnered with a developer (Egvin Kay Ltd.) to register a land subdivision agreement for the construction of 41 dwellings, but with the outbreak of WWII, this plan was abandoned. The final boundaries of the property acquired by Wartime Housing Ltd. are shown on Plan H-20-A, registered with the Deed of Land as Instrument 3431 on October 29, 1942. Excluded was a 50' wide strip of land in the south-east corner conveyed to the National Steel Car Company¹⁴ and a truncated L-shaped parcel owned by E. Johnson (according to the Deed of Land). In the end, Wartime Housing Ltd. acquired all of Codlin's property in Lot 11 except the excluded strip, which in total comprised 91.4 acres.¹⁵

It is speculated that the Victory Village subdivision was surveyed immediately following the registration of the Deed of Land and plans for the subdivision drawn up by Wartime Housing Ltd. as quickly as possible, given the urgency of the housing situation in Malton. A small subdivision for the construction of 200 dwellings, surveyed by H.C. Sewell, OLS, was intended to provide rental housing for workers at the Victory Aircraft plant and their families. It became known as Victory Village. All of the rectangular lots were a standard size: 40' wide and 100' deep. After the war, these houses continued to accommodate families of workers engaged in

¹⁴ See Section 2.1 for background on the National Steel Car Company. The purpose of the 50' strip is not known.

¹⁵ The title search undertaken by Paul Dilse for his Heritage Impact Statement for the property at 7181 Lancaster Avenue (August 2013) yielded a survey plan dated April 1942 which shows three parcels of land acquired by Wartime Housing Ltd. (H-20) but not the final boundaries shown on Plan H-20-A. (**Figure 15** and **Figure 16**)

aircraft production and related aeronautical manufacturing, a primary industry providing employment for up to 12,000 men and women through the 1950s. The industrial complex, concentrated around Airport Road and Derry Road East was demolished in recent history, leaving only the housing subdivision as a visible reminder of this important aspect of Malton's history.¹⁶

The Plan of Subdivision shows the layout of the streets, the number and shape of the building lots and two plots set aside for park space and a public school. The irregularly shaped Block A became Victory Park. A public school (now Malton Bible Chapel) was built on the rectangular Block B at the corner of Churchill Avenue and Victory Crescent and a community centre, known as Victory Hall was built on parkland to the north of the school site facing Victory Crescent. The informal street layout included one curvilinear roadway: Victory Crescent. The park and streets were given war-related names. For example, Churchill Avenue was named after England's prime minister, Winston Churchill; Lancaster Avenue after the Lancaster Bomber, and McNaughton after Lieutenant-General Andrew McNaughton, Commanding Officer of the Canadian Army during WWII. As was typical of wartime housing, the dwellings were all modelled on a few standard plans and partially prefabricated off-site to expedite construction.

2.4 Victory Village since WWII

Figure 3; Figure 4; Figure 19

Central Mortgage and Housing Corporation had the Plan of Subdivision registered in the County of Peel Registry Office in February 1952, after which individual lots and houses were first offered for sale to existing tenants. Unfortunately, there is no known dated plan showing the number of lots developed by 1952. It might be assumed that houses were built on all 200 lots during the war, given the critical need, but according to local sources cited in the Site Description for the *Malton Victory Housing Cultural Landscape*, one in four of the houses were moved from Bramalea Road when the airport was expanded in 1950. According to a 2008 CM Corporate Report, the subdivision then contained only 192 dwellings, 21 of which had been replaced or substantially altered, but the number since demolished and replaced has not been enumerated.¹⁷

The original Victory Village subdivision is now surrounded by more recent residential development. The westward extension of Merritt Avenue as a cul-de-sac and Landen Court to the south both appear to have been built in the early to mid 1960s and include a mix of detached and semi-detached one and two storey dwellings. A short post-1966 cul-de-sac extension of Merritt Avenue to the east contains six semi-detached residences. All of the original streets except Churchill and McNaughton have since been extended. To the west of the original subdivision, on the west side of Airport Road (now a busy multi-lane street), appear to be typical 1960s two-storey mixed commercial / residential buildings with retail space on the ground floor and apartments above.

¹⁶ Previously cited CM report, "Proposed Zoning and Design Guidelines for Malton Victory Housing Cultural Landscape", p. 2.

¹⁷ Previously cited CM report, "Proposed Zoning and Design Guidelines ...", pp. 2-3.

Victory Hall and Victory Public School are still standing but have not continuously served their original functions. In 1948 Victory Hall became an annex to Victory Public School. The school remained open until the 1960s and has since been converted to a place of worship, known as the Malton Bible Chapel.¹⁸ With the opening of the Malton Community Complex in 1977, Victory Hall was used by the Malton Community Service group and remains a community centre at 3091 Victory Crescent. Victory Park must have been extended northward and westward when this area was developed post 1966.¹⁹

Prior to the turn of this century, changes to the housing stock within wartime/ early post-war subdivisions were mainly limited to alterations (e.g. new doors, windows, siding and porches) and additions to the houses as well as the construction of carports and garages. This is still the case in Kitchener's St. Mary's postwar neighbourhood of veterans' housing, now recognized and protected as the *St. Mary's Heritage Conservation District*, where there are tighter restrictions on the demolition and replacement of the original dwellings, as well as alterations and additions.²⁰ (Figure 20) In contrast, the wartime dwellings in the Malton Victory Village Cultural Landscape may be altered, enlarged and demolished without any such restrictions. Moreover, due to relatively high land prices in the Malton area, there is an escalating trend towards the demolition and replacement of the original housing, which threatens to undermine the special wartime character of this area. Recently built replacement dwellings are a full two storeys with brick veneer or stone cladding and most have built-in garages for one or two vehicles. Their design is typical of new subdivision housing, which stylistically falls into the general category identified as *Neoelectic*.²¹ Their footprints are larger than the existing original dwellings and with their increased height, these new houses have a considerably larger mass and footprint to yard ratio. In sum, they stand out rather than fit in sympathetically with the original wartime housing. If this trend is allowed to continue, the former Victory Village will lose its wartime heritage and increasingly take on the character of a typical middle-class suburban subdivision.

3 3110 MERRITT AVENUE: SETTING AND SITE DESCRIPTION

3.1 Present Setting and Cultural Background

Figure 1; Figure 2; Figure 3; Figure 4; Photo 1 to Photo 16:

The Malton Victory Village Cultural Landscape is located within the planning district known as Malton, whose boundaries are shown on the Malton District Land Use Map. (Figure 2) To the north is the City of Brampton and to the east the City of Toronto. The oldest part of Malton,

¹⁸ Kathleen Hicks, *Malton: Farms to Flying*, "Victory Village-1942", pp. 138-40.

¹⁹ Research for this paragraph was undertaken for the *Heritage Impact Statement for 7157 Lancaster Avenue...*, which also includes photographs of Victory Park, Victory Hall and the Malton Bible Chapel.

²⁰ The St. Mary's Heritage Conservation District was described in the *Heritage Impact Statement for 7157 Lancaster Avenue...*, completed by the author of this report in 2011 and recently explored through Google Street View.

²¹ Term borrowed from *A Field Guide to American Houses*: "Neoelectic, ca. 1965 to present", pp. 486-95.

the former Police Village of Malton, is located west of Airport Road and north of Derry Road and was originally settled by immigrants of British descent.

Since WWII, the demography of Malton has changed significantly, beginning with an influx of Italian and Polish immigrants from the immediate post-war period through the 1960s. Since then, Malton's proximity to an international airport has attracted many immigrants from India (including a large Sikh community) and in recent years an increasing number from Pakistan, Sri Lanka, and Trinidad and Tobago. Malton's current population of over 40,000 consists largely of immigrants from these countries and seniors of Italian-Canadian descent.²²

The broader setting for the subject property consists of the area defined as the Malton Victory Village Cultural Landscape, located on the east side of Airport Road north of Derry. Its immediate setting comprises the original section of Merritt Avenue between Lancaster Avenue and Victory Crescent, which includes a total of 36 lots. The houses with even numbers, including 3110 Merritt Avenue, back onto the rear property lines of properties facing Purnell Court, which lies outside the boundaries of the original plan of subdivision. The wide roadway of Merritt Avenue has no sidewalks or boulevards but, similarly to other parts of the Malton Victory Village Cultural Landscape, is lined by trees of varying maturity. To date, there have been no drastic changes to the streetscape by the intrusion of new two-storey replacement residences. In fact, the two-storey houses at #3319 (**Photo 14**) and #3094 (**Photo 15**) are both original dwellings with second storey additions and other alterations. The only entirely new dwelling is the one at #3155 (one lot from the corner of Lancaster Avenue), which is typical of the new houses built in this area, most of which have a built-in single or double garage. (**Photo 16**) The lots throughout the original subdivision are all similar in size and depth (and rectangular in shape, except for some on the curved section of Victory Crescent). All 36 lots facing Merritt Avenue had and appear to still have the standard rectangular lot size of 40' wide by 100' deep.

3.2 Site Description

Figure 3; Figure 21; Photo 1; Photo 2; Photo 17 to Photo 24

The subject property comprises a long, narrow 40' x 100' lot occupied by a wartime bungalow and a free-standing wood-frame shed, originally built as a garage with storage space, in the west corner of the rear yard. Along the south-west side of the dwelling is a worn gravel driveway abutting the asphalt driveway of the adjacent dwelling at #3114. Frontyard landscaping consists of grass cover with a medium-sized flowering deciduous tree (possibly a Crab Apple) planted close to the dwelling. Located on the City's road allowance is a fire hydrant. There are several trees in the front yard of the adjacent 1 ½ storey dwelling at #3106, including a tall but sparse spruce tree close to the property line, a beech sapling on the City's road allowance, a semi-mature maple very close to the property line shared with #3110 and the east corner of its dwelling, and a mature maple tree close to the driveway, which largely obscures the front facade when in full leaf. The front yard of the adjacent dwelling at #3114 includes one semi-mature deciduous tree.

²² Website sources: www.malton.org/maltonhistory/intro.htm; [http://en.wikipedia.org/wiki/Malton, Ontario](http://en.wikipedia.org/wiki/Malton,_Ontario)

The rear yard is enclosed by a chain link fence extending forward of the shed in the east corner of the property to about mid-way between the two dwellings (property line shared with #3014) and on the south-west side between the two dwellings (property line shared with #3106), terminating approximately at the road allowance. As shown on the 2013 Survey Plan, there are three semi-mature to mature maples trees: one in the east corner of the property beside the shed, one straddling the property line shared with #3106, one between the two houses just inside the fence on the side of #3106 and one in the rear yard, which appears to straddle the south-west property line. Beyond the rear property line to the south-east are the back yards of more recent housing on Purnell Court.

4 3110 MERRITT AVENUE: HISTORY, ARCHITECTURE, AND CULTURAL HERITAGE VALUE

4.1 Chain of Ownership and Building History

Figure 5; Figure 15 to Figure 18; Appendix A: Chain of Ownership

The Crown grantee for the 200 acre parcel of land, identified as Lot 11, Concession 7 was King's College, founded in 1827. This grant was part of an endowment by Royal Charter of 225,000 acres of unsettled land to King's College in 1828, much of which was located in Toronto Township (now part of the City of Mississauga).²³ The entire lot 11 was sold in two 100-acre parcels respectively to Hugh Cook in 1841 and to Alexander McDonald in 1842. That same year, McDonald's property was willed to his wife Mary then passed on to his son Alex in 1853. Upon Alex's death it was willed to Eliza McDonald (wife or sister). The farm property stayed in the McDonald family until her death in 1890, when the west half of lot 11 north of the Grand Trunk Railway tracks, was sold by the Executor of her estate to Thomas Codlin (95 acres). Codlin retained ownership of most of this parcel until 1942, prior to which he partnered with a building company, Egvin Kay Ltd. to plan the small subdivision described in Section 2.3.²⁴ Through three transactions listed in Appendix A, the parcel of land shown in a survey plan registered with Deed of Land #3431 (**Figure 16**) was sold to or expropriated by Wartime Housing Ltd. in 1942. This deed also indicates that an odd-shaped parcel north of the G.T.R. tracks belonged to E. Johnston. Soon after Wartime Housing Ltd. acquired the property, the lands designated for a wartime housing subdivision were surveyed and developed. However, the Plan of Subdivision (#436) was not registered until 1952, by which time the Central Mortgage and Housing Corporation (CMHC) had acquired the assets of Wartime Housing Ltd. with the intent of selling the lots and dwellings, previously occupied by tenants of Wartime Housing Ltd.

The subject property comprising Lot 114, Plan 436, was first sold by CMHC to Frederick and Lilian Norman in 1963. It remained in the Norman family until 1999, when it was sold to Barbara Bonifacio, whose family retained ownership until 2005. It was then sold to Marlene

²³ Information provided in a telephone message by Matthew Wilkinson, Historian, Heritage Mississauga. Founded by Royal Charter in 1827 as the first institution of higher learning in Upper Canada, King's College expanded to become the present-day University of Toronto.

²⁴ The title search did not reveal any transaction between Fred Codlin for the transfer of a parcel of land north of the CNR right-of-way to E. Johnston.

and Saitnauth Dyal, who in turn sold to Shevil Ford in 2006. In 2010, Ford sold the property to Otis Kwaku Duah; in November 2011, he sold it to current owner, Rafika Manjra (registered owner) and spouse Mohammed Manjra. Short-term ownership since 2005 suggests that the property may have been purchased as an investment and occupied by tenants since that time..

4.2 Historical Associations

Given the humble nature of the existing wartime dwelling, it would not be expected to have any important historical associations with respect to the original tenants and succession of owners. Victory Village, as a whole, was built on farmland owned by the McDonald family from 1842 to 1890 and subsequently by the Codlin family up to 1942. Fred Codlin appears to have been a prosperous farmer and prominent member of the Malton community. Moreover, his intent to build a small subdivision on part of his property indicates that he had ambitions beyond farming. Kathleen Hicks, in her history of Malton, includes the following interesting facts about Fred Codlin. When the first telephone exchange was opened in Malton during WWI, Fred Codlin became the first resident to receive a telephone. The Codlins were also the first family in Malton to own an automobile: the Ford Model T.²⁵

Collectively, all of the residential properties in the former Victory Village subdivision have an important historical association with the whole phenomenon of wartime industry and the need for expediently built temporary housing to accommodate the multitude of employees needed to meet the production demands of the war.

4.2.1 Dwelling Exterior

Photo 17 to Photo 25

The existing one-storey dwelling is a variation of the standard H1 design, with a square floor plan, in this case with exterior dimensions of 25' x 25', which originally had no basement but now has a partial one. It has a side-gabled medium-pitch roof, a symmetrical facade with a central doorway now hidden within a small enclosed entrance vestibule with two horizontal sliding windows. The roof still largely maintains its original form, with virtually no eaves on the gable ends, finished with simple moulded wood fascia/soffit boards, but has been extended the full width of the front facade to create a verandah (now partially enclosed). At the rear, the kitchen has been enlarged by a shallow extension with a doorway and horizontal sliding window. Based on historic photos of wartime bungalows, one can easily visualize its original construction and appearance. The frame structure would have likely just been supported by timber posts and sat lower to the ground. The original exterior cladding of asbestos shingles is still intact on the front and side facades. The enclosed front porch and rear extension are clad with vinyl siding. Fenestration would have consisted of the standard prefabricated multi-paned wood sash units (two sizes: six-over-six and four-over-four). This dwelling has most of its original window frames intact, except for the rear extension. All of the original windows or their replacements have been replaced by vinyl vertical sash thermopane units. The horizontal sliding windows suggest that both the rear extension and front entrance vestibule were added

²⁵ Kathleen Hicks, *Malton: Farms to Flying*, p. 97 (telephone); p. 103 (automobile).

by the original owners in the 1960s. Since that time, a large wood deck spanning the entire back wall of the extension has been built.

It is not known if this dwelling originally had a front porch. If so, it would have looked like one of the designs illustrated in historic plans and photos. Given that the first owners of this property did not take possession until 1963, it is quite possible that the house was already raised onto a concrete block foundation with a partial basement built by CMHC but all other additions/ alterations were made by the original or subsequent owners.

4.2.2 Dwelling Interior

Photo 26 to Photo 36

The floor plan consists of six rooms, including the added entrance vestibule, from which one enters the living room. To the left is the front bedroom and in the corner is an added enclosure for the relatively recent forced-air furnace with overhead ductwork. Beyond the living room is a very short hallway leading into the kitchen with a bathroom on the right. On the left of the kitchen is a second bedroom. Flooring consists of new laminate flooring in the living room, carpeting in the bedrooms with the original hardwood flooring intact beneath, and tile in the kitchen and bathroom. The original wall construction is still intact. Original panelled doors have been replaced with hollow core flat slab wood doors typical of the 1960s.

4.2.3 Condition and Integrity

Through a succession of owners, the existing dwelling has been maintained in good condition with energy-efficient improvements such as the relatively recent window and door installations. These and other alterations, such as the enclosure of the front porch, have compromised somewhat the architectural integrity of the original design, as is very often the case with “improved” wartime dwellings. However, they may be considered *reversible*, in the sense that theoretically, the existing windows and doors could be replaced with new ones replicating the look of the original prefabricated units and an open porch recreated, thereby restoring the detailing characteristic of the simplified Colonial Revival style adopted by Wartime Housing Ltd. More important, there have been only minimal alterations to the original gable-roofed, rectangular form, with a shallow rear extension, and most of the original asbestos shingle siding is still intact. All of the original window and door frames have been preserved except for ones in the section of original rear wall removed for the extension. Likewise on the inside, original partition walls and doorway openings have not been altered. Original hardwood flooring survives under wall-to-wall carpeting in the bedrooms and possibly also below the laminate flooring in the living room. Overall, this small wartime bungalow has survived in good condition with a medium degree of integrity.

4.3 Cultural Heritage Value

4.3.1 Evaluation based on the Heritage Designation Criteria, Regulation 9/06 of the Ontario Heritage Act

The following evaluation of the property is based on the *Criteria for Determining Cultural Heritage Value or Interest, O. Reg. 9/06*, of the Ontario Heritage Act (abbreviated as OHA). A property may be designated under Section 29 if it meets one or more of 9 criteria (3 in each category).

1. DESIGN OR PHYSICAL VALUE:

The existing dwelling at 3110 Merritt Avenue is typical in design of temporary housing built across Canada by Wartime Housing Ltd. between 1941 and 1945 based on standardized plans and built quickly and cheaply using prefabrication techniques. Apart from the alterations described above, the existing dwelling largely retains its original form and original features such as door and window frames. While the original cladding is still intact, its architectural integrity has been somewhat compromised by the replacement of original doors and windows and the enclosed entrance vestibule. Individually, the dwelling is not a rare, unique, or early example of a style, type, expression, material or construction method; it does not display a high degree of craftsmanship or artistic merit; nor does it demonstrate a high degree of technical or scientific achievement. Hence, the existing dwelling does not have sufficient design or physical value to meet this criterion.

2. HISTORICAL OR ASSOCIATIVE VALUE:

Similarly to all of the properties in Victory Village, 3110 Merritt Avenue has distant historical associations with one of the early settlers of Toronto Gore Township, Alexander McDonald and family and subsequently the family of Fred Codlin, a well-known member of the Malton community in early 20th century. Given that all of the Victory Village housing was conceived as modest rental units for single families whose income was derived from employment in the nearby aircraft industries, it would not be expected that any of these dwellings would have a strong ranking in any of the three criteria under item 2: The Victory Village subdivision, as a whole, has significance with respect to an organization/ institution, namely Wartime Housing Ltd. that had a huge impact on communities across Canada, including Malton, by providing the necessary housing for industry workers. However, individually, the subject property is not known to have any significance relating to a theme, event, belief, person, activity, organization or institution in the community; it is not known to possess any characteristics that contribute to an enhanced understanding of the community or culture; nor does it represent the work of a well-known architect, artist, designer or theorist in the community. Hence, it does not meet this criterion.

3. CONTEXTUAL VALUE

The subject property does have some contextual value with respect to criteria 3ii, in that it is physically, functionally, visually and historically linked to its surroundings, as are all

the Victory Village properties with original dwellings of modest size on relatively large lots. The existing dwelling is certainly not a landmark (3iii) but the house and lot together, similarly to other properties retaining original housing stock, collectively define, maintain and support the character of the Malton Victory Village Cultural Landscape, defined by its surviving wartime dwellings, combination of straight and curvilinear streets with boulevards, central public park, original community hall and school building, all laid out and built according to plans developed by Wartime Housing Ltd.

As per the nine criteria set out in Regulation 9/06 of the Ontario Heritage Act, the subject property is not considered to be worthy of designation under Part IV of the Act. This conclusion supports its listing on the Heritage Register only as part of the Malton Victory Village Cultural Landscape and not for its individual architectural or historical significance or contextual value. However, the Malton Victory Village Cultural Landscape may be eligible for designation as a heritage conservation district under Part V of the Act, as was the case with the St. Mary's post-war housing subdivision in Kitchener.

4.3.2 Evaluation for Conservation according to the Provincial Policy Statement Definition

Part 2.6 of the *Ontario Provincial Policy Statement* (Cultural Heritage and Archeology) states that "significant built heritage resources and significant cultural heritage landscapes shall be preserved."²⁶ As there is no definition of *significant*, it must be assumed in the case of built heritage resources, to mean properties designated or eligible for designation under the Ontario Heritage Act. As concluded above, the subject property does not merit such designation on an individual basis (Part IV), only as part of a potential heritage conservation district (Part V).

5 PROPOSED NEW RESIDENCE

5.1 Replacement Dwellings in the Malton Victory Village Cultural Landscape

Photo 3; Photo 7

As with other residential cultural landscapes in the City of Mississauga, current R4 zoning regulations work against the preservation of the defining character of the Malton Victory Village Cultural Landscape. The regulations allow a maximum lot coverage of 40%, a maximum building height of 10.7 m (35') and minimum sideyard setbacks of 1.2 m, which permits the construction of full two-storey houses with considerably larger footprints than the existing wartime dwellings. Given that the lots are relatively narrow, the largest discrepancy is in the depth of the new houses. Viewed out of context, they cannot be distinguished in appearance from typical new subdivision housing, in terms of their massing, built-in garages, design and materials. However, within the context of the Malton Victory Village Cultural Landscape, they stand out in stark contrast to the wartime housing. Unfortunately, given the zoning provisions,

²⁶ *Ontario Provincial Policy Statement*, p. 21 (see SOURCES: Section 7.1.3).

lack of site plan control and absence of any design guidelines for new construction, little can be done to prevent the intrusion of these larger-scale residences.

5.2 Description of Proposed New Residence and Landscaping

Figure 23 to Figure 30

At the time that the author of this report was retained to undertake the required Heritage Impact Statement, a site plan and set of architectural drawings had already been completed for building permit purposes by Sanpro Engineering Ltd. With a proposed lot coverage of 35.68% (slightly less than the 40% maximum) and two-storey height of 30'4" (short of the 35' maximum allowed), the proposed new dwelling is comparable in size to the typical replacement residences built to date in this neighbourhood. Also characteristic is the built-in garage (in this case double with two doors) and the brick and stone veneer siding. Knowing the client would oppose any reduction of the footprint and the accompanying loss of floor space, the decision was made to focus entirely on the facade elevation, the original design for which was considered to be acceptable even for a new subdivision. **(Figure 25)** Visually, the roof projection over the double garage was too high, horizontal windows were inconsistent with the vertical proportions of windows on every other facade, and the false balcony was redundant and did not visually enhance the facade. Furthermore, the stone veneer ended abruptly at the building corners instead of wrapping around the corners, which is the customary and visually more attractive practice. After obtaining feedback from an acquaintance with her own architectural practice: Melissa Rocchi Architect, a number of changes were recommended, as illustrated and described in **Figure 26**. These changes were agreed to by the owner and the proposed modifications and the facade and front side elevations were largely revised accordingly. One discrepancy is the second storey window treatment, with the addition of simulated divided lights, that is, grilles resembling traditional muntins, to some of the panes. **(Figure 27)** Though not preferred to the simpler treatment recommended, it is a relatively minor detail.

The proposed frontyard landscaping consists of a double-width poured stamped concrete driveway with a similar perpendicular concrete walkway extending to the far side of the porch. According to the owners, the ornamental tree close to the front of the existing dwelling is to be relocated to the back yard.²⁷ **(Figure 23)**

5.3 Design Evaluation

5.3.1 Cultural Landscape Criteria

The following checklist of criteria to be addressed for the Mineola West Cultural Landscape is found in the *City of Mississauga Cultural Landscape Inventory*, Section: L-RES-6. This Heritage Impact Statement must demonstrate how the proposed development will conserve the following criteria that define the character of Victory Village as a cultural landscape.

LANDSCAPE ENVIRONMENT

²⁷ As conveyed by the owners' daughter, Shirin Manjra, in an email dated 7 March 2014.

Scenic and Visual Quality: The scenic/ visual quality of the site of the subject property, with respect to the proposed dwelling, will be less adversely affected by the revised front and front side elevations than by the original design. The visual impact of the front facade will be reduced by the elimination of the pseudo balcony. The scenic and visual quality of the site, as viewed from the street, will be diminished by the removal of the ornamental tree.

Natural Environment: The semi-mature flowering ornamental tree in the front yard is to be relocated to the back yard. If successfully implemented, the natural environment of the site will not be adversely affected by this procedure. The construction of the proposed new residence close to property line shared with #3106, however, raises concerns about potential damage to the two maple trees, located close to the chain link fence: both in terms of the root systems and tree canopies.

Landscape Design, Type and Technological Interest: There are currently no landscaping or technological features of interest on the subject property. The frontyard landscaping will be visually enhanced by the stamped concrete driveway, replacing the remains of an aged gravel driveway. It was suggested that the driveway be narrowed a single car width at the curb, thereby reducing the area of hard surface landscaping, as proposed for 1171 Stavebank Road in Mineola West and recommended for 7161 Lancaster Avenue.²⁸ However, this suggestion was rejected by the owners. Nevertheless, stamped concrete is a welcome alternative to asphalt, as proposed for 7161 Lancaster Avenue, and illustrated by the single driveway of the adjacent property at 3114 Merritt Avenue. **(Photo 2)**

HISTORICAL ASSOCIATIONS

Illustrates Style, Trend, or Pattern: Similarly to other larger replacement dwellings already approved and built in the Malton Victory Village Cultural Landscape, the size, design and construction of the proposed two-storey residence does not support the character of the neighbourhood as built, with its small 1 to 1 ½ storey wartime dwellings.

Illustrates Important Phase in Mississauga's Social or Physical Development: Victory Village represents a unique and historically significant component of Mississauga's WWII history and heritage and part of the important legacy of Wartime Housing Ltd, which played a vital role in the provision of adequate housing for industrial workers and their families across the country. However, the historical integrity of this wartime residential neighbourhood is slowly being eroded by the lack of protective measures such as tighter zoning regulations or district designation under Part V of the Ontario Heritage Act, as exemplified by the area of wartime housing in Kitchener, protected as the St. Mary's Heritage Conservation District.

BUILT ENVIRONMENT

Aesthetic/ Visual Quality and Consistent Scale of Built Features: The visual quality of existing wartime dwellings has inevitably been compromised to some extent by alterations and additions but the overall character of the Malton Victory Village Cultural Landscape will be much more seriously undermined by the current trend towards the construction of

²⁸ See Figure 24 of the *Heritage Impact Statement for 7161 Lancaster Avenue*.

considerably larger and stylistically incompatible two-storey replacement houses with brick/stone cladding that differs from the clapboard or composite siding of the original dwellings. This trend will also create a neighbourhood with housing of increasingly inconsistent scale.

5.3.2 Mitigation Measures

Mitigation measures for the design of the proposed new dwelling on the subject property have been addressed in Section 5.2 by the recommended and largely accepted facade alterations. If the ornamental frontyard tree can be successfully dug up and transplanted to the rear yard, this action will preserve a component of the site's natural environment but will diminish the streetscape view. It is therefore recommended that a suitable native species be planted between the concrete walkway and the fire hydrant. It is also recommended that a certified arborist be consulted about the protection of the two maple trees mentioned above under the criterion, Natural Environment, and the procedure for safely relocating the ornamental tree. The work should be undertaken by a qualified landscaping firm, to ensure that the transplanted tree survives and thrives in its new location.

As the Malton Victory Village Cultural Landscape is *not* under Site Plan Control, property owners are *not* required to provide detailed plans for the site landscaping for a new replacement dwelling. This contrasts with residential cultural landscapes under Site Plan Control, such as Mineola West, where property owners must provide:

- A tree inventory, location and protection plan prepared by a certified arborist.
- A detailed landscaping plan, showing the location of hard surface areas, fencing, plant beds, trees and shrubs, and identifying building hard surface materials and plant and tree species, etc. prepared by a certified landscape architect.

Overall, the mitigation of the visual impact of the new replacement dwellings could only be fully achieved by zoning that further restricts the lot coverage, the implementation of Site Plan Control in this cultural residential landscape, and design guidelines, which address massing, height, setbacks, materials, window configuration, garages, porches, driveways, etc. that could be enforced by a regulatory process.

6 CONCLUSIONS AND RECOMMENDATIONS

6.1 Existing Dwelling

The evaluation of the existing wartime dwelling under the Ontario Heritage Act criteria for designation concluded that the subject property does not merit individual designation under Part IV of the Act. It is therefore recommended that the City of Mississauga Building Department be permitted to issue a demolition permit. Nevertheless, it is one of a group of similar wartime dwellings, which collectively possess historic value as part of a subdivision planned by the Crown Corporation, Wartime Housing Ltd. This subdivision, formerly known as Victory Village, has been recognized as the Malton Victory Village Cultural Landscape (or by other names), which despite the increasing intrusion of larger two-storey replacement

dwellings, may still be eligible for designation under Part V of the Act. District designation provides tools for better preserving the layout of the original 1941 Plan of Subdivision and the original housing stock. However, it is not known whether there would be any community or political support for a City initiative to undertake a heritage conservation district study. It seems unlikely, given the cultural transformation of the Malton area over the past 25 years but it may still be worthwhile conducting a residents' survey. With an increased awareness of this unique aspect of Malton's heritage (in part through research undertaken for Heritage Impact Statements), there might be some interest in preserving its tangible remains.

6.2 Proposed New Residence

The design of the proposed new residence, as revised in accordance with recommended alterations to the front and front side elevations, is considered to be acceptable in the context of the new larger and taller replacement residences already approved by Heritage Planning staff. These include houses already built, under construction, or proposed development in the 60-day delay of demolition period (as exemplified by 7161 Lancaster Avenue).

6.3 Recommendations

The author of this report supports the general recommendations made by Heritage Planning Consultant Paul Dilse in his Heritage Impact Statement for 7181 Lancaster Avenue, as presented in the section, "Conclusions and Recommendations": notably, a City initiative to undertake a heritage conservation district study for the designation of the Malton Victory Village Cultural Landscape under Part V of the Ontario Heritage Act and related planning reviews.²⁹

In addition, the following initiative is proposed. Given that the City owns and maintains Victory Park, it is conceivable that one wartime dwelling, destined for demolition, could be relocated to the park and restored to its original appearance (at least the exterior) for seasonal staff or public use. The choice would depend on the owner's willingness to co-operate and the condition and architectural integrity of the dwelling. Missing original components, such as doors and windows could be salvaged from other dwellings to be demolished. For example, the dwelling since replaced at 7157 Lancaster Avenue, still retained some original interior doors, and an exterior door and window relocated to the facade of the rear addition, which could have been salvaged for such a project.³⁰ Given the prefabricated nature of the wartime dwellings, as documented in this report, the salvage and relocation of one would certainly be technically feasible; but of course there would be financial implications. It is envisaged that such a project would be initiated by the City but could provide an excellent opportunity to involve interested community members on a volunteer basis, in particular, individuals with construction experience and trade skills. Perhaps a small volunteer group, such as the Friends of Victory Village, could be formed to oversee and raise funds for the project.

²⁹ Paul Dilse, Heritage Impact Statement on the Property at 7181 Lancaster Avenue, Mississauga (Malton Community) Lot 193, Plan 436August 12, 2013, pp. 10-11.

³⁰ See Photo 17 of *Heritage Impact Statement for 7161 Lancaster Avenue....* by the author of this report.

Regardless of whether such a project ever comes to fruition, it is also recommended that an interpretive plaque for the *Malton Victory Village Cultural Landscape*, similar to the one for St. Mary's Heritage Conservation District in Kitchener (**Figure 20**), be designed and fabricated for mounting at a suitable location in Victory Park –in front of a restored wartime dwelling (if implemented) or the community centre.

Without any effort to educate the residents within the Malton Victory Village Cultural Landscape and the larger Malton community, there can be little hope of preserving the remnants of this wartime housing subdivision beyond the layout of the streets, the institutional buildings and the public park. However, education is only one tool; beyond that, the City of Mississauga needs to investigate all of the tools available for preserving the architectural integrity of this cultural landscape, before it is too late.

7 SOURCES, CONTACTS, SITE VISITS AND QUALIFICATIONS

NOTE: A number of the sources cited below are on-line resources provided by the City of Mississauga on its website (abbreviated as CM). Navigation links are provided for documents available on-line.

7.1 Sources

7.1.1 City of Mississauga and Heritage Mississauga Documents

Landplan Collaborative Ltd., *Cultural Landscape Inventory* (January 2005)

Property Information for 7161 Lancaster Avenue and other nearby properties: CM > Services Online > Plan & Build eServices > Property Information

City of Mississauga Zoning By-law and Index Map: CM > Residents > Planning & Building > Official Plans & Zoning By-laws > Zoning By-Law

City of Mississauga, *Cultural Landscape Heritage Impact Statement Terms of Reference*, June 2012

CM> Aerial Photos, 1952 to 2010: CM > eMaps > Map Layers > Aerial Photography

Heritage Mississauga website: www.heritagemississauga.com/page/History

7.1.2 Secondary Sources

Annmarie Adams and Pieter Sijpkens, "Wartime Housing and Architectural Change, 1942-1992." PDF version found on-line:

<http://people.mcgill.ca/files/annmarie.adams/1995AdamsASijpkensPWartimeHousingandArchitecturalChange.pdf>; original source cited by Paul Dilse as *Vernacular Architecture*, V. 17 N. 2 (1995)

Donald Wetherell and Irene Kmet, *Homes in Alberta: Building, Trends, and Design 1870 – 1967* (University of Alberta Press: 1991)

Kathleen A. Hicks, *Malton: Farms to Flying* (Mississauga Library System: 2006)

Mississauga's Heritage: The Formative Years, 1798-1879 (City of Mississauga: 1983)

John Blumenson, *Ontario Architecture: A Guide to Styles and Building Terms, 1784 to the Present* (Fitzhenry & Whiteside: 1990), Chapter 24: Victory Housing (1940-50), pp. 219-223

Central Mortgage and Housing Corporation, *Renovating Distinctive Homes –1 ½ Storey Post-War Homes*

Ontario Architecture website created by Shannon Kyles, Mohawk College, City of Hamilton:
www.ontarioarchitecture.com/Victory.htm

7.1.3 Miscellaneous

Ontario Ministry of Municipal Affairs and Housing, *Provincial Policy Statement*, 2005: PDF version available online at www.mah.gov.on.ca/Asset1421.aspx

Former reports by Gillespie Heritage Consulting: see Section 7.4

Survey Plan, Site Plan, Architectural Drawings as cited in Section 8: Illustrations

Peel–Land Registry Office #43: title search documents referenced in APPENDIX ‘A’

Paul Dilse, *Heritage Impact Statement on the Property at 7181 Lancaster Avenue, Mississauga (Malton Community) Lot 193, Plan 436August 12, 2013*

7.2 Contacts

Mohammed Manjra, property owner (legally his spouse, Rafika Manjra) and daughter Shirin (email correspondent)

Sanjeev Kumar, P. Eng., Sanpro Engineering Inc. (design/ build consultant)

Elaine Eigl, Heritage Co-ordinator, Community Services, City of Mississauga

Chris Aplin, M.C.A. Paralegal Services, Brampton (title search for 1370 Milton Avenue, completed August 2013)

Paul Dilse, Heritage Planning Consultant, Toronto

Matthew Wilkinson, Historian, Heritage Mississauga

In addition, the author of this report would like to acknowledge the much appreciated support and assistance of her spouse Stewart Patch: in particular for site visit photography, measuring the original dwelling, and proofreading the final report.

7.3 Site Visits

One site visit was made on October 29th, when Stewart Patch (spouse) and myself met with Mohammed Manjra. Photos were then taken of the site, setting and the house exterior and interior. A site visit was also made on the same day to 7161 Lancaster Avenue, for which a Heritage Impact Statement has already been completed and approved by Heritage staff.

7.4 Qualifications of the Author

The author of this Heritage Impact Statement, Ann Gillespie, graduated in 1985 from the Institute of Canadian Studies, Carleton University, Ottawa with an M.A. (1985) specializing in the history of Canadian architecture and building technology. Her thesis topic focused on the manufacture and use of decorative sheet-metal building components in Canada from 1870 to 1930 (galvanized iron cornices, pressed-metal ceilings, etc.).

After graduation she joined the Research Sub-committee of the Hamilton LACAC (Local Architectural Conservation Advisory Committee) and soon afterwards gained employment with the City of Hamilton as a research assistant to Architectural Historian Nina Chapple. She remained with the City in the position of Heritage Researcher/ Planner for 16 years. During this time, she researched and prepared numerous designation reports for buildings to be designated under Part IV the Ontario Heritage Act and contributed to the research for and preparation of feasibility studies and plans for several heritage conservation districts in the former City of Hamilton, notably the *St. Boulevard Heritage Conservation District and Plan* (April 1992) for which she was the principal author. After taking early retirement at the end of 2001, she became a part-time heritage consultant and has been a member of CAHP (Canadian Association of Heritage Professionals) since 2002.

Most relevant to this report are the following Heritage Impact Statements previously undertaken for properties in the City of Mississauga by Gillespie Heritage Consulting:

Heritage Impact Statement for 7161 Lancaster Avenue, Malton Victory Village Cultural Landscape, City of Mississauga (February 2014)

Heritage Impact Statement for 3170 Milton Avenue, in the Mineola West Cultural Landscape, City of Mississauga (September 2013)

Heritage Impact Statement for 1171 Stavebank Road, in the Mineola West Cultural Landscape, City of Mississauga (April 2013)

Heritage Impact Statement for 350 Indian Valley Road, in the Mineola West Cultural Landscape, City of Mississauga (October 2011)

Heritage Impact Statement for 7157 Lancaster Avenue, Malton, City of Mississauga (May 2011)

Heritage Impact Statement for 60 Inglewood Drive, in the Mineola West Cultural Landscape, City of Mississauga (March 2009)

Heritage Impact Statement for 1525 Glenburnie Road, in the Mineola West Cultural Landscape, City of Mississauga (February 2008)

Heritage Impact Statement for 14 Princess Street, Streetsville, City of Mississauga (December 2007)

Heritage Impact Statement for 16 Front Street, Old Port Credit Heritage Conservation District, City of Mississauga (November 2006)

8 ILLUSTRATIONS

The following illustrations, identified as **Figure 1, 2**, etc., include maps, aerial photos, site plans and floor plans of the existing property and dwelling and for the proposed replacement residence. References to links from City of Mississauga website are abbreviated as CM > [specific page].



Figure 1: Section of the MapArt page showing the south-west part of Malton, City of Mississauga and the location of the Malton Victory Village Cultural Landscape.

SOURCE: MapArt Publishing, Ontario Atlas Series, *Book 2: Golden Horseshoe* (2002), p. 459; annotated by the author of this report to show the area encompassing the cultural landscape.

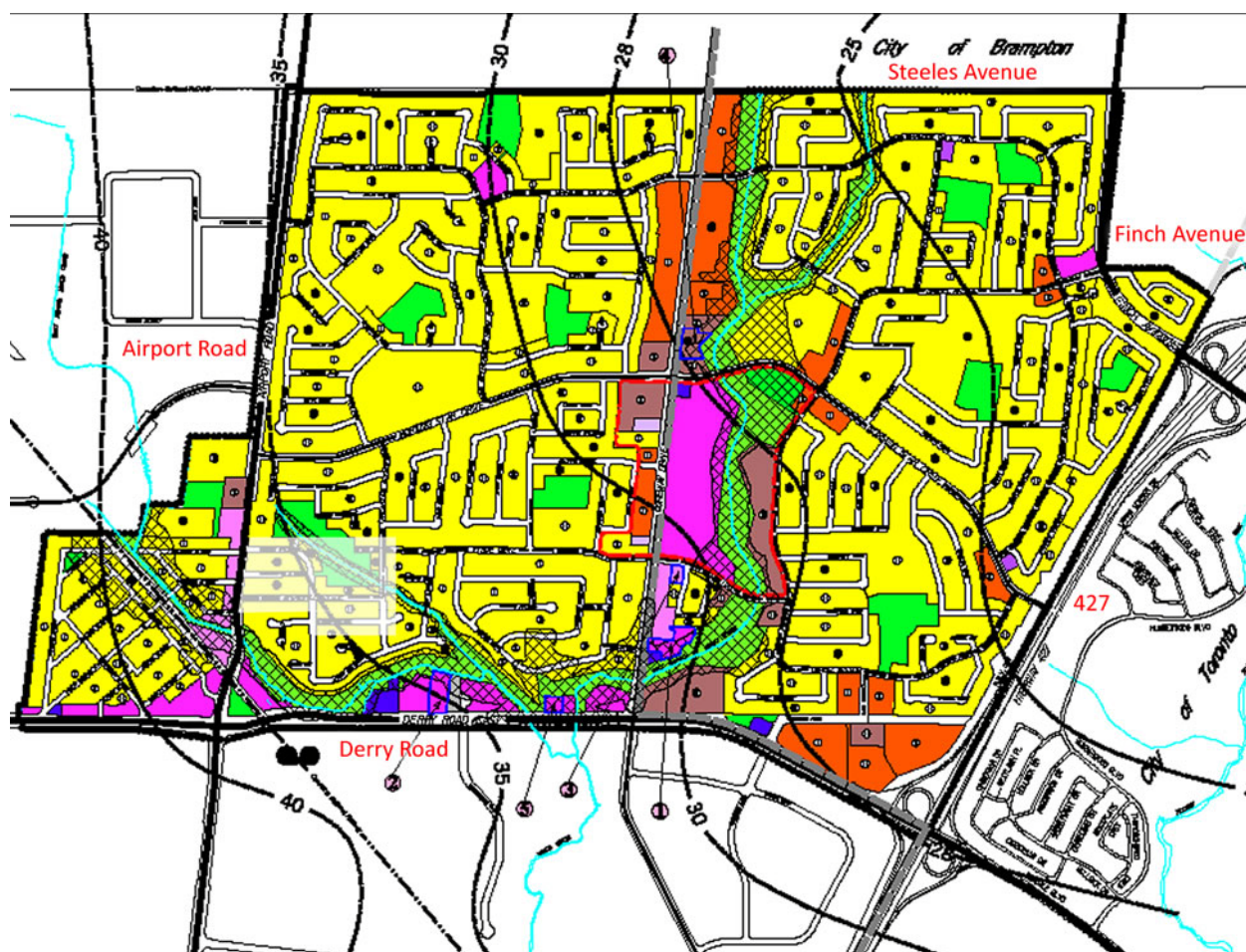


Figure 2: Section of the Malton District Land Use Map showing the shape and boundaries of the present-day neighbourhood and planning district of Malton at the northern and eastern borders of the City of Mississauga, with the City of Brampton to the north and the City of Toronto to the east. White shaded area shows the location and approximate shape of the Malton Victory Village Cultural Landscape.

SOURCE: Mississauga Plan, Malton District Land Use Map, April 2010; red text and white shading added by the author of this report.



Figure 3: Map showing the boundaries of the Malton Victory Village Cultural Heritage Landscape, which corresponds to the boundaries of the original Victory Village subdivision, as shown on the 1952 Plan of Subdivision (see **Figure 17**).

SOURCE: Digital copy provided by the Heritage staff at the City of Mississauga; annotated by the author of this report (boundary line, north arrow and superimposed text).



Figure 4: 2013 aerial view showing the boundaries of the Malton Victory Village Cultural Landscape and the location of 3110 Merritt Avenue. When the residential subdivision to the north of Etude Drive was built, Victory Park was expanded to the north-west.

SOURCE: CM website> E-maps; annotated by the author of this report.

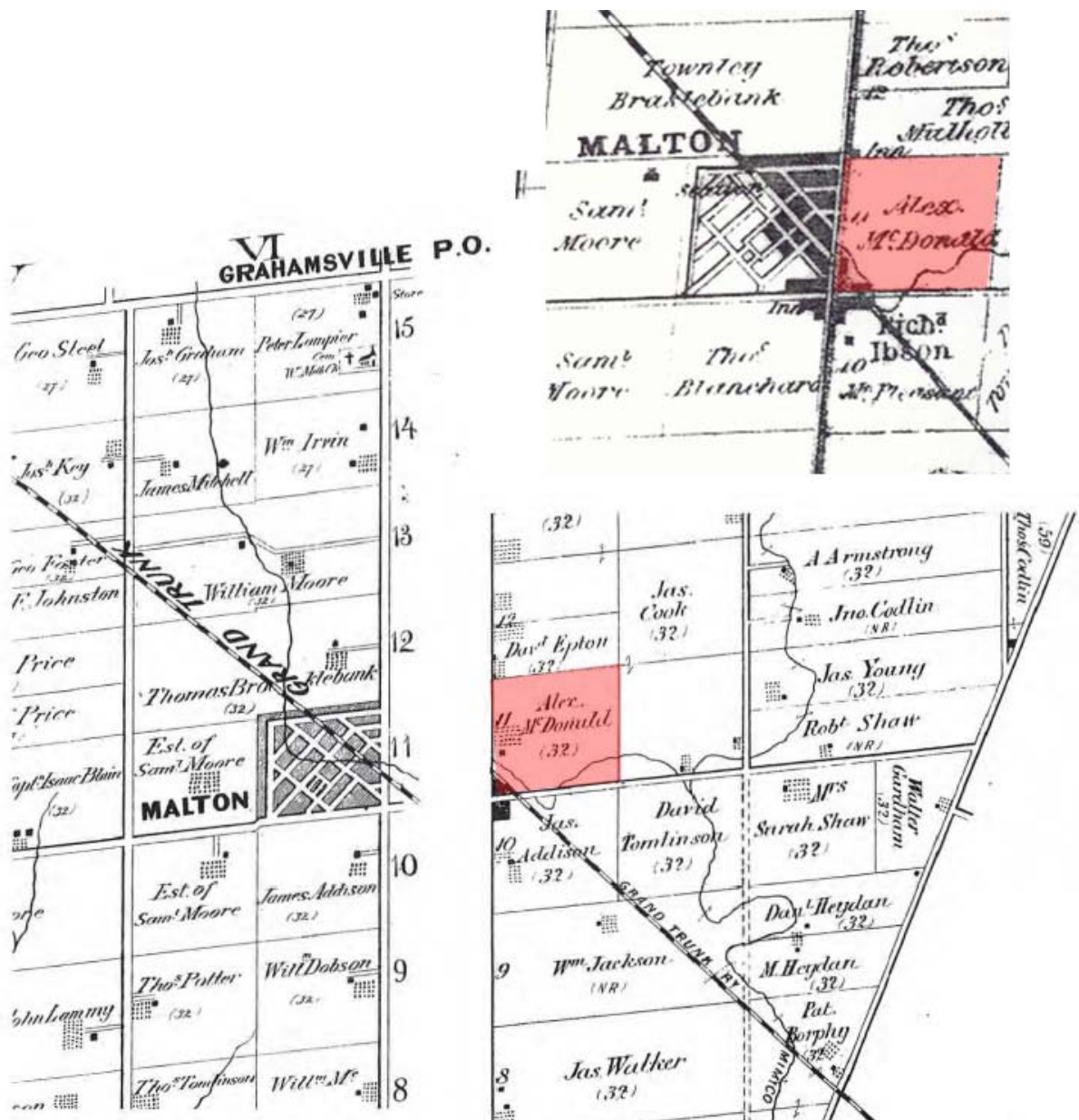


Figure 5: Section of the 1859 Tremain map (top) showing the layout of the village of Malton which evolved around and to the north-west of the “four corners”, where 6th line (now Airport Road) intersected with the Malton Side Road (now Derry Road). Below are two contiguous sections of the 1877 County of Peel Atlas. The diagonal railway line was built by the Grand Trunk Railway in 1854. Highlighted areas show the farmland owned by Alex McDonald north of the railway line directly east of the village, where land was expropriated for the Victory Village subdivision.

SOURCES: 1859 map segment from the Heritage Mississauga website:

www.heritagemississauga.com/photo/Malton

1877 map segments: cropped and annotated version of Figure 13 of the Heritage Impact Statement for 7181 Lancaster Avenue completed by Heritage Planning Consultant Paul Dilse, August 2013 and credited as follows: “North Part of Toronto” and “Gore of Toronto” in J.H. Pope, Illustrated Historical Atlas of the County of Peel, Ont. (Toronto: Walker & Miles, 1877), pp.21 and 33.



Figure 6: Historic photos of Malton: view of its “four corners” intersection, circa 1940; terminal building for Malton Airport around the time of its opening in 1937.

SOURCE (top photo): Cropped version of photo from the Heritage Mississauga website:
www.heritagemississauga.com/photo/Malton; source of original unknown.

SOURCE (bottom photo): Cropped version of a real photo postcard posted on the website:
www.malton.org/maltonhistory/maltonairport.htm

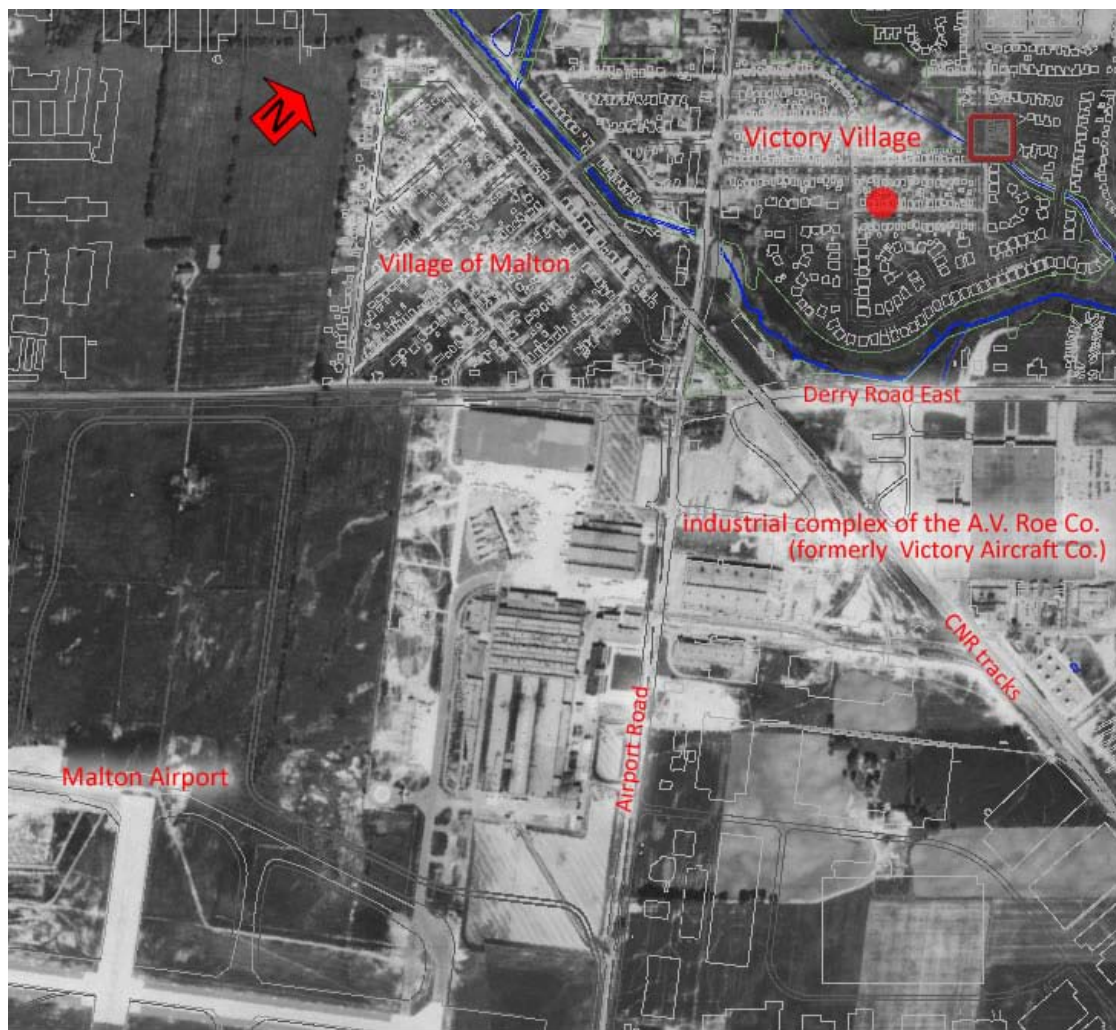


Figure 7: Section of the 1954 aerial photograph showing Victory Village prior to the extension of streets for the expansion of residential development and the industrial lands to the south. Red circle shows the approximate location of 3110 Merritt Avenue.

SOURCE: CM website> E-maps (www.mississauga.ca/portal/services/maps); annotated by the author of this report.

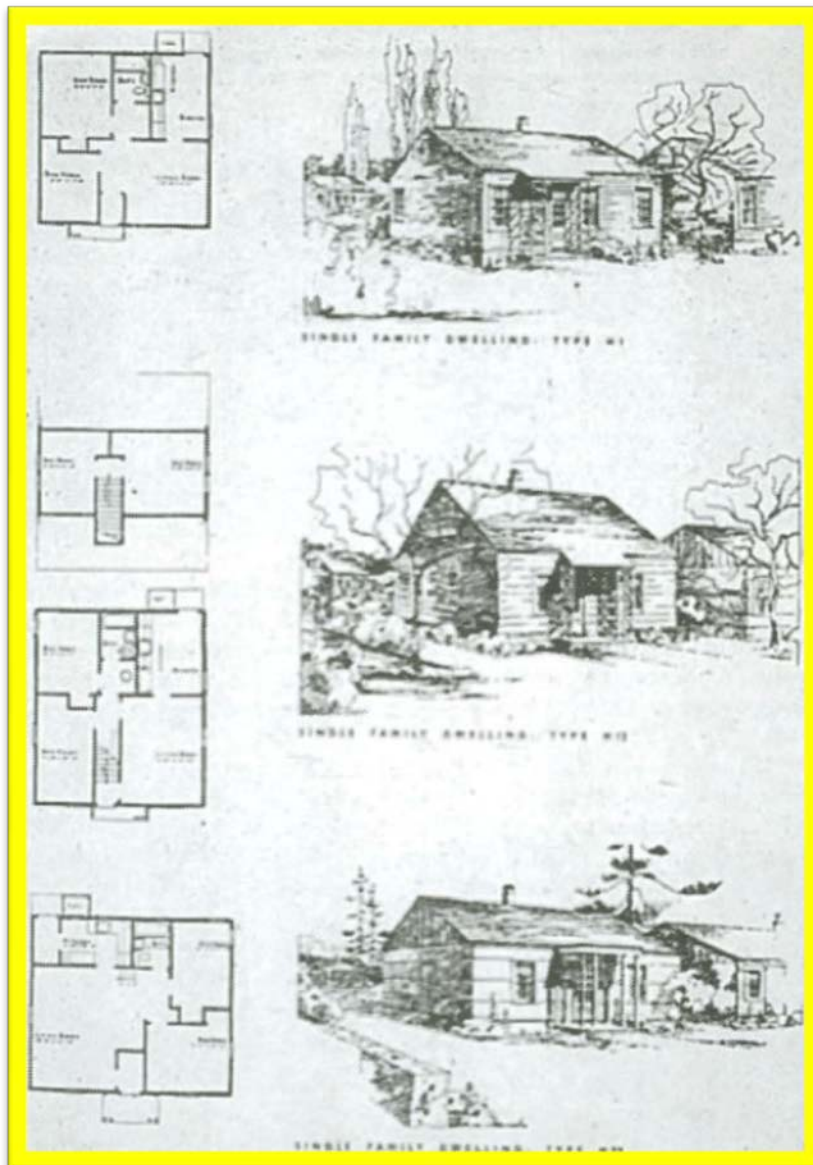


Figure 8: Typical designs for wartime housing: three standard plans for one and 1 ½ storey dwellings with no basements.

SOURCE: Annmarie Adams and Pieter Sijpkes, "Wartime Housing and Architectural Change, 1942-1992," *Vernacular Architecture* V. 17 N. 2 (1995), p. 17; found on-line:

<http://people.mcgill.ca/files/annmarie.adams/1995AdamsASijpkesPWartimeHousingandArchitectualChange.pdf>

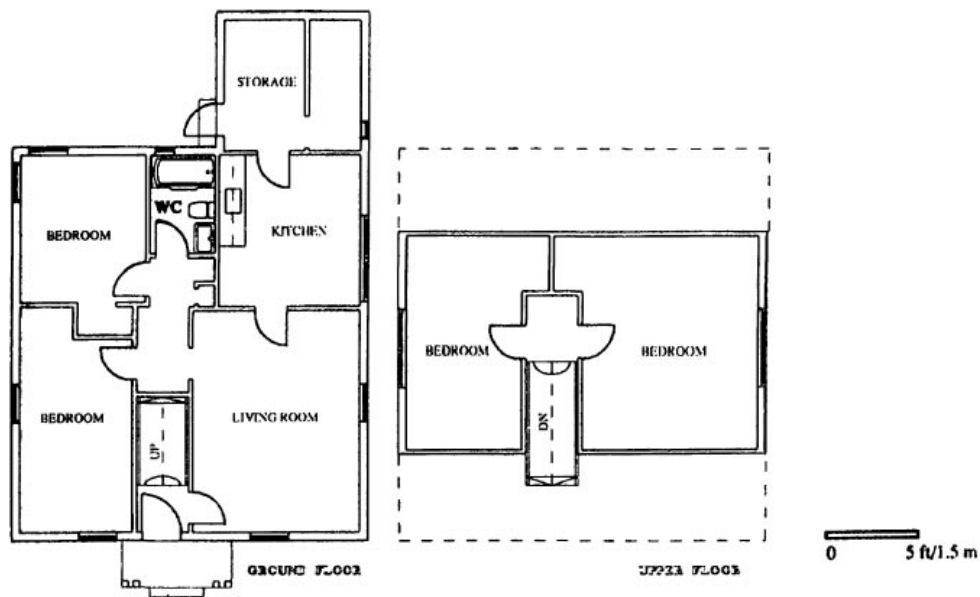


Figure 1a. Original layout of wartime homes: One-and-a-half-story, 24-x-28-foot (Cape Cod) unit.

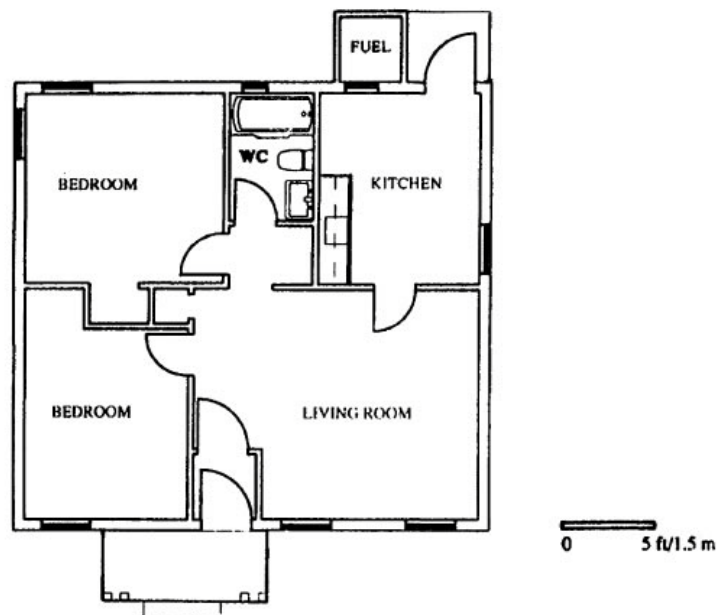


Figure 1b. Original layout of wartime homes: 24-x-24-foot bungalow.

Figure 9: More legible floor plans for a 24' x 24' bungalow and a 24' x 28' 1 ½ storey dwelling, showing the location of the fuel box beside the rear doorway (1b) and a larger divided storage room at the back of the 1 ½ storey dwelling which must have included the fuel box (1a).

SOURCE: Avi Friedman and Maria D. Pantelopulos, Space Management in Montreal's Wartime Housing, History and Society, Vol. 23 No. 2; found on-line: www.housingeducators.org/Journals/H&S_Vol_23_No_2_Space_Management_in_Montreal's_Wartime_Homes.pdf



Figure 10: A typical streetscape of wartime housing in Peterborough, photographed shortly after completion in 1943. The houses shown were based on the same plan for a 1 ½ storey house with a steep-pitched, tight-eaved gable roof and projecting canopy over the front entrance with trellis-like supports; six-over-six paned sash windows. These dwellings had clapboard siding and the standard asphalt-shingled roofing. As was the case in many parts of Canada, these wartime dwellings had no basements and were supported on cedar or concrete posts. Heating was provided by a single stove on the main floor vented through a stove pipe (partially visible on the backside of the roofs).

SOURCE: John Blumenson, *Ontario Architecture: A Guide to Styles and Building Terms, 1784 to the Present*, figure 24-3, p. 221.



Figure 11: View of part of a streetscape of dwellings built by Wartime Housing Inc. in Edmonton in 1944, shortly after construction. In contrast to most wartime houses in Canada, these dwellings had full basements, a necessity for climatic reasons, with side chimneys suggesting that they were equipped with coal-burning furnaces in their basements.

SOURCE: Donald Wetherell and Irene Kmet, *Homes in Alberta: Building, Trends, and Design*, p. 178; original source of photograph: Public Archives of Alberta BL720.



Figure 12: A recently completed streetscape in the St. Mary's post-war neighbourhood in Kitchener, Ontario, circa 1948, which shows the wood board sidewalks and walkways to the front doorways. There appears to be a strip of land between the roadway and sidewalk.

SOURCE: Kitchener-Waterloo Record Photographic Record Collection, Dana Porter Library, University of Waterloo; used on the interpretive plaque for the St. Mary's Heritage Conservation District in Kitchener (**Figure 20**).

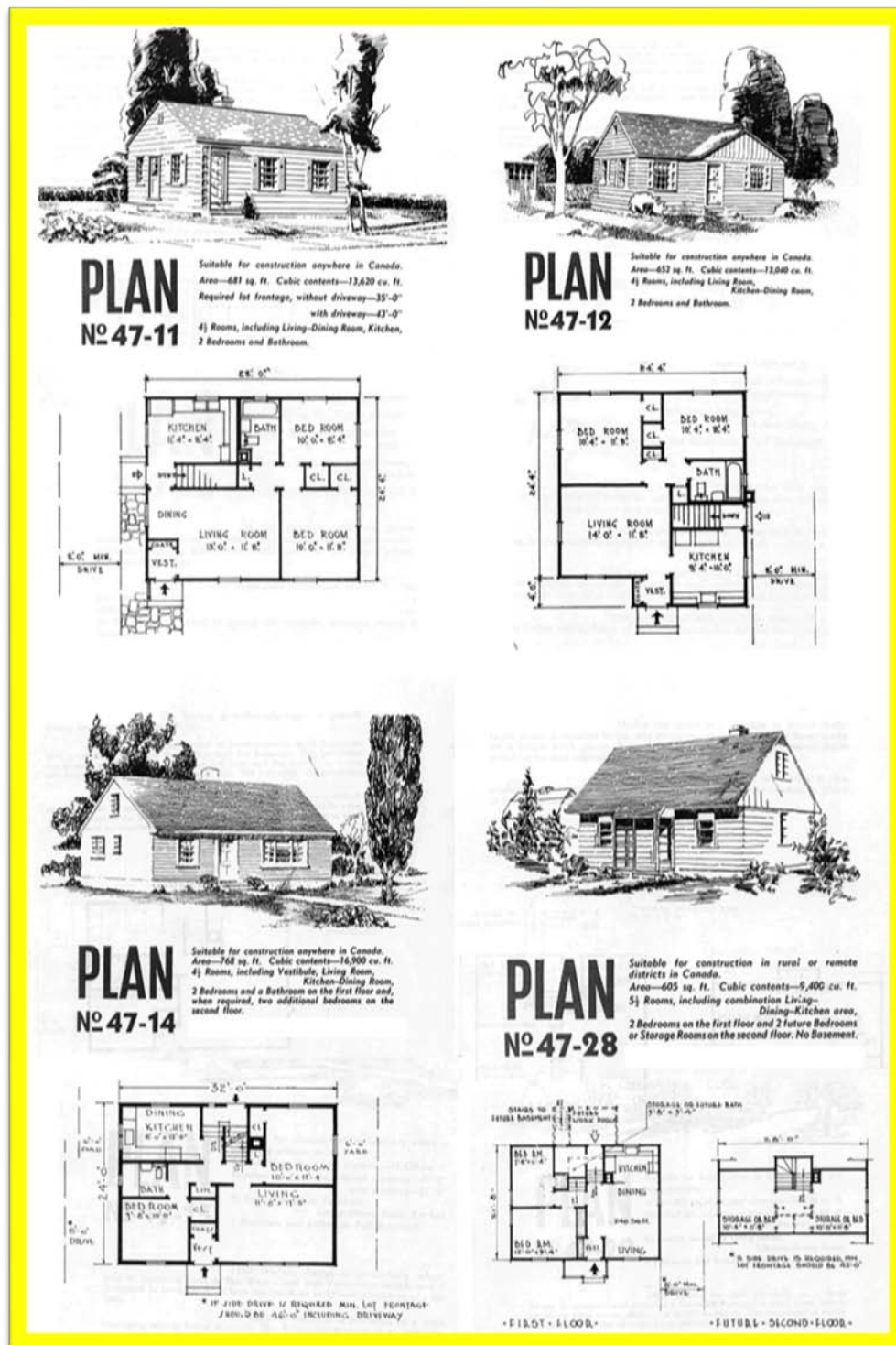


Figure 13: Renderings and floor plans for some standard post-war dwellings published by CMHC in 1947: “67 Homes for Canadians”, some with basements and some without (e.g. Plan 47-28 specifies no basement). These houses are similar in size and design to the wartime dwellings shown above.

SOURCE: Posted on the website: *Home from the War: St. Catherines’ Wartime Neighbourhoods*:
<http://wartimehouses.com/the-homes/the-designs>



Figure 14: Similar streetscape view of north Toronto's Winston Park shortly after the houses were built but before completion of the road (with gravel surface) or any sidewalks and front walkways. This post-war subdivision, now located just north of the 401, was clearly built with permanence in mind. It mainly consisted of 1 ½ dwellings, which appear to have been constructed with full basements and heated with coal boilers as indicated by the chimneys. A number of these houses are still standing with the usual alterations and additions made over time. The Winston Park area with its wide roadways and generous setbacks still retains its sense of spaciousness (as observed on Google street view).

SOURCE: August 1945 photograph accompanying an article by Thomas Wicks posted on the Toronto Spacing website: <http://spacing.ca/wire/2007/12/12/wartime-housing>; original source: City of Toronto Archives. *Globe and Mail* collection, SC 266, Item 98646.

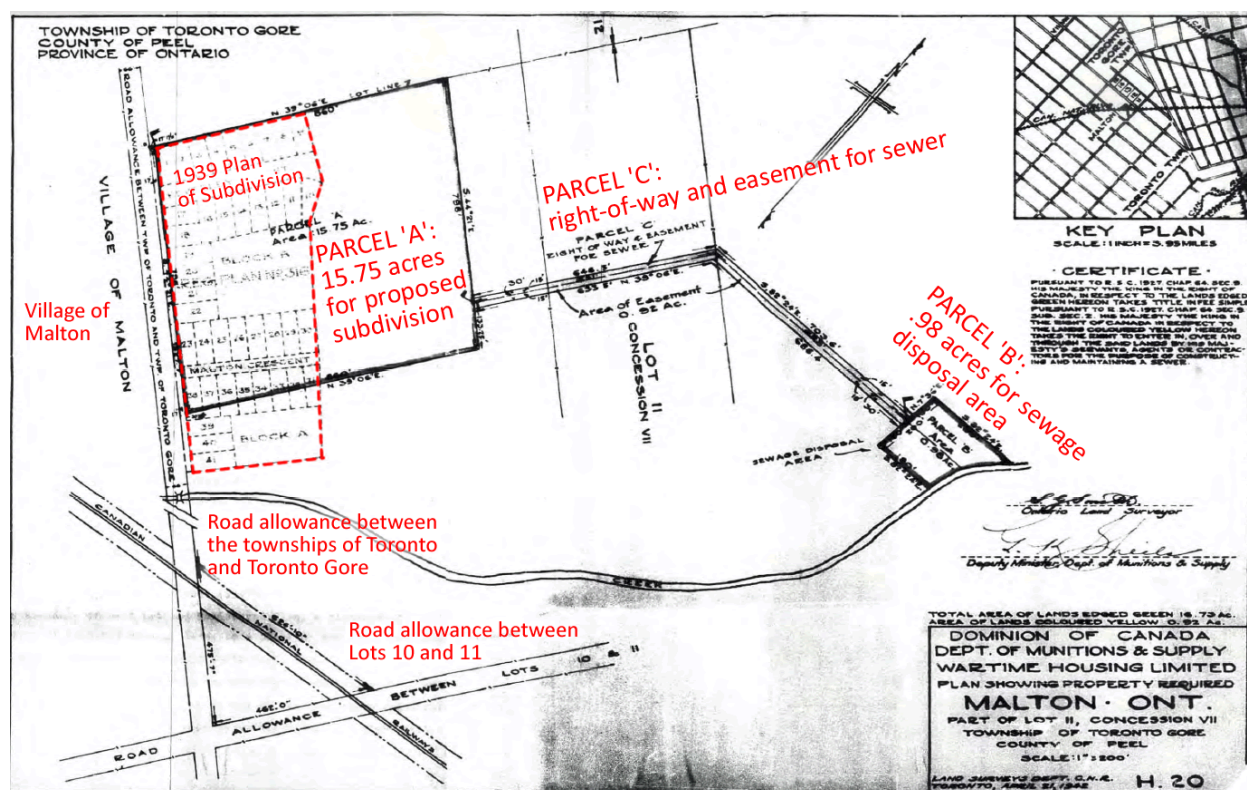


Figure 15: 1942 Survey Plan showing the three parcels to be expropriated for the proposed Victory Housing subdivision and the smaller area previously surveyed for property owner Fred Codlin in 1939, with boundaries and lots shown with dotted lines.

SOURCE: Original hard copy found by Heritage Planning Consultant Paul Dilse at the Peel Land Registry Office: S.G. Smith, "Dominion of Canada Dept. Of Munitions & Supply, Wartime Housing Limited, Plan Showing Property Required, Malton, Ont., Part of Lot 11, Concession VII, Township of Toronto Gore, County of Peel," Plan H 20, 21 April 1942, Toronto Gore Instrument #3412; digital copy from his Heritage Impact Statement for 7181 Lancaster Avenue (Figure 15) annotated by the author of this report.

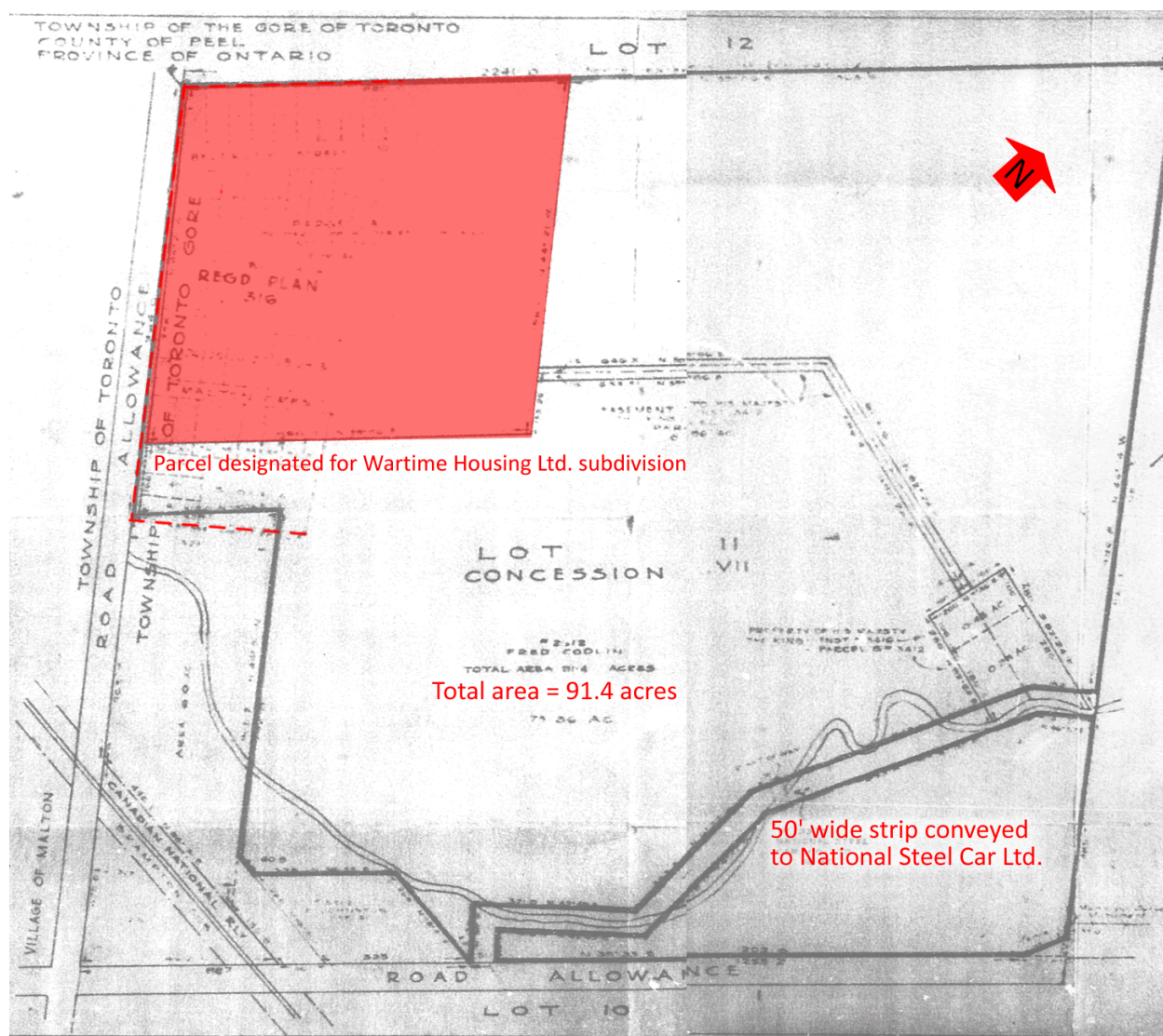


Figure 16: The final plan of lands acquired by Wartime Housing Ltd. (H-20-A), dated [?] October 1942.

SOURCE: Scanned version of two photocopies made by Chris Aplin as part of the Deed of Land from Fred Codlin to "His Majesty the King in the Right of Canada", dated 15 October 1942; highlighting and annotations by the author of this report.

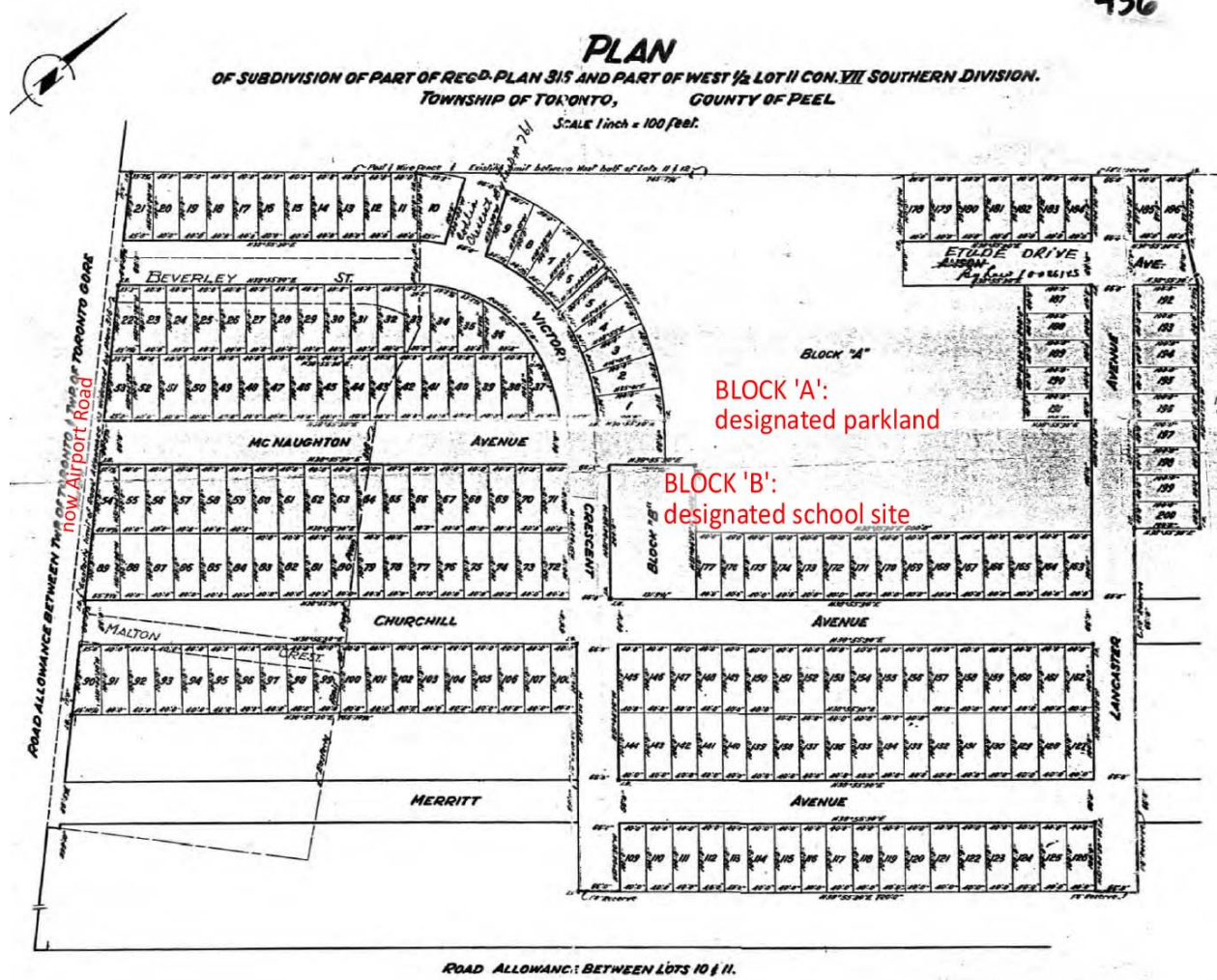


Figure 17: Plan of Subdivision of part of Registered Plan 315 and Part of West ½ Lot 11 Conc. VII Southern Division, Township of Toronto, County of Peel; registered February 1952 by the Central Housing and Mortgage Corporation.

SOURCE: Full-size photocopy obtained by Chris Aplin from the Region of Peel Registry Office in 2011; digitized and annotated by the author of this report.

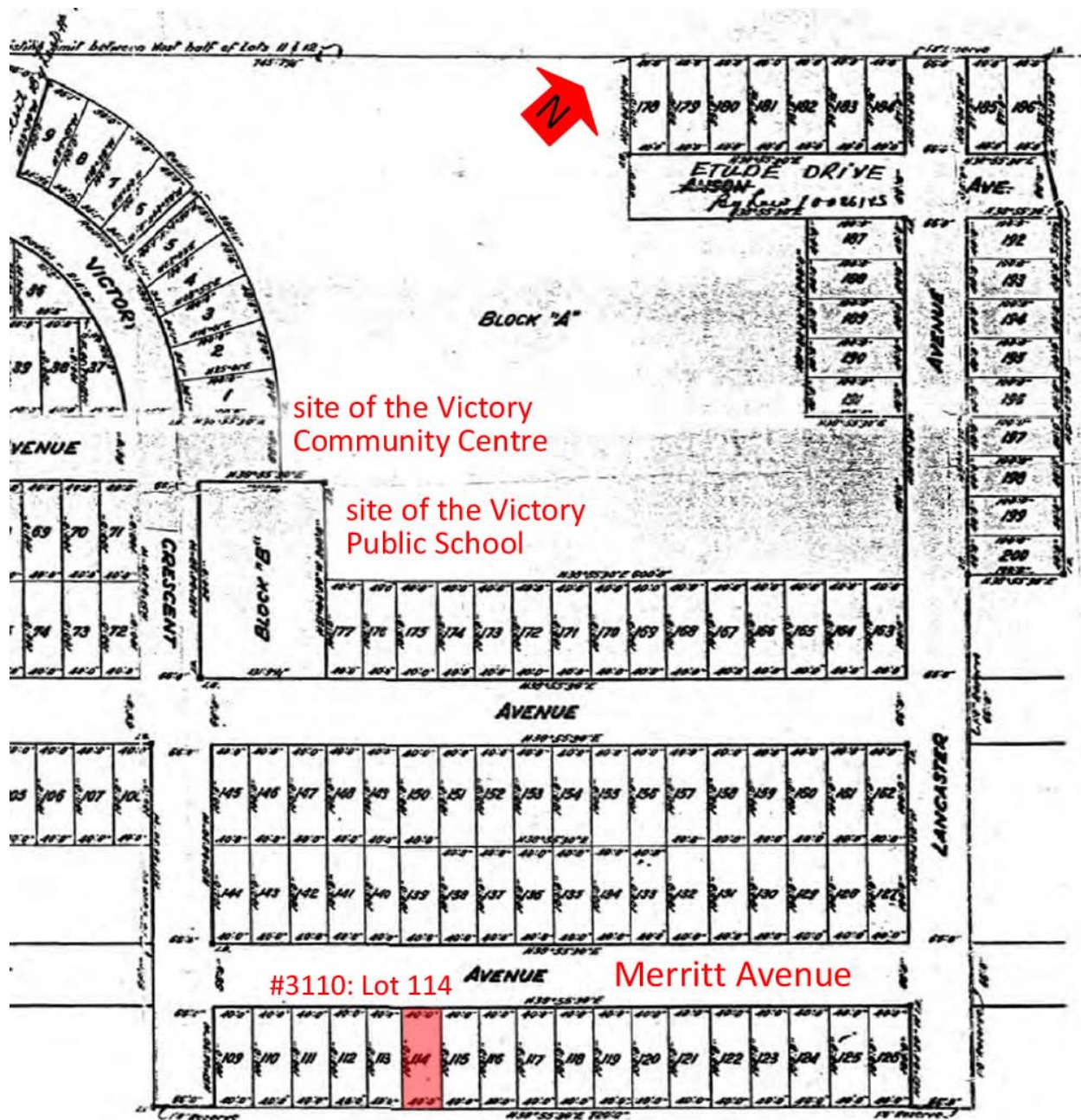


Figure 18: Section of the above Plan of Subdivision showing lot 114 (subject property at 3110 Merritt Avenue).



Figure 19: Section of the 1966 aerial photograph showing the expansion of the original subdivision to the south with the westward extension of Merritt Avenue as a cul-de-sac and the creation of a second cul-de-sac called Landen Court to the south.

SOURCE: CM website> E-maps; annotated by the author of this report.

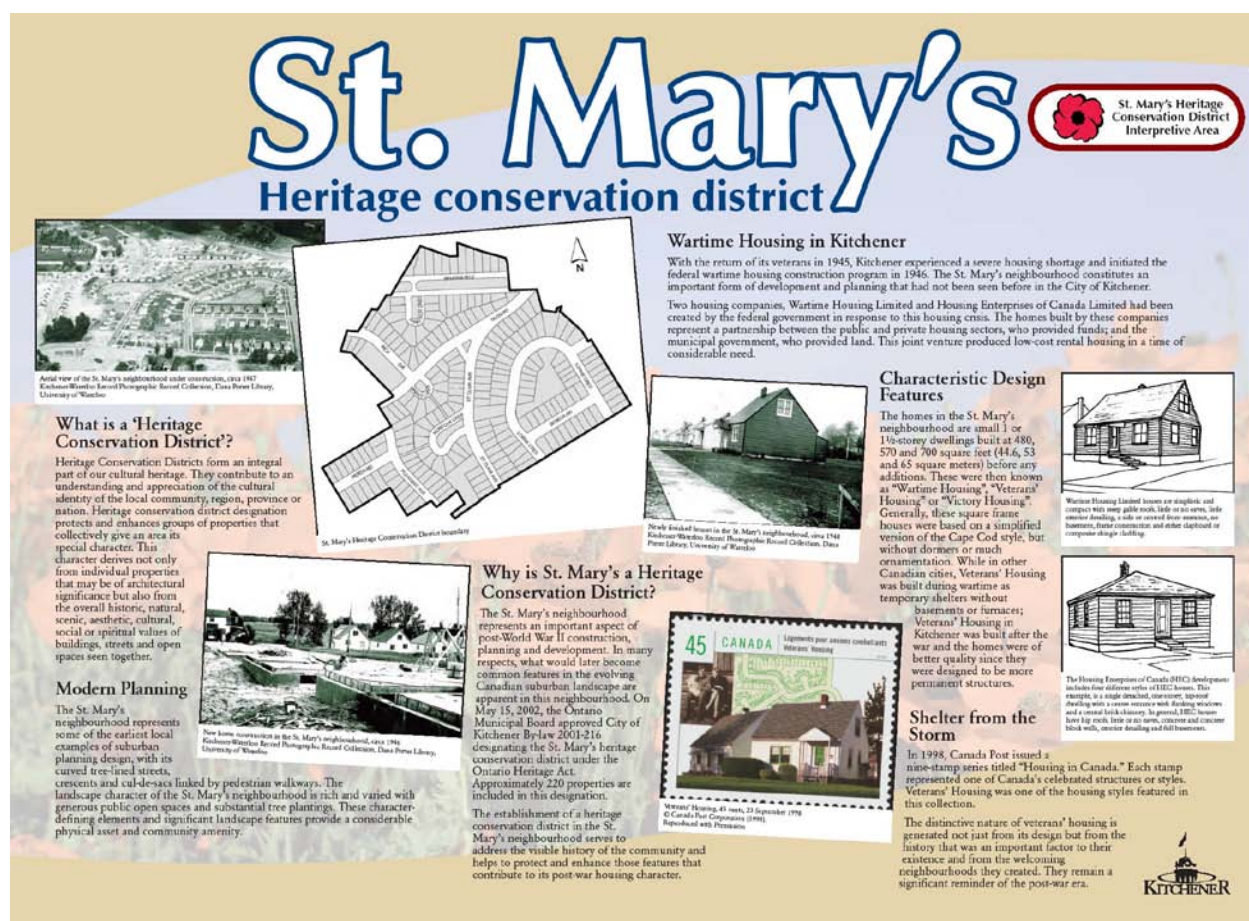


Figure 20: Interpretive plaque for the St. Mary's Heritage Conservation District in Kitchener, showing the boundaries of the district, typical house designs and historic photos.

SOURCE: PDF provided by City of Kitchener Heritage Planner Leon Bensason in 2011.

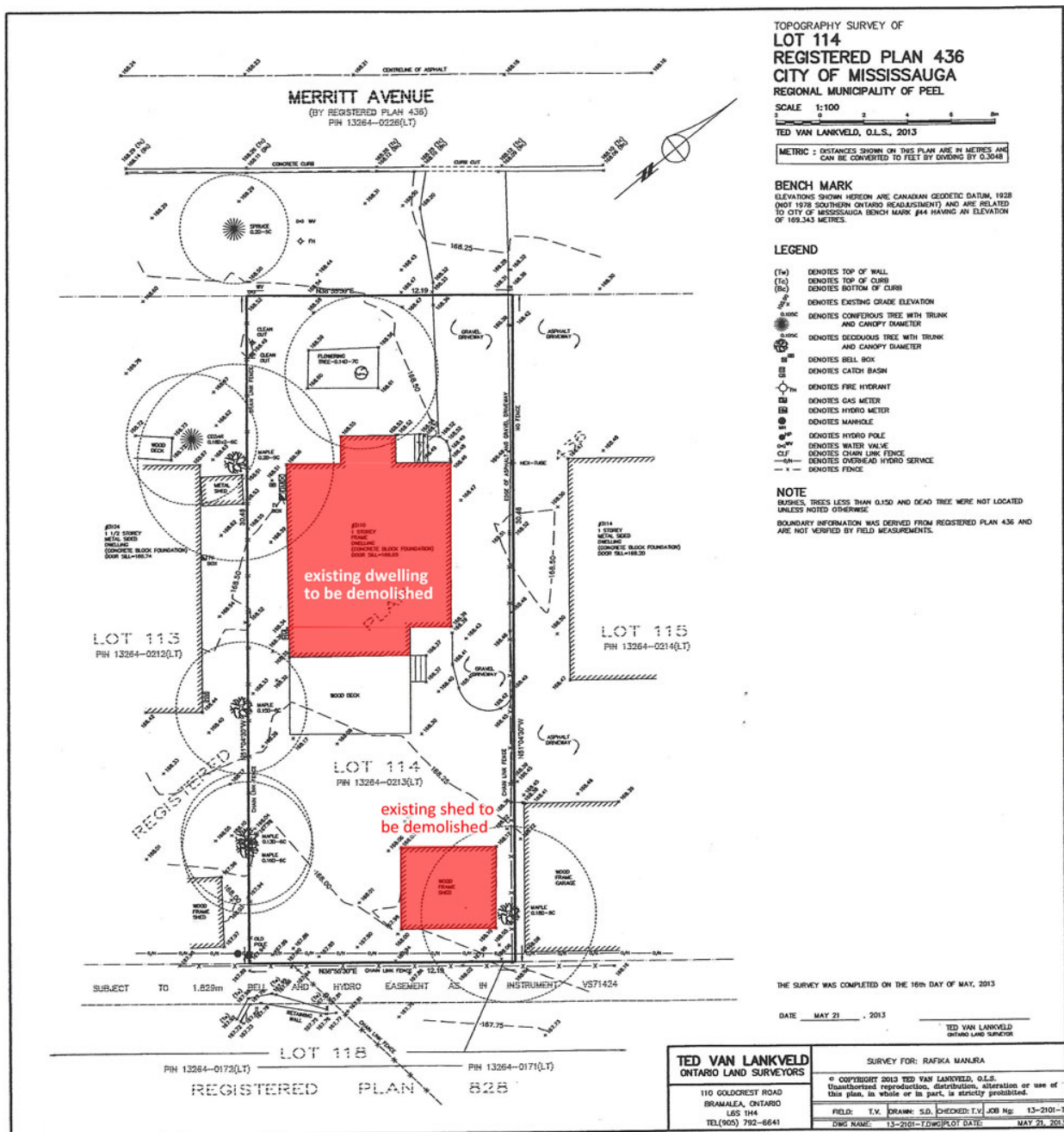


Figure 21: 2013 Plan of Survey for Lot 114 showing the existing buildings, driveway and fencing.

SOURCE: Ted Van Lankveld, OLS, Plan of Building Survey of Lot 114, Registered Plan 436, City of Mississauga, 21 May 2013, based on PDF provided by the project architect and annotated by the author of this report.

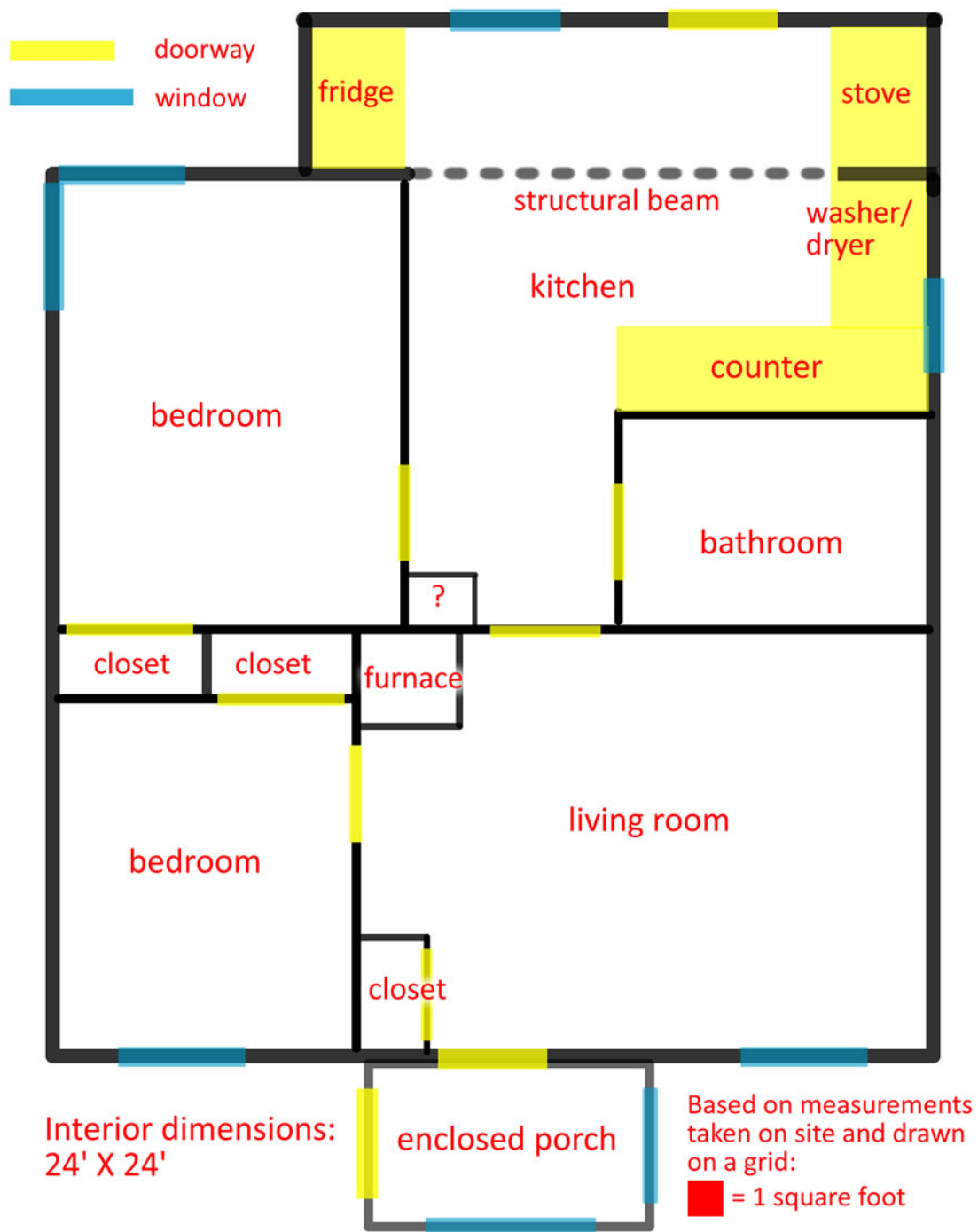


Figure 22: Rough floor plan of the existing dwelling.

SOURCE: Drawing prepared by the author of this report based on measurements taken on site with Stewart Patch.

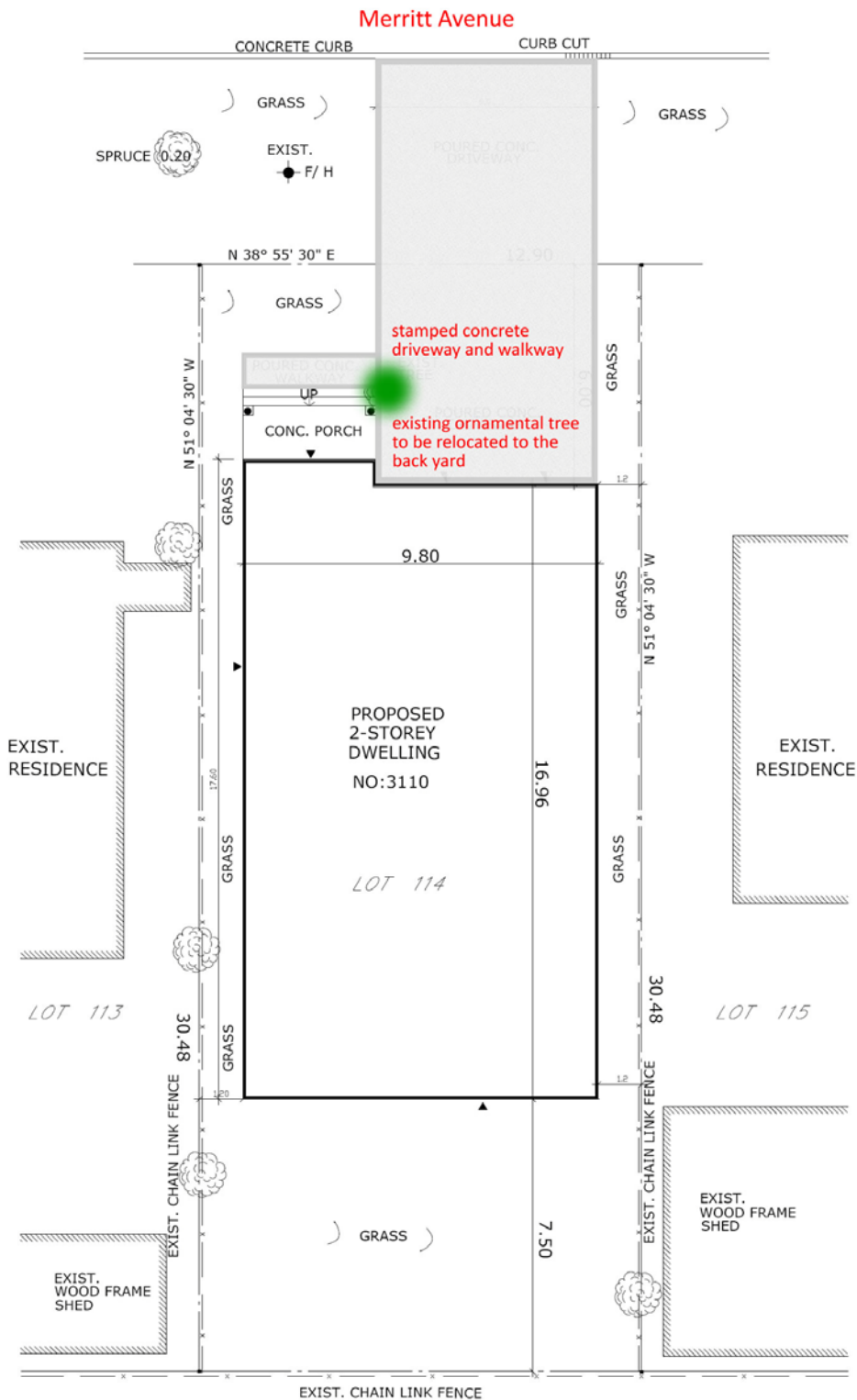


Figure 23: Site plan showing the footprint of the proposed new dwelling, the concrete driveway and walkway, and the existing ornamental tree to be relocated to the back yard.

SOURCE: Section of Site Plan prepared by Sanpro Engineering Inc. (September 2013) with annotations and highlighting by the author of this report.

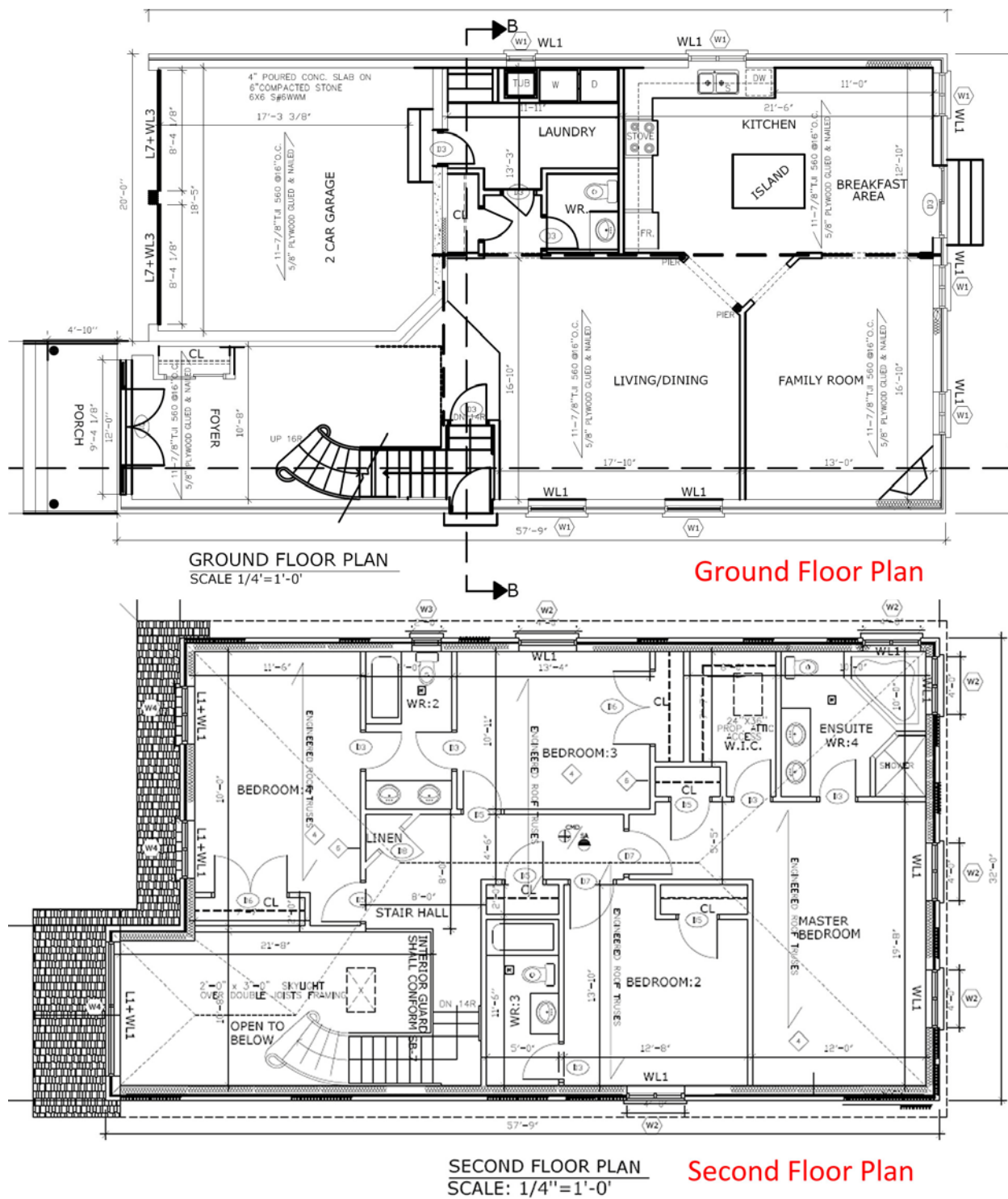


Figure 24: First and second floor plans of the proposed new residence, revised to show changes resulting from the elimination of the false balcony.

SOURCE: Cropped sections of two architectural drawings showing the three floors and roof plan, prepared by Sanpro Engineering Inc., updated since November 2013 to show facade modifications recommended by the author of this report.

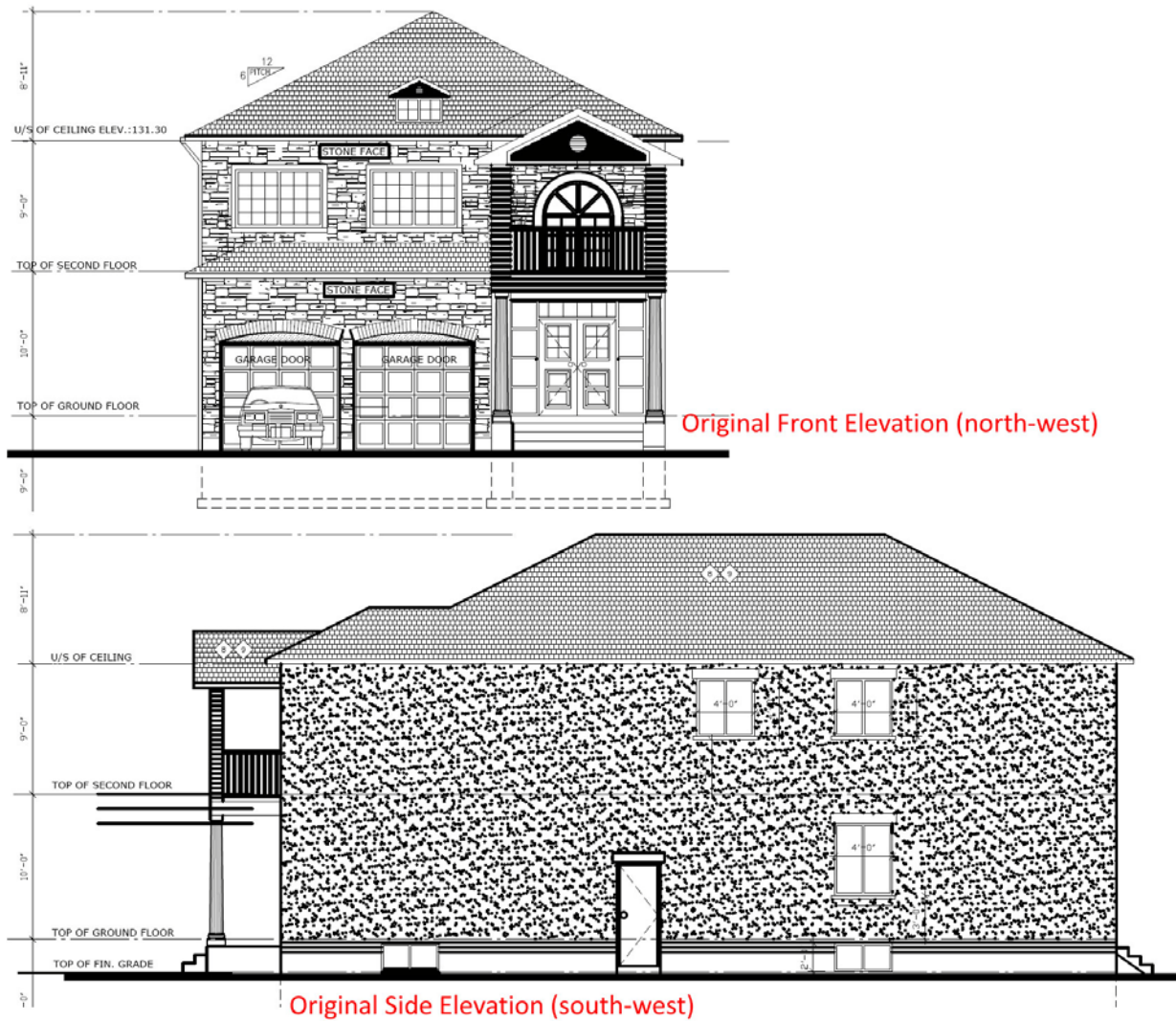


Figure 25: Original front facade elevation and one side elevation for proposed new residence at 3110 Merritt Avenue (identical to original elevations for 7161 Lancaster Avenue).

SOURCE: Cropped sections of architectural drawing showing all four elevations and two sections, prepared by Sanpro Engineering Inc., July 2013, with text annotations by the author of this report.

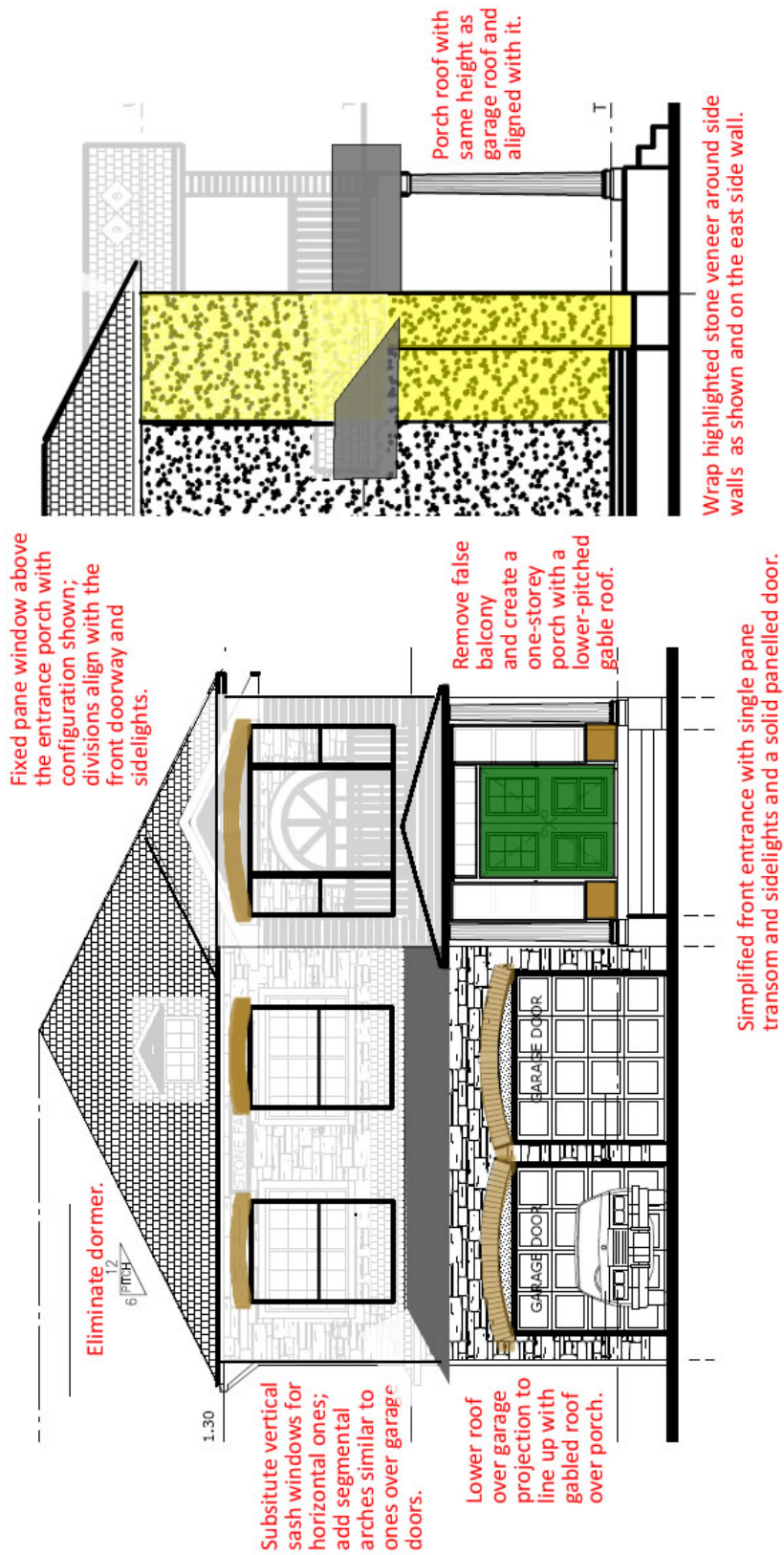


Figure 26: Overlay of front facade and part of one side elevation showing alterations recommended and illustrated by the author of this report.

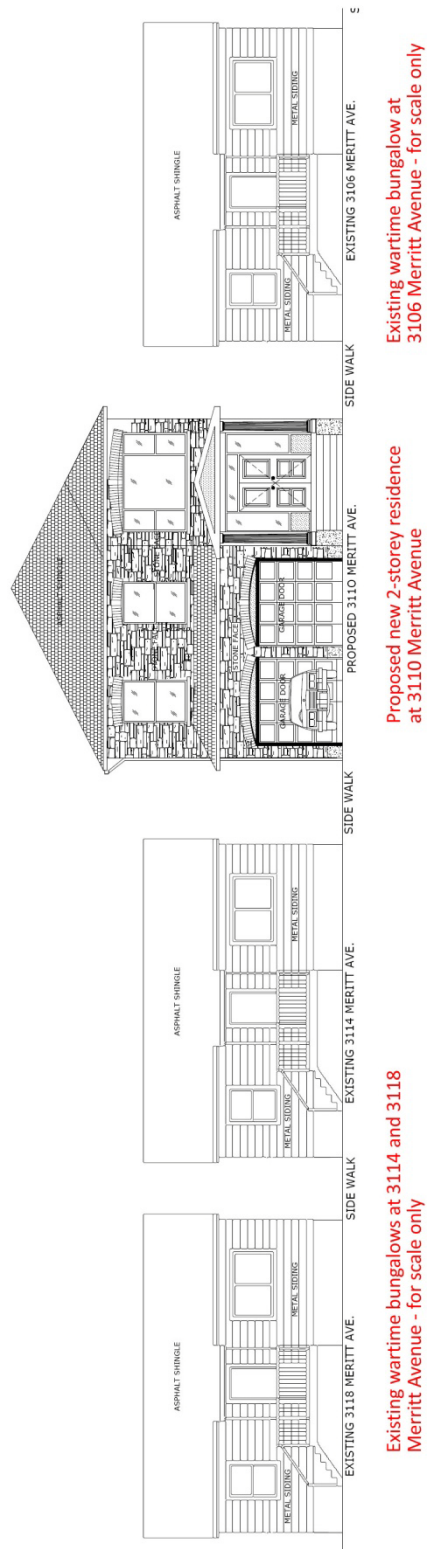


Figure 29: Streetscape elevation showing the proposed two-storey dwelling at #3110 in the context of neighbouring wartime dwellings.

SOURCE: Drawing prepared by Sanpro Engineering Inc., February 2014, with annotations by the author of this report.

9 SITE VISIT PHOTOGRAPHS

NOTE: Photos taken by Stewart Patch, 29 October 2013.

9.1 Setting



Photo 1: 3110 Merritt Avenue looking south-east, with a cluster of semi-mature deciduous and coniferous trees on the adjacent property to the south-west. Its gravel driveway merges with an asphalt driveway on the property to the north-east. The only tree in the front yard of the subject property is a small flowering species shown on the survey plan (**Figure 21**).



Photo 2: 3110 Merritt Avenue and the adjacent original wartime dwelling of similar design at #3114.



Photo 3: View looking north-east down Merritt Avenue towards Lancaster Avenue and the extension of Merritt Avenue as a cul-de-sac beyond the boundary of the original subdivision. In contrast to some other streets in Victory Village, Merritt Avenue was not laid out with sidewalks.



Photo 4: View of Merritt Avenue looking north-east with both sides of the street visible.



Photo 5: View of Merritt Avenue looking more directly north from the same vantage point.



Photo 6: View of Merritt Avenue looking north to north-west from the front yard of the subject property.



Photo 7: View looking south-west along Merritt Avenue towards Victory Crescent.



Photo 8: View looking almost directly south along Merritt Avenue with and the well-tree front yard of the neighbouring property at #3114 and the fire hydrant on the City's road allowance in front of the subject property on the far left.

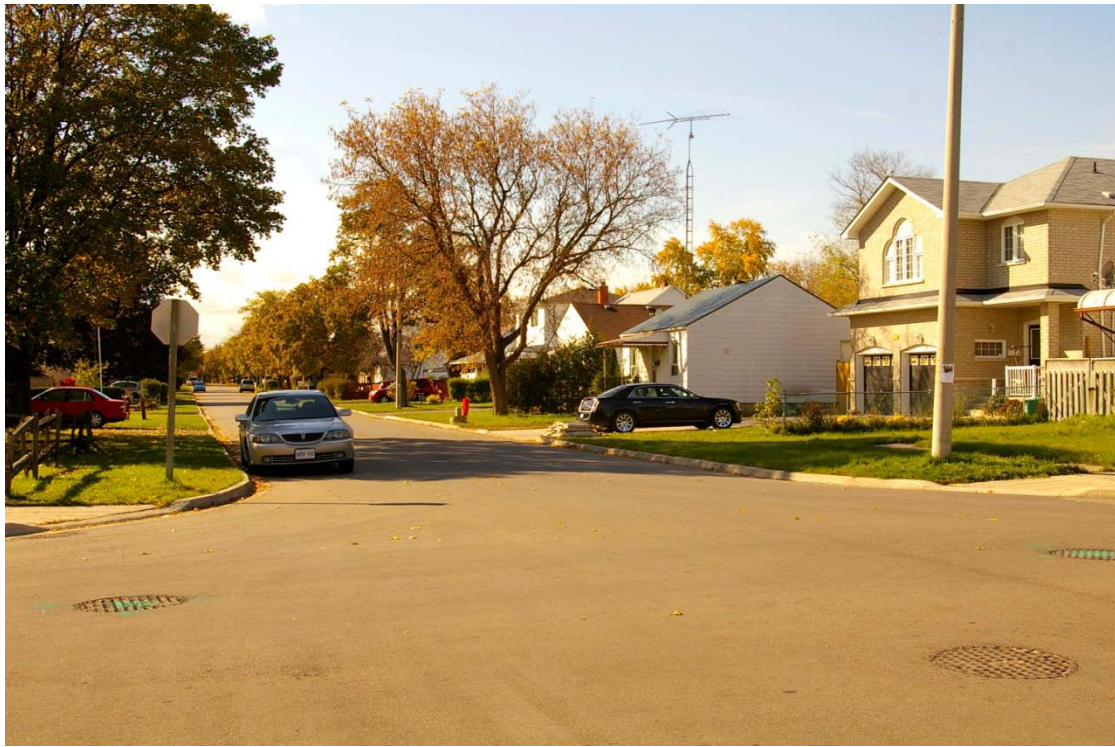


Photo 9: View of Merritt Avenue looking west from the intersection of Lancaster Avenue, with a two storey replacement residence on the right.



Photo 10: View looking east of the curved extension of Merritt Avenue from the intersection of Lancaster Avenue, with two-storey single-family and semi-detached housing built in the late 1960s/early 1970s.



Photo 11: Two original wartime dwellings on the same side of Merritt Avenue as #3110: a front-gabled one-storey at #3146 and a side-gabled 1 ½ storey at #3142, with typical alterations such as replacement doors and windows, vinyl/ aluminum siding and a front porch enclosure.

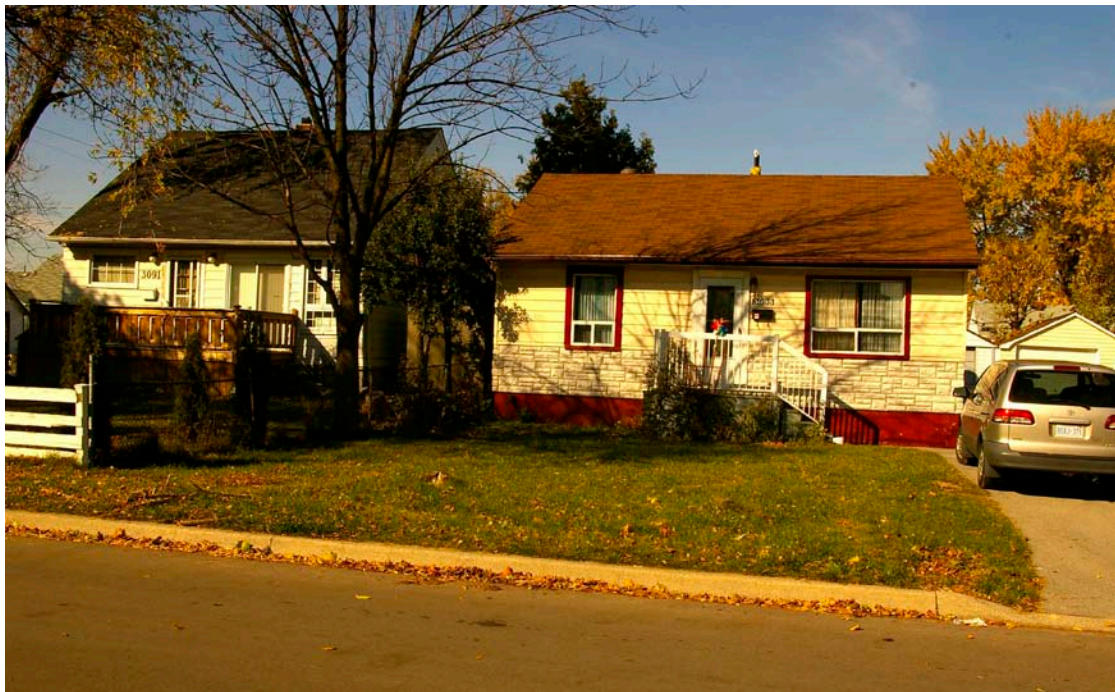


Photo 12: Two original wartime dwellings on the opposite side of Merritt Avenue close to the subject property: a 1 ½ storey at #3091 and a one-storey at #3095, both with side-gabled roofs. Typical alterations include new siding, and replacement doors and windows with some new openings.



Photo 13: A 1 ½ storey wartime dwelling at 3119 Merritt Avenue, directly opposite #3110. The roof has been extended with shallower pitched sections covering a full-width front porch and a rear addition. The front facade has been altered by new window openings/ windows and added siding materials.



Photo 14: An original 1 ½ storey wartime dwelling at 3145 Merritt Avenue (right) and an adjacent two-storey house at #3139, which is an original bungalow with a second-storey addition (completed in 1989).



Photo 15: An original one-storey dwelling at 3094 Merritt Avenue with a second storey addition and enclosed porch, all clad in brick veneer (completed in 2005).

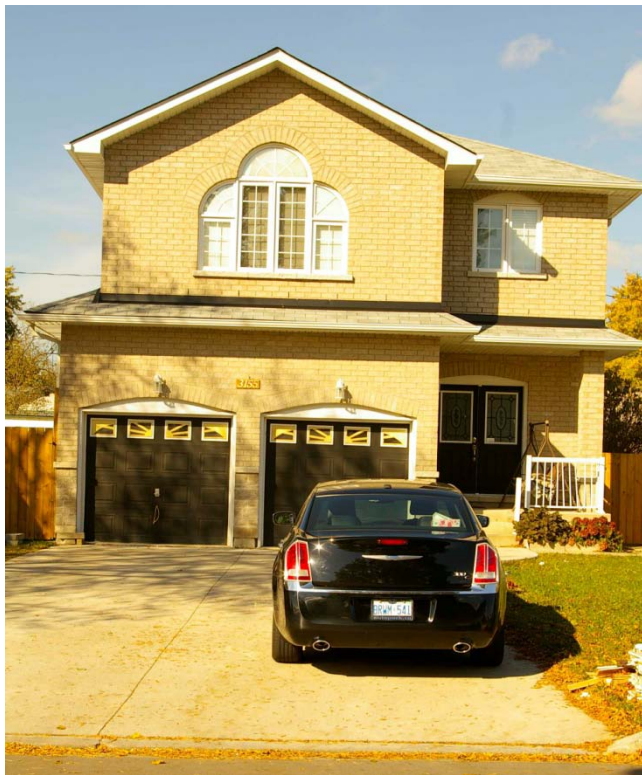


Photo 16: A two-storey replacement residence at 3155 Merritt Avenue also clad in brick veneer, with a double built-in garage and poured concrete driveway (completed in 2008).

9.2 3110 Merritt Avenue



Photo 17: 3110 Merritt Avenue looking east with an ornamental deciduous tree in the front yard.



Photo 18: Front facade of 3110 Merritt Avenue with the free-standing outbuilding in the back yard partially visible to the left.



Photo 19: South-west facing side wall with three windows (living room, bathroom and kitchen).

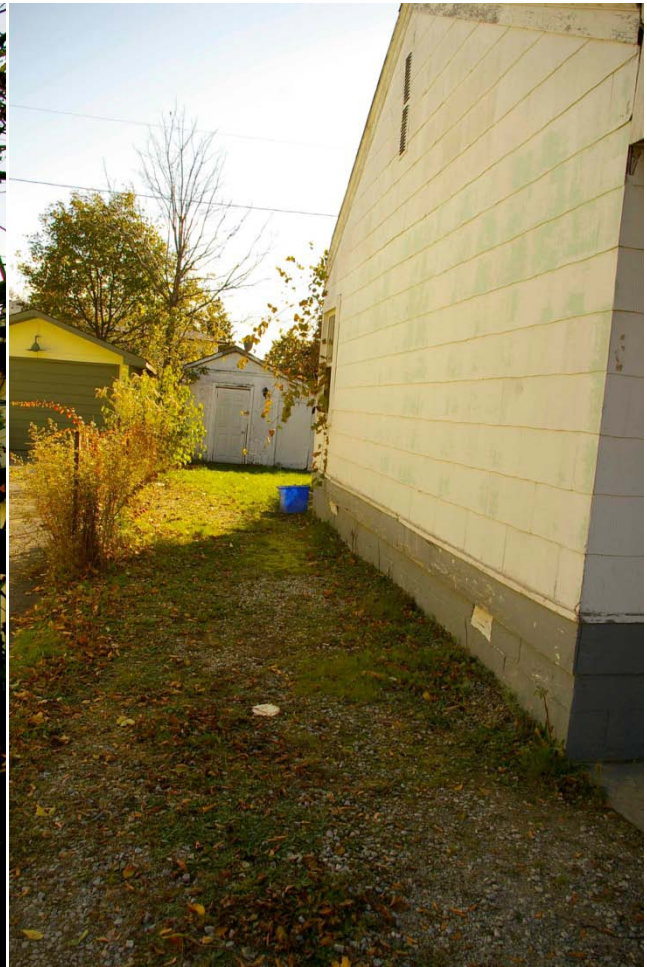


Photo 20: North-east facing side wall with one back bedroom window, looking towards the shed. Shows the concrete block foundation and asbestos shingle siding.



Photo 21: Rear facade looking north-west towards Merritt Avenue.



Photo 22: Rear facade showing the shallow addition with a doorway onto a wood deck extending the width of the addition. The chimney for the existing furnace appears to be in the same location as originally constructed for a coal burning stove.



Photo 23: Rear yard looking south-east towards the rear yard of a two-storey dwelling on Purnell Court, developed since 1966 (see **Figure 19**).



Photo 24: Rear yard looking east showing the free-standing outbuilding, originally intended for use as a garage and storage shed.



Photo 25: View of the side-facing front doorway into the entrance vestibule.



Photo 26: View into the entrance vestibule showing the side-facing horizontal sliding window.

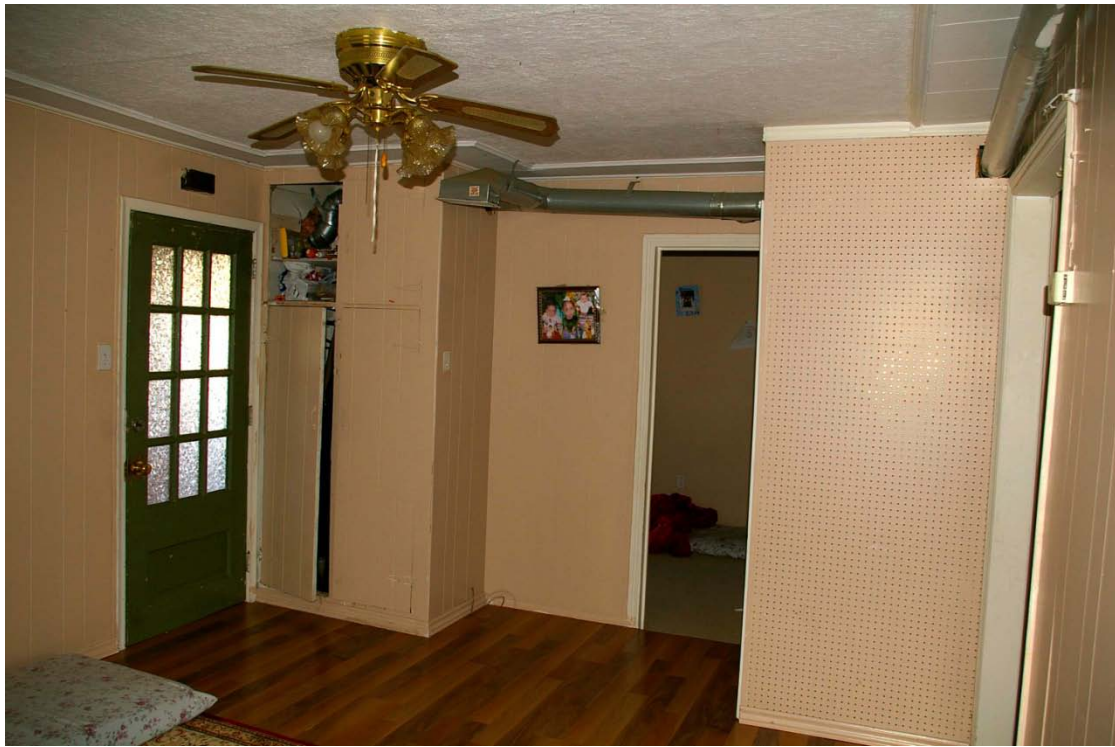


Photo 27: Living room looking towards the original front entrance (door more recent) with the doorway to the front bedroom and furnace enclosure on the right.

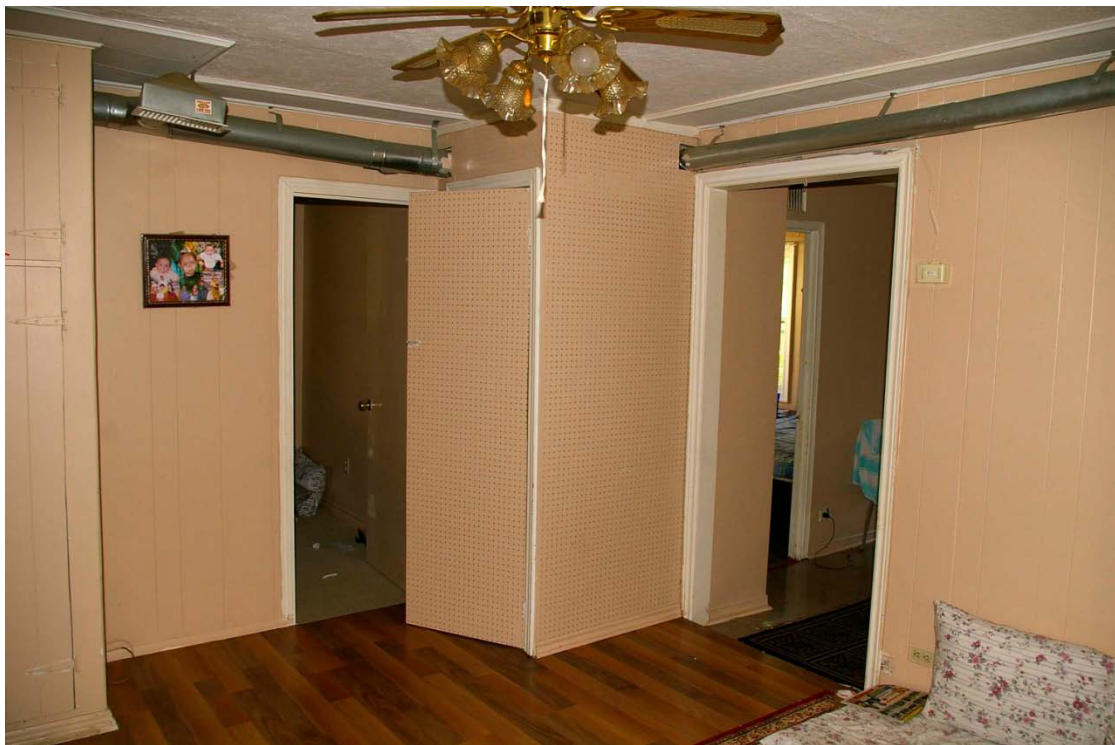


Photo 28: Living room looking towards the front bedroom with the hallway to the back bedroom (visible doorway), bathroom and kitchen on the right. Shows the relatively recent wood laminate flooring installed in the living room.



Photo 29: Living room looking towards the south-west facing window on the side wall.

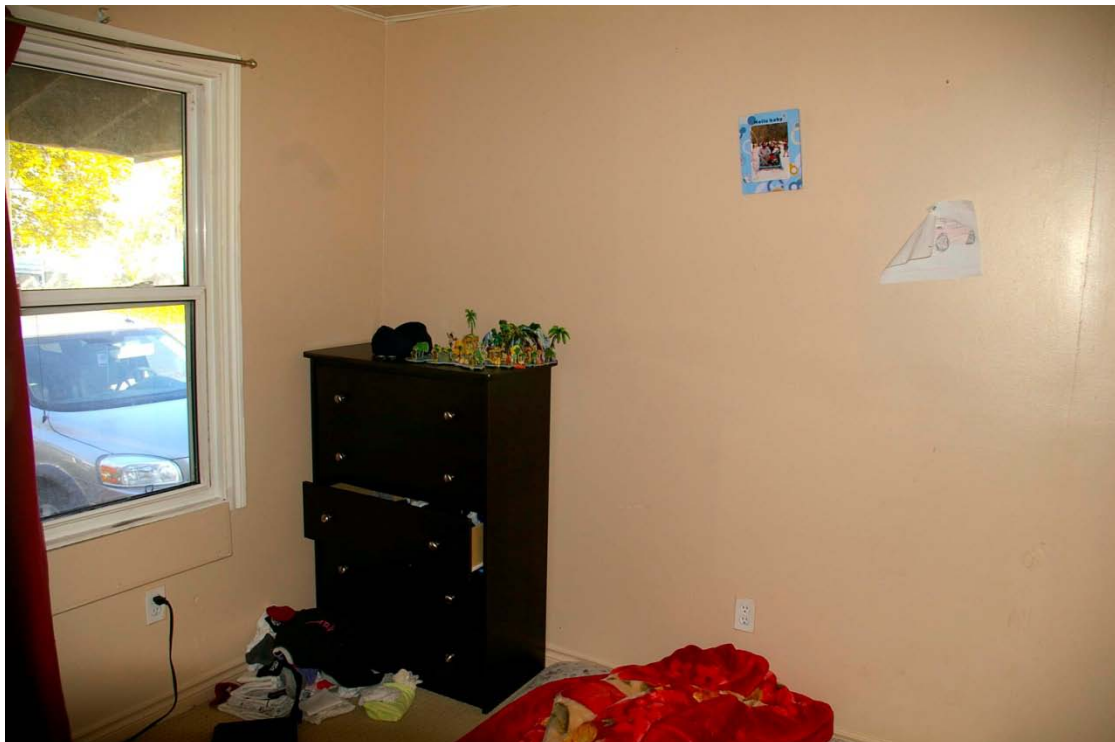


Photo 30: Front bedroom looking through the window facing the front yard and driveway.



Photo 31: Bathroom on the right side of the hallway with a south-west facing window (original opening) and what could be the original claw-foot cast-iron bathtub.



Photo 32: Kitchen looking towards the window on the south-west facing side wall, with wood cabinetry that appears to be original (with a more recent countertop).



Photo 33: Kitchen looking into the extension beyond the supporting beam towards the doorway onto the rear deck.

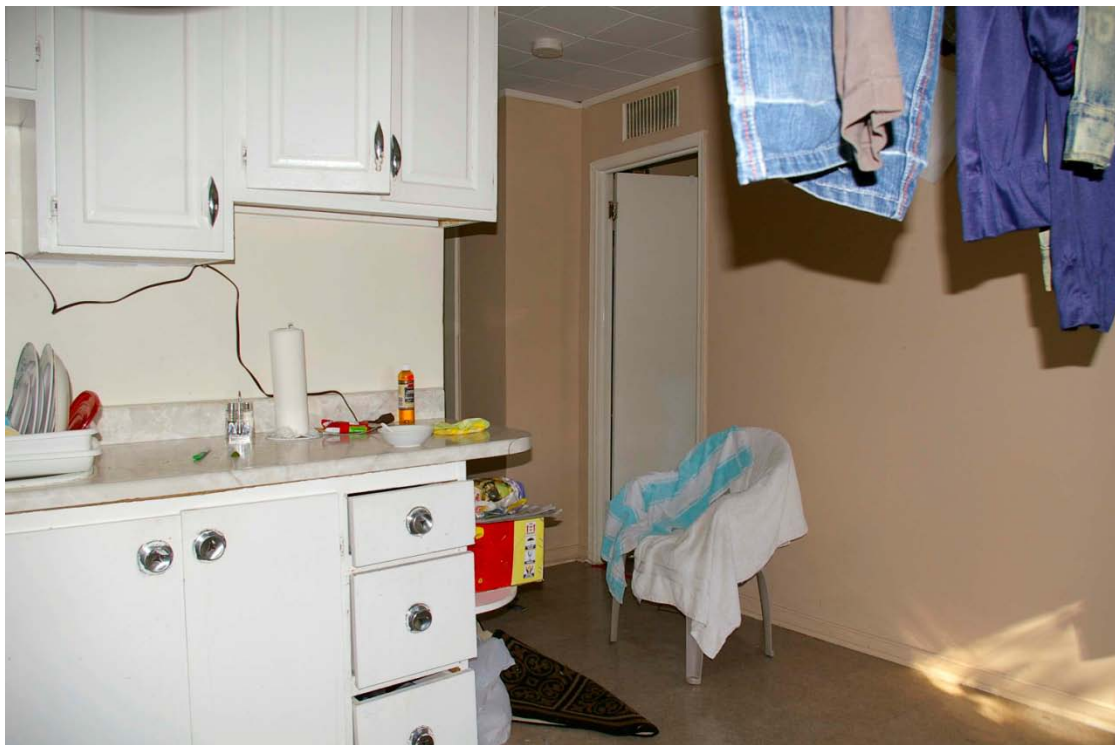


Photo 34: View from the kitchen towards the doorway of the back bedroom.



Photo 35: Back bedroom looking through the doorway to the archway between the hall and the living room.

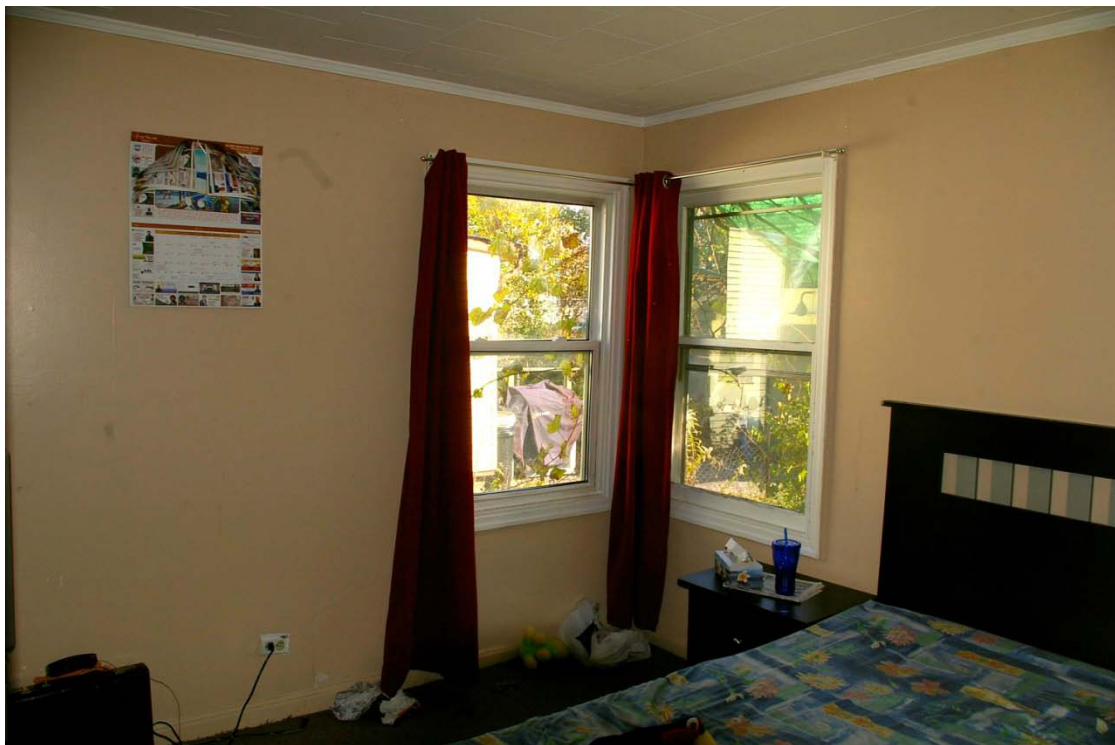


Photo 36: Back bedroom looking towards the east corner of the house with windows on the side and rear walls (original openings and frames).

APPENDIX A: Chain of Ownership

LEGAL DESCRIPTION

Original crown grant: Lot 11, Concession 7, Township of Toronto Gore (South Division), County of Peel

Subject property: Lot 114, Plan of Subdivision 436.

NOTE: Legal terms for the sale of property evolved over time from B & S (Bargain and Sale); Grant to Transfer.

Reg. Num.	Date Yr/mth/day	Instrument Type	Grantor	Grantee	Lands
	1828/01/3	Patent	Crown	King's College	200 acres (Lot 11)
21187	1841/11/11	B & S	King's College	Hugh Cook	100 acres (NE half)
22051	1842/07/01	B & S	King's College	Alexander McDonald	100 acres (NW half)
30556	1842/07/31	Will	Alexander McDonald	[Mary McDonald, wife]	W ½ Lot 11
50805	1853/07/18	Indenture	Mary McDonald (widow)	Alex McDonald (son)	"
1808	1863/07/31	Will	Alex. McDonald	Eliza McDonald [relationship not specified]	"
1218	1890/12/03	B & S	Executor of the Estate of Eliza McDonald	Thomas Codlin	All W ½, north of the G.T.R.
2518	1913/09/08	Will	Thomas Codlin	Fred Codlin	"
2518	1918/11/02	B & S	James Codlin et al., executors of the will	Fred Codlin	"
3306 (see	1939/05/03	Agreement	Fred Codlin	Egvin Kay Ltd.	Land subdivision

note below)					
3379	1941/06/25	Grant	Frederick Codlin et ux.	National Steel Car Corporation Ltd.	Pt W ½ [50' wide strip in SE. corner]
3412	1942/04/21	Expropriation Plan	Wartime Housing Ltd.	for easement and sewer	Pt.
341[?]	1942/06/05	Expropriation Plan	Wartime Housing Ltd.	for sewer, etc.	Pt.
3431 (see note below)	1942/10/15	Grant	Fred Codlin et ux. [wife]	His Majesty the King in the Right of Canada *	Pt W ½ and O.L. [other lands]
436	1952/01/28	Plan [of subdivision]	Central Mortgage and Housing Corporation		Pt W ½ and O.L.
<p>NOTE RE: INSTRUMENTS 3306 TO 3431</p> <p>Instrument #3306 was an agreement registered as Plan 316 on July 4, 1939 between Fred Codlin and a developer to lay out a small plan of subdivision on the west side of his property with 41 lots. This plan was soon abandoned when the federal government became interested in his property as the ideal site for a subdivision of wartime housing.</p> <p>Instruments 3379 to 3431 all pertain to the acquisition of land parcels by Wartime Housing Ltd. from Fred Codlin. The Deed of Land between Fred Codlin and His Majesty the King (instr. 3431) is accompanied by the survey plan prepared by the federal Department of Munitions & Supply, Wartime Housing Limited (Figure 16), which shows the 1939 plan of subdivision. Excluded from Fred Codlin's property was a 50' wide strip of land in the south-east corner conveyed to the National Steel Car Company. (Instrument #3379). It appears that Fred Codlin retained ownership of a truncated L-shaped portion of land abutting the CNR lands in the south-west corner of his property. The Deed indicates the Dominion of Canada paid \$20,000 for a 91.4 acre parcel of land, the boundaries of which are shown on the accompanying plan.</p>					

TRANSACTIONS FOR LOT 114 OF PLAN 436					
172792	1963/11/18	Grant	Central Mortgage and Housing Corporation	Frederick Charles and Lilian May (wife) Norman as joint tenants	LT 114, PL 436
LT1906285	1999/01/22	Transfer	Lilian May Norman [Frederick deceased]	Lilian May Norman, Jason Carew and Donna Norman.	
PR367891	1999/03/26	Transfer	Lilian May Norman, Jason Carew and Donna Norman	Barbara Bonifacio	
PR367891	2002/12/20	Transfer	Barbara Bonifacio	Francesco and Adelina Bonifacio	
PR887547	2005/07/15	Transfer	Francesco and Adelina Bonifacio	Saitnauth and Marlene Dyal	
PR1040369	2006/04/03	Transfer	Saitnauth and Marlene Dyal	Shevil Ford	
PR2118914	2010/07/02	Transfer	Shevil Ford	Otis Kwaku Duah	
PR2109546	2011/11/17	Transfer	Otis Kwaku Duah	Rafika Manjra	



Corporate Report

Clerk's Files

Originator's
Files

DATE: April 25, 2014

TO: Chair and Members of the Heritage Advisory Committee
Meeting Date: May 20, 2014

FROM: Paul A. Mitcham, P. Eng, MBA
Commissioner of Community Services

SUBJECT: **Designated Heritage Property Grants 2014**

RECOMMENDATION: That the Heritage Property Grant Program requests be approved as outlined in the report from the Commissioner of Community Services, dated April 25, 2014.

BACKGROUND: In May 2007 Council adopted By-law 0184-2007, as amended February 25, 2009, to provide grants to owners of heritage designated properties. The program assists heritage designated property owners with financial assistance from a minimum of \$500 to a maximum of \$5,000 in matching funds for conservation projects, and up to \$10,000 for structural projects. Properties must be designated under the *Ontario Heritage Act*, and the work proposed must be a restoration or reconstruction of original architectural elements

COMMENTS: In accordance with procedural guidelines, the Heritage Property Grant Review Subcommittee, appointed by the Heritage Advisory Committee, reviewed the applications to ensure they are complete and meet all of the program's criteria.

Eligible projects include:

- Conservation of existing architectural elements;
- Reconstruction of existing architectural elements that need repair;
- Restoration of architectural elements which have been lost but can

be replicated based on documentary evidence; and

- Repair and restoration of building elements required for structural soundness.

Twenty-four applications were submitted by the advertised deadline of April 14, 2014. Two were deemed ineligible and one was incomplete. The number of eligible projects amount to \$99,586.47, which exceeds the \$75,000 cap. Therefore all eligible grant requests have been lowered. A summary of the recommended grant awards is attached as Appendix 1.

Successful grant applicants will be notified of these results with any conditions, including whether the work proposed requires a heritage permit. Work must be complete by October 31, 2014, so that final inspections can be made by staff shortly thereafter. Invoices are due by November 28, 2014.

FINANCIAL IMPACT: \$75,000 funding is available in cost centre 21134 for the Designated Heritage Property Grant program.

CONCLUSION: A total of twenty-one Designated Heritage Property Grant applications are recommended for approval in the 2014 Heritage Property Grant Program. Payment of the grants to the successful applicants will be within the allotted \$75,000 fund.

ATTACHMENTS: Appendix 1: Summary of Heritage Grants 2014



Paul A. Mitcham, P. Eng, MBA
Commissioner of Community Services

Prepared By: Elaine Eigl, Heritage Coordinator

Summary of 2014 Designated Heritage Property Grants

Appendix 1

Number	Address	Applicant	Project	Max. Grant Amount
1	1045 Barberry Lane	Kimberly Van Wart	Window replacement	\$ 3,765.57
2	41 Bay Street	Trevor Crawford	Porch replication	\$ 3,765.57
3	185 Derry Road	Melissa Wooder	Roof (tar & gravel) replacement	\$ 3,765.57
4	1556 Dundas Street West	Helen Grubisa	Shutter replicas	\$ 3,765.57
5	1560 Dundas Street West	Bill Raney	Window restoration (3 on west elevation)	\$ 939.95
6	36 Lake Street	Ryan Hunwicks	Garage roof (cedar shake) restoration	\$ 3,265.79
7	2025 Mississauga Road	Linda Bouji	Restoration/replication of 11 'masks' below the fascia	\$ 3,765.57
8	1074 Old Derry Road	Mike DeLongte	Foundation repair	\$ 7,531.14
9	7050 Old Mill Lane	Mike Byrne	Replace 5 rotten structural beams in crawl space	\$ 4,158.83
10	7005 Pond Street	Martin Boeykens	Foundation repair (leaking)	\$ 7,531.14
11	7015 Pond Street	Dave Moir	Replace rotten decks and restore flagstone	\$ 1,779.23
12	7057 Pond Street	Mike & Sue Peco	Exterior painting	\$ 3,765.57
13	7067 Pond Street	Janet Clewes	Roof (shingle) replacement	\$ 3,765.57
14	7105 Pond Street	Rosemary Wilson	Roof repairs	\$ 1,137.25
15	7135 Pond Street	Brian Carmody	Repair and preservation of cedar roof.	\$ 2,967.93
16	208/210 Queen Street South	Karim Samra	Repair & paint all original wood components	\$ 1,340.59
17	264 Queen Street South	Kurtis Van Keulen	Restore front façade.	\$ 3,765.57
18	271 Queen Street South	Patricia Phan	Restore building exterior	\$ 3,765.57
19	295 Queen Street South	Ray Gaule	Window restoration/replication	\$ 3,765.57
20	7059 Second Line West	Elizabeth Backhouse	Fascia & eaves trough/downpipe replacement	\$ 2,926.85
21	1101 Willow Lane	Bernadette	Window/door repair or stucco repair	\$ 3,765.57
				\$75,000.00

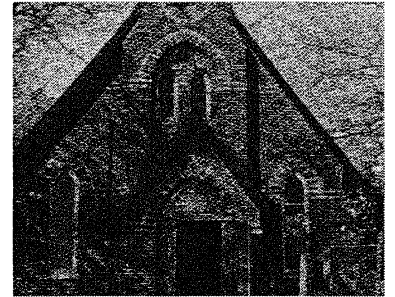
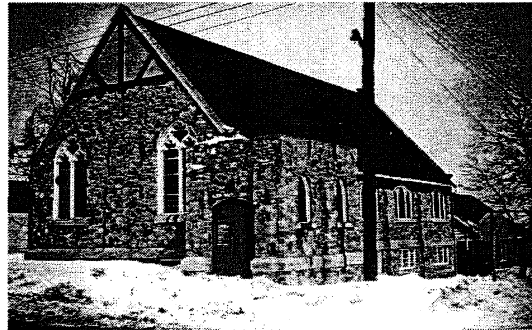
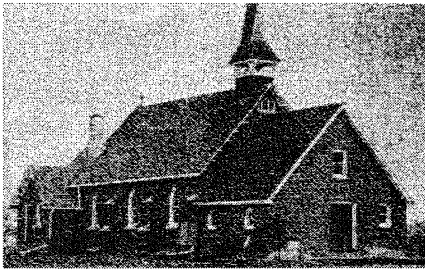


CALEDON HERITAGE FOUNDATION

4th Annual Heritage Bus Tour

CHURCHES/SITES/CEMETERIES

Thursday, May 29, 2014



***Leave Caledon Community Complex
at 9 a.m. sharp***

Tickets: \$40 each

***Includes morning snack and lunch
at Spirit Tree Cidery***

Reserve tickets by calling

Fay 519-927-5370 or

email f.mccrea@sympatico.ca

A fundraising project for Caledon Heritage Foundation

Ministry of Citizenship
and Immigration

Minister

6th Floor
400 University Avenue
Toronto ON M7A 2R9
Tel.: (416) 325-6200
Fax: (416) 325-6195

Ministère des Affaires civiles
et de l'immigration

Ministre

6^e étage
400, avenue University
Toronto ON M7A 2R9
Tél.: (416) 325-6200
Télééc.: (416) 325-6195



April, 2014

Re : Ontario Medal For Good Citizenship

Dear Friends:

I am pleased to invite you to participate in the Ontario Medal for Good Citizenship by nominating a deserving citizen.

Established in 1973, the Ontario Medal for Good Citizenship honours Ontarians who, through exceptional, long-term efforts, have made outstanding contributions to community life.

Recipients will be presented with their medal by the Lieutenant Governor of Ontario at a special ceremony at Queen's Park in the Fall of 2014.

Nominations can be made by visiting the Ministry of Citizenship and Immigration's website at: www.ontario.ca/honoursandawards. You may submit your nomination on-line by choosing the "Nominate Online" option, or download the PDF format from the website to submit by mail. For any further information, please contact the Ontario Honours and Awards Secretariat at 416 314-7526, 1 877 832-8622 or (TTY) 416 327-2391. Nominations must be received by July 17, 2014.

I encourage you to take the time to nominate a deserving citizen in your community for an Ontario Medal for Good Citizenship. The men and women we honour stand as shining examples to us all.

Sincerely,

A handwritten signature in black ink, appearing to read 'Michael Coteau', written over a large, stylized, looping flourish.

Michael Coteau
Minister



NOTICE OF COMMENCEMENT
Detail Design - GWP 2163-10-00
Queen Elizabeth Way (QEW) and Highway 403 Structural Rehabilitation and
Replacements from Trafalgar Road to Winston Churchill Boulevard

THE PROJECT

The Ministry of Transportation (MTO) is undertaking the Detail Design for the rehabilitation and/or replacement of bridge/culvert structures on the QEW and Highway 403 from Trafalgar Road to Winston Churchill Boulevard, a distance of approximately 7 kilometres, in the Town of Oakville and the City of Mississauga. The project also includes illumination, tall wall, barrier/retaining wall, pavement rehabilitation, overhead signs and drainage improvements.

The proposed improvements will be divided into separate contracts. The final contract will include the pavement rehabilitation of the QEW lanes and ramps within the project limits.

The following structures are included in the project:

- QEW westbound and eastbound lanes over the Highway 403 West-North Ramp
- QEW at Joshua's Creek Culvert
- Highway 403 Dundas Street Underpasses
- Highway 403 West-North Ramp over Ford Drive
- QEW westbound and eastbound lanes over Ford Drive
- Highway 403 North-West Ramp over Upper Middle Road
- Ford Drive Off-Ramp over Highway 403 West-North Ramp
- Upper Middle Road over Highway 403 West-North Ramp
- Highway 403 North-West Ramp over Ford Drive

THE PROCESS

The project is being carried out in accordance with the *Class Environmental Assessment (EA) for Provincial Transportation Facilities* (2000) as a 'Group B' undertaking. The MTO completed a Preliminary Design Study for the proposed improvements as part of a larger study that included the future expansion of the highway. The study was documented in a TESR that received environmental clearance in Fall 2013. A separate Design and Construction Report (DCR) will be prepared for each contract included in the project and made available for a 30-day public review period.

A Public Information Centre (PIC) for this project is currently anticipated in late spring 2015 to display and seek input on the proposed improvements and the construction staging/traffic management plan.

COMMENTS

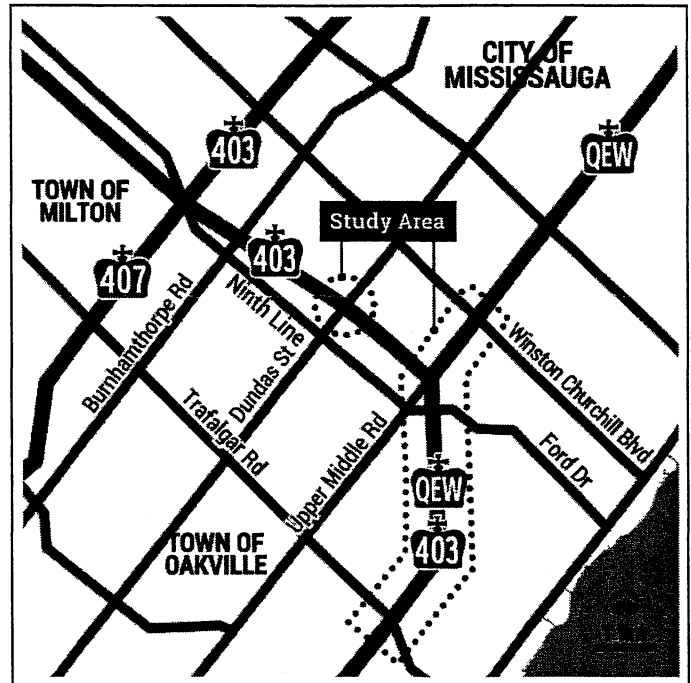
Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

If you wish to comment on this project, have your name added to the project mailing list, or have any questions about this project, please contact one of the individuals identified below:

Mr. Adam Barg, P.Eng.
 Consultant Project Manager
 Stantec Consulting Ltd.
 200 - 835 Paramount Drive
 Stoney Creek ON L8J 0B4
 Tel.: 905-385-3234 (call collect)
 Fax: 905-385-3534
 E-mail: Adam.Barg@stantec.com

Mr. Moin Khan, P.Eng.
 Senior Project Engineer
 Ministry of Transportation
 1201 Wilson Avenue
 Building D, 4th Floor
 Downsview ON M3M 1J8
 Tel.: 416-235-5271
 Fax: 416-235-3576
 E-mail: Moin.Khan@ontario.ca

Ms. Maya Caron, MCIP, RPP
 Senior Environmental Planner
 Stantec Consulting Ltd.
 100 - 401 Wellington Street
 Toronto ON M5V 1E7
 Tel.: 416-598-7162 (call collect)
 Fax: 416-596-6680
 E-mail: Maya.Caron@stantec.com



If you have any accessibility requirements in order to participate in this project, please contact one of the Project Team members listed above.

Version française disponible en composant le (416) 598-7162, (Maya).



ServiceOntario

ServiceOntario

Central Production and
Verification Services BranchDirection des services centraux de
production et de vérification20 Dundas St. West, 4th Floor
Toronto ON M5G 2C220 rue Dundas Ouest, 4^e étage
Toronto ON M5G 2E5Telephone: (416) 314-4442
Facsimile: (416) 314-4899Téléphone: (416) 314-4442
Télécopieur: (416) 314-4899

May 12, 2014

Laura Waldie, MA, CAHP
Team Leader/Acting Senior Heritage Coordinator of Planning
Culture Division, Community Services, City of Mississauga
201 City Centre Drive, 2nd Floor
Mississauga, ON
L5B 2T4

Dear Ms. Waldie,

I understand that you have expressed concerns regarding the alleged "future removal (anticipated to take place in 2015) of land registry documents from local ServiceOntario locations, currently located within individual upper-tier municipalities across the province, to a "central" office in Thunder Bay". I further understand that you were contacted by a ServiceOntario representative indicating that ServiceOntario does not have plans to transfer land registry documents from their current locations to Thunder Bay.

I can assure you that ServiceOntario's objective is not to place unreasonable burden on the users of the electronic land registration system. In fact, the modernization of the land registration system has enabled the professional community to search and register documents electronically from remote locations, making Ontario the first jurisdiction to transform its land registration system to a modern electronic one, an achievement that has received worldwide recognition.

The integrity of the data in the land registration records is of utmost importance to the government. The decision to remove books and records from land registry offices and to provide the public with alternate methods of viewing documents was made after careful consideration. This has been occurring for several years across the province and has been widely accepted by the professional community to maintain the integrity of the records and preserve their historical value. While ServiceOntario continues to look for efficiencies and improved service delivery, there are no plans for future removal and transfer of land registry documents to a central office in Thunder Bay in 2015.

Thank you for providing me the opportunity to address your concerns.

Sincerely,

Denis Blais
Acting Director
Central Production and Verification Services Branch
ServiceOntario

c: Bert Duclos, Ministry of Tourism, Culture and Sport

MemorandumCommunity Services Department
Culture Division

TO: Chair and Members of the Heritage Advisory Committee

FROM: Laura Waldie, Heritage Coordinator

DATE: April 28, 2014

SUBJECT: Monthly Update Memorandum from Heritage Coordinators

The following non-substantive alterations came forward to Heritage Planning and did not require a Heritage Property Permit. These items are for information only.

Ward 1:

- a. 107 Veronica Drive: A cabana to the rear of the property near the existing swimming pool is to be constructed. The structure is located at the rear of the property and out of view of the public realm.

Ward 2:

- a. 2489 North Sheridan Way: A wider window is to be installed at the rear of the building and new roof brackets install under the soffits. The alteration is deemed non-substantive.

Ward 11:

- a. 6755 Mississauga Road: The renovations were interior and did not affect the façade of the exterior.
- b. 31 Thomas Street: A change to the door or window materials which are like for like in materials.

Laura Waldie
Heritage Coordinator
Culture Division
905-615-3200, ext. 5366
laura.waldie@mississauga.ca

STATUS OF OUTSTANDING ISSUES FROM THE HERITAGE ADVISORY COMMITTEE
Prepared by Sacha Smith, Legislative Coordinator, for the May 20, 2014 Heritage Advisory Committee Agenda

Property Name or General Issue	Property Address	HAC Recommendation or Council Resolution (if passed)	Latest Status
N/A	3650 Eglinton Avenue West	Council Resolution 0041-2014	That the request to demolish a portion of the structure at 3650 Eglinton Avenue West, which is listed on the City's Heritage Register, be deferred until a Site Plan Approval application has been filed with the Planning and Building Department, at which point Heritage Planning will submit formal comments on the application.
Clarkson General Store and William Clarkson House	1130-1132 Clarkson Road North and 1140 Clarkson Road North, respectively	HAC-0103-2013	That Heritage staff prepare a Cultural Heritage Assessment to designate the Clarkson General Store and William Clarkson House properties located at 1130-1132 and 1140 Clarkson Road North, respectively, for consideration at a future Heritage Advisory Committee meeting once the ownership of the properties has been determined by the judicial system.
Grand Duchess Olga and her Connections to Camilla Road and Mississauga	2130 Camilla Road	HAC-0008-2014	In response to the Chair, Ms. Eigl said that she was working on a Corporate Report for consideration at a future Committee meeting regarding Ms. Fisher's suggestion for the City to formally recognize Grand Duchess Olga's time in the City in some way.
Transfer of Microfilm Land Registry Records to Thunder Bay, Ontario	N/A	N/A	Laura Waldie spoke with respect to the transfer of Microfilm Land Registry Records to Thunder Bay, Ontario. She noted that the project is on hold while the process is being reviewed to see how they can work around it. She further noted that Service Ontario received a lot of complaints from heritage planners and researchers on how the change would affect their work.