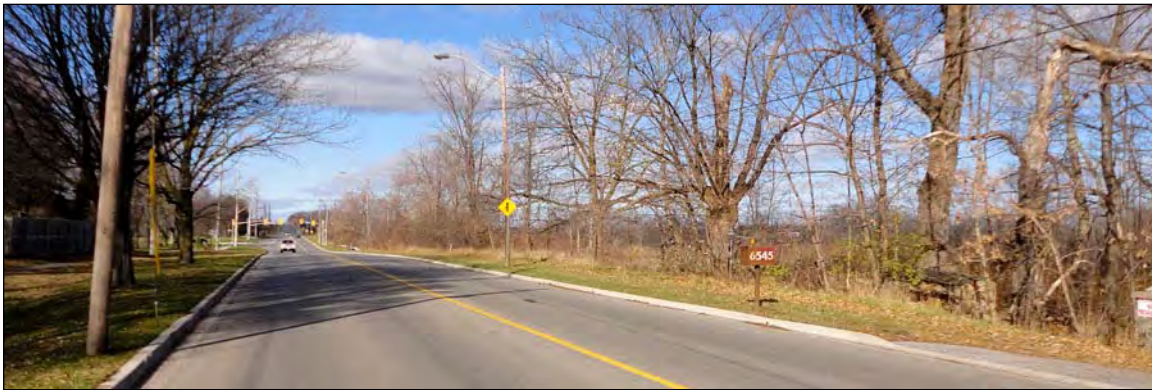


CULTURAL HERITAGE ASSESSMENT REPORT

CULTURAL HERITAGE LANDSCAPES & BUILT HERITAGE RESOURCES

CLASS ENVIRONMENTAL ASSESSMENT (EA) STUDY AND PRELIMINARY DESIGN: CREDITVIEW ROAD FROM BANCROFT DRIVE TO OLD CREDITVIEW ROAD CITY OF MISSISSAUGA, ONTARIO



September 2015

**Prepared for:
AECOM**

Prepared by:



UNTERMAN McPHAIL ASSOCIATES
HERITAGE RESOURCE MANAGEMENT CONSULTANTS

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September 2015

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1.0 INTRODUCTION

1.1 Project Description

AECOM retained Unterman McPhail Associates, Heritage Management Resource Consultants, to undertake a Cultural Heritage Assessment Report (CHAR) for cultural heritage landscapes and built heritage resources as part of the Class Environmental Assessment (EA) Study and Preliminary Design for Creditview Road, from Bancroft Drive to Old Creditview Road in the City of Mississauga (**Figure 1**). The City of Mississauga has initiated a Class EA Study to investigate the need for additional capacity and traffic management improvements along this stretch of Creditview Road. The study is being undertaken in accordance with the planning and design process for Schedule 'C' projects as set out in the Municipal Engineers' Association *Municipal Class Environmental Assessment* document (October 2000, amended in 2007 and 2011).

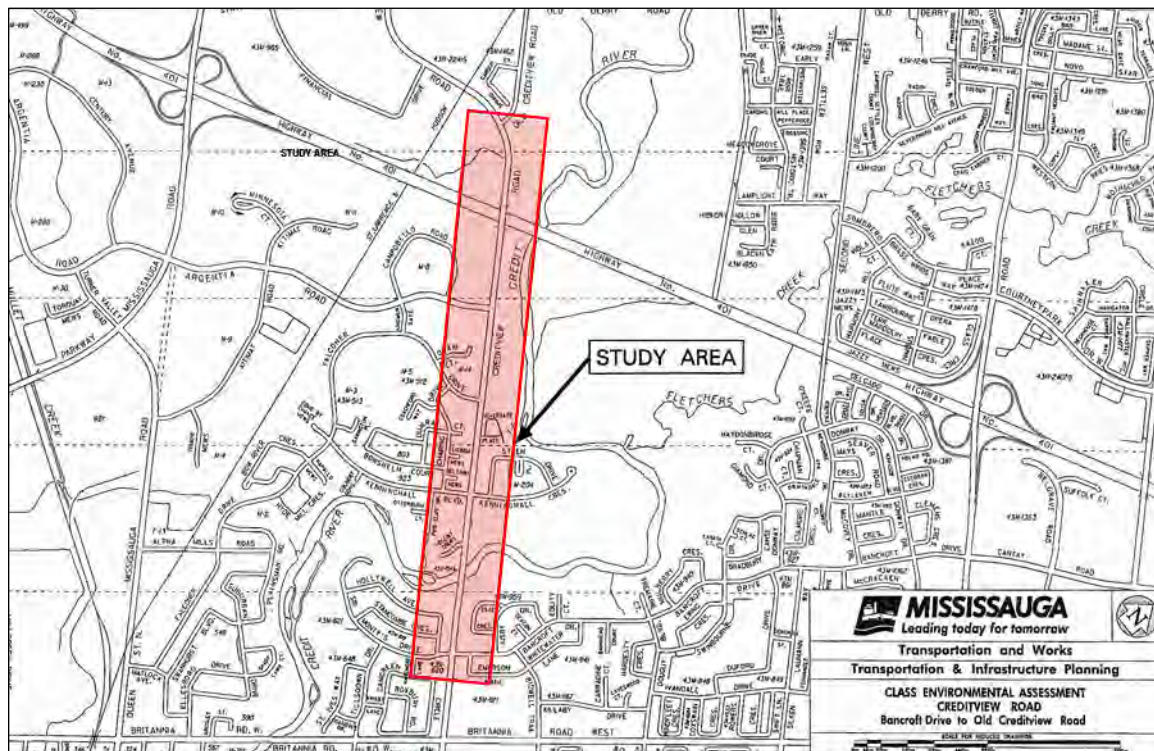


Figure 1. Study Area map for the Class Environmental Assessment, Creditview Road from Bancroft Drive to Old Creditview Road in the City of Mississauga [City of Mississauga, 2013].

The principal objectives of this CHAR are:

- to prepare an historical summary of the development of the study area through the review of both primary and secondary sources as well as historical mapping;
- to conduct a survey of the cultural heritage landscapes and built heritage resources found within the study area;

- to identify cultural heritage landscapes and built heritage resources within the study area through the analysis of major historical themes and activities, historic mapping and site review activities;
- to identify sensitivities for change; and
- to provide general mitigation recommendations respecting the proposed endeavour.

Creditview Road was laid out as part of the New Survey of Toronto Township in 1819, as the road allowance between Concessions 3 and 4 West of Hurontario Street (WHS), and was known initially as Third Line West. It ran through Toronto Township from the Base Line (Eglinton Avenue) north to Churchville on the border with Chinguacousy Township. The road allowance was opened as the neighbouring lands were settled for agricultural purposes in the first half of the 1800s. Creditview Road formed part of the local road network and provided connections for residents travelling south to Streetsville and north to Meadowvale. It remained a township road with a gravel surface through the first half of the 20th century. As lands to the west of the roadway were redeveloped for residential purposes in the late 1960s, the municipality undertook improvements to Creditview Road. The route was paved and a boulevard and a sidewalk were introduced along the west side. Creditview Road is currently identified as a Major Collector Road in the City of Mississauga Official Plan.

Richard Unterman, Unterman McPhail Associates and Jean Simonton, Heritage Consultant, undertook a windshield survey of the study area in December 2013. Cultural heritage landscapes and principal above ground built heritage resources, 40 years of age or older, located within and adjacent to the Creditview Road corridor were identified. Given the nature of the project, the emphasis of the site review was placed on the identification of those cultural heritage resources located within and adjacent to the existing right-of-way for Creditview Road.

2.0 ENVIRONMENTAL ASSESSMENT REQUIREMENTS & CULTURAL HERITAGE RESOURCES

The need for the identification, evaluation, management and conservation of Ontario's heritage is acknowledged as an essential component of environmental assessment and municipal planning in Ontario.

For the most part, the analysis of cultural heritage resources in the study area addresses those above-ground, person-made heritage resources 40 years of age and older. The application of this rolling 40-year principle is an accepted federal and provincial practice for the preliminary identification of cultural heritage resources that may be of heritage interest or value. However, its application does not imply that all built heritage resources or cultural heritage landscapes that are over 40 years old are worthy of the same levels of protection or preservation.

2.1 Environmental Assessment Act (EAA)

An environmental assessment provides a decision-making process used to promote good environmental planning by assessing the potential effects and benefits of certain activities on the environment. In Ontario, this process is defined and finds its authority in the *Environmental Assessment Act* (EAA). The purpose of the EAA is to provide for the protection, conservation, and wise management of Ontario's environment.

The EAA applies to all public activities. This includes projects originating from Ontario ministries and agencies, municipalities, public utilities, and Conservation Authorities. Projects subject to the Act are typically infrastructure developments and include such things as public roads and highways, transit facilities, waste management facilities, electrical generation and transmission facilities, and flood protection works.

The analysis throughout the study process addresses that part of the *Environmental Assessment Act*, subsection 1(c), which defines “*environment*” to include:

“...*cultural conditions that influence the life of humans or a community;*”

as well as,

“...*any building, structure, machine or other device or thing made by humans.*”

Infrastructure work and its associated construction activities may potentially affect cultural heritage resources in a number of ways. The effects may include displacement through removal or demolition and/or disruption by the introduction of physical, visual, audible or atmospheric elements that are not in keeping with the character of the cultural heritage resources and, or their setting.

2.1.1 Municipal Class Environmental Assessment (MCEA)

The *Municipal Class Environmental Assessment* (MCEA) (October 2000, as amended in 2007 and 2011) outlines a procedure whereby municipalities can comply with the requirements of the EAA. It identifies potential positive and negative effects of projects such as road improvements, facility expansions or to facilitate a new service. The process includes an evaluation of impacts on the natural and social environment including culture. The MCEA applies to municipal infrastructure projects including roads, water and wastewater projects.

Since projects undertaken by municipalities can vary in their environmental impact, such projects are classified in terms of schedules. Schedule ‘A’ generally includes normal or emergency operational and maintenance activities where the environmental effects are usually minimal, and therefore these projects are pre-approved. A Schedule ‘A+’ activity is also pre-approved by the Ministry of the Environment, and therefore work can proceed

upon public notification of the project. Schedule 'B' projects generally include improvements and minor expansions to existing facilities where there is the potential for some adverse environmental impacts, and therefore the municipality is required to proceed through a screening process including consultation with those who may be affected. Lastly, Schedule 'C' projects generally include the construction of new facilities and major expansions to existing facilities, and these projects proceed through a five phased environmental assessment planning process.

Part B.1.1 (4) for Municipal Road Projects refers to the identification of cultural environment that includes built heritage resources and cultural heritage landscapes. The Ministry of Tourism, Culture and Sport (MTCS) is responsible for the administration of the *Ontario Heritage Act* (OHA), which is responsible for determining policies, priorities and programs for the conservation, protection and preservation of Ontario's heritage which includes cultural heritage landscapes and built heritage.

As well, Section B.1.1 (4) states significant cultural heritage features should be avoided, where possible. Where they cannot be avoided, then effects should be minimized where possible, and every effort made to mitigate adverse impacts, in accordance with provincial and municipal policies and procedures. Cultural heritage features should be identified early in the process in order to determine significant features and potential impacts.

Section C.1.1 (4) defines built heritage resources and cultural heritage landscapes as follows.

Built heritage resources means one or more significant buildings, structures, monuments, installations or remains associated with architectural, cultural, social, political, economic military history and identified as being important to a community. These resources may be identified through designation or heritage conservation easements under the OHA, or listed by local, provincial or federal jurisdictions.

Cultural heritage landscapes means a defined geographical area of heritage significance, which has been modified by human activities and is valued by a community. It involves grouping(s) of individual heritage features such as structures, spaces, archaeological sites, and natural elements, which together form a significant type of heritage form, distinctive from that of its constituent elements of parts. Examples may include, but are not limited to, neighbourhoods, cemeteries, trailways, and industrial complexes of cultural heritage value.

2.2 Ontario Heritage Act (OHA)

The OHA provides the framework for provincial and municipal responsibilities and powers in the conservation of cultural heritage resources. Furthermore, it gives the MTCS, the responsibility for the conservation, protection and preservation of Ontario's cultural heritage resources. Section 2 of the OHA charges the Minister of MTCS with the responsibility to,

“...determine policies, priorities and programs for the conservation, protection and preservation of the heritage of Ontario.”

MTCS describes heritage buildings and structures, cultural heritage landscapes and archaeological resources as cultural heritage resources. Since cultural heritage resources may be impacted adversely by both public and private land development, it is incumbent upon planning and approval authorities to consider heritage resources when making planning decisions. Heritage attributes, in relation to a property, are defined in the OHA as the attributes of the property that cause it to have cultural heritage value or interest.

Individual properties may be designated of cultural heritage value under Part IV of the OHA by a municipality or the MTCS. In addition, municipalities may designate Heritage Conservation Districts under Part V of the legislation. As laid out in subsections 27 (1) and 39.2 (1) of the OHA, the municipal clerk is required to keep a current register of properties of cultural heritage value or interest located in the municipality. The municipal register must include all properties designated under Parts IV and V of the OHA by the municipality or under Part IV by the Minister. Designation of heritage resources publicly recognizes and promotes awareness of heritage properties, provides a process for ensuring that changes to a heritage property are appropriately managed and that these changes respect the property's heritage value. This includes protection from demolition.

The OHA subsection 27(2) also allows a property that is not designated, but considered to be of cultural heritage interest or value by the municipal council, to be placed on the register. This is commonly referred to as “listing”. In many cases, listed (non-designated properties) are candidates for protection under Section 29 of the OHA. Although listing of non-designated properties does not offer any specific protection under the OHA, Section 2 of the Provincial Policy Statement of the Planning Act acknowledges listed properties.

2.2.1 Standards and Guidelines for Conservation of Provincial Heritage Properties

Provincial heritage properties are not subject to designation by municipalities or the Minister. Part III.1 of the OHA enables the Minister of Tourism, Culture and Sport (MTCS), in consultation with the ministries and public bodies affected, to prepare standards and guidelines that set out the criteria and process for identifying provincial heritage properties and to set standards for their protection, maintenance, use, and

disposal. In the development of the *Standards and Guidelines for Conservation of Provincial Heritage Properties* (2010), MTCS drew from existing standards, policies, and best practices currently in use by the Government of Ontario, the federal government, and leading international jurisdictions, and consulted with affected ministries, public bodies and the Ontario Heritage Trust.

The *Standards and Guidelines* apply to properties owned or controlled by the Government of Ontario that have cultural heritage value or interest (i.e., provincial heritage properties). They are issued under the authority of Section 25.2 of the OHA and came into effect on July 1, 2010. They are mandatory for ministries and prescribed public bodies and have the authority of a Management Board of Cabinet directive. All Ontario government ministries and prescribed public bodies must comply with the *Standards and Guidelines* in the management of properties in their ownership or under their control.

The *Standards and Guidelines* define **provincial heritage property** as,

“...real property, including buildings and structures on the property, that has cultural heritage value or interest and that is owned by the Crown in right of Ontario or by a prescribed public body; or that is occupied by a ministry or a prescribed public body if the terms of the occupancy agreement are such that the ministry or public body is entitled to make the alterations to the property that may be required under these heritage standards and guidelines.”

Section B: Identification and Evaluation, subsection B.2, *Standards and Guidelines* states an evaluation process to identify provincial heritage properties will consist of a description of the property, historical information, a determination of the cultural heritage value or interest, including potential provincial significance, based on the advice of qualified persons and with appropriate community input, a report outlining the historical research and evaluation process, and a Statement of Cultural Heritage Value with a description of its heritage attributes. This cultural heritage evaluation report shall be submitted to the MTCS for review and approval.

The *Standards and Guidelines* state ministries and prescribed public bodies shall apply the “Criteria for Determining Cultural Heritage Value of Interest” set out in the Ontario Regulation 9/06 under the OHA to determine the cultural heritage value or interest of a property. If the property meets the criteria in Ontario Regulation 9/06, it is a provincial heritage property. If deemed to be a provincial heritage property the “Criteria for Determining Cultural Heritage Value of Provincial Significance” set out in Ontario Regulation 10/06 to determine whether or not a property is of provincial significance are to be applied. If the property meets the criteria in Ontario Regulation 10/06, it is a provincial heritage property of provincial significance.

The *Standards and Guidelines* define **provincial heritage property of provincial significance** as,

“...provincial heritage property that has been evaluated using the criteria found in Ontario Heritage Act O. Reg. 10/06 and has been found to have cultural heritage value or interest of provincial significance.”

The *Standards and Guidelines* also provide advice on the conservation of provincial heritage properties and provincial heritage properties of provincial importance.

2.3 Ministry of Tourism, Culture and Sport (MTCS)

The MTCS is responsible for the administration of the OHA and is responsible for determining policies, priorities and programs for the conservation, protection and preservation of Ontario’s heritage, which includes cultural heritage landscapes, built heritage and archaeological resources.

MTCS guidelines assist in the assessment of cultural heritage resources as part of an environmental assessment. They are, *Guideline for Preparing the Cultural Heritage Resource Component of Environmental Assessments* (October 1992), and *Guidelines on the Man-Made Heritage Component of Environmental Assessments* (1980). The *Guidelines on the Man-Made Heritage Component of Environmental Assessments* state:

“When speaking of man-made heritage we are concerned with works of man and the effects of his activities in the environment rather than with moveable human artifacts or those environments that are natural and completely undisturbed by man.”

The guidelines state one may distinguish broadly between two basic ways of visually experiencing cultural heritage resources in the environment, that is, as cultural heritage landscapes and as built heritage. Cultural heritage landscapes are a geographical area perceived as a collection of individual person-made built heritage resources set into a whole such as historical settlements, farm complexes, waterscapes, roadscares, railways, etc. They emphasize the interrelationship of people and the natural environment and convey information about the processes and activities that have shaped a community. Cultural heritage landscapes may be organically evolved landscapes as opposed to designed landscapes. Some are ‘continuing landscapes’, which maintain the historic use and continue to evolve, while others are ‘relict landscapes’ where the evolutionary process has come to an end but important landscape or built heritage resources from its historic use are still visible. Built heritage comprises individual, person-made or modified, parts of a cultural heritage landscape such as buildings or structures of various types including, but not limited to, residences, commercial, religious, institutional, industrial or agricultural buildings, bridges, etc.

The guidelines also describe the attributes necessary for the identification and evaluation of any discrete aggregation of person-made features or cultural heritage landscapes and built heritage resources.

3.0 ASSESSMENT METHODOLOGY

3.1 Introduction

AECOM retained Unterman McPhail Associates, Heritage Management Resource Consultants, to undertake a Cultural Heritage Assessment Report (CHAR) for cultural heritage landscapes and built heritage resources as part of the Class Environmental Assessment (EA) Study and Preliminary Design for Creditview Road from Bancroft Drive to Old Creditview Road in the City of Mississauga. The City of Mississauga has initiated a Class EA Study to investigate the need for additional capacity and traffic management improvements along this stretch of Creditview Road. The study is being undertaken in accordance with the planning and design process for Schedule 'C' projects as set out in the Municipal Engineers' Association *Municipal Class Environmental Assessment* (October 2000, amended in 2007 and 2011).

For the purposes of this built heritage resource and cultural heritage landscape assessment, Unterman McPhail Associates undertook the following tasks:

- the identification of major historical themes and activities of the study area in the former Township of Toronto, now within the boundaries of the City of Mississauga through a review of both primary and secondary sources as well as topographic and historical mapping;
- a survey of lands within and adjacent to the Creditview Road corridor between Bancroft Drive to Old Creditview Road in the City of Mississauga;
- the identification of cultural heritage landscapes and built heritage resources within the study area through the analysis of major historical themes and activities, historic mapping and site review activities;
- the identification of sensitivities for change to cultural heritage landscapes and built heritage resources through the review of the historical information, the results of the survey and the proposed changes to the road network; and,
- the development of mitigation recommendations.

3.2 Public Consultation and Recognition

Consultation with the City of Mississauga confirms the following heritage recognition of resources within the project study corridor.

- 1506 Estes Crescent (formerly 6125 Creditview Road) is municipally designated under Part IV of the OHA;
- 1200-1220 Old Derry Road is municipally designated under Part IV of the OHA;
- 6545 Creditview Road is listed on the *City of Mississauga Heritage Register*; and,
- Creditview Road, the Credit River Corridor, Creditview Road and 1200-1220 Old Derry Road are included in the *City of Mississauga Cultural Landscape*

Heritage Inventory and are referenced in the *City of Mississauga Heritage Register*.

4.0 HISTORICAL SUMMARY

4.1 Township of Toronto, County of Peel

Toronto Township was formed on August 2, 1805, after government officials from York purchased land extending from Burlington Bay to the Etobicoke Creek from the Mississaugas. This area became known as the Old Survey. Samuel Wilmot completed the survey of the southern half, or the Old Survey, of Toronto Township in 1806, and the area was opened up for Euro-Canadian settlement, principally United Empire Loyalists, who had emigrated from the United States after the American Revolutionary War and received land grants from the British Government for their service and loyalty. The northern area or New Survey of Toronto Township was acquired by the British Government in 1818 and surveyed in 1819. Administratively, Toronto Township was located in the Home District in the early 19th century.

The principal transportation roads in Toronto Township were constructed in the first part of the 19th century. Dundas Street was opened in the early 19th century as an important military route between York (Toronto) and London. Lakeshore Road, also opened in the early 19th century, and it was a main transportation route along the northern shore of Lake Ontario that provided a link between York and settlements to its west. Centre Road (Hurontario Street), which divided the township into east and west parts, was opened soon after the township survey. Concessions were numbered East of Hurontario Street (EHS) and West of Hurontario Street (WHS). As settlement progressed, other township roads were opened in the first half of the 19th century including Eglinton Avenue, known as the Base Line because it was the boundary line between the old and new surveys.

Smith's Canadian Gazetteer (1846) describes Toronto Township as,

*"...one of the best settled townships in the Home District: it contains a large portion of very excellent land, and a number of well cultivated farms. For from two to three miles from the lake the land is light and sandy, and the timber principally pine; afterwards, it becomes rolling, and the timber the best kinds of hardwood. The Rivers, Credit and Etobicoke both run through the township. The village of Cooksville is situated in the township on the Dundas Street; and the villages of Springfield, Streetsville, Churchville, and Port Credit, on the River Credit—the first is situated on Dundas Street, and the last at the mouth of the River Credit. There are four grist and twenty-one saw mills in the township. Population in 1842: 5,377."*¹

¹ Wm. H. Smith, *Smith's Canadian Gazetteer* (Toronto: H. & W. Rowsell, 1846) 192-193.

Tremaine's Map of the County of Peel (1859) indicates the township had developed into an agricultural landscape from the shoreline of Lake Ontario north to Eglinton Road in the south part of Toronto Township, and from Eglinton Avenue north to the northern township boundary (**Appendix**). With the advancement of settlement, hamlets and villages emerged, often established along the waterways, notably the Credit River and the Etobicoke Creek as well as the roadways leading to York. Tremaine's Map shows the Old Survey communities included Port Credit, Springfield (Erin), Cooksville, Sydenham (Dixie), Summerville and Sandhill (Burnhamthorpe). In the New Survey, they included Streetsville, Barbertown, Meadowvale, Derry West, Malton, Meadowvale and Churchville.

By the 1870s, most of present-day Mississauga comprised agricultural land outside of the villages, including fruit growing orchards. The north and south maps of Toronto Township contained in the *Illustrated Historical Atlas of the County of Peel* (1877) show a well-developed agricultural landscape with numerous farmsteads, a local road system, hamlets and villages (**Appendix**). The *Illustrated Historical Atlas* describes Toronto Township as,

*"Where were dense wildernesses and howling of the wild beasts are now beautifully cultivated farms and almost palatial residences. Toronto Township contains a large portion of arable land, the greater portion of which is rolling. The soil varies in quality, some portions of it being sandy loam, while others are stiff loam and clay... The River Credit runs through the western portion of the Township, and has proved a great source of wealth to its inhabitants, as it is not only a good watering stream, but there are endless mill privileges the whole length of the river, which has been largely utilized in this Township... The principal and only incorporated village in the Township of Toronto is Streetsville."*²

Railway development in the township began in the 1850s with the construction of the Hamilton Toronto Railway along the lakefront north of the road along the lakeshore. This railway became part of Great Western Railway before the Grand Trunk Railway acquired it. The Credit Valley Railway (CVR) was incorporated in February 1871 to construct lines from Toronto north to Orangeville and west to Milton, Woodstock, Ingersoll and St. Thomas via Streetsville. A branch from the Forks of the Credit ran to Fergus and Elora. Surveys were undertaken in 1873 and construction began in 1874; the railway line opened as far as Milton in 1877, Galt in 1879, and St. Thomas in 1881. To the north, tracks reached Brampton by 1878 and the challenging Credit Forks trestle was completed in 1879. The line was opened to both Orangeville and Elora in December 1879. An official opening featuring a train running from Parkdale to Milton was held on September 19, 1879. The CVR was amalgamated into the Ottawa & Quebec Railway (O&Q) in November 1883. In turn, Canadian Pacific Railways (CP) took over O&Q through a perpetual lease in January 1884.

² *Illustrated Historical Atlas of the County of Peel* (Toronto: Walker & Miles, 1877) 60.

Early 20th century topographical maps show the steady progress of development in Toronto Township northward from the lakefront (*Appendix*). Lakeshore Road and Dundas Street remained key routes in the township eventually becoming part of the provincial highway system. In 1917, Lakeshore Road became Provincial Highway 2 and continued to serve as a main transportation route between Toronto and Hamilton until the opening of the Queen Elizabeth Way (QEW) in 1939. Dundas Street was designated Provincial Highway 5 in 1920 and Hurontario Street became Provincial Highway 10.

The Guelph Radial Line of the Toronto Suburban Railway (TSR), an electric interurban railway was constructed through Toronto Township in the first part of the 20th century. The 49-mile (80 km) line between West Toronto and Guelph was surveyed in 1911, with construction commencing in 1912; the route opened between West Toronto and Guelph on April 14, 1917. The Guelph Radial Line generally paralleled Dundas Street to Cooksville before veering to the northwest through Streetsville and Meadowvale. It carried on in a northwest direction to Georgetown before heading west to Acton and Guelph. The line remained in operation until 1931.

Post Second World War development accelerated in the 1950s, particularly in the vicinity of the QEW and then moved northward. The 1960 topographic map still shows a rural landscape with farm complexes north of Burnhamthorpe Road East. Large-scale residential developments in Mississauga, such as Erin Mills and Meadowvale, were developed in the 1960s and 70s. By the late 1970s, suburban development had engulfed all of the land from the lakefront to Burnhamthorpe Road East. Highway 401 was introduced into the landscape in the late 1950s while the Mississauga section of Highway 403 was opened in 1982. Highway 5 (Dundas Street) in Mississauga and Highway 2 (Lakeshore Road) become municipal roads in 1997 and 1998, respectively.

Grading began on a 13.75-mile (22-km) section of Highway 401 between Highway 10 and Highway 25 in 1957. Construction continued the following year with the awarding of paving contracts. The Department of Highways reported Toronto Township Bridge 13 (Credit River), Lot 9, Concession 3 WHS and Toronto Township Bridge 14 (Creditview Road Underpass), Lot 9, Concessions 3 & 4 WHS were completed in 1958.³ The section of Highway 401 between Highway 10 and Highway 25 was opened officially on November 26, 1959.⁴

In the 1960s, Markborough Properties Limited acquired significant land holdings in proximity to the Highway 401 corridor. On April 25, 1969, the company announced its plans for the development of new, urban communities on 3,000 acres (1,200 hectares) located in the then Town of Mississauga. As envisioned in the master plan prepared by Project Planning Associates Limited of Toronto, the project would comprise fully integrated communities that would provide employment, residential, educational,

³ Ontario, *Department of Highways Ontario Annual Report 1958-59*, 244 and 248.

⁴ Ontario Ministry of Transportation and Communications, '401' *The Macdonald-Cartier Freeway*, 8.

recreational and commercial opportunities. A central business park would be supported by three residential subdivisions, identified as Meadowvale South, Meadowvale West and Meadowvale North. Meadowvale South between Creditview Road and Mississauga Road to the south of Highway 401 was constructed in the early 1970s and was the first of the three subdivisions to be completed. It contained low-scale industrial buildings set in landscaped grounds beside Highway 401 and along CP rail corridor with housing to the south. As part of the overall development, the municipality undertook improvements to Creditview Road.

With the exception of Port Credit and Streetsville, the Township of Toronto and its settlements were amalgamated in 1968 to form the Town of Mississauga. In 1974, Port Credit and Streetsville were annexed by Mississauga when it became a city. The City of Mississauga experienced its greatest growth in the 1980s and 1990s. The population increased from 174,982 residents in 1971, to 321,289 in 1981, and 479,624 in 1991.⁵ In 2006, 703,292 people lived in Mississauga.⁶

Streetsville

Timothy Street developed several mill sites along the banks of the Credit River, the first by 1821, and a settlement, which became known as Streetsville, grew up around them on Lots 2 to 6, Concession 4 WHS, New Survey. As the community grew, the land around it was cleared and cultivated for agriculture; the primary crop produced was wheat. To meet the need for a local gristmill, John Beaty selected a point on the Credit River on the southeast edge of Streetsville, and built a mill to serve the area farmers by the mid-1830s.

By the mid-1840s, Streetsville had progressed beyond a pioneer settlement to a prosperous and regional service centre with many businesses. *Smith's Canadian Gazetteer* describes the village as being, "*prettily situated on the River Credit*," with good farming land around it.⁷ The village had a population of 550 people, and a daily stagecoach service to Toronto had just opened.⁸ Smith also recorded there were two gristmills, three saw mills and other factories and manufacturing businesses located in Streetsville. In 1849, it was noted that Street, Baty (Beaty) and Rowe owned the three gristmills, and that there was an extensive woollen factory owned by Barber, as well as three saw mills and a large tannery in Streetsville. The population was noted as 1,200 people.⁹

⁵ Region of Peel, Population 1971 to 2011 from Statistics Canada, Historical Population in Peel with Access: --<www.peelregion.ca/planning/pdc/data/census/population-1971-2006.htm> (July 2015).

⁶ Ibid.

⁷ Wm. H. Smith, *Smith's Canadian Gazetteer* (Toronto: H. & W. Rowsell, 1846) 177.

⁸ Ibid.

⁹ A Tour of the West, Letters by Viator, Letter No. IV, *The British Whig*, Kingston, September 6, 1847; transcript by the Esquesing Historical Society. Access: --<<http://www.esquesinghistoricalsociety.ca/NewsPaperArticleFiles/bw18474.HTML>> (July 2015).

Streetsville's economic growth stalled somewhat in the mid 19th century when the railways bypassed the village and Brampton was named the county seat. The community was incorporated as a village in 1857. The Credit Valley Mills continued to operate as a major gristmill in the area. Gooderham & Worts Mills moved to Toronto after a fire in 1877, and the Barbers changed their mill production from flour to woollen goods. The CVR arrived in Streetsville in 1879, providing a much needed line to Toronto. Streetsville was the junction of the CVR lines to St. Thomas and Orangeville and was the site of the CVR wye that permitted the turning of complete trains as well as movements to St. Thomas or Orangeville.

Streetsville was incorporated as a town in 1962 and amalgamated with the City of Mississauga in 1974. Streetsville contains the largest concentration of historic structures within the municipality.

Meadowvale

Settlement in the Meadowvale area of Toronto Township began c1820 with the arrival of some 30 Irish families from the United States following the War of 1812. Lots of 200 acres each were awarded to the new immigrants. Two of the first lots taken up, namely Lots 10 and 11 in Concession 3 WHS, came to form the nucleus of the village of Meadowvale. The lack of roads, stores or mills in the northern part of Toronto Township hampered settlement and the early years were characterized by the frequent change of ownership of lands in the Meadowvale area. The establishment of mills on the Credit River included a sawmill by John Crawford in 1831, and a sawmill and a carding mill by John Simpson in 1836, which initiated a period of growth and prosperity. Francis Silverthorn took over Crawford's mill in 1844 and enlarged the complex to incorporate a gristmill. By the 1850s, Meadowvale had two hotels, a wagon shop, a foundry and a school. Mail was picked up in Derry West to the east of the village, until the opening of the Meadowvale post office in 1857.

Gooderham & Worts acquired an interest in Silverthorn's mill in 1860. The company expanded the mill complex, upgraded the equipment and increased production significantly. Gooderham & Worts established a large store and barrel and cooper factory in the village as well as purchasing land in the area. A Wesleyan Methodist church was constructed in Meadowvale in the 1860s. The *Illustrated Historical Atlas of the County of Peel* (1877) depicts the village extending between Second Line West to the Credit River and to the north and south of Derry Road. The route of the CVR passed to the west of the community in 1879. Despite the proximity of the railway, the milling business began to decline in the 1880s. Gooderham & Worts sold its Meadowvale interests and some of the mills closed.

By the early 20th century only one mill remained in operation. Subsequently, the remains of the large gristmill were demolished in 1957. Despite the loss of mill buildings, Meadowvale retained a large concentration of 19th century buildings. As a result of its

distinct heritage character, the village was designated a Heritage Conservation District under the OHA in 1980, the first community in Ontario to be so recognized.

4.2 Creditview Road

Creditview Road was laid out as part of the New Survey of Toronto Township in 1819 as the road allowance between Concessions 3 and 4 WHS and was known initially as Third Line West. It ran through Toronto Township from the Base Line (Eglinton Avenue) north to Churchville on the border with Chinguacousy Township. The road allowance was opened as the neighbouring lands were settled for agricultural purposes. Creditview Road formed part of the local road network and provided connections for residents to Streetsville to the south and Meadowvale to the north.

Early settlers along Creditview Road between Britannia Road and Derry Road included the Brown, Steen, Pearson and McCracken families in Concession 3 WHS to the east and Folwell, Steen, Laidlaw and Kearney families in Concession 4 WHS to the west. Christopher Rowe established Alpha Mills on the Credit River on Lot 7, Concession 4 WHS in the mid-1800s. Historical mapping indicates the site operated initially as a saw mill and later as a gristmill. By 1877, Robert Ramsey ran the gristmill while Gooderham & Worts operated the nearby Alpha Knitting Mills.

Creditview Road remained a township road with a gravel surface through the first half of the 20th century. Topographic maps (1909 and 1931) note a wood bridge on Creditview Road over the Credit River (*Appendix*). An aerial photograph (1954) shows an agricultural landscape along Creditview Road to the north of Streetsville just prior to the introduction of Highway 401 across Toronto Township (*Appendix*). Farm complexes are seen fronting on Creditview Road.

Construction of Highway 401 across the northern part of Toronto Township in the latter part of the 1950s resulted in the construction of the Toronto Township Bridge 14 (Creditview Road Underpass) on Lot 9, Concessions 3 and 4 WHS, in 1958 (*Appendix*).¹⁰ The Creditview Road Underpass provided an important link across the highway corridor and maintained important connections between Streetsville and Meadowvale.

As part of the Markborough Properties Limited project to develop its land located close to the Highway 401 corridor into new urban communities, as outlined in the master plan by Planning Project Associates Limited, the municipality undertook improvements to Creditview Road. The roadway was paved and a boulevard and a sidewalk were introduced along the west side. A three-span prestressed concrete girder bridge had been completed over the Credit River in 1967.

¹⁰ Ontario, *Department of Highways Ontario Annual Report 1958-59*, 244 and 248.

5.0 DESCRIPTION OF STUDY AREA

5.1 Introduction

For the purposes of cultural heritage landscape and built heritage resource identification, this section provides a brief description of the existing environment of the study area (i.e., Creditview Road from Bancroft Drive to Old Creditview Road) and the associated principal cultural heritage landscapes and built heritage resources.

5.2 Description of the Existing Environment

The study area is situated in the northwestern part of the City of Mississauga to the northwest of the City of Toronto.

The area lies within the South Slope physiographic region of Southern Ontario. The South Slope comprises a strip of land between the Iroquois plain along Lake Ontario to the south and the Peel Plain to the north, as well as from the Niagara Escarpment in the west to the Trent River in the east. The physical landscape of the region consists mainly of a till plain. The bedrock is buried under a thick layer of drift or till deposited during the Wisconsin glaciation. The surface deposits within the region are largely of glacial origin and contain components of the underlying bedrock. The till is an accumulation of ground rock of various sizes. The till plain has a gently undulating topography and the soil generally proved well suited for agricultural development.

The Credit River watershed is situated primarily within the Region of Peel boundaries and drains the area. The river originates in Mono Township and flows approximately 90 km in a generally southeasterly direction from the Orangeville area and empties into Lake Ontario at Port Credit. The Lower Credit River through Mississauga is largely urbanized. Very few wetlands or forests remain and several small streams and creeks have been channelized or buried.

Historically, the area lay within the Township of Toronto in the County of Peel. Settlers arriving in the early 1800s commenced the clearing of the original forest that covered the region. The fertile soils were cleared quickly and by the mid-1800s an established pattern of agricultural fields, hedgerows, tree lines, woodlots and rural gravel roads were well established. The Credit River proved to be suited to waterpower development and several saw mills and gristmills constructed along the waterway were a benefit to the agricultural settlers. Local communities, such as Streetsville and Meadowvale, along with schools and churches grew up to support the largely rural population. The proximity to the growing City of Toronto provided a readily accessible market for local products including milk, fruits, vegetables and poultry.

Topographic maps illustrate the urbanization of the Greater Toronto Area through the 20th century. Until the Second World War, most of Toronto Township was in agricultural

use. As the century advanced, development progressed westward from Toronto into Mississauga. Growth in Mississauga from the 1950s to the 1980s moved steadily northwards from the lakeshore with a resulting loss of agricultural land. Lands to the west of Creditview Road and south of Highway 401 were redeveloped in the early 1970s as part of the new community named Meadowvale South.

Creditview Road is considered to run in a north to south direction between Bancroft Drive and Old Creditview Road, a distance of approximately 2.2 km. The two-lane paved roadway with a posted speed limit of 60 km/hr., follows a generally straight alignment through the study area. A utility line parallels the west side of the Creditview Road. Bridge structures carry the roadway over the Credit River and Highway 401, at the south and north ends of the study area, respectively.

Natural and residential areas characterize the surrounding land uses. Woodlands and grasslands bordering the east side of Creditview Road to the south of Highway 401 provide a distinctive character to the roadway. The Credit River flows in a generally southerly direction through a broad, alluvial terrace to the south of Highway 401 and to the east of Creditview Road. Towards the south end of the study area, the river curves to the west to pass under the roadway before resuming its southerly course. Scenic views are afforded from the road bridge to the river valley. The waterway is a posted wild trout/salmon river. Credit Meadows Park is located to the northeast of the bridge over the river while Pine Cliff Park is situated a short distance upstream to the northwest.

Most of the farmland beside Creditview Road was redeveloped for light industrial and housing purposes from 1970 on. Some reminders of the 19th century agricultural history survive in the area, notably along the east side of the roadway. They include,

- the former farmhouse at 1506 Estes Crescent (formerly 6125 Creditview Road);
- the farm complex at 6545 Creditview Road; and
- the farm complex at 1200-1220 Old Derry Road.

The area to the west of Creditview Road forms part of the community of Meadowvale South. Meadowvale South was conceived as a fully integrated community that would provide employment opportunities supported by a range of housing options, shopping, schools and recreational activities. The master plan undertaken by Project Planning Associates Limited, located industrial uses along the Highway 401 corridor with housing to the south along Creditview Road. The residential areas were oriented onto an internal local road network. The development was set back from Creditview Road to allow for a buffer between the housing and the roadway. As a result, back yards enclosed with fencing generally border Creditview Road. Sidewalks and a landscaped boulevard were installed along the west side of Creditview Road between Argentia Road and Kenninghall Boulevard. Construction on the first phase of the development was undertaken in early 1970s.

5.3 Description of Identified Cultural Heritage Resources

The survey of the study corridor was completed in December 2013. Ten (10) cultural heritage resources were identified during the survey of the study corridor. A description of the identified cultural heritage resources, including cultural heritage landscapes (CHL) and built heritage resources (BHR), are listed in the following ***Table 1: Identified Cultural Heritage Landscapes (CHL) and Built Heritage Resources (BHR) Located Within or Adjacent to the Creditview Road Study Area*** and mapped in ***Figure 2***. Table 1 includes a site number, resource category, resource type, location, description and digital photograph. The following explanatory notes provide background material on the information contained in Table 1.

- Sites are numbered generally from south to north.
- Resources are identified by category: Cultural Heritage Landscape (CHL) or Built Heritage Resource (BHR) and by type: roadscape, residential subdivision, cemetery, bridge, residence, church, etc.
- The municipal address, when applicable, locates the identified cultural heritage resources. The identified cultural heritage resources are mapped on Figure 2.
- A brief description of the cultural heritage resource, e.g., notable landscape features, structures on the property, construction period(s), building materials, roof shape, number of storeys, important architectural details, architectural style or influence and alterations/additions, is based upon information gained from the public roadway.
- Known heritage value as identified through listings in a local inventory, designation under the OHA, recognition through a commemorative plaque, and inclusion in the Ontario Heritage Bridge List is provided.
- Digital photographs with caption are supplied for each resource.



Figure 2. Identified cultural heritage landscapes (CHL) and built heritage resources (BHR) located within or adjacent to the Creditview Road Study Area [GoogleMaps 2013 as adapted].

TABLE 1: IDENTIFIED CULTURAL HERITAGE LANDSCAPES (CHL) AND BUILT HERITAGE RESOURCES (BHR) LOCATED WITHIN OR ADJACENT TO THE CREDITVIEW ROAD STUDY AREA



Site #	Resource Category	Resource Type	Location	Description	Known Heritage Recognition	Photographs/Digital Image
1.	CHL	Roadscape	Lots 6-9, Concessions 3 & 4, geographic Township of Toronto City of Mississauga	Creditview Road Creditview Road is an early settlement road in Mississauga and relates to the "New Survey" of Toronto Township as the road allowance between Concessions 3 & 4. It ran from the Base Line (Eglinton Avenue) to Churchville on the border with Chinguacousy Township. The route formed part of the local road network and retained a gravel surface into the second half of the 20 th century. As neighbouring lands were redeveloped for residential purposes, the road was improved and boulevard, sidewalks and lighting were introduced. The two-lane roadway has a posted speed limit of 60 km/hr. The road offers scenic views of the river valley, wooded areas and the agricultural landscape.	Creditview Road is listed in the City of Mississauga <i>Cultural Landscape Inventory</i> (F-TC-3) as a cultural heritage feature and noted in the City's Heritage Register.	 <p>North along Creditview Road from Falconer Road with the drive to 6545 on the right.</p>  <p>Northwest to tree line on Creditview Road to the south of Falconer Road.</p>

TABLE 1: IDENTIFIED CULTURAL HERITAGE LANDSCAPES (CHL) AND BUILT HERITAGE RESOURCES (BHR) LOCATED WITHIN OR ADJACENT TO THE CREDITVIEW ROAD STUDY AREA


Site #	Resource Category	Resource Type	Location	Description	Known Heritage Recognition	Photographs/Digital Image
2.	CHL	Waterscape	Lots 6-9, Concessions 3 & 4, geographic Township of Toronto City of Mississauga	Credit River Corridor The Credit River originates in Mono Township and flows approximately 90 km in a generally southwesterly direction to Lake Ontario at Port Credit. The river was suited to waterpower development and several saw and gristmills constructed in Meadowvale and Streetsville became a benefit to the agricultural settlers in the 1800s. The waterway flowing from east to west across Creditview Road to the north of Bancroft Drive forms a visual feature from the roadway.	The Credit River Corridor is listed in the City of Mississauga <i>Cultural Landscape Inventory</i> (L-NA-2) and included in the City's Heritage Register.	 <p>Northeast (upstream) along the Credit River from the road bridge at the south end of the study area.</p>

TABLE 1: IDENTIFIED CULTURAL HERITAGE LANDSCAPES (CHL) AND BUILT HERITAGE RESOURCES (BHR) LOCATED WITHIN OR ADJACENT TO THE CREDITVIEW ROAD STUDY AREA



Site #	Resource Category	Resource Type	Location	Description	Known Heritage Recognition	Photographs/Digital Image
3.	BHR	Residential: Farmhouse (former)	1506 Estes Crescent (formerly 6125 Creditview Road) Lot 6, Concession 3 WHS, geographic Township of Toronto City of Mississauga	Brown-Milsom-Fitzwood House Samuel Brown, an early settler in the north part of Toronto Township, built the one and-a-half storey brick residence with attached brick driveway in the 1860s. The property originally fronted on the Creditview Road and the farm lane remains identifiable on the south side of the Credit River at the bridge. Access was reoriented to Estes Crescent in the 1990s.	The property is designated (Bylaw 2-92) under the <i>Ontario Heritage Act</i> and included in the City of Mississauga <i>Cultural Landscape Inventory</i> (L-AG-5).	 <p>West to the former farmhouse at 1506 Estes Crescent.</p>  <p>The former farm lane is located on the south side of the Credit River.</p>

TABLE 1: IDENTIFIED CULTURAL HERITAGE LANDSCAPES (CHL) AND BUILT HERITAGE RESOURCES (BHR) LOCATED WITHIN OR ADJACENT TO THE CREDITVIEW ROAD STUDY AREA



Site #	Resource Category	Resource Type	Location	Description	Known Heritage Recognition	Photographs/Digital Image
4.	BHR	Transportation: Bridge	Creditview Road, Lot 7, Concessions 3 & 4, geographic Township of Toronto City of Mississauga	Creditview Road Bridge over the Credit River The Creditview Road Bridge is a three-span, prestressed concrete girder structure with cast-in-place concrete abutments and piers that was constructed in 1967. The open-style handrail features horizontal pipe rails and stone faced end posts. It is one of a series of bridges built over the Credit River in this location. Remains of the abutment of an earlier structure exist on the north bank of the Credit River.	Application of the "Criteria for Determining Cultural Heritage Value or Interest" under 'Ont. Reg. 09/06' determined the Creditview Road Bridge is of cultural heritage value [Archaeological Services Inc., <i>Heritage Impact Assessment of the Creditview Road Bridge</i> , January 2013].	 <p>Northwest to the Creditview Road Bridge over the Credit River.</p>  <p>An open-style handrail and sidewalks extend along either side of the two-lane bridge.</p>

TABLE 1: IDENTIFIED CULTURAL HERITAGE LANDSCAPES (CHL) AND BUILT HERITAGE RESOURCES (BHR) LOCATED WITHIN OR ADJACENT TO THE CREDITVIEW ROAD STUDY AREA



Site #	Resource Category	Resource Type	Location	Description	Known Heritage Recognition	Photographs/Digital Image
5.	CHL	Farm Complex	6545 Creditview Road West half of Lot 8, Concession 3 WHS, geographic Township of Toronto City of Mississauga	Pearson-Harris Farm The property is associated with the James Pearson family, early settlers in the area. The land remains in agricultural use. A long lane with bridge over the Credit River leads into the site from Creditview Road. The two storey brick farmhouse with hipped roof dates to c1850 and features elements of the Classic Revival style including symmetrically arranged front elevation rectangular openings, main entry with transom light and half sidelights and six over six window sash. A verandah with decorative woodwork is a notable feature.	The property is listed in the City of Mississauga Heritage Register (Inventory # 119) and considered to be part of the Credit River Corridor and the Creditview Road Scenic Route.	 <p>East along the long drive leading into 6545 Creditview Road.</p>  <p>Front elevation of the farmhouse at 6545 Creditview Road [City of Mississauga].</p>

TABLE 1: IDENTIFIED CULTURAL HERITAGE LANDSCAPES (CHL) AND BUILT HERITAGE RESOURCES (BHR) LOCATED WITHIN OR ADJACENT TO THE CREDITVIEW ROAD STUDY AREA



Site #	Resource Category	Resource Type	Location	Description	Known Heritage Recognition	Photographs/Digital Image
6.	CHL	Residential Subdivision	Lots 6-9, Concession 4 WHS, geographic Township of Toronto City of Mississauga	Meadowvale South A residential subdivision that formed part of the Meadowvale South development was constructed in the early 1970s on former agricultural lands along the west side of Creditview Road. Project Planning Associates Limited undertook the master planning on behalf of Markborough Properties. Meadowvale South comprised a business park supported by a mixture of housing types, plazas, schools and playing fields.	The property is listed in the City of Mississauga Heritage Register because of its location within the Creditview Road Scenic Route corridor.	 <p>Northeast to townhouses at 6730 Falconer Road that back onto Creditview Road.</p>
7.	CHL	Transportation: Roadscape	Lot 9, Concessions 3 & 4, geographic Township of Mississauga City of Mississauga	Highway 401 The section of Highway 401 between Highway 10 and Highway 25 was opened officially on November 26, 1959. The Highway 401 corridor initially comprised two WBL and two EBL separated by a depressed grass median. Grade separations were provided at interchanges and intersecting road. Widening activities have resulted in the paving of the median.	In 1969, the Department of Highways erected a plaque near Ivy Lea in Eastern Ontario to commemorate the completion of the highway the previous year.	 <p>Southwest to the Highway 401 corridor from the Creditview Road Underpass.</p>

TABLE 1: IDENTIFIED CULTURAL HERITAGE LANDSCAPES (CHL) AND BUILT HERITAGE RESOURCES (BHR) LOCATED WITHIN OR ADJACENT TO THE CREDITVIEW ROAD STUDY AREA





Site #	Resource Category	Resource Type	Location	Description	Known Heritage Recognition	Photographs/Digital Image
8.	BHR	Transportation: Bridge	Highway 401 Lot 9, Concessions 3 & 4 WHS, geographic Township of Toronto City of Mississauga	Creditview Road Underpass The Creditview Road Underpass (1958) is a four-span (37'-6", 63'-0 ½", 63'-0 ½", 37'-6"), continuous concrete beam and slab structure featuring five constant depth beams and piers comprising three columns and pier caps. The bridge retains its original handrail system. The structure dates to the construction of Highway 401 between Highway 10 and Highway 25 and carries Creditview Road over Highway 401.	Included in Archaeologix Inc., and Historica Research Ltd., <i>Inventory of Highway 400 & 401 Underpass Bridges</i> , Ministry of Transportation, Central Region, Ontario (November 2004).	 Oblique view along the west side of the Creditview Road Underpass.
9.	BHR	Transportation: Bridge	Highway 401 Lot 9, Concession 3 WHS, geographic Township of Toronto City of Mississauga	Credit River Bridge The Credit River Bridge (1958) is a three-span (66', 91', 66') continuous concrete girder bridge featuring distinctive shallow arches. The structure dates to the construction of Highway 401 between Highway 10 and Highway 25 and carries Highway 401 over the Credit River. The bridge has been widened and the original railing has been replaced.	There is no known heritage recognition for the property.	 Southeast to the Credit River Bridge to the east of Creditview Road on Highway 401.

TABLE 1: IDENTIFIED CULTURAL HERITAGE LANDSCAPES (CHL) AND BUILT HERITAGE RESOURCES (BHR) LOCATED WITHIN OR ADJACENT TO THE CREDITVIEW ROAD STUDY AREA

Site #	Resource Category	Resource Type	Location	Description	Known Heritage Recognition	Photographs/Digital Image
10.	CHL	Farm Complex	1200-1220 Old Derry Road Part Lots 9 & 10, Concession 3 WHS, geographic Township of Toronto City of Mississauga	Simpson-Humphries House, Sandford Farm The property straddles the Credit River between Highway 401 and Old Derry Road. The two storey farmhouse with dichromatic brickwork dates to the 1860s. John Simpson, a prominent mill owner and founder of Meadowvale, constructed the house. Several barns and outbuildings remain on the property.	The property is designated (Bylaw 833-23) under the <i>Ontario Heritage Act</i> and included in the <i>City of Mississauga Cultural Landscape Inventory</i> (L-AG-5).	 <p>Northeast to the agricultural landscape associated with 1200-1220 Old Derry Road.</p>  <p>Front elevation of the farmhouse at 1200-1220 Old Derry Road [City of Mississauga].</p>

6.0 POTENTIAL EFFECTS OF UNDERTAKING ON CULTURAL HERITAGE RESOURCES

6.1 Introduction

This section provides a preliminary assessment of the potential adverse effects to the identified cultural heritage resources as a result of the proposed improvements to Creditview Road from Bancroft Drive and to Old Creditview Road in the City of Mississauga. Creditview Road is identified as a Major Collector Road in the City of Mississauga Official Plan and is an important component of the municipality's transportation network. The Class EA Study and Preliminary Design addresses the need for additional capacity and traffic management improvements along this stretch of Creditview Road. The conservation of cultural heritage resources in planning is considered to be a matter of public interest.

Generally, road improvement projects such as the widening of an existing roadway, the revision to existing intersections and the construction of new bridge structures have the potential to adversely affect cultural heritage landscapes and built heritage resources by displacement and/or disruption during, as well as after construction. Cultural heritage landscapes and/or built heritage resources may experience displacement, or direct impacts (i.e., removal) if they are located within the right-of-way of the undertaking. There may also be potential for disruption, or indirect impacts, to cultural heritage resources by the introduction of physical, visual, audible or atmospheric elements that are not in keeping with their character and/or setting.

The study is being undertaken in accordance with the planning and design process for Schedule 'C' projects as set out in the Municipal Engineers' Association *Municipal Class Environmental Assessment* (October 2000, amended in 2007 and 2011). Five alternatives were developed to address the future needs of the Creditview Road corridor. Analysis and evaluation of the alternatives resulted in selection of a Preliminary Preferred Alternative as well as a Long-Term Solution. It is anticipated the Preliminary Preferred Alternative will support traffic operations to 2031, and possibly longer. Further community consultation will take place prior to the implementation of the Long-Term Solution.

The Preliminary Preferred Alternative includes the following key elements,

- maintaining two lanes from Bancroft Drive to Argentia Road;
- widening to four lanes from Argentia Road to Old Creditview Road;
- maintaining Bancroft Drive and Old Creditview Road as signalized intersections;
- proposing one-lane roundabouts at Kenninghall Boulevard and Falconer Drive;
- proposing a two-lane roundabout at Argentia Road;
- implementing a continuous multi-use trail along the west side of the corridor;
- implementing a continuous sidewalk along the east side of the corridor; and,
- introducing noise attenuation barriers through a portion of the corridor.

The Long-Term Solution includes the following key elements,

- widening to four lanes from Bancroft Drive to Argentia Road; and,
- proposing two-lane roundabouts at Kenninghall Boulevard and Falconer Drive.

The potential direct impacts (displacement) and indirect impacts (disruption) of this project are principally associated with modifications to the existing Creditview Road corridor, a municipally recognized cultural heritage landscape. The discussion of impacts focuses on the potential adverse effects of the Preliminary Preferred Alternative. A review of the proposed Long-Term Solution indicates its impacts will be comparable to the Preliminary Preferred Alternative.

The City of Mississauga completed a separate Class Environmental study for the Creditview Road Bridge over the Credit River in 2013. The proposed widening of the bridge is expected to be undertaken in 2016. Therefore, the Creditview River Bridge over the Credit is considered to be outside the parameters of this study in respect to any further discussion of impacts.

The direct and indirect impacts are discussed in Section 6.2 and Section 6.3, respectively. The Community Session Display Panels (June 2015) prepared by AECOM that include the Creditview Road Class EA, Preliminary Preferred Alternative drawing (June 2015) were used to assess the potential impacts to cultural heritage resources identified within the study area (*Figures 3-11*).

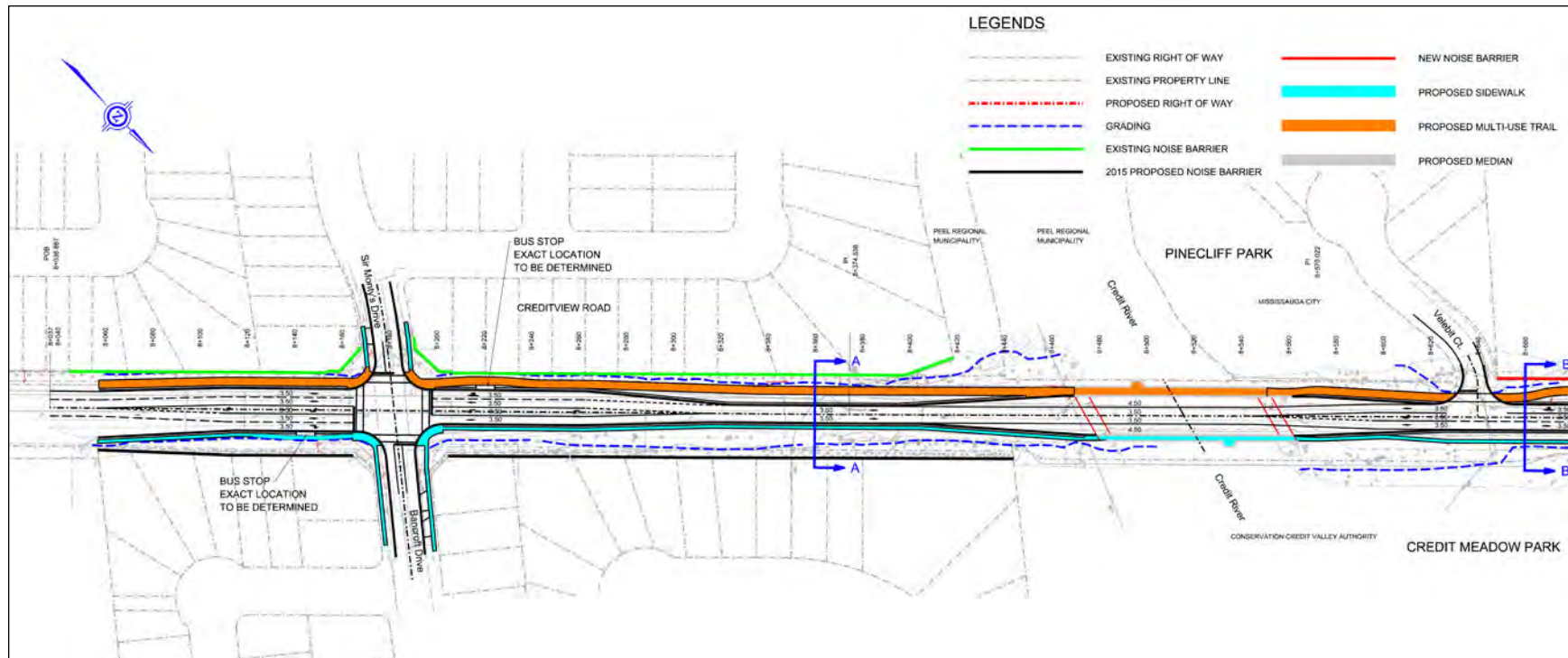
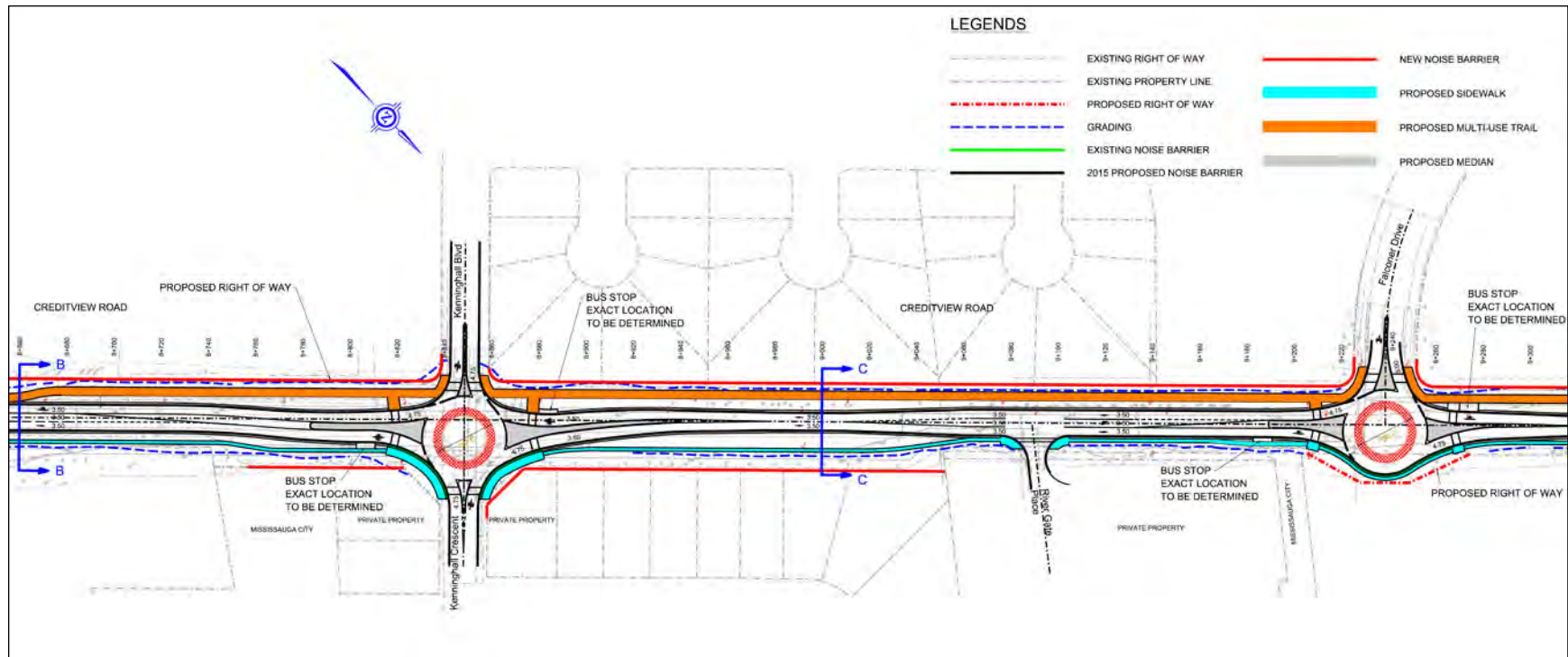


Figure 3. Creditview Road Class EA, Preliminary Preferred Alternative: Plan from Station 8+037 to Station 8+660 [AECOM, 2015].



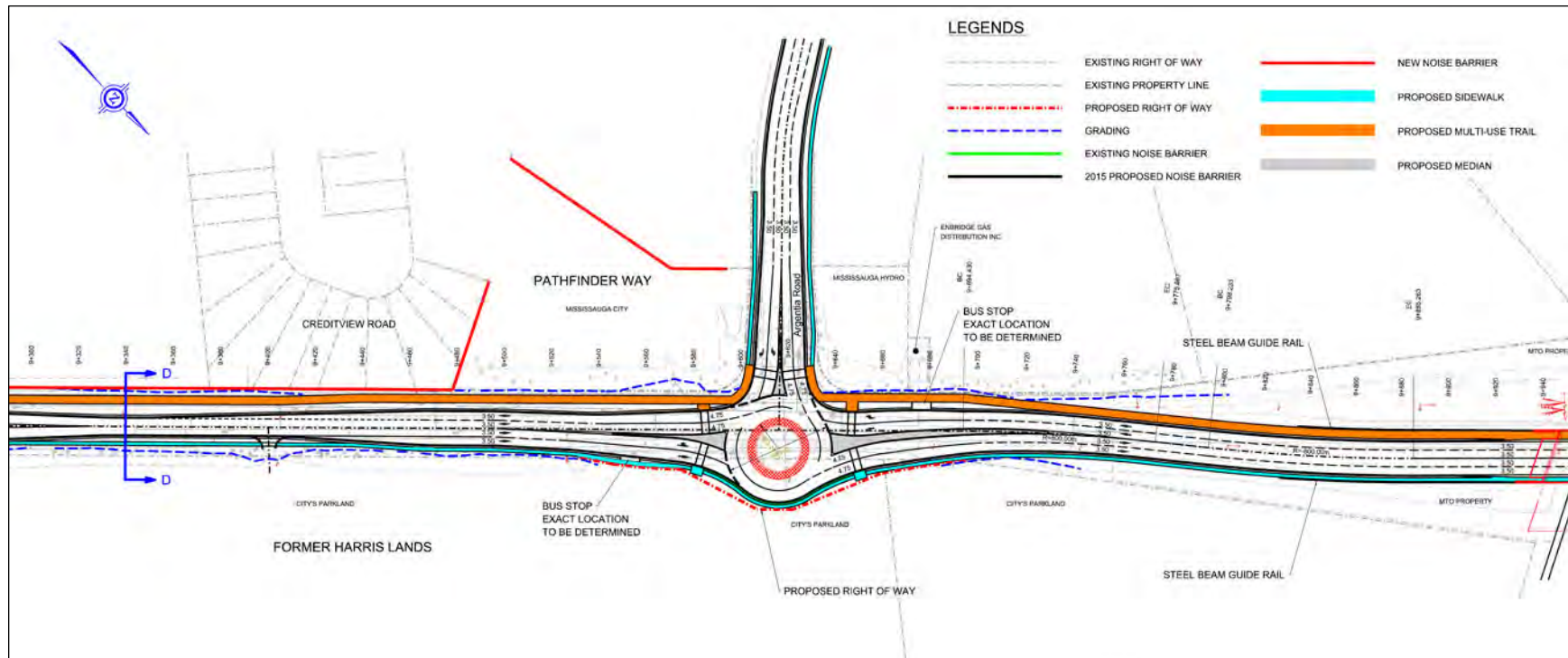


Figure 5. Creditview Road Class EA, Preliminary Preferred Alternative: Plan from Station 9+300 to Station 9+940 [AECOM, 2015].

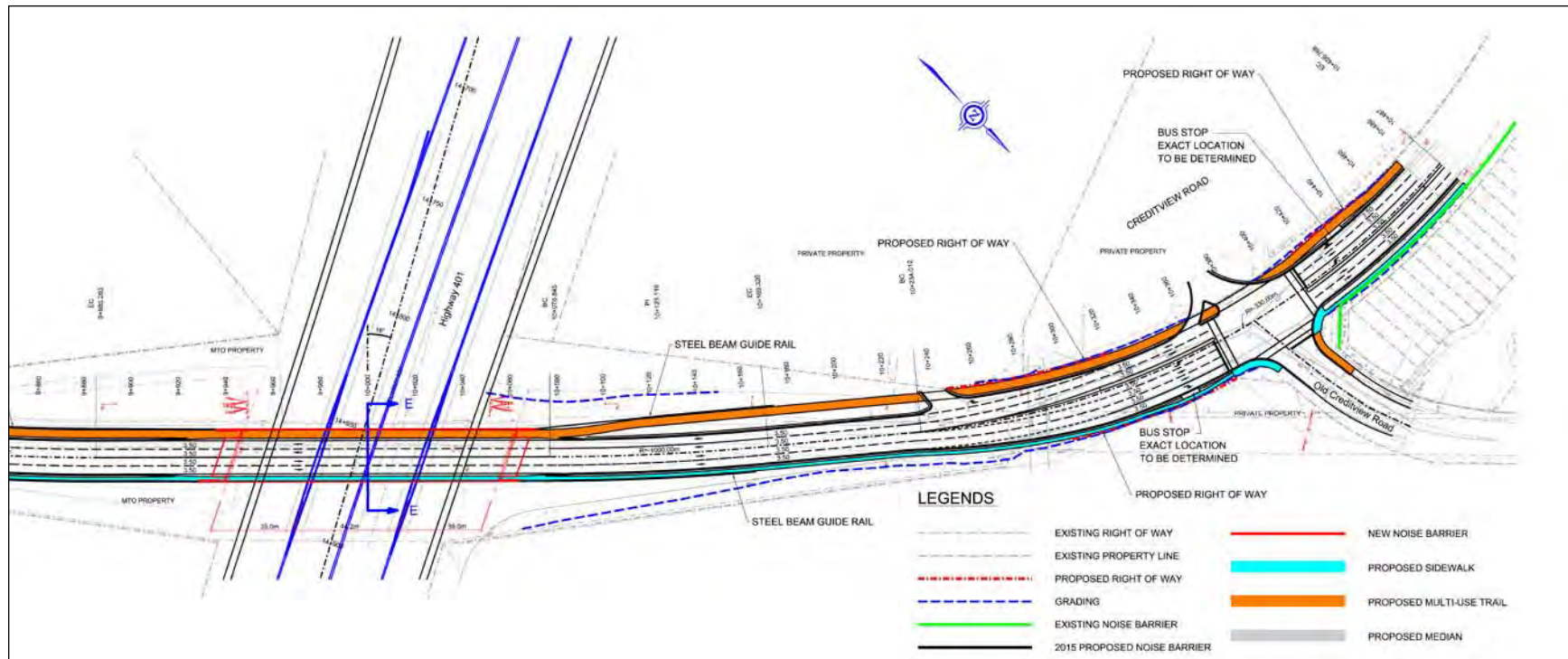


Figure 6. Creditview Road Class EA, Preliminary Preferred Alternative: Plan from Station 9+860 to Station 10+847 [AECOM, 2015].

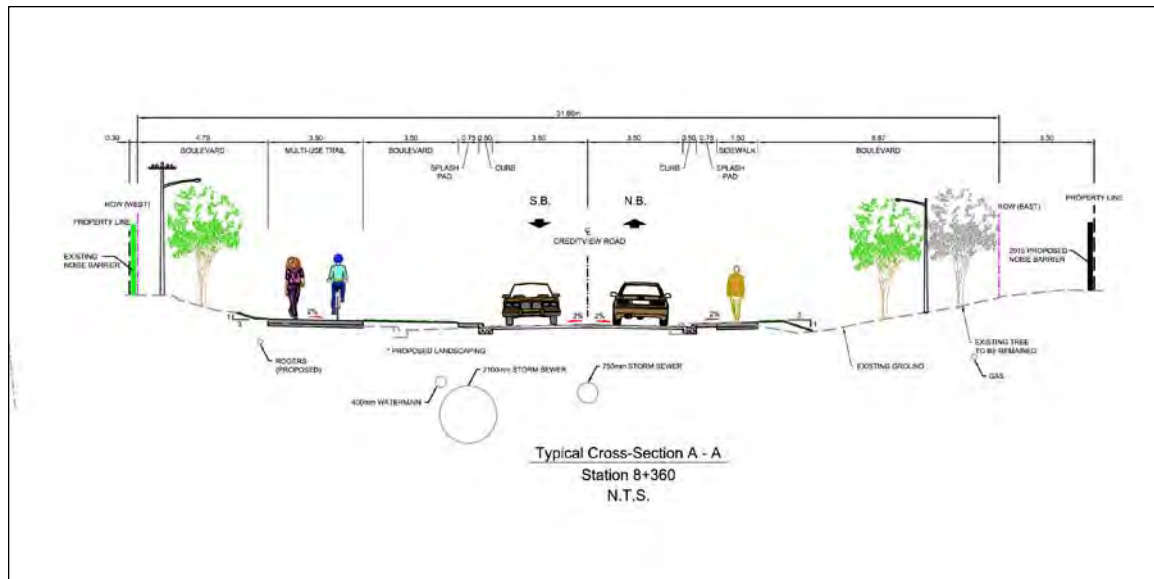


Figure 7. Creditview Road Class EA, Preliminary Preferred Alternative: Typical Cross-Section A-A at Station 8+360 [AECOM, 2015].

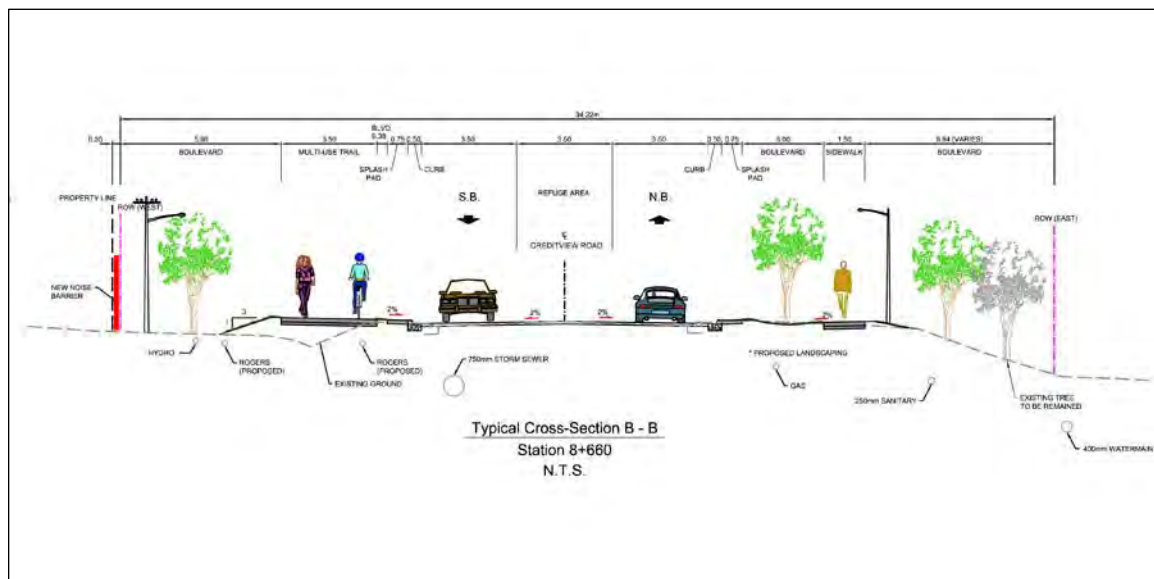


Figure 8. Creditview Road Class EA, Preliminary Preferred Alternative: Typical Cross-Section B-B at Station 8+660 [AECOM, 2015].

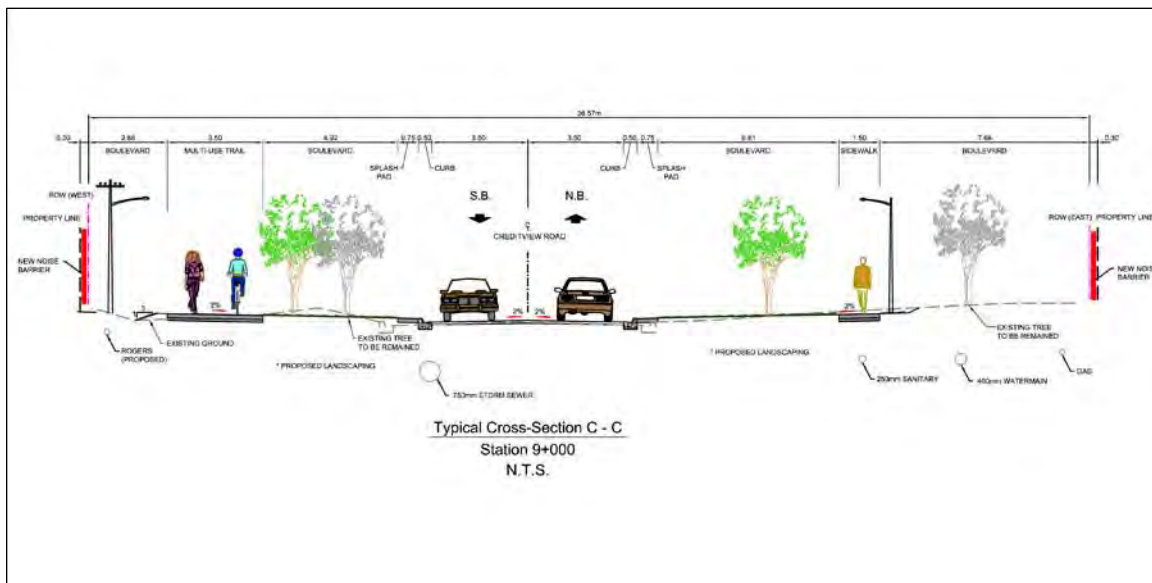


Figure 9. Creditview Road Class EA, Preliminary Preferred Alternative: Typical Cross-Section C-C at Station 9+000 [AECOM, 2015].

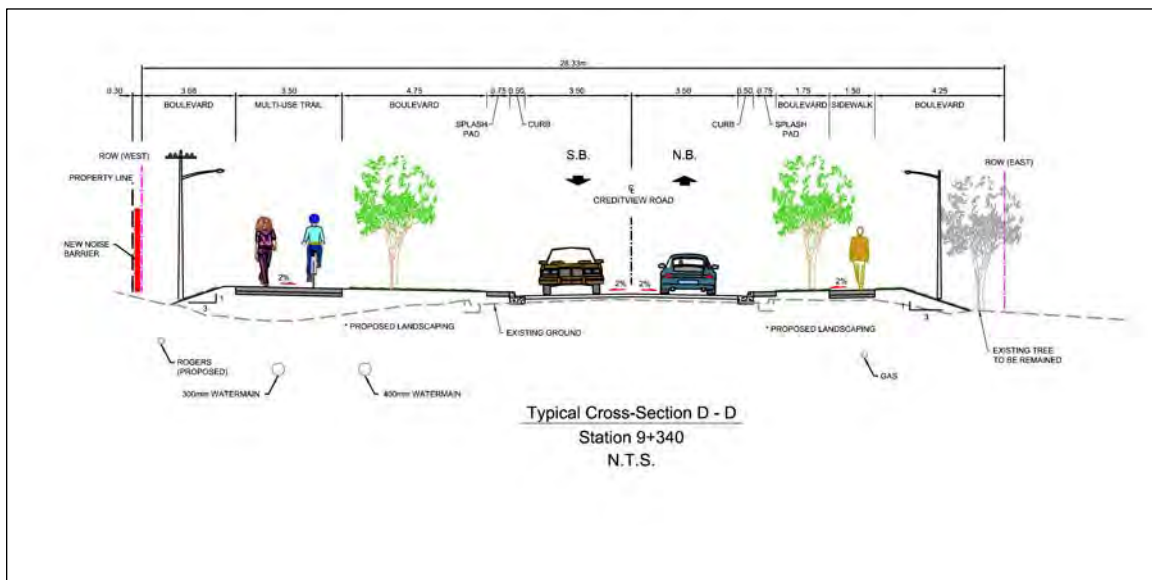


Figure 10. Creditview Road Class EA, Preliminary Preferred Alternative: Typical Cross-Section D-D at Station 9+340 [AECOM, 2015].

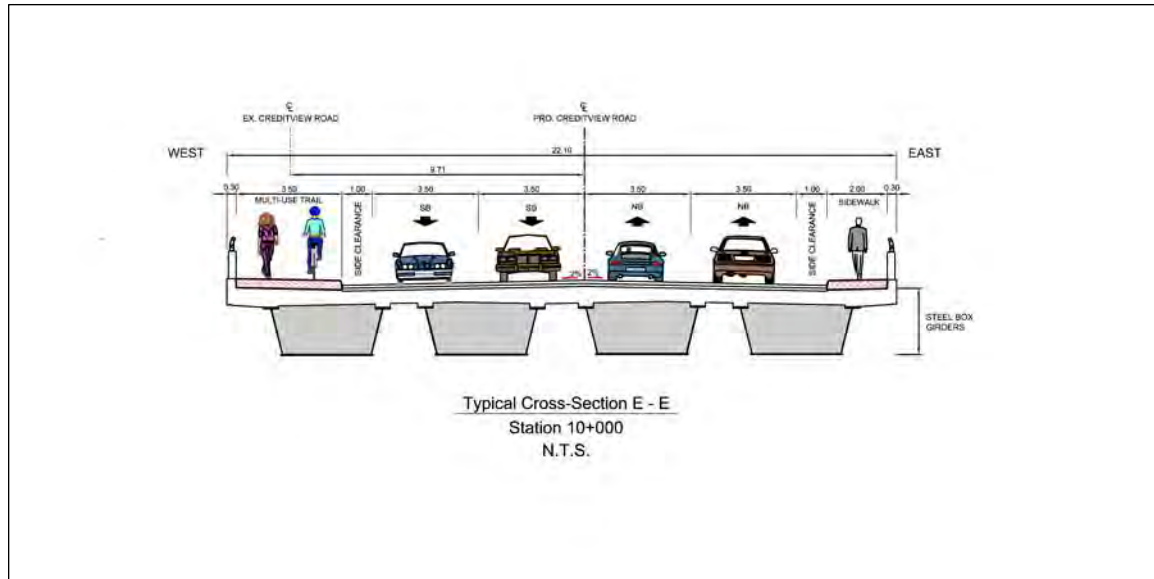


Figure 11. Creditview Road Class EA, Preliminary Preferred Alternative: Typical Cross-Section E-E at Station 10+000 [AECOM, 2015].

6.2 Direct Impacts

One (1) potential direct impact with respect to cultural heritage resources was identified.

- Table 1: Site #8, Creditview Road Underpass

6.3 Indirect Impacts

Three (3) potential indirect impacts in respect to cultural heritage resources were identified.

- Table 1: Site #1, Creditview Road
- Table 1: Sites #2, Credit River Corridor
- Table 1: Site #7, Highway 401

7.0 MITIGATION RECOMMENDATIONS

The proposed undertaking should not adversely affect cultural heritage resources and intervention should be managed in such a way that its impact is sympathetic with the value of the resources. When the nature of the undertaking is such that adverse impacts are unavoidable it may be necessary to implement management or mitigation strategies that alleviate the deleterious effects to cultural heritage resources. Mitigation is the process of causing lessening or negating anticipated adverse impacts to cultural heritage resources and may include, but are not limited to, such actions as avoidance, monitoring, protection, relocation, remedial landscaping, documentation of the cultural heritage landscape and/or built heritage resource if to be demolished or relocated, and salvage of building materials.

Mitigation measures and best management practices will be implemented to address potential impacts. Identified mitigation strategies will be carried through the detailed design as applicable. Refinements and enhancements to the mitigation recommendations will be made as warranted throughout all phases of the project.

Table 2: Potential Impacts and Mitigation Recommendations provides a summary of potential impacts and recommended mitigation measures for each of the identified cultural heritage sites within and adjacent the study area. Table 2 identifies the need to complete cultural heritage evaluation work for those properties of 40 years of age or older, where direct or indirect impacts have been identified.

For properties in private, municipal or regional ownership, the Cultural Heritage Evaluation Reports (CHERs), as needed, will follow the evaluation criteria set out under the 'Ontario Regulation 9/06', which were developed for the purpose of identifying and

evaluating the cultural heritage value or interest of a property proposed for protection under Section 29 of the OHA.

For properties owned or controlled by the Government of Ontario, the CHERs will follow the process set out in Section B, Identification and Evaluation of the *Standards and Guidelines for Conservation of Provincial Heritage Properties* (April 28, 2010). In the case of provincially owned bridges, CHERs, as needed, will be completed under the provisions of the Ontario Heritage Bridge Guideline (OHBG).

Conservation recommendations based on the results of the cultural heritage evaluation will be included in each CHER.

TABLE 2: POTENTIAL IMPACTS AND MITIGATION RECOMMENDATIONS

Site #	Resource Category	Resource Type	Location	Impact	Mitigation
1.	CHL	Transportation: Roadscape	Creditview Road Lots 6-9, Concessions 3 & 4, geographic Township of Toronto City of Mississauga	Indirect: The character of the linear corridor will be changed permanently. The existing attributes of the narrow two-lane roadscape with a tree canopy and grass verges will be removed in certain sections. The roundabouts will introduce a new design layout into the existing linear roadscape. The construction of noise attenuation barriers through a portion of the corridor will alter the appearance and character of the roadway.	Creditview Road is listed in the City of Mississauga <i>Cultural Landscape Inventory</i> (F-TC-3) as a cultural heritage feature and noted in the City's Municipal Heritage Register. Prepare a Cultural Heritage Documentation Report for the municipally recognized cultural heritage landscape. This photographic record will provide a record of the Creditview Road and its context prior to any change in the study area. Develop a sensitive and high quality design for the noise attenuation barriers that is compatible with the character of the roadway. Develop an interpretive plaque to commemorate the CHL.
2.	CHL	Waterscape	Credit River Corridor Lots 6-9, Concessions 3 & 4, geographic Township of Toronto City of Mississauga	Indirect: There will be change to the existing character and setting of the Credit River Corridor due to the introduction of noise attenuation barriers along Creditview Road in proximity to the Creditview River.	The Credit River Corridor is listed in the City of Mississauga <i>Cultural Landscape Inventory</i> (L-NA-2) and included on the City's Municipal Heritage Register. Prepare a Cultural Heritage Documentation Report for the municipally recognized cultural heritage landscape. This photographic record will provide a record of the Credit River and its context prior to any change in the study area. Develop an interpretive plaque to commemorate the CHL.

TABLE 2: POTENTIAL IMPACTS AND MITIGATION RECOMMENDATIONS

Site #	Resource Category	Resource Type	Location	Impact	Mitigation
7.	CHL	Transportation: Roadscape	Highway 401 Lot 9, Concessions 3 & 4, geographic Township of Mississauga City of Mississauga	Indirect: There will be change to the existing character and setting of the Highway 401 corridor as a result of the potential replacement of the Creditview Road Underpass over Highway 401 and the proposed improvements to Creditview Road..	Review during detailed design to confirm the design has not changed in this area. No mitigation anticipated.
8.	BHR	Transportation: Bridge	Creditview Road Underpass Highway 401 Lot 9, Concessions 3 & 4 WHS, geographic Township of Toronto City of Mississauga	Direct: It is anticipated the existing MTO-owned Creditview Road Underpass will be replaced as part of the Highway 401 Expansion Project. A wider structure will be required to accommodate the proposed improvements along Creditview Road. Dating to 1958, the structure relates to the construction of Highway 401 between Highway 10 and Highway 25 and is a potential candidate for evaluation under the Ontario Heritage Bridge Guideline (OHBG) and inclusion on the Ontario Heritage Bridge List.	MTO is required to undertake a CHER of the existing bridge under the OHBG to fulfill the requirements of the MCTS Standards & Guidelines. The City and the Province should co-ordinate any improvements/replacement work to the Creditview Road Underpass to ensure the cultural heritage attributes of the underpass and the Creditview Road Corridor (Site #1) are enhanced. Consider the use of an open metal PL2 handrail design on any new structure to lessen visual impact.

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Contact

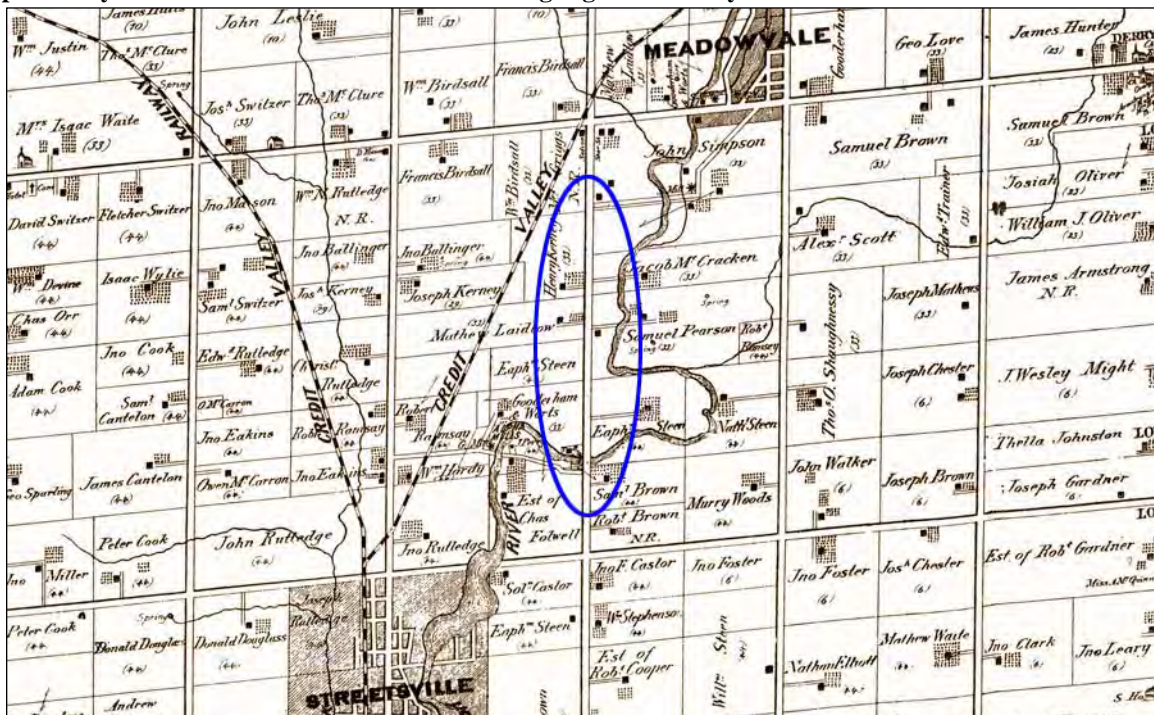
Paula Wubbenhorst, Senior Heritage Coordinator, Community Services Department,
Culture Division, City of Mississauga, July 2015.

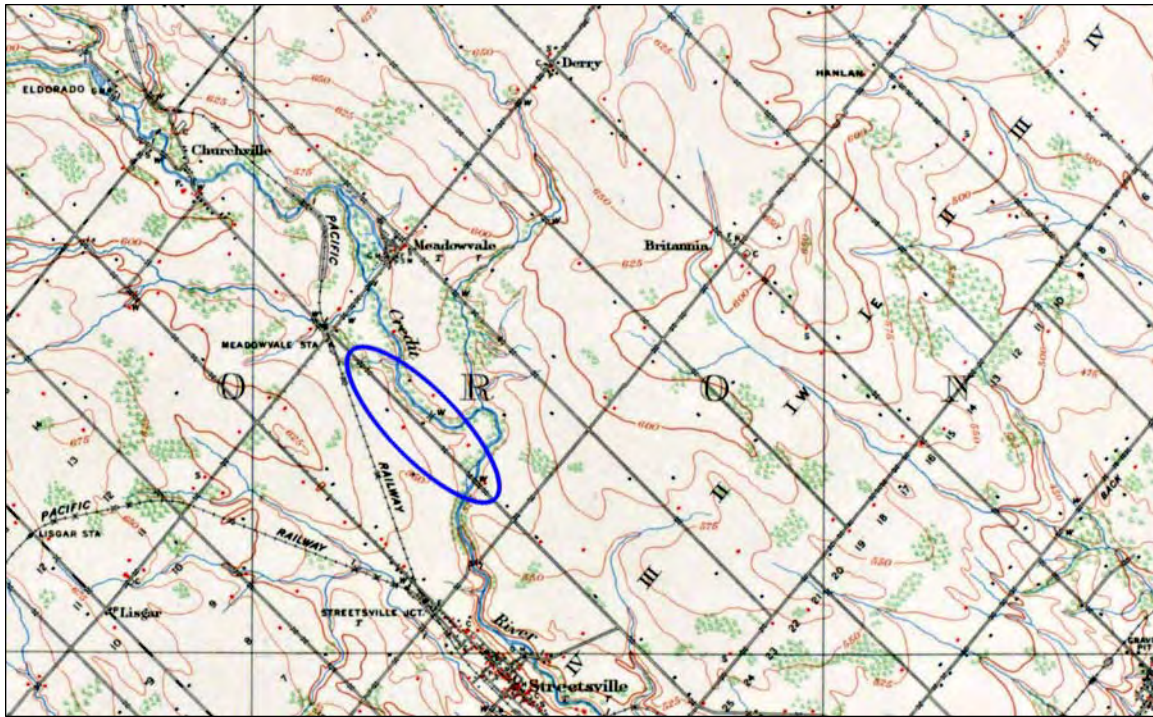
APPENDIX:
Historical Maps and
Aerial Photograph



Tremaine's Map (1859) shows Creditview Road as an open road between Concessions 3 & 4 WHS (highlighted) in the northern part of Toronto Township. The blue oval highlights the study area.

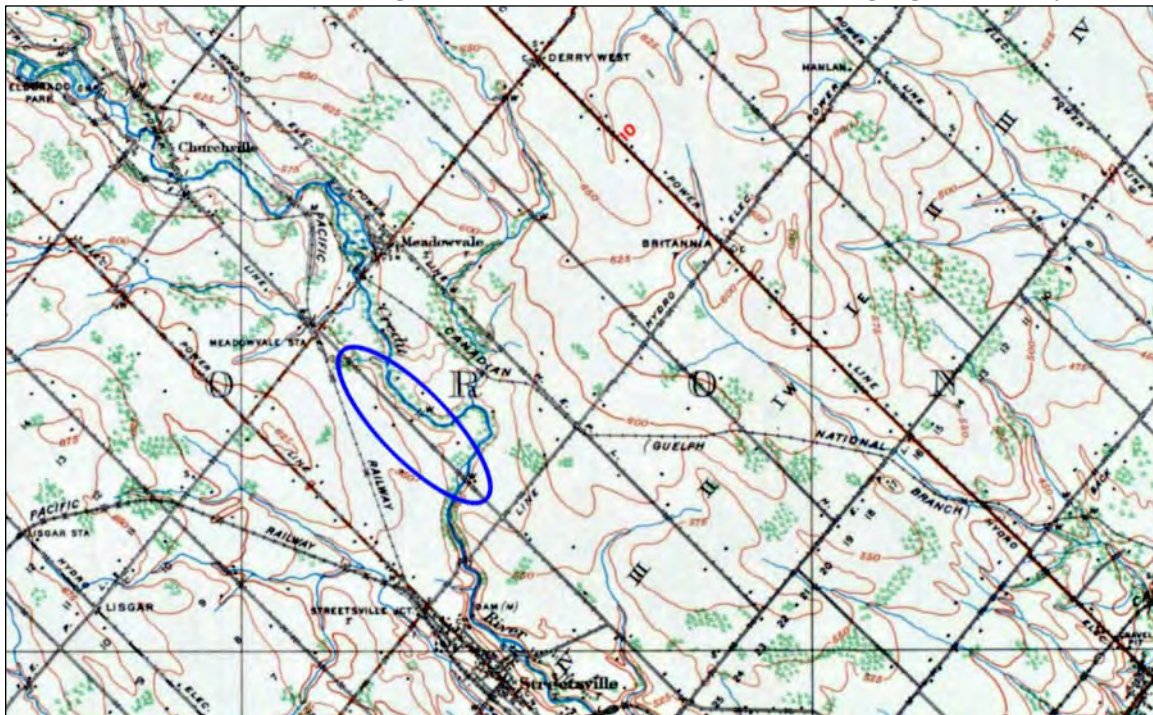
The North Part of Toronto Township map (1877) depicts a well-developed agricultural landscape in proximity to Creditview Road. The blue oval highlights the study area.

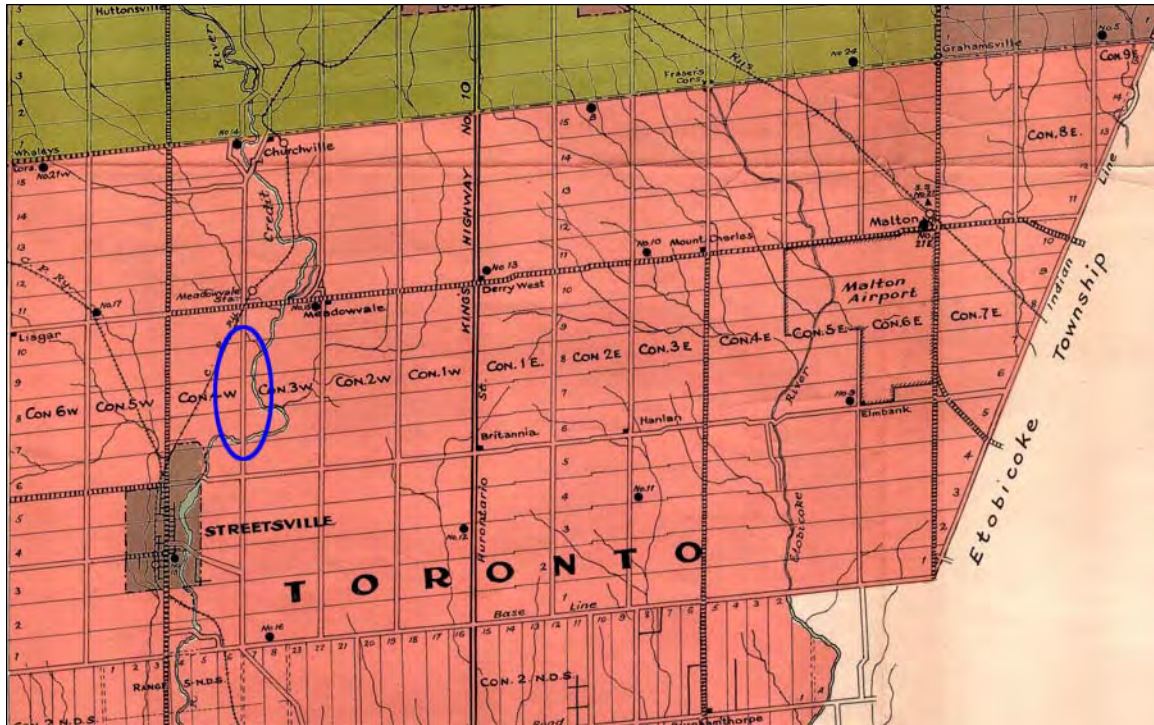




The NTS map 30 M/12 Brampton (1909) depicts the rural landscape in the vicinity of Creditview Road with a wood bridge over the Credit River. The blue oval highlights the study area.

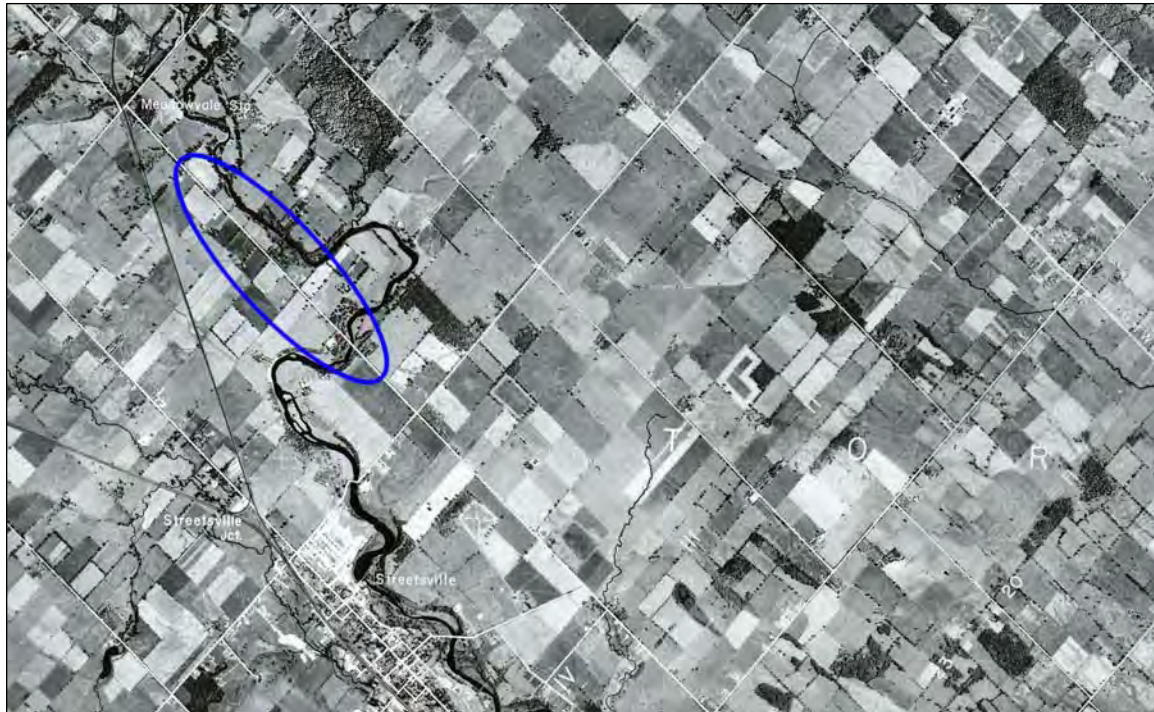
The NTS map 30 M/12 Brampton (1931) continues to show a rural landscape in the vicinity of Creditview Road with a wood bridge over the Credit River. The blue oval highlights the study area.

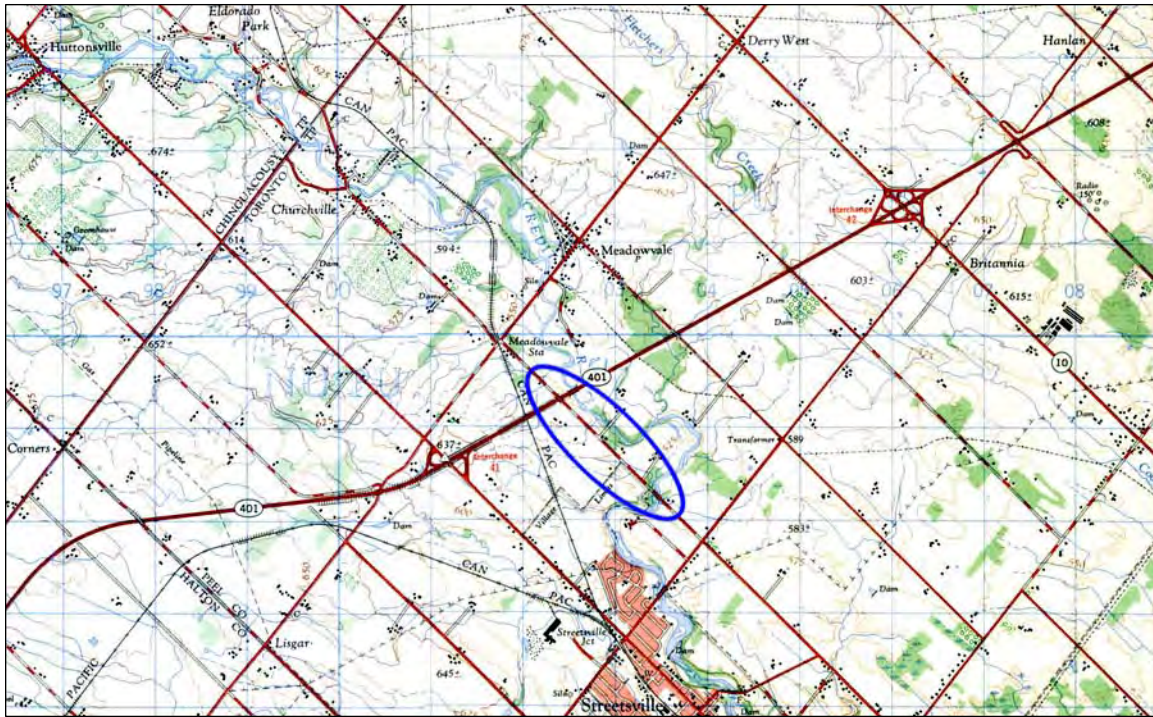




Creditview Road is designated a township road on a Map of Peel County (1953). The blue oval highlights the study area.

An aerial photograph (1954) depicts the rural landscape in proximity to Creditview Road shortly before the introduction of Highway 401. The blue oval highlights the study area.





The NTS map 30 M/12 Brampton (1964) shows Highway 401 traversing Toronto Township with an underpass at Creditview Road. The blue oval highlights the study area.

The NTS map 30 M/12 Brampton (1994) illustrates the development of the Meadowvale South community to the west of Creditview Road. The blue oval highlights the study area.

